

**Application for Permission under Section 16 of
the Town Planning Ordinance (Cap. 131)**

**Temporary Vehicle Park for Crane Lorries and
Associated Filling of Land for a Period of 3 Years
in “Agriculture” Zone, Lots 768 and 769 in DD78,
Ta Kwu Ling, New Territories**

SUPPLEMENTARY PLANNING STATEMENT

Applicant:

Welltune Transportation Limited

Planning Consultant:



Top Bright Consultants Ltd.

August, 2025

Executive Summary

This planning application is submitted on behalf of Welltune Transportation Limited (the “Applicant”) to seek planning permission from the Town Planning Board (the “Board”) for a temporary vehicle park for crane lorries and associated filling of land (the “Proposed Development”) for a period of 3 years at Lots 768 and 769 in DD78, Ta Kwu Ling, New Territories (the “Application Site”).

The Applicant is a local transport company that was established in 1997. Over the past 28 years, it has contributed to various large-scale infrastructure projects in Hong Kong. The Applicant provides professional crane lorry services and currently owns a fleet of 22 crane lorries in Hong Kong. They offer a variety of crane lorries of different sizes for delivering, collecting, and self-loading and unloading a wide range of items. The Applicant mainly serves large-scale civil construction projects, such as airports, highways, and new town developments.

The Applicant has been operating their business at Lot 29(Part) in DD95, Ho Sheung Heung, Kwu Tung North, N.T. (the “affected site”) since 2003. This site falls within the boundary of the Remaining Phase Development of Kwu Tung North New Development Area (NDA). On 16.1.2014, the Lands Department (LandsD) posted a notice at the affected site informing the Applicant that the draft outline zoning plan (OZP) for the Kwu Tung North and Fanling North NDAs had been published in the Gazette on 20.12.2013. As the Applicant’s operating premises are located within the affected area, demolition/relocation is required. In August 2022, the Applicant relocated from the affected site at the landowner’s request. Due to the short notice provided by the landowner, the Applicant temporarily leased portions of Lots 1265, 1268, 1269, 1270 and 1278RP in DD39, Sha Tau Kok, N.T. with an area of approximately 1,400 sq.m. Due to the insecure tenancy period at the relocated site, the Applicant has actively sought suitable alternative premises for relocation.

Due to the ongoing resumption of land in the Northern Metropolis for the development of NDAs, the Applicant is finding it difficult to identify potential relocation sites, as there are limited options available on the market. In December 2024, the Applicant finally identified and purchased the Application Site, which was used as a chicken farm in the 1980s and has been vacant for many years. The site is accessible via Lin Chuk Road, which connects to Lin Ma Hang Road and Heung Yuen Wai Highways, offering convenient transport links. The Application Site can meet the Applicant’s operational needs, and securing a relocation site is an imminent necessity in order to continue the affected business operations.

The Application Site, which covers an area of approximately 1,725 sq.m., is currently occupied by several vacant temporary structures. It falls within an “Agriculture” (“AGR”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2. The Application Site is small in scale and consists of six parking spaces for crane lorries (under 24 tonnes) under two 7m high shelters (225 sq.m. each), and two private car parking spaces for staff/visitors. There will be sufficient space within the Application Site for manoeuvring vehicles. The Application Site will be fenced off with 2.5 m-high hoardings. The proposed operating hours at the Application Site are restricted to 8:00 a.m. to 6:00 p.m., Mondays to Saturdays, with no operation on Sundays and public holidays.

The justifications of this application are: meeting the relocation needs of the Applicant; site unsuitable for agriculture rehabilitation; site falls outside the New Territories North New Town; more desirable interim use and efficient use of land resource; compatible with surrounding land uses; no adverse environmental or visual, drainage and traffic impacts; and not an undesirable precedent.

The Applicant therefore seeks the Board’s permission to use the Application Site for the proposed use for a temporary period of 3 years.

行政摘要

這宗規劃申請乃代表王棠記有限公司向城市規劃委員會(“城規會”)呈交，擬議在新界打鼓嶺丈量約份第 78 約地段第 768 號及 769 號(“申請地點”)，用作臨時吊機車停車場及相關填土工程(“申請用途”)，為期三年。

申請人是一家成立於 1997 年的本地運輸公司。過去 28 年來，該公司為香港多項大型基建項目作出貢獻。申請人提供專業的起重車服務，目前在香港擁有一支由 22 輛起重車組成的車隊。他們提供多種不同大小的起重貨車，用於運送、收集及自行裝卸各種物品。申請人主要服務於大型土木工程項目，如機場、高速公路及新市鎮發展。

申請人自 2003 年起一直在新界古洞北河上鄉丈量約份第 95 約地段第 29 號(部分)(“受影響地點”)經營業務。該地點位於古洞北新發展區餘下發展階段的範圍內。地政總署於 2014 年 1 月 16 日在受影響地點張貼告示，通知申請人古洞北及粉嶺北新發展區的分區計劃大綱草圖已於 2013 年 12 月 20 日刊憲。由於申請人的營運處所位於受影響範圍內，故須拆卸/搬遷。於 2022 年 8 月，申請人應土地擁有人的要求遷離受影響地點。由於土地擁有人給予的通知時間較短，申請人暫時租用新界沙頭角丈量約份第 39 約地段第 1265、1268、1269、1270 及 1278RP 號的部分地段，面積約 1,400 平方米。由於搬遷地點的租期不穩定，申請人已積極尋找其他合適的搬遷地點。

由於北部都會區持續收回土地以發展新發展區，申請人發現難以物色具潛力的搬遷地點，因為市場上可供選擇的地點有限。於 2024 年 12 月，申請人終於物色並購入申請地點，該地點於 1980 年代曾用作養雞場，並已空置多年。申請人可經蓮竹路前往該址，蓮竹路連接蓮麻坑路及香園圍公路，交通方便。申請地點能滿足申請人的營運需要，而為了繼續受影響的業務運作，覓地搬遷是迫在眉睫的需要。

申請地點的面積約為 1,725 平方米，目前有數個空置的臨時構築物，座落於打鼓嶺北分區計劃大綱核准圖編號 S/NE-TKLN/2 中的“農業”地帶。申請場地規模較小，包括兩個上蓋(每個上蓋約 225 平方米及 7 米高)以停泊六輛吊機車(24 噸以下)及兩個供職員/訪客使用的私家車停車位。申請地點內將有足夠空間供車輛調動。申請地點將以 2.5 米高的圍板圍起。申請地點的營運時間為星期一至星期六上午 8 時至下午 6 時，星期日及公眾假期不營業。

這宗規劃申請的理由為：滿足申請人的搬遷需要；選址不適合農業復耕；選址位於新界北新市鎮發展計劃以外；更理想的臨時用途及有效利用土地資源；與周邊土地用途相容；不會對環境或視覺、排水及交通造成不良影響；以及不會開創不良先例。因此，申請人希望城規會批准三年的臨時用途。

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SUMMARY OF APPLICATION

Applicant	: Welltune Transportation Limited
Applied Use	: Temporary Vehicle Park for Crane Lorries and Associated Filling of Land for a Period of 3 Years
Location	: Lots 768 and 769 in DD78, Ta Kwu Ling, New Territories
Site Area	: About 1,725 square metres Government Land: Nil
Lease	: Block Government Lease demised as agricultural land
Statutory Plan	: Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2 gazetted on 13.5.2016
Zoning	: "Agriculture" ("AGR")
Previous Application	: Nil

1. INTRODUCTION

- 1.01 This application is prepared by Top Bright Consultants Ltd. on behalf of Welltune Transportation Limited (the “Applicant”) to seek planning permission from the Town Planning Board (the “Board”) for a temporary vehicle park for crane lorries and associated filling of land (the “Proposed Development”) for a period of 3 years at Lots 768 and 769 in DD78, Ta Kwu Ling, New Territories (the “Application Site”).
- 1.02 The Applicant is a local transport company that was established in 1997. Over the past 28 years, it has contributed to various large-scale infrastructure projects in Hong Kong. The Applicant provides professional crane lorry services and currently owns a fleet of 22 crane lorries in Hong Kong. They offer a variety of crane lorries of different sizes for delivering, collecting, and self-loading and unloading a wide range of items. The Applicant mainly serves large-scale civil construction projects, such as airports, highways, and new town developments.
- 1.03 The Applicant has been operating their business at Lot 29(Part) in DD95, Ho Sheung Heung, Kwu Tung North, N.T. (the “affected site”) since 2003. This site falls within the boundary of the Remaining Phase Development of Kwu Tung North New Development Area (NDA) (see **Appendix 1**). On 16.1.2014, the Lands Department (LandsD) posted a notice at the affected site informing the Applicant that the draft outline zoning plan (OZP) for the Kwu Tung North and Fanling North NDAs had been published in the Gazette on 20.12.2013. As the Applicant’s operating premises are located within the affected area, demolition/relocation is required (see **Appendix 2**). In August 2022, the Applicant relocated from the affected site at the landowner’s request. Due to the short notice provided by the landowner, the Applicant temporarily leased portions of Lots 1265, 1268, 1269, 1270 and 1278RP in DD39, Sha Tau Kok, N.T. with an area of approximately 1,400 sq.m. (the “relocated site”).
- 1.04 Due to the insecure tenancy period at the relocated site, the Applicant has actively sought suitable alternative premises for relocation. The basic requirements are that the premises must have sufficient space for crane lorries to park and manoeuvre; and proper roads and access points. Between 2023 and 2024, the Applicant conducted a thorough search for suitable sites and explored 4 other potential relocation sites. However, the proposed temporary relocation sites were ultimately not selected due to various reasons (see **Appendix 3**).
- 1.05 Due to the ongoing resumption of land in the Northern Metropolis for the development of NDAs, the Applicant is finding it difficult to identify potential relocation sites, as there are limited options available on the market. In December 2024, the Applicant finally identified and purchased the Application Site, which was used as a chicken farm in the 1980s and has been vacant for many years. The site is accessible via Lin Chuk Road, which connects to Lin Ma Hang Road and Heung Yuen Wai Highways, offering convenient transport links. The Application Site can meet the Applicant’s operational needs, and securing a relocation site is an imminent necessity in order to continue the affected business operations.
- 1.06 The Application Site, which covers an area of approximately 1,725 sq.m., is currently occupied by several vacant temporary structures. It falls within an “Agriculture” (“AGR”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan (OZP) No. S/NE-TKLN/2, which is intended to retain and safeguard good quality agricultural land/farm/fish ponds for agricultural purposes. According to the covering Notes of the OZP, planning permission from the Board is required for

the temporary use or development of any land or buildings not exceeding a period of three years.

- 1.07 The following sections are intended to help facilitate the Board's decision, and will briefly describe the Application Site and its surroundings, give details of the proposed development and provide justifications in support of the application.

2. SITE CONTEXT

Location

- 2.01 The Application Site is located north of Lin Chuk Road in Ta Kwu Ling, around 500 metres southeast of the Heung Yuen Wai Boundary Control Point in the New Territories. **Figure 1** shows the Application Site in its regional context.

Existing Site Conditions

- 2.02 The Application Site, covers an area of approximately 1,725 square metres, was formerly a chicken farm and is currently vacant (see **Figure 2**). It consists of several small platforms at different levels, which were built to accommodate the farm's original structures. The Applicant will demolish all the vacant structures on the Application Site.

Surrounding Land Uses

- 2.03 The Application Site is located in a rural area dominated by abandoned farmland, vacant land and temporary structures. It is bounded by 3 permitted burial ground sites to the east, west and further west within the adjacent 'Green Belt' zones. An existing watercourse runs along the north-west side of the Application Site. To the south, across Lin Chuk Road, there is a pigsty. Further southwest is the site of Chuk Yuen Village. Further northwest is the Heung Yuen Wai Boundary Control Point. About 300 metres to the east is the NENT Landfill. **Figure 3** shows the Application Site in the context of its local surroundings.

Access

- 2.04 The Application Site is accessible via a local track that connects Lin Chuk Road to the south. Lin Chuk Road, meanwhile, connects Lin Ma Hang Road and the Heung Yuen Wai Highways to the west. A plan showing the access road is in **Figure 3**.

3. LAND STATUS

- 3.01 According to the Land Registry, the Application Site comprises Lots 768 and 769 in DD78, Ta Kwu Ling, New Territories. A plan showing the configuration of the Application Site is in **Figure 2**.
- 3.02 The subject lots are held under Block Government Lease and demised as agricultural land. The lease is virtually unrestricted, apart from the standard non-offensive trade clause, and will expire on 30.6.2047.

- 3.03 Should this application be approved, the Applicant will apply to the LandsD for a Short Term Waiver (STW) for the erection of the temporary structure.

4. PLANNING CONTEXT

Statutory Plan

- 4.01 According to the Approved Ta Kwu Ling North OZP No. S/NE-TKLN/2 gazetted on 13.5.2016, the Application Site falls within an "Agriculture" ("AGR") zone (see **Figure 4**). The planning intention of the "AGR" zone is "to retain and safeguard good quality agricultural land/farm/fish ponds for agricultural purposes. It is also intended to retain fallow arable land with good potential for rehabilitation for cultivation and other agricultural purposes." According to the covering Notes of the OZP, planning permission from the Board is required for the temporary use or development of any land or buildings not exceeding a period of three years.

Previous Application

- 4.02 No previous application has ever been submitted for the Application Site.

5. DEVELOPMENT PROPOSAL

Proposed Use

- 5.01 The Applicant seeks planning permission to use the Application Site as a temporary vehicle park for crane lorries for a period of 3 years.

Site Layout and Design

- 5.02 The Layout Plan in **Figure 5** illustrates the proposed site configuration. The Application Site is small in scale and consists of six parking spaces for crane lorries (under 24 tonnes) under two 7m high shelters (225 sq.m. each), and two private car parking spaces for staff/visitors. There will be sufficient space within the Application Site for manoeuvring vehicles. The application site will be fenced off with 2.5 m-high hoardings. The development parameters of the proposed development are summarized as follows:

Application Area	About 1,725m ²
Total Floor Area (Non-domestic)	About 450m ²
No. and Height of Structures	2 temporary shelters for parking of crane lorries (7m, 1 storey)
No. of Parking Spaces	6 (for crane lorries) (3.5m x 11m each) 2 (for private vehicles) (2.5m x 5m each)
Operation Hours	8:00am to 6:00pm, with no operation on Sundays and Public Holidays

Access and Traffic Arrangement

- 5.03 The Application Site is accessible from Lin Chuk Road to the south via a local track. The existing 6.5-metre-wide access point to the southeast of the Application Site will be maintained by the Applicant. Taking into account the nature of the business and its operational requirements, the proposed development will generate a maximum of 6 round trips for crane lorries and 2 for private vehicles per day. The estimated traffic arising from the proposed development is as follows:

Time	No. of Trips (Crane Lorries)		No. of Trips (Private Cars)	
	In	Out	In	Out
08:00-09:00 (peak hour)	0	3	2	0
09:00-10:00 (peak hour)	0	3	0	0
10:00-11:00	0	0	0	0
11:00-12:00	0	0	0	0
12:00-13:00	0	0	0	0
13:00-14:00	0	0	0	0
15:00-16:00	1	0	0	0
16:00-17:00	2	0	0	0
17:00-18:00 (peak hour)	3	0	0	2

Site Operations

- 5.04 The Application Site will only be used to park crane lorries (not exceeding 24 tonnes) owned by the Applicant. No vehicle washing, paint spraying, repairs or workshop activities will be conducted at the Application Site. The proposed operating hours at the Application Site are restricted to 8:00 a.m. to 6:00 p.m., Mondays to Saturdays, with no operation on Sundays and public holidays.

Land Filling Proposal

- 5.05 A land filling proposal (see **Appendix 4**) prepared by Ho Tin & Associates Consulting Engineers Limited is submitted together with this application. In general, the existing ground levels of the Application Site vary at about +20.5mPD on the south to about +22.5mPD on the north. The existing site levels of the whole Application Site (1,725 sq.m.) are proposed to be graded to form a gently sloping platform to accommodate the proposed parking of crane lorries. In order to protect the formed platform from erosion, the surface of the formed platform would be paved with 50mm thick bituminous material. The maximum height of filling would be about 1 metre above the existing ground level.
- 5.06 Since the proposed land filling works would merely consist of grading the existing ground levels in associated with construction of peripheral drainage channels, it would not cause any adverse impacts onto the surroundings.

Drainage Proposal

- 5.07 A drainage proposal (see **Appendix 5**) prepared by Ho Tin & Associates Consulting Engineers Limited is submitted together with this application. There are no prominent engineering

channels within the Application Site. Surface runoff of the concerned area including the Application Site would flow westward due to gravity into the existing watercourse running near the northwest of the Application Site. The watercourse runs toward the northwest direction further until reaching an existing nullah near Lin Ma Hang Road.

- 5.08 525mm U-channel at 1 in 200 gradient will be constructed at the periphery of the Application Site to intercept all crossing surface runoff. 100mm high gap will be formed at the bottom of the security hoarding/fence along the site boundary to ensure no surface runoff from the adjacent area to be obstructed. A new catchpit with trap will be constructed at the ends of the 525mm U channel before discharging into the existing watercourse to the northwest.

Fire Service Installations

- 5.09 The Applicant commits to providing fire service installations (FSIs) at the Application Site that meet the requirements of the Fire Services Department (FSD), ensuring that the proposed development can be operated safely.

Environmental Consideration

- 5.10 The entire site will be paved with 50mm thick bituminous material to minimise fugitive dust impacts from vehicle manoeuvring. Furthermore, to minimise the visual impact and any noise from site operations, no vehicle washing, paint spraying, repairing or workshop activities will be carried out within the Application Site.

6. JUSTIFICATIONS

Meeting the Relocation Needs of the Applicant

- 6.01 The Applicant is having difficulty identifying a suitable relocation site due to the ongoing resumption projects in the Northern Metropolis area. A significant number of brownfield sites in the Northern New Territories have been or will be resumed by the Government for the development of NDAs. Four prospective sites in the North District were reviewed but found to be unsuitable for various reasons (see **Appendix 3**). The Application Site is considered suitable to meet the operational needs of the Applicant due to its suitable size, affordable acquisition cost and relative isolation from residential developments. The applicant plays a significant role in the construction industry by supplying crane lorries to construction companies in Hong Kong. Approving this application would be a practical and reasonable solution to the applicant's relocation needs, as well as supporting the construction industry in Hong Kong.

Site Unsuitable for Agriculture Rehabilitation

- 6.02 Despite falling within an area zoned 'AGR', the Application Site is unsuitable for agricultural rehabilitation. The Application Site has been vacant for a considerable amount of time and has not been used for agricultural activities. As the surrounding areas of the Application Site consist mostly of burial grounds, abandoned agricultural land and unused/vacant land, the planning intention of the 'AGR' zone is not realised to its full potential until the surrounding characteristics revert entirely to agricultural use. Making more efficient use of the not fully developed

agricultural land would allow the Applicant to continue their operation and make better use of Hong Kong's scarce land resources. In contrast, interim development of the Application Site would not be deemed incompatible and would be able to flexibly meet the Applicant's operational needs and demand.

Site Falls Outside the New Territories North New Town

- 6.03 The Application Site is located outside the boundary of the New Territories North New Town project. The Applicant understands that the Application Site will not be affected by land resumption in the future. Over the next few years, the Government intends to carry out large-scale land resumption for the development of the Northern Metropolis. The Applicant can rest assured that they will be able to operate stably at the Application Site.

More Desirable Interim Use and Efficient Use of Land Resource

- 6.04 The Application Site is currently vacant. Rather than leaving it idle and allowing it to deteriorate, the proposed development provides an interim solution that makes full use of the land and involves the Applicant in upkeep and maintenance. Additionally, the proposed development would make full use of the location and the Application Site's exceptional physical state. It is considered fully commensurate with its local geographical setting and ideal for maximising land use without having an undesirable environmental impact.

Compatible with Surrounding Land Uses

- 6.05 The Application Site is located approximately 300m west of the NENT Landfill site. Its proximity to the landfill makes the area unattractive and unsuitable for NDA developments. However, the Application Site is conveniently located near a local track off Lin Chuk Road, which connects to Lin Ma Hang Road and the Heung Yuen Wai Highway. There are no residential dwellings in the immediate vicinity of the Application Site. The Application Site will only be used as a car park for crane lorries, which is a desirable interim use and compatible with the surrounding area.

No Adverse Environmental or Visual Impact

- 6.06 The Application Site will be used solely for parking crane lorries, with no workshop activities taking place there. Only two temporary shelters will be erected, and the site will be fenced off. Relevant mitigation measures will be implemented to minimise potential environmental impacts on the surrounding areas, and operating hours will be restricted to between 08:00 and 18:00. Therefore, the potential environmental impact of the proposed development is limited.

No Adverse Drainage Impact

- 6.07 The Application Site is not at risk of flooding and peripheral surface channels would be provided along the site boundary to intercept all surface runoff within the Application Site (see **Appendix 5**). The proposed development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works. Therefore, the proposed development would not cause any adverse drainage impact onto the area.

No Adverse Traffic Impact

- 6.08 The proposed development is small in scale, with a maximum of six round trips per day by crane lorries to and from the Application Site. It is anticipated that the proposed development will generate minimal traffic on Lin Cheuk Road and that this will not cause congestion should the application be approved. In addition, sufficient space will be reserved at the Application Site for manoeuvring vehicles. The minimal volume of traffic generated by the proposed development is unlikely to have an adverse effect on the existing road networks. Under no circumstances will crane lorries need to queue outside the Application Site. Therefore, no adverse traffic impact is anticipated.

Not an Undesirable Precedent

- 6.09 The proposed development is an exceptional case, justified by the demand for land for parking space due to the resumption of land. The proposed development at the Application Site would support the construction industry, particularly given the increasing number of development projects in the Northern Metropolis in the coming years. Therefore, permission should only be granted for the development outlined in this application. As the Board considers each planning application on its individual merits, the physical state of the Application Site and its surroundings is always assessed on a case-by-case basis, even if they are of a similar nature. Approval of the current application should not imply approval of other similar applications under any circumstances.

7. CONCLUSION

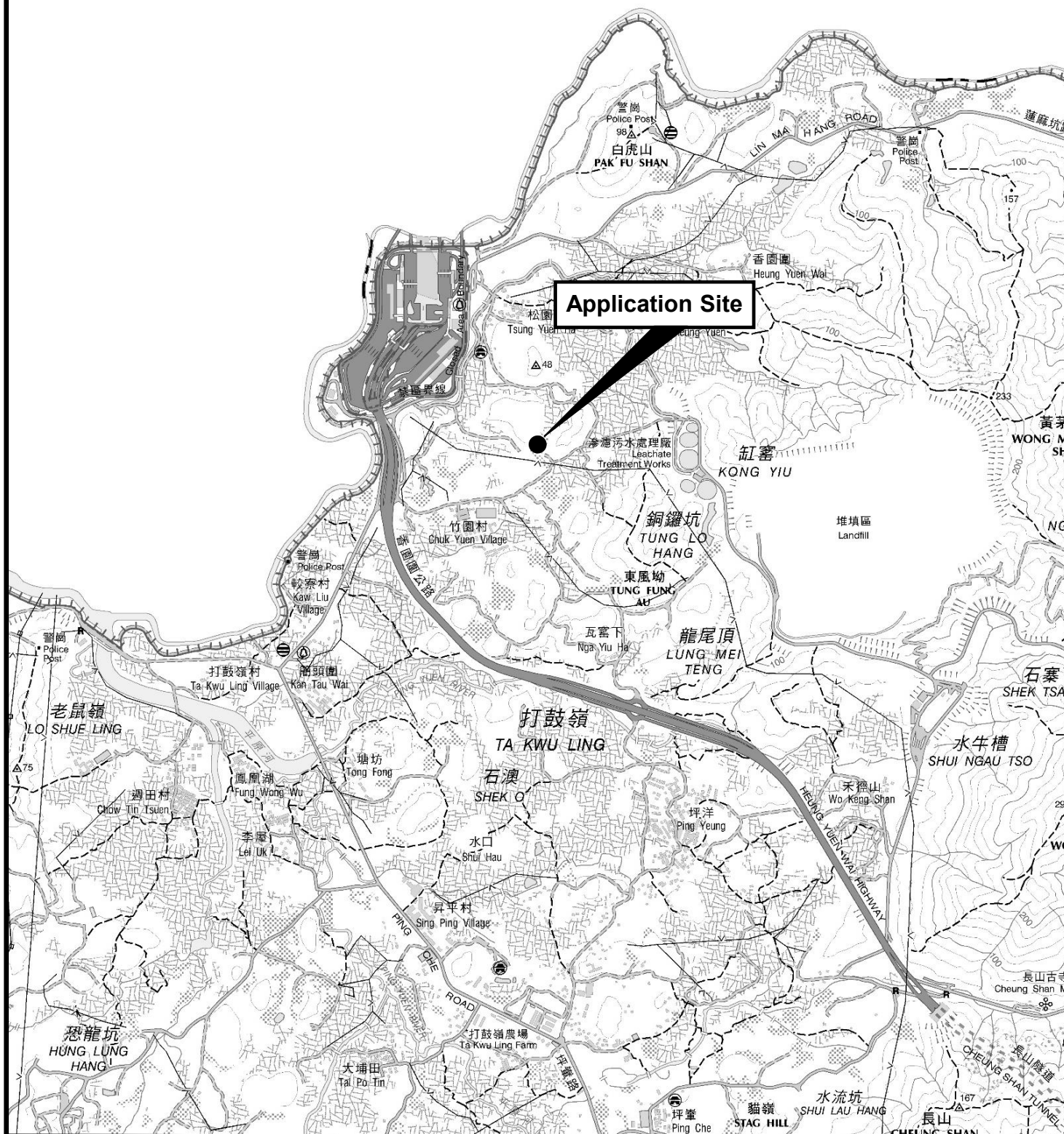
- 7.01 The Application Site falls within an “AGR” zone on the Ta Kwu Ling North OZP No. S/NE-TKLN/2. The Applicant is the operator affected by the resumption of land for the Remaining Phase Development of Kwu Tung North NDA project and this application is intended to facilitate the relocation of their businesses. Due to difficulties in identifying a suitable relocation site, the Applicant could only find temporary parking site with shorter tenancies before identifying the Application Site. Given the location of the site and the surrounding land uses, as well as the nature of the proposed development, the Application Site is considered to be the best location for the continuation of the Applicant’s operations in the form of a temporary vehicle park for crane lorries.
- 7.02 Despite the Application Site falling within the 'AGR' zone, the intended 'AGR' use is unlikely to be realised in the short term until the surrounding area is fully reverted to agricultural use. In contrast, the proposed interim development at the Application Site is deemed suitable for temporary use, with no interface problems expected to arise. Its location and geographical advantages will encourage the optimisation of valuable land resources and enable the Applicant to continue contributing to the construction industry in Hong Kong.
- 7.03 As no adverse impacts on the surrounding areas are anticipated, and the technical concerns of the relevant government departments can be addressed by implementing the approval conditions, the proposed development will not set an undesirable precedent.

7.04 In view of the foregoing submission and justifications, the Applicant respectfully requests that the Board give favourable consideration and approve this application for a period of 3 years.

Top Bright Consultants Ltd.

August 2025

蓮塘 LIANTANG



Top Bright Consultants Ltd.

Extract Plan Based on Map
Series HM20C of Sheet 3

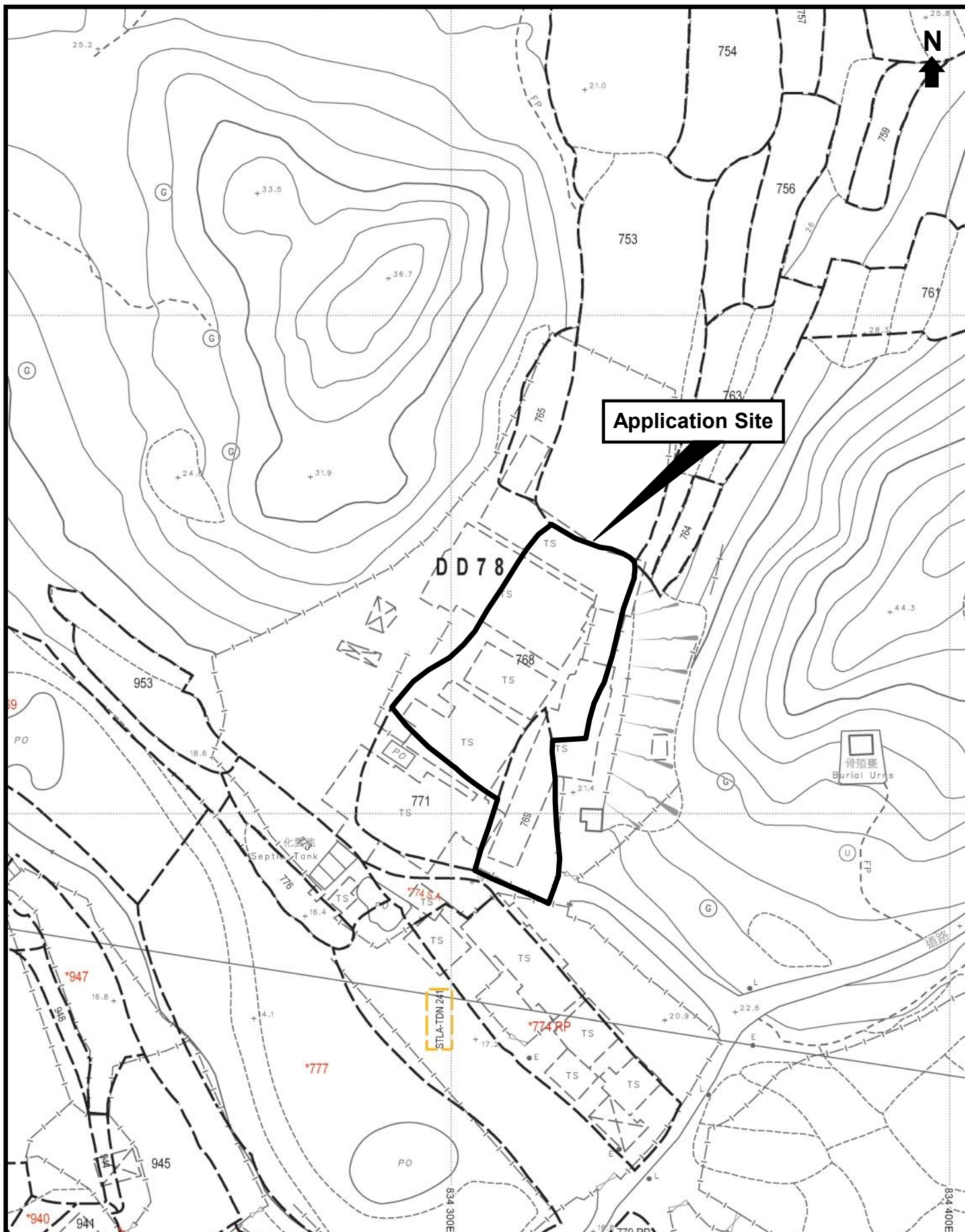
Location Plan

Scale 1 : 20 000

FIGURE 1

For Identification Purpose

Date: 30.7.2025



Top Bright Consultants Ltd.

Extracted from Lot Index Plan No.
3-NW-13B

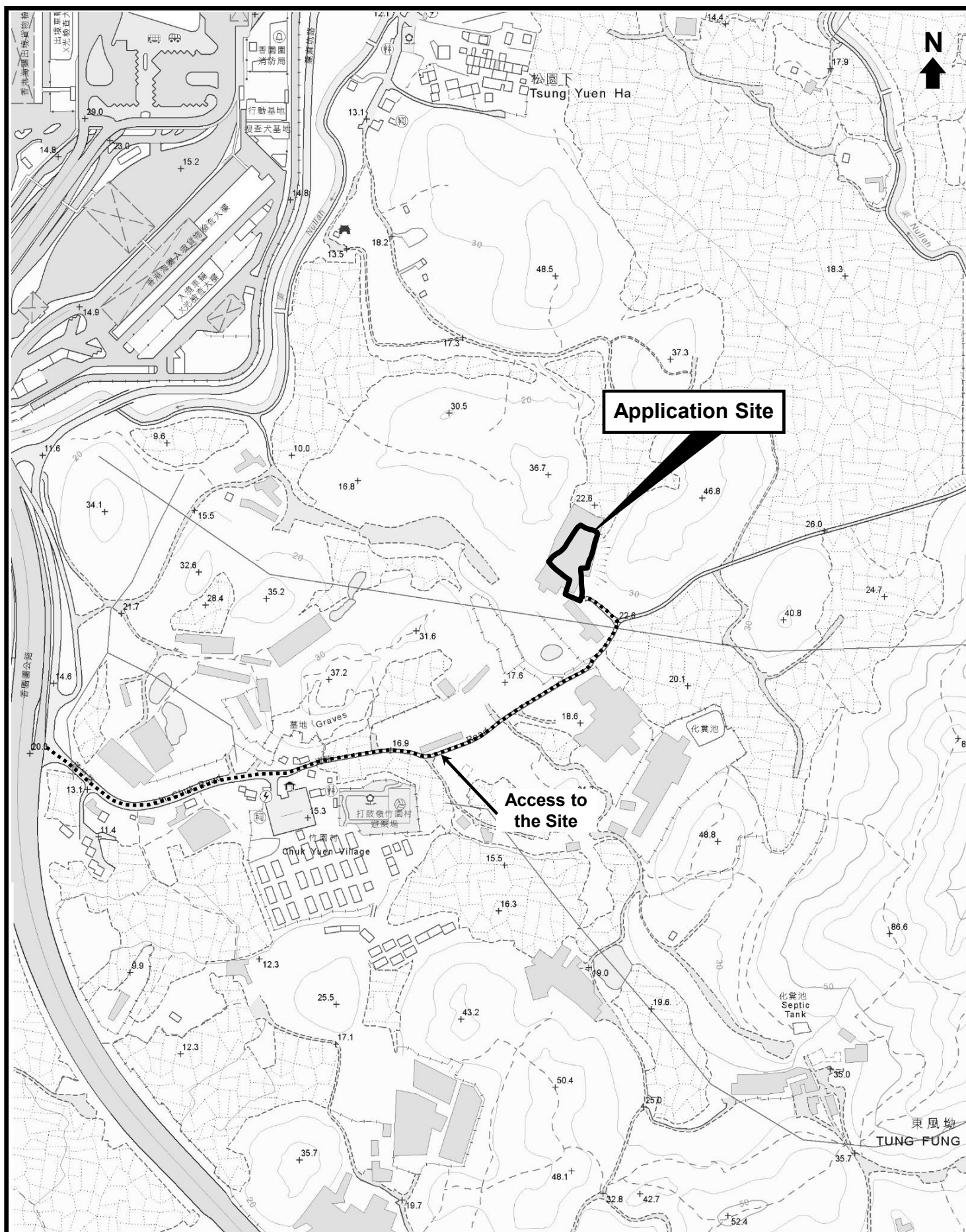
Site Plan

Scale 1 : 1 000

FIGURE 2

For Identification Purpose

Date: 30.7.2025



Top Bright Consultants Ltd.

Extracted Plan Based on Map
Series HP5C of Sheet No.
3-NW-B & D

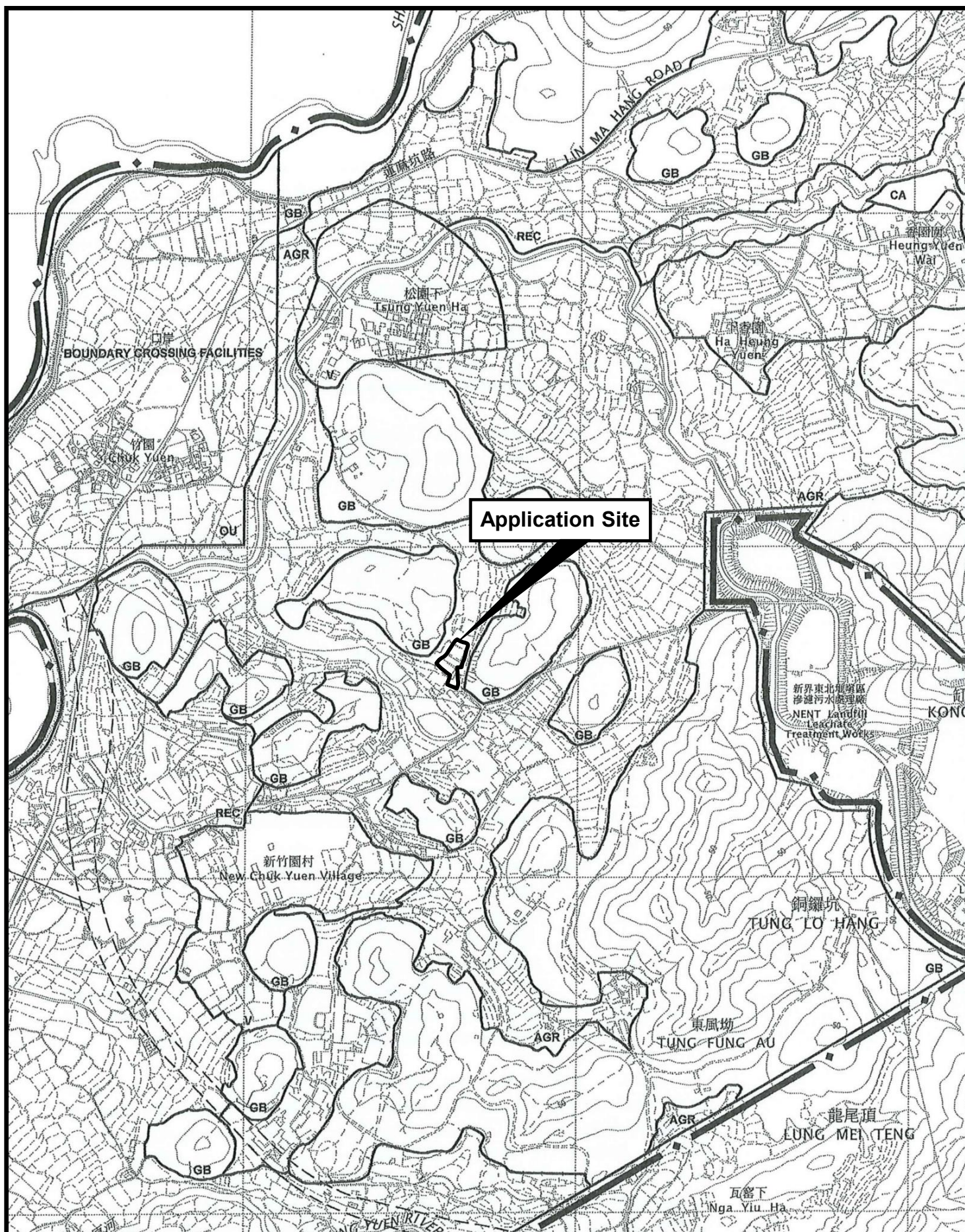
Plan Showing the General Area

Scale 1 : 5 000

FIGURE 3

For Identification Purpose

Date: 30.7.2025

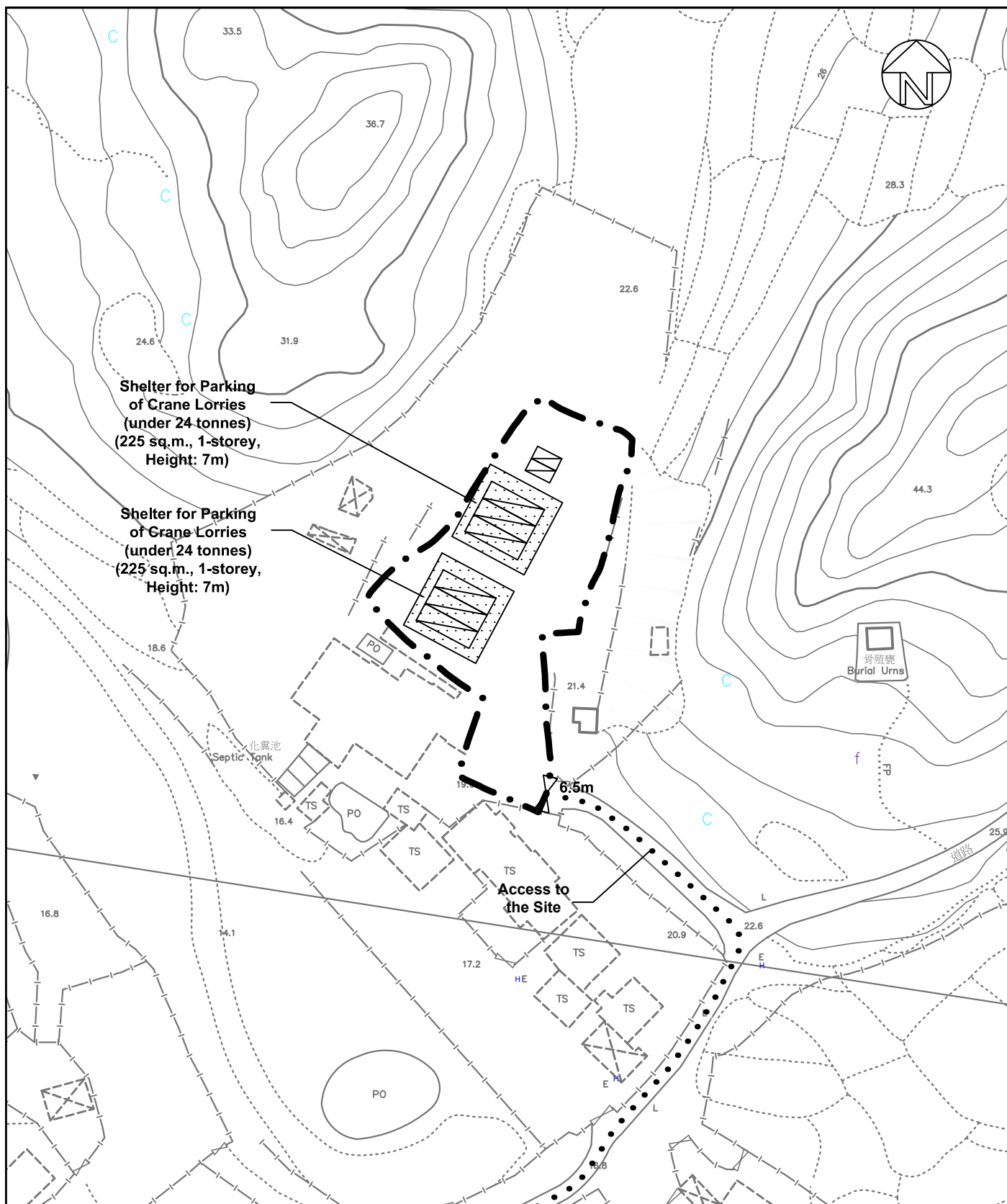


Extract from Ta Kwu Ling North Outline Zoning
Plan No. S/NE-TKLN/2 gazetted on 13.5.2016

FIGURE 4

For Identification Purpose

Date: 30.7.2025



LEGEND:

- Application Site (Area = about 1,725 sq.m.)
- Parking Spaces for Crane Lorries (3.5m x 11m)
- Parking Spaces for Staff/Visitors (2.5m x 5m)
- Shelters for Parking



Top Bright Consultants Ltd.

Drawing No. :TB/25/864/05

Layout Plan

Lots 768 and 769 in DD78,
Lin Ma Hang, N.T.

FIGURE 5

FOR IDENTIFICATION PURPOSE

Date: 30.7.2025

Scale: 1 : 1 000 (A4)