

## **Section 16 Application**

**Proposed Underground Vehicular Tunnel (Connecting Lee Garden One and Lee Garden Three) in “Commercial” zone and an area shown as “Road”, Area below Hysan Avenue between Lee Garden One (33 Hysan Avenue) and Lee Garden Three (10 Hysan Avenue) (Tunnel T1)**

**Perfect Win Properties limited and Silver Nicety Company Limited**

**August 2024**

**Executive Summary**  
**(Tunnel T1)**

- S.1 This application is a section 16 application made for an underground vehicular tunnel connecting Lee Garden One and Lee Garden Three on the Causeway Bay Outline Zoning Plan Number S/H6/17. There have been approved s.16 and s.16A applications for the captioned proposed use and location. The original approval letter for the s.16 application No. A/H6/78 is dated 15 July 2016 and the s.16A extension of time application approval letter is dated 23 June 2020. The approval letters are attached in **Appendix 1** and the permission has expired on 24 June 2024.
- S.2 The Tunnel T1 is located between Lee Garden One and Lee Garden Three under a section of Hysan Avenue. The site involves three planning zones; Lee Garden One is zoned “Commercial (2)”, Lee Garden Three is zoned “Commercial”, and Hysan Avenue is zoned as “Road”.
- S.3 This application is to obtain approval for the Tunnel T1 to be implemented. The desire is to have the approval, including the Approval Conditions on the original application, to be extended for an additional 4 years. The applicant is eager to implement the approval, and has made significant progress on discharging the approval conditions and on the lease modification process. The main delay relates to the lease modification process which is yet to be completed.
- S.4 The Planning Statement demonstrates that there have been no material changes to the situation since the application was Approved in 2016, and it would be beneficial to approve the Tunnel T1 Application again. The construction of Tunnel T1 would continue to be of benefit to the area in vehicular traffic terms, for the pedestrians and also for the improvement of the air quality. The construction would not cause disruption to the existing traffic flow, and similarly would not result in any negative impact to the Old and Valuable Trees on Hysan Avenue. The only impacts of the proposal to the traffic, and the public realm in Causeway Bay are public planning gains.

## 行政摘要

(以英文版本為準)

### (T1 隧道)

- S.1 本第 16 條申請旨在希望城市規劃委員會 (城規會) 核准涉及銅鑼灣分區計劃大綱核准圖編號 S/H6/17 上接連利園一期及利園三期的地下行車隧道。上述擬議用途和地點的第 16 條和第 16A 條申請已獲得批准。先前的第 16 條申請編號 A/H6/78 的批准信於 2016 年 7 月 15 日發出，而根據第 16A 條延長期限申請的批准信於 2020 年 6 月 23 日發出，並附在附錄一中。其規劃許可的期限直至 2024 年 6 月 24 日。
- S.2 T1 隧道位於連接利園一期及利園三期的希慎道地底。地盤包括三個規劃地帶：利園一期屬於「商業(2)」，利園三期屬於「商業」，而希慎道則屬於「道路」地帶。
- S.3 此規劃申請旨在獲得申請許可令隧道 T1 得以實施，希望將先前的規劃申請包括履行其附帶條件的期限延長四年。申請人積極實行已核准計劃，並在履行附帶條件和契約修訂方面有顯著進展。延誤的主要原因是契約修訂尚未完成。
- S.4 本規劃綱領闡明白 2016 年核准申請以來，情況並無重大改變，再次核准 T1 隧道的申請能帶來好處。建設 T1 隧道能為區內交通帶來好處，不但方便行人，還能改善空氣質素。有關的建設工程不會影響現行的交通，亦不會為希慎道的珍貴古樹帶來負面影響，涉及交通和公眾的影響只會為公眾帶來規劃增益。

## Contents

	Page
1. Introduction .....	1
2. The Application Site and Surrounding Area .....	1
3. Land Administration .....	2
4. The Planning Context .....	3
5. Purpose of this Application .....	3
6. Background .....	4
7. The Proposal .....	8
8. Implementation .....	9
9. Discharge of Conditions Progress .....	10
10. Planning Assessment and Justification .....	11
11. Conclusion .....	14

## Appendices

1. S.16 Approval Letters and s.16A Extension of Time Approval Letters for Application Number A/H6/78
2. Letter Accepting the Provisional Basic Terms Offer
3. Letter from Buildings Department Rejecting the Tunnel T1 General Building Plans Application
4. A&A Works General Building Plans Approval Letter
5. Location Plan and Sectional Plans
6. Plan showing Excavation Area

## Consultants

Masterplan Limited  
Ove Arup & Partners Hong Kong Limited  
Ronald Lu & Partners (Hong Kong) Limited  
Savills (Hong Kong) Limited  
SYSTRA MVA Asia Limited

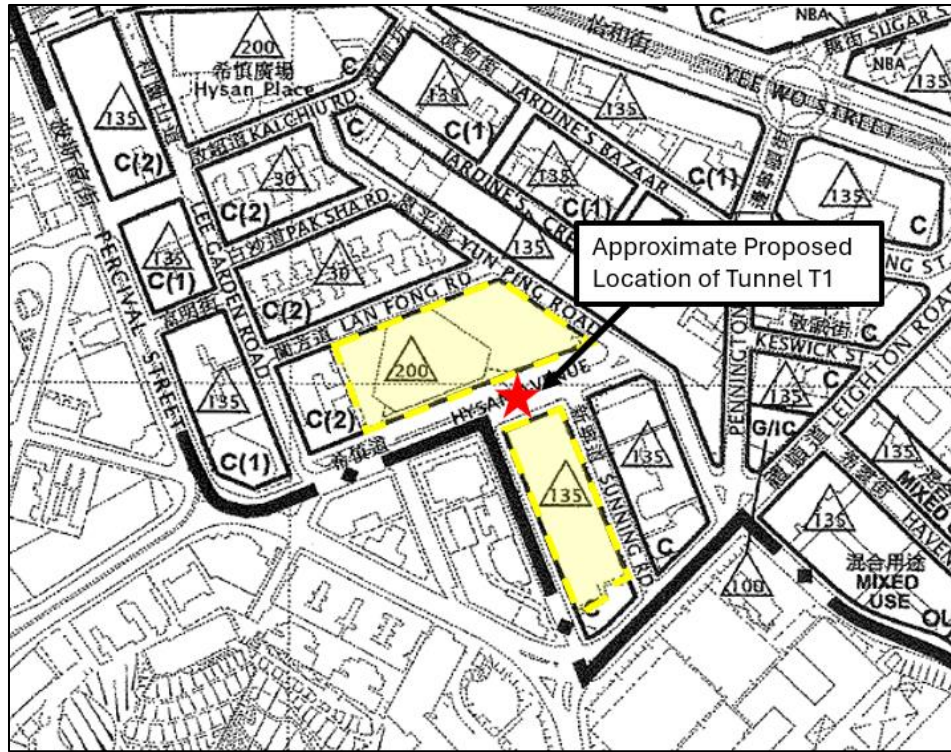
**Section 16 Application**  
**Proposed Underground Vehicular Tunnel (Connecting Lee Garden One and Lee Garden Three) in**  
**“Commercial” zone and an area shown as “Road”, Area below Hysan Avenue between Lee Garden One**  
**(33 Hysan Avenue) and Lee Garden Three (10 Hysan Avenue)**  
**(Tunnel T1)**

**1. Introduction**

- 1.1 This application is a section 16 application made for an underground vehicular tunnel connecting Lee Garden One and Lee Garden Three on the Causeway Bay Outline Zoning Plan Number S/H6/17. There have been approved s.16 and s.16A applications for the captioned proposed use and location. The original approval letter for the s.16 application No. A/H6/78 is dated 15 July 2016 and the s.16A extension of time application approval letter is dated 23 June 2020. The approval letters are attached in **Appendix 1** and the permission has expired on 24 June 2024. Since the s.16A approval letter stated that *“...any further extension of time for commencement of the development would require a fresh application under section 16 of the Town Planning Ordinance”*, this application is prepared.

**2. The Application Site and Surrounding Area**

- 2.1 The application site is shown on **Figure 1**. It is located between Lee Garden One and Lee Garden Three under a section of Hysan Avenue. Hysan Avenue is a two-way road that connects Yun Ping Road to the east and Percival Street to the west and provides access to Sun Wui Road, Hoi Ping Road and Sunning Road. The site is bounded by the basement car park of Lee Garden One to the north and the basement car park of Lee Garden Three to the south. The site involves three planning zones; Lee Garden One is zoned “Commercial (2)”, Lee Garden Three is zoned “Commercial”, and Hysan Avenue is zoned as “Road”.
- 2.2 The two buildings Lee Garden One and Lee Garden Three are owned by the Applicants. Hysan Avenue, which the tunnel will pass under, is a public road.
- 2.3 Causeway Bay is a key decentralized commercial node, as well as, one of the most popular shopping destinations in Hong Kong featuring high end retail spaces. The larger Causeway Bay area also includes a component of residential developments. There is good access to public transport through the MTR, the tram, and a comprehensive bus network. The site involves three planning zones; Lee Garden One is zoned “Commercial (2)”, Lee Garden Three is zoned “Commercial”, and Hysan Avenue is zoned as “Road”.



**Figure 1:** Location Plan (an extract from the Causeway Bay Outline Zoning Plan No S/H6/17)

### 3. **Land Administration**

- 3.1 The vehicle tunnel T1 is proposed underneath Hysan Avenue connecting lots IL 29 s.MM (part) and IL 29 S.L. RP (part) with IL 29 s.J ss.1 RP (part). These form part of the lots on which Lee Garden One and Lee Garden Three are located respectively.
- 3.2 IL 29 is held under a Government Lease for a term of 982 years from 25 June 1860. The Government Lease contains virtually no development restriction save for the non-offensive trade clause, and a range clause (IL 29 s.L be maintained as open space as per a Deed of Covenant dated 18.8.1949).
- 3.3 An application to Lands Department for the Grant of the land for construction is currently in process.

#### 4. **The Planning Context**

“Road”

##### 4.1 The Notes of the Causeway Bay OZP state that:

“In any area shown as ‘Road’, all uses or developments except those specified in paragraph (7) above and those specified below require permission from the Town Planning Board”

The uses specified in Paragraph (7) and (8) of the Notes do not include a vehicular tunnel.

“Commercial” Zone

##### 4.2 In both “Commercial” and “Commercial (2)” the Planning Intention in the Notes is:

“This zone is intended primarily for commercial developments, which may include uses such as office, shop, services, place of entertainment, eating place and hotel, functioning as territorial business/financial centre(s) and regional or district commercial/shopping centre(s). These areas are usually major employment nodes.”

The “Commercial (2)” sub-zone has some additional set back and non-building areas identified, however, none of these relate to the portion of the site fronting onto Hysan Avenue, and therefore has no impact on this application.

#### 5. **Purpose of the Application**

##### 5.1 This application is to obtain approval for the Tunnel T1 to be implemented. The applicant is eager to implement the approval, and has made significant progress on discharging the previous approval conditions and on the lease modification process. The discharge of approval condition progress is discussed in section 9 below.

##### 5.2 As part of Hysan’s desire to further develop Causeway Bay into a high quality commercial and retail node, it is essential that the area functions efficiently and the amenity value of the public realm is increased wherever practical. Improvements in the public realm may be achieved by improving the private/public realm interface, as well as directly improving the public space. Hysan would like to slightly restructure the vehicular traffic flow within the area to reduce the amount of vehicular circulation at ground level. Tunnel T1 partially moves vehicular traffic into an integrated basement. This would provide dual benefits: making the area easier for drivers to locate available car parking spaces without circulating around the blocks: and to further reduce vehicular dominance at the ground level providing a better pedestrian environment.

- 5.3 The purpose of the application is to obtain approval for Tunnel T1 in the “Road” zone between the two buildings to integrate their basements. This would allow drivers to locate the closest car park through entering either building and then using the tunnel to travel to the other building underground if the car park is actually located in the other buildings. Or alternatively to exit through the other building if this reduces the ground level circulation required to leave the area efficiently.

## 6. **Background**

### *The Applicants*

- 6.1 The Applicants are either wholly owned or majority owned subsidiaries of Hysan Development Company Limited (“Hysan”). Hysan is a property development, investment and management company based in Hong Kong. It has an investment property portfolio of approximately 4.5 million square feet of retail, office and residential space, excluding properties under redevelopment. Its roots run deep in Causeway Bay. In 1923, Hysan’s founding Lee family purchased land in Causeway Bay’s East Point Hill and built the Lee Gardens Amusement Park and the Lee Theatre. Since then, the Lee family, and eventually Hysan have continued to develop and manage their significant landholdings in Causeway Bay in a manner that not only creates world class commercial and retail spaces, but also encourages and enables a vibrant public realm. Hysan is also at the forefront of Hong Kong’s sustainable building development.
- 6.2 Completed in late 2017, Lee Garden Three is the newest addition to Hysan’s portfolio of premium commercial buildings in Causeway Bay and has green building features that conform to the highest international sustainability standards. It is comprised of a retail podium and a 21- storey Grade A office tower. This kind of large-scale sustainable building is rare in Hong Kong. The building's many advanced green architectural designs include the clever use of natural lighting and green elements such as largescale vertical green walls, indoor greenery designs, a roof-top garden, and Hong Kong's first sky jogging track in a commercial building.
- 6.3 The vehicular tunnel connection T1 proposed in this application is part of an overall plan of improved connectivity Hysan have for the area. There are the two vehicular Tunnels T1 and T2 which connect the basement car parks of Lee Garden One, Lee Garden Two and Lee Garden Three. This intends to make the at-grade level more pedestrian orientated by reducing vehicular traffic on the at-grade roads. In addition, there are 5 pedestrian link bridges, that are shown in **Figure 2**, and a covered walkway shown in **Figure 3**. The link bridges would provide a sheltered connection from the MTR in Hysan Place and right through to the Caroline Hill Road development that is currently under construction. The gazette for the proposed Pedestrian Link in Causeway Bay has already been published under Section 3(3) of the Roads (Works, Use and Compensation) Ordinance (Chapter 370) on 5 May 2023. While the full benefit will be achieved once all the



pedestrian and vehicular connections are implemented, there are benefits to the general public achieved as each individual connection is implemented.

#### *Tunnel T1 Lease Modification Progress*

- 6.4 Since the original approval in 2016 the applicant applied for lease modification, and has been in discussion with DLO ever since. Originally DLO wanted to bundle the Tunnel T1 lease modification with the “Nil Premium Application of Covered Walkway and Linkbridge System”. However, DLO then accepted to continue processing the Tunnel T1 lease modification separately. It was hoped that separating the Tunnel T1 from the Nil Premium application would speed up the approval process. On 24 February 2023 Hysan accepted the provisional basic terms for Tunnel T1 (see the letter attached in **Appendix 2**). It was hoped that the binding offer of basic terms would be issued prior to the lapsing of the section 16 approval. Currently, relevant government departments are assessing whether the lease modification can be processed as a minor works provision under the Roads (Works, Use and Compensation) Ordinance and be exempt from the gazettal procedures.

#### *Tunnel T1 General Building Plans*

- 6.5 General Building Plans (“GBPs”) were submitted on 28 November 2023 for Tunnel T1. The disapproval letter is included in **Appendix 3**. The GBPs cannot be approved until the lease modification is completed.
- 6.6 The applicant has an A&A Works General Building Plans approved for the modification of the basement carparks to enable the tunnel connection to be implemented once the lease modification has been approved. The GBP Approval letter is included as **Appendix 4**. This is an approval for the tunnel opening and modification to the existing buildings to receive Tunnel T1 and not for the tunnel itself.

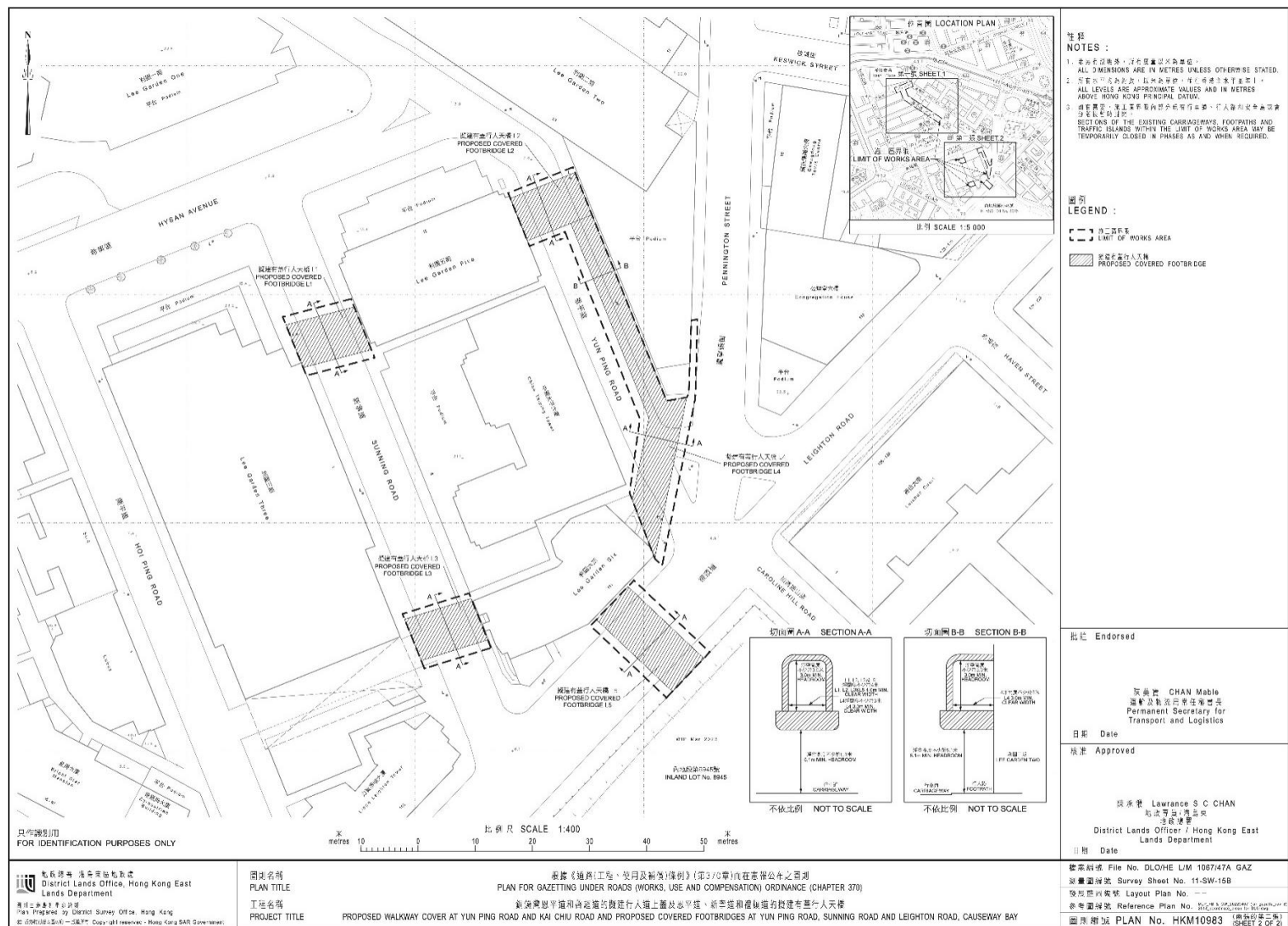


Figure 2: Plan showing the location of the 5 link bridges at Yun Ping Road, Sunning Road and Leighton Road, Causeway Bay.

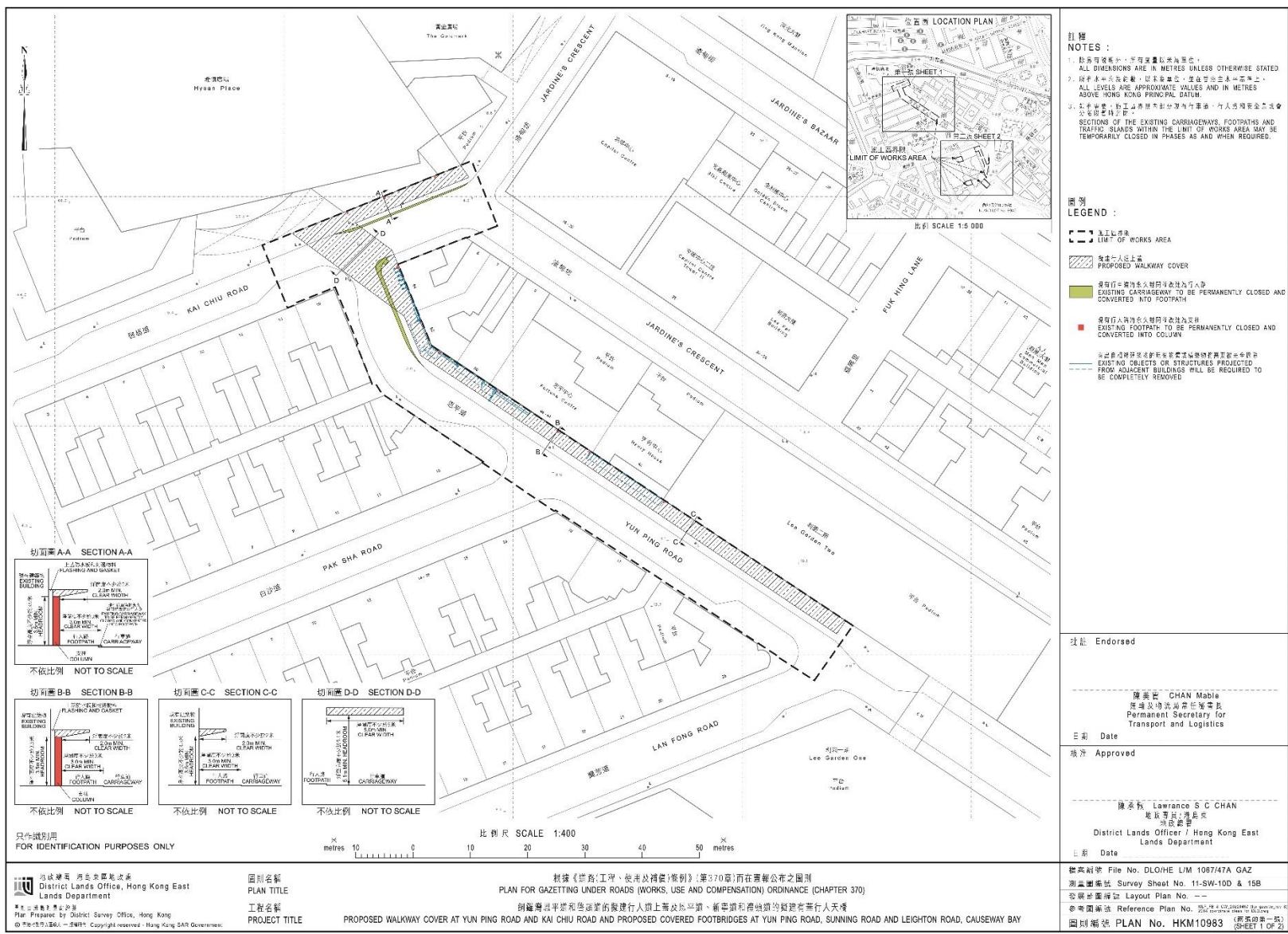


Figure 3: Plan showing the location of the covered walkway at Yun Ping Road and Kai Chiu Road, Causeway Bay.

## 7. The Proposal

- 7.1 The approved permission is for an underground vehicular tunnel, Tunnel T1, connecting the basement car parks of Lee Garden One and Lee Garden Three. The proposed tunnel will become an integral part of the two commercial developments. On the Approved Causeway Bay OZP No. S/H6/17, Tunnel T1 falls within an area mainly shown as “Road” and marginally encroaches onto the “Commercial” (“C”) zone. Commercial development is always permitted within the “C” zone, but planning permission is required in the area shown as “Road”.
- 7.2 The T1 Tunnel will connect the southern side of basement B3 car park of Lee Garden One (at - 3.30mPD) across Hysan Avenue to the northern side of basement B2 car park of Lee Garden Three (at -1.35 mPD) so that the two basement car parks will be integrated as one. Tunnel T1 will have an area of about 381m<sup>2</sup>, a length of approximately 26m, an overall width of approximately 15m, a gradient of about 1:13 and minimum clear headroom of 2.4m. It will provide two lanes for private cars with a minimum width of 3.5m each to serve two-way traffic. An electrical and mechanical zone will be provided at the upper portion of the tunnel above the vehicular access. The location plan, floorplans and sections of the proposed Tunnel T1 are included in **Appendix 5**.
- 7.3 At present, vehicular access to the car park of Lee Garden One can only be made via Lan Fong Road. With Tunnel T1, vehicles from the west (Percival Street and Leighton Road eastbound) can access Lee Garden One through the alternative ingress of Lee Garden Three at Hoi Ping Road to avoid the busy road sections of Yun Ping Road and Lan Fong Road. In addition, the alternative egress at Lee Garden Three allows vehicles to leave Lee Garden One to the east directly from Lee Garden Three to Leighton Road eastbound without the need of circulation at Lan Fong Road, Lee Garden Road and Hysan Avenue.
- 7.4 The car parks at Lee Garden One and Lee Garden Two will provide barrier-free access to the connections of the tunnel to benefit the general public between 7:30am – 12:30am. This is the same as in 2016, when the application was approved.
- 7.5 Tunnel T1 is part of Hysan’s overall plan to improve the connectivity for the area, which also includes the covered walkway and 5 pedestrian link bridges (shown in **Figures 2 and 3**). In addition, there is another approved but not yet constructed Tunnel, T2, under application number A/H6/79 which connects Lee Garden One and Lee Garden Two under Yun Ping Road.
- 7.6 Once both Tunnel T1 and Tunnel T2 are constructed the car parks at Lee Garden One, Lee Garden Two and Lee Garden Three will be interconnected and operate as a single car park. Vehicles from the east (Leighton Road) and the south (Caroline Hill Road) will also be able to access the Lee Garden One car park via the ingress of Lee Garden Two at Pennington Street without passing through the busy road sections of Yun Ping Road and Lan Fong Road.

## 8. Implementation

- 8.1 Tunnel T1 will be constructed by a tunnelling method so that there will be no excavation, nor vertical shaft, on public roads. The Launching Shaft of the tunnel will be formed within the basement of Lee Garden One while the receiving shaft will be formed within the basement of Lee Garden Three so that all the construction work will be conducted within the private premises. This construction method will help to preserve and safeguard the rootballs of the Old and Valuable Trees (“OVTs”) along Hysan Avenue above Tunnel T1. **A plan showing the excavation area is included in Appendix 6.**
- 8.2 Accesses of construction traffic to the underground construction sites will be via the existing car park accesses of Lee Garden One and Lee Garden Three, and therefore new opening of site access on public road is not required. To further minimize traffic impact, the access of construction traffic will be arranged outside the peak periods of 08:00-10:00, 12:00-14:00 and 16:00-20:00.
- 8.3 Besides, the car park operators would dedicate staff to patrol traffic at the car park entrances in order to avoid any obstruction at the car park entrances leading to adverse impact to the operation of public transportation and the traffic on public roads. Furthermore, the loading/unloading activities for the buildings would be well coordinated and managed by the car park operators to avoid such activities being carried out during the peak of the car park hours and/or during the access periods of construction traffic.
- 8.4 In view of the latest status of the project, it is anticipated that the construction of Tunnel T1 would not commence earlier than year 2026. Since the construction of the link bridges and the covered walkway are planned to be completed by year 2026 which will be aligned with construction programme of the Caroline Hill Road development, it is unlikely that the construction of Tunnel T1 would overlap the construction of the link bridges and the covered walkway.

## 9. Discharge of Conditions Progress

9.1 The Applicants have been in the process of discharging the Approval Conditions. **Table 1** below summarizes the current situation regarding the progress made to discharge the conditions. A number of the approval conditions can only be discharged upon implementation and therefore cannot be discharged at this point in the project.

**Table 1 : Current Status of the Discharge of Approval Conditions**

Approval Conditions	Submission to DPO	Decision / Comments
(a) “no excavation works on public roads for construction of the proposed tunnel is allowed”	-	<i>This can only be discharged upon implementation.</i>
(b) “the submission and implementation of a Construction Traffic Management Scheme to the satisfaction of the Commissioner for Transport (C for T) or of the TPB”	2 Feb 2024	Comments from LCSD received on 20 Feb 2024, Comments from TD received on 23 Feb 2024.  <i>The comments are being reviewed, and the responses will be submitted upon further detailed study.</i>
(c) “the provision of ingress/egress of the carparks to the satisfaction of the Commissioner for Transport or of the TPB”	-	<i>This can only be discharged upon implementation.</i>
(d) “the submission and implementation of a car parking layout to the satisfaction of the Commissioner for Transport or the TPB”	28 March 2024	Comments from TD received on 11 April 2024.  <i>A response to these comments is currently being prepared.</i>
(e) “the submission of a tree protection proposal for the Old and Valuable Trees (OVTs) on Hysan Avenue before commencement of construction	22 Feb 2024	Condition (e) is considered complied with on 17 June 2024.

works to the satisfaction of the Director of Leisure and Cultural Services or of the TPB”	19 Apr 2024 Revised TPP + RtoC	Comments from LCSD received on 9 May 2024  <i>A response to these comments is currently being prepared.</i>
(f) “the implementation of a tree protection proposal for the OVTs on Hysan Avenue to the satisfaction of the Director of Leisure and Cultural Services or of the TPB”	-	<i>This can only be discharged upon implementation.</i>
(g) “the submission of monthly tree monitoring reports for the OVTs at least 3 months before the commencement of construction works for the proposed tunnel until 12 months after works completion to the satisfaction of the Director of Leisure and Cultural Services or of the TPB”	-	<i>This can only be discharged upon implementation.</i>
(h) “the provision of fire service installations and water supplies for fire fighting to the satisfaction of the Director of Fire Services of the TPB”	-	<i>This can only be discharged upon implementation.</i>

## 10. **Planning Assessment and Justification**

- 10.1 While the full justifications were made in the original application (2016), this section will summarize the main planning justifications and considerations.

### *At-grade Traffic Improvements*

- 10.2 Lan Fong Road is a one-way local road which serves not only as the main vehicular access to Lee Garden One, but also for a number of green minibus (“GMB”) stands, as well as on street loading and unloading. Lan Fong Road is also a major route for vehicles travelling from Leighton Road westbound to the Sharp Street East area. It is common for the existing GMB stands and on-street loading and unloading areas to be fully occupied by GMBs and illegal parking. This results in pick-up and drop-off activities taking place on the carriageway and accordingly frequently vehicles tailing back from Lan Fong Road to its junction with Yun Ping Road. As a consequence, vehicular access to Lan Fong Road, Pak Sha Road and Kai Chiu Road and Sharp Street East Area will occasionally be obstructed.

10.3 A summary of the proposed Tunnel T1 benefits is:

- a) Reduced at-grade traffic on local roads – the Traffic Impact Assessment (TIA) for the approved Application No. A/H6/78 has concluded that Tunnel T1 would reduce the traffic flows along Hysan Avenue, Yun Ping Road and Lan Fong Road. There would be a positive traffic impact to the local road network from the development of Tunnel T1;
- b) Efficient use of available car parking spaces – the available car parking spaces at the two car parks can be shared used upon linking up by the Tunnel T1, by which the utilisation of available car parking spaces can be maximised while the way-finding of vacant spaces can be taken place internally, therefore minimizing the queuing at car park entrances as well as the traffic circulation on the at-grade roads;
- c) Balanced use of roads/junctions' capacities – the proposed tunnel will provide route choices for drivers to enter or leave the car parks in different traffic situations on the surrounding roads. This will result in a more balanced use of the spare capacities of the surrounding roads and junctions; and
- d) Provides flexibility in access and traffic management – Tunnel T1 would enable alternative ingress and egress points for the car parks, which allows flexibility for the car park operators to manage ingress and egress of vehicles. It also provides more opportunities to implement short-term/temporary traffic management schemes for special events/traffic situations.

*Improved Public Realm*

- 10.4 The existing two individual car parks will be linked up by the proposed Tunnel T1 to form an integrated car park. This makes the area easier for drivers to access the car parks and locate available car parking spaces without circulating around the blocks, while simultaneously reducing the vehicular dominance at the ground level. As a result, this creates a better pedestrian environment.
- 10.5 Due to the reduction of vehicles at grade, it would allow pedestrians to enjoy a higher quality pedestrian environment. The reduction of cars on streets with heavy pedestrian usage, such as Yun Ping Road, would build upon the pedestrian areas already existing in the area to further enhance the local pedestrian environment and improve the public realm.



#### *Improved Air Quality*

- 10.6 With the proposed tunnel T1 cars can locate a car park quicker and less time will be spent at-grade circling around the area. This will help to reduce traffic emissions. In addition, the reduction in vehicles should translate into a more reasonable speed of traffic flow at grade which will reduce the overall emission levels.
- 10.7 Causeway Bay has been identified as an area where there is a need to improve the air quality and as a result it is one of the three Franchised Bus Low Emission Zones. This shows the importance of identifying ways to improve the air quality in Causeway Bay and implementing them.

#### *No Impact to the Old and Valuable Trees on Hysan Avenue*

- 10.8 The protection of the Old and Valuable Trees (“OVTs”) on Hysan Avenue has been carefully considered in this proposal. The construction work will be carried out inside the basements of the connecting buildings and will not require any above ground works, or road closure. As the permanent structure of Tunnel T1 will be at least 5 meters below the ground it will not affect the OVTs as the roots of the OVTs do not go further than 2 meters below ground. The extent of the roots of the OVTs are currently being surveyed and will be established in detail in advance of the commencement of any works. This will ensure that there is no disruption to the OVTs.

#### *No Impact to Traffic During Construction*

- 10.9 Construction vehicles will access Lee Garden One from the vehicular access on Lan Fong Road. The maximum construction traffic of the two tunnels under this application and Application No. A/H6/79 will be approximately 14 vehicles per day. Access of construction vehicles will be arranged during off-peak time periods to avoid overlapping with the peak traffic in the surrounding area during 08:00-10:00, 12:00-14:00 and 16:00-20:00. The car park operators would dedicate staff to patrol traffic at the car park entrances while the loading/unloading activities for the buildings would be well coordinated and managed by the car park operators throughout the construction period. Appropriate internal temporary traffic arrangements will also be implemented within the car parks of both Lee Garden One and Lee Garden Three in order to maintain the operation of the majority of the car parking spaces during the construction period.

#### *No Impact to the Surrounding Buildings, Structures, Land, Streets or Services*

- 10.10 The proposed Tunnel T1 will not cause any damage to the adjacent buildings, structures, land, streets or services. Precautionary measures such as ground investigation along Hysan Avenue will be conducted before construction. During construction, regular monitoring of the settlement, tilting, vibration and ground water drawdown at the adjacent ground, utilities and buildings will be provided at the detailed technical design submission stage.

- 10.11 Based on a utilities survey, the lowest level of underground utilities is approximately 1.8 meter below the ground level, and the structure surface level of Tunnel T1 is approximately 5m below ground level. Therefore, it is unlikely that there will be any disturbance or disruption to the underground utilities.

*Air Quality Levels Inside the Car Parks*

- 10.12 The Tunnel T1 will be designed with supply and exhaust ventilating fans to ensure that sufficient outdoor air is drawn into, and polluted air is extracted from, the car parks and Tunnel T1. Local jet fans and carbon monoxide sensors will be installed to enhance the local ventilation effectiveness and to monitor the concentration of carbon monoxide. This will ensure the air quality is high in the Tunnel T1 and the car parks.

*No Adverse Noise Impact*

- 10.13 The noise disruption to nearby noise sensitive receivers from road traffic will be reduced. Construction noise is expected to be minimal, due to the works exclusively taking place underground. In addition, appropriate mitigation measures such as quiet powered mechanical equipment and movable barriers will be used to ensure that noise can be reduced to acceptable levels. It will be ensured that good site practice and noise management will be adopted to mitigate noise impact. Should it arise that noise impact to nearby noise sensitive receivers are identified, then remedial measures such as additional acoustic treatment or enclosure will be formulated.

11. **Conclusion**

- 11.1 The Planning Statement demonstrates that there have been no material changes to the situation since the application was Approved in 2016, and it would be beneficial to approve the Tunnel T1 Application again. The construction of Tunnel T1 would continue to be of benefit to the area in vehicular traffic terms, for the pedestrians and also for the improvement of the air quality. The construction would not cause disruption to the existing traffic flow, and similarly would not result in any negative impact to the Old and Valuable Trees on Hysan Avenue. The only impacts of the proposal to the traffic, and the public realm in Causeway Bay are public planning gains.
- 11.2 The applicant has taken significant steps to implement the approved development. The main delay relates to the lease modification process which is yet to be completed. The TPB is requested to consider the application on these merits and to decide in favour of Approving the application again.

# **Appendix 1**

**S.16 Approval Letter and s.16A Extension of  
Time Approval Letter for Application Number  
A/H6/78**

城市規劃委員會

香港北角渣華道三百三十三號  
北角政府合署十五樓

TOWN PLANNING BOARD

15/F., North Point Government Offices  
333 Java Road, North Point,  
Hong Kong.

傳 真 Fax: 2877 0245 / 2522 8426

電 話 Tel: 2231 4810

來函檔號 Your Reference:

覆函請註明本會檔號

In reply please quote this ref.: TPB/A/H6/78

By Registered Post & Fax (25877068)

15 July 2016

Masterplan Ltd.  
Room 3516B, 35/F, China Merchants Tower  
Shun Tak Centre  
200 Connaught Road, Central, Hong Kong  
(Attn: Kira Brownlee)

Dear Sir/Madam,

**Proposed Underground Vehicular Tunnel (Connecting Lee Garden One and the Sunning Plaza/Sunning Court Redevelopment) in "Commercial" zone and an area shown as 'Road', Area below Hysan Avenue between Lee Garden One (33 Hysan Avenue) and 10 Hysan Avenue (the Sunning Plaza/Sunning Court Redevelopment)**

I refer to my letter to you dated 20.6.2016.

After giving consideration to the application, the Town Planning Board (TPB) approved the application for permission under section 16 of the Town Planning Ordinance on the terms of the application as submitted to the TPB. The permission shall be valid until 24.6.2020; and after the said date, the permission shall cease to have effect unless before the said date either the development hereby permitted is commenced or the permission is renewed. The permission is subject to the following conditions :

- (a) no excavation works on public roads for construction of the proposed tunnel is allowed;
- (b) the submission and implementation of a Construction Traffic Management Scheme to the satisfaction of the Commissioner for Transport (C for T) or of the TPB;
- (c) the provision of ingress/egress of the car parks to the satisfaction of C for T or of the TPB;
- (d) the submission and implementation of a car parking layout to the satisfaction of C for T or of the TPB;
- (e) the submission of a tree protection proposal for the Old and Valuable Trees (OVTs) on Hysan Avenue before commencement of construction works to the satisfaction of the Director of Leisure and Cultural Services (DLCS) or of the TPB;



- (f) the implementation of a tree protection proposal for the OVTs on Hysan Avenue to the satisfaction of the DLCS or of the TPB;
- (g) the submission of monthly tree monitoring reports for the OVTs at least 3 months before the commencement of construction works for the proposed tunnel until 12 months after works completion to the satisfaction of the DLCS or of the TPB; and
- (h) the provision of fire service installations and water supplies for fire fighting to the satisfaction of the Director of Fire Services or of the TPB.

The TPB also agreed to advise you to note the advisory clauses as set out at Appendix V of the TPB Paper.

If you wish to seek an extension of the validity of this permission, you may submit an application to the TPB for renewal of the permission no less than six weeks before its expiry. This is to allow sufficient time for processing of the application in consultation with the concerned departments. The TPB will not consider any application for renewal of permission if the time limit for commencement of development specified in the permission has already expired at the time of consideration by the TPB. Please refer to the TPB Guidelines No. 35B and 36A for details. The Guidelines and application forms are available at the TPB's website ([www.info.gov.hk/tpb/](http://www.info.gov.hk/tpb/)), the Planning Enquiry Counters (PECs) of the Planning Department (Hotline : 2231 5000) at 17/F, North Point Government Offices, 333 Java Road, North Point; 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin; and the Secretariat of the TPB at 15/F, North Point Government Offices.

For amendments to the approved scheme that may be permitted with or without application under section 16A, please refer to TPB Guidelines No. 36A for details.

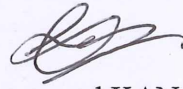
A copy of the TPB Paper in respect of the application (except the supplementary planning statement/technical report(s), if any) and the relevant extract of minutes of the TPB meeting held on 24.6.2016 are enclosed herewith for your reference.

Under section 17(1) of the Town Planning Ordinance, an applicant aggrieved by a decision of the TPB may apply to the TPB for a review of the decision. If you wish to seek a review, you should inform me within 21 days from the date of this letter (on or before 5.8.2016). I will then contact you to arrange a hearing before the TPB which you and/or your authorized representative will be invited to attend. The TPB is required to consider a review application within three months of receipt of the application for review. Please note that any review application will be published for three weeks for public comments.

This permission by the TPB under section 16 of the Town Planning Ordinance should not be taken to indicate that any other government approval which may be needed in connection with the development, will be given. You should approach the appropriate government departments on any such matter.

If you have any queries regarding this planning permission, please contact Miss Irene Lai of Hong Kong District Planning Office at 2231 4935. In case you wish to consult the relevant Government departments on matters relating to the above approval conditions, a list of the concerned Government officers is attached herewith for your reference.

Yours faithfully,

A handwritten signature in dark ink, appearing to be 'Raymond KAN', written in a cursive style.

( Raymond KAN )  
for Secretary, Town Planning Board

RK/DY/syl

**規 劃 署**

港島規劃處  
香港北角渣華道 333 號  
北角政府合署 14 樓

**Planning Department**

Hong Kong District Planning Office  
14/F, North Point Government Offices,  
333 Java Road, North Point,  
Hong Kong

本函檔號	Your Reference		
本署檔號	Our Reference	TPB/A/H6/78-1 (EOT)	Please quote our reference
電話號碼	Tel. No. :	2231 4930	in future correspondence
傳真機號碼	Fax No. :	2895 3957	

**BY POST & FAX (2587 7068)**

23 June 2020

Masterplan Limited  
Room 3516B, 35/F,  
China Merchants Tower, Shun Tak Centre,  
200 Connaught Road Central, Hong Kong  
(Attn: Ms. Kira BROWNLEE)

Dear Sir/Madam,

**Application for Permission  
To Extend the Time for Commencement of Development**

**Proposed Underground Vehicular Tunnel (Connecting Lee Garden One and  
Lee Garden Three, Former Sunning Plaza)  
in "Commercial" Zone and an area shown as 'Road',  
Area below Hysan Avenue between Lee Garden One  
(33 Hysan Avenue) and Lee Garden Three (10 Hysan Avenue)  
(Section 16A Application No. A/H6/78-1)**

I refer to your application of 27.5.2020 which was received by the Town Planning Board (TPB) on 29.5.2020, to extend the time for commencement of the above approved development proposal under Application No. A/H6/78.

After giving consideration to your application, the Director of Planning, under the delegated authority of the TPB, approved your application for amendment to permission under section 16A of the Town Planning Ordinance on the terms of the application as submitted to the TPB. The amended permission is subject to the following conditions and shall be valid until 24.6.2024; and after the said date, the permission shall cease to have effect unless before the said date either the development hereby permitted is commenced or the permission is renewed:

- (a) no excavation works on public roads for construction of the proposed tunnel is allowed;
- (b) the submission and implementation of a Construction Traffic Management Scheme to the satisfaction of the Commissioner for Transport (C for T) or of the TPB;
- (c) the provision of ingress/egress of the carparks to the satisfaction of C for T or of the TPB;

.../2

- 2 -

- (d) the submission and implementation of a car parking layout to the satisfaction of C for T or the TPB;
- (e) the submission of a tree protection proposal for the Old and Valuable Trees (OVTs) on Hysan Avenue before commencement of construction works to the satisfaction of the Director of Leisure and Cultural Services (DLCS) or of the TPB;
- (f) the implementation of a tree protection proposal for the OVTs on Hysan Avenue to the satisfaction of the DLCS or of the TPB;
- (g) the submission of monthly tree monitoring reports for the OVTs at least 3 months before the commencement of construction works for the proposed tunnel until 12 months after works completion to the satisfaction of the DLCS or of the TPB; and
- (h) the provision of fire service installations and water supplies for fire fighting to the satisfaction of the Director of Fire Services or of the TPB.

The advisory clauses as advised per our letter dated 15.7.2016 are still valid and attached with this letter for your information.

This permission will expire on **24.6.2024**. According to the TPB Guidelines No. 35C, any extension of time for commencement of development shall not result in an aggregate extension period longer than the original duration for commencement of the approved development proposal, i.e. 4 years for application No. A/H6/78. As such, you are reminded to **strictly** adhere to the time limit for commencement of the approved development. Any further extension of the validity of this permission will be outside the scope of Class B amendments. If you wish to seek any further extension of time for commencement of the development, you may submit a fresh application under section 16 of the Town Planning Ordinance. You are also reminded that submissions for compliance with the planning approval conditions should be made as soon as possible.

Under section 17(1) of the Town Planning Ordinance, an applicant aggrieved by a decision of the TPB may apply to the TPB for a review of the decision. If you wish to seek a review, you should inform the Secretariat of the TPB (15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong – Tel: No. 2231 4810 or 2231 4835 and Fax. No. 2877 0245 or 2522 8426) within 21 days from the date of this letter (on or before 14.7.2020). The Secretariat of the TPB will then contact you to arrange a hearing before the TPB which you and/or your authorized representative will be invited to attend.

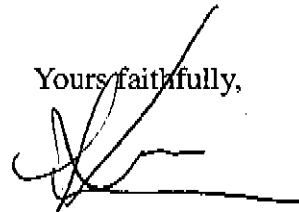
.../3



- 3 -

If you have any queries regarding this permission, please contact Mr. T.W. NG of the Hong Kong District Planning Office at 2231 4935.

Yours faithfully,

A handwritten signature in black ink, appearing to be 'Louis K. H. KAU', written over a horizontal line.

( Louis K. H. KAU )

for and on behalf of Director of Planning

Encl.

**Appendix V of  
MPC Paper No. A/H6/78A**

**Advisory Clauses**

- (a) to note the comments of the District Lands Officer/Hong Kong East, Lands Department (LandsD) in paragraph 7.1.1 of the paper regarding the need for prior written approval for any trees affected, the applicants should confirm whether the proposed tunnel will be opened for public use and the opening hours, the need for application for proper land document and variation/extinguishment of the Deed of Covenant dated 18.8.1949 to effect the proposed development and to note the gazettal requirements under the Roads (Works, Use and Compensation) Ordinance Cap. 370;
- (b) to note the advice of the Secretary for Housing and Transport in Appendix III of the paper in respect of the Roads (Works, Use and Compensation) Ordinance Cap. 370;
- (c) to note the comments of the Chief Building Surveyor/Hong Kong East and Heritage, Buildings Department in paragraph 7.1.3 of the paper regarding the requirements under Buildings Ordinance and submission of structural plans to the Building Authority for approval;
- (d) to note the detailed requirements of the Commissioner for Transport in paragraph 7.1.4(a) of the paper that the Construction Traffic Management Scheme under approval condition (b) shall include but not limit to that the access of construction vehicles to the car park of Lee Garden One should only be arranged during the off-peak time periods of 1000-1200 hours and 1400-1600 hours on weekdays, a maximum of 14 construction vehicle trips (round trips) per day and not exceeding 6 vehicle trips (round trips) per hour;
- (e) to note the comments of the Commissioner of Police in paragraph 7.1.5(b) of the paper on the need to take appropriate measures to minimise traffic obstruction or congestion and noise nuisance;
- (f) to note the comments of the Chief Highway Engineer/Hong Kong, Highways Department in paragraph 7.1.6 of the paper that sufficient ground cover should be provided for the proposed tunnel and to consult relevant utility undertakers for any necessary diversion of existing underground utilities;
- (g) to note the comments of the Chief Architect/Central Management Division 2, Architectural Services Department in paragraph 7.1.8(b) of the paper to follow up with relevant Government departments on GFA and land ownership issues;
- (h) to note the comments of the Hong Kong East Tree Team of the Leisure and Cultural Services Department in paragraph 7.1.9(c) to (e) of the Paper on the information to be included in the tree protection proposal under planning condition (c) and for tree monitoring purpose. The applicants are reminded that any work inside or near the Old and Valuable Tree (OVT) tree protection zone that would affect the OVTs shall be approved by LandsD according to ETWB TC(W) No. 29/2004, and that works concerning the 5 OVTs shall be agreed by LCSD;
- (i) to note the comments of the Chief Town Planner/Urban Design and Landscape, Planning Department in paragraph 7.1.10(c) of the paper to routinely monitor the

condition of the OVTs and precaution should be taken to avoid any impact to the underground water level;

- (j) to note the comments of the Director of Environmental Protection in paragraph 7.1.11(b) of the paper to consider using non-percussive method in demolishing slabs on basement floors for the construction of launching and receiving shafts to reduce the ground-borne construction noise impact, and to strictly comply with relevant legislation/guidelines for proper management and disposal of waste generated by the proposed development;
- (k) to note the comments of the Head of Geotechnical Engineering Office (GEO), Civil Engineering and Development Department in paragraph 7.1.12 of the paper that the detailed technical proposal on the design and construction methods of the proposed tunnel, as well as precautionary measures should be submitted for GEO's consideration through BD at the building plan submission stage; and
- (l) to note the comments of the Chief Engineer/Construction, Water Supplies Department (WSD) in paragraph 7.1.13 of the paper regarding the need to allow free access by WSD to carry out operation and maintenance of the existing water mains, submit diversion proposal, if any, for WSD's approval, water mains diversion works shall be carried out at the applicants' own cost, the waterworks reserve requirement within the application site, and coordinate with WSD on the construction programme of the proposed development before commencement of works.

# **Appendix 2**

## **Letter Accepting the Provisional Basic Terms Offer**

**[ Dated : 24 February 2023 ]**



**District Lands Office / Hong Kong East**

19/F, Southorn Centre  
130 Hennessy Road  
Wanchai  
Hong Kong

**Attention: Ms. Carrie Ng**

24 February 2023

**Your Ref: (57) in LD DLO/HE 1067/47A MOD IX**

Our Ref : PS/2019/VPS/0306/L072(T1)/WAC/HIN/ENL/bs

Walter Cheung  
E: wacheung@savills.com.hk  
DL: (852) 2840 4698  
F: (852) 3007 0530

Room 1208,  
1111 King's Road,  
Taikoo Shing, Hong Kong

EA LICENCE: C-023750  
T: (852) 2801 6100  
savills.com

**BY FAX AND BY POST  
(FAX NO.: 2834 4324)**

Dear Madam,

**RE: PROPOSED LEASE MODIFICATION FOR  
PROPOSED TUNNEL T1 LINKING LEE GARDEN ONE AND LEE GARDEN THREE**

We refer to your letter dated 10 February 2023 regarding the provisional basic terms offer of the captioned.

We hereby confirm that our client accepts the provisional basic terms offer in the abovementioned letter.

If you have any questions or wish to discuss, please do not hesitate to contact the undersigned at 2840 4698 or our Mr. C H Kwok at 2842 4571 / Mr. Enoch Lee at 2840 4685.

Yours faithfully,  
For and on behalf of

**Savills Valuation and Professional Services Limited**

**Walter Cheung**  
MHKIS RPS (GP)  
Director

c.c. Client

# **Appendix 3**

**Letter from Buildings Department Rejecting  
the Tunnel T1 General Building Plans  
Application**

CHEUNG Man Ching Anthony  
33rd Floor,  
Wu Chung House,  
213 Queen's Road East,  
Wanchai, Hong Kong

18 January 2024

88 255

RONALD LU  
& PARTNERS

22 JAN 2024

RECEIVED  
(1)

Dear Mr Cheung,

**33 Hysan Avenue, Hong Kong (The Lee Garden One) –  
I.L. 29 s.DD, s.MM & s.L R.P. and I.L. 457 s.L & R.P.  
and  
10 Hysan Avenue, Hong Kong - I.L. 29 s.J R.P., s.J ss.1 R.P. and s.J ss.2**

I refer to your application received on 20 November 2023 for approval of proposals in respect of BUILDING (ALTERATIONS & ADDITIONS).

2. Your submission of plans has been checked under the curtailed check system announced in Practice Note for Authorized Persons, Registered Structural Engineers and Registered Geotechnical Engineers ADM-19. On this basis, the submission has been found to be fundamentally unacceptable and I hereby disapprove the submission for the reasons detailed in paragraph 7 of this letter.

3. You are reminded that the curtailed check system covers only the fundamental issues of a building proposal. Although non-fundamental issues will not be raised as reasons for disapproving a submission, I expect that all contraventions of the Buildings Ordinance (BO) and its subsidiary legislation are rectified as and when they are discovered and in any event, before completion of the works is certified. In this connection, I ask you to note that the Building Authority (BA) attaches great importance to the proper assumption of duties and responsibilities by authorized persons, registered structural engineers and registered geotechnical engineers.

4. Your client has been sent a copy of this letter but I would request that you ensure that the contents are understood by him.

5. With the exception of one set of plans which I will retain for record purposes, the remaining copies of your submission are returned herewith.

6. You are advised that under section 44(1) of the BO, any person aggrieved by a decision made by the BA may appeal from that decision. Should your client wish to appeal against my decision in this letter, please refer to the provisions of Part VI of the BO. In particular, please note that a Notice of Appeal should be served by your client in writing directly on the Secretary to the Appeal Tribunal to be received by him not later than 21 days from the date of this letter.

7. Your plans are disapproved on the grounds as detailed in **Appendix I**.

8. You are reminded that the refusal grounds as mentioned in paragraph 7 above should not be treated as being exhaustive, and no such refusal shall be construed as implying any approval of any part of such plans. Your attention is drawn to section 15(2) of the BO.

/9.....

Our Ref.: BD 2-3/3046/93/70

9. Please note that your plans are not approvable under BO as the fundamental issues have not been resolved, the procedures under PNAP ADM-14 for making minor amendment to plans are not applicable.
10. You are reminded that under regulation 29(3) of the Building (Administration) Regulations (B(A)R) and without prejudice to regulation 30(3) of the B(A)R, the submission of further particulars or other plans consequent upon the refusal of the BA to give his approval under section 16(1)(i) or (2)(f) of the BO shall be deemed to be a fresh application in respect of the plans which the BA had refused to approve. Your future re-submission of plans for the subject proposal will be considered as plans submitted for the first time to the BA for approval, which regulation 30(3)(a) of the B(A)R applies.
11. Your plans have been referred to the District Planning Officer/Hong Kong, Planning Department for comments. His comments were conveyed to you in his fax dated 3 January 2024.
12. Your plans have been referred to the District Lands Officer/Hong Kong East for scrutiny under the lease condition. His comments were conveyed to you in his letter dated 15 January 2024.
13. Chief Engineer/ Hong Kong & Islands, Drainage Services Department (contact officer: Calvin Lo at tel. no.: 3101 2364) has comments to your plans in Appendix II.
14. Commissioner for Transport, Transport Department (contact officer: CHAN Quan Wai at tel. no.: 2829 5425 ) has comments to your plans in Appendix III to your plans.
15. Chief Highway Engineer/Hong Kong, Highways Department (Contact Officer: Mr. CHAN Kai-yin at tel. no. 2231 5722) has comments to your plans in Appendix IV.
16. Your plan has been referred to Chief Geotechnical Engineer/Island, Geotechnical Engineering Office, Civil Engineering and Development Department, and Leisure and Cultural Services Department for comments. Their comments would be conveyed to you once available.

Yours sincerely,



( LAM Tsz-fung )  
Senior Building Surveyor  
for Building Authority

c.c. Perfect Win Properties Limited  
49/F, Lee Garden One  
33 Hysan Avenue, Hong Kong

D of FS  
DLO/HKE  
DPO/HK  
C for T  
CHE/HK, HyD  
LCSD  
DSD  
CGE/I, GEO, CEDD

SL8 (11/2013) (T-ND f)



## Appendix II

Our Ref.: BD 2-3/3046/93/72

Address: 33 Hysan Avenue, Hong Kong and 10 Hysan Avenue, Hong Kong

Comments from Chief Engineer/ Hong Kong & Islands, Drainage Services Department (contact officer: Calvin Lo at tel. no.: 3101 2364)

1. Please ensure the existing sewage / stormwater terminal manhole is in good serviceable condition and ensure that the dimension and gradient of its downstream sewer / stormwater drain are adequate to discharge for the site.
2. Should there be any abandoned pipes as a result of the proposed development, they shall be removed or filled up at the developer's cost and to the satisfaction of this Department. For detailed requirements on the handling of abandoned pipes, please refer to DSD Technical Circular No. 1/2022 - Handling of Abandoned Pipes under DSD's Purview. The location and the method of handling such abandoned pipes should be indicated on drawings for future reference. The schedule of abandoned pipes / manholes should be submitted when any pipe / manhole is abandoned, and updated when any more pipes / manholes are abandoned; such schedule should not be delayed until the HBP1 / BA13 / BA14 stage or the completion of development.
3. The AP is required to ensure that no construction debris, silt and sediments, untreated site runoff or cementitious materials will be discharged to or deposited inside the public stormwater drains (SWD) or sewers within or in the vicinity of the development site. The AP shall monitor the internal conditions of the existing public SWD/sewers within or in the vicinity of the development site by CCTV surveys (or other alternatives to be agreed by DSD), prior to commencement and upon completion of the construction works to our satisfaction. The AP shall propose the detailed arrangement including the timing, methodology and extent of the public SWD/sewer to be surveyed by CCTV (or by other alternatives) for DSD's agreement. Nevertheless, such CCTV surveys (or other agreed alternatives) serve no intention to relieve the AP's liabilities on any damage to other SWD/sewers that are not included in the survey. Any pipe blockage or damage arising from the construction works shall be made good at the cost of the developer and to our satisfaction. In case the CCTV survey is abandoned for any section of SWD/sewers, the AP shall notify DSD by the following working day and arrange joint site inspection with DSD prior to the formal submission of CCTV report for considering the follow-up actions required from the AP. In addition, the AP shall also seek DSD's agreement on the proposed discharge point(s) of site runoff and the detailed discharge arrangement before application of the discharge licence for approval by EPD. During the process, DSD might

request the AP to arrange joint-site inspection to facilitate determination of the suitable discharge point(s).

In addition, the A.P. is reminded of the following general comments:-

4. The AP is required to liaise with relevant utility undertakers to obtain the latest records, plans and alignments of their utilities in order to ensure the feasibility of the proposed drainage works. The AP is also required to excavate inspection pits and conduct utility detection to verify the alignments of utilities shown in such utility records if considered necessary.
5. It is the developer's responsibility to identify/locate the existing government sewers and stormwater drains to which drainage connections from his site are to be proposed. The AP should verify the existence of any drains/sewers/utilities and also their exact locations, levels and alignments on site in order to ascertain the positions and levels of the proposed manholes and the associated connection works. The AP should also verify that the existing government drains/sewer, to which connections are proposed, are in normal working conditions and capable of taking the discharge from the site. Besides, for any excavation works over or in close vicinity to existing government drains/sewers, the AP should notify DSD in writing at least 14 working days before backfilling the excavation works and arrange joint site inspection with DSD prior to covering up.

The AP is also reminded that any person wilfully, except with the permission in writing of the Authority, or negligently damages, alters, disconnects or otherwise interferes with any public sewer or drain or any connection therewith, shall be guilty of an offence under Section 6 of Public Health and Municipal Services Ordinance (Cap 132).

6. Under the Water Pollution Control Ordinance (Cap 358), discharge of wastewater into stormwater drains is not permitted. The AP shall ensure that the proposed sewerage works shall convey all wastewater, including but not limited to those wastes generated by the domestic use of toilets, water closets, baths, showers, sinks, basins and other sanitary and kitchen fittings, through the sewage terminal manhole(s) to the public sewers. Besides, to ensure the sustainability of the public sewerage network, the AP shall ensure that the surface runoff within the development site will be collected and discharged via a stormwater drainage system and not be drained to the public sewerage network.
7. The submitted drawings nos. AA\_T1/GBP/GBP\_AA1 & AA2 are retained for future reference.

### Appendix III

Our Ref.: BD 2-3/3046/93/72

Address: 33 Hysan Avenue, Hong Kong and 10 Hysan Avenue, Hong Kong

Comments from Commissioner for Transport, Transport Department (contact officer: CHAN Quan Wai at tel. no.: 2829 5425 )

I refer to your MUR dated 29 Nov 2023 (received by this office on 6 Dec 2023) regarding the submission of T1 Tunnel only. Please find my comments below from traffic engineering viewpoint:

- The internal clear width of tunnel is 11,560mm. Please indicate the clear width of each traffic lane within the tunnel for clarity. Also, please clarify the compliance of its width and headroom with the proposed lease modification.
- Noting that a portion of tunnel (i.e. 1400mm) is separated by metal railing, please clarify its purpose and indicate its clear width on plan. Also, please demonstrate whether it fulfills with the proposed lease modification (i.e. passage of "pedestrians" within the tunnel).
- We have already provided our comments from traffic engineering on the proposed change of car parking spaces at Lee Garden I & III under separate submissions.

## Appendix IV

Our Ref.: BD 2-3/3046/93/72

Address: 33 Hysan Avenue, Hong Kong and 10 Hysan Avenue, Hong Kong

Comments from Chief Highway Engineer/Hong Kong, Highways Department (Contact Officer: Mr. CHAN Kai-yin at tel. no. 2231 5722)

- 1) As the Tunnel is outside the lot boundary and on the government land, LandsD, TD and PlanD's comments should be sought;
- 2) As the Proposed Tunnel will only connect with private developments, this Office will not take up its maintenance in future;
- 3) Necessary statutory procedures, such as gazettal or authorization of minor works under relevant Ordinance, lease modification and other land matters should be resolved between the developer and relevant government departments;
- 4) If trenchless works are involved, the AP shall demonstrate he has followed all requirements stipulated in the guideline "Control of Trenchless Works by Non-government Proponents to Avoid Subsidence and Damage to Public Roads". A copy is attached.
- 5) Prior to carrying out any excavation works on public footpath and carriageway, the AP shall apply for Excavation Permit from this Office;
- 6) It is presumed that the AP will assess the impact on the ventilation and fire services provision (e.g. means of escape) and seek comments from other relevant departments;
- 7) The AP should exercise extreme care and take adequate measures including any necessary investigations, in order not to damage adjacent HyD's road/structures/slopes/facilities/road drains/services and utilities in the vicinity of the proposed site;
- 8) The lot owner shall be responsible for the cost of making good any settlement or damages to the above-mentioned properties resulting directly or indirectly from the works. Any such settlement or damages shall be brought to the attention of this Regional Office immediately.
- 9) Any affected pavement and street furniture should be reinstated to the latest HyD standards and to our satisfaction.

**Control of Trenchless Works by Non-government Proponents  
to Avoid Subsidence and Damage to Public Roads  
(March 2019)**

**1 Basis of the Requirements in these Guidelines**

- 1.1 Further to the issue of the Block Licence/Lease Conditions to the non-government proponent by the Lands Department (LandsD) setting out conditions that require proponent to take or cause to be taken all proper and adequate care, skill and precautions at all times to avoid doing any damage to any government or other existing works and services, and to submit proposals for dealing with the works and services in writing to the appropriate authority for approval, these guidelines on the control of trenchless works by non-government proponents ("Guidelines") set out the requirements that the non-government proponent shall comply with in order to satisfy those conditions in the Block Licence/Lease Conditions that are relevant to the Highways Department (HyD).
- 1.2 In case of non-compliance of the non-government proponent with requirements stipulated in these Guidelines, the HyD shall report the cases to the LandsD for taking necessary action under the Block Licence/Lease Conditions.

**2 Scope**

- 2.1 These Guidelines apply to non-government underground works (hereafter referred to as "Trenchless Works") involving tunnels, pipes, crossings, conduits, etc. that:
- are constructed by trenchless excavation methods within unallocated government land and affecting public roads, under public roads, or within a horizontal distance from public roads equal to the existing ground cover of the works;
  - are proposed by non-government proponents; and
  - have the diameter of the excavation for the tunnel, pipe, crossing, or conduit etc. more than 1 metre.
- 2.2 The requirements specified in these Guidelines are to avoid subsidence and damage to public roads and features maintained by the HyD due to Trenchless Works only. For other aspects of the Trenchless Works, including but not limited to construction site safety of works and impact to existing facilities, the non-government proponent responsible for the Trenchless Works ("Project Proponent") shall satisfy the requirements of all other relevant government departments and utility undertakers.

**3 Liability of the Project Proponents, its Designer, Contractor and Independent Checking Engineer (ICE)**

- 3.1 No work carried out and no submission made by the Project Proponent, its Designer, Contractor and ICE (specified below) in connection to the requirements stipulated in these Guidelines shall absolve the Project Proponent, its Designer, Contractor and ICE from their liability on the Trenchless Works. The HyD will not be responsible for any deficiency in the work or submissions of the Project Proponent, its Designer, Contractor and ICE according to the requirements in

these Guidelines in connection to the Trenchless Works, nor any works carried out thereof.

- 3.2 It is the solely obligation of the Project Proponent to timely notify the Labour Department (LD), all other relevant government departments and utility undertakers in writing the details of its Trenchless Works, including but not limited to the location, commencement and completion dates of the proposed Trenchless Works, and to satisfy their requirements, including those stipulated in the "Guidance Notes on Safety and Health of Hand-dug Tunnelling Work" issued by the LD. The written notification to the LD shall be copied to the HyD for record purpose.

#### **4 The Requirements of the Highways Department**

##### **4.1 General Requirements**

- 4.1.1 The Project Proponent shall employ a professionally qualified and experienced consultant as the designer ("Designer") to undertake the ground investigation (GI), ground water monitoring, design and supervision of the proposed Trenchless Works. At least one of the staff of the consultant responsible for the work shall be registered with the Engineers Registration Board in the Geotechnical or Civil discipline (and also in the Structural discipline if structural works are required), with a minimum of 5 years of experience in GI, planning, design and construction supervision of Trenchless Works. The Designer shall be independent from the Contractor and the ICE.
- 4.1.2 The Project Proponent shall also employ a professionally qualified and experienced engineer as the ICE to independently check and certify the GI and design including subsequent design amendments and independently audit the construction supervision of the Designer for the Trenchless Works. The ICE shall be registered with the Engineers Registration Board in the Geotechnical or Civil discipline (and also in the Structural discipline if structural works are required), with a minimum of 5 years of experience in GI, planning, design and construction supervision of Trenchless Works. The ICE shall be independent from the Contractor and Designer.
- 4.1.3 The Project Proponent shall employ also a suitably experienced and competent contractor ("Contractor") with sufficient resources to carry out the Trenchless Works.
- 4.1.4 The Project Proponent shall require its Designer, Contractor and ICE to strictly follow the procedures set out in these Guidelines for the planning, investigation, design, construction and supervision of its Trenchless Works. The Project Proponent, its Designer, Contractor and the ICE shall be fully responsible for the adequacy and appropriateness of all matters and all submissions in connection to the Trenchless Works.
- 4.1.5 The Project Proponent shall submit details of its Designer, Contractor, and ICE, and the curriculum vitae of the engineers of its Designer responsible for the Trenchless Works, to the HyD for record purpose. The submissions shall include engagement confirmation of the Project Proponent with the Designer, the Contractor and the ICE, and the employment of the engineers responsible for the Trenchless Works issued by the Designer. The above

submissions may pass to other relevant government departments for their specific use.

#### 4.2 Pre-construction Requirements

- 4.2.1 The design of the Trenchless Works, including GI and associated temporary works, shall be undertaken by the Designer.
- 4.2.2 The design shall comply with all relevant geotechnical standards listed in GEO TGN 1 and recognised structural and safety standards. The design and construction method shall ensure that the Trenchless Works will not cause excessive road settlement.
- 4.2.3 The Designer shall recommend the road settlement limit with due consideration of the recommendations given in Appendix B to Practice Note for Authorized Persons, Registered Structural Engineers and Registered Geotechnical Engineers (PNAP) APP-137 published by Buildings Department. If the Designer considers this road settlement limit not applicable to the particular conditions of the Trenchless Works and/or concerned road, the Designer shall carry out an analysis to review and justify any revised settlement limit so required.
- 4.2.4 The Designer shall make design submission to the ICE for checking and certification. The submission shall include:
- (a) plans showing existing nature of the site, including accurate survey plan with ground level contours, road levels, geological conditions, groundwater conditions and surface water conditions, and details of streets, structures, foundations, public utilities and other services;
  - (b) plans showing all proposed works, including any excavation, ground support, ground treatment and groundwater control, incorporating the findings, recommendations and requirements in the report specified in paragraph 4.2.4 (j) below;
  - (c) a schedule of geotechnical design assumptions and verification requirement of these assumptions in the design review ("Design Review") which includes verifying and reviewing, during construction, the geotechnical assumptions made;
  - (d) sequence of the proposed works and the methods to be adopted, highlighting the critical stages of the Trenchless Works;
  - (e) an assessment of the magnitude and extent of ground settlement/heave/lateral movement, including estimates of the maximum allowable water inflow rates/groundwater pressure changes to be specified for the control of ground deformation, as well as their effects;
  - (f) particulars of monitoring to be carried out for sensitive receivers, ground and sub-surface movements and vibrations, variations in piezometric levels, the period of the monitoring to be carried out and the mitigation measures for settlement/subsidence of road pavement reaching Alert, Alarm and Action (AAA) levels;

- (g) a site supervision plan (SSP) which shall include details of the Category A supervisor and Category B supervisor who are appointed by the Designer specified below to carry out site supervision:

Category A supervisor is an engineer who shall be registered with the Engineers Registration Board in the Geotechnical or Civil discipline (and also in the Structural discipline if structural works are required), with a minimum of 5 years of experience in GI, planning, design and construction supervision of Trenchless Works.

Category B supervisor acceptable for site supervision shall possess a minimum of a higher certificate/diploma in civil/structural/geotechnical engineering and 2 years relevant site experience in trenchless works.

- (h) a report containing the results of a study including topography, geology, groundwater, surface water, site history, public utilities, other services and geotechnical records;
- (i) a report containing the results of ground investigation and laboratory testing;
- (j) a report on critical examination and interpretation of the reports specified in paragraphs 4.2.4 (h) and (i) above. The report shall also contain:
  - (i) a schedule of the geotechnical assumptions;
  - (ii) discussion of anticipated geotechnical problems;
  - (iii) an outline of possible variations of the works; and
  - (iv) geotechnical requirements for the design and construction of the works including inspection, monitoring and testing requirements.
- (k) design calculations for the proposed works, calculations of the effects of the works on groundwater conditions, the site and any structure, street, land or service, and calculations for and consideration of all other relevant geotechnical matters.
- (l) construction procedures to be adopted if, during the carrying out of the works, a geotechnical design assumption is revealed to be erroneous so that there is an unacceptable level of risk to public life and property due to damage/subsidence of public road; and
- (m) the schedule for submitting all the above to the ICE for checking and certification.

4.2.5 The ICE shall be fully responsible to ensure and certify that the GI, design, construction method statements and procedures, performance, risk control limits and measures, monitoring and SSP and all the details submitted under Paragraph 4.2.4 are satisfactory and meet all required standards for the proposed Trenchless Works.

4.2.6 Prior to commencement of construction, the Project Proponent shall submit to the HyD the Design Checking Certificate from the ICE confirming that the requirements stipulated in Paragraphs 4.2.5 are satisfactorily completed.



4.2.7 The ICE shall keep a copy of the design submission with details stipulated in Paragraphs 4.2.4 that ICE certified and agreed. This copy of the design submission shall be made available for checking by any parties when requested by the HyD or other Government Departments.

4.2.8 The Project Proponent is required to submit the following information to the HyD for record purpose at least two months before commencement of works:

- (a) brief account of the design assumptions and construction method;
- (b) layout plan and longitudinal profile of the proposed works with existing structures and utilities;
- (c) typical cross-section;
- (d) the estimated values of settlement/subsidence;
- (e) the AAA levels of settlement to be adopted;
- (f) the mitigation measures for settlement/subsidence of road pavement reaching AAA levels;
- (g) particulars of monitoring to be carried out for ground and sub-surface movements and variations in piezometric levels;
- (h) baseline measurement including contour of existing road level, water table level and ground level of the settlement markers, etc;
- (i) the curriculum vitae of Category A and Category B supervisors; and
- (j) the written notification to the LD with details stipulated in Paragraph 3.2.

4.2.9 The Project Proponent is required to notify the HyD at least one week in advance of the commencement date of the works.

#### 4.3 Requirements during Construction

4.3.1 The Category A supervisor shall conduct inspections at least twice a week and shall critically inspect the Trenchless Works including but not limited to the excavated ground, the tunnel faces and the support system.

4.3.2 The Category B supervisor shall conduct full time site supervision, to inspect and check the compliance of the works with drawings and specifications, and working procedures are followed properly.

4.3.3 During the critical stages of the works, or when irregularities are reported by site personnel on ground movement, seepages and changes of soil conditions during excavation, the Category A supervisor shall conduct inspections more frequently as necessary, and shall be available at all time to deal with any emergency incidents that arise from the works. The Designer shall review its design including geotechnical design assumptions made, and submit the design amendment to the ICE for checking and certification if amendments are made.

4.3.4 During construction, the ICE shall ensure that the Designer, through the Project Proponent, submits to him the Category A supervisor's reports on the works at the times specified. The reports shall document the site

inspection observations, Design Review and design amendments made, and a review of the ground investigation, groundwater condition, inspection, monitoring and test data obtained during construction. Relevant data and site records should be included in the reports. The ICE shall carry out audit inspections, including the excavated ground, tunnel faces, and support system, to confirm the adequacy of the Design Review and risk control action taken, at least once a week or more frequently if necessary during the critical stages of the works and if irregularities are identified and reported by the Designer. The ICE shall be available at all time to deal with any emergency incident that arises from the works.

4.3.5 The ICE shall keep copies of the Category A supervisor's reports together with his own inspection report for inspection by any parties upon request by the HyD.

4.3.6 The ICE should submit a summary list bi-weekly to the HyD upon commencement and until completion of Trenchless Works with the following information:

- (a) The results of monitoring of ground and sub-surface movements and variations in piezometric levels;
- (b) Confirmation on the adequacy of the Design Review and risk control action taken;
- (c) The dates of site inspection conducted by the ICE and Category A supervisor;
- (d) The amendments of the works and construction procedures which should be adopted if, during the carrying out the works, a geotechnical design assumption is revealed to be erroneous so that there is an unacceptable level of risk to public life and property due to damage/subsidence of public road; and
- (e) Mitigation measures taken if the AAA levels reached or any emergency incidents arisen from the works.

4.3.7 The Designer shall ensure that the Trenchless Works will not cause road settlement affecting the safe use of the roads. The Designer shall notify the HyD immediately if any of the AAA levels is reached or any emergency incident that arises from the works. Where the ground settlement reaches or exceeds the Alert level given in the AAA plan, the Designer shall conduct more frequent monitoring measurements and/or install additional monitoring points. Where the ground settlement reaches or exceeds the Alarm level given in the AAA plan, the Designer shall take necessary action to carry out Design Review and implement necessary remedial/emergency measures with the agreement and certification of the ICE. Where the ground settlement reaches or exceeds the Action level given in the AAA plan, the Trenchless Works shall suspend immediately. The Trenchless Works shall not resume without the agreement of the ICE. The Project Proponent shall notify the HyD before resuming works.

#### 4.4 Requirements upon Completion of Construction

4.4.1 Upon completion of the Trenchless Works, the ICE shall carry out a site inspection, if necessary, recommend remedial works that need to be carried out to restore the site to a satisfactory condition and ensure the remedial

works are completed accordingly, before certifying completion of his checking of the works. The ICE shall confirm, in writing, to the Project Proponent and the HyD, that his checking of the site works has been completed and the site has been restored to a satisfactory condition, and submit copies of all his own inspection report and Category A supervisor's reports to the Project Proponent and the HyD for record at the same time.

## **5 Notification and Auditing**

- 5.1 The Regions of the HyD shall notify LandsD to take appropriate follow up action under the Block Licence/Lease Conditions in the event that the Trenchless Works result in subsidence/damages of public road which causes damage to property or injury to members of the public.
- 5.2 The Regions of the HyD together with the Geotechnical Engineering Office of Civil Engineering and Development Department may carry out site audits of the Trenchless Works in the event that the works result in damage to property or injury to members of the public due to subsidence/damages of public road and or any emergency incidents arisen by the works. The Project Proponent shall make all necessary arrangements and provide all necessary assistance to facilitate these audits swiftly once it is required by the HyD.

# **Appendix 4**

**A&A Works General Building Plans  
Approval Letters**

CHEUNG Man Ching Anthony  
33rd Floor,  
Wu Chung House,  
213 Queen's Road East,  
Wanchai, Hong Kong.

24 March 2020

IN 000565  
RONALD LU  
& PARTNERS

17 APR 2020

RECEIVED

Dear Sir,

33 Hysan Avenue, Hong Kong (The Lee Garden One) –  
I.L. 29 s.DD, s.MM & s.L R.P. and I.L. 457 s.L & R.P.

I refer to your application dated received on 25 February 2020 for approval of proposals in respect of Building (Alterations and Additions).

2. Your submission of plans has been checked under the curtailed check system announced in Practice Note for Authorized Persons, Registered Structural Engineers and Registered Geotechnical Engineers ADM-19. On this basis, I am satisfied that your submission is fundamentally acceptable and may be approved.

3. You are reminded that the curtailed check system covers only the fundamental issues of a building proposal. Although non-fundamental issues will not be raised as reasons for disapproving a submission, I expect that all contraventions of the Buildings Ordinance and its subsidiary legislation are rectified as and when they are discovered and in any event, before completion of the works is certified. In this connection, I ask you to note that the Building Authority attaches great importance to the proper assumption of duties and responsibilities by authorized persons and registered structural engineers.

4. In accordance with the provisions of regulation 30(1) of the Building (Administration) Regulations, this is to notify that the above – mentioned plans submitted with your application dated received on 25 February 2020 are hereby approved. One set of the said plans, on which I have signified my approval, is enclosed. Your client has been sent a copy of this letter but I would request that you ensure that the contents are understood by him.

5. This approval should not be deemed to confer any title to land or to act as a waiver of any term in any lease or licence. This approval does **NOT** authorize the commencement or the carrying out of any works shown in the approved plans. Section 14(2) of the Buildings Ordinance refers.

6. This approval only covers the building works as coloured on the submitted plan on the basis that the uncoloured parts of the plans are exactly the same as the corresponding parts in the previous approved plans, and should not be construed as implying the approval of the uncoloured portion of the plan.

7. Your plans have been referred to the District Planning Officer/Hong Kong, Planning Department for comments. Your attention is drawn to Buildings Ordinance section 4(3) in case any amendments are required.

/8. ....

SL 9(11/2013) (T-b+ND a)

Our Ref: BD 2-3/3046/93 /55

8. Your plan and development schedule have been referred to the District Lands Officer/Hong Kong East for scrutiny under the lease condition. His comments will be directly conveyed to you.

9. The Commissioner for Transport, Transport Department (Contact Officer Mr. TANG Siu-chung at tel. no. 2829 5425) has the following comments on the proposed A&A works

- (a) It is noted that 12 numbers of parking spaces are proposed to be omitted in this A&A works. The AP should consider re-provisioning the affected parking spaces to other locations in the building and provide the re-provisioning proposal and car parking layout for our consideration. Otherwise, the AP should demonstrate that the compliance of the remaining provision of internal transport facilities in accordance with the requirements as stipulated in HKPSG is still maintained.
- (b) No excavation works on public roads for construction of the proposed tunnels is allowed.
- (c) The AP shall submit the Construction Traffic Management Scheme and provision of ingress/egress of the carparks for consideration by TD and other relevant department.
- (d) The AP shall be reminded that all the manoeuvring of vehicles during the construction shall be carried out within the development. No reversing movement of vehicles from/to the development would be allowed.

Yours faithfully,



( NG Pelene )  
Chief Building Surveyor  
for Building Authority

c.c. Perfect Win Properties Limited  
49/F  
Lee Garden One  
33 Hysan Avenue  
Hong Kong

D of FS  
DLO/HKE  
DPO/HK  
C for T, TD  
CO/SM

SL 9(11/2013) (T-b+ND a)



CHEUNG Man Ching Anthony  
33rd Floor,  
Wu Chung House,  
213 Queen's Road East,  
Wanchai, Hong Kong.

24 March 2020

Dear Sir,

10 Hysan Avenue, Hong Kong - I.L. 29 s.J R.P., s.J ss.1 R.P. and s.J ss.2

I refer to your application dated received on 25 February 2020 for approval of proposals in respect of Building (Alterations and Additions).

2. Your submission of plans has been checked under the curtailed check system announced in Practice Note for Authorized Persons, Registered Structural Engineers and Registered Geotechnical Engineers ADM-19. On this basis, I am satisfied that your submission is fundamentally acceptable and may be approved.

3. You are reminded that the curtailed check system covers only the fundamental issues of a building proposal. Although non-fundamental issues will not be raised as reasons for disapproving a submission, I expect that all contraventions of the Buildings Ordinance and its subsidiary legislation are rectified as and when they are discovered and in any event, before completion of the works is certified. In this connection, I ask you to note that the Building Authority attaches great importance to the proper assumption of duties and responsibilities by authorized persons and registered structural engineers.

4. In accordance with the provisions of regulation 30(1) of the Building (Administration) Regulations, this is to notify that the above – mentioned plans submitted with your application dated received on 25 February 2020 are hereby approved. One set of the said plans, on which I have signified my approval, is enclosed. Your client has been sent a copy of this letter but I would request that you ensure that the contents are understood by him.

5. This approval should not be deemed to confer any title to land or to act as a waiver of any term in any lease or licence. This approval does **NOT** authorize the commencement or the carrying out of any works shown in the approved plans. Section 14(2) of the Buildings Ordinance refers.

6. This approval only covers the building works as coloured on the submitted plan on the basis that the uncoloured parts of the plans are exactly the same as the corresponding parts in the previous approved plans, and should not be construed as implying the approval of the uncoloured portion of the plan.

7. Your plans have been referred to the District Planning Officer/Hong Kong, Planning Department for comments. Your attention is drawn to Buildings Ordinance section 4(3) in case any amendments are required.

SL 9(11/2013) (T-b+ND a)

/8. ....

Our Ref: BD 2-3/3001/09 /(2)/8

8. Your plan and development schedule have been referred to the District Lands Officer/Hong Kong East for scrutiny under the lease condition. His comments will be directly conveyed to you.

9. The Commissioner for Transport, Transport Department (Contact Officer Mr. TANG Siu-chung at tel. no. 2829 5425) has the following comments on the proposed A&A works

- (a) It is noted that 6 numbers of parking spaces are proposed to be omitted in this A&A works. The AP should consider re-provisioning the affected parking spaces to other locations in the building and provide the re-provisioning proposal and car parking layout for our consideration. Otherwise, the AP should demonstrate that the compliance of the remaining provision of internal transport facilities in accordance with the requirements as stipulated in HKPSG is still maintained.
- (b) No excavation works on public roads for construction of the proposed tunnels is allowed.
- (c) The AP shall submit the Construction Traffic Management Scheme and provision of ingress/egress of the carparks for consideration by TD and other relevant department.
- (d) The AP shall be reminded that all the manoeuvring of vehicles during the construction shall be carried out within the development. No reversing movement of vehicles from / to the development would be allowed.

Yours faithfully,



( NG Pelene )  
Chief Building Surveyor  
for Building Authority

c.c. Silver Nicety Company Limited  
49/F  
Lee Garden One  
33 Hysan Avenue  
Hong Kong

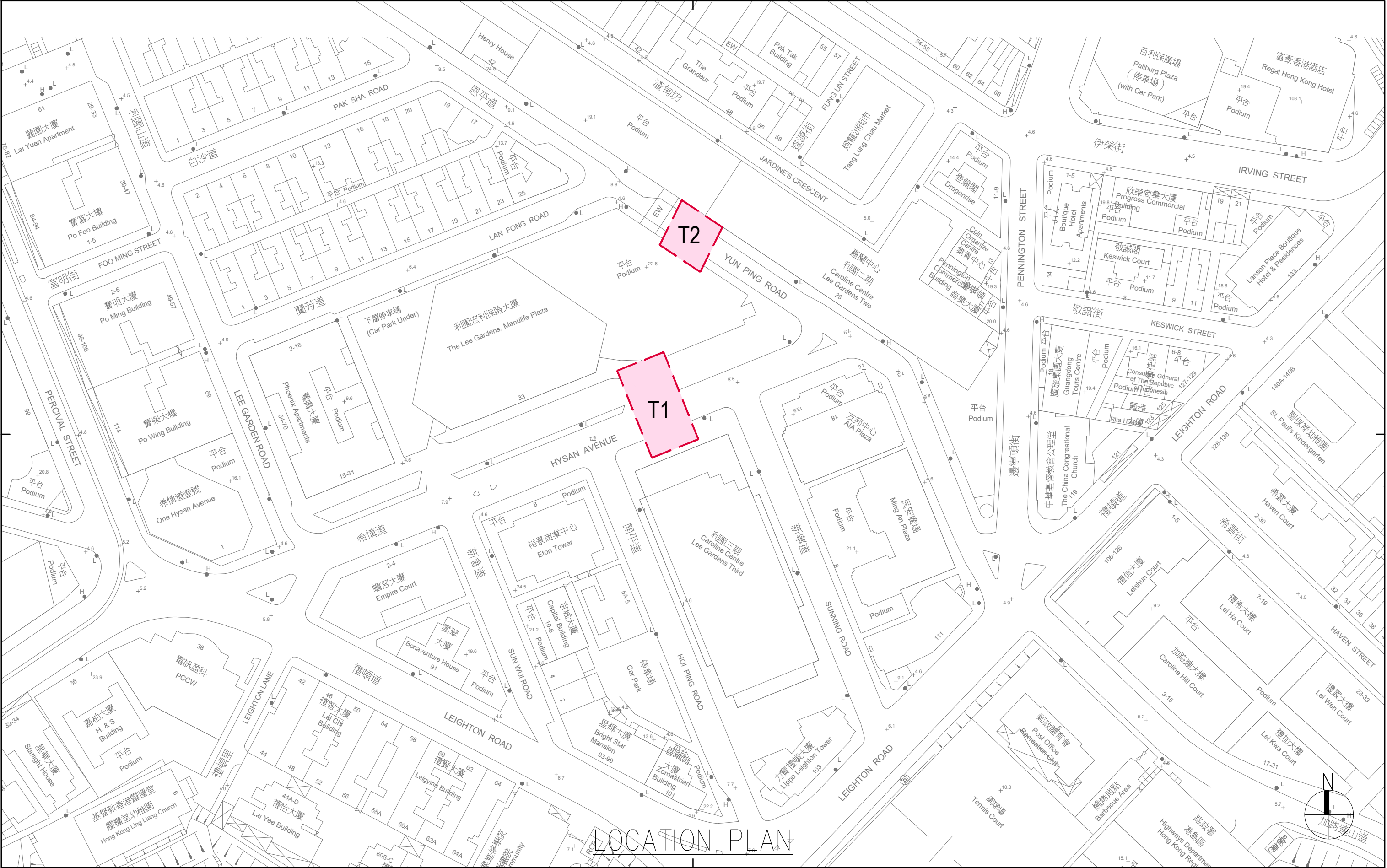
D of FS  
DLO/HKE  
DPO/HK  
C for T, TD  
CO/SM

SL 9(11/2013) (T-b+ND a)



# **Appendix 5**

## **Location Plan and Sectional Plans**



Rev.	Description	Drawn	Checked	Approved	Date

Rev.	Description	Drawn	Checked	Approved	Date

Check all measurements on site.  
Do not scale off drawings.

This drawing is to be read in conjunction with the specification and any discrepancies are to be immediately reported to the Architect.

This drawing remains the copyright property of the Architect and is not to be reproduced in whole or in part without permission of the Architect.

B.D. Ref.	-
F.S.D. Ref.	-
D.L.O. Ref.	-
Drawn	FKL Date 07/2024
Checked	LEL Date 07/2024
Approved	CMC Date 07/2024
Cad File No.	-



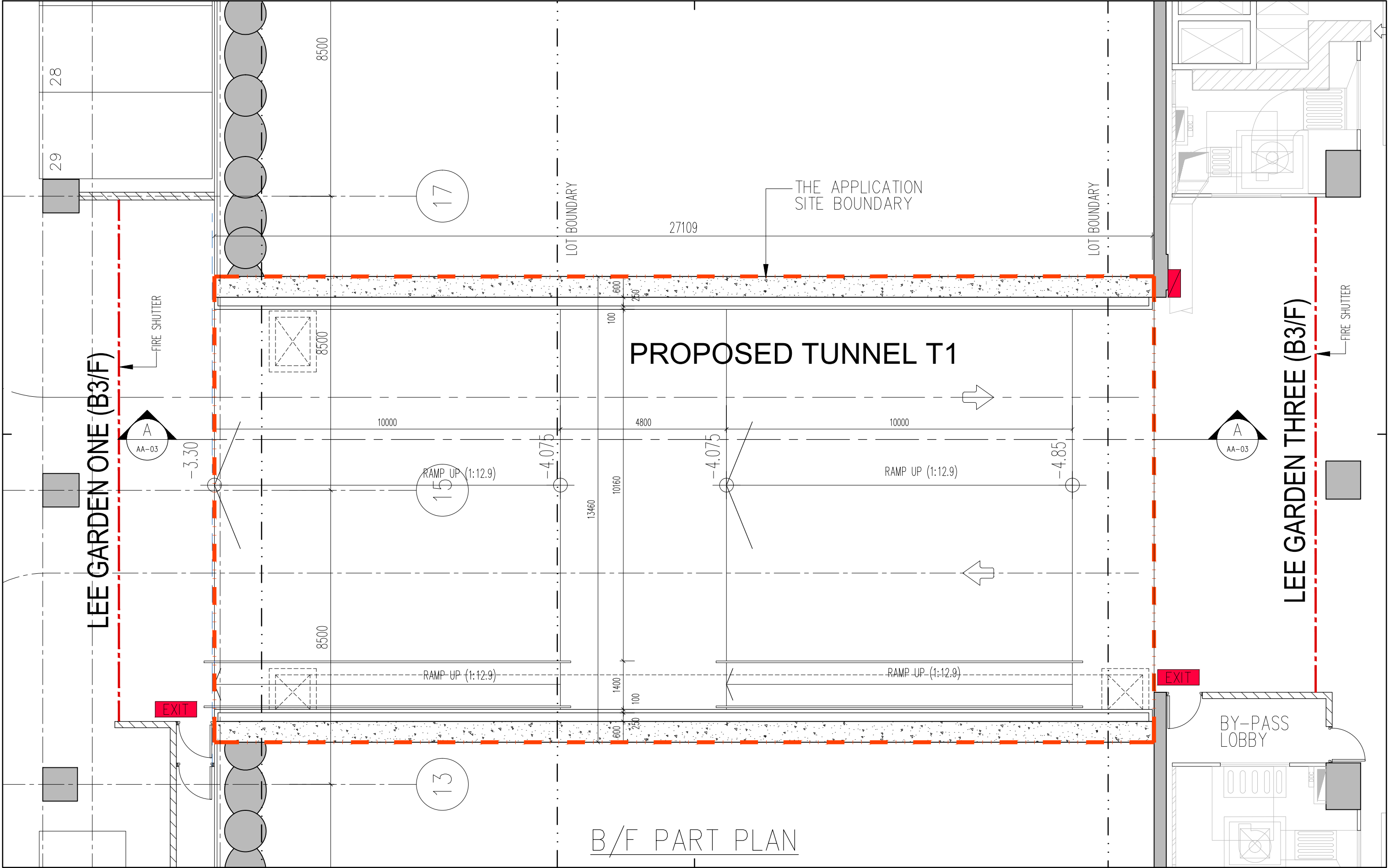
呂元祥建築師事務所

Project Title  
PROPOSED TUNNEL T1 LINKING  
LEE GARDEN ONE AND  
LEE GARDEN THREE

Drawing Title	T1, T2 LOCATION PLAN
Project No.	19030HK
Scale	1:1000 @A3 Issue Date 07/2024
Drawing No.	/ GBP / AA-01

AP's Signature

Drawing Purpose



Rev.	Description	Drawn	Checked	Approved	Date

Rev.	Description	Drawn	Checked	Approved	Date

Check all measurements on site.  
Do not scale off drawings.

This drawing is to be read in conjunction with the specification and any discrepancies are to be immediately reported to the Architect.

This drawing remains the copyright property of the Architect and is not to be reproduced in whole or in part without permission of the Architect.

B.D. Ref.	-
F.S.D. Ref.	-
D.L.O. Ref.	-
Drawn	FKL Date 07/2024
Checked	LEL Date 07/2024
Approved	CMC Date 07/2024
Cad File No.	-



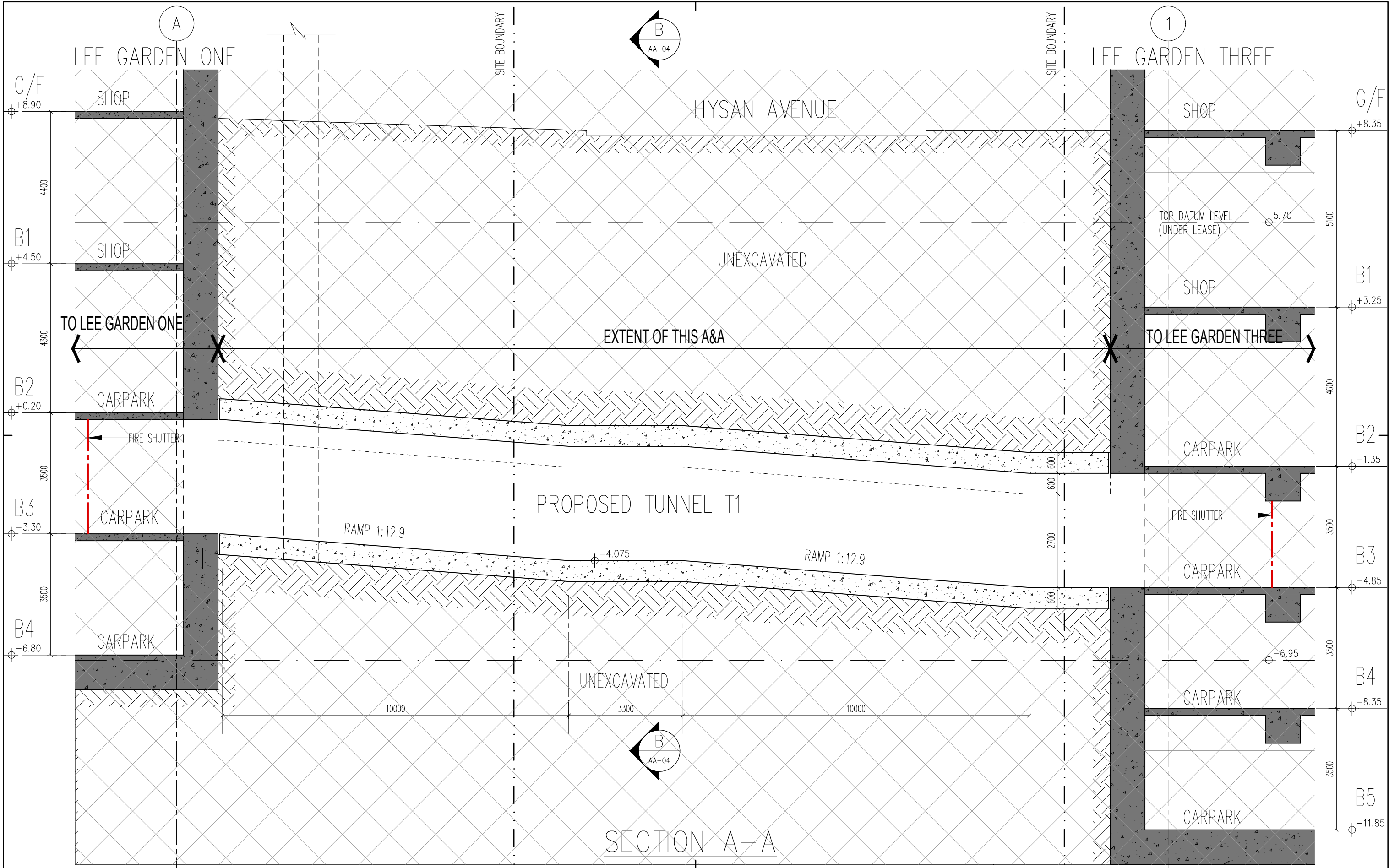
呂元祥建築師事務所

Project Title  
PROPOSED TUNNEL T1 LINKING  
LEE GARDEN ONE AND  
LEE GARDEN THREE

Drawing Title	T1 BASEMENT FLOOR PART PLAN
Project No.	19030HK
Scale	1:100 @A3
Issue Date	07/2024
Drawing No.	/ GBP / AA-02

AP's Signature

Drawing Purpose



Rev.	Description	Drawn	Checked	Approved	Date

Rev.	Description	Drawn	Checked	Approved	Date

Check all measurements on site.  
Do not scale off drawings.

This drawing is to be read in conjunction with the specification and any discrepancies are to be immediately reported to the Architect.

This drawing remains the copyright property of the Architect and is not to be reproduced in whole or in part without permission of the Architect.

B.D. Ref.	-
F.S.D. Ref.	-
D.L.O. Ref.	-
Drawn	FKL
Checked	LEL
Approved	CMC
Cad File No.	-
Date	07/2024
Date	07/2024
Date	07/2024



**RONALD LU  
& PARTNERS**

呂元祥建築師事務所

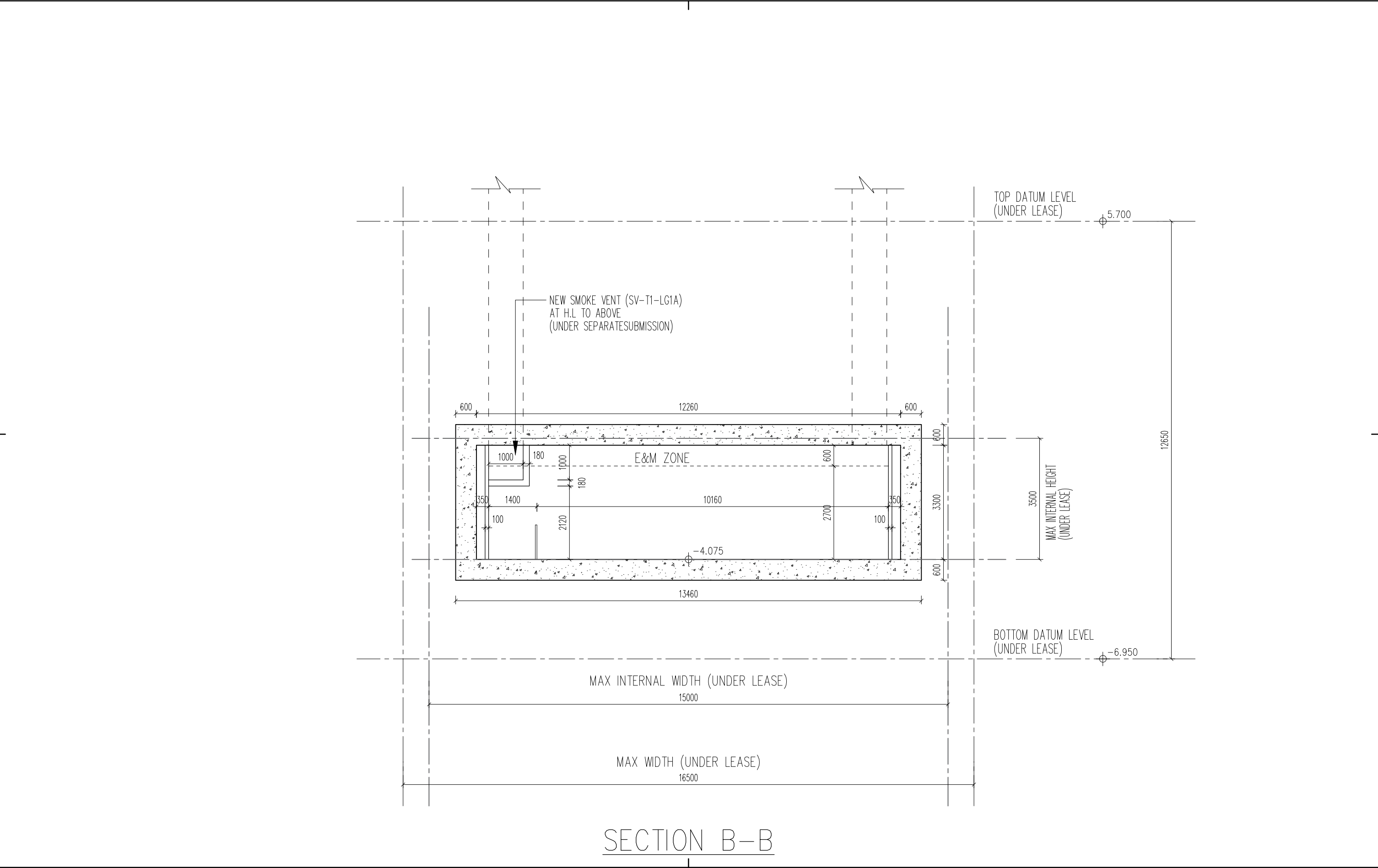
Project Title  
PROPOSED TUNNEL T1 LINKING  
LEE GARDEN ONE AND  
LEE GARDEN THREE

Drawing Title  
T1 SECTION A-A

Project No.	19030HK
Scale	1:100 @A3
Issue Date	07/2024
Drawing No.	/GBP/AA-03

AP's Signature

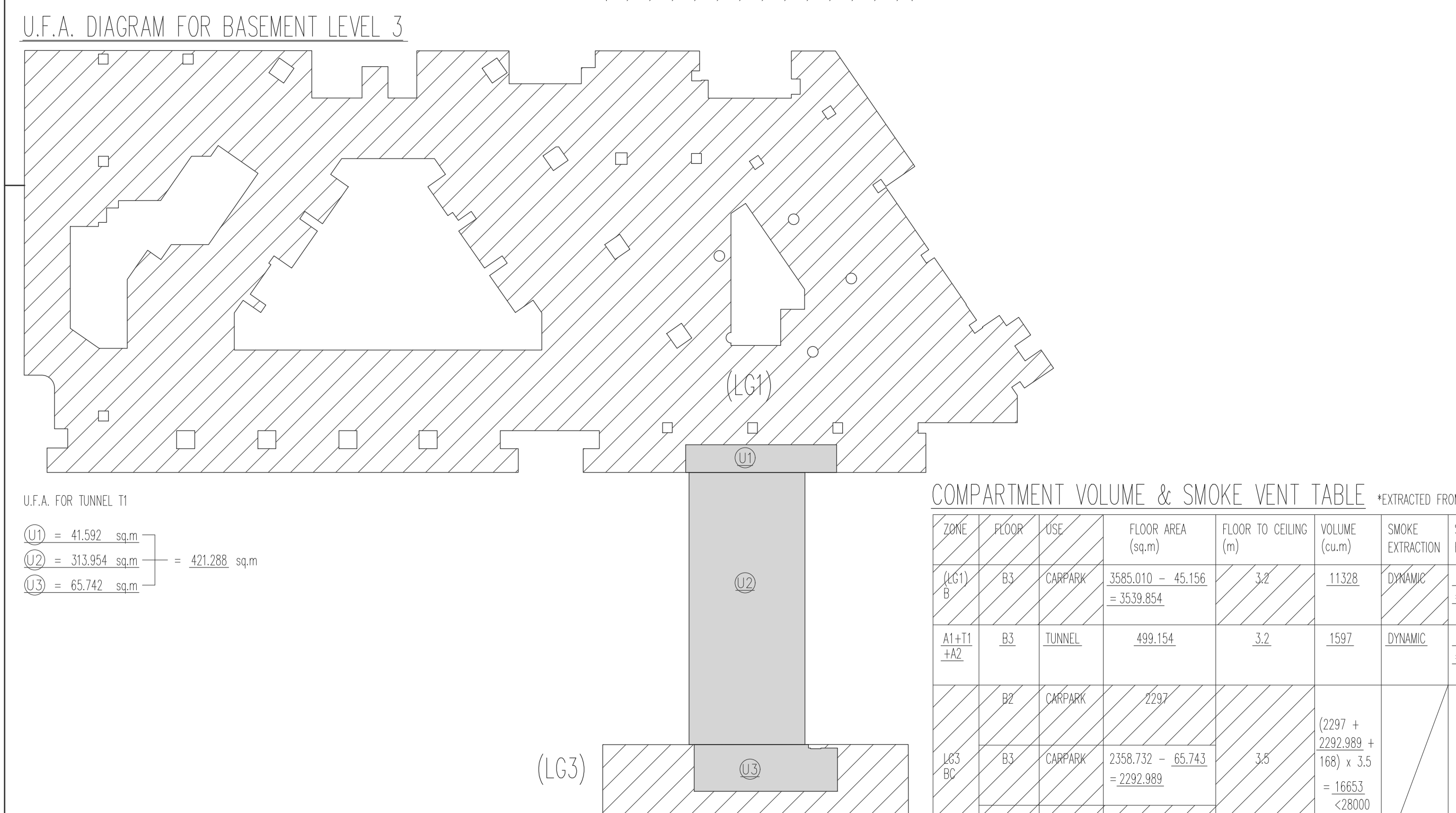
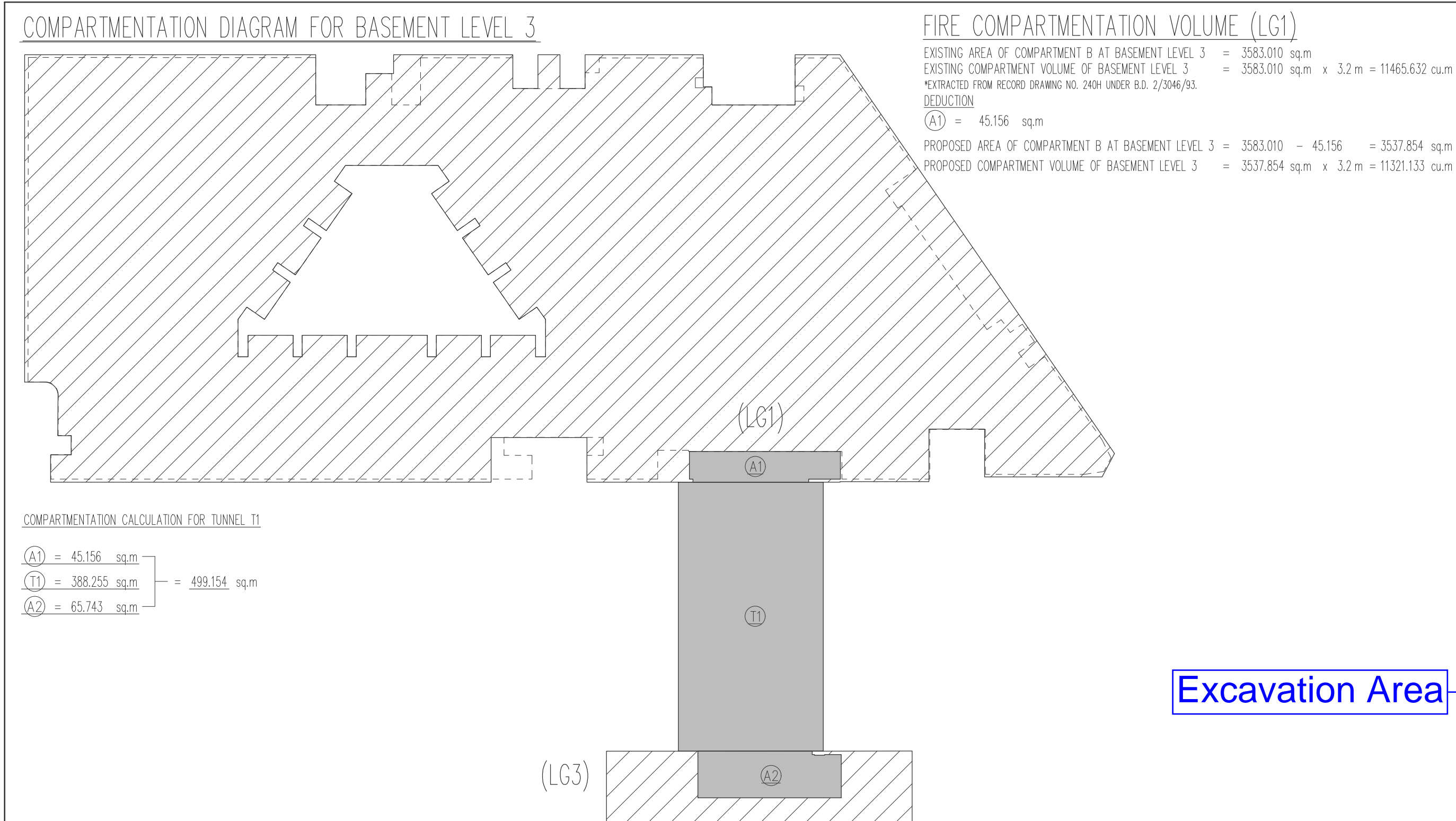
Drawing Purpose



# **Appendix 6**

**Plan showing Excavation Area**





#### REQUIREMENT OF EXIT DOOR & EXIT ROUTE

LOCATION		U.F.A. (sq.m)	OCCUPANCY FACTOR	TOTAL NO. OF PERSON	REQUIRED	PROVIDED	MIN. WIDTH OF EACH				MIN. TOTAL WIDTH OF			
					MIN. NO OF EXIT DOORS OR EXIT ROUTES	NO OF EXIT DOORS OR EXIT ROUTES	REQUIRED	PROVIDED	REQUIRED	PROVIDED	REQUIRED	PROVIDED	REQUIRED	PROVIDED
							EXIT DOORS	EXIT ROUTES	EXIT DOORS	EXIT ROUTES	EXIT DOORS	EXIT ROUTES	EXIT DOORS	EXIT ROUTES
B3/F	TUNNEL	421.288	30	15	1	2	750	1050	750	1050	=	=	1500	2100

#### SCHEDULE OF FIRE RESISTANCE PERIOD

LOCATION	USE	CLASS	COMPARTMENT VOLUME		F.R.R. (HOURS)	MIN. DIMENSION FOR ELEMENTS OF CONSTRUCTION										MIN. THICKNESS OF PROTECTION					
						R.C. SLAB		R.C. BEAM		R.C. COLUMN		R.C. WALL		BRICK WALL	CONCRETE BLOCK WALL	R.C. STAIR		STEEL COLUMN		STEEL BEAM	
			FLOOR AREA (sq.m)	VOLUME (cu.m)		THICKNESS (mm)	COVER TO STEEL (mm)	MIN. WIDTH (mm)	COVER TO STEEL (mm)	THICKNESS (mm)	COVER TO STEEL (mm)	THICKNESS (mm)	COVER TO STEEL (mm)			SLAB THICKNESS (mm)	COVER TO STEEL (mm)	STEEL COLUMN (mm)	SPRINK MINER* FIBRE (mm)	STEEL BEAM (mm)	SPRINK MINER* FIBRE (mm)
T1 TUNNEL	CARPARK	7	499.154	1597	4	170	55 (S) 45 (C)	280	80 (S) 60 (C)	450	35	180	25	—	—	—	—	75	—	75	—

Rev.	Description	Drawn	Checked	Approved	Date
1	1st A&A AMENDMENT	FKL	COL	JHY	1_07/2023

Rev.	Description	Drawn	Checked	Approved	Date

Check all measurements on site. Do not scale off drawings.

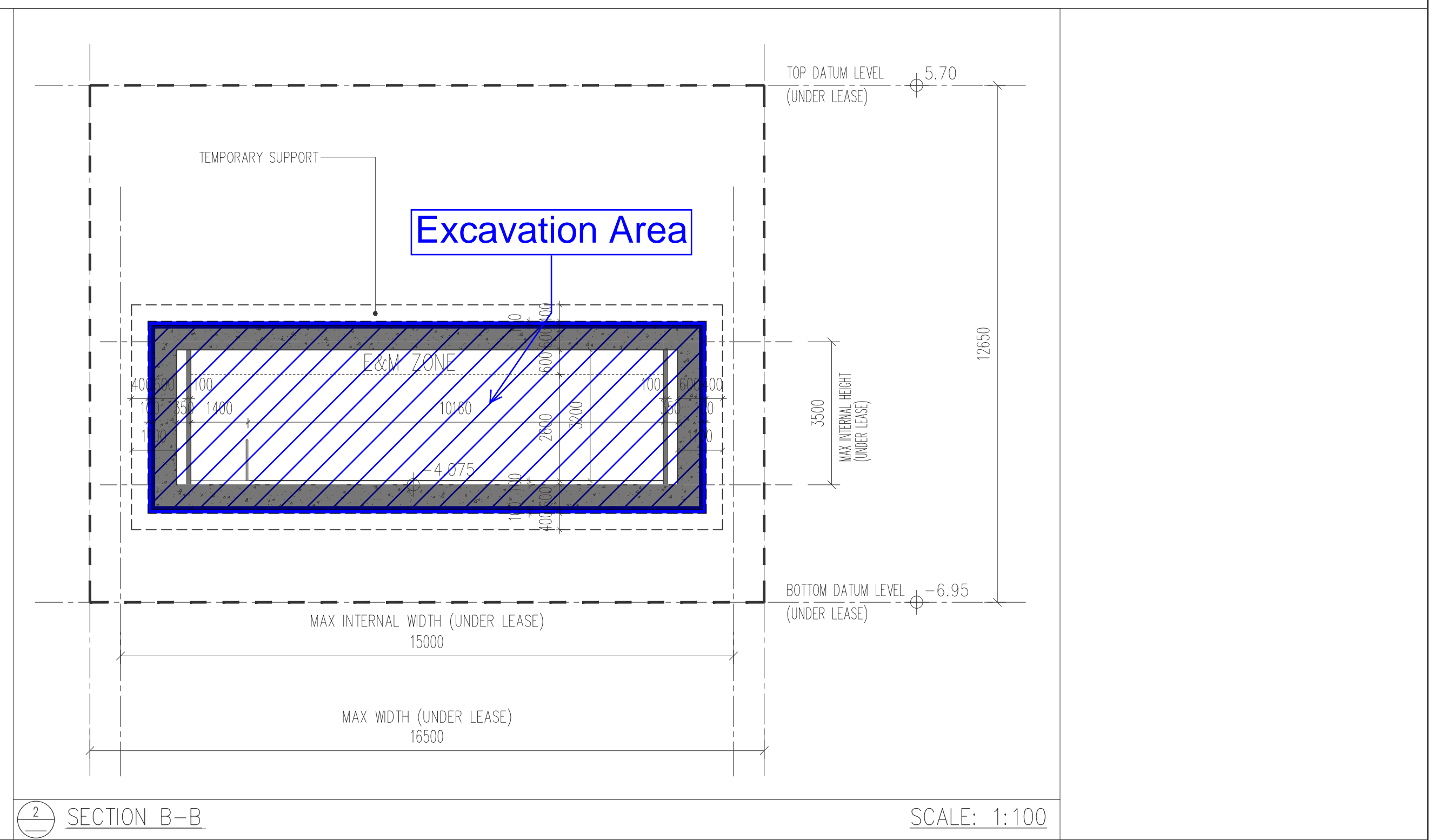
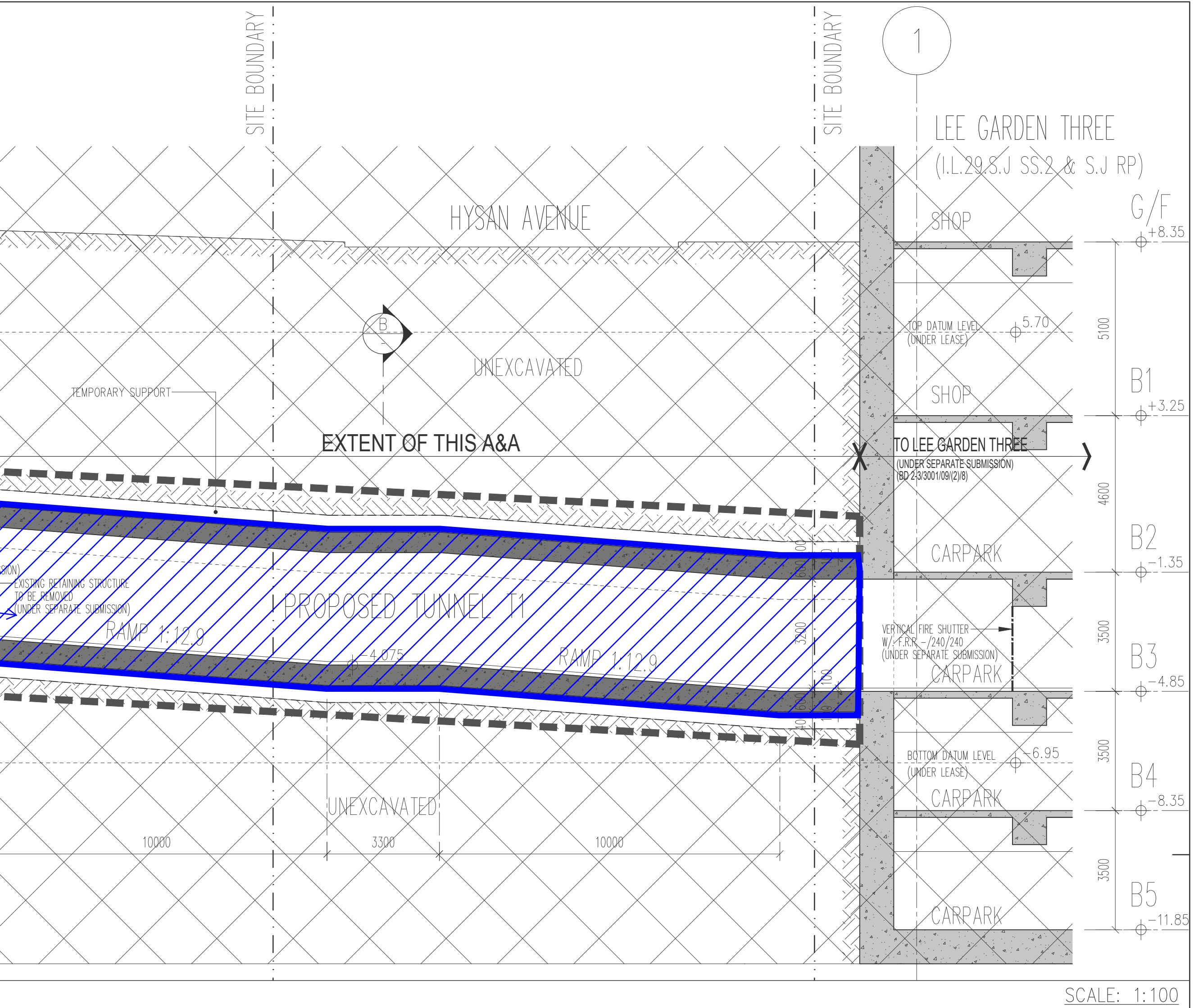
This drawing is to be read in conjunction with the specification and any discrepancies are to be immediately reported to the Architect.

This drawing remains the copyright property of the Architect and is not to be reproduced in whole or in part without permission of the Architect.

B.D. Ref.	-
F.S.D. Ref.	-
D.L.O. Ref.	-
Drawn	FKL Date 07/2023
Checked	COL Date 07/2023
Approved	JHY Date 07/2023
Cad File No.	-

**RONALD LU & PARTNERS**

呂元祥建築師事務所



Project Title

PROPOSED A&A WORKS AT LEE GARDEN ONE, 33 HYSAN AVENUE, CAUSEWAY BAY, HONG KONG.

Drawing Title

SCHEDULES AND CALCULATIONS

Project No. 19030HK

Scale A:S Issue Date 07/07/2023

Drawing No. AA\_T1 / GBP / GBP\_AA2

AP's Signature

CHEUNG Man-Ching, Anthony  
Authorized Person (Architect)

Drawing Purpose

**Buildings Department**