

TOWN PLANNING BOARD

**TPB Paper No. 10184
For Consideration by the
Town Planning Board on 7.10.2016**

**SUBMISSION OF THE DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A
PREPARED UNDER SECTION 25 OF
THE URBAN RENEWAL AUTHORITY ORDINANCE**

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<u>Development Scheme Area</u>	1-51 Hung Fook Street (odd nos.), 2-42A Ngan Hon Street (even nos.), the scavenging lane in between; and the adjoining pavements
<u>Area</u>	4,951 m ²
<u>Lease</u>	Private land (1-51 Hung Fook Street, 2-42A Ngan Hon Street) and Government land (scavenging lane and pavements)
<u>OZP</u>	Approved Hung Hom Outline Zoning Plan (OZP) No. S/K9/24
<u>Zoning</u>	“Residential (Group A)” (“R(A)”)
<u>Proponent</u>	Urban Renewal Authority (URA)
<u>Proposed Amendments</u>	<p>(a) To rezone the site from “R(A)” to “R(A)8” to include provisions for exemption of plot ratio (PR) calculation for an underground car park, that will accommodate car parking spaces and loading/unloading (L/U) facilities ancillary to development and/or redevelopments commenced by URA within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street, as well as coach parking spaces.</p> <p>(b) To include ‘public vehicle park (coach parking only)’ as a Column 1 use for the “R(A)8” zone and to amend the corresponding Column 2 use to ‘public vehicle park (excluding container vehicle)(not elsewhere specified)’.</p>

1. The Proposal

Urban Renewal in Kowloon City

- 1.1 According to the Urban Renewal Plan for Kowloon City recommended under the District Urban Renewal Forum (DURF) study, an area bounded by Ngan Hon Street, Ma Tau Wai Road, Bailey Street and Sung On Street was identified as a ‘proposed redevelopment priority area’ where urban renewal in the form of redevelopments led by URA or privately-led should be given priority in view of the poor building and environmental conditions in the area. The subject development scheme (DS) is located within this area (**Plan 1**).

- 1.2 An area further south bounded by Bailey Street, Ma Tau Wai Road, Hok Yuen Street and Sung Chi Street was identified as a 'proposed mixed redevelopment and rehabilitation area' where redevelopment and / or rehabilitation works are to be carried out at suitable locations having regard to the conditions of individual buildings (**Plan 1**). The Chun Tin Street/Sung Chi Street DS being considered at the same meeting under TPB Paper No. 10183 is located within this area.
- 1.3 Based on the DURF Study, the URA has undertaken a community planning study in Kowloon City/To Kwa Wan district which aims to improve living environment of an old urban district through holistic master planning. Specifically, two DSs and three development projects (DPs) were commenced by URA in the area between March and June 2016 and two earlier projects were authorised in 2010 and 2014 as set out below and shown on **Plan 2**.

Project Name	URA Project Code	Commencement/Authorisation
<i>URA Project Commenced between March and June 2016</i>		
Bailey Street/Wing Kwong Street DP ¹	KC-009	4.3.2016
Chun Tin Street/Sung Chi Street DS ² (subject of TPB Paper No. 10183)	KC-008A	6.5.2016
Hung Fook Street/Ngan Hon Street DS (subject of the paper)	KC-010	3.6.2016
Hung Fook Street/Kai Ming Street DP	KC-011	3.6.2016
Wing Kwong Street DP	KC-012	3.6.2016
<i>URA Projects Authorised before 2016</i>		
Ma Tau Wai Road/Chun Tin Street DP	TKW/1/002	17.12.2010
Kai Ming Street DP	DL-8:KC	30.5.2014
Notes:		
¹ DP are projects implemented under section 26 of the URA Ordinance, such projects conform to the zoning and planning controls under the extant OZP.		
² DS are projects implemented under section 25 of the URA Ordinance, such projects require submission to the Town Planning Board for their agreement to gazette as a DS Plan (involving new zonings and/or planning controls) under the Town Planning Ordinance.		

- 1.4 All these URA projects are for residential developments with commercial uses on lower floors. According to URA's notional schemes, the above seven project sites covered a total land area of 2.3 ha and would yield a total GFA of about 183,800 m² and some 3,380 flats. Of the total GFA, there would be some 28,900 m² of commercial GFA in the seven projects, 1,450 m² of GFA for community facilities in the two projects authorised in 2010 and 2014, and 500 m² of public open space in the project authorised in 2010. The projects were targeted for completion in 2019, 2021 and 2026.
- 1.5 Through the community planning study and the opportunities offered by the redevelopment projects, URA also proposed to redesign the local transport and road networks of the area (**Drawing 1**).

Hung Fook Street/Ngan Hon Street Development Scheme (the DS)

- 1.6 On 10.6.2016, the URA submitted the draft Hung Fook Street/Ngan Hon Street DS Plan (DSP) No. S/K9/URA2/A for the consideration Town Planning Board (the Board) in accordance with section 25(5) of the URA Ordinance (URAO). The

submission comprises the draft DSP with its Notes and the Explanatory Statements (ES); and a planning report with technical assessments and social impact assessment (SIA) (Stage 1) report (**Annex B**). On 20.7.2016, URA further submitted the SIA (Stage 2) report to the Board (**Annex D**).

- 1.7 The DS is to rezone the private lots and scavenging lane within the DS area from “R(A)” to “R(A)8” so as to include provisions to exempt the floor space of an underground car park from PR calculations and ‘Public Vehicle Park (Coach Parking Only)’ as a Column 1 use. The underground car park is intended to accommodate car parking spaces and L/U facilities ancillary to development and/or redevelopments commenced by URA within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street (the Wider Area) (see **Plan 3**) and coach parking spaces. Under the Notes of the original “R(A)” zone, only floor space for car park, loading/unloading bays for the development within the DS area may be disregarded for PR calculation, hence, URA proposed to rezone the site with new planning controls to take forward the project in the form of a DS.
- 1.8 In support of the DS, the proponent has submitted the following documents:
- (a) Letter dated 10.6.2016 (**Annex A**)
 - (b) Planning Report (**Annex B**)
 - (c) Letter dated 20.7.2016 (**Annex C**)
 - (d) SIA (Stage 2) Report (**Annex D**)
 - (e) Letter dated 5.8.2016 providing responses to comments of Government departments (part 1) (**Annex E-1**)
 - (f) Letter dated 23.8.2016 providing responses to comments of Government departments with revised technical assessments (part 2) (**Annex E-2**)
 - (g) Letter dated 7.9.2016 providing responses to comments of Government departments (part 3) and responses to public comments (**Annex E-3**)
 - (h) E-mail dated 12.9.2016 providing revised DSP, Notes and ES (**Annex E-4**)
 - (i) Letter dated 14.9.2016 providing responses to comments of Government departments (part 4) (**Annex E-5**)
 - (j) E-mail dated 29.9.2016 providing responses to comments of Government departments (part 5) (**Annex E-6**)
- 1.9 The purpose of this paper is to invite the Board to consider the draft DSP together with the Notes and ES as submitted by URA as suitable for gazetting under section 5 of the Town Planning Ordinance (TPO) (**Annex E-4**).
- 1.10 The area proposed to be rezoned “R(A)8” on the DSP is currently zoned “R(A)” on the approved Hung Hom OZP No. S/K9/24, which covered private lots at 1-51 Hung Fook Street (odd nos.), 2-42A Ngan Hon Street (even nos.) and the scavenging lane between the private lots (the Area) (**Plans 2 and 3**). URA has included the

adjoining pavements where there are some overhanging of the affected buildings within the boundary of the DS, to facilitate its implementation of the project.

- 1.11 According to URA's notional scheme, three residential towers over two 3-storey commercial podia with a basement car park is proposed. The preliminary design drawings are in Appendix 1 of **Annex B (Drawings 3 and 4)** and the development parameters are set out in the table below.

Gross site area	4,951m ²		
Net site area ¹	4,562m ²		
Proposed zoning	“R(A)8”		
Total PR	9		
- maximum domestic PR	- 7.5		
- non-domestic PR	- 1.5		
Total GFA	41,058m ²		
- maximum domestic GFA	- 34,215m ²		
- maximum non- domestic GFA	- 6,843m ²		
Maximum building height	100mPD		
No. of Towers	3		
No. of Floors			
- domestic floor	22		
- clubhouse floor	1		
- commercial podia	3		
- basement car park ²	2		
No. of flats	about 750		
Parking Spaces (not more than)	For the DS	For Wider Area ³	Total
- Car parking spaces	110	164	274
- Loading /Unloading bays	12	8	20
- Coach parking spaces	4		
Notes:			
¹ Net site area is the area adopted for PR calculation, subject to survey and detailed design.			
² In the responses to comments in Annex E-1 , URA indicated that the exact extent of the car park and the number of levels will be determined at the detailed design stage.			
³ See Plan 3 .			

Development Intensity

- 1.12 The gross site area of the DS is 4,951m² and the net site area for PR calculation is 4,562m², including all private lots within the DS and the scavenging lane but excluding pavements. The proposed domestic and non-domestic PRs conform to the restrictions for the original "R(A)" zone, i.e. maximum PR of 9 for a building that is partly domestic and partly non-domestic and PR of the domestic part not exceeding 7.5.
- 1.13 A building height (BH) restriction of 100mPD is proposed to be stipulated in the DSP, being the same as the BH restriction stipulated on the original "R(A)" zone.

Exemption of Underground Car Park from PR Calculation

- 1.14 URA proposed that the underground car park that is intended to accommodate car parking spaces and L/U bays ancillary to developments and/or redevelopments commenced by URA within the Wider Area should be exempted from PR calculation. URA stated in the ES that the management of the underground car park will be arranged at the development stage.
- 1.15 It is stated in paragraph 7.7 of the ES that the number of car parking and L/U facilities in the underground car park will be as required by government and in any event there shall be not more than 274 car parking spaces and not more than 20 L/U bays. The maximum provisions stated in the ES are based on the upper range of the relevant requirements under the Hong Kong Planning Standards and Guidelines (HKPSG) to serve the URA development and other redevelopment projects within the Wider Area that had commenced or are under consideration by URA with a total GFA of about 104,000 m² and about 2,000 residential units. It is further set out in the TIA in Appendix 2 of **Annex B** that the developments within the Wider Area that are taken into account in calculation of the above car parking spaces and L/U bay requirements are as follows:
- (a) the DS on the site;
 - (b) the Hung Fook Street/Kai Ming Street DP;
 - (c) the Wing Kwong Street DP;
 - (d) the Kai Ming Street DP; and
 - (e) other development sites within the Wider Area (these are under planning/consideration by URA and which location had to be kept confidential at this stage).
- 1.16 A maximum of four coach parking spaces will be provided in the underground car park. The coach parking is required by Commissioner for Transport (C for T) to cater for the need for such parking in the district.
- 1.17 For the other URA projects within the Wider Area to be served by the underground car park, there will only be limited on-site L/U bays and no on-site car parking spaces. The on-site L/U bays to be provided in the other developments in the Wider Area will be in the form of on-street lay-bys within the respective project sites (revised Drawing 3.5 of the TIA in **Annex E-2**).

Local Road Improvement

- 1.18 A through road connecting Hung Fook Street and Ngan Hon Street is proposed within the DS on ground level above the underground car park. The road in the DS is 13m-wide, with a one-way north-bound carriageway (8m) and footpaths (2.5m) on both sides. The proposed through road will form part of URA's proposed new north-south running road network extending towards Wan On Street in the south through the site boundaries of three other URA DPs at Bailey Street, Wing Kwong Street and Hung Fook Street/Kai Ming Street (**Drawing 1**).

- 1.19 The above road works will be always permitted in the DS and their implementation including resumption of private land under Lands Resumption Ordinance (Cap 124) and relevant gazettals under Roads (Works, Use and Compensation) Ordinance (Cap 370) will be taken forward by URA upon completion of the statutory procedures under the TPO.

Proposed Amendments to Notes

Planning Intention

- 1.20 The planning intention of the original “R(A)” zone will be retained under the proposed “R(A)8” zone, with the additional stipulation that “to provide an underground car park to accommodate car parking spaces and L/U facilities ancillary to developments and/or redevelopments commenced by URA within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street as well as coach parking spaces to cater for the need for coach parking in the district” will be added. The above area to be served by the underground car park is referred to as the Wider Area and delineated on Plan 1 of the ES.

Exemption from PR calculation for Underground Car Park

- 1.21 It is proposed to state in the Notes that the number of car parking spaces, L/U bays and coach parking spaces to be provided in the underground car park shall be as required by government and may be disregarded for PR calculation.

Coach Parking Spaces

- 1.22 As the coach parking spaces to be provided in the underground car park are for use by the public, it falls within the definition of ‘public vehicle park’. To cater for this, the original Notes of the “R(A)” zone is amended by including ‘public vehicle park (coach parking only)’ as a Column 1 use that is permitted as of right and amending the corresponding Column 2 use to ‘public vehicle park (excluding container vehicle)(not elsewhere specified)’.

2. Justifications provided by URA

General

- 2.1 The DS will replace the old buildings of over 50 years in deteriorating conditions to new modern residential development with commercial facilities. The living conditions of the existing over-crowded and sharing households in sub-divided flats will be improved. There will be provision of more small to medium-sized flats in the urban area.

Underground Car Park to Serve the Local Community

- 2.2 Provision of an underground car park at one site can reduce car park ingress/egress points and avoids disruption to traffic and pedestrian movements in the other developments within the Wider Area. It will also help to maintain continuous and active street frontages at the other sites in the Wider Area where pedestrians can enjoy safe walking and street vibrancy can be enhanced.

- 2.3 Among the sites within the Wider Area to be served by the underground car park, the site area of the DS is relatively large and will allow for more efficient car park layout.

Exemption of Underground Car Park for PR Calculation

- 2.4 URA considers that as the underground car park is for accommodating car parking and L/U bays ancillary to the developments and/or redevelopments within the Wider Area and the coach parking spaces are to cater for such need in the district, they should be disregarded for PR calculation.

Local Road Improvement

- 2.5 The proposed road through the DS will improve traffic and pedestrian accessibility. The through road will reduce circulating traffic at the main public road network around the area by providing alternative more direct routes via the new road system (see paragraph 3.7 of TIA in Appendix 2 of **Annex B**).
- 2.6 The setback of the podium of the development in the DS for widened footpath will enhance pedestrian street vibrancy.

Technical Assessments

- 2.7 Technical assessments, including traffic impact assessment (TIA) (Appendix 2 of **Annex B**), and revised drainage, sewerage and water impact assessments and revised environmental assessment (EA) in **Appendix E-2** are submitted by URA to demonstrate that the proposed DS would not cause adverse traffic, environmental, drainage, sewerage and water impacts.

3. Background

- 3.1 The DS is included in the approved URA's 15th business plan (2016/17). On 3.6.2016, the URA published the notification of commencement of the Hung Fook Street/Ngan Hon Street DS in Government gazette under section 23(1) of the URAO. On the same day, URA also published the notification of commencement of the Hung Fook Street/Kai Ming Street DP and the Wing Kwong Street DP (that are both within the Wider Area) under section 23(1) of the URAO. On 10.6.2016, URA submitted the draft DSP to the Board for consideration.
- 3.2 According to section 25(6) of the URAO, the Board may deem the draft DSP as suitable for publication, or being suitable for publication subject to such amendments as the Board shall specify, or refuse to deem the DSP as being suitable for publication. If the Board deems the draft DSP suitable for publication under section 25(7) of the URAO, the DSP shall be deemed to be a draft plan prepared by the Board for the purposes of the TPO and the provisions of the TPO shall apply accordingly. These include exhibition for public inspection, consideration of representations and comments, and submission of the draft DSP to the Chief Executive in Council for approval.

4. **Development Scheme Boundary**

- 4.1 According to URA, the scheme boundary was delineated based on several factors, including building conditions (building structure, fire safety and building services), building height, building age, local environmental conditions, size of site, development potential, planning gain, ownership distribution and financial viability etc.
- 4.2 According to URA, the tenement buildings within the DS are 6 to 8 storeys and were built between 1957 and 1960. The buildings are mainly residential on the upper floors with commercial premises on the ground floor including vehicle repair workshops, retail shops for sale of construction materials, restaurants and beauty salon. Staircase shops are found in some buildings. Based on URA's building condition survey updated in May 2016, the buildings are in deteriorating conditions and spalling concrete and cracks are found in the staircase of the buildings. There are sub-divided units in the buildings and unauthorized building structures on the roofs of many buildings. The living condition in the DS is considered crowded and not satisfactory.

5. **Social Impact Assessment**

- 5.1 According to section 25(3) of the URAO, an assessment of the likely effect of the implementation of the DS should be prepared by the URA. In accordance with the guidelines stipulated in the Urban Renewal Strategy, the URA should undertake a Stage 1 SIA before publication of the DS and a Stage 2 SIA after the freezing survey to fully assess the social impact of the proposed project and the social re-housing needs of the residents affected. From 3.6.2016 to 5.6.2016, a SIA survey for the Hung Fook Street/Ngan Hon Street redevelopment project was conducted by URA to survey the opinions of people for planning purposes together with the freezing survey. The SIA (Stage 1) report is in Appendix 5 of **Annex B** and the SIA (Stage 2) report is in **Annex D**.
- 5.2 A brief summary of the findings of the two stages SIA is as follows:

	<u>Development Scheme Area¹</u>	<u>Territorial Level²</u>
Population and Household Characteristics		
Total population	1,462	7,071,576
Average household size	2.5	2.9
Degree of sharing ³		
Household (588) / original units (368)	1.57	1.0
Household (307) / units with sub-divided flats (135)	2.3	-
Age group		
0-14	14.3%	11.6%
15-24	12.5%	12.4%
25-64	60.6%	62.7%

65 & above	12.6%	13.3%
Socio-economic Characteristics		
Monthly income (per month/household)		
below HK\$10,000	30%	24%
below HK\$4,000	10%	9%
Unemployed	6.4%	3.4% (March 2016 – May 2016)
% of households receiving comprehensive social security assistance (CSSA)	10% (49 households)	5.3% (as at end of 2014)
Residing in Hong Kong for less than 7 years	6%	-
Household Type		
Owner-occupiers	34% (199 households)	52%
Tenants (including principal tenants and sub-tenants)	64% (376 households)	
Licensee	2% (13 households)	
Groups with Special Needs		
Persons with disability	1.8% (26 residents)	-
No. of single-parent families	29 (with 38 children)	-

Notes:

- ¹ Based on 588 households (i.e. 576 successfully interviewed households living in the project area and owners of 12 vacant units that has submitted freezing survey/SIA forms)
- ² Based on the 2011 Population By-census (except CSSA and unemployed population which is based on statistics from the Census and Statistics Department)
- ³ The original number of domestic units in approved general building plans/occupation permits are 368. Of these, 135 units were found to be sub-divided.

5.3 The findings of the survey for the redevelopment project also indicated the following:

- (a) impact on employment - the majority (70%) of owner-occupiers and more than half of the tenants (65%) considered that the project would have positive impact or no impact; whilst 11% of owner-occupiers and 19% of tenants considered that there would be negative impact;
- (b) impact on economic conditions - 50% owner-occupiers and 45% tenants considered that there would be positive impact or no impact; whilst 25% owner-occupiers and 37% tenants considered that there would be negative impact;
- (c) impact on social network - 61% owner-occupiers and 63% tenants considered that their current social networks would not be affected; whilst 31% of owner-occupiers and 27% of tenants considered that social network would be affected;

5.4 In terms of business impact, 75 non-domestic premises (including 57 ground floor premises, 16 staircase structures and 2 cockloft premises) were identified. Of the 71 operators who answered the freezing survey, 17% were owners and 66% were

tenants. Of the 68 of them who answered both freezing survey and the SIA forms, 47% strongly supported or supported the proposed redevelopment while 16% either did not support or was strongly against the proposed development.

- 5.5 According to the detailed SIA (Stage 2) report, the Social Service Team (SST) has successfully contacted 242 households with problem or enquiry related to compensation, rehousing and tenancy being identified. Immediately after the SIA survey, URA had arranged 4 public briefing sessions for all the stakeholders with questions on freezing survey, planning, acquisition and compensation and rehousing issues being addressed. The URA also answer enquires and provides hotline services to residents with matters of enquiry covering project information, acquisition compensation and rehousing as well as household survey.
- 5.6 The URA considers that the prevailing compensation and rehousing policies and arrangements, coupled with the services offered by the SST, will be sufficient to reasonably mitigate the impact on majority of the residents/business operators arising from the proposed redevelopment. The major mitigation measures being pursued include, inter alia, organizing outreach activities, offering assistance in finding public rental housing for eligible persons in need, conducting initial assessment of elderly, disability and other vulnerable groups for eligibility of compassionate housing, providing orientation assistance for those in need after moving home and providing assistance to identify suitable replacement premises for affected businesses.
- 5.7 The SIA (Stage 1) and SIA (Stage 2) reports were circulated and no adverse comments were received from concerned government departments.

6. Implementation

- 6.1 The tentative implementation programme is in Appendix 6 of **Annex B**. Construction of the project will commence around 2021 for completion around 2025.
- 6.2 The URA does not own or lease any land within the boundaries of the DS and will acquire the property within the DS by purchase. Documents detailing URA's principles for acquisition and resumption of affected properties as well as URA's rehousing and ex-gratia payment packages for domestic and non-domestic tenants are in Appendices 7 and 8 of **Annex B**.

7. The Development Scheme and its Surrounding Areas (Plans 1 to 3, and photos in Plans 5 to 8)

- 7.1 The DS is:
- (a) located in the northern part of Hung Hom;
 - (b) bounded by Hung Fook Street to the south, Ngan Hon Street to the north, Wing Kwong Street to the east and a backlane (abutting 17 to 31 To Kwa Wan Road) to the west;

- (c) Ngan Hon Street and Hung Fook Street are both east bound with dead ends at the western end of Hung Fook Street (near To Kwa Wan Road) and eastern end of Ngan Hon Street (near Sung On Street). The section of Wing Kwong Street connecting the above two streets and abutting the site is southbound; and
- (d) characterised by 6 to 8-storey tenement buildings built between 1957 to 1960. The existing building conditions are detailed in paragraph 4.2 above.

7.2 The surrounding areas have the following characteristics:

- (a) the street block to the north of Ngan Hon Street are mainly residential buildings built between 1971 and 1974 with two newer developments built in 2001;
- (b) the remaining areas within the Wider Area are predominantly residential buildings with commercial premises on the ground floor. With the exception of Wong Teck Building that was built in 1974 and Kai Ming Mansion built in 1963, the other buildings within the Wider Area were all built between 1953 to 1959. Some of the tenement buildings to the south are included in the Hung Fook Street/Kai Ming Street DP and the Wing Kwong Street DP. There is an authorised URA project at the junction of Wing Kwong Street and Kai Ming Street (**Plans 2 and 4**);
- (c) the URA Bailey Street/Wing Kwong Street DP is located to the further south, immediately abutting the south side of Wing Kwong Street (**Plan 2**);
- (d) further east and northeast across Sung On Street and Yuk Yat Street are two schools and the Hoi Sham Park (**Plan 2**); and
- (e) the planned Ma Tau Wai Station of the Shatin to Central Link is located to the west across To Kwa Wan Road (**Plan 2**).

8. Planning Intention

The DS area is currently zoned “R(A)” on the OZP. The planning intention of the “R(A)” zone is primarily for high-density residential developments. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building. The DS area is to be rezoned “R(A)8”, the planning intention of the original “R(A)” zone will be retained and the intention about provision of an underground car park to serve the Wider Area will be added.

9. Comments from Relevant Government Departments

9.1 The following Government departments have been consulted and their comments on the draft DSP and URA’s responses are in **Annexes E-1, E-2, E-3, E-5 and E-6**. Their major comments are summarised below.

9.2 Comments of the District Lands Officer/Kowloon West (DLO/KW) and Chief Estate Surveyor/Urban Renewal (CES/UR), Lands Department (LandsD):

- (a) the new through public road should be formed and constructed by URA under the conditions of land grant and should be opened up for public use. Subject to agreement of Transport Department (TD) and Highways Department, the new road will be surrendered and delivered up to the government on demand;
- (b) URA should clarify mechanisms that could ensure that the parking spaces, L/U facilities in the underground car park will be used to serve the Wider Area and enforceable under lease. Otherwise, URA may need to explore other administrative / statutory means for enforcing the underground car park for the Wider Area;
- (c) the car park and L/U bays that are considered additional to the provision in the DS site as well as the coach parking may be accountable for GFA calculations under the lease;
- (d) the proposed mechanism for the shared use of car park facilities and GFA exemption, if any, of the said additional parking/ L/U provisions shall be governed by lease conditions of the future land grant and will be subject to agreement among relevant government departments during the lease processing stage;
- (e) the existing government lane at the rear of 17 to 31 To Kwa Wan Road should be maintained as service lane to serve those lots; and
- (f) other detailed comments regarding land grant boundary, road gazettal procedures and land resumption were raised, and LandsD will reconsider the whole development at the land grant application stage.

9.3 Comments of the Chief Building Surveyor/Kowloon, Buildings Department (CBS/K, BD)

- (a) the proposed extinguishment and inclusion of an existing scavenging lane for site area and PR calculations will contravene the Buildings Ordinance (BO). Submissions are required to be made to the Building Authority for exemptions and modifications under the BO;
- (b) sustainable building design (SBD) requirements should be incorporated in the DS;
- (c) car parking spaces and L/U bays that are intended for use by the occupants of other developments in the Wider Area and the coach parking spaces should be included in GFA calculations under the Building (Planning) Regulations; and
- (d) detailed comments under the BO can only be formulated at the building plan submission stage.

9.4 Comments of C for T :

- (a) no adverse comment on URA's proposed DSP, underground car park, new road system, and proposal for reprovisioning of on-street car parking space; and
- (b) suitable improvement works at the junction of To Kwa Wan Road and Ngan Hon Street should be considered to allow long vehicles, in particular fire engines, accessing the proposed development via Ngan Hon Street without making a detour.

9.5 Comments of the Director of Social Welfare (DSW):

A 'wish list' of social welfare facilities (including residential care home and activity centres for the elderly; integrated vocational rehabilitation service centre, care and attention home/hostels for persons with various physical and mental disabilities; hostel for single persons; integrated family service centre etc.) is suggested for inclusion in the various URA projects in the area.

9.6 Comments of the Chief Town Planner/Urban Design & Landscape, Planning Department (CTP/UD&L, PlanD):

- (a) no particular comment on proposed development that is in keeping with the restrictions under the existing "R(A)" zone from urban design perspective; and
- (b) the DS does not fall within any major air path/breezeway in the district. Based on the design parameters of the proposed development, the proposal does not fall within the categories in which an AVA is required in accordance with the joint HPLB-ETWB Technical Circular No. 1/06 on AVA. It is anticipated that the proposal would not create significant impact on the pedestrian wind environment of its surrounding.

9.7 Comments of the Director of Environmental Protection (DEP) :

URA has conducted EA for the URA project at Hung Fook Street/Ngan Hon Street. According to the EA, insurmountable environmental impacts are not anticipated from the project with implementation of recommended mitigation measures. DEP has no adverse comment on the Sewerage Impact Assessment and EA. Other minor amendments on the assessments are subject to acceptance of DEP after detailed checking.

9.8 URA's responses to the above departmental comments on the draft DSP are at **Annexes E-1, E-2, E-3, E-5 and E-6** and briefly highlighted below:

- (a) URA will liaise with LandsD regarding appropriate lease conditions that are enforceable to fulfil the intention of the underground car park. URA as the grantee of the sites can enforce the management of the underground car park through Development Agreement and management company which the Authority trust are enforceable mechanisms;

- (b) URA will handle matters relating to BO at a later stage and will incorporate SBD requirements;
- (c) regarding SWD's requirements for social welfare facilities, URA responded that an underground car park and a through road network are already provided as planning gains. Besides, a neighbourhood elderly centre will be provided at the development project at the Kai Ming Street DP. As such, no additional social welfare facility will be provided in the DS;
- (d) regarding TD's proposed road improvement works, URA has no objection to undertake suitable improvement works at the junction of To Kwa Wan Road and Ngan Hon Street to allow long vehicles accessing to the development without making a detour; and
- (e) Regarding sewerage impact, URA indicated that in the planning stage assessment, no significant sewerage impact is anticipated. However, the Sewerage Impact Assessment will be updated during the detailed design stage and upgrading or improvement works may be required.

10. **Public Consultation**

- 10.1 Under the administrative arrangements to enhance transparency in the processing of draft DSP submitted after the commencement of the Town Planning (Amendment) Ordinance, the draft DSP (including SIA (Stage 1) report) and the SIA (Stage 2) report were made available for public inspection/comment in the Planning Enquiry Counters of PlanD starting from 14.6.2016 to 5.7.2016 and from 25.7.2016 to 8.8.2016 respectively.
- 10.2 During the two inspection periods of the draft DSP and SIA (Stage 2) Report, a total of 12 comments were received with 3 comments supporting, 4 objecting and 5 expressing views. 10 comments are from general public, one from local concern group and one from the Incorporated Owners of Yuk Shing Street/Hung Fook Street.
- 10.3 URA has provided responses to the public comments in **Annex E-3**. The main public comments and URA's responses are summarised below:

Public Comments	URA's responses
<i>Support the DS</i>	
(a) Three comments, submitted by residents/owners of premises within the DS, supported the project due to safety concern and deteriorating building condition. The redevelopment process should be expedited.	Noted.
<i>Inclusion of other lots in the DS</i>	
(b) Objected to the DS and	The URA considers different

<p>considered that the lots in the vicinity, that are similarly dilapidated, should be included in the DS for more comprehensive planning. The lots mentioned in the comments covered all remaining lots within the Wider Area (other than the lots abutting To Kwa Wan Road between Kai Ming Street and Wing Kwong Street).</p> <p>(c) 2 commenters did not indicate objection but suggested the inclusion of Nos. 1 to 31 To Kwa Wan Road, that are similarly old and deteriorated, into the DS.</p>	<p>factors such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability etc. for determining the suitability of a site for redevelopment.</p> <p>URA will not consider including all the lots mentioned in the comments into the DS as the lots are not all in dilapidated conditions for redevelopment; one of the buildings Eiver House has been renovated; and including the lots into the subject DS will not have planning gains. For those lots that are separated from the DS, it is not practical to include them into the subject DS as it might involve road closures and cause adverse traffic impacts. URA will continue to consider whether there are buildings within the area that are in need for redevelopment and they may be considered as separate URA projects.</p>
<p><i>Transport</i></p>	
<p>(d) The new road will not help to alleviate traffic that is at the bottle-neck of Ma Tau Wai Road, To Kwa Wan Road and Ma Tau Kok Road. It will create more traffic jam junction, pollution and reduce space for development.</p>	<p>The proposed new road extending from Wan On Street up to Ngan Hon Street will improve accessibility of the DS and adjacent areas and alleviate traffic congestion of To Kwa Wan Road and Ma Tau Wai Road and reduce detour traffic within the area as detailed in the TIA (paragraph 3.7 of Appendix 2 in Annex B).</p>
<p>(e) There was doubt on the need for the coach parking in view of possible re-positioning of tourism strategy that may no longer target Mainland tourists, and the sustainability of the tourist shops in the area.</p>	<p>The provision of coach parking spaces is a requirement imposed by TD. URA has no objection if such requirement is waived.</p>
<p>(f) The proposed road will dissect sites into small parcels that are not optimal for comprehensive redevelopment.</p>	<p>The road design has taken into consideration the permitted development parameters as well as the layout and design of the proposed residential towers in the DS.</p>

(g) The new road will cause air and noise pollution and concerns on traffic and pedestrian safety.	The TIA and revised EA in Appendix 2 of Annex B and Annex E-2 respectively have addressed and demonstrated that the proposed redevelopment and road will not cause adverse traffic, air quality and noise impacts. Road safety issues have been considered and pedestrian crossing are proposed at appropriate locations in the new road system.
<i>Other Matters</i>	
(h) Redevelopment brings about various adverse impacts on delay in maintenance and rehabilitation of buildings.	Property owners should carry out required works to maintain the buildings and fulfil government orders on building and fire safety.
(i) Preferred rehabilitation over redevelopment.	Buildings in the DS are in dilapidated conditions, and living environment is over-crowded especially with the sub-divided flats. Redevelopments is considered appropriate and 80% of the respondents in the SIA (Stage 2) expressed support/strong support for the DS.
(j) Suggested elderly owner occupiers of domestic property to be eligible for purchase of home ownership scheme flats.	Under the prevailing compensation policy, URA will offer an owner-occupier of domestic property the market value of his property plus an ex-gratia house purchase allowance.
(k) Demand 'flat-for-flat' re-housing within the same district.	URA may offer 'flat-for-flat' in a URA new development in-situ or in the same district as URA may select for the purpose.

- 10.4 On 23.6.2016, URA consulted the Housing and Infrastructure Committee (HIC) of the Kowloon City District Council (KCDC) on the draft DSP and the two DPs at Kai Ming Street and Wing Kwong Street. The minutes of the meeting are in **Annex F** and the main comments of members and URA's responses are summarised below:

Members Comments	URA Responses
(a) Two members indicated support for URA's proposed redevelopments.	Noted.

(b) URA should include other building lots in the vicinity of Wing Kwong Street, Yuk Shing Street, To Kwa Wan Road and Kai Ming Street for more comprehensive redevelopment. The community based planning recommended by URA cannot improve traffic problem in the area. URA should also proceed with the redevelopments in Ma Tau Wai.	The URA considers different factors such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability etc. for determining the suitability of a site for redevelopment. Comments about inclusion of other lots into the DS boundary may be submitted to the Board upon gazettal of the DSP for the Board's consideration.
(c) URA's back-up plan if the DSP is not approved by the Board.	The commenced DS will be ceased but the URA will re-examine the project and consider whether there is room for amendments.
(d) Details about the underground car park in the DS.	The underground car park will accommodate about 270 car parking spaces and the actual number will be determined by government at a later stage. The car parking spaces are to serve the DS as well as the three other URA projects at Hung Fook Street/Kai Ming Street, Kai Ming Street and Wing Kwong Street as well as other projects within the Wider Area under consideration.
(e) Whether coach parking will be provided in the underground car park?	As required by TD, a maximum of 4 coach parking spaces will be provided in the underground car park.
(f) How will URA assist owners of properties in the DS and DPs to defer implementation of the government order for building and fire safety?	The redevelopment process is prolonged and the owners have an obligation to fulfil the relevant government orders before the properties are sold to URA. URA will repay part of the rehabilitation expenses if the properties are acquired within 7 years from fulfilment of the relevant government orders.
(g) How can owners/tenants aggrieved by the SIA (Stage 2) report seek remedy?	If any persons wish to rectify wrong information provided in the freezing survey, they can write to the URA. The information will be verified when URA proceed

	with the redevelopment.
(h) Where should expelled tenants seek assistance?	The affected persons may approach URA's STT for assistance.

11. Consultation

- 11.1 Relevant Government bureaux and departments have been consulted and their comments have been incorporated in the above paragraphs.
- 11.2 The following Government departments have no comment on the draft DSP/URA's responses to departmental comments (**Annexes E-1 to E-3**):
- (a) Secretary for Development;
 - (b) Chief Engineer/Construction; Water Supplies Department;
 - (c) Director of Fire Services;
 - (d) Chief Highways Engineer/Kowloon, Highways Department;
 - (e) Chief Engineer/Mainland South, Drainage Services Department;
 - (f) Director of Leisure and Cultural Services; and
 - (g) District Office (Kowloon City), Home Affairs Department.

12. Planning Department's Views

- 12.1 For the following reasons, the PlanD has no objection to the draft DSP:

Development Intensity

- (a) the PR restrictions proposed to be stipulated in the Notes are in line with the PR restrictions for the original "R(A)" zone, that is, a maximum PR of 9 for a development that is partly domestic and partly non-domestic and with the maximum PR for the domestic part not exceeding 7.5;

Provision of One Underground Car Park in the DS

- (b) the proposal for providing one underground car park in the DS to accommodate the ancillary car parking spaces and L/U facilities for developments and/or redevelopments commenced by URA within the Wider Area is supported. URA's justifications including reduction of ingress/egress points and retention of more continuous street frontages in the other development sites are agreed;
- (c) URA will also provide limited L/U facilities in the other developments and/or redevelopments in the Wider Area for serving the daily needs of the residents and shop owners, including moving house, goods delivery and refuse collection etc. URA has proposed relocation of meter car parks and motor cycle parking spaces affected by the proposed through road (revised Figure 3.5 of TIA in **Annex E-2**). C for T has no adverse comment on these arrangements;

- (d) it is proposed in the Notes that the number of car parking spaces, L/U bays and coach parking spaces to be provided in the underground car park shall be as required by government. The maximum provision figures (maximum of 274 car parking spaces and 20 L/U bays) as set out in the TIA in Appendix 2 of **Annex B** and highlighted in paragraph 1.11 above are for reference, and the provision figures represent the high end of the HKPSG requirements based on the estimated residential units in redevelopment projects already commenced by URA as well as projects under consideration (and yet to be confirmed) by URA within the Wider Area. The traffic impact of such maximum provision is supported by the TIA and C for T has no adverse comment on the TIA. Government will decide on the actual provision figures in the underground car park, that will be same or lower than the maximum provisions stated above, taking into account all projects commenced by URA within the Wider Area at the time of the land grant;

Management of Underground Car Park and Exemption from PR Calculations

- (e) the DS and the other URA developments and/or redevelopments to be served by the underground car park currently fall within “R(A)” zone. Under the Notes of the “R(A)” zone, floor space for use solely as ancillary car park and L/U bays for the use and benefit of all the owners/occupiers of the domestic building may be disregarded from PR calculation. This meant that should the ancillary car parks and L/U bays be provided in each of the redevelopment sites, they will be exempted from PR calculation under the “R(A)” zone. As such, exemption of the floor space for the underground car park to accommodate ancillary car park and L/U bays for developments within the Wider Area is generally in line with the intention of the “R(A)” zone in the OZP. However, as the ancillary car park and L/U bays on various sites are to be accommodated within one single site, clear specifications in the Notes under a separate “R(A)8” zone in the DSP is more appropriate;
- (f) regarding the four coach parking spaces in the underground car park, it is a ‘public vehicle park’ use which is normally accountable for PR calculations. Given that the coach parking spaces are required by C for T to cater for demand in the district and that they will be provided underground and will not affect the building bulk above ground, PlanD has no objection to exempting them from PR calculations under the DSP;
- (g) given that URA only indicated that the management of the underground car park will be arranged at the development stage and no management mechanism has been put forward at this stage, LandsD considers that URA will need to propose enforceable mechanisms for management of the underground car park to ensure that the car parking spaces and L/U bays will be used to serve the DS and the Wider Area under the land grant. Otherwise, alternative administrative/statutory means may be required to enforce the intention for the underground car park to serve the Wider Area. In the absence of details of the management mechanism for the car park, LandsD considers that the car parking spaces and L/U bays that are additional to the required provision in the DS site may be accountable for GFA calculations under the lease. This is a lands matter that has to be

resolved at a later stage and URA will need to devise appropriate and enforceable mechanisms to LandsD's satisfaction at the land grant stage;

Road Improvement Schemes

- (h) URA considered that the new road system shown in **Drawing 1** will improve traffic circulation and accessibility of the area. C for T has no adverse comment on the proposed road system and its traffic impacts as detailed in the TIA in Appendix 2 of **Annex B** from traffic planning perspective; and

Other Technical Matters

- (i) the submissions from URA have been circulated to relevant departments for comments and their comments are highlighted in paragraph 9 above. In gist, comments from government departments have been addressed by URA in **Annexes E-1 to E-3, E-5 and E-6** and they have no adverse comment on the DS from environmental, traffic, drainage and sewerage impact perspectives. Where appropriate, comments from relevant government department have been incorporated in the revised Notes and ES in **Annexes G-2 and G-3**. The comments from CBS/K, BD and LandsD relating to matters pertaining to general building plan submissions and land grant matters may be dealt with at a later stage. The comments regarding requirements for additional social welfare facilities, that are uses always permitted under the DSP, should be subject to further discussion between SWD and URA at a later stage and if required, be stipulated under relevant lease conditions. For the implementation of junction improvement works at To Kwa Wan Road and Ngan Hon Street, it will be worked out between URA and TD at a later stage.

- 12.2 In respect of the public comments on the draft DSP and comments of HIC KCDC members mentioned in paragraphs 10.3 and 10.4 above, the above responses are applicable. Regarding the request to include other development sites into the DS, URA has explained its rationale for defining the site boundary of the DS. Other issues on acquisition, compensation and re-housing would be dealt with by URA according to the established policies.

13. Proposed Amendments to the Approved Hung Hom OZP No. S/K9/24

Proposed Amendments to Matters Shown on the OZP (Annex H-1)

- 13.1 If the Board decides to deem the draft DSP as being suitable for publication, in accordance with section 25(9) of the URAO, the draft DSP shall, from the date that the exhibition of the DSP is first notified in the Gazette, replace or amend according to their tenor the OZP relating to the site. In addition to the rezoning of a site at Lee Kung Street for Hong Kong Housing Society's Senior Citizen Housing project (**Item A** and some technical amendments) agreed by the Metro Planning Committee (MPC) at its meeting on 14.9.2016, the following amendments will also be made to the approved Hung Hom OZP No. S/K9/24.

Chun Tin Street/Sung Chi Street DSP (about 1,636 m²) (Plan 9)

- 13.2 To excise the area covered by the Chun Tin Street/Sung Chi Street DSP from the OZP (subject to the Board's consideration of that DSP under TPB Paper No. 10183 at the same meeting).

Hung Fook Street/Ngan Hon Street DSP (about 4,562 m²) (Plan 9)

- 13.3 To excise the area covered by the Hung Fook Street/Ngan Hon Street DSP from the OZP (subject DSP).

Proposed Amendments to the Notes of the OZP

- 13.4 There is no need to make amendments to the Notes of the OZP in relation to the two DSPs mentioned above.

Revision to the Explanatory Statement of the OZP

- 13.5 The revised ES of the OZP agreed by the MPC at its meeting on 14.9.2016 is further revised to take into account the two DSPs mentioned above as well as corresponding amendments to the planning scheme area of the OZP and area of land under the "R(A)" zoning in the OZP. An extract of the relevant pages of the revised ES (with proposed additions highlighted in bold and italics and deletions ~~crossed-out~~) is at **Annex H-2**.

Plan Number

- 13.6 Upon exhibition for public inspection, the OZP will be renumbered as S/K9/25.

14. Decision Sought

Draft DSP

- 14.1 If the Board agrees with the DSP approach for the Hung Fook Street/Ngan Hon Street site, the Board is invited to consider the draft DSP and:
- (a) deem the draft URA Hung Fook Street/Ngan Hon Street DSP No. S/K9/URA2/A (to be renumbered No. S/K9/URA2/1 upon exhibition for public inspection) and the Notes at **Annexes G-1** and **G-2** as being suitable for publication as provided for under section 25(6) of the URAO, so that the draft DSP shall be exhibited for public inspection under section 5 of the TPO;
 - (b) endorse the ES of the draft DSP at **Annex G-3** and adopt it as an expression of the Board's planning intention and objectives of the Plan, and agree that the ES as being suitable for public inspection together with the draft DSP;
 - (c) agree that the draft DSP, its Notes and ES are suitable for submission to the KCDC for consultation/information upon exhibition of the DSP; and

- (d) note the Social Impact Assessment (Stages 1 and 2) reports of the DSP.
- 14.2 Alternatively, in relation to para. 14.1 above, the Board may decide to deem the draft DSP, submitted with amendment as being suitable for publication, or may refuse to deem the DSP, as being suitable for publication.
- 14.3 If the Board does not agree with the DSP as stated in para 14.1 above, the Notes and ES of the draft DSP will be revised by URA for the Board's consideration.

Proposed Amendments to OZP

- 14.4 If the Board decides to deem the draft DSP under paragraph 14.1 above, as being suitable for publication, Members are invited to consider the related amendments to the OZP and to:
- (a) agree to the proposed amendments to the approved Hung Hom OZP and that the draft Hung Hom OZP No. S/K9/24B at **Annex H-1** (to be renumbered as S/K9/25 upon exhibition) and its Notes agreed by MPC at its meeting dated 14.9.2016 are suitable for exhibition under section 5 of the TPO;
 - (b) adopt the revised ES at **Annex H-2** for the draft Hung Hom OZP No. S/K9/24B as an expression of the Board's planning intention and objectives for the various land use zones of the OZP, and agree that the revised ES as being suitable for public inspection together with the draft OZP.

15. Attachments

Annex A	Letter dated 10.6.2016
Annex B	Planning Report
Annex C	Letter dated 20.7.2016
Annex D	SIA Stage 2 Report
Annex E-1	Letter dated 5.8.2016 providing responses to comments of Government departments (Part 1)
Annex E-2	Letter dated 23.8.2016 providing responses to comments of Government departments (Part 2) with revised EA and revised Drainage, Sewerage and Water Impact Assessment, revised Drawing 3.4 and 3.5 of TIA
Annex E-3	Letter dated 7.9.2016 providing response to comments of Government departments and responses to public comments
Annex E-4	E-mail dated 12.9.2016 on the revised DSP, Notes and ES
Annex E-5	Letter dated 14.9.2016 providing response to comments of Government departments
Annex E-6	E-mail dated 29.9.2016 providing response to comments of Government departments
Annex F	Extracts of Minutes of HIC KCDC Meeting held on 23.6.2016
Annex G-1	Draft URA Hung Fook Street/Ngan Hon Street DSP No. S/K9/URA2/A

Annex G-2	Notes of the Draft URA Hung Fook Street/Ngan Hon Street DSP No. S/K9/URA2/A
Annex G-3	ES of the Draft URA Hung Fook Street/Ngan Hon Street DSP No. S/K9/URA2/A
Annex H-1	Draft Hung Hom OZP No. S/K9/24B
Annex H-2	Extract of ES of the Draft Hung Hom OZP No. S/K9/24B
Drawing 1	URA's Proposed Traffic Network
Drawing 2	Wider Area Served by Underground Car Park
Drawing 3	Notional Layout
Drawing 4	Notional Section
Plan 1	Urban Renewal Plan for Kowloon City
Plan 2	Location Plan - Projects Commenced by URA in the Vicinity of the DS
Plan 3	Site Plan
Plan 4	Building Age and Building Height Plan
Plan 5 to 8	Site Photos
Plan 9	Proposed Amendments to OZP to excise the areas covered by the DSPs

PLANNING DEPARTMENT
OCTOBER 2016

Our File Ref: PDP/KC-010

10 June 2016



By Hand

Secretary,
Town Planning Board,
15/F, North Point Government Offices,
333 Java Road,
North Point,
Hong Kong

Dear Sir/Madam,

RECEIVED
2016 JUN 10 P 12:31
TOWN PLANNING BOARD

**Submission of the Draft Development Scheme Plan
for the Urban Renewal Authority
Hung Fook Street / Ngan Hon Street Development Scheme (KC-010)**

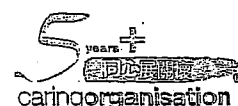
I am pleased to submit 90 copies of the draft Development Scheme Plan (DSP) for the Urban Renewal Authority Hung Fook Street / Ngan Hon Street Development Scheme (KC-010) (the Scheme) for Town Planning Board's consideration in accordance with section 25(5) of the URA Ordinance (URAO).

The Scheme was included in the Urban Renewal Authority (URA)'s 15th Business Plan (2016/17) approved by the Financial Secretary for implementation in the year of 2016/17. On 3 June 2016, the URA has published the commencement date of the implementation of the Scheme in the Government Gazette under section 23 of the URAO and subsequently in local newspapers.

The submission booklet for the draft DSP of KC-010 includes the Planning Report as Part 1 to summarize the planning proposals; the draft DSP, its Notes and Explanatory Statement as Part 2; the supplementary information to support the DSP submission as Part 3, which includes Preliminary Design, Traffic Impact Assessment (TIA), Environment Assessment (EA), Drainage and Sewerage Impact Assessment (DSIA), Stage 1 Social Impact Assessment (SIA) Reports and the implementation programme and URA's compensation policies.

In accordance with the Urban Renewal Strategy, the Stage 2 SIA Report will be included as part of the DSP submission. Based on the Gazette, we will submit the Stage 2 SIA report to the TPB not later than 20 July 2016. We believe that there will still be sufficient time for TPB's consideration for the foresaid report together with the submitted DSP.

...../P.2





Our File Ref: PDP/KC-010.

Date: 10 June 2016

To facilitate the inspection of the draft DSP by the general public, we are pleased to submit also 10 copies of the Chinese version of the Notes and the Explanatory Statement of the draft DSP, and the Stage 1 SIA Report for your use and consideration.

Should you have any enquiry on the draft DSP submission, please feel free to contact Mike Kwan at 2588 2630. Thank you very much.

Yours sincerely,

Michael Ma
Director, Planning and Design

Encl.

c.c. (w/o - by fax)

S for Dev	(Attn: Mr. Raymond Sy)	(Fax No.: 2905 1002)
AD/M, PlanD	(Attn: Mr. Michael Chan)	(Fax No.: 2576 3266)
DPO/Kln, PlanD	(Attn: Mr. Tom Ip)	(Fax No.: 2894 9502)



Our Ref: PDP/KC-010

20 July 2016

By Hand

Secretary to the Town Planning Board
15/F North Point Government Offices
333 Java Road, North Point,
Hong Kong.



Dear Sir/ Madam,

**Submission of Stage 2 Social Impact Assessment Report
for the Urban Renewal Authority
Hung Fook Street / Ngan Hon Street Development Scheme (KC-010)**

We refer to the captioned Development Scheme which was commenced on 3 June 2016 and the draft Development Scheme Plan submission (DSP) was made to Town Planning Board (TPB) on 10 June 2016.

In accordance with the Gazette Notice published on 3 June 2016, the URA will submit a Stage 2 Social Impact Assessment (SIA) Report to the TPB as part of the submission.

We enclose a total of 90 English copies and 10 Chinese copies of the Stage 2 SIA report for your use and consideration. As stated in the Gazette Notice, the Stage 2 SIA Report shall be made for public inspection at the two Planning Enquiry Counters from 25 July 2016.

Should you have any enquiry on the submission, please feel free to contact our Mr. Matthew Law at 2588 2176. Thank you very much.

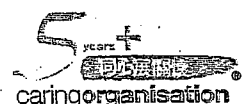
Yours sincerely,

Mike Kwan
Assistant General Manager,
Planning and Design Division

Encl.

c.c.: (w/o encl. – by fax)

S for Dev	(Attn: Mr. Raymond Sy)	(Fax No.: 2905 1002)
AD/M, PlanD	(Attn: Mr. Michael Chan)	(Fax No.: 2576 3266)
DPO/Kln, PlanD	(Attn: Mr. Tom Ip)	(Fax No.: 2894 9502)





Our Ref: PDP/KC-010

5 August 2016

By Fax (2894 9502) & By Post

District Planning Officer/ Kowloon
14/F North Point Government Offices
333 Java Road, North Point,
Hong Kong.

(Attn.: Ms. Johanna Cheng)

Dear Ms. Cheng,

**Urban Renewal Authority
Hung Fook Street /Ngan Hon Street Development Scheme (KC-010)
Responses to Government Departmental Comments (Batch 1)**

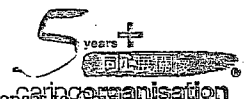
I refer to our submission of the captioned draft Development Scheme Plan (the Scheme) dated 10 June 2016, the Departmental comments received via your emails dated 7, 14, and 22 July 2016 respectively. We would like to enclose our responses to comments (R to C) to the Government bureau and departments (i.e. BD, UDU/PlanD, DEVB, LandsD (UR), LandsD (Kowloon West), HAD, FSD, LCSD, and HyD) for your necessary action (**see Attached 1**).

Thank you for your kind attention.

Yours sincerely,

Mike Kwan
Assistant General Manager
Planning & Design

encl.
RT/MK/ML
R



URA Hung Fook Street/ Ngan Hon Street Development Scheme (KC-010)
Responses to Comments from Government Departments on Draft DSP submission

Government Department	Comments	Response
Buildings Department (Contact officer: Mr George Cheung 2626 1529)	<p>1. I have reservation on the proposed development scheme under the Buildings Ordinance (BO) as it may contravene the following provisions under the BO:</p> <p>(a) The scheme involves extinguishment of an existing scavenging lane between the buildings at 1-51 Hung Fook Street and the buildings at 2-42 Ngan Hon Street with erection of buildings over, under, upon and/or on the lane, which would contravene section 31 of the BO.</p> <p>(b) It is unclear that whether the site area for assessment of plot ratio and site coverage under the Building (Planning) Regulation (B(P)R) 20 and the B(P)R 21 for the proposed development scheme has included the areas of the existing scavenging lane and the proposed through road connecting the existing streets of Hung Fook Street and Ngan Hon Street or not. If the areas are included in the site area, it will contravene the B(P)R 23(2).</p> <p>2. Exemptions and/or modifications with or without condition(s) under the BO may be considered for the proposed extinguishment of the existing</p>	<p>1(a) The existing scavenging lanes between the buildings at 1-51 Hung Fook Street and the buildings at 2-42 Ngan Hon Street would be resumed under the proposed Development Scheme and would be included as part of proposed future site area, thus, erection of buildings over, under, upon and/or on that area would not contravene section 31 of the BO.</p> <p>(b) For the scavenging lanes, please refer to point 1(a) above. For the proposed through road, the mechanisms for the provision of a "road" is to be confirmed and its implications under B(P)R shall be further discussed. The proposed through road is supported by TD and is within the proposed future site boundary. As the current zoning under the DSP restricts PR to 1.5 + 7.5. Therefore, even if the proposed "road" is to be accountable for PR under the OZP, the development bulk is still within item permitted under the B(P)R Schedule 1.</p> <p>Noted.</p>

Government Department	Comments	Response
	<p>scavenging lane and inclusion of the existing lane and the proposed road in site area for the purposes of plot ratio and site coverage calculations under the B(P)R, but they will only be considered on its individual merits based on relevant information or justification submitted to the Building Authority. In this connection, the applicant is advised to appoint an Authorized Person to submit a pre-submission enquiry on the issues arisen under the PNAP ADM-19 at an early stage to seek determination from the Building Authority in these respects.</p> <p>3. In addition, I have the following comments on the proposed development scheme under the BO:</p> <p>(a) In accordance with the Government's committed policy to implement building design to foster a quality sustainable built environment, the sustainable building design requirements (including building separation, building setback and site coverage of greenery) should be included, where possible, in the conditions in the planning approvals.</p> <p>(b) Private carparking spaces intended for the use of the occupants and their bona fide visitors of the parent building may be disregard from GFA calculation under the B(P)R 23(3)(b) subject to the compliance of the criteria set out in PNAP APP-2. Therefore, the carparking spaces intended for</p>	<p>3(a) Noted. and the SBD requirements will be incorporated in the development scheme.</p> <p>(b) – (d) Noted.</p> <p>(e) – (f) Noted. and they shall be provided.</p> <p>(g) Noted.</p>

Government Department	Comments	Response
	<p>the use by the occupants of adjoining buildings should be included in GFA calculation under the B(P)R 23(3)(a).</p> <p>(c) Loading/unloading bays with provision making reference to the standards set out in the HKPSG or requirements stipulated by the C for T for the building may be disregard from GFA calculation under the B(P)R 23(3)(b). Therefore, any loading/unloading bays intended for the use by adjoining building should be included in GFA calculation under the B(P)R 23(3)(a).</p> <p>(d) Coach parking spaces should be included in GFA calculation under the B(P)R 23(3)(a).</p> <p>(e) Adequate prescribed windows in accordance with the B(P)R 30 and the B(P)R 31 should be provided for every domestic flat.</p> <p>(f) Refuge floor/roof should be provided for all buildings exceeding 25 storeys in height above the lowest ground storey in accordance with Subsection B18 of the Code of Practice for Fire Safety in Buildings 2011.</p> <p>Detailed comments under the BO can only be formulated at the building plan submission stage.</p>	

Government Department	Comments	Response
Urban Design Unit, Planning Department (Contact officer: Miss Carmen Chan 2231 4840)	<p><u>Urban Design Perspective</u></p> <p>2. The proposed scheme is in keeping with the maximum buildings height of 100mPD and maximum domestic PR of 7.5 and non-domestic PR of 1.5 as stipulated for the subject "Residential (Group A)" zone on the approved Hung Hom OZP No. S/K9/24. We have no particular comment on the proposed development from urban design point of view.</p> <p><u>Air Ventilation Perspective</u></p> <p>3. Detailed comments on air ventilation assessment are at Attachment I as addressed below:</p> <p>(Attachment I)</p> <p><u>Planning Statement – Air Ventilation Aspect</u></p> <p>1. We refer to paragraphs 4.23 to 4.28 in the draft Planning Statement (PS) regarding the implications of the captioned project on air ventilation. While these paragraphs have provided some rather detailed information/discussion, they are incomprehensive containing incorrect information and unconvincing arguments. Yet, rather than drilling into unnecessary details, we suggest that the PS should only need to provide the following basic information/observation and discussion considering the nature and scale of development.</p>	<p>Noted.</p>

Government Department	Comments	Response
	<p>Wind Environment</p> <ul style="list-style-type: none"> Based on the RAMS wind data at grid x:083 and y:041 from Planning Department's website, the annual prevailing winds are from NE, E and ESE, while the summer prevailing winds are from the E, ESE, SSW, SW and WSW. <p>Air Ventilation Performance of Proposed Development</p> <ul style="list-style-type: none"> Referring to the Expert Evaluation Report from Hung Hom (2008), the project site is not located within any identified breezeways. In accordance with the joint HPLB and ETWB Technical Circular No. 1/06 on AVA, the proposed development does not fall within the categories in which an AVA is required. Considering the above, it is not anticipated that the proposed development would create any significant adverse air ventilation impact on the existing pedestrian wind environment. <p>2. However, should the applicant maintain with the current approach, it is required that the followings be addressed in the PS in providing comprehensive discussion.</p>	<p>Noted</p> <p>Noted</p> <p>Noted</p> <p>Noted and data analysis has the same conclusion as stated in the report.</p> <p>Based on the revised assessment criteria and methodology as suggested by Urban Design Unit, it is no longer necessary to further detail review on the annual and summer prevail winds, sea breezes, street alignment, Kowloon Bay and CCC Kei To Secondary School's effect on air quality and</p>

Government Department	Comments	Response
	<p><u>Paragraph 4.23</u></p> <ul style="list-style-type: none"> ▪ Annual Prevailing Winds – The annual prevailing winds should come from NE, E and ESE, not SW, in accordance with RAMS wind data. ▪ Sea Breezes – It should be clarified that the project site could enjoy some of the sea breezes due to its location. Sea breezes should not be related to the annual prevailing winds. The consultant should revise the relevant argument to avoid confusion. ▪ Street Alignment – The existing streets which are aligned in E-W and SE-NW (not N-S) could facilitate penetration of sea breezes. <p><u>Paragraph 4.24</u></p> <ul style="list-style-type: none"> ▪ Summer Prevailing Winds – The consultant should confirm and directly report that the summer prevailing winds are mainly from ESE, E, SSW, SW and WSW. The first three sentences should be revised appropriately to avoid confusion. ▪ Kowloon Bay – It should be Noted. that Kowloon Bay, not Victoria Harbour, is located to the east and southeast of the project site. ▪ Existing Wind Environment – Although 	<p>existing wind environment.</p>

Government Department	Comments	Response
	<p>Chatham Road North and Kowloon City Road are the major air paths under summer condition, these roads are relatively far away from the project site. In order to evaluate the potential wind impact in the vicinity of the project site due to the proposed development, the consultant should discuss the existing wind environment and identified the existing wind corridors and breezeways around the project site.</p> <p><u>Paragraphs 4.25 to 4.27</u></p> <ul style="list-style-type: none"> ▪ The consultant should specify which wind direction is being discussed in these paragraphs. ▪ The consultant should discuss whether the proposed development would create significant air ventilation impact on sensitive receivers (e.g. open spaces, parks and pedestrian areas) other than CCC Kei To Secondary School under both annual and summer prevailing winds. <p><u>Paragraphs 4.27</u></p> <ul style="list-style-type: none"> ▪ Air path to be blocked by CCC Kei To Secondary School – Although CCC Kei To Secondary School may directly block SW wind from entering Hung Fook Street, it is anticipated that SW wind should still be able to travel along other building separations to reach the project site. The consultant should revise the relevant arguments appropriately. 	

Government Department	Comments	Response
Development Bureau (Contact officer: Mr C K Tsang 3509 8874)	<p>I have no comment on the SIA report (Stage 1) from the urban renewal policy perspective noting the report generally covers the main elements of the social impact assessments mentioned in the Urban Renewal Strategy (2011). I trust URA will verify and update the relevant information and assessments (e.g. population and household characteristics, degree of sharing ratio, tenant split etc.) and consider appropriate mitigation measures in the SIA report (Stage 2).</p>	<p>Noted.</p>
Lands Department (Urban Renewal) (Contact officer: Mr Ming Tsang 2854 2167)	<p>1. According to the Report, the following road works are proposed by URA under the Scheme:</p> <p>(i) The existing government lane sandwiched by Nos. 2-42A Ngan Hon Street and Nos. 1-51 Hung Fook Street will be permanently closed and extinguished; and</p> <p>(ii) A new through road connecting Hung Fook Street and Ngan Hon Street within the Site which in turn forms part of the proposed road network extending from Wan On Street of URA Development Project KC-009 via the URA Development Project KC-011 and KC-012 is proposed.</p> <p>Subject to any other statutory procedural requirement may have, the proposed road works shall go through the following statutory/gazettal requirements before any proposed land grant can be further proceeded:</p>	<p>Noted</p> <p>Noted</p>

Government Department	Comments	Response
	<p>(i) The authorisation of the Scheme under the Urban Renewal Authority Ordinance (Cap. 563);</p> <p>(ii) The authorisation of the proposed road closure and road works under the Roads (Works, Use and Compensation) Ordinance (Cap. 370); and</p> <p>(iii) The approval by the CE in council for the resumption of those private property interests in the Site under the Lands Resumption Ordinance (Cap. 124) that URA cannot acquire by private agreement.</p> <p>2. According to the Report and Plans A1a and A1b of Appendix 1 of Part 3 of the Report, it is noted that a new through road connecting Hung Fook Street and Ngan Hon Street dividing the Site into two portions (i.e. western portion with proposed Tower A and eastern portion with proposed Towers B & C and with two levels of basement car park) will be provided. From land administration perspective, the future lot will be granted as a single lot. The new through public road should be formed and constructed by the Grantee under the Conditions of Grant and should be opened up for public use. Subject to the agreements of TD and HyD, the new road will be surrendered and delivered up to the Government on demand.</p>	<p>Noted.</p> <p>Noted.</p> <p>Noted.</p> <p>As required under the Development Scheme Plan, the car park will be located in the basement. The exact extent of the car park and the no. of levels will be determined in the detailed design stage. The land lot composition and the new road surrender arrangement shall be dealt with at the surrender and re-grant stage and subject to the agreement among relevant government departments.</p>

Government Department	Comments	Response
	<p>3. <u>Proposed Land Grant Boundary</u></p> <p>(i) Subject to the comments of TD and HyD, the existing public pavements serving Hung Fook Street, Ngan Hon Street and Wing Kwong Street should not be included in the future land grant boundary.</p> <p>(ii) Subject to the comments of TD and BD, the existing government lane at the rear of Nos. 17-31 To Kwa Wan Road should be maintained as the service lane serving the existing buildings of Nos. 17-31 To Kwa Wan Road and would not be included in the future land grant boundary. URA should also take note that there are air-conditioner projections onto the said government lane.</p> <p>(iii) URA should provide a proposed land grant boundary plan delineating the boundary of the proposed lot in arriving at the net site area of 4,562 sq m as stated in Table 4.1 of Part 1 of the Report.</p> <p>4. <u>Underground Communal Car Park</u></p> <p>It is noted in paras. 4.6-4.12 of Part 1 of the Report that an underground car park will be provided to cater for the car parking spaces and loading/unloading facilities requirements for the developments on the Site and also the future</p>	<p>The boundary of the land grant should basically follow the boundary of the R(A)8 zone. The exact boundary will be determined during the land grant stage.</p> <p>Noted.</p> <p>Refer to 3(i) above.</p>

Government Department	Comments	Response
	<p>redevelopments within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street ("the Wider Area") as demarcated on the Plan 1 for Explanatory Statement in Part 2 of the Report, as well as to include coach parking spaces to cater for the need of coach parking in the district. Our comments in this aspect are as follows:</p> <p>(i) The additional parking/loading/unloading provisions of the proposed "communal car park" in the Scheme (to accommodate the parking requirements generated by the developments within the Wider Area) is subject to the comments and agreements of TD and PlanD.</p> <p>(ii) There is no guarantee in the future land grant that:</p> <p>(a) The additional parking/loading/unloading provisions will be designated/reserved for the Wider Area in the Scheme nor solely serve the new developments within the Wider Area; and</p> <p>(b) these parking/loading/unloading provisions may not be enforceable under the subject land grant of KC-010. URA has to clarify their</p>	<p>Noted.</p> <p>The intention of the communal car park is to provide car parking facilities for the URA projects within the area as specified in the DSP, e.g. KC-010, KC-011, KC-012, DL-8:KC, etc. and it should be exempted from GFA calculation. Based on this principle, the Applicant is willing to accept the related land grant restriction clause(s) to reflect this intention.</p> <p>Refer to point (a) above. The parking/loading/unloading provisions shall be subject to future land administration stage and the</p>

Government Department	Comments	Response
	<p>future management and whether they would be alienated in multiple ownership.</p> <p>(iii) The additional parking/loading/unloading provisions, which exceed the normal provisions under the HKPSG, may be accountable for GFA under lease.</p> <p>(iv) The coach parking spaces may be accountable for GFA under lease.</p> <p>5. As there will be limited loading/unloading facilities serving the individual sites of KC-011, KC-012 and DL-8:KC, TD should consider whether sufficient on-site loading facilities should be provided to avoid bulky transportation of goods between KC-010 and the said individual sites.</p> <p>6. <u>Closure of Government Land and Provision of New Through Road</u></p> <p>Regarding the proposed road closure and road works under Cap. 370, URA as the project proponent, has to reimburse Government all the administrative and gazettal costs and compensation arising from the proposed road project and gazettal. A written agreement will be executed if the gazettal application is to be proceeded. In the event that the proposed road</p>	<p>decision of District Land Conference.</p> <p>4.(iii)&(iv). The intention for exemption of the GFA for the additional parking/ loading/ unloading provision exceeding the normal provision under HKPSG was agreed by PlanD and has explicitly stated in the Notes of the DSP.</p> <p>Due to the individual site constraints, TD agreed that the remaining required number of loading/unloading facilities serving the individual sites KC-011, KC-012 and DL-8:KC could be provided at the underground car park of the Development Scheme.</p> <p>Noted.</p>

Government Department	Comments	Response
	<p>works are not authorised under Cap. 370, URA has to consider the fall-back scenario on the future traffic arrangement and whether the land resumption application will be proceeded.</p> <p>7. It is noted in Figure 3.2 of Appendix 2 of Part 3 of the Report that the width of the three streets enclosing the Scheme (i.e. Ngan Hon Street, Wing Kwong Street and Hung Fook Street) are all less than 15m. Subject to the comment of BD, the future development on the Site may be required to set back from these streets so as to fulfill the QBB requirements under the prevailing policy.</p> <p>8. Subject to the aforesaid comments, this Office will reconsider the whole re-development proposal during the implementation and land grant application stage. There is no guarantee that any proposed resumption of the private lot interests within the Site, the implementation of the proposed road works, the future land grant and the proposed development parameters, will be approved. The land grant, if approved, will be subject to the decision of the relevant authority and the lease conditions to be imposed by Lands Department.</p>	<p>Noted. Under URA prevailing policy, development projects have to fulfil BD's SBD guidelines to achieve a good and healthy living and cityscape environment.</p> <p>Noted.</p>

Government Department	Comments	Response
Lands Department (Kowloon West) (Contact officer: Ms Sandy Sin 2300 1739)	<p>2. According to the prevailing administrative arrangement within LandsD, future land grant for the Scheme will be handled by URS. Whilst I would offer my comments from district point of view, I trust that URS will advise you on the land status information and any related land administrative implications arising from the Scheme, including but not limited to road gazette and resumption etc.. Our comments on the Scheme are as follows:</p> <p>3. From district point of view, I would raise the following issues for URA and CES/UR to note:</p> <p>(i) <u>Underground Carpark</u></p> <p>URA proposes to provide one underground carpark comprising car parking spaces, loading and unloading facilities and coach parking facilities to serve the subject development and also the adjoining developments within the "Wilder Area". However, the Planning Report is silent on by what "mechanism" these facilities can serve the Wilder Area. It is also silent on whether the parking facilities would be sold to the future owners of subject development and the developments within the Wilder Area. URA shall clarify the following:-</p> <p>(a) Whether assignment of the parking spaces would be allowed and, if</p>	<p>(a) and (b) The intention of the communal car park is to provide car parking facilities for the URA projects within the area as specified in the DSP, e.g. KC-010, KC-011, KC-012, DL-8:KC, etc. and it should be exempted from GFA calculation. Based on this principle, the Applicant is willing to accept the related land grant restriction clause(s) to reflect this intention.</p>

Government Department	Comments	Response
	<p>affirmative, to whom and how?</p> <p>(b) If the parking facilities are not to be assigned, should they be simply regarded as “a public carpark” which will be held and operated by one single owner just like in the situation for public vehicle park?</p> <p>(c) For the loading and unloading facilities which are supposed not to be sold to individual owners, again, who will be their owner? Would CES/UR bear in mind the possible future criticism and disputes raised by the flat owners if the liability in managing and maintaining the facilities are to be passed on them.</p> <p>I believe that CES/UR will take into account the possible enforcement problems arising from the proposed “underground carpark” when processing the required land grant.</p>	<p>It is a land administrative issue and please refer to the response to LandsD/UR for the similar comments.</p>

Home Affairs Department (Contact officer: Ms. Mabel Fung 3586 2807)	<ol style="list-style-type: none"> 1. It is envisaged that the property owner/residents within the Scheme Area would welcome and support the captioned Development Scheme Plan. It would be within expectation for them to strive for a better compensation package for their properties, which may include the market value of the property plus extra allowance. 2. As regards the owners of properties immediately adjacent to the Scheme Area, it is learnt that many of them were discontented with the Plan and may lodge objection to TPB, requesting for their properties to be included in the Plan. 	<p>Noted. URA had attended the KC DC Housing and Infrastructure Committee on 23 June to present the project and the District Council generally supported the development scheme and expected the project could be commenced as soon as possible.</p> <p>As of 4 August 2016, the TPB has received and referred to the Applicant 6 objections, 3 supports and 2 general comments. URA will respond to those owners who were expressed their discontent with the Plan by response to public comments to be submitted to the TPB. The objection cases will be considered by TPB after the public consultation period.</p>
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URA noted the following departments have no comment.

- Fire Services Department
- Leisure and Cultural Services Department
- Highways Department



Our Ref: PDP/KC-010

23 August 2016

**By Fax (w/o encl. 2894 9502) &
By Hand**

District Planning Officer/ Kowloon
14/F North Point Government Offices
333 Java Road, North Point,
Hong Kong.

(Attn.: Ms. Johanna Cheng)

Dear Ms. Cheng,

**Urban Renewal Authority
Hung Fook Street /Ngan Hon Street Development Scheme (KC-010)
Responses to Government Departmental Comments (Batch 2)**

I refer to our submission of the captioned draft Development Scheme Plan (the Scheme) dated 10 June 2016, the Departmental comments received via your emails dated 7, 14, and 22 July 2016 respectively. We would like to enclose our responses to comments (R to C) to the Government's departments (i.e. EPD, WSD, DSD, TD and DPO/K PlanD) (see Attachment A) with one copy of revised Environmental Assessment report, and one copy of Drainage, Sewerage, and Water Impact Assessment report as well as two pages of revised figures for Transport Impact Assessment for your necessary action.

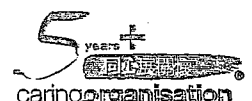
Further to the R to C (Batch 1) sent to your office dated 5 August 2016 and this batch (Batch 2) of R to C, URA has cleared all departmental comments received from your office in the above emails.

Thank you for your kind attention.

Yours sincerely,

Mike Kwan
Assistant General Manager
Planning & Design

encl. w/ reports & figures
RT/MK/ML



URA Hung Fook Street/ Ngan Hon Street Development Scheme (KC-010)
Responses to Comments from Government Departments on Draft DSP submission

Government Department	Comments	URA Responses
<p>Environment Protection Department (Contact officer: Mr Leo Luk 2835 1096)</p>	<p>2. Based on the information provided, we understand that URA propose to rezone the site to “R(A)8” and develop the site into three residential towers on top of two 3-storey podia of commercial/retail use and an underground car park.</p> <p>3. After reviewed the supporting Planning Report and the relevant supplementary documents including <i>Appendix 3 – Environmental Assessment Report (EA)</i> and <i>Appendix 4 – Sewerage Impact Assessment Report (SIA)</i>, we considered that the assessment in the EA is <u>incomplete to support the conclusions</u> in the EA report and Planning Report that “impacts on air quality, noise and waste management are not insurmountable”. For the SIA, the quality of the SIA is not satisfactory. Incorrect assumptions, incorrect reference figures as well as errors in the calculation are noted in the SIA. As such, we are <u>not able to agree on the conclusion</u> of the SIA that “no upgrading or improvement works of the existing sewerage system will be required”</p> <p>4. URA/ the consultant is required to critically reviewed and re-submit the EA and SIA for</p>	<p>Noted</p> <p>The EA and SIA are revised, please refer to the revised reports.</p> <p>Please refer to the revised reports.</p>

Government Department	Comments	URA Responses
	<p>our consideration. Our detailed technical comments on the EA and SIA are attached at <u>Annex 1</u> for your following-up.</p>	
	<p>Annex 1</p> <p>Planning Report</p> <p>1. <u>S.4.30</u></p> <p>The section is recommended to amend as follow to improve clarity:</p> <p><i>"...The study concluded that the impacts on air quality, noise, waste management was are not insurmountable. The URA will ensure that <u>satisfactory</u> the established environmental standards are being met at the detailed design stage and during implementation"</i></p> <p>However, we are not able to agree on the above conclusion in the Planning Report due to the followings:</p> <ul style="list-style-type: none"> a) The environmental assessment is incomplete as there is no traffic noise assessment nor traffic data in this report. b) The buffer distance requirement for vehicle emission stipulated under the HKPSG cannot be met. 	<p>Noted. Quantitative road traffic noise impact assessment and air quality impact assessment with the supporting traffic data on the proposed redevelopment project and the waste management have been included in the latest version of the Environmental Assessment (EA) report</p>

Government Department	Comments	URA Responses
	<p>As such, without further detailed assessment and revision of the Environmental Assessment Report, we do not agree on the conclusion that the impacts on air quality, noise, waste management are not insurmountable.</p>	
	<p>Comment on the Appendix 3 of the Environmental Assessment Report (EA)</p> <p><i>S.2 Noise Impact Assessment</i></p> <p>(A) General Comments</p> <ol style="list-style-type: none"> 1. This EA is incomplete as there is no traffic noise assessment or traffic data in this report. We have not been able to offer any comment on the traffic noise impact on this development. For traffic noise impact assessment purpose, the Consultant should obtain the TD's endorsement on the posted speed limits, and the HyD's endorsement on the types of road surfaces. 2. The Consultant should review the Area Sensitivity Ratings adopted for the Noise Sensitive Receivers (NSRs) not facing the Influencing factors. 3. The consultant should provide information on whether there will be fixed noise sources 	<p>Quantitative road traffic noise impact assessment with traffic data provided by traffic consultant have been included in the revised EA report. Please refer to S.2.3 in the EA report for the traffic noise impact assessment. The traffic forecast provided by traffic consultant applied for the road traffic noise impact assessment has been provided in Appendix 2.1. Endorsement are being sought from TD and HyD which will be provided when they are available.</p> <p>The Area Sensitivity Ratings (ASRs) adopted for the NSRs not facing the Influencing factors have been reviewed and updated in Section 2.4.4.</p> <p>All chiller and cooling tower within this development for commercial / retail podium are</p>

Government Department	Comments	URA Responses
	<p>within this development, e.g. from the commercial / retail podium. If affirmative, the consultant should adopt ANL-5dB or the prevailing background noise levels, whichever is the lower, for assessing noise from these fixed noise sources.</p> <p>(B) Specific Comments</p> <p>1. <u>S.2.2</u></p> <ul style="list-style-type: none"> - Please refer to comment (A)(1) above. <p>2. <u>Table 1</u></p> <ul style="list-style-type: none"> - Please refer to comment (A)(3) above. - The Consultant should have made clear that the determined Area Sensitivity Ratings is for assessment purpose only and should not prejudice the Authority's discretion on the enforcement based on the contemporary conditions 	<p>proposed in an enclosed indoor environment that the noise generated from these equipment would not affect the proposed redevelopment and the surrounding NSRs. Moreover, the inlet/outlet of the system would be equipped with acoustic louvre to minimize any potential fixed noise impact from the central air-conditioning system. According to the observation during the site survey, there are different kinds of noise sources identified (such as road traffic noise from the surrounding roads with heavy traffic flows, fixed noise sources, etc.) and the proposed redevelopment project is in an urban area. Therefore, it is anticipated that the prevailing background noise levels should be higher than ANLs. Therefore, ANLs have been adopted as the noise criteria.</p> <p>Please see the response for general comment (A)(1).</p> <p>Please see the response for general comment (A)(3). Table 1 has been renamed to Table 2.1.</p> <p>Noted.</p>

Government Department	Comments	URA Responses
	<p>3. <u>S.2.3.1.1</u></p> <ul style="list-style-type: none"> - The Consultant should have reviewed if there is any fixed noise sources on the rooftop of the nearby premises (e.g. the rooftop of the 2 nearby industrial buildings). - In addition to site visits, the consultant should conduct interviews with the operators of the fixed noise sources concerned to ascertain <u>whether there would be night time operation.</u> - 9th line: Should "...Appendix A2." Be amended as "...Appendix A1."? - Antepenultimate sentence: Should "other" be deleted? <p>4. <u>Table 3</u></p> <ul style="list-style-type: none"> - There should be a noise descriptor for the predicted noise levels. <p>5. <u>S.2.3.4.2</u></p> <ul style="list-style-type: none"> - Should "...are 66 dB(A) for..." be amended as "...are 65 dB(A) for..."? 	<p>S.2.3 has been renumbered to S.2.4. No chiller, cooling tower or potential fixed noise source was being observed during the site survey carried out on 25th May, 2016. Please refer to the S2.4.2.2 for details..</p> <p>There is no night-time operation from the identified fixed noise source locations. Please refer to the Section 2.4.2.3 for details.</p> <p>Text has been revised in Section 2.4.2.</p> <p>Text has been revised in Section 2.4.2.</p> <p>Table 3 has been renumbered to Table 2.2. Table 2.2 has been revised accordingly</p> <p>The result and text have been revised in S.2.4.6.</p>

Government Department	Comments	URA Responses
	<p>6. <u>Figure 1</u></p> <ul style="list-style-type: none"> - This figure is not clear enough for showing the environment. <p>7. <u>Appendix A2</u></p> <ul style="list-style-type: none"> - The corrected noise levels shall be presented in integers. - The fixed noise assessment contains errors. Some calculated noise levels are incorrect. - The Consultant should have justified why corrections for the characteristics of tonality, intermittency and impulsiveness are unnecessary. 	<p>Figure 1 has been revised.</p> <p>Appendix A2 has been renumbered to Appendix 2.6. Appendix 2.6 has been revised accordingly.</p> <p>Calculations for fixed noise impact assessment have been revised. Please refer to Appendix 2.6 for details.</p> <p>Please refer to S.2.4.5 in the revised report for the methodology and assumption for fixed noise impact assessment and calculations.</p>
	<p><u>S.3 Air Quality Assessment</u></p> <p>1. Please add a section to address the air quality impact arising from the construction phase of the project. In particular, please confirm that relevant mitigation measures under the Air Pollution Control (Construction Dust) Regulation shall be implemented to minimize dust emissions during construction phase.</p>	<p>Section 3.3 has been added to address the air quality impact and minimize dust emissions during construction phase.</p>

Government Department	Comments	URA Responses
	<p>2. <u>Section 3.1.1</u></p> <ul style="list-style-type: none"> - Please add a section to address the air impact arising from chimney emissions, if any. <p>3. <u>Section 3.3.1</u></p> <ul style="list-style-type: none"> - Please provide sufficient information, e.g. TD's annual traffic census, or other confirmation/advice/other documentation from TD, to confirm the road type of concerned roads including Ngan Hon Street, Wing Kwong Street, and Hung Fook Street. <p>4. <u>Section 3.3.3</u></p> <ul style="list-style-type: none"> - "...For all three roads, the buffer distance between the road and Towers B and C of the proposed redevelopment is slightly less than the HKPSG's recommendation. ..." indicates that the buffer distance requirement stipulated under HKPSG <u>CANNOT</u> be met. Unless the design of block layout of the site can be revisited with a view to fulfilling the above buffer distance requirement, i.e. no air sensitive uses within 5m buffer zone (subject to confirmation of road type(s) of concerned roads) with the provision of figure(s) showing the buffer distance between road kerb and ASR, we suggest 	<p>No active chimneys were identified in the assessment area and it has been stated in Section 3.4.2.</p> <p>Traffic forecast up to 15 years from the commencement year provided by traffic consultant has been appended in Appendix 3.1.</p> <p>Since the buffer distance requirement stipulated under HKPSG cannot be met, quantitative air quality impact assessment (AQIA) has been conducted to predict the air quality impacts. The AQIA are discussed in Section 3.4.</p>

Government Department	Comments	URA Responses
	that the Project Proponent / Consultant should carry out quantitative air quality impact assessment (AQIA) to address the air quality impacts associated with the proposed Scheme.	
	<p><u>S.4 Waste Management & Land Contamination Assessment</u></p> <p>1. <u>General</u></p> <ul style="list-style-type: none"> - Please address the potential waste management issue for the proposed development. <p>2. <u>S.4.3.5</u></p> <ul style="list-style-type: none"> - Please provide correspondence/ information from relevant authorities (e.g. FSD, EPD) on whether there are any Dangerous Goods stores, spillage/leakage incidents, registration of chemical waste producer, etc. If affirmative, please also address the potential land contamination issue accordingly. <p>3. <u>S.4.3.8</u></p> <ul style="list-style-type: none"> - Please provide site visit photos for the current land use activities and the location plan of the potential contaminative uses identified. 	<p>The potential waste management issue for the proposed development is included in Section 4.</p> <p>Section 4 for Land Contamination Review has been renumbered to Section 5.</p> <p>The correspondence/information from relevant authorities are included in Table 5.3.1, Table 5.3.2, and Appendix 5.3.</p> <p>Photo records and location of where the photos were taken are presented in Figure 5.</p>

Government Department	Comments	URA Responses
	<p>4. <u>S.4.3.9</u></p> <ul style="list-style-type: none"> - Please clarify in the EA that all related documentation including Contamination Assessment Plan, Contamination Assessment Report, Remediation Action Plan and Remediation Report (if any) should be submitted to EPD for agreement prior to commencement of site investigation, construction/development works according to prevailing guidelines. <p>5. <u>S.5.1.4</u></p> <ul style="list-style-type: none"> - Please clarify if the CAP would cover the whole site to be developed instead of just the car repairing workshops identified. 	<p>It is clarified that CAP, CAR, RAP and RR (if any) shall be submitted to EPD for agreement prior to commencement of site investigation, construction/development works according to prevailing guidelines.</p> <p>Since there is potential land contamination within the Subject Site, the CAP would cover the whole site to be developed</p>
	<p>Appendix 4 – Sewage Impact Assessment Report</p> <p>1. <u>Para. 3.2.1</u></p> <ul style="list-style-type: none"> - 150mm diameter sewers are not found in the western side of the Subject Site, please clarify. <p>2. <u>Figure 2 and 3</u></p> <ul style="list-style-type: none"> - The sewer diameters shown in the figures are wrong, please amend. 	<p>It is clarified that a 300mm diameter sewer runs along the western side of the Subject Site.</p> <p>The sewer diameters shown in Figures 2 and 3 have been amended.</p>

Government Department	Comments	URA Responses
	<p>3. <u>Para. 3.3.4</u></p> <ul style="list-style-type: none"> - The Subject Site belongs to Central Kowloon, not East Kowloon and thus the Catchment Inflow Factor is wrong, please amend. <p>4. <u>Table 1</u></p> <ul style="list-style-type: none"> - 1 employee per 29.4m² is on low side, normally 20m²-25m² per employee for retail trade is used based on Table 2 in Chapter 5 of Hong Kong Planning Standards and Guidelines; - The peaking factor used should be based on the contributing population instead of the actual population. The contributing population is defined as calculated total average flow (m³/day) divided by 0.27 (m³/day). Please refer to para 11.4 and 12.1 of GESF for more details. <p>5. <u>Para 3.5.1</u></p> <ul style="list-style-type: none"> - The diameter of the sewer between manholes FMH4024355 and FMH4024356 is 300mm, not 525mm, please amend. <p>6. The pipe length of Segment S7-S8 should be far less than 26m, please check and revise accordingly.</p>	<p>The Catchment Inflow Factor for Central Kowloon is adopted for the calculations.</p> <p>The factor of 25m² per employee for retail trade is adopted in Table 1. The hydraulic calculations of Appendix A have also been updated accordingly.</p> <p>The peaking factor based on the contributing population is updated accordingly.</p> <p>Para 3.5.1. has been amended.</p> <p>The pipe length of Segment S7-S8 is updated to be 8.4m.</p>

Government Department	Comments	URA Responses
	<p>7. <u>Appendix A, Table 2</u> Gradient of Segment S1-S2 is wrong, gradient of segment S2-S3 has been rounded off from 0.000692 to 0.001 which cause the full bore velocity calculated to be inaccurate, please specify gradient in 1/x to avoid any mistakes;</p> <ul style="list-style-type: none"> - Invert level 1 and invert level 2 of segment S4-S5 are same, please advise how the gradient of this segment derives. <p>8. <u>Appendix A</u></p> <ul style="list-style-type: none"> - The number of total residential units are assumed, please provide the basis of the assumption; - Page 3 to 7 of Appendix A need to be amended in view of comment no. 3, 4, 6, 7 & 8, please amend. 	<p>The gradients in 1/x of all sewer segments is now included in Table 2 of Appendix A. For Segment S4-S5, as the invert levels are the same, the invert levels of S5 (FMH4024359) have been adjusted to 3.10mPD, so as to align with S4 (FMH4024358) and S6 (FMH4025890). The estimated capacities of the pipes and hydraulic calculations have also been updated accordingly.</p> <p>The basis of the computation of the nos. of household is based on the proposed development parameters in Table 4.1 in the planning report. Therefore the proposed design will apply the max. domestic GFA 34,215m² with average flat area approx. 45m² such that to make up the 750 flats no. in the scheme.</p> <p>Appendix A has been updated accordingly.</p>
<p>Water Supplies Department (Contact officer: Mr Philip Au 2152 5739)</p>	<p>2. It is envisaged that increase in service trade from the non-domestic GFA and increase in no. of flats from the domestic GFA due to the development will increase in the fresh and salt water demand in the areas. A detailed Water Impact Assessment (WIA) will need to be</p>	<p>In view of the comments, a Water Impact Assessment has been included to address the water demand issue. It is noted that the proposed development will result in increases in both the fresh and salt water demands. However, as concluded from the report,</p>

Government Department	Comments	URA Responses
	<p>undertaken by the project proponent to ensure that the existing water supply system including service reservoir, pumping station and water mains can meet the increase in fresh/salt water demand. In particular, the existing capacity of Ho Man Tin High Level Fresh Water, Lok Fu Salt Water Service Reservoir and Tai Wan Salt Water Pumping Station should be assessed to ensure that they can cope with the increase in fresh and salt water demand due to the proposed development. Otherwise, upgrading works for those waterworks facilities should be proposed.</p> <p>3. It is envisaged that existing fresh/salt water mains will be affected by the proposed redevelopment. The project proponent shall bear all cost associated with the necessary diversion, upgrading, connection, protection, extension and capping off of the existing water mains.</p>	<p>the increases can be accommodated by the existing main supply facilities.</p> <p>Noted</p>
<p>Drainage Services Department (Contact officer: Mr C L Wong 2300 1573)</p>	<p>I have the following comments on Appendix 4:</p> <p>a. Para 2.2 – Although the total surface runoff from the whole development site will not be increased, drainage impact depends on how the runoff from the site is to be distributed to the nearby discharge points. Please note that some sections of stormwater drains are only 150mm</p>	<p>In view of the comments, a Drainage Impact Assessment has been included to address the potential runoff from the site.</p> <p>A new stormwater drain is proposed to convey runoff from the Subject Site to the existing main</p>

Government Department	Comments	URA Responses
	<p>diameter. As such, the assessment fails to conclude that there is no adverse drainage impact.</p> <p>b. Table 2 of the Sewerage Impact Assessment – Any missing / doubtful data has to be verified on site and not to be judged desktop. The method used to calculate the hydraulic capacity assumes that there is a free flow condition, and this condition has to be verified on site as backwater effect from further downstream may happen.</p>	<p>drainage system in Ngan Hon Street.</p> <p>Noted. The concern on backwater effect can be catered for by conducting actual on-site investigation and flow test which can investigate the extent of effect towards the sewerage capacities. Given the proposed development is at notional design stage and the purpose of the SIA at this preliminary stage is to assess the potential sewerage impacts based on the available sewerage data and assumptions, on-site investigation will only be carried out by the future appointed developer when the proposed development is to be implemented in detailed design stage.</p>
<p>Transport Department (Contact officer: Ms. Joyce Lee 2399 2504)</p>	<p>a. There are a total of 21 metered parking spaces affected by the proposed developments. Apart from the 7 metered parking spaces to be relocated to Kai Ming Street, the other affected parking spaces should be reprovisioned at suitable on-street locations in other vicinity areas, to be identified by the project proponent.</p>	<p>Additional 14 nos. of on-street parking spaces are proposed for reprovig the affected on-street metered parking spaces. The proposed locations are illustrated in revised Figure 3.5 rev.A as provided.</p>

Government Department	Comments	URA Responses
	<p>b. The proposed layby adjacent to the through road is in conflict with the pedestrian crossing. Please review.</p> <p>c. Please check whether there are sufficient turning areas near the junctions of To Kwa Wan Road/Ngan Hon Street and Ma Tau Wai Road/Wing Kwong Street for long vehicles including coaches, accessing to the proposed developments.</p>	<p>The length of the proposed layby for Hung Fook Street/Kai Ming Street development project (KC-011) adjacent to the through road is reduced to avoid conflicting with the pedestrian crossing, while the length of the proposed layby on Wing Kwong Street is extended to make up the length. Please refer to revised Figure 3.5 rev.A for the revised arrangement.</p> <p>Swept path analysis has been conducted at the junctions of To Kwa Wan Road/Ngan Hon Street and Ma Tau Wai Road/Wing Kwong Street and the results are illustrated in revised Figure 3.4 rev.B. Based on the existing road layout of To Kwa Wan Road/Ngan Hon Street, vehicles of length up to 8m can make the left turn from the nearside lane of To Kwa Wan Road southbound to Ngan Hon Street. Nevertheless, longer vehicle can still make the turn using the middle lane of To Kwa Wan Road practically. Upon the extension of Wan On Street upto Ngan Hon Street, long vehicles can then access the Ngan Hon Street/Wing Kwong Street area using the new route, via Bailey Street and Wan On Street.</p>

Government Department		Comments	URA Responses
	d.	Please advise the arrangement of refuse collection, in particular for Site DL-8 where the length of the proposed layby appears insufficient for parking of a refuse collection vehicle.	The length of the proposed layby on Wing Kwong Street is extended to accommodate refuse collection vehicles. Please refer to revised Figure 3.5 rev.A for the revised arrangement.
	e.	The future management of the through road should be clarified.	The new road management arrangement shall be dealt with at the surrender and re-grant stage and subject to the agreements among relevant government departments.
	f.	URA is required to engage LandsD for conducting the relevant statutory gazettal process for the new through road.	Noted.
	g.	Para. 4.10 of Planning Report should read as "The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park are agreed between URA and the Government. The number of car parking and loading/unloading...".	Noted as required.
	h.	Please consider widening the footpath at the junction of the new through road and Kai	Buildings adjacent to the junction of the proposed through road at Kai Ming Street are not URA's

Government Department	Comments	URA Responses
	Ming Street to TPDM standard.	redevelopment sites and due to the site constraint of the existing private buildings, footpath at the junction of the new through at Kai Ming Street can only be provided approximate 1.2m width.
Kowloon, Planning Department (Contact officer: Mr Barry Yan 2231 4978)	<ul style="list-style-type: none"> • With regard to DLO/KW's views on the carpark, you are advised to provide more detailed implementation arrangement for the subject communal carpark to help TPB members comprehend the proposed parking arrangement. • With regard to para (c) of TD's comments, you are advised to check turning areas for all streets within the Wider Area also. 	<p>The intention of the communal car park is to provide car parking facilities for the URA projects within the area as specified in the DSP, e.g. KC010, KC-011, KC-012, DL-8:KC, etc. The parking/ loading/ unloading provisions shall be subject to future land administration stage and decision of District Land Conference.</p> <p>Swept path analysis has been conducted in the turning areas of all streets within the area as specified in the DSP. Please refer to URA response to TD for details.</p>



Our Ref: PDP/KC-010

7 September 2016

By Fax (2894 9502) & By Post

District Planning Officer/ Kowloon
14/F North Point Government Offices
333 Java Road, North Point,
Hong Kong

(Attn.: Ms. Johanna Cheng)

Dear Ms. Cheng,

Urban Renewal Authority
Hung Fook Street /Ngan Hon Street Development Scheme (KC-010)
Responses to Government Departmental Comments (Batch 3) and
Response to Public Comments

I refer to our submission of the captioned draft Development Scheme Plan (the Scheme) dated 10 June 2016, the Departmental comments received via your emails dated 18 August 2016. I would like to enclose our responses to comments (R to C) to Social Welfare Department (see Attachment I) for your necessary action.

Besides, I would like to provide our response to public comments. Please find my responses to these comments at the Attachment II.

Thank you for your kind attention.

Yours sincerely,

Mike Kwan
Assistant General Manager
Planning & Design

Encl.

✓
RT/MK/ML



URA Hung Fook Street/ Ngan Hon Street Development Scheme (KC-010)
Responses to Comments from Government Departments on Draft DSP submission

Government Department	Comments	Response									
Social Welfare Department (Contact officer, Mr. Alex Yung at Tel No 2116 3583)	<p>1. You may remember that when being consulted on the URA development projects at Kowloon City area back in 2014, we have furnished a 'wish list' of welfare facilities to be incorporated in the subject sites. We have reviewed and updated the 'wish list' of welfare facilities as follows for your consideration and follow up, please.</p> <table border="1" data-bbox="481 662 1310 1452"> <tr> <td data-bbox="481 662 638 766">Kowloon City</td><td data-bbox="638 662 884 766">Chung Tin Street / Sung Chi Street</td><td data-bbox="884 662 1310 766" rowspan="4"> <ul style="list-style-type: none"> • 150-place Residential Care Home for the Elderly cum 20-place Day Care Unit (2,625 sq m) • 160-place Integrated Vocational Rehabilitation Services Centre (1,110 sq m) * • 50-place Care and Attention Home for Severely Disabled Persons (1,170 sq m) • 50-place Hostel for Severely Physically Handicapped Persons (1,043 sq m) * • 50-place Hostel for Severely Mentally Handicapped Persons (967 sq m) ** • Integrated Family Service Centre (588.5 sq m) • 50-place Hostel for </td></tr> <tr> <td data-bbox="481 766 638 837"></td><td data-bbox="638 766 884 837">Bailey Street / Wan On Street</td></tr> <tr> <td data-bbox="481 837 638 941"></td><td data-bbox="638 837 884 941">Ma Tau Wai Road / Sheung Heung Road</td></tr> <tr> <td data-bbox="481 941 638 1452"></td><td data-bbox="638 941 884 1452">Hung Fook Street/ Ngan Hon Street</td></tr> </table>	Kowloon City	Chung Tin Street / Sung Chi Street	<ul style="list-style-type: none"> • 150-place Residential Care Home for the Elderly cum 20-place Day Care Unit (2,625 sq m) • 160-place Integrated Vocational Rehabilitation Services Centre (1,110 sq m) * • 50-place Care and Attention Home for Severely Disabled Persons (1,170 sq m) • 50-place Hostel for Severely Physically Handicapped Persons (1,043 sq m) * • 50-place Hostel for Severely Mentally Handicapped Persons (967 sq m) ** • Integrated Family Service Centre (588.5 sq m) • 50-place Hostel for 		Bailey Street / Wan On Street		Ma Tau Wai Road / Sheung Heung Road		Hung Fook Street/ Ngan Hon Street	<p>1. KC-010 Hung Fook Street/ Ngan Hon Street Development Scheme (the Scheme) is one of the redevelopment projects identified by the District-Based Study for Kowloon City. To improve traffic accessibility and to alleviate traffic congestion of the scheme area, a through road connecting Hung Fook Street and Ngan Hon Street is proposed as community benefits of the Scheme.</p> <p>Among the redevelopment projects in Kowloon City, Kai Ming Street Demand-led Redevelopment Project (DL-8: KC) which is located immediately to the south of the Scheme has taken the opportunity to provide about 450m² area for a Neighbourhood Elderly Centre in order to meet the need of social welfare facility for the district.</p> <p>Apart from the new through road, an underground communal car park is proposed to accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopment within the area as specified in the Development Scheme Plan (DSP) as a community gain for the district. Therefore, G/IC use area will not be provided within the Scheme.</p>
Kowloon City	Chung Tin Street / Sung Chi Street	<ul style="list-style-type: none"> • 150-place Residential Care Home for the Elderly cum 20-place Day Care Unit (2,625 sq m) • 160-place Integrated Vocational Rehabilitation Services Centre (1,110 sq m) * • 50-place Care and Attention Home for Severely Disabled Persons (1,170 sq m) • 50-place Hostel for Severely Physically Handicapped Persons (1,043 sq m) * • 50-place Hostel for Severely Mentally Handicapped Persons (967 sq m) ** • Integrated Family Service Centre (588.5 sq m) • 50-place Hostel for 									
	Bailey Street / Wan On Street										
	Ma Tau Wai Road / Sheung Heung Road										
	Hung Fook Street/ Ngan Hon Street										

Government Department	Comments			Response
			<p>Moderately Mentally Handicapped Persons (780 sq m) *</p> <ul style="list-style-type: none"> • 40-place Supported Hostel for Mentally Handicapped Persons (576 sq m) • Subvented Urban Hostel for Single Persons (488 sq m) • 50-place Day Activity Centre (415 sq m) ** • Neighbourhood Elderly Centre (394 sq m) • Counselling Centre for Psychotropic Substance Abusers (246 sq m) <p>* , ** facilities to be paired up</p>	<p>2. Noted.</p> <p>3. Please refer to the response in paragraph. 1 above.</p>
	<p>2. Please note that we have no further comments on the SIA report.</p> <p>3. We will review the proposed welfare facilities with regard to the finalized development scheme, and shall let you have the detailed requirement of respective facilities (such as schedule of accommodation, etc.) once the proposal is confirmed.</p>			

Urban Renewal Authority

Hung Fook Street / Ngan Hon Street Development Scheme (KC-010)

Response to Public Comments

Summary of Figure

<u>Type of Comment</u>	<u>Number</u>
Support	3
Objection	7
General Comments	2
Total	12

Ref. Nos.	Comments	Response of URA
	Category: Support the Scheme	
002	<ul style="list-style-type: none"> 落雨外牆嚴重滲水，牆壁裂痕，擔心建築物老而帶危險，堅決支持拆遷。 	<ul style="list-style-type: none"> 備悉 Noted
003	<ul style="list-style-type: none"> 落雨外牆漏水很利害，晚上不敢睡覺，嚴重影響我的學習，支持盡快拆遷。 	<ul style="list-style-type: none"> 備悉 Noted

Ref. Nos.	Comments	Response of URA
004	<ul style="list-style-type: none"> 盡快發展計劃 	<ul style="list-style-type: none"> 備悉 Noted
	Category: General Comment	
005	<ul style="list-style-type: none"> 建議受重建影響之單一層無出租自住樓宇高齡業主，可享有（綠、白）表資格購（二手或一手）居屋自住。 	<p>1. 市建局現行的物業收購準則是根據 2001 年 3 月立法會財務委員會通過的修訂自置居所津貼及發放予商用物業業主的特惠津貼而訂定。</p> <p>市建局會以抽籤形式委託七家註冊專業產業測量師行就評估七年樓齡(不是十年) 的假設重置單位的單位價值提供獨立意見，以計算各住宅物業的「自置居所津貼」。根據該七家註冊專業產業測量師行提供的估值，市建局會採用剔除最高最低並以平均數的方法計算出假設重置單位的呎價。市建局認為這個計算呎價的機制完全符合公平公正的原則，並且沿用至今行之有效。</p> <p>市建局在收購物業時會給予業主其物業市值交吉價。物業市值是指若業主自願在公開市場出售有關物業，其可合理預期的成交價格。除物業市值，市建局在收購自住業主的住宅物業時，會加上一筆以「七年樓」呎價為基礎計算的「自置居所津貼」。這項津貼屬特惠性質，旨在使業主能在被收購單位的所在地區，購置一個面積相若但比被收購</p>

Ref. Nos.	Comments	Response of URA
		<p>的物業較新的單位。受影響的住宅物業自住業主，可按個人需要及經濟能力考慮不同的遷置安排。</p> <p>1. The acquisition principles adopted by the URA are based on the revised Home Purchase Allowance (HPA) and ex-gratia allowance for commercial properties endorsed by the Finance Committee of the Legislative Council in March 2001.</p> <p>The assessment of HPA is based on the value of a notional flat, which is defined as a seven-year-old flat in a building of comparable quality, situated in a similar locality in terms of characteristics and accessibility, and located at the middle floor with average orientation. The assessment is carried out by 7 nos. of independent surveyor firms which are selected by draw. The highest and the lowest value of the assessment are not selected and the final value of the HPA is based on the average value of the five assessments.</p> <p>Under prevailing compensation policy, URA will offer an owner-occupier of domestic property the market value (valued on vacant possession basis) of his property plus an ex-gratia allowance, i.e. HPA.</p>

Ref. Nos.	Comments	Response of URA
		<p>2. 市建局會在該發展計劃的原址或同區的市建局地盤提供「樓換樓」(受制於相關法律的更改)選擇(同區的市建局地盤只限於在提供「樓換樓」選擇時,該市建局地盤是用作「樓換樓」計劃及已得到授權/批准進行),作為給住宅自住業主的現金補償以外的一個額外的選擇。</p> <p>2. URA may offer "flat-for-flat" in a URA new development in-situ or in the same district (as URA may select for the purpose provided that necessary approval/ authorization has been obtained at the time of FFF offer) as an additional choice to cash compensation to owner-occupiers of domestic units (subject to any changes in the relevant policies).</p> <p>3. 居者有其居計劃(居屋)是香港房屋委員會(房委會)的資助置業計劃。房委會負責興建居屋單位,並以低於市值的價格(扣除地價)售予合格的市民。此外,「居屋 / 私人參建居屋計劃單位第二市場」(居屋第二市場)是讓現居公屋住戶和綠表資格證明書持有人可選購由首次出售日期起計第三年的居屋 / 私人參建居屋 / 租者置其屋單位。由於居屋屬資助房屋性質,假若市建局容許受重建影響的業主得到自置居所津貼後,再享有綠表資格購買居屋便會出現得到雙重福利的情況。</p>

Ref. Nos.	Comments	Response of URA
		<p>3. Home Ownership Scheme (HOS) is a subsidized home purchase scheme offered by the Housing Authority (HA) which offers the selling price lower than the market price (after deduction of land premium), to eligible buyers. HOS secondary market offers domestic tenants of HA and holders of Green Form Certificate issued by the Housing Department (HD) to purchase HOS/PSPS/TPS units. HOS is a kind of subsidized housing scheme while calculation of compensation offer to owner-occupier by the URA is based on market price plus an ex-gratia allowance, i.e. HPA. It will be a case of double benefit if a property owner has received compensation (including HPA) from URA and with the right to purchase subsidized HOS unit.</p>
006	<ul style="list-style-type: none"> • 本人是業主，希望能同區安置以樓換樓形式。 	<ul style="list-style-type: none"> • 市建局會在該發展計劃的原址或同區的市建局地盤提供「樓換樓」(受制於相關法律的更改)選擇(同區的市建局地盤只限於在提供「樓換樓」選擇時，該市建局地盤是用作「樓換樓」計劃及已得到授權/批准進行)，作為給住宅自住業主的現金補償以外的一個額外的選擇。 • URA may offer "flat-for-flat" in a URA new development in-situ or in the same district (as URA may select for the purpose provided that necessary approval/ authorization has been obtained at the time of FFF offer) as an

Ref. Nos.	Comments	Response of URA
		additional choice to cash compensation to owner-occupiers of domestic units (subject to any changes in the relevant policies).
	Category: Object the Scheme	
001	<ul style="list-style-type: none"> 榮光街 72-94 號、榮光街 96-118 號、銀漢街 44-54A 號範圍的大廈代表反對市建局土瓜灣鴻福街／銀漢街發展計劃（KC-010），理由是沒有將他們所住的居所列入重建範圍。 	<ol style="list-style-type: none"> 市建局一般會考慮下列因素而決定個別樓宇是否適合劃定為重建項目，例如樓齡、樓宇狀況、地盤面積、發展潛力、規劃裨益、業權分佈及財務可行性等。 For redevelopment, URA considers different factors, such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability, etc, for determining the suitability of a site for redevelopment. 除了上述因素外，鴻福街／銀漢街發展計劃與榮光街 72-94 號、榮光街 96-118 號、銀漢街 44-54A 號範圍的大廈被榮光街分隔，若將上述範圍合併發展，部份榮光街便要封閉，對交通有嚴重影響，亦沒有規劃效益。總括而言，經本局考慮以上因素，榮光街 72-94 號、榮光街 96-118 號、銀漢街 44-54A 號範圍的大廈並不適合納入發展計劃範圍內。

Ref. Nos.	Comments	Response of URA
		<p>2. The Scheme, and the buildings located at 72-94 Wing Kwong Street, 96-118 Wing Kwong Street, and 44-54A Ngan Hon Street are geographically separated by Wing Kwong Street. If these building are included in the Scheme, closure of part of Wing Kwong Street is required. As such, road traffic will be seriously affected and it will not bring about planning gain for the scheme as a whole. Therefore, 72-94 Wing Kwong Street, 96-118 Wing Kwong Street, and 44-54A Ngan Hon Street are not considered to be included in the scheme.</p>
007	<ul style="list-style-type: none"> 土瓜灣道 19 號為被發展的鴻福街／浙江街一樣殘舊，希望政府一起收購。 	<ul style="list-style-type: none"> 市建局於一般會考慮下列因素而決定個別樓宇是否適合劃定為重建項目，例如樓齡、樓宇狀況、地盤面積、發展潛力、規劃裨益、業權分佈及財務可行性等。土瓜灣道 19 號與附近土瓜灣道 17-31 號相連，不能單獨重建。況且，土瓜灣道 17-31 號的樓宇並非殘破，經本局考慮以上因素，市建局認為土瓜灣道 19 號現不適合納入發展計劃範圍內。 When assessing a site whether is deemed suitable for redevelopment, URA considers several factors, such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability, etc. Building of no. 19 To Kwa Wan Road is attached to the adjacent buildings of nos. 17-31 To Kwa Wan Road, to which building of no. 19 To Kwa Wan

Ref. Nos.	Comments	Response of URA
		Road cannot be redeveloped alone. Besides, these buildings are not dilapidated. As such, URA will not consider including no. 19 To Kwa Wan Road in the Scheme.
008	<ul style="list-style-type: none"> 一樣殘舊，希望政府一起收購 	<ul style="list-style-type: none"> 反對者沒有明確指出希望一併納入重建範圍的範圍。市建局重申，考慮個別樓宇是否適合劃定為重建項目時，需考慮多方面的因素，例如樓齡、樓宇狀況、地盤面積、發展潛力、規劃裨益、業權分佈及財務可行性等。 The objector has not indicated clearly which no. of building he/she asks for including in the Scheme. When assessing a site whether is deemed suitable for redevelopment, the URA considers several factors, such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability, etc.
009	<p>1. The bottle-neck of the traffic is Ma Tau Kok Road, To Kwa Wan Road and Ma Tau Wai Road. Construct a new road that bisect Hung Fook Street leading to Bailey Street and Ngan Hon Street has no help to the traffic alleviation but create more traffic jam junction, traffic pollution and waste resources, reduce built environment space.</p>	<p>1. The proposed new road extending from Wan On Street upto Ngan Hon Street aims at improving accessibility of the Scheme and adjacent areas and alleviating road traffic of To Kwa Wan Road and Ma Wai Road. According to the Traffic Impact Assessment, para. 3.7, the three benefits of the proposed traffic routing are listed as follows (a.-c.):-</p>

Ref. Nos.	Comments	Response of URA
		<ul style="list-style-type: none"> a. The egress traffic from Wing Kwong Street heading to Hung Hom Road and the east direction can exit directly via Wan On Street. It can minimize the detouring of the traffic at Ma Tau Wai Road/Wing Kwong Street junction and at Ma Tau Wai Road/Bailey Street junction; b. The extension of Wan On Street upto Ngan Hon Street can enhance the ingress traffic routing from the south to Wing Kwong Street area via Wan On Street and its extension, instead of detouring via Bailey Street eastbound, Sung On Street northbound, Lok Shan Road westbound, and To Kwa Wan Road southbound. c. The egress traffic can use the new extension from Wan On Street to the north via Lung Tak Street to Chi Kiang Street, instead of detouring from Wing Kwong Street, Ma Tau Wai Road southbound, Bailey Street eastbound and Sung On Street northbound. d. A detailed Air Quality Impact Assessment (AQIA) has been conducted in accordance with comments from the Environmental Protection Department. The AQIA has concluded that there is no serious air quality being affected by the proposed development and mitigation measures for air quality improvement have been proposed.

Ref. Nos.	Comments	Response of URA
	<p>2. Wong Tak Building, Eliver House and Ka Ming Mansion are also old, deteriorated and over 50 years. Including them can make the redevelopment more planning, environment and cost effectiveness.</p> <p>3. About provision of coach parking spaces</p> <ul style="list-style-type: none"> a. The trend of Hong Kong tourism positing still targeted on Mainland China tourists to buy luxury goods will go on or we need to re-position the tourism strategy. b. Those jewelry traders have received lots of complaints about their business operation and strategy. Will this kind of business last for long? c. To Kwa Wan/Hung Hom traditional is not a tourist consumption district. This area consists of community treasures such as cuisines, low price shopping, inter-action with local people, community spirit that no 	<p>2. When assessing a site whether is deemed suitable for redevelopment, the URA considers several factors such as, building age, building condition, area of the site, development potential, planning gain, ownership distribution, and financial viability, etc. Eliver House has been renovated and the building condition is satisfactory not dilapidated for redevelopment. As far as the planning merit is concern, Wong Tak Building, Eliver House and Ka Ming Mansion are geographically separated from the Scheme and are separated by Hung Fung Street and Ka Ming Street. Including these buildings in the Scheme is not practical.</p> <p>3. The provision of coach parking spaces is a condition / requirement imposed by TD. URA has no objection if such requirement is waived.</p>

Ref. Nos.	Comments	Response of URA
	<p data-bbox="510 371 902 403">suitable for high-end shopping.</p> <p data-bbox="389 496 1055 831">4. Building no. 1-31 To Kwa Wan Road and buildings enclosed by Kai Ming Street/ Yuk Shing Street/ Hung Fook Street are also seriously dilapidated and should be included in the redevelopment scheme. Including these buildings can increase the plot ratio and make the planning, design, disposition of the built elements more tidy, efficient and meeting the market needs.</p> <p data-bbox="389 1015 1055 1318">5. CDA incorporated in the urban development proofed to be effectively and beneficially to both development and the society. The URA adopts small scale community project development method, i.e. back to 70s and 80s situation, in small community scale, piece by piece, block by block, what is the outcome? What is the benefit? What is the effectiveness?</p>	<p data-bbox="1122 480 1899 895">4. Building condition of 1-31 (odd nos.) To Kwa Wan Road, building enclosed by Ka Ming Street/Yuk Street/Hung Fook Street are not dilapidated for redevelopment although building age of these building has been over 50 years. Besides, according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24, buildings of 1-31 (odd nos.) are currently zoned as Residential (Group A) R(A). Domestic building plot ratio for R(A) is restricted at 7.5 and non-domestic building plot ratio is restricted at 9. Including these buildings in the Scheme would not lead to increase in plot ratio as mentioned by the objector.</p> <p data-bbox="1122 1015 1899 1358">5. Adopting appropriate approach for redevelopment depends on the scale of the site and local characteristics of the district. Large scale comprehensive redevelopment areas involve more residents and businesses are affected, and a longer lead time is needed for this redevelopment model. Local community planning approach, on the other hand, has considered maintaining urban fabric and unique characteristics of the district. Under the proposed scheme, existing roads are kept</p>

Ref. Nos.	Comments	Response of URA
	<p>6. According to the TPB paper No. 9350 for consideration by the TPB on 31.5.2013, "Draft Urban Renewal plan for Kowloon City Stage 2 public engagement". – Revitalising heritage and designation for a themed walking trail. It stated that the walking trail is sub-divided into four sections, namely Waterfront Leisure Walk, Artistic Community Walk, Local Cultural Walk and Walled City Historic Walk to reflect the elements/characters of each section of the trail (p.3). However, the increase in volume of coaches may decrease the safety of the road user and pose potential dangers to pedestrian, walkers, and sightseeing tourists. This contradicts to the planning intention.</p>	<p>intact. Given the intention to maintain the existing continuous streetscape, an underground car park serves the areas as specified in the plan of the Explanatory Statement could lessen the conflicts between pedestrians and vehicular run-in/outs in the area so that existing community ambience will be preserved.</p> <p>6. The walking trial proposed by the Urban Renewal Plan for Kowloon City will be implemented by the Government. It does not have any overlapping portion with the proposed road routing (To Kwa Wan Road, Ma Tau Wai Road, Bailey Street, Wan On Street and Hung Fook Street) for coaches to enter the underground car park. The provision of coach parking spaces is the requirement/ condition of TD. URA has no objection if such requirement is waived.</p>

Ref. Nos.	Comments	Response of URA
	<p>7. In the TPB paper No. 9350, it emphasized on the mitigating the environmental impact caused by the East Kowloon Corridor to nearby residents (p.5). It proposed to rezone the land along the flyover from Residential Group A to Other Specific Uses. Without acquisition of the front buildings along Ma Tau Kok Road, the pollution problem to the nearby residents will not be improved.</p>	<p>7. The proposed area for rezoning from Residential (Group A) to Other Specific Uses is located to the west of the Scheme physically separated by the To Kwa Wan Road and the East Kowloon Corridor (at least 250m away). It is not feasible to include it into the Scheme.</p>
010	<p>1. 新道路以西之重建範圍面積過小</p> <p>a. 鴻福街及銀漢街之間擬議開闢一段新道路, 但新路段以西地段 (現時鴻福街 1 至 17 號 (單數) 和銀漢街 2 及 14 號 (雙數)) 重建後面積過小, 因被過分切割而未能提供最大發展效益和發展潛力, 故市建局應重新考慮道路和地段佈局, 增加住宅供應, 完善社區設施, 造福當區和全港市民。</p>	<p>1. 開闢由環安街伸展至銀漢街之擬議一段新道路的目的是改善發展計劃範圍及其周邊地方的交通可達性及紓緩土瓜灣道和馬頭圍道接壤銀漢街、榮光街和庇利街交界處的交通負荷量, 因此, 道路設計已考慮上述目的。此外, 擬議的道路設計已考慮對地盤劃分後對面積、新發展樓宇設計、樓宇位置及對附近環境影響 (例如噪音) 等因素。新道路的西面設置兩幢樓宇, 東面設置一幢樓宇的設計佈局是一個平衡發展參數/樓宇設計等因素的方案。</p> <p>1. The proposed through road extending from Wan On Street to Ngan Hon Street aims at improving accessibility of the Scheme and its adjacent area and alleviating traffic loading of the junctions at To Kwa Wan Road/</p>

Ref. Nos.	Comments	Response of URA
	<p>2. 未能說明不收購物土瓜灣 17 至 31 號(單數)原因</p> <p>a. 因為新道路以西重建範圍面積過小，所以市建局應將毗鄰地段土瓜灣 17 至 31 號(單數)納入重建範圍。</p> <p>b. 將上述地段納入重建範圍的優勢: a.有助於增強兩塊土地聯繫性，增加住宅和社區設施的供應； b.減少新舊建築物之間的差異感及違和感，使觀感統一； c.可改善更多當區市民的生活環境,是有利社區，有利區民的多贏之舉。</p> <p>3. 新道路帶來潛在空氣污染、噪音及交通安全問題</p> <p>a. 新道路會增加該區交通流量，將導致該區受到更嚴重的空氣污染和噪音污染。</p>	<p>Ngan Hon Street and Wing Kwong Street/ Bailey Street. Separation of the existing site into two parts by the proposed through road has allowed for development parameter and the layout/design of the proposed residential towers.</p> <p>2. 市建局一般會考慮下列因素而決定個別樓宇是否適合劃定為重建項目，例如樓齡、樓宇狀況、地盤面積、發展潛力、規劃裨益、業權分佈及財務可行性等。土瓜灣 17-31 號(單數)的樓宇並非殘破，因此，局方未有計劃將土瓜灣 17-31 號(單數)納入發展計劃之內。</p> <p>2. For redevelopment, URA considers different factors, such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability, etc, for determining the suitability of a site for redevelopment. Building condition of nos. 17-31 (odd nos.) is not dilapidated. As such, URA will not consider including nos. 17-31 (odd nos.) Tò Kwa Wan Road in the Scheme.</p> <p>3. 鴻福街／銀漢街發展計劃的規劃報告附件二及三分別附載交通影響評估及環境評估，兩份附件已就擬議的發展計劃及新道路對交通影響、空氣質素影響和噪音影響等範疇作出評估，綜合回應如下。</p>

Ref. Nos.	Comments	Response of URA
	<p>b. 過於密集的交通網絡更容易造成交通事故，交通安全問題成為公眾的疑慮。</p> <p>c. 因此市建局應以市民的健康安全為首要考慮，規劃更為完善的道路網絡。</p>	<p>i) 空氣質素影響評估和噪音影響評估的指出該發展計劃並未受擬議新道路及周邊環境狀況對空氣質素和噪音帶來嚴重影響。按環境保護署的意見，市建局已就空氣質素影響評估和噪音影響評估作更詳細的數據分析，並列出改善空氣質素和噪音影響的措施，有關報告已提交環境保護署參考。</p> <p>ii) 擬議新道路已考慮行人過路的安全而分別於榮光街、啟明街、鴻福街及銀漢街的地面過路處增設非燈號控制的行人輔助線，詳細位置可參閱交通影響評估報告圖 2.2。此外，進入環安街的車輛可利用擬議新道路由榮光街駛通往馬頭圍道南行方向，代替現時須由環安街右轉通過底利街東行的三條行車線駛往西行的行車線，新的路線能帶來更安全的安通安排(詳細可參閱交通影響評估(圖 3.3)。</p> <p>3. Traffic Impact Assessment (TIA) and Environmental Assessment (EA) for the Scheme have addressed the traffic impact, air quality impact and noise impact imposed by the proposed development and the through road. Results of the assessments are summarized as follow.</p> <p>i) Air quality impact assessment and noise impact</p>

Ref. Nos.	Comments	Response of URA
	<p>4. 未能說明重建範圍不包括新道路以東一段鴻福街之原因及未能保證計劃不會推倒重來</p> <p>a. 避免像重建項目 KC-008A 的情況再次發生，市建局應承諾不會再把任何重建計劃</p>	<p>assessment for the Scheme have concluded that no adverse impact is imposed by the proposed development. Detailed quantitative analysis for the above assessments mitigation measures for the air have been submitted to EPD for reference.</p> <p>ii) At grade pedestrian crossing (cautionary crossing) is proposed along the through road at Wing Kwong Street, Kai Ming Street and Ngan Hon Street (refer to TIA, figure 2.2,). Besides, the egress traffic from Wan On Street can exit via Wing Kwong Street and Ma Tau Wai Road southbound carriageway. This routing replace the right turn movements from Wan On Street to Bailey Street, where egress traffic heading to Ma Tau Wai Road and the west direction has to cross 3 traffic lanes (refer to TIA figure 3.3). To sum up, road safety issues have been considered and measures have been proposed.</p> <p>4. 市建局根據《市區重建局條例》第 25 條以發展計劃形式開展鴻福街/銀漢街發展計劃，根據上述條例的規定，市建局需要向城規會提交發展計劃草圖，城規會如認為該發展計劃草圖適宜公布，便會按《城市規劃條例》第 5 條的程序進行 2 個月的公眾諮詢，公眾可就發展計劃草圖提出</p>

Ref. Nos.	Comments	Response of URA
	<p>以任何理由推倒重來，且詳細說明重建範圍不包括（環安街延長後）新道路以東一段鴻福街之原因，及為不會推倒本項目並重推新計劃而做出保證。</p>	<p>申述/意見(包括項目範圍)，城規會按程序對發展計劃草圖不提出/提出修訂。市建局需獲得核准的發展計劃圖才能開展物業收購的程序。</p> <p>4. The Scheme will be implemented by way of a development scheme under section 25 of the Urban Renewal Authority Ordinance (URAO). Town Planning Board (TPB) will consider whether the draft Development Scheme Plan (DSP) of the Scheme submitted by URA is deemed to be suitable for publication under the Town Planning Ordinance (TPO). If it is exhibited for public inspection under section 5 of the TPO for a period of two months, any person may make representation (e.g. boundary of the Scheme) in writing to the TPB in respect of the draft DSP. TPB may or may not propose amendment to the draft DSP after hearing of representation and comments. URA will implement the Scheme according to the approved DSP.</p>
011	<p>1. 建議將 九龍土瓜灣玉成街 1-3 號、啟明結 1-5 號、玉成街 5-7 號及鴻福街 2-6 號 納入重建範圍，有關範圍增大，更有效進行社區規劃，增加成本效益，居民也能藉此重建機會，得以改善生活。</p>	<p>1. 市建局於評估個別樓宇是否適合劃定為重建項目時，需考慮多方面的因素，例如樓齡、樓宇狀況、地盤面積、發展潛力、規劃裨益、業權分佈及財務可行性等。除了上述原因外，鴻福街／銀漢銀發展計劃與玉成街 1-3 號及啟明結</p>

Ref. Nos.	Comments	Response of URA
	<p>2. 居民對於市建局願意通過加快舊區重建步伐，表示十分認同和支持，希望城市規劃委員會可以接納市建局的重建項目計劃。</p>	<p>1-5 號和玉成街 5-7 號及鴻福街 2-6 號的大廈位於發展計劃的西南面而且被鴻福街及玉成街分隔。如納入發展，部份鴻福街和玉成街便要封閉，將上述範圍合併發展共未能帶來規劃效益。總括而言，經本局考慮以上因素，玉成街 1-3 號及啟明結 1-5 號和玉成街 5-7 號及鴻福街 2-6 號的大廈並不適合納入發展計劃範圍內。</p> <p>1. When assessing a site whether is deemed suitable for redevelopment, the URA considers several factors, such as, building condition, size of the site, development potential, planning gain, ownership distribution and financial viability etc. Besides, nos. 1-3 Yuk Shing Street and nos. 1-5 Kai Ming Street, nos. 5-7 Yuk Shing Street and nos. 2-6 Hung Fook Street are located southwest of the Scheme and are separated by Hung Fook Street and Yuk Shing Street. If these buildings are included in the Scheme, closure of portion of Hung Fook Street and Yuk Shing Street are required. Including this area of buildings in the Scheme would not bring about much planning gain. As such, URA has considered not to include these buildings in the Scheme.</p> <p>2. 備悉</p> <p>2. Noted</p>

Ref. Nos.	Comments	Response of URA
SIA 2 - 001	<p>1. 重建的需求已經成為個別業主忽視大廈衛生和拖延大廈進行必要維修的誘因，持份者和政府不應從容或助長此等不良動機或風氣。</p>	<p>1. 銀漢街 12 號樓齡約 58 年，按市建局樓宇狀況調查顯示，該樓宇狀況被評為“失修”（即第二差級別）。據反對項目者所指，於市建局開展鴻福街/報銀漢街發展計劃（即 2016 年 6 月 3 日）前，有關政府部門於 2014 年 7 月及更早的時間曾去信該大廈所有業主，要求他們對大廈天台及公共部分進行維修，但大廈的業主委員會卻沒有理會有關要求。市建局重申，大廈維修屬業主負責，大廈不論是否納入市建局的重建項目，業主亦應履行政府部門對大廈發出的維修命令或要求。即使有關大廈被市建局宣佈納入重建範圍，當發展計劃未獲核准及市建局仍未能成功收購有關物業前，業主仍然有責任大廈及單位的維修，以確保居住環境安全。</p> <p>1. Building age of no. 12 Ngan Hon Street is about 58 years. According to the Building Condition Survey (BCS) conducted by URA, building condition of no. 12 Ngan Hon Street was categorized as “varied” (the second worst category). According to the objector, relevant Government departments have issued letter to all owners of the building to carry out maintenance works for the roof and public area of the building. However, no maintenance works has been carried out so far. It is reiterated that the responsibility of building maintenance lies in owners of the building. Property owners are</p>

Ref. Nos.	Comments	Response of URA
	<p>2. 已經支付的維修費用和相關損失因重建太早落實而無法及時追討。</p> <p>3. 重建催生了非法佔用人。</p>	<p>required to carry out building maintenance works to fulfil building order or request issued by relevant Government departments regardless the building is fallen redevelopment project area promulgated by URA or not.</p> <p>2. 根據反對項目者所指，其單位維修工程分別於 2015 年及 2016 年初進行，對於他與項目內其他業主就維修費用由誰支付的糾紛，市建局不適宜作出評論。市建局重申，大廈或單位的維修屬業主責任。</p> <p>2. URA is not in an appropriate position to comment on dispute between the objector and other owners regarding share of building maintenance works fee.</p> <p>3. 市建局會對已收購物業的租客提供編配安置或發放特惠金作為回收其租住地方的補償。就反對項目者與租客的租務糾紛，市建局不宜作出評論。雖然如此，假若重建計劃得以落實進行，市建局會於成功收購物業時，與租客進行補償資格的身份核實工作，評估他們是否合資格獲得補償。</p> <p>3. URA will offer ex-gratia allowance or rehousing unit to tenants of the acquired properties. URA is not in an appropriate position to comment on the dispute between</p>

Ref. Nos.	Comments	Response of URA
	<p>4. 大廈重建與維修之間，如果有得選，本人寧可選擇大廈維修。</p>	<p>the objector and his tenant. Eligibility for receiving compensation to tenants offered by URA will be assessed once URA has acquired the property</p> <p>4. 上文第一段已提及，發展計劃內銀漢街 12 號的樓宇屬“失修”狀況(即第二最差類別)。從樓宇狀況及第二階段社會影響評估報告中的居民意見顯示，發展計劃內的居住環境狀況並不理想。超過一半受訪者表示其住所過去一年曾間中或經常出現滲水/漏水問題及出現石屎剝落問題。</p> <p>在發生計劃內亦發現約 40%已調查的原有單位被劃分為有獨立或共用廁所及煮食設施的分間單位，居住環境擠迫及對樓宇安全造成威脅，依靠個別業主進行大廈維護仍未能徹底解決樓宇安全問題。</p> <p>此外，第二階段社會影響評估報告中，約 80%受訪者表示支持/強烈支持進行建議中的發展計劃。</p> <p>4. As mentioned in the first paragraph above, building condition of no. 12 Ngan Hon Street, is categorized as “varied”. Besides, as revealed in the Stage 2 Social Impact Assessment (SIA) report, more than half of respondents indicated that their units occasionally or frequently suffered from water seepage or concrete</p>

Ref. Nos.	Comments	Response of URA
		<p>spalling. Living condition of the building is undesirable.</p> <p>It is found that about 40% of surveyed unit have been either subdivided into self-contained units or partitioned rooms. Living condition of these units is overcrowded and imposes potential danger to the building safety. Relying on individual owners for carrying out building maintenance cannot resolve the building safety issue utterly.</p> <p>Besides, according to the Stage 2 SIA report, about 80% respondents have expressed support/strongly support of the proposed development scheme.</p>

- End -

☐ Urgent ☐ Return receipt ☐ Sign ☐ Encrypt ☐ Mark Subject Restricted ☐ Expand groups



Fw: KC-010 Hung Fook St - Ngan Hon St : DSP Notes and ES

From: "Law, Matthew" <MTWLaw@ura.org.hk>
To: "bysyan@pland.gov.hk" <bysyan@pland.gov.hk>,
Cc: "jwycheng@pland.gov.hk" <jwycheng@pland.gov.hk>, "Kwan, Mike" <MYFKwan@ura.org.hk>
Date: 12/09/2016 11:36
Subject: RE: KC-010 Hung Fook St - Ngan Hon St : DSP Notes and ES

Dear Barry,

I refer to your attachment file containing comments on the revised DSP, Notes, and ES. Attached please find the revised DSP, Notes, ES and Plan 2 of the ES for your follow up.

While we have followed most of the comments provided by your department and amended the above documents accordingly, I want to draw your attention on the following different views on the comments which we have not followed.

Both Notes and ES

Your comment "undertaken by Urban Renewal Authority" will be replaced by "commenced by Urban Renewal Authority"

For Chinese version your comment "由市區重建局進行" will be replaced by "由市區重建局開展".

ES

Para. 7.6 "Kai Ming Street (DL-8:KC)" remains unchanged and it is reflected in Plan 2 of the ES.

Notes (Chinese Version)

Page 3, para (2). The word (8) cannot be aligned with the first line due to default alignment constraint of MS Word.

Regards,

Matthew Law
Planning and Design Division
Tel. 2588 2176

From: bysyan@pland.gov.hk [mailto:bysyan@pland.gov.hk]
Sent: Friday, September 09, 2016 4:08 PM
To: Law, Matthew
Cc: jwycheng@pland.gov.hk
Subject: KC-010 Hung Fook St - Ngan Hon St : DSP Notes and ES
Importance: High

Dear Matthew,

I refer to your mail dated 10.8.2016. Please find attached our comments on the revised DSP, Notes and ES for your follow-up action.

Regards
Barry Yan
for DPO/K, PlanD
2231 4978

GR (bcc), pl file

From:	"Law, Matthew" <MTWLaw@ura.org.hk>
To:	"jwycheng@pland.gov.hk" <jwycheng@pland.gov.hk> ,
Cc:	"bysyan@pland.gov.hk" <bysyan@pland.gov.hk>
Date:	10/08/2016 10:27
Subject:	FW: KC-010 Hung Fook St - Ngan Hon St : Departmental Comemnts Batch 2

Dear Johanna,

Just known that Barry is on leave till 15/8/2016.
I forward below email to Barry for your information please.

Regards,

Matthew Law
Planning and Design Division
Tel. 2588 2176

From: Law, Matthew
Sent: Wednesday, August 10, 2016 10:10 AM
To: bysyan@pland.gov.hk
Cc: Kwan, Mike
Subject: RE: KC-010 Hung Fook St - Ngan Hon St : Departmental Comemnts Batch 2

G] [attachment "KC-010 Site A B - DSP_PLAN 1_with PlanD comment.pdf" deleted by Barry YS YAN/PLAND/HKSARG] [attachment "KC-010 Site A B - DSP_with PlanD comment.pdf" deleted by Barry YS YAN/PLAND/HKSARG] [attachment "KC-010 Site A B - DSP_PLAN 2_with PlanD comment.pdf" deleted by Barry YS YAN/PLAND/HKSARG] [attachment "KC010 DSP Notes_r_eng_with PlanD comment.pdf" deleted by Barry YS YAN/PLAND/HKSARG] [attachment "KC010 Explanatory Statement_r_eng_with PlanD comment_r1.pdf" deleted by Barry YS YAN/PLAND/HKSARG]

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KC_010 DSP Explanatory Statement with PlanD comment_Chi_R5.pdf



KC_010 DSP Explanatory Statement with PlanD comment_Eng_R2.pdf



KC_010 DSP Notes with PlanD comment_Chi R5.pdf



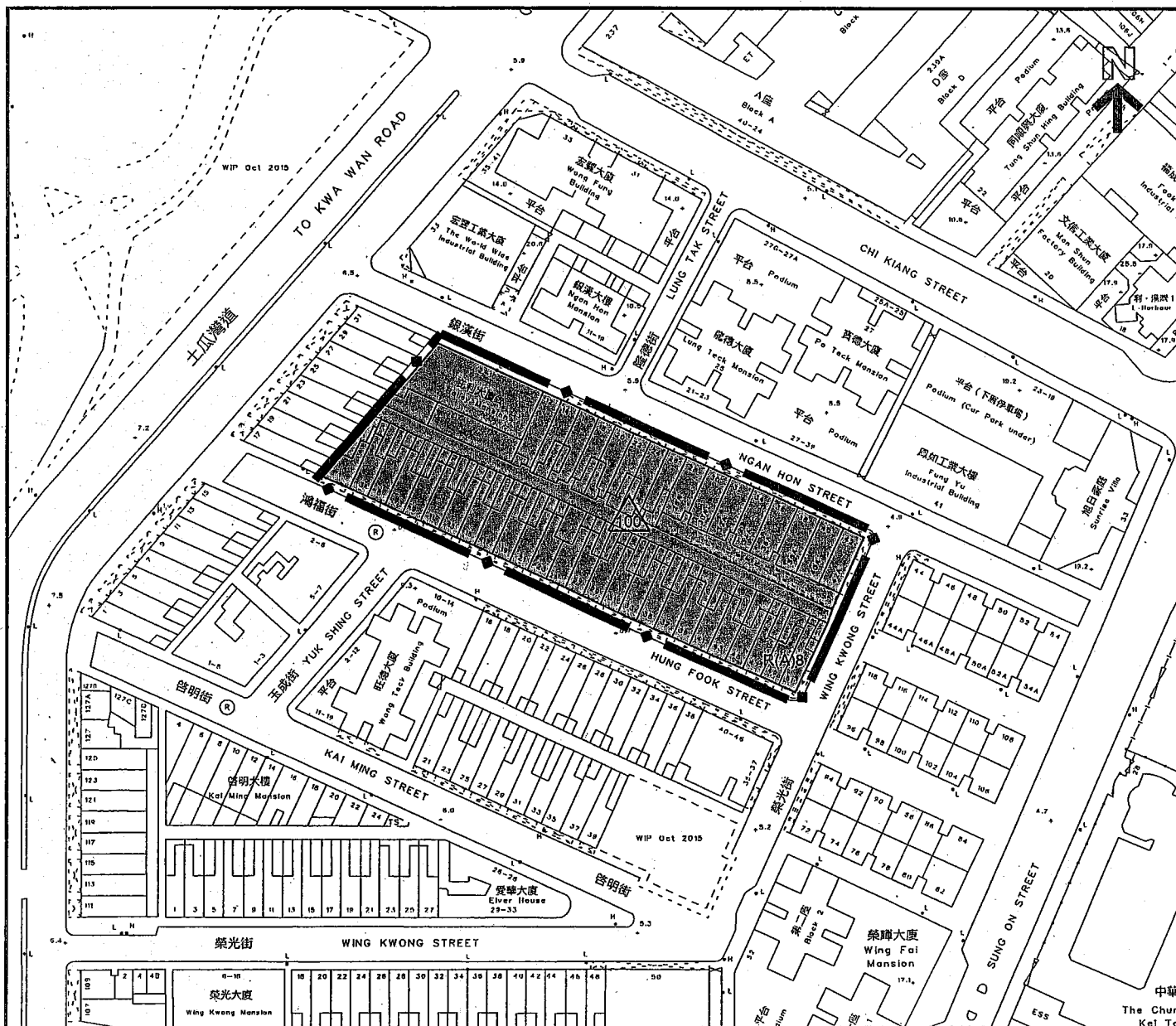
KC_010 DSP Notes with PlanD comment_Eng_R2.pdf



KC-010 Site A B - DSP_20160912 PLAN 2.pdf



KC-010 Site A B - DSP_20160912.pdf



圖例 NOTATION

- BOUNDARY OF DEVELOPMENT.....發展計劃範圍界線
SCHEME
- RESIDENTIAL (GROUP A).....住宅(甲類)
- MAXIMUM BUILDING HEIGHT.....最高建築物高度
(100 METRES ABOVE PRINCIPAL DATUM) (在主水平基準上100米)

夾附的<<註釋>>屬這份圖則的一部分
THE ATTACHED NOTES
ALSO FORM PART OF THIS PLAN

2016年 月 日城市規劃委員會根據市區重建局條例第25(3)(a)條認為圖則
適宜公布，並於2016年 月 日按照城市規劃條例第5條顯示。
PLAN DEEMED SUITABLE BY THE TOWN PLANNING BOARD FOR
PUBLICATION UNDER SECTION 25(3)(a) OF THE URBAN RENEWAL
AUTHORITY ORDINANCE ON 2016 AND EXHIBITED UNDER
SECTION 5 OF THE TOWN PLANNING ORDINANCE ON 2016.

香港城市規劃委員會依據城市規劃條例擬備的市區重建局鴻福街/銀漢街發展計劃圖則 TOWN PLANNING ORDINANCE, HONG KONG TOWN PLANNING BOARD URBAN RENEWAL AUTHORITY HUNG FOOK STREET / NGAN HON STREET DEVELOPMENT SCHEME PLAN

依據市區重建局條例第25(3)(a)條擬備
PREPARED UNDER SECTION 25(3)(a) OF THE
URBAN RENEWAL AUTHORITY ORDINANCE

圖則編號
PLAN No. S/K9/URA2/A

SECRETARY, TOWN PLANNING BOARD 城市規劃委員會秘書

SCALE 1:1000 比例尺
米 METRES 10 0 10 20 30 40 50 METRES 米

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the Purposes of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

NOTES

(N.B. These form part of the Plan)

- (1) These Notes show the uses or developments on land falling within the boundaries of the Plan which are always permitted and which may be permitted by the Town Planning Board, with or without conditions, on application. Where permission from the Town Planning Board for a use or development is required, the application for such permission should be made in a prescribed form. The application shall be addressed to the Secretary of the Town Planning Board, from whom the prescribed application form may be obtained.
- (2) Any use or development which is always permitted or may be permitted in accordance with these Notes must also conform to any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, as may be applicable.
- (3)
 - (a) No action is required to make the existing use of any land or building conform to this Plan until there is a material change of use or the building is redeveloped.
 - (b) Any material change of use or any other development (except minor alteration and/or modification to the development of the land or building in respect of the existing use which is always permitted) or redevelopment must be always permitted in terms of the Plan or, if permission is required, in accordance with the permission granted by the Town Planning Board.
 - (c) For the purposes of subparagraph (a) above, "existing use of any land or building" means –
 - (i) before the publication in the Gazette of the notice of the first statutory plan covering the land or building (hereafter referred as 'the first plan'),
 - a use in existence before the publication of the first plan which has continued since it came into existence; or

S/K9/URA2/A

- a use or a change of use approved under the Buildings Ordinance which relates to an existing building; and
- (ii) after the publication of the first plan,
- a use permitted under a plan which was effected during the effective period of that plan and has continued since it was effected; or
 - a use or a change of use approved under the Buildings Ordinance which relates to an existing building and permitted under a plan prevailing at the time when the use or change of use was approved.
- (4) Except as otherwise specified by the Town Planning Board, when a use or material change of use is effected or a development or redevelopment is undertaken, as always permitted in terms of the Plan or in accordance with a permission granted by the Town Planning Board, all permissions granted by the Town Planning Board in respect of the site of the use or material change of use or development or redevelopment shall lapse.
- (5) Road widths, road junctions and alignments of roads may be subject to minor adjustments as detailed planning proceeds.
- (6) Temporary uses (expected to be 5 years or less) of any land or building are always permitted as long as they comply with any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, and there is no need for these to conform to the zoned use or these Notes. For temporary uses expected to be over 5 years, the uses must conform to the zoned use or these Notes.
- (7) The following uses or developments are always permitted on land falling within the boundaries of the Plan except where the uses or developments are specified in Column 2 of the Schedule of Uses:
- (a) provision, maintenance or repair of plant nursery, amenity planting, open space, rain shelter, refreshment kiosk, road, bus/public light bus stop or lay-by, cycle track, Mass Transit Railway station entrance, Mass Transit Railway structure below ground level, taxi rank, nullah, public utility pipeline, electricity mast, lamp pole, telephone booth, telecommunications radio base station, automatic teller machine and shrine; and
 - (b) geotechnical works, local public works, road works, sewerage works, drainage works, environmental improvement works, marine related facilities, waterworks (excluding works on service reservoir) and such other public works co-ordinated or implemented by Government.

S/K9/URA2/A

- (8) Unless otherwise specified, all building, engineering and other operations incidental to and all uses directly related and ancillary to the permitted uses and developments within the same zone are always permitted and no separate permission is required.
- (9) In these Notes, "existing building" means a building, including a structure, which is physically existing and is in compliance with any relevant legislation and the conditions of the Government lease concerned.
- (10) Any development not compatible with the Urban Renewal Authority's Development Scheme for the area is prohibited by virtue of section 25(4) of the Urban Renewal Authority Ordinance.

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

Schedule of Uses

	<u>Page</u>
RESIDENTIAL (GROUP A) 8	1

RESIDENTIAL (GROUP A) 8

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Ambulance Depot	Commercial Bathhouse/ Massage Establishment
Flat	Eating Place
Government Use (not elsewhere specified)	Education Institution
House	Exhibition or Convention Hall
Library	Government Refuse Collection Point
Market	Hospital
Place of Recreation, Sports or Culture	Hotel
Public Clinic	Institutional Use (not elsewhere specified)
Public Transport Terminus or Station (excluding open-air terminus or station)	Mass Transit Railway Vent Shaft and/or Other Structure above Ground Level other than Entrances
Public Vehicle Park (coach parking only)	Office
Residential Institution	Petrol Filling Station
School (in free-standing purpose-designed building only)	Place of Entertainment
Social Welfare Facility	Private Club
Utility Installation for Private Project	Public Convenience
	Public Transport Terminus or Station (not elsewhere specified)
	Public Utility Installation
	Public Vehicle Park (excluding container vehicle) (not elsewhere specified)
	Religious Institution
	School (not elsewhere specified)
	Shop and Services
	Training Centre

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

In addition, the following uses are always permitted: (a) on the lowest three floors of a building, taken to include basements; or (b) in the purpose-designed non-residential portion of an existing building, both excluding floors containing wholly or mainly car parking, loading / unloading bay and / or plant room:

Eating Place
Educational Institution
Institutional Use (not elsewhere specified)
Off-course Betting Centre
Office
Place of Entertainment
Private Club
Public Convenience
Recyclable Collection Centre
School
Shop and Services
Training Centre

Planning Intention

This zone is intended primarily for high-density residential developments with the provision of an underground car park, that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments commenced by Urban Renewal Authority within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

Remarks

- (1) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in the plot ratio for the building upon development and/ or redevelopment in excess of 7.5 for a domestic building or 9.0 for a building that is partly domestic and partly non-domestic, or the plot ratio of the existing building, whichever is the greater. Except where the plot ratio is permitted to be exceed under paragraphs (8) and/or (9) hereof, under no circumstances shall the plot ratio for the domestic part of any building, to which this paragraph applies, exceed 7.5.

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

Remarks (Cont'd)

- (2) For a non-domestic building to be erected on the site, the maximum plot ratio shall not exceed 9.0 except where the plot ratio is permitted to be exceeded under paragraphs (8) and/or (9) hereof.
- (3) For the purposes of paragraph (1) above, no addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of the relevant maximum domestic and/or non-domestic plot ratio(s), or the domestic and/or non-domestic plot ratio(s) of the existing building, whichever is the greater, subject to, as applicable -
 - (i) the plot ratio(s) of the existing building shall apply only if any addition, alteration and/or modification to or redevelopment of an existing building is for the same type of building as the existing building, i.e. domestic, non-domestic, or partly domestic and partly non-domestic building; or
 - (ii) the maximum domestic and/or non-domestic plot ratio(s) stated in paragraph (1) above shall apply if any addition, alteration and/or modification to or redevelopment of an existing building is not for the same type of building as the existing building, i.e. domestic, non-domestic, or partly domestic and partly non-domestic building.
- (4) In determining the relevant maximum plot ratio for the purposes of paragraphs (1) and (2), area of any part of the site that is occupied or intended to be occupied by free-standing purpose-designed buildings (including both developed on ground and on podium level) solely for accommodating Government, institution or community facilities including school(s) as may be required by Government shall be deducted in calculating the relevant site area.
- (5) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of the maximum building height in terms of metres above Principal Datum (mPD) as stipulated on the Plan, or the height of the existing building, whichever is the greater.

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

Remarks (Cont'd)

- (6) In determining the relevant maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for use solely as plant room, caretaker's office, or caretaker's quarters and recreational facilities for the use and benefit of all the owners or occupiers of the domestic building or domestic part of the building, provided such uses and facilities are ancillary and directly related to the development or redevelopment, may be disregarded.
- (7) An underground car park shall be provided. The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by government. In determining the maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for the use solely as the underground car park may be disregarded.
- (8) Where the permitted plot ratio as defined in Building (Planning) Regulations is permitted to be exceeded in circumstances as set out in Regulation 22(1) or (2) of the said Regulations, the plot ratio for the building on land to which paragraphs (1) or (2) above applies may be increased by the additional plot ratio by which the permitted plot ratio is permitted to be exceeded under and in accordance with the said Regulation 22(1) or (2), notwithstanding that the relevant maximum plot ratio specified in the paragraphs (1) or (2) above may thereby be exceeded.
- (9) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the plot ratio and building height restrictions as stated in paragraphs (1), (2) and (5) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

DRAFT URBAN RENEWAL AUTHORITY

HUNG FOOK STREET/NGAN HON STREET

DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

EXPLANATORY STATEMENT

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

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2.	AUTHORITY FOR THE PLAN AND PROCEDURES	1
3.	OBJECT OF THE PLAN	2
4.	NOTES OF THE PLAN	3
5.	AREA COVERED BY THE PLAN	3
6.	EXISTING CONDITIONS	3
7.	PLANNING AND LAND USE PROPOSALS	4
8.	IMPLEMENTATION OF THE SCHEME	7

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the Purpose of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

EXPLANATORY STATEMENT

Note: For the purposes of the Town Planning Ordinance, this statement shall not be deemed to constitute a part of the Plan.

1. INTRODUCTION

This explanatory statement is intended to assist an understanding of the draft Urban Renewal Authority (URA) Hung Fook Street/Ngan Hon Street Development Scheme Plan (the Plan) No. S/K9/URA2/A. It reflects the planning intention and objectives of the Town Planning Board (the Board) for the area covered by the Plan.

2. AUTHORITY FOR THE PLAN AND PROCEDURES

2.1 In the URA's 15th Business Plan (2016/17) approved by the Financial Secretary in early 2016, Hung Fook Street/Ngan Hon Street Development Scheme (KC-010) was proposed to be processed as a Development Scheme under section 25 of the URA Ordinance (URAO).

2.2 On 3 June 2016, pursuant to section 23(1) of the URAO, the URA notified in the Government Gazette the commencement of implementation of the Hung Fook Street/Ngan Hon Street Development Scheme.

- 2.3 On 10 June 2016, the URA submitted the draft URA Hung Fook Street/Ngan Hon Street Development Scheme Plan for the Development Scheme to the Board under section 25(5) of the URAO.
- 2.4 On XXXX, the Board, under section 25(6)(a) of the URAO, deemed the draft URA Hung Fook Street/Ngan Hon Street Development Scheme Plan as being suitable for publication. Under section 25(7) of the URAO, the draft Development Scheme Plan, which the Board has deemed suitable for publication under section 25(6)(a) of the URAO, is deemed to be a draft plan prepared by the Board for the purposes of the Town Planning Ordinance (the Ordinance).
- 2.5 On XXXX, the draft Hung Fook Street/Ngan Hon Street Development Scheme Plan No. S/K9/URA2/1 (the Plan) was exhibited under section 5 of the Ordinance. By virtue of section 25(9) of the URAO, the Plan has from the date replaced the draft Hung Hom Outline Zoning Plan (OZP) No. S/K9/25 in respect of the area delineated and described herein.

3. OBJECT OF THE PLAN

The Plan illustrates that the Development Scheme Area (the Area) is designated as "Residential (Group A)8" ("R(A)8"). It is planned to be developed by means of the Development Scheme prepared under section 25 of the URAO. The Scheme intends to be primarily for high-density residential developments with the provision of an underground car park that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments commenced by URA within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street (the Wider Area) as demarcated on **Plan 1** as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

4. NOTES OF THE PLAN

- 4.1 Attached to the Plan is a set of Notes which shows the types of uses or developments which are always permitted within the Area in this zone and which may be permitted by the Board, with or without conditions, on application. The provision for application for planning permission under section 16 of the Ordinance allows greater flexibility in land use planning and control of development to meet changing needs.
- 4.2 For the guidance of the general public, a set of definitions that explains some of the terms used in the Notes may be obtained from the Technical Services Division of the Planning Department and can be downloaded from the Board's website at <http://www.info.gov.hk/tpb>.

5. AREA COVERED BY THE PLAN

- 5.1 The Scheme boundary which is shown in heavy broken line on the Plan, covers a total area of about 4,951m². The Scheme comprises tenement buildings which are bounded by Ngan Hon Street to the north, Wing Kwong Street to the east, Hung Fook Street to the south and a back lane of the adjacent buildings, nos. 17-31 To Kwa Wan Road, to the west.
- 5.2 The Scheme boundary has included Government lane and pavement area.
- 5.3 On the approved Hung Hom OZP No. S/K9/24, the Area is zoned "Residential (Group A)" before the exhibition of the Plan.

6. EXISTING CONDITIONS

- 6.1 The buildings within the Area are between 6 and 8 storeys and residential in nature with construction material retail shops, eating places, workshops and vehicle repair workshops on lower floors. Most of the existing buildings are in deteriorating or poor condition with

some sub-divided units on the upper floors. Staircase shops are found at the entrance of staircase in some buildings at ground floor.

- 6.2 The poor housing condition, the presence of illegal rooftop structures, temporary structures at back lanes, and a number of vehicle repair workshops at ground floor are sources of environmental nuisance in the area.

7. PLANNING AND LAND USE PROPOSALS

- 7.1 On the Plan, the Area has been zoned "R(A)8" and the Notes of the Plan indicated broadly the intended land use within the Area.

Uses

- 7.2 Development or redevelopment within the "R(A)8" zone is intended for high-density residential development with commercial uses on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.
- 7.3 The maximum plot ratio within the "R(A)8" zone is 9.0, or the plot ratio of the existing building(s), whichever is the greater. Except where the plot ratio is permitted to be exceeded under the Notes of the Plan or under Building (Planning) Regulations 22(1) or (2), under no circumstances shall the plot ratio for the domestic part of any development exceed 7.5.
- 7.4 To provide design flexibility, minor relaxation of the building height and plot ratio restrictions may be considered by the Board on application under section 16 of the Ordinance taking into account its individual planning and design merits.

Car Park

- 7.5 An underground car park will be provided within the Development Scheme. It will accommodate car parking spaces and loading/unloading bays ancillary to developments and/or

redevelopments commenced by URA within the Wider Area to meet the parking needs and avoid car park run in/outs at individual developments in the Wider Area. Coach parking spaces will also be provided to cater for the demand for coach parking in the district.

- 7.6 Three URA Development Projects are located within the Wider Area, i.e., Kai Ming Street (DL-8:KC), Hung Fook Street/Kai Ming Street (KC-011) and Wing Kwong Street (KC-012) to the immediate south of the Development Scheme. KC-011 and KC-012 are commenced by the URA under section 26 of the URAO on the same day as the Development Scheme (Plan 2). Limited on-site loading/unloading bays will be provided at each of the Development Projects while car parking spaces will not be provided. The underground car park within the Development Scheme will accommodate the remaining number of loading/unloading bays and car parking spaces provision for the three Development Projects.
- 7.7 The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by the Government and in any event there shall be not more than 274 car park spaces, 20 loading/unloading bays and 4 coach parking spaces. The maximum number of car park space and loading/unloading bay provisions are based on the relevant requirements under the Hong Kong Planning Standards and Guidelines with reference to the total gross floor area (GFA) of the Development Scheme, developments and/or redevelopments commenced by URA within the Wider Area; i.e. about 104,000m² GFA and about 2,000 residential units. The number of coach parking spaces is as required by the Government to cater for the need for coach parking in the district.
- 7.8 Among the sites within the Wider Area to be served by the underground car park, the site area of the Development Scheme is relatively large and more efficient car park layout can be achieved. Provision of a single underground car park in the Development Scheme avoids disruption to through traffic movements and pedestrians while maintaining continuous street frontages on other sites in the Wider Area.

- 7.9 The underground car park is for accommodating car parking and loading/unloading bays ancillary to the development and/or redevelopment commenced by URA within the Wider Area and the coach parking spaces are to cater for such need in the district. As such, in determining the maximum plot ratio for the Development Scheme, any floor space that is constructed or intended for the use solely as the underground car park, may be disregarded. Management of the underground car park will be arranged at the development stage.

Local Road Improvement

- 7.10 Through road connecting Hung Fook Street and Ngan Hon Street within the Development Scheme, which in turn forms part of the proposed road network extending from Wan On Street of URA Bailey Street/Wing Kwong Street Development Project KC-009 (commenced on 4 March 2016), URA Hung Fook Street/Kai Ming Street Development Project KC-011 and Wing Kwong Street Development Project KC-012 (commenced on 3 June 2016), will improve the accessibility of the vicinity.
- 7.11 **Plan 2** shows the proposed road network system extending from Wan On Street to Ngan Hon Street. It passes through the sites of the Development Scheme and other redevelopment projects KC-009, KC-011 and KC-012. Except a road section of Wan On Street between Wing Kwong Street and Bailey Street is two-way carriageway, the road section between Wing Kwong Street and Ngan Hon Street is a one-way carriageway.
- 7.12 With the proposed road network system, egress traffic from Wing Kwong Street heading to Hung Hom Road and the east direction can exit directly via Wan On Street. It minimizes the detouring of the traffic at Ma Tau Wai Road/Wing Kwong Street junction and Ma Tau Wai Road/Bailey Street junction.
- 7.13 Ingress traffic from Bailey Street to Ngan Hon Street could take a route through the proposed network system from Wan On Street via its extension instead of detouring via Bailey Street eastbound, Sung On

Street northbound, Lok Shan Road Westbound, and To Kwa Wan Road southbound.

- 7.14 Extension of Wan On Street facilitates egress traffic to the north via Lung Tak Street to Chi Kiang Street instead of detouring via Bailey Street eastbound and Sung On Street northbound.
- 7.15 A section of extension of Wan On Street between Wing Kwong Street and Ngan Hon Street involves resumption of land and is implemented by Chapter 124 Lands Resumption Ordinance. URA will make an application for resumption within 12 months after the Development Scheme Plan prepared under the Town Planning Ordinance (TPO) has been approved by the Chief Executive in Council in accordance with section 9 of the Ordinance.

8. IMPLEMENTATION OF THE SCHEME

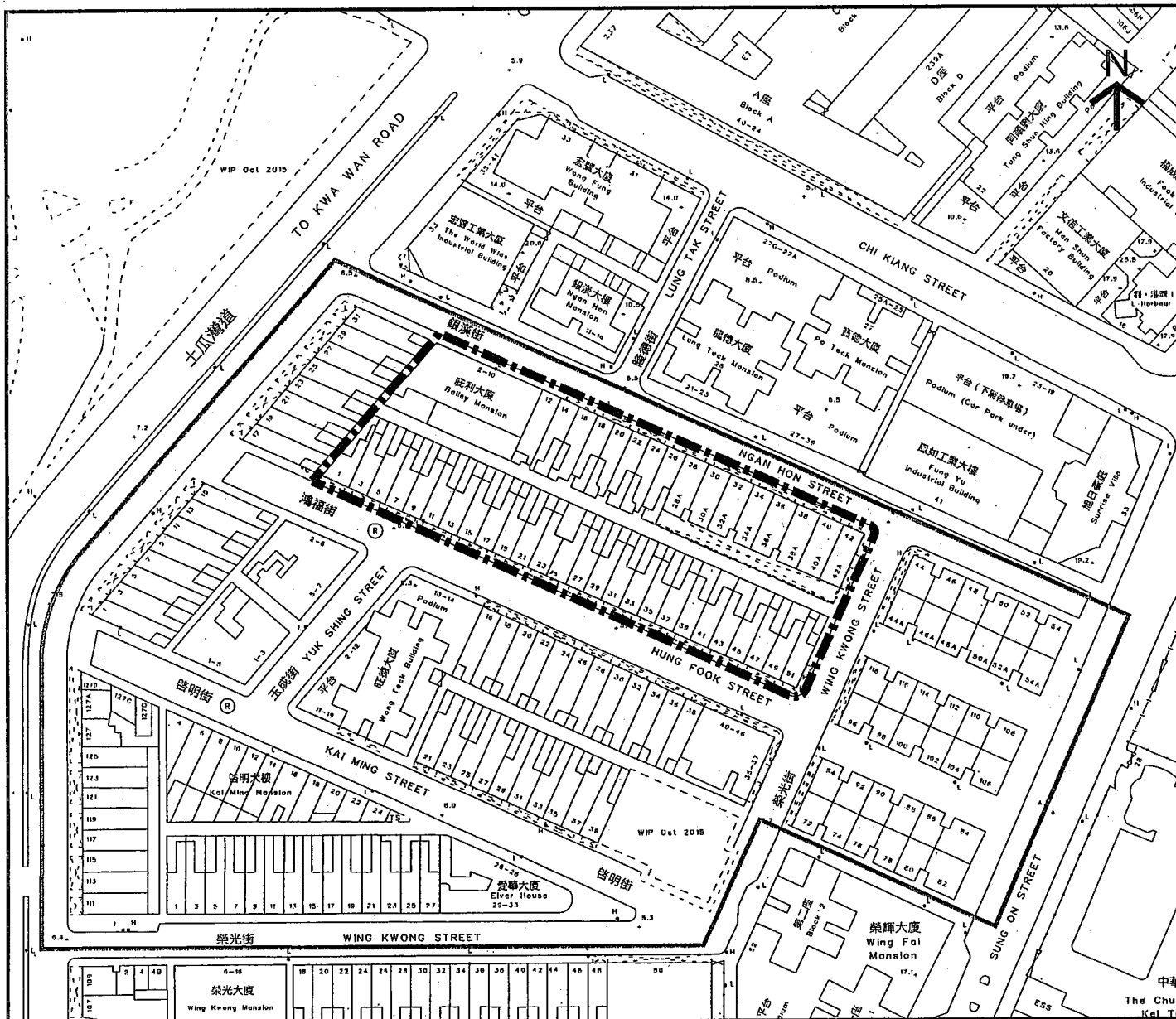
- 8.1 The proposals set out in the Plan form an integral part of the Development Scheme for the Area.
- 8.2 The URA does not own or lease any land within, the boundaries of the Development Scheme. The URA intends to acquire the properties within the Area of the Development Scheme. With respect to any of such properties which cannot be acquired by purchase, the Secretary for Development would consider, upon the application of the URA, recommending to the Chief Executive in Council the resumption of properties under the Lands Resumption Ordinance, if necessary.
- 8.3 All eligible tenants will be offered an ex-gratia payment package in accordance with URA's policy. The URA has already entered into agreement with the Hong Kong Housing Society (HKHS) and the Hong Kong Housing Authority (HKHA) for the purpose of making available rehousing units by HKHS or HKHA to rehouse affected tenants who satisfy the eligibility criteria of HKHS or HKHA.
- 8.4 Non-domestic tenants of properties acquired by URA whose tenancies are terminated by URA due to implementation of the Development

Scheme may be offered an ex-gratia allowance to assist in their business relocation.

- 8.5 The URA may implement the Development Scheme on its own or in association with one or more joint venture partners.

TOWN PLANNING BOARD

XXXX 2016



圖例 NOTATION

BOUNDARY OF HUNG FOOK STREET/
NGAN HON STREET DEVELOPMENT
SCHEME

BOUNDARY OF WIDER AREA

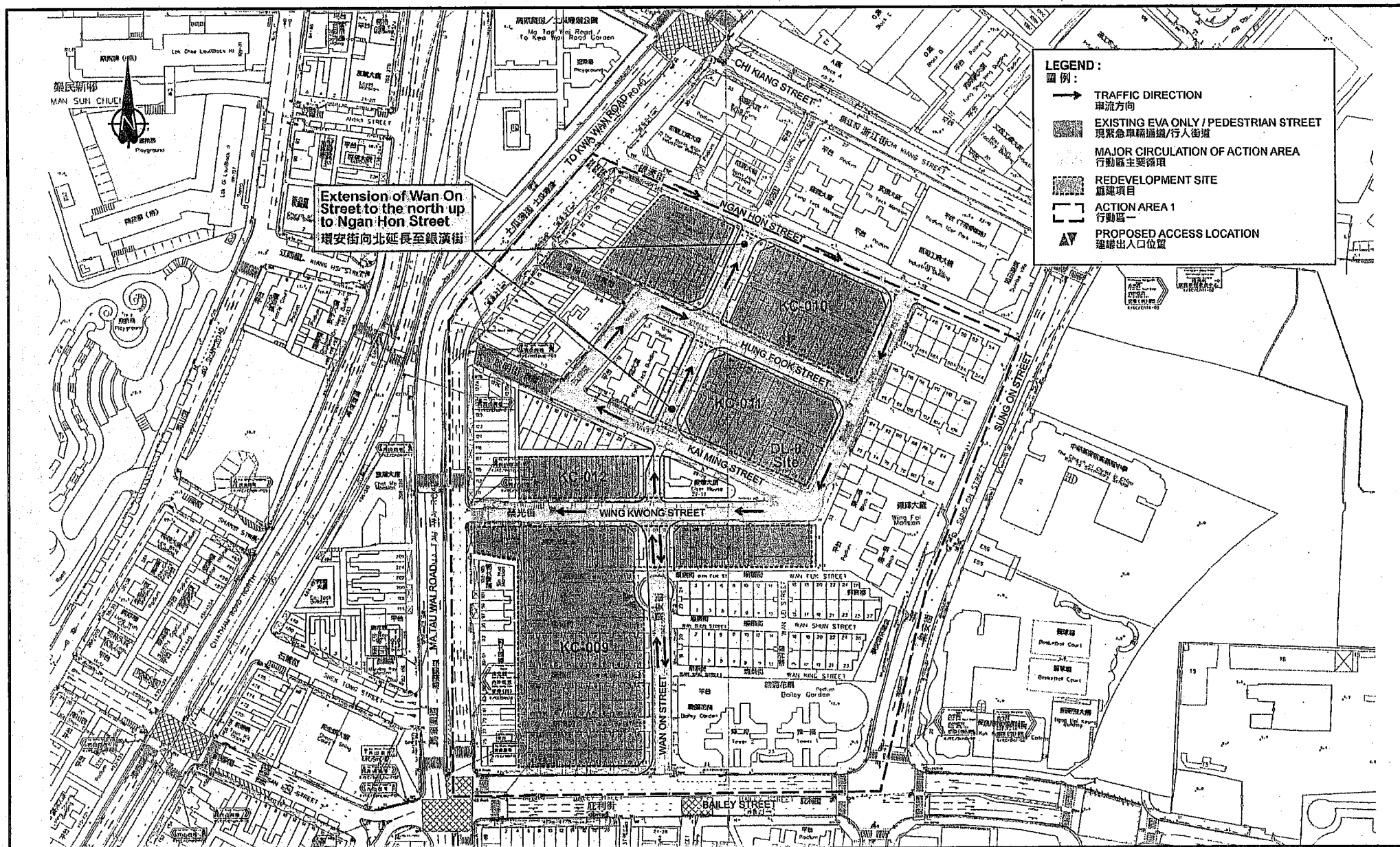
鴻福街/銀漢街
發展計劃範圍界線

周邊範圍界線

發展計劃內地下停車場提供服務的周邊範圍
WIDER AREA SERVED BY UNDERGROUND CAR PARK IN THE
DEVELOPMENT SCHEME



圖則編號
PLAN No. 1



PROPOSED TRAFFIC NETWORK 擬議交通網絡
HUNG FOOK STREET / NGAN HON STREET DEVELOPMENT SCHEME 鴻福街/銀漢街發展計劃



Our Ref: PDP/KC-010

14 September 2016

By Fax (2894 9502) & By Post

District Planning Officer/ Kowloon
14/F North Point Government Offices
333 Java Road, North Point,
Hong Kong

(Attn.: Ms. Johanna Cheng)

Dear Ms. Cheng,

Urban Renewal Authority
Hung Fook Street /Ngan Hon Street Development Scheme (KC-010)
Responses to Follow up Comments from Government Departments

I refer to our submission of the captioned draft Development Scheme Plan (the Scheme) dated 10 June 2016, the follow up comments from Government departments, i.e. BD, LandsD (UR), LandsD/KW, CTP/UD&L, PlanD, DSD, and TD via your emails dated 19 August, 6 September and 8 September 2016. I would like to enclose our responses to comments (R to C) (see Attachment A) for your necessary action.

Thank you for your kind attention.

Yours sincerely,

Mike Kwan
Assistant General Manager
Planning & Design

Encl.

RT/MK/ML



URA Hung Fook Street/ Ngan Hon Street Development Scheme (KC-010)

Responses to Follow up Comments from Government Departments on Draft DSP submission

Government Department	Comments	URA Responses
Buildings Department	1. I would like to clarify that as the land status of the said lane is government land, no resumption of the said lane would be required.	Noted.
Lands Department (Urban Renewal) (Contact officer: Mr Ming Tsang 2854 2167)	<p>2. It is noted that the underground car park is proposed underneath the new through road. The detailed arrangement for the new through road and the future lot boundary shall be subject to further discussion and be governed by lease conditions to be agreed by the Government.</p> <p>4(ii)(a) I would like to reiterate that there is no guarantee in the future land grant that the additional parking/ loading/ unloading provisions would be exempted from GFA calculation. Communal car park may be regarded as commercial car park and is GFA accountable.</p> <p>4(ii)(b) Provision of parking/ loading/ unloading facilities on this site for the other URA projects within the area as specified in the</p>	<p>Noted.</p> <p>The communal car park is neither a commercial car park nor a public car park. In the land grant stage, URA can provide all the exact sites served by the communal car park. As the Grantee of the land will be URA, the Authority trust that management of the communal car park is enforceable. As stated in the Notes of the Development Scheme Plan, any floor space that is constructed or intended for the use solely as the underground car park is GFA non-accountable.</p> <p>The requirement of the provision of parking/ loading/ unloading facilities for the sites commenced by URA within the area as specified in the DSP can be laid</p>

Government Department	Comments	URA Responses
	<p>DSP, e.g. KC-011, KC-012, DL-8:KC, etc may not be feasible and will create future management and lease enforcement problem. It shall be subject to the agreement of relevant government departments and approval of District Land Conference.</p> <p>4(iii)&(iv) Refer to point 4(ii)(a).</p>	<p>down in the Special Clause. URA as the Grantee of all the sites can enforce the management of the communal car park through Development Agreement and management company which the Authority trust enforceable.</p> <p>The communal car park serves the sites commenced by URA within the area as specific in the DSP shall be GFA non-accountable which has been explicitly stated in the Notes of the DSP.</p>
<p>Lands Department DLO/KW (Contact officer: Ms Sandy Sin 2300 1739)</p>	<p>In my previous comments, I have raised the request for clarification on the mechanism by which the parking spaces, loading and unloading facilities etc. within the communal carpark can serve the "Wider Area", and whether they would be assigned or let to the flat owners etc. According to the proposed parking arrangement put up by URA, the communal carpark to be provided within one single lot but to serve a number of many other independent private lots would be a disparity from the usual land grants. The additional 164 parking spaces and 8 loading and unloading spaces for the "Wider Area" can by no means be regarded as "ancillary" to the "lot" to be granted and therefore relevant departments like TD, PlanD, and BD should give due consideration if GFA exemption can be allowed. If URA is prepared to hold the communal carpark as a whole and let the parking spaces to the public just like in commercial</p>	<p>URA will deal with the land grant issue which is handled by the URS.</p> <p>The communal car park is neither commercial nor public car park. The intention of the communal car park is to serve the sites commenced by URA within the area as specified in the DSP. URA will confirm all the exact sites served by it in the land grant stage. As URA will be the Grantee of the land, management arrangement of the communal car park be dealt with by Special Clause and Development Agreement to which the Authority trusts enforceable.</p>

Government Department	Comments	URA Responses
	<p>public carparks, this office will have no adverse comment from lease enforcement perspective subject to the future land grant to reflect the same. Unless clarification from URA on its intention and its proposed mechanism for the shared use is given and it is considered enforceable under lease, URA may need to explore other administrative/ statutory means for enforcing the communal carpark.</p> <p>The future land grant would be handled by URS. I believe that it will offer its comments.</p>	
<p>Planning Department (Contact officer: Miss Carmen Chan 2231 4840)</p>	<p>It should be noted that the consultant should decide whether they will revise/re-submit the AVA by providing basic information/ observation and discussion considering the nature and scale of development. If the consultant is to proceed within the detailed technical assessment on air ventilation, the relevant part of the Planning Statement should be suitably revised to properly address our comments (para. 2 regarding the PS on air ventilation aspect as stated in the R-to-C refers.) If not, the consultant is still required to provide a brief appraisal to supersede the detailed assessment by providing discussion on the pedestrian wind environment and whether the proposed development will likely have a significant impact on the pedestrian environment. The 'R-to-C' is not clear in this regard.</p>	<p>Under annual winds, prevailing winds passing through the Hoi Sham Park and open spaces will penetrate into the Subject Site. During the summer period, since To Kwa Wan Typhoon Shelter is located to the east and southeast of the area, it is considered that the sea breeze from east to southeast direction would dominate of the area during summer period. As the proposed development is located immediate next to the existing compact medium to high-rise buildings, it does not encroach into any air paths of the area and the wind speed for the annual and summer prevailing winds would not be reduced. Therefore, it is concluded that the proposed development would not have any significant impact on the pedestrian wind environment."</p>

Government Department	Comments	URA Responses
Drainage Services Department (contact officer : Mr. C. L. WONG at Tel. No. 2300 1573)	<p>For the Sewerage Impact Assessment (SIA), URA pointed out the SIA at this preliminary stage is to assess the potential sewerage impacts based on the available sewerage data and assumptions. Given there are issues of missing / doubtful data and the validity of assumption used in hydraulic capacity calculation, the findings of the SIA are not conclusive. I would like to remark that local sewerage upgrading works might be required.</p>	<p>The conclusion of the SIA is amended as follows:</p> <p>Para. 3.6.5 In the Planning stage assessment, no adverse sewerage impact is anticipated. However, the Sewerage Impact Assessment may be updated during the detailed design stage (if required) and upgrading or improvement works may be required later on.</p> <p>Para. 5.1.3 Based on the sewerage impact assessment results, it is found that the capacity of the existing sewerage system serving the area would be sufficient to cater for the sewage generation from the proposed development. In this planning stage assessment, no adverse sewerage impact is anticipated. However, the Sewerage Impact Assessment may be updated during the detailed design stage (if required) and upgrading or improvement works may be required later on.</p>
Transport Department (contact officer : Ms. Joyce LEE at Tel. No. 2399 2504)	<p>(a) I have no adverse comment on the reprovision of the affected parking spaces, subject to the results of local consultation. In case local objection to the proposed relocation of the metered parking spaces is received, you are required to identify other suitable relocation sites for our consideration.</p> <p>(b) Suitable improvement works at the junction of To Kwa Wan Road and Ngan Hon Street</p>	<p>Noted</p> <p>Improvement works at the junction of To Kwa Road and Ngan Hon Street involves the area which is outside the</p>

Government Department	Comments	URA Responses
	<p>should be considered to allow long vehicles, in particular fire engine, accessing the proposed development via Ngan Hon Street without making a detour.</p> <p>(c) The swept path analysis shows that long vehicles at the junction of Ma Tau Wai Road and Wing Kwong Street would encroach onto the adjacent traffic lanes when turning into Ma Tau Wai Road. Please consider widening this junction to provide sufficient turning area.</p>	<p>boundary of KC-010 Development Scheme. URA has no jurisdiction to conduct the road works. Based on the existing road layout of To Kwan Wan Road/Ngan Hon Street, vehicles of length up to 8m can make left turn from the nearside lane of To Kwa Wan Road southbound to Ngan Hon Street and long vehicle like fire engine can still make the turn using the middle lane of To Kwa Wan Road practically. Upon the completion of the extension of Wan On Street upto Ngan Hon Street, long vehicle can access the Ngan Hon Street/Wing Kwong Street area using the new route, via Bailey Street and Wan On Street.</p> <p>The junction of Ma Tau Wai Road / Wing Kwong Street is currently operating with traffic signal. Vehicles turning from the only lane at Wing Kwong Street to Ma Tau Kok Road have full right of way at green signal and no conflicting movements when turning.</p>

☐ Urgent ☐ Return receipt ☐ Sign ☐ Encrypt ☐ Mark Subject Restricted ☐ Expand groups



To:
Cc:
Bcc:

Subject: Fw: Urban Renewal Authority (URA) Hung Fook Street / Ngan Hon Street, Development
Scheme Plan No. S/K9/URA2/A - Response to Comments (EPD comments)

----- Forwarded by Barry YS YAN/PLAND/HKSARG on 29/09/2016 17:00 -----

From: "Law, Matthew" <MTWLaw@ura.org.hk>
To: "bysyan@pland.gov.hk" <bysyan@pland.gov.hk>,
Cc: "jwycheng@pland.gov.hk" <jwycheng@pland.gov.hk>, "Kwan, Mike" <MYFKwan@ura.org.hk>, "Tang, Roger" <RMHTang@ura.org.hk>
Date: 29/09/2016 14:23
Subject: RE: Urban Renewal Authority (URA) Hung Fook Street / Ngan Hon Street, Development
Scheme Plan No. S/K9/URA2/A - Response to Comments (EPD comments)

Dear Barry,

I refer to your email dated 27 and 28 September 2016 regarding comments of EPD and DSD on URA project KC-010.

Attached please find the table for our response to comments, files "Attached A for Environmental Assessment" and "Attachment B for Sewage Impact Assessment" for your follow up.

Regards,

Matthew Law
Planning and Design Division
Tel. 2588 2176



Response to departmental comments EPD_DSD 20160929.pdf



Attachment A for Environmental Assessment.pdf



Attachment B for Sewage Impact Assessment.pdf

URA Hung Fook Street/ Ngan Hon Street Development Scheme (KC-010)

Responses to Follow up Comments from Government Departments on Draft DSP submission

Government Department	Comments	URA Responses
<p>Environmental Protection Department (Contact officer : Mr. Leo Luk 2835 1096)</p>	<p>Environmental Assessment Report</p> <p><u>S.2 Noise Impact Assessment</u></p> <p>(A) General Comments</p> <p>1. The consultant should have explored more effective measures, e.g. various types of acoustic windows, and enhanced acoustic balcony, to further minimize the traffic noise impact.</p> <p>2. The building information provided in this report is insufficient. Therefore, we could not check some of the data in the traffic noise model.</p> <p>3. Our cursory check found that the fixed source noise assessment may contain errors, e.g. the calculations for NSR NC-E1.</p> <p>(B) Specific Comments</p> <p>1. <u>S.2.3.3.1 and Appendix 2.1</u> - As we mentioned in our previous comments, the Consultant should obtain the TD's endorsement on the traffic data and posted speed limits, and the HyD's endorsement on the types of road surfaces.</p>	<p>Mitigation measures for effective minimizing traffic noise impact such as change in the length of architectural fins or building layout/orientation etc. might be explored if required.</p> <p>Section drawing is provided for additional information. Please refer to revised Appendix 1.1.</p> <p>Errors in fixed source noise assessment have been rectified. Please refer to revised Appendix 2.6.</p> <p>Endorsement from TD & HyD shall be provided once available.</p>

Government Department	Comments	URA Responses
	<p>2. <u>Appendix 1.1</u></p> <ul style="list-style-type: none"> - There should be a scale. <p>3. <u>S.2.3.5, S.2.3.5.1e and Figure 2.2</u></p> <ul style="list-style-type: none"> - S.2.3.5.1e: We are skeptical about the statement "Further increase of fins length on specific location is not feasible due to the nearby road carriageways." in the 3rd sentence. - Figure 2.2: The Consultant should have highlighted all the noise mitigation measures mentioned in S.2.3.5. <p>4. <u>Appendices 2.2 and 2.3</u></p> <ul style="list-style-type: none"> - There should be a unit, dB(A), and a noise descriptor, L₁₀ (1 hour), for the sound pressure levels. <p>5. <u>S.2.3.7</u></p> <ul style="list-style-type: none"> - The report has introduced various design constraints under the Building Regulations. The consultant should have included confirmation of the relevant constraints by an authorized person. - Please refer to my above comment (A)(1). <p>6. <u>S.6.1.2</u></p> <ul style="list-style-type: none"> - The Consultant should have included the conclusion on the assessment of noise impact from fixed sources in this section. 	<p>Scale is added in the revised Appendix 1.1.</p> <p>URA might consider to increase the lengths of the fins at detailed design stage subject to the approval of Building Authority.</p> <p>Noise mitigation measures including proposed vertical fins and acoustic balconies are highlighted in revised Figure 2.2.</p> <p>Please refer to revised Appendices 2.2. and 2.3 for the amendment.</p> <p>Relevant constraints will be confirmed by authorized person when General Building Plan is submitted. Please refer to response for (A)(1) above.</p> <p>S.6.1.2 is revised as "Potential road traffic noise and fixed noise impact for the proposed development have been assessed. It is concluded that with noise</p>

Government Department	Comments	URA Responses
	<p>7. <u>S.2.4.3.1</u> - Our cursory check found that there may be more fixed noise sources such as garages in the vicinity.</p> <p>8. <u>Table 2.1</u> - As we mentioned in our previous comments, the Consultant should have made clear that the determined ASR is for assessment purpose only and should not prejudice the Authority's discretion on the enforcement based on the contemporary conditions.</p>	<p>mitigation measures such as disposition and orientation of the building towers and vertical finds provided, the proposed redevelopment will not be subject to adverse traffic noise impact. According to the fixed noise impact assessment results, the proposed development will not be subject to adverse noise impact from fixed noise sources.”</p> <p>There is a URA redevelopment project, Hung Fook Street/ Kai Ming Street (KC-011), opposite to the subject development site along Hung Fook Street which was commenced on the same date of the subject site. The current building with concerned fixed noise sources in KC-011 will be demolished before completion of development of the subject site. Therefore, potential fixed noise sources, such as garages along Hung Fook Street shall not be considered in the Fixed Noise Impact Assessment.</p> <p>Please refer to revised Table 2.1 and s.2.4.4.1 for the confirmation of the signed ASRs are for assessment purposed only and should not prejudice the Authority’s discretion on the enforcement based on the contemporary conditions.</p>

Government Department	Comments	URA Responses
	<p>9. <u>S.2.4.4.2</u> - The Consultant should have advised the fixed noise sources within the development would be designed and installed to meet the relevant noise criteria in the HKPSG.</p> <p>10. <u>S.2.4.5.2</u> - In general, hammering cannot be avoided in garages. We are skeptical that there would be no impulsive noise from all the fixed noise sources.</p>	<p>As mentioned in S.2.4.4.2, potential fixed noise source, such as chiller and cooling tower, within the development are proposed in an enclosed indoor environment. Meanwhile, inlet/outlet of the system would be equipped with acoustic louvre to eliminate any potential fixed noise impact from the system to the proposed redevelopment project or surrounding NSRs. Therefore, no potential fixed noise is anticipated to the surrounding NSRs.</p> <p>Reference has been made to “Noise Impact Assessment for Proposed Comprehensive Residential Development with “Shop and Services” and “Eating Place” in “Comprehensive Development Area (2)” Zone at KIL 10578, 7628 and 7626 in Ma Tau Kok” Report (abbreviated as NIA-MTK Report).</p> <p>It is understood that hammering cannot be avoided in garages. However, the SWL of 91.7dB used in calculation of Fixed Noise Impact Assessment had already captured all of the operation activities from the motor service workshop, such as Hand-held Pneumatic Tools, Vehicle lifting machine, hammering and grinding etc., according to the NIA-MTK Report. Therefore, hammering from the concerned garages had been already counted in the assessment.</p> <p>Please refer to the revised S. 2.4.2.4 and S.2.4.2.5.</p>

Government Department	Comments	URA Responses
	<p>11. <u>Appendix 2.6</u></p> <ul style="list-style-type: none"> - Please refer to my above comment (A)(3). <p>12. <u>Traffic Noise Assessment</u></p> <ul style="list-style-type: none"> - The Consultant should have justified why they have adopted the AM traffic data for the assessment. - The traffic noise assessment contains errors. For example, a podium on the southeast side of the project has not been included. <p><u>S.4&5 Waste Management & Land Contamination Assessment</u></p> <p>1. <u>S.4, Waste Management</u></p> <ul style="list-style-type: none"> - Please provide the estimated quantities, respective handling and disposal arrangement of different streams of wastes (e.g. inert C&D materials, non-inert C&D materials, chemical wastes, general refuse, any excavated sediment and asbestos wastes, etc.) to be generated from the Project according to relevant legislation and guidelines. 	<p>Errors in fixed noise source assessment have been rectified. Please refer to the revised Appendix 2.6.</p> <p>Both AM and PM traffic data for the assessment has been provided in the submitted report in Appendix 2.2.</p> <p>The podium on the southeast of the project is included in the noise assessment model. Please refer to Appendix 2.2. for the updated result for traffic noise assessment.</p> <p>Please refer to the revised S.4 for the details of the estimated quantities, respective handling and disposal arrangement of different streams of wastes.</p>

Government Department	Comments	URA Responses
	<p>2. <u>S.5.3, Site Inspection and Observation</u></p> <ul style="list-style-type: none"> - Please confirm the existing uses indicated in the photo records of Figure 5 are updated as per site visits conducted in 2016. - Please provide the site walkover checklist, site visit photos, replies from the authorities (e.g. FSD and EPD) and identify all potentially contaminating activities (e.g. any waste recycling workshop, metal hardware processing workshop) in the Project site. <p>3. <u>S.5.3, Discussion</u></p> <ul style="list-style-type: none"> - Please confirm if further site appraisal will be conducted for the whole site to be developed. For avoidance of doubt, please state in relevant sections (e.g. S.5.3.11 and S.6.1.5) that the land contamination assessment and/or remediation works will be completed before commencement of any construction works for the development. 	<p>It is confirmed that the existing used indicated in the photo records of Figure 5 are as per site visit conducted in 2016.</p> <p>Site walkover checklist, site visit photos, etc. will be provided at the detail design stage.</p> <p>The subject site is situated on reclaimed land in 1920s. No significant land contamination can be identified. Contamination assessment might need to carry out at detailed stage if required.</p>
	<p>Sewage Impact Assessment Report</p> <p>1. <u>Appendix B, Table 2</u></p> <ul style="list-style-type: none"> - The gradients of the Segment S1-S2 and S2-S3 are incorrect. <p>2. The capacities of the Segments S1-S2 and S2-S3 are incorrect in Table 2 and Table 4 of Appendix B due to incorrect gradients of these 2 segments as mentioned</p>	<p>Please refer to the revised Appendix B, Table 2 for the updated data.</p> <p>Same as above.</p>

Government Department	Comments	URA Responses
	<p>in comment 1 above, please amend.</p> <p>3. <u>Appendix B, Table 4</u> - It is noticed that segment S6-S7 is missing.</p> <p>4. <u>Para. 3.4.3</u> - The no. of employees for the retail shops and residential clubhouse does not tally with the no. of employees in the total non-domestic area (including clubhouse) in Table 1, please clarify.</p> <p>5. <u>Para. 3.6.2</u> - Manhole S3 is FMH4024357, not FMH2024357.</p>	<p>Please refer to the revised Appendix B, Table 4 for the updated data.</p> <p>The total number of employees for the retail shop and residential clubhouse in S.3.4.3. Should be 274.</p> <p>Noted and amended accordingly.</p>
Drainage Services Department	<p>(a) Para 3.6.5 should be amended as "In the Planning stage assessment, no significant sewerage impact is anticipated. However, the Sewerage Impact Assessment has to be updated during the detailed design stage and upgrading or improvement works may be required later on."</p> <p>(b) For Para 5.1.3 should be amended as "Based on sewerage impact assessment results, it is found that the capacity of the existing sewerage system serving the area would likely be sufficient to cater for the sewage generation from the proposed development. In this planning stage assessment, no significant</p>	<p>Para 3.6.5 is amended as "In the Planning stage assessment, no significant sewerage impact is anticipated. URA shall update the Sewerage Impact Assessment during the detailed design stage if necessary and upgrading or improvement works may be required later on."</p> <p>Para 5.1.3 is amended as "Based on sewerage impact assessment results, it is found that the capacity of the existing sewerage system serving the area would likely be sufficient to cater for sewage generation from the proposed development. In this planning stage assessment, no significant sewerage impact is anticipated. URA shall update the Sewerage Impact Assessment during the</p>

Government Department	Comments	URA Responses
	sewerage impact is anticipated. However, the Sewerage Impact Assessment has to be updated during the detailed design stage and upgrading or improvement works may be required later on."	detailed stage if necessary and upgrading or improvement works may be required later on."

APPENDIX 2.2: RESULTS OF ROAD TRAFFIC NOISE IMPACT ASSESSMENT

Predicted Road Traffic Noise (L_{NI}+100) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Unmitigated Scenario (A9)

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-I2	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	27.55	58	61	65	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55
2/F	30.70	60	62	65	66	66	66	66	66	66	66	66	66	66	67	67	67	66	65	64	62	61	58	57	57	57	57
3/F	33.85	62	63	66	66	66	66	66	66	66	66	66	66	67	67	68	67	67	66	65	63	62	60	60	60	61	61
4/F	37.00	65	65	66	66	66	66	66	66	66	66	66	67	67	67	67	68	68	67	66	65	63	62	60	64	64	65
5/F	40.15	69	68	67	67	67	67	67	66	66	67	65	67	67	67	67	68	68	67	67	67	67	67	67	68	68	70
6/F	43.30	72	71	68	68	68	67	67	67	67	67	67	67	67	67	69	69	69	69	69	69	69	69	69	69	69	70
7/F	46.45	75	74	70	70	69	68	68	68	67	67	67	67	67	67	69	69	70	70	70	70	70	70	70	70	70	70
8/F	49.60	78	77	70	69	68	68	68	68	68	68	67	67	67	67	70	70	70	70	70	70	70	70	70	70	70	70
9/F	52.75	81	80	71	70	70	69	69	68	68	68	68	68	68	70	70	70	70	70	70	70	70	70	70	70	70	70
10/F	55.90	84	83	72	71	70	70	69	69	69	68	68	68	68	70	70	70	70	70	70	70	70	70	70	70	70	70
11/F	59.05	87	86	74	73	72	71	70	70	70	69	69	69	68	68	68	68	68	68	68	68	68	68	68	68	68	68
12/F	62.20	90	89	77	76	75	74	73	72	71	70	70	70	69	69	69	69	69	69	69	69	69	69	69	69	69	69
13/F	65.35	93	92	80	79	78	77	76	75	74	73	72	71	70	70	69	69	69	69	69	69	69	69	69	69	69	69
14/F	68.50	96	95	83	82	81	80	79	78	77	76	75	74	73	72	71	70	70	69	69	69	69	69	69	69	69	69
15/F	71.65	99	98	86	85	84	83	82	81	80	79	78	77	76	75	74	73	72	71	70	70	69	69	69	69	69	69
16/F	74.80	102	101	89	88	87	86	85	84	83	82	81	80	79	78	77	76	75	74	73	72	71	70	70	69	69	69
17/F	77.95	105	104	92	91	90	89	88	87	86	85	84	83	82	81	80	79	78	77	76	75	74	73	72	71	70	69
18/F	81.10	108	107	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82	81	80	79	78	77	76	75	74	73
19/F	84.25	111	110	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82	81	80	79	78	77	76
20/F	87.40	114	113	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82	81	80	79
21/F	90.55	117	116	105	104	103	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82
22/F	93.70	120	119	108	107	106	105	104	103	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85
23/F	96.85	123	122	110	109	108	107	106	105	104	103	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87
Exceedance		18	18	16	15	14	13	12	11	11	10	9	8	3	13	14	15	15	16	16	16	17	17	17	18	18	18
No. of Exceedance		18	18	16	15	14	13	12	11	11	10	9	8	3	13	14	15	15	16	16	16	17	17	17	18	18	18
No. of Premises		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-I2	NA-J1	NA-J2	NA-K1	NA-K2	NA-L1	NA-L2
1/F	27.55	60	65	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
2/F	30.70	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
3/F	33.85	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
4/F	37.00	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
5/F	40.15	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
6/F	43.30	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
7/F	46.45	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
8/F	49.60	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
9/F	52.75	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
10/F	55.90	66	66	66	65	65	65	65	65	65	65	65	66	67	67	66	65	64	63	61	60	56	55	55	55	55	55
11/F	59.05	66	67	67	66	66	66	66	66	66	66	66	67	67	67	66	65	64	63	61	60	56	55	55	55	55	55
12/F	62.20	67	67	67	66	66	66	66	66	66	66	66	67	67	67	66	65	64	63	61	60	56	55	55	55	55	55
13/F	65.35	67	67	67	66	66	66	66	66	66	66	66	67	67	67	66	65	64	63	61	60	56	55	55	55	55	55
14/F	68.50	68	68	68	67	67	67	67	67	67	67	67	68	68	68	67	66	65	64	63	61	60	56	55	55	55	55
15/F	71.65	68	68	68	68	68	68	68	68	68	68	68	69	69	69	68	67	66	65	64	63	61	60	56	55	55	55
16/F	74.80	68	68	68	68	68	68	68	68	68	68	68	69	69	69	69	68	67	66	65	64	63	61	60	56	55	55
17/F	77.95	69	69	69	69	69	69	69	69	69	69	69	70	70	70	69	68	67	66	65	64	63	61	60	56	55	55
18/F	81.10	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
19/F	84.25	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
20/F	87.40	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
21/F	90.55	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
22/F	93.70	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
23/F	96.85	69	69	69	69	69	69	69	69	69	69	69	70	70	70	70	69	68	67	66	65	64	63	61	60	56	55
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of Premises		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-A4	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E3	NA-D4	NA-E1	NA-E2	NA-E3	NA-F1	NA-F2	NA-G1	NA-H1	NA-I1	NA-I2	NA-J1	NA-J2	NA-K3
1/F	27.55	67	69	70	70	70	70	70	70	70	70	69	69	69	69	67	61	60	59	58	57	52	61	61	62
2/F	30.70	67	69	70	70	69	69	69	69	69	69	69	69	69	69	67	62	61	60	59	58	55	62	62	63
3/F	33.85	67	68	69	69	69	69	69	69	69	69	69	69	69	68	67	63	62	61	59	58	56	63	63	63
4/F	37.00	67	68	69	69	69	69	69	69	69	69	69	68	68	68	67	63	63	62	60	59	56	64	63	64
5/F	40.15	67	68	69	68	68	68	68	68	69	69	69	68	68	68	67	64	63	62	60	60	57	64	64	65
6/F	43.30	67	68	68	68	68	68	68	68	68	68	68	68	68	68	66	64	63	61	60	57	64	64	64	65
7/F	46.45	67	67	68	68	68	68	68	68	68	69	67	68	68	68	66	64	64	63	62	61	57	64	64	66
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	62	61	58	65	65	66
9/F	52.75	67	68	68	68	68	68	68	68	68	68	67	67	67	67	66	64	64	63	62	58	65	65	66	66
10/F	55.90	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	63	62	58	65	65	66	66
11/F	59.05	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	63	63	59	66	66	66	66
12/F	62.20	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	63	63	59	66	66	67	67
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	59	67	67	67	67
14/F	68.50	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	67
15/F	71.65	68	68	68	68	68	68	68	67	67	68	66	67	67	67	66	64	64	64	64	60	67	68	68	68
16/F	74.80	68	68	69	68	68	68	68	68	68	68	66	67	67	67	66	64	65	65	64	60	68	68	68	68
17/F	77.95	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68
18/F	81.10	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68
19/F	84.25	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	65	65	60	68	68	68
20/F	87.40	69	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	60	68	68	69
21/F	90.55	69	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	60	68	68	69
22/F	93.70	69	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	61	68	68	69
23/F	96.85	69	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	61	68	68	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		23				23		23		23		23		23		23		23		23		23		23	
No. of Exemption																									

Predicted Road Traffic Noise (L10+10vuv) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Unmitigated Scenario (PM)

Hour	mpd	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	37.55	58	62	65	65	65	65	65	65	65	65	65	65	65	65	67	66	65	64	63	61	59	56	55	55	54
2/F	30.70	60	62	65	65	65	65	65	65	65	65	65	66	66	66	67	67	66	65	64	62	61	*	58	57	57
3/F	33.85	62	63	65	66	66	66	66	66	66	66	66	66	66	66	67	67	66	66	65	63	62	60	60	60	61
4/F	37.00	65	65	66	66	66	66	66	66	66	66	66	66	66	66	66	68	68	67	66	66	65	64	63	64	65
5/F	40.15	69	68	67	67	66	66	66	66	66	66	66	66	66	66	66	68	68	68	67	67	67	67	67	68	68
6/F	43.30	72	71	68	68	67	67	67	66	66	66	66	66	66	66	66	68	68	68	68	68	69	69	69	70	71
7/F	46.45	75	75	69	69	68	68	67	67	67	67	66	66	66	66	66	66	66	69	69	70	70	70	71	72	73
8/F	49.60	78	78	73	73	70	70	69	69	68	68	67	67	67	67	67	69	69	70	70	70	70	71	71	72	73
9/F	52.75	81	81	76	76	72	72	70	69	69	68	68	67	67	67	67	70	70	70	71	71	71	71	72	72	73
10/F	55.90	84	84	78	78	74	74	71	71	70	70	69	69	68	68	68	70	71	71	71	71	71	72	72	73	73
11/F	59.05	87	87	81	81	77	77	73	73	70	70	69	69	69	68	68	71	71	71	71	71	71	72	72	73	73
12/F	62.20	90	90	84	84	80	80	76	76	72	72	71	71	70	69	69	71	71	71	71	71	71	72	72	73	73
13/F	65.35	93	93	87	87	83	83	79	79	75	75	74	73	72	71	70	69	71	71	71	71	71	72	72	73	73
14/F	68.50	96	96	90	90	86	86	82	82	78	78	77	76	75	74	73	72	71	71	71	71	71	72	72	73	73
15/F	71.65	99	99	93	93	89	89	85	85	81	81	80	79	78	77	76	75	74	73	72	71	71	72	72	73	73
16/F	74.80	102	102	96	96	92	92	88	88	84	84	83	82	81	80	79	78	77	76	75	74	73	72	72	73	73
17/F	77.95	105	105	99	99	95	95	91	91	87	87	86	85	84	83	82	81	80	79	78	77	76	75	74	73	73
18/F	81.10	108	108	102	102	98	98	94	94	90	90	89	88	87	86	85	84	83	82	81	80	79	78	77	76	75
19/F	84.25	111	111	105	105	101	101	97	97	93	93	92	91	90	89	88	87	86	85	84	83	82	81	80	79	78
20/F	87.40	114	114	108	108	104	104	100	100	96	96	95	94	93	92	91	90	89	88	87	86	85	84	83	82	81
21/F	90.55	117	117	111	111	107	107	103	103	99	99	98	97	96	95	94	93	92	91	90	89	88	87	86	85	84
22/F	93.70	120	120	114	114	110	110	106	106	102	102	101	100	99	98	97	96	95	94	93	92	91	90	89	88	87
23/F	96.85	123	123	117	117	113	113	109	109	105	105	104	103	102	101	100	99	98	97	96	95	94	93	92	91	90
Exceedance		18	18	16	15	14	13	12	11	11	10	9	8	8	0	13	14	14	15	15	16	16		17	17	18
No. of exceedance		18	18	16	15	14	13	12	11	11	10	9	8	8	0	13	14	14	15	15	16	16		17	17	18
No. of Breach		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

Flour	mpD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2
1/F	27.55	66	65	66	65	65	65	65	65	65	65	62	61	60	59	58	53	61	61	61	62	58	64	66
2/F	30.70	66	65	66	65	65	65	65	65	65	65	62	61	60	59	59	61	54	62	62	62	63	60	65
3/F	33.85	66	65	66	65	65	65	65	65	65	65	62	61	60	59	62	54	62	62	63	64	60	60	65
4/F	37.00	65	65	66	65	65	65	65	65	65	65	62	61	60	60	60	62	55	63	63	64	64	60	66
5/F	40.15	65	65	66	65	65	65	65	65	65	65	62	61	61	60	60	63	56	63	64	64	65	61	66
6/F	43.30	65	65	66	65	65	65	65	65	65	65	62	62	61	61	63	57	64	64	65	65	61	66	67
7/F	46.45	65	65	66	65	65	65	65	65	65	65	63	62	62	61	64	57	64	64	65	65	61	66	67
8/F	49.60	65	66	66	66	66	66	66	66	66	66	63	62	62	61	64	58	64	65	65	66	61	67	67
9/F	52.75	66	66	66	66	66	66	66	66	66	66	63	63	62	62	64	58	65	65	66	66	61	67	67
10/F	55.90	66	66	67	66	66	66	66	66	66	66	63	63	62	62	65	59	65	66	66	67	61	67	68
11/F	59.05	66	66	67	66	66	66	66	66	66	66	63	63	63	62	65	59	66	66	67	67	61	68	68
12/F	62.20	67	67	67	67	67	66	66	66	66	66	64	63	63	63	66	59	66	67	67	67	61	68	68
13/F	65.35	67	67	68	67	67	66	66	66	66	66	64	64	63	63	66	60	67	67	67	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	66	60	67	67	68	68	68	68	69
15/F	71.65	68	68	68	68	68	67	67	67	67	67	64	64	63	63	67	61	68	68	68	68	68	68	69
16/F	74.80	68	69	68	68	68	67	67	67	67	67	64	64	63	63	67	61	68	68	68	68	68	68	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	63	63	67	61	68	68	68	68	68	68	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	63	63	68	61	68	68	68	68	68	68	69
19/F	84.25	69	69	70	69	69	69	68	68	68	68	65	64	63	63	68	61	68	68	68	68	68	68	69
20/F	87.40	69	69	70	69	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	68	68	69
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	68	68	69

Floor	MPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-K1
1/F	27.50	67	69	69	69	70	70	70	70	70	70	70	69	69	69	69	67	62	61	60	59	57	52	61	60
2/F	30.70	67	69	69	69	69	69	69	69	69	69	69	69	69	69	69	67	62	61	60	59	58	54	62	61
3/F	33.85	67	68	69	69	69	69	69	69	69	69	69	68	68	68	68	67	63	62	60	59	59	56	63	62
4/F	37.00	67	68	69	69	69	69	69	69	69	69	69	68	68	68	68	66	63	63	61	60	59	56	63	63
4/F	40.15	67	68	69	68	68	68	68	68	68	68	68	68	68	68	68	66	63	63	62	60	59	56	64	63
4/F	43.30	67	68	68	68	68	68	68	68	68	68	68	68	68	68	66	64	63	62	61	60	57	64	64	
7/F	46.45	67	67	68	68	68	68	68	68	68	68	68	68	68	68	68	66	64	63	62	61	57	64	64	
8/F	49.60	67	67	68	68	68	68	68	68	68	68	68	68	67	67	67	66	64	63	62	61	57	64	64	
9/F	52.75	67	67	68	68	68	68	68	68	68	68	68	68	67	67	67	66	64	64	63	62	58	65	65	
10/F	55.90	67	67	68	68	68	67	67	68	67	68	67	67	67	67	67	66	64	64	63	62	58	65	66	
10/F	59.05	67	67	68	68	68	67	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	
12/F	62.20	67	67	68	68	68	67	67	67	67	67	68	66	67	67	67	66	64	64	63	63	59	66	66	
13/F	65.35	67	68	68	68	68	67	67	67	67	67	68	66	67	67	67	66	64	64	64	64	59	66	67	
14/F	68.50	68	68	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	60	67	67	
15/F	71.65	68	68	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	60	67	67	
16/F	74.80	69	68	68	68	68	68	68	68	68	68	68	66	67	67	67	66	64	64	64	64	60	67	67	
17/F	77.95	68	69	69	69	68	68	68	68	68	68	68	66	67	67	67	66	64	64	64	65	60	68	68	
18/F	81.10	69	69	69	69	68	68	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	
19/F	84.25	69	69	69	69	69	68	68	68	68	68	68	66	66	67	67	66	64	65	65	65	60	68	68	
20/F	87.40	69	69	69	69	69	68	68	68	68	68	68	66	66	67	67	66	64	65	65	65	60	68	68	
21/F	90.55	69	69	69	69	69	68	68	68	68	69	66	66	67	67	67	66	64	65	65	65	60	68	68	
22/F	93.70	69	69	69	69	69	69	69	69	69	69	66	66	67	67	67	66	64	65	65	65	60	68	68	
23/F	96.85	69	69	69	69	69	68	68	68	68	69	65	65	67	67	67	66	64	65	65	65	60	68	68	
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
No. of exceedance																									
No. of Premises		23				23		23		23		23			23		23		23		23		23		

Max. Noise Level (dB(A)) =	75
Total no. of Exceedance =	187
Total no. of Premises =	759
% Compliance =	75%

N.B. shaded cells denote noise level that will exceed limit of 70dB(A)

Predicted Road Traffic Noise (L₁₀(1-hour)) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Mitigated Scenario (AM)

Floor	TPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-H1	NA-J1	NA-J2	NA-K1	NA-K1	NA-M3	NA-M2
1/F	77.55	58	61	63	64	65	65	64	64	65	65	63	63	66	65	67	69	63	64	63	59	60	56	55	53	55
2/F	30.70	60	62	63	64	66	65	64	64	66	66	64	64	66	65	67	67	64	65	64	60	61	58	57	55	58
3/F	33.85	62	63	64	64	66	66	64	64	66	66	64	64	67	65	68	67	65	66	65	61	62	60	60	58	61
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	65	67	65	68	68	65	66	66	63	64	63	64	62	65
5/F	40.15	69	68	65	65	67	67	65	64	66	66	64	65	67	65	68	68	66	67	67	65	67	67	68	66	70
6/F	43.30	72.27	71.15	66	66	68	67	65	65	67	67	65	65	67	65	68	69	67	69	69	67	69	69	70	69	72.27
7/F	46.45	75.42	72.68	68	67	68	68	66	65	67	67	65	65	67	65	69	69	68	69	70	68	70	68	70	69	75.42
8/F	49.60	78.57	75.93	69	68	69	69	68	68	69	68	65	65	67	65	70	69	68	70	69	69	69	70	70	70	78.57
9/F	52.75	81.72	79.08	69	69	70	69	67	67	68	68	66	66	67	66	69	70	69	71.15	71.15	69	70	70	70	70	81.72
10/F	55.90	84.87	82.23	70	69	71.15	70	68	67	69	69	66	66	68	66	69	70	69	71.15	71.15	69	70	70	70	70	84.87
11/F	59.05	88.02	85.38	70	70	74.30	71.15	68	68	70	69	67	67	68	66	70	69	71.15	71.15	69	70	70	70	70	70	88.02
12/F	62.20	91.17	88.53	70	70	77.45	74.30	69	68	70	70	67	67	69	69	70	70	71.15	71.15	69	70	70	70	70	70	91.17
13/F	65.35	94.32	91.68	70	70	80.60	77.45	69	69	70	70	68	68	69	67	70	70	71.15	71.15	69	70	70	70	70	70	94.32
14/F	68.50	97.47	94.83	70	70	83.75	80.60	69	69	70	70	68	68	69	70	68	70	71.15	71.15	69	70	70	70	70	70	97.47
15/F	71.65	100.62	98.08	69	69	86.90	83.75	69	69	70	70	68	68	69	70	68	70	71.15	71.15	69	70	70	70	70	70	100.62
16/F	74.80	103.77	101.23	69	69	90.05	86.90	69	69	70	70	68	68	69	70	68	70	71.15	71.15	69	70	70	70	70	70	103.77
17/F	77.95	106.92	104.38	70	70	93.20	90.05	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	106.92
18/F	81.10	110.07	107.53	70	70	96.35	93.20	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	110.07
19/F	84.25	113.22	110.68	70	70	99.50	96.35	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	113.22
20/F	87.40	116.37	113.83	70	70	102.65	99.50	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	116.37
21/F	90.55	119.52	116.98	70	70	105.80	102.65	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	119.52
22/F	93.70	122.67	120.13	70	70	108.95	105.80	70	69	70	70	68	68	69	70	69	70	71.15	71.15	69	70	70	70	70	70	122.67
23/F	96.85	125.82	123.28	70	70	112.10	108.95	70	70	70	70	69	69	69	70	69	70	71.15	71.15	69	70	70	70	70	70	125.82
Exceedance		18	18	11	4	14	13	0	0	10	10	0	0	6	0	0	14	0	15	15	0	16	17	12	18	
No. of exceedance		23	23	13	13	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23
No. of Premises		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

Flour	m/p	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-F3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2
1/F	27.55	66	66	66	66	65	65	65	65	65	65	62	60	59	59	61	53	61	61	61	62	67	58	64
2/F	30.70	66	66	66	66	65	65	65	65	65	65	62	61	59	59	62	54	62	62	62	63	63	60	55
3/F	33.85	66	66	66	66	65	65	65	65	65	65	62	61	60	59	62	54	63	63	63	64	60	55	66
4/F	37.00	66	66	66	66	65	65	65	65	65	65	62	61	60	60	63	55	63	63	64	65	60	66	67
5/F	40.15	66	66	66	66	65	65	65	65	65	65	62	62	61	60	63	56	64	64	65	65	61	66	67
6/F	43.30	66	66	66	66	65	65	65	65	65	65	63	62	61	61	64	57	64	64	65	65	61	66	67
7/F	46.45	66	66	66	66	66	66	65	65	65	66	63	62	62	61	64	57	64	65	65	65	61	66	67
8/F	49.60	66	66	66	66	66	66	66	66	66	66	63	62	62	61	64	58	64	65	65	65	61	67	68
9/F	52.75	66	66	66	66	66	66	66	66	66	66	63	63	62	62	58	58	65	65	65	65	61	67	68
10/F	55.90	66	66	67	66	66	66	66	66	66	66	63	63	63	62	65	59	66	66	67	67	61	67	68
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	63	63	62	65	59	66	66	67	67	61	68	68
12/F	62.20	67	67	67	67	67	66	66	66	66	66	64	64	63	63	66	59	66	67	67	67	61	68	68
13/F	65.35	67	67	68	67	67	67	66	66	66	67	64	64	63	63	66	60	67	67	68	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	69
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	60	68	68	68	68	63	69	69
16/F	74.80	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	63	69	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	63	63	68	61	68	68	68	68	63	69	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
19/F	84.25	69	69	69	69	69	69	69	69	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
20/F	87.40	69	69	70	69	69	69	69	69	68	69	65	64	64	63	68	61	68	68	68	68	63	69	69
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69

Floor	nPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-J1	NC-K1	NC-L1	NC-M1	NC-N1
1/F	21.55	67	69	69	70	70	70	70	70	70	70	70	70	69	69	67	61	60	59	58	57	52	61	61	62	62
2/F	30.70	67	69	70	69	69	69	69	69	69	69	69	69	69	69	67	62	61	60	59	58	55	62	62	63	63
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	69	68	67	63	62	61	59	59	56	63	63	63	63
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	63	63	62	60	59	56	64	63	64	64
5/F	40.15	67	68	69	68	68	68	68	68	69	69	68	68	68	68	67	64	63	62	60	60	57	64	64	64	65
6/F	43.30	67	68	68	68	68	68	68	68	68	69	68	68	68	68	66	64	64	63	61	60	57	64	64	64	65
7/F	46.45	67	67	68	68	68	68	68	68	68	69	67	68	68	68	66	64	64	63	62	61	57	64	64	64	65
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	62	61	58	65	65	65	66
9/F	52.75	67	67	68	68	68	68	68	68	68	67	67	67	67	67	66	64	64	63	62	61	57	64	64	64	65
10/F	55.90	67	67	68	68	68	68	68	68	68	67	67	67	67	67	66	64	64	63	63	63	57	64	64	64	65
11/F	59.05	67	67	68	68	68	67	67	67	67	67	67	67	67	67	66	64	64	64	63	63	59	66	66	66	66
12/F	62.20	67	67	68	68	68	67	67	67	67	67	67	67	67	67	66	64	64	64	63	63	59	66	66	67	67
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	64	59	67	67	67	67
14/F	68.50	68	68	68	68	68	68	67	67	67	67	68	66	67	67	66	64	64	64	64	64	59	67	67	67	67
15/F	71.65	68	68	68	68	68	68	68	68	68	67	68	66	67	67	66	64	64	64	64	64	60	67	67	67	67
16/F	74.80	68	68	68	68	68	68	68	68	68	68	66	67	67	67	66	64	64	64	64	64	60	67	67	67	67
17/F	77.95	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	64	64	60	68	68	68	68
18/F	81.10	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68	68
19/F	84.25	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68	68
20/F	87.40	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68	69
21/F	90.55	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68	69
22/F	93.70	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68	69
23/F	96.85	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	68	69
		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		23															23									
No. of Premises			23			23		23		23		23		23		23		23		23		23		23		23

Max. Noise Level (dB(A)) =	75
Total no. of Exceedance =	183
Total no. of Premises =	759
%Compliance =	76%

 Applied with Acoustic Balcony (-2dB(A))

N.B. shaded cells denote noise level that will exceed limit of 70dB(A)

Predicted Road Traffic Noise (L₁₀(1-hour)) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Mitigated Scenario (PM)

Year	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-I2	NA-J1	NA-K1	NA-M1	NA-M2
1/f	27.53	58	61	63	63	65	65	63	63	64	63	63	65	67	67	67	66	63	64	63	58	59	56	55	53
2/f	30.70	60	62	63	63	65	65	63	63	65	65	63	64	66	65	67	67	64	65	64	60	60	58	57	55
3/f	33.85	62	63	63	64	66	66	64	64	66	66	64	64	66	65	67	67	64	65	65	61	62	60	60	58
4/f	37.00	65	65	64	64	66	66	64	64	66	66	64	64	66	65	68	68	65	66	66	62	64	63	64	62
5/f	40.15	69	68	65	65	66	66	64	64	66	66	64	64	66	65	68	68	66	67	67	65	67	67	68	66
6/f	43.30	72	71	66	66	67	67	64	64	68	68	64	64	68	65	68	68	68	68	68	67	69	69	70	69
7/f	46.45	75	73	68	67	68	68	63	63	67	67	64	64	68	65	68	68	69	67	69	70	70	70	70	70
8/f	49.60	77	73	69	68	69	66	66	67	67	67	65	65	67	65	69	70	68	70	70	68	70	71	70	70
9/f	52.75	79	73	69	69	70	69	67	66	68	68	66	65	67	65	69	70	68	71	71	69	69	71	71	70
10/f	55.90	81	74	70	69	71	70	68	67	69	68	66	65	68	66	69	70	69	71	71	69	69	71	71	70
11/f	59.05	84	74	70	70	71	71	68	68	69	69	67	67	68	66	69	70	69	71	71	69	69	71	71	70
12/f	62.20	87	74	70	70	71	71	69	68	69	70	67	67	69	67	69	70	69	71	71	69	69	71	71	70
13/f	65.35	89	74	70	70	72	71	69	69	70	70	68	68	69	67	69	70	69	71	71	69	69	71	71	70
14/f	68.50	91	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
15/f	71.65	93	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
16/f	74.80	95	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
17/f	77.95	97	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
18/f	81.10	99	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
19/f	84.25	101	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
20/f	87.40	103	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
21/f	90.55	105	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
22/f	93.70	107	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
23/f	96.85	109	74	70	70	72	70	69	69	70	70	68	68	70	68	69	70	69	71	71	69	69	71	71	70
Exceedance		18	18	11	0	14	13	0	0	10	10	0	0	4	0	0	13	0	15	15	0	16	17	10	18
No. of Premises		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

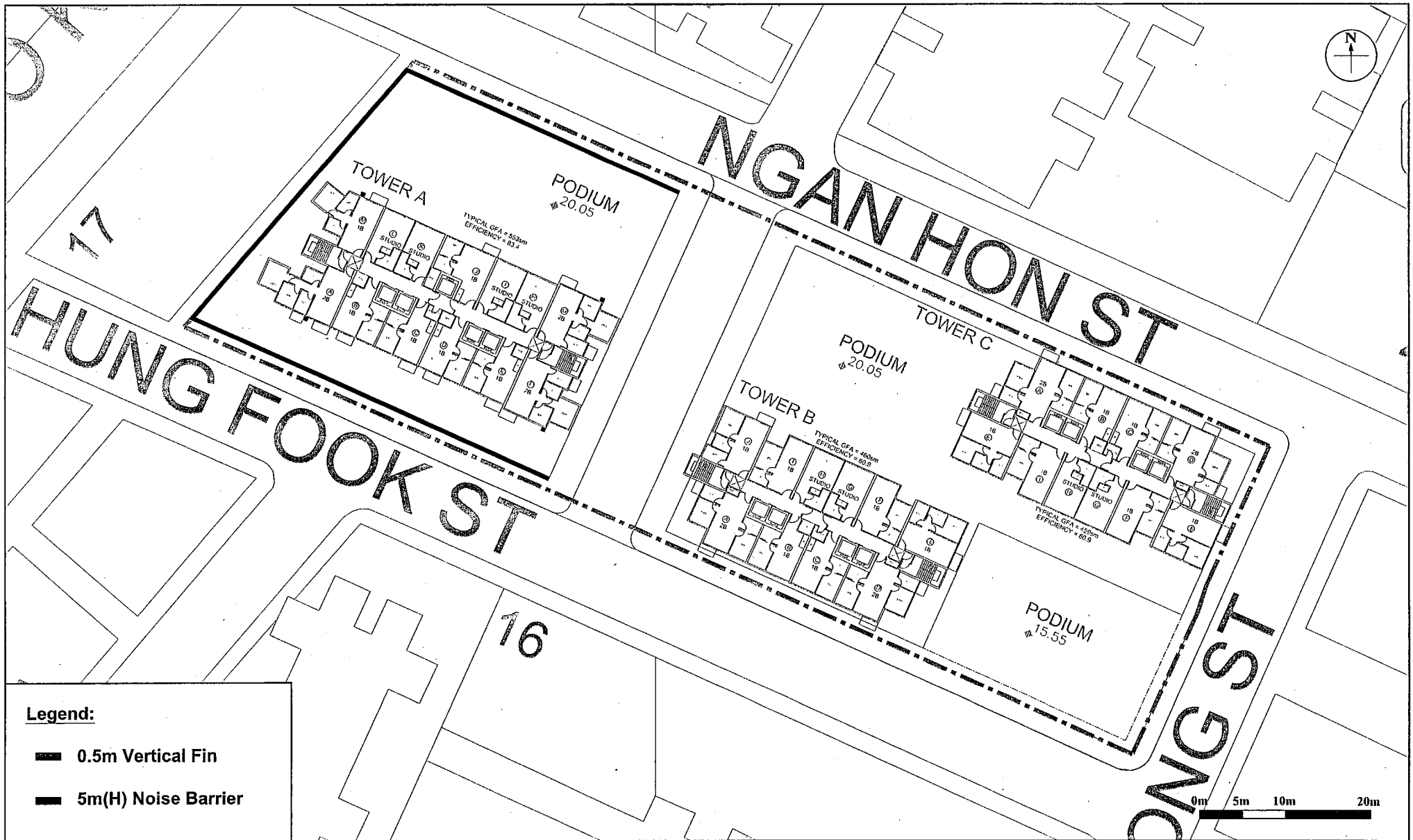
[illegible]

floor	mPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-J2	NC-K1	NC-L2	NC-M3
1/F	27.55	67	69	69	69	69	69	69	70	70	70	69	69	69	69	67	63	60	58	57	57	52	61	60	61
2/F	30.70	67	69	69	69	69	69	69	69	69	69	69	69	69	69	67	62	61	60	59	58	54	62	61	62
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	63	62	60	59	59	55	63	62	63
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	66	63	63	61	60	59	56	63	63	64
5/F	40.15	67	68	69	68	68	68	68	68	68	68	68	68	68	68	66	63	63	62	60	60	56	64	63	64
6/F	43.30	67	68	68	68	68	68	68	68	68	68	68	68	68	68	66	64	63	62	61	60	57	64	64	65
7/F	46.45	67	67	68	68	68	68	68	68	68	68	68	68	68	68	66	64	64	63	62	61	57	64	64	65
8/F	49.60	67	67	68	68	68	68	68	68	68	68	68	68	68	68	66	64	64	63	62	61	57	64	64	65
9/F	52.75	67	67	68	68	68	68	68	68	68	68	68	68	67	67	67	66	64	64	63	62	62	58	65	66
10/F	55.90	67	67	68	68	68	68	67	67	68	67	67	67	67	67	66	64	64	63	62	62	58	65	66	66
11/F	59.05	67	67	68	68	68	68	67	67	67	67	67	67	67	67	66	64	64	64	63	63	59	66	66	66
12/F	62.20	67	67	68	68	68	68	67	67	67	67	68	65	67	67	66	64	64	64	64	63	63	59	66	66
13/F	65.35	67	68	68	68	68	68	67	67	67	67	68	65	67	67	66	64	64	64	64	63	59	66	67	67
14/F	68.50	68	68	68	68	68	68	67	67	67	68	68	65	67	67	66	64	64	64	64	64	59	67	67	67
15/F	71.65	68	68	68	68	68	68	67	67	67	68	68	65	67	67	66	64	64	64	64	64	59	67	67	67
16/F	74.80	68	68	68	68	68	68	68	68	68	68	68	65	67	67	66	64	64	64	64	64	59	68	68	68
17/F	77.95	69	69	69	69	69	68	68	68	68	68	68	66	68	67	66	64	65	64	64	64	60	68	68	68
18/F	81.10	69	69	69	69	69	68	68	68	68	68	68	66	66	67	66	64	65	65	64	65	60	68	68	68
19/F	84.25	69	69	69	69	69	69	68	68	68	68	68	66	66	67	66	64	65	65	64	65	60	68	68	68
20/F	87.40	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68
21/F	90.55	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68
22/F	93.70	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68
23/F	96.85	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	64	65	60	68	68	68
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of Premises		23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23

☐ Applied with Acoustic Balcony (-2dB(A))

N.B. shaded cells denote noise level that will exceed limit of 70dB(A)

APPENDIX 2.3: SENSITIVE TEST OF THE NOISE PERFORMANCE FOR THE NOISE BARRIER OR CANOPY AT PODIUM



Appendix: 2.3a		RAMBOLL ENVIRON
Title:	Location of Noise Barrier at Podium for the Sensitivity Test (Scenario with Noise Barrier)	Drawn by: JC
Project:	Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City	Checked by: TC
		Rev.: 1.0
		Date: Jul 2016

Predicted Road Traffic Noise (L₁₀(1-hour)) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Mitigated Scenario (AM)

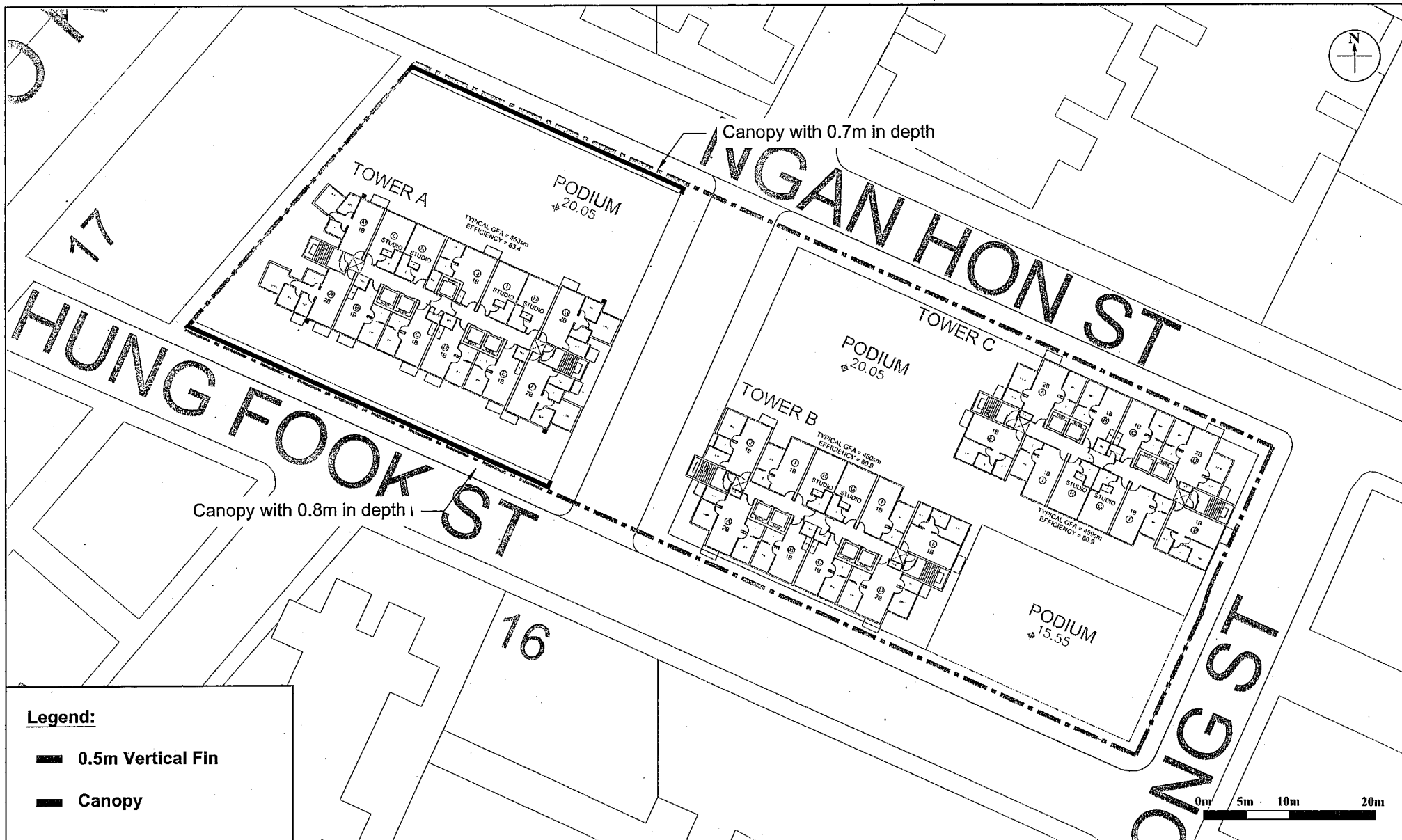
Floor	nPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-K1	NA-L1	NA-M1	NA-N1
1/F	27.55	55	59	61	63	62	60	60	61	59	60	63	63	66	65	61	63	65	65	64	54	54	54	52	53
2/F	30.70	58	61	62	63	65	62	62	64	64	62	62	65	64	67	66	63	64	62	57	58	57	56	54	57
3/F	33.85	61	62	63	64	65	65	63	63	65	65	63	63	66	65	67	67	64	65	64	60	61	59	59	57
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	64	66	65	67	67	64	65	65	62	64	63	63	62
5/F	40.15	69	68	65	65	67	66	64	64	66	66	64	64	68	65	67	68	65	67	67	65	67	67	68	66
6/F	43.30	72	71	68	67	67	65	65	66	66	67	64	65	65	68	68	66	68	66	68	69	69	70	69	72
7/F	46.45	75	74	68	67	68	67	67	67	67	67	65	65	67	65	67	65	69	70	70	70	71	70	70	73
8/F	49.60	77	74	69	68	69	68	69	68	69	68	67	65	67	65	69	70	68	70	70	70	71	70	70	73
9/F	52.75	77	74	69	69	70	69	67	67	68	68	68	66	66	67	66	69	70	68	70	70	71	70	70	73
10/F	55.90	77	74	70	69	71	70	68	67	69	66	66	68	66	69	70	68	71	71	71	70	72	70	70	73
11/F	59.05	77	74	70	70	71	68	68	68	69	69	67	67	68	66	69	71	71	71	69	71	71	72	70	73
12/F	62.20	77	74	70	70	72	68	68	68	70	70	67	67	69	67	69	71	71	71	69	71	71	72	70	74
13/F	65.35	77	74	70	70	72	68	69	69	70	70	68	68	69	67	69	71	71	69	71	71	72	70	70	74
14/F	68.50	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
15/F	71.65	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
16/F	74.80	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
17/F	77.95	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
18/F	81.10	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
19/F	84.25	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
20/F	87.40	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
21/F	90.55	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
22/F	93.70	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
23/F	96.85	77	74	70	70	72	68	69	69	70	68	68	68	69	67	69	71	71	69	71	71	72	70	70	74
Exceedance		18	18	11	4	14	13	0	0	10	10	0	0	1	0	0	13	0	14	15	0	16	16	17	10
No. of exceedance																									
No. of Premises		23	23	14	13	13	13	23	23	23	23	23	23	6	6	13	23	23	23	23	23	23	23	23	23

Flavor	NP-01	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2	
1/F	27.55	65	65	66	66	66	65	65	65	65	66	63	63	63	63	61	53	58	58	61	62	60	62	65	
2/F	30.70	65	66	66	66	66	65	65	65	65	66	63	63	63	63	62	54	60	60	61	62	60	64	66	
3/F	33.85	66	66	66	66	66	65	65	65	65	66	63	63	63	63	63	55	62	62	63	64	60	65	66	
4/F	37.00	66	66	66	66	66	65	65	65	65	66	63	63	63	63	63	55	63	63	63	64	60	65	66	
5/F	40.15	66	66	66	66	66	65	65	65	65	66	63	63	63	63	64	56	63	64	64	65	61	65	66	
6/F	43.30	66	66	66	66	66	66	65	65	65	66	64	63	63	63	64	57	64	64	64	65	61	66	66	
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	64	58	64	64	65	65	61	66	67	
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	54	65	65	66	64	67	67	67	
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67	
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	65	59	66	66	66	67	61	67	68	
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68	
12/F	62.20	67	67	67	67	67	67	66	66	66	67	64	64	63	63	66	59	66	67	67	67	61	68	68	
13/F	65.35	67	67	68	67	67	67	67	67	66	67	64	64	63	63	66	60	67	67	67	67	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	68	
15/F	71.65	68	68	68	68	68	67	67	67	67	67	64	64	63	63	67	61	68	68	68	68	62	68	69	
16/F	74.80	68	68	68	68	68	68	68	67	67	67	64	64	63	63	67	61	68	68	68	68	63	69	69	
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69	
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69	
19/F	84.25	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69	
20/F	87.40	69	69	70	69	69	69	69	69	68	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
23/F	96.85	69	70	70	70	70	69	69	69	69	69	65	64	64	64	68	62	68	68	68	68	63	69	69	
Exceedance	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
No. of exceedance																									
No. of Premises	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23	23			23	

Flour	mpD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-J3
1/F	37.53	67	69	69	70	70	70	70	70	70	70	69	69	69	69	69	69	69	69	63	63	63	63	66	69
2/F	30.70	67	69	70	69	69	69	69	69	69	69	69	69	69	69	67	63	64	63	63	62	59	62	60	61
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	69	68	67	64	64	63	63	62	59	63	62	63
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	63	62	59	64	62	63
5/F	40.15	67	68	69	68	68	68	68	68	69	69	68	68	68	68	67	64	64	63	63	62	59	64	63	64
6/F	43.30	67	68	68	68	68	68	68	68	68	69	68	68	68	68	67	64	64	63	63	62	59	64	64	65
7/F	46.45	67	67	68	68	68	68	68	68	68	69	67	68	68	68	66	64	64	63	63	62	59	65	64	65
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	67	67	67	66	64	64	63	62	59	65	65	66	66
9/F	52.75	67	67	68	68	68	68	68	68	68	68	67	67	67	67	66	64	64	63	62	59	65	65	66	66
10/F	55.90	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	63	63	59	65	66	66	66
11/F	59.05	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	66
12/F	62.20	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	67
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	59	67	67	67	67
14/F	68.50	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	60	67	67	67	67
15/F	71.65	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	60	67	67	67	67
16/F	74.80	69	69	69	69	68	68	68	68	68	68	66	67	67	67	66	64	64	64	64	60	68	68	68	68
17/F	77.95	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	64	60	68	68	68
18/F	81.10	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68
19/F	84.25	69	69	69	69	69	69	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68
20/F	87.40	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
21/F	90.55	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
22/F	93.70	69	69	69	69	69	69	69	69	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
13/F	96.85	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance																									
No. of Premises		23				23		23		23		23		23		23		23		23		23		23	

☐ Applied with Acoustic Balcony (-2dB(A))

N.B. shaded cells denote noise level that will exceed limit of 70dB(A)



Appendix: 2.3b

Title: Location of Canopy at Podium for the Sensitivity Test (Scenario with Canopy)

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.0

Date: Jul 2016

Predicted Road Traffic Noise (L₁₀(1-hour)) at Selected Sensitive Receivers (Based on Year 2041 Traffic Forecast)
Mitigated Scenario (AM)

Floor	mP0	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-J1	NB-K1	NB-L1
1/F	27.55	66	65	66	66	66	65	65	65	65	66	63	63	63	63	63	63	61	62	62	58	64	66	
2/F	30.70	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	64	62	62	63	63	60	65	66
3/F	33.85	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	55	63	63	63	64	60	65	66
4/F	37.00	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	55	63	63	64	65	60	66	66
5/F	40.15	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	56	64	64	65	65	60	66	67
6/F	43.30	66	66	66	66	66	65	65	65	65	66	64	63	63	63	64	57	64	64	65	65	60	66	67
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	64	58	65	65	65	66	60	66	67
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	63	58	65	65	66	66	61	67	67
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	58	65	66	66	66	61	67	67
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	65	59	66	66	67	67	61	67	68
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68
12/F	62.20	67	67	67	67	67	67	66	66	66	66	64	64	63	63	66	59	66	67	67	67	61	68	68
13/F	65.35	67	67	68	67	67	67	67	67	67	66	67	64	63	63	65	60	67	67	68	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	69
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	62	69	69
16/F	74.80	68	68	68	68	68	68	68	68	67	67	65	64	64	63	67	61	68	68	68	68	62	69	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
19/F	84.25	69	69	69	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
20/F	87.40	69	69	70	69	69	69	69	69	69	68	69	65	64	64	63	68	61	68	68	68	63	69	69
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
23/F	96.85	69	70	70	70	69	69	69	69	69	69	66	64	64	64	68	62	68	68	68	68	63	69	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance																								
No. of Premises		23				23		23		23		23		23		23		23		23		23		23

Max. Noise Level (dB(A)) =	75
Total no. of Exceedance =	180
Total no. of Premises =	759
% Compliance =	76%

Applied with Acoustic Balcony (-2dB(A))

N.B. shaded cells denote noise level that will exceed limit of 70dB(A)

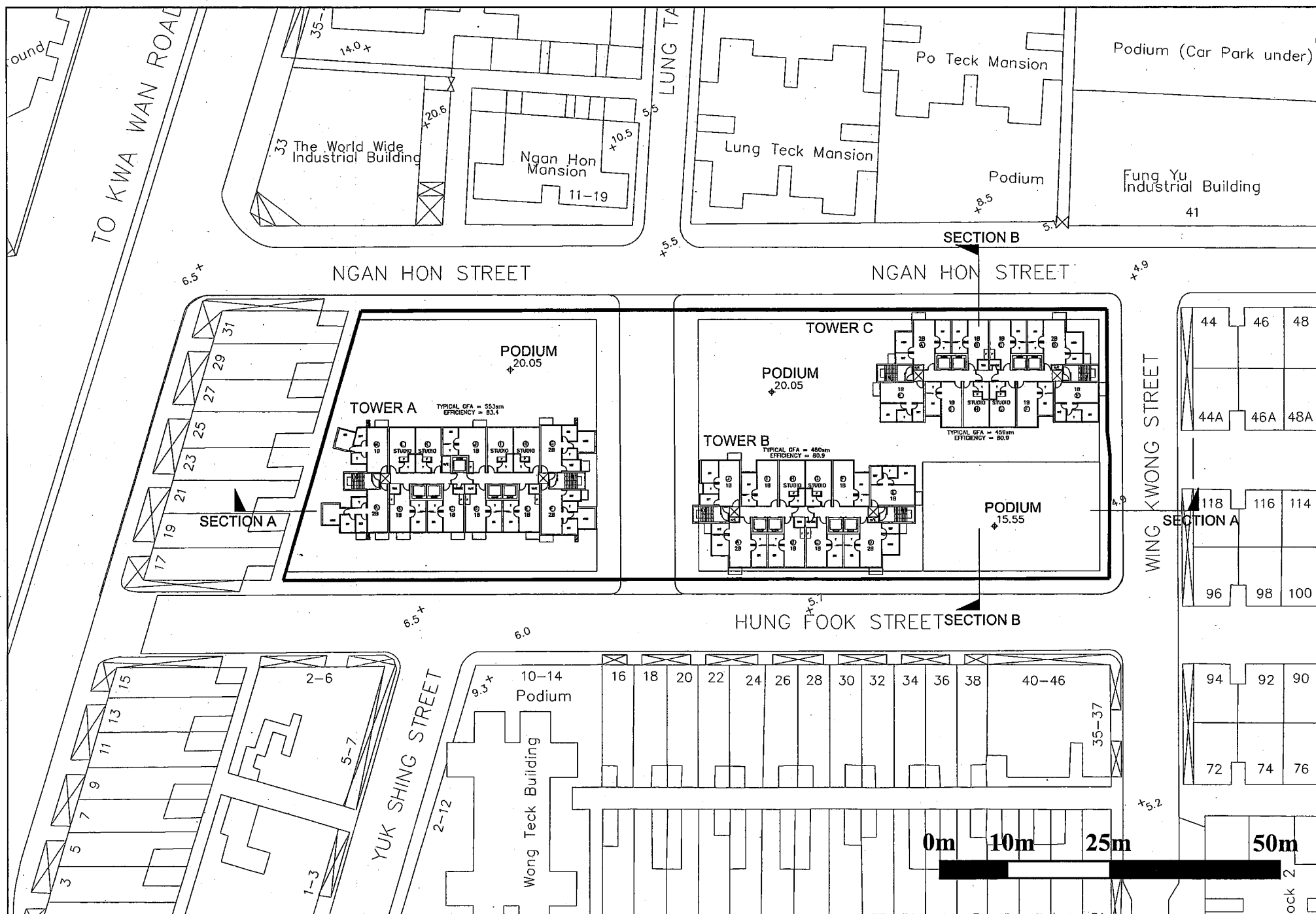
Appendix 2.6 - Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

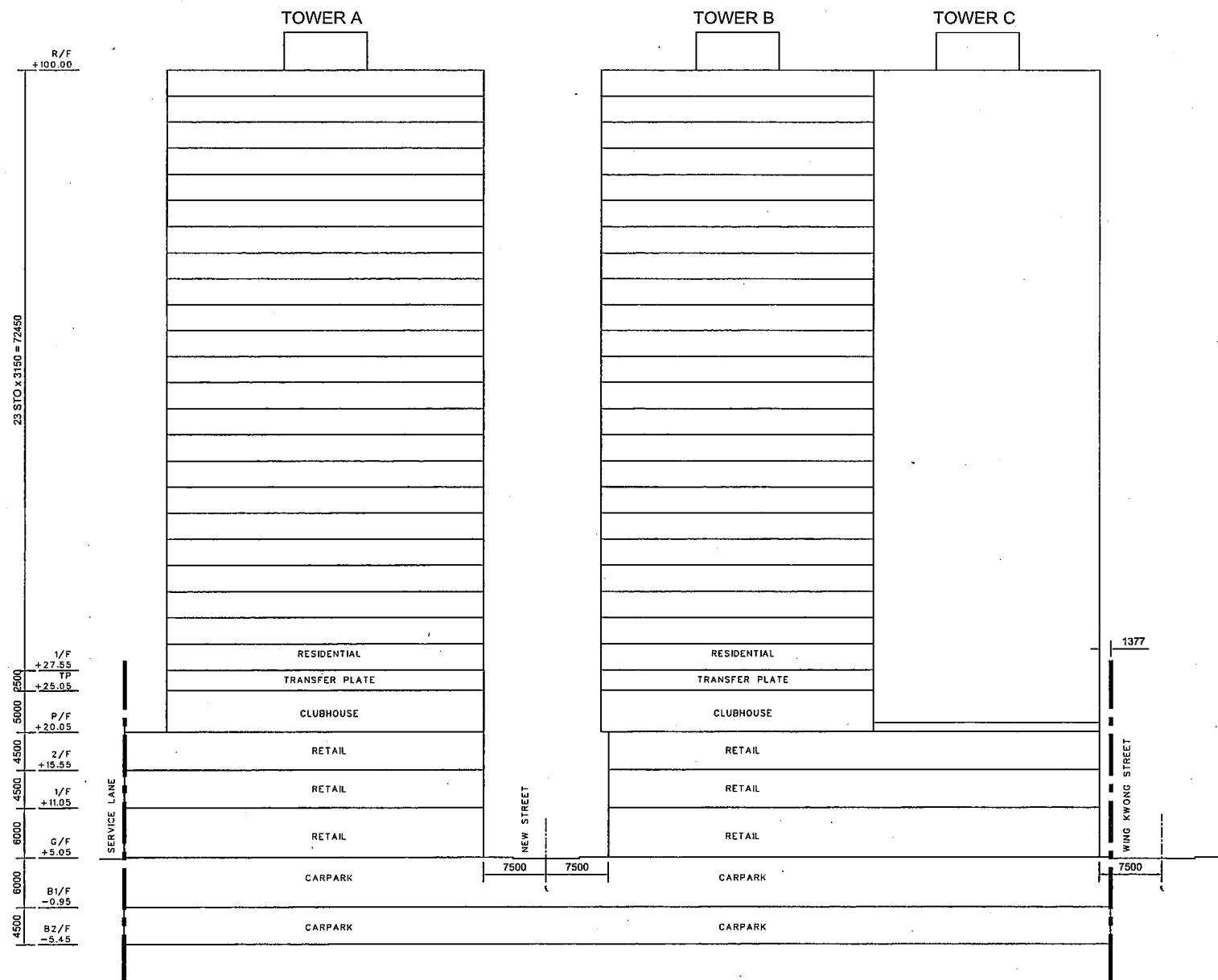
Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A) [1]	Corrected Noise Level, Leq (30 min), dB(A)
NC-A3	837546.4121	819487.2658	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	64.2	-44.2	0	3	0	51
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	32.5	-38.2	0	3	0	56
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	26.7	-36.5	0	3	0	58
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	16.5	-32.4	0	3	0	62
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	19.8	-33.9	0	3	0	61
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	23.8	-35.5	0	3	0	59
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	31.6	-38.0	0	3	0	57
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	51.5	-42.2	0	3	0	52
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	58.4	-43.3	0	3	0	51
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	46.0	-41.3	-10	3	0	43
Total															68

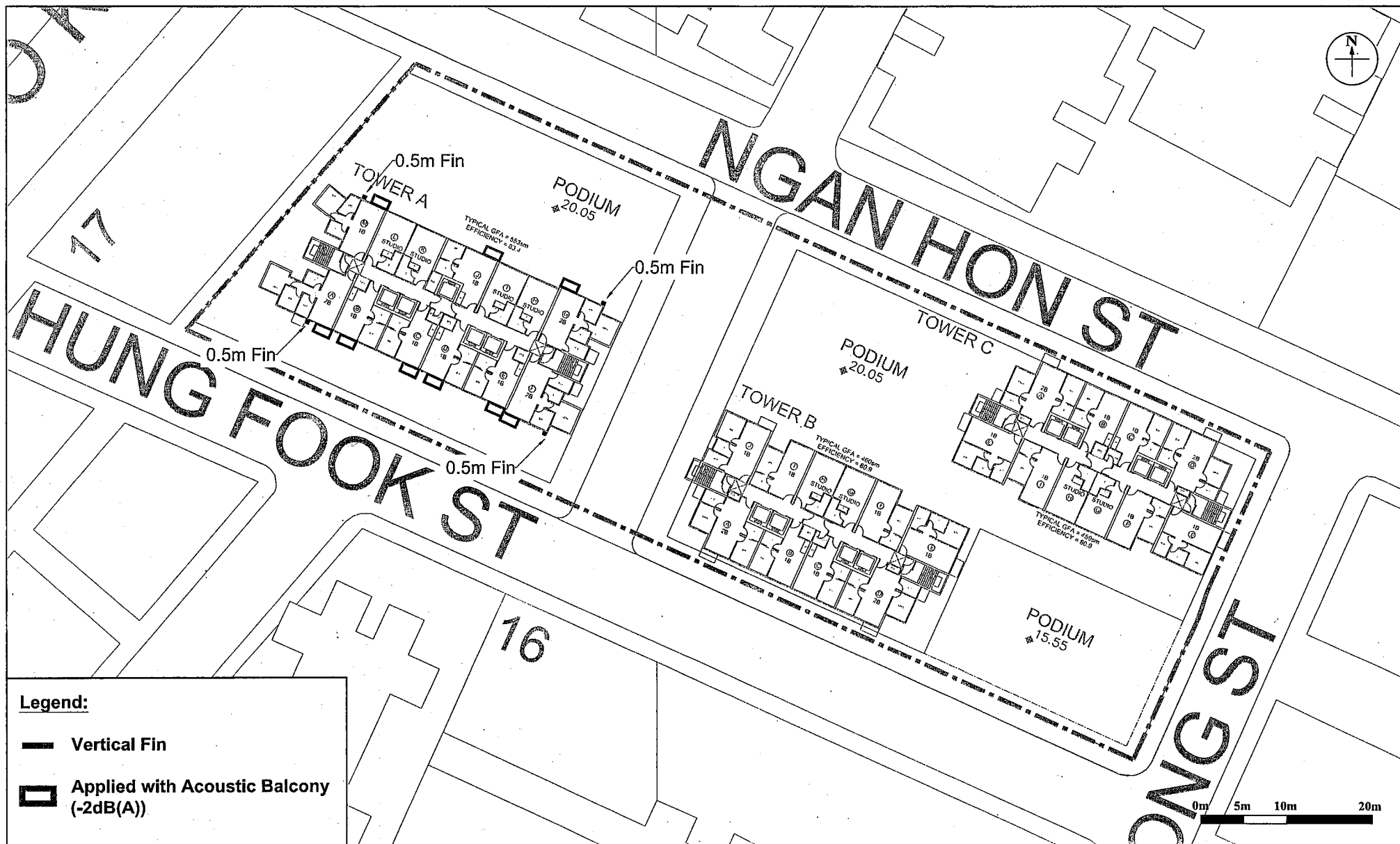
Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A) [1]	Corrected Noise Level, Leq (30 min), dB(A)
NC-D1	837562.02	819480.1666	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	80.9	-46.2	0	3	0	49
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	47.8	-41.6	0	3	0	53
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	41.1	-40.3	0	3	0	54
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	21.3	-34.6	0	3	0	60
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	17.6	-32.9	0	3	0	62
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	16.3	-32.2	0	3	0	62
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	19.7	-33.9	0	3	0	61
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	35.7	-39.0	0	3	0	56
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	41.3	-40.3	0	3	0	54
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	29.9	-37.5	0	3	0	57
Total														69	

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A) [1]	Corrected Noise Level, Leq (30 min), dB(A)
NC-E1	837566.6733	819465.2197	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	93.9	-47.5	-10	3	0	37
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	62.2	-43.9	-10	3	0	41
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	55.9	-42.9	-10	3	0	42
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	36.9	-39.3	-10	3	0	45
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	32.7	-38.3	-10	3	0	46
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	29.8	-37.5	-10	3	0	47
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	29.0	-37.3	0	3	0	57
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	35.2	-38.9	0	3	0	56
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	32.0	-38.1	0	3	0	57
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	16.7	-32.5	0	3	0	62
														Total	65

Note: [1] Reference is made to "Noise Impact Assessment for Proposed Comprehensive Residential Development with "Shop and Services" and "Eating Place" in "Comprehensive Development Area (2)" Zone at KIL 10578, 7628 and 7626 in Ma Tau Kok" Report (refer to as NIA-MTK Report). According to NIA-MTK Report, no impulsive noise is identified. Compared to motor service workshops in NIA-MTK Report, motor service workshops in the vicinity of Subject Site are relatively smaller. According to site survey, the major activities include car washing and car painting. Limited hammering works were observed. Considered that no tonal, impulsiveness/ intermittency were observed in NIA-MTK Report, no impulsive noise from garages in the vicinity of Subject Site, which are of relatively smaller size and involves less noisy activities, are expected.







3. Sewerage Impact Assessment

3.1 Scope of Work

- 3.1.1 The aim of this study is to assess whether the capacity of the existing sewerage networking to the Subject Site is sufficient to cope with the sewage flow from the proposed redevelopment and existing development in the vicinity. Drainage Record Plans from the Drainage Services Department were obtained for the information of sewerage impact assessment.

3.2 Existing Sewerage System

- 3.2.1 According to the Drainage Record obtained from the Drainage Services Department, there are existing Ø300mm sewers running along the western side of the Subject Site (manhole reference no. FMH4024355 to FMH4024356), which are then connected to the Ø525mm sewers running along the Hung Fook Street (manhole reference no. FMH4024356 to FMH4025769).
- 3.2.2 The existing sewer network along Hung Fook Street is also connected to sewers including Yuk Shing Street, Kai Ming Street, and Wing Kwong Street. The sewage flow from the connected sewer network is included as catchment areas as shown in **Figure 3**.

3.3 Assessment Criteria and Methodology

- 3.3.1 Environmental Protection Department's (EPD's) Guidelines for Estimating Sewage Flows for Sewage Infrastructure Planning, Version 1 (GESF) is referred to estimate the quantity of the sewage generated from the proposed development and the existing development. Sewage flow parameters and global peaking factors in this document are adopted.
- 3.3.2 According to the Table T-1 of the said EPD Guideline, the unit flow factors of Domestic Flow – Private R2 is 0.27 m³/day.
- 3.3.3 According to the Table T-2 of the said EPD Guideline, the general unit flow factors is 0.20 m³/day and the flow of commercial employee is 0.08 m³/day.
- 3.3.4 The Catchment Inflow Factor (P_{CIF}) for Central Kowloon – 1.00 is used for both upstream and downstream catchment areas.

3.4 Wastewater Generated by the Proposed Development

- 3.4.1 Wastewater arising from the proposed redevelopment will be primarily contributed by the residents, and commercial employees of the clubhouse and retail shops. Under the current proposal, there will be a total of 759 residential units, with non-domestic gross floor area of 6,843m².
- 3.4.2 The average household density of 2.9 according to 2011 Hong Kong Population Census (Kowloon City District) is assumed as a worse cast scenario, which would amount to about 2201 residents.
- 3.4.3 There will be a total of 274 employees worked in the total non-domestic area (including clubhouse).

3.4.4 Detailed calculation for the proposed development is given in **Table 1** below.

Table 1 Estimated Peak Flow

<i>Calculation for Sewage Generation Rate of the Proposed Development</i>		
Proposed Development		
1a. Total number of residential units	=	759 units
1b. Total number of residents	=	2201 residents (average household density of 2.9 in Kowloon City)
1c. Design flow	=	270 litre/employee/day -- (Private R2 in Table-1 of GESF)
1d. Sewage generation rate	=	594.3 m ³ /day
2. Total Non-Domestic Area (Including Clubhouse)	=	6843 m ²
2a. Assumed floor area per employee	=	25 m ² per employee – (refer to Table 2 in Chapter 5 of HKPSG)
2b. Total number of employees	=	274 employees
2c. Design flow for commercial employee	=	80 litre/employee/day -- (refer to Table T-2 of GESF)
2d. Design flow for commercial activities	=	200 litre/employee/day -- (refer to Table T-2 of GESF – General)
2e. Sewage generation rate	=	76.6 m ³ /day
Proposed Connection to Manhole S1 (FMH4024355)		
Flow Rate	=	670.9 m ³ /day
Contributing Population	=	2485 persons
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	46.6 litre/sec

3.5 Assessment of Sewerage Impact

3.5.1 Sewage generated from the Subject Site will be discharged to the existing sewerage system along Hung Fook Street, to manhole no. FMH4024355, as shown in **Figure 3**. According to the drainage record available from DSD, the sewers on Hung Fook Street are 300mm (i.e. pipe segments from manhole no. FMH4024355 to FMH4024356) and 525mm in diameter (i.e. pipe segments from manhole no. FMH4024356 to FMH4025769).

3.5.2 It is estimated that the capacity of the aforementioned pipe segments is sufficient to cater the sewage generated from its catchment and from the Subject Site.

3.5.3 **Appendix B** shows the detailed calculation on the estimated hydraulic capacity of the existing sewer sections and the calculation of the amount of the sewage entering each segment of the said sewer network.

3.6 Discussion

3.6.1 The potential sewerage impact due to the proposed development has been quantitatively addressed as shown in **Appendix B**. The average and peak flow rates from the proposed development are respectively about 670.9 m³/day and 46.6 litre/sec.

3.6.2 Since the incoming invert level of Manhole S2 (FMH4024356) is missing on the DSD Drainage Record, the outgoing level of 3.15mPD is used. For the inconsistent invert levels

(upward slope) on the DSD Drainage Records, the incoming and outgoing invert levels of Manholes S3 (FMH4024357) have been amended to 3.13mPD to align with the downstream sewer pipes. Also, the invert levels of Manhole S4 (FMH4024358) and Manhole S5 (FMH4024359) are identical. To maintain velocity between the two manholes, the invert levels of Manhole S5 have been adjusted to align with Manhole S4 and Manhole S6.

- 3.6.3 After calculating the appropriate invert levels as mentioned above, the estimated sewage flow from the Subject Site has been compared with the capacity of the existing sewerage system to determine whether it has adequate spare capacity to accommodate the flow from the proposed connection.
- 3.6.4 According to **Table 4 of Appendix B**, it is found that the existing sewerage system has sufficient capacity to cater the sewage generated from the proposed development.
- 3.6.5 In the Planning stage assessment, no significant sewerage impact is anticipated. However, the Sewerage Impact Assessment has to be updated during the detailed design stage and upgrading or improvement works may be required later on.

4. Waste Management

4.1 Construction Phase Impact

- 4.1.1 This section presents the management and disposal strategy of the wastes generated from the construction work for the proposed development. The options for waste minimization, reuse, recycling, collection, transport and disposal of wastes arising from the demolition work have been examined. Where appropriate, procedures for waste reduction and management are considered and environmental control measures for avoiding and minimising the potential impacts are recommended.

4.2 Relevant Legislation & Guideline

- 4.2.1 The principal legislation controlling waste materials in Hong Kong is the Waste Disposal Ordinance (WDO) (Cap. 354) and its subsidiary regulations.
- 4.2.2 The Environment, Transport and Works Bureau Technical Circular (Works) No. 19/2005 – Environmental Management on Construction Sites outlines the requirement of waste management for on-site sorting of all Construction & Demolition Material (C&D material) prior to disposal. This circular states a particular specification clause to be included in the tender documents for mandatory on-site sorting, processing and disposal of the same.
- 4.2.3 The Development Bureau Technical Circular (Works) No. 6/2010– Trip-ticket System for Disposal of Construction and Demolition Material promulgates the policy to implement a trip-ticket system in Public Works Programme (PWP) contracts for the proper disposal of C&D material at public filling facilities or landfills.

4.3 Waste Generation During Construction Phase

General Site Wastes

- 4.3.1 Materials and equipment used on site would produce packaging wastes. However, the volume of general site wastes generated will be dependent on the Contractor's operating procedure and practices and cannot be quantified. Mitigation measures should include provision of a collection area where waste can be sorted, stored and loaded prior to removal from the site. With the implementation of recommended mitigation measures in the RPCC available on the EPD website, the potential environmental impacts resulting from the storage, handling and transportation of general site wastes would be minimal.

Workforce on the Wastes

- 4.3.2 During the construction phase, the workforce would generate general refuse, comprising food scraps, paper, empty containers etc. Rapid and effective collection of site wastes is therefore required to prevent waste materials being blown around by wind, flushed or leached into the environment, and odour nuisance. The amount of general refuse which is likely to arise cannot be quantified at this time as it will be largely dependent on the size of the workforce employed by the contractor. By implementation of waste management practices listed in the PRCC available on the EPD website, it is not expected that there would be any adverse environmental impacts arise from the storage, handling and transportation of workforce wastes.

Chemical Wastes

- 4.3.3 Construction plant and equipment will require regular maintenance and servicing, which would generate waste such as cleaning fluids, solvents, lubrication oil and fuel, etc. Chemical wastes arising during the construction phase may pose serious environmental, health and safety hazards if not stored and disposed of in an appropriate manner.
- 4.3.4 The contractor is required to register as a chemical waste producer if chemical wastes would be produced from the construction activities. The Waste Disposal Ordinance (Cap 354) and its subsidiary regulations in particular the Waste Disposal (Chemical Waste) (General) Regulation should be observed and complied with for control of chemical wastes.
- 4.3.5 Materials classified as chemical wastes will need special handling and storage arrangements before removal for appropriate treatment at the chemical waste treatment facility at Tsing Yi. Mitigation and control requirements for chemical waste are provided in the PRCC available on the EPD website mentioned the handling, storage and disposal of chemical wastes. With good management and site practices, adverse environmental impacts should not result.

Construction and Demolition (C&D) Material

- 4.3.6 The planning and design stages of the project has considered measures to reduce the generation of construction and demolition (C&D) materials. The project proponent will therefore require its contractors to reuse inert C&D materials (e.g. excavated soil) on site or in other suitable construction sites as far as possible, in order to minimize the disposal of C&D materials to public fill reception facilities. The project proponent should also encourage its contractors to maximize the use of recycled or recyclable C&D materials, such as metal site hoardings and signboards, as well as the use of non-timber formwork to further minimize the generation of construction waste. Besides, the project proponent has introduced more prefabricated building elements into the design to reduce temporary formwork and construction waste. These include dry-wall partitioning and proprietary fittings and fixtures.
- 4.3.7 The general waste management strategy is to avoid waste generation in the first place. Should it be unavoidable, reduction and segregation at-source should be exercised as far as practicable and recycling and reuse should be adopted at the same time to salvage all the recyclable and reusable materials as much as possible.
- 4.3.8 Inert C&D materials should be disposed of at public filling area or other CEDD designated public filling facilities. Nevertheless, on-site sorting of all C&D materials should be provided prior to disposal. Non-inert portion of C&D materials (i.e. C&D wastes) that cannot be reused or recycled should be disposed of to designated landfill sites.
- 4.3.9 The project proponent should also require the contractors to submit waste management plans (WMPs) for approval. The WMPs would include appropriate mitigation measures to avoid, reduce, reuse and recycle C&D materials. It would ensure that the day-to-day operations on site comply with the approved WMPs. It would control the disposal of public fill, C&D materials and C&D waste to public fill reception facilities, sorting facilities¹ and landfills respectively through a trip-ticket system. It will require the contractors to separate public fill from C&D waste

¹ Sorting facilities and public fill reception facilities are specified in Schedule 3 and Schedule 4 respectively of the Waste Disposal (Charges for Disposal of Construction Waste) Regulation. Disposal of public fill in public fill reception facilities requires a licence issued by the Director of Civil Engineering and Development.

for disposal at appropriate facilities. It will record the disposal, reuse and recycling of C&D materials for monitoring purposes.

- 4.3.10 The Contractor should be responsible for ensuring that waste is collected by approved licensed waste collectors and that appropriate measures are taken to minimise adverse impacts, such as dust generation. The Contractor must also ensure that all necessary waste disposal permits are obtained.
- 4.3.11 Prior to disposal of C&D waste, it is recommended that wood, steel, glass and other metals be separated for re-use and/or recycling and inert waste utilized as fill material to minimise the quantity of waste to be disposed of to landfill.
- 4.3.12 All the soil generated from the underground work should be reused on site to form the site to the existing level. Other C&D material should be used on-site as far as practicable.
- 4.3.13 Preliminary quantity estimation of construction waste involved and disposal method is summarised in the **Table 4.1** below.

Table 4.1 Summary Table of Estimated Construction Waste and Disposal Method

Waste Material Type	Generated from works item	Total Quantity Generated	Filling materials required for site formation works	Quantity to be disposed off-site	Disposal Route
Excavated Materials	Site formation, and retaining wall, etc.	Approximate 38,100m ³	Approximate 41,700m ³	0 m ³	Not Applicable
Other Inert C&D Materials	Rock, rubble, boulder, earth, soil, sand, concrete, used bentonite and demolition for exiting building.	Approximate 13,664m ³	0 m ³	Approximate 13,664m ³	To be reused or recycled on site or in other projects and delivered to public fill reception facilities for beneficial reuse
Non-inert C&D waste	Bamboo, timber, vegetation, packaging waste and other organic materials	Approximate 420m ³	0 m ³	Approximate 420m ³	To be reused or recycled on site or in other projects, remainder which can't be reused or recycled will be disposed of at SENT landfill (to be confirmed)
General Refuse	Food waste, waste paper, empty container generated from workforce	117kg/day (preliminary estimate, assuming there are 180 workers at any one time with generation rate of 0.65kg per worker per day)	Not Applicable	117kg per day	Refuse station for compaction and containerisation and then dispose of at SENT landfill
Chemical Waste	Cleansing fluids, solvent, lubrication oil and fuel from	Less than few cubic meters/month (preliminary estimate)	Not Applicable	Less than few cubic meters/month	Chemical Waste Treatment Centre

Waste Material Type	Generated from works item	Total Quantity Generated	Filling materials required for site formation works	Quantity to be disposed off-site	Disposal Route
	construction plants and equipment				

4.4 Mitigation Measures

- 4.4.1 The project proponent has considered the proposed development in the planning and design stages to reduce the generation of construction and demolition (C&D) materials where possible. In addition, the project proponent will require the contractor to reuse inert C&D materials (e.g. excavated soil) on site or in other suitable construction sites as far as possible, in order to minimize the disposal of C&D materials to public fill reception facilities. The project proponent should also encourage the contractor to maximize the use of recycled or recyclable C&D materials, as well as the use of non-timer formwork to further minimize the generation of construction waste.
- 4.4.2 Environmental Management Plan (EMP) should be prepared in accordance with ETWB TCW No. 19/2005 – Environmental Management on Construction Sites should be prepared by the Project main contractor and submitted to the Engineer for approval and implemented. The potential for recycling or reuse should be included in the EMP to achieve waste generation. The EMP should provide recommendations for appropriate disposal routes if waste cannot be recycled. The EMP should include the method statement for demolition and transportation of the excavated materials and other construction wastes. The EMP should be approved before the commencement of construction. All mitigation measures arising from the approved EMP should be fully implemented. The project proponent would ensure that the day-to-day operations on site comply with the approved EMP. According to the EMP, the project proponent should control the disposal of public fill, C&D materials and C&D waste to public fill reception facilities, sorting facilities and landfills respectively through a trip-ticket system. The project proponent shall require the contractor to separate public fill from C&D waste for disposal at appropriate facilities. In addition, the project proponent shall record the disposal, reuse and recycling of C&D materials for monitoring purposes.
- 4.4.3 Good management and control can also prevent the generation of significant amount of waste. On-site sorting of construction wastes will be recommended. Secondary on-site sorting can be achieved by avoiding the generation of “mixed waste” through good site control. Construction wastes shall be sorted to remove contaminants, with the inert materials broken up into small pieces for subsequent delivery to landfill sites.
- 4.4.4 Chemical and oily wastes generated from the construction activities, vehicle and plant maintenance and oil interceptors should be disposed of as chemical waste in strict compliance with the Waste Disposal (Chemical Waste) (General) Regulations.
- 4.4.5 The demolition and construction work should be considered in the planning and design stages to reduce the generation of C&D waste where possible. In addition, the project proponent should require the contractor to reuse inert C&D materials (e.g. excavated soil) on site or in other suitable construction sites as far as possible, in order to minimize the disposal of C&D materials to public fill reception facilities. Further the project proponent should also encourage

the contractor to maximize the use of recycled or recyclable C&D materials, as well as the use of non-timber formwork to further minimize the generation of construction waste.

- 4.4.6 "Recommended Pollution Control Clauses for Construction Contracts" must also be followed by the contractor in respect to removal of waste material from the construction site are available on EPD website.

4.5 Operation Phase Impact

- 4.5.1 The volume of construction waste is expected to be insignificant as the materials on site will be reused and recycled as far as engineering practicable.
- 4.5.2 The proposed residential use of the Project would generate a small amount of municipal waste during its operation. Also, general refuse from daily operation from non-domestic area (including clubhouse) are mainly generated from office activities and pantries such as waste paper, food scraps, etc. Thus, the waste impact generated by the proposed development is minimal.
- 4.5.3 During the operational phase of the proposed development, waste generated will be properly controlled through the implementation of refuse collection points and measures to minimise the generation of waste and to reuse or recycle, if applicable. Therefore, no adverse waste impact is anticipated.

2.4.2 Fixed Noise Sources

- 2.4.2.1 Site visits were carried out on 5th May 2016 and 25th May 16 to study the environment of the proposed development in adjacent. Potential fixed noise sources to the Subject Site mainly include the garages and workshops for car repair, motor services and metal alloys along Ngan Hon Street and Wing Kwong Street. Two industrial buildings are also identified at both ends of the Ngan Hon Street namely the World Wide Industrial Building (Location ID: L1) at the junction of the Ngan Hon Street and To Kwa Wan Road and the Fung Yu Industrial Building (Location ID: L2) behind the Sunrise Villa. Location of two industrial buildings and potential fixed noise sources (Location ID: S1 to S10) is indicated in **Figure 2.3** while the site photos are shown in **Appendix 2.4**.
- 2.4.2.2 According to the site visits, no industrial activities were observed at both World Wide Industrial Building (L1) and Fung Yu Industrial Building (L2) as most are office use only while some units are already vacant. Also, no cooling towers or either chillers were observed at the roof-top of both concerned industrial buildings while only ordinary window type air conditioners were noticed from the exterior wall. Therefore, no adverse fixed noise impact from industrial activities or chiller plant are anticipated from both World Wide Industrial Building and Fung Yu Industrial Building.
- 2.4.2.3 For the potential fixed noise impact arising from the garages and the workshops for car repair, motor services (e.g. car washing and car painting) and metal alloys (S1 to S10), no night-time (23:00 – 07:00) operation were observed during the site visits. It is also confirmed with the workers of the garages and workshops that there is no night-time operation. Therefore, no adverse night-time fixed noise impact arising from the above fixed noise sources is anticipated.
- 2.4.2.4 Potential noise impact from manual and tool/machine assisted operations at source during day-time (07:00 – 23:00) would be highly concerned. However, no operating noisy activity was observed during the site visits, therefore, no on-site noise measurement was conducted to obtain representative noise data. In order to evaluate the fixed noise impact arising from S2 to S7, S9 and S10 to the proposed development, the Sound Power Levels (SWLs) of those noise sources are referenced from the "Noise Impact Assessment for Proposed Comprehensive Residential Development with "Shop and Services" and "Eating Place" in "Comprehensive Development Area (2)" Zone at KIL 10578, 7628 and 7626 in Ma Tau Kok" Report (refer to as NIA-MTK Report) where the study area is near to the Subject Site in same district while the studied noise sources are of similar size. Major activities of motor service workshops involves car-repairing. According to the NIA-MTK Report as shown in Appendix 2.5, the relevant SWL of similar motor service workshop calculated based on the on-site noise measurement is 91.7 dB which had already captured all of the operation activities such as Hand-held Pneumatic Tools, Vehicle lifting machine, hammering, grinding, etc.
- 2.4.2.5 Compared to motor service workshops in NIA-MTK Report, motor service workshops in the vicinity of Subject Site are relatively smaller. According to site survey, the major activities include car washing and car painting. Limited hammering works were observed. Considered that no tonal, intermittency were observed in NIA-MTK Report, no impulsive noise from garages in the vicinity of Subject Site, which are of relatively smaller size and involves less noisy activities, are expected. Therefore, it is considered that the adoption of a noise level for larger garages involving noisier activities from NIA-MTK Report is a conservative approach.

2.4.3 Noise Sensitive Receivers

2.4.3.1 Representative Noise Sensitive Receivers (NSRs) (NA-M1, NA-J1, NA-G2, NC-A3, NC-D1 and NC-E1) of residential towers nearest to the identified noise sources have been selected for the assessment. **Figure 2.4** shows the locations of the representative NSRs.

2.4.4 Assessment Criteria

2.4.4.1 The Subject Site is located in an urban area and surrounded by To Kwa Wan Road to the northwest, Ngan Hon Street to the northeast, Wing Kong Street to the southeast and Hung Fook Street to the southwest. In accordance with the *Technical Memorandum for the Assessment of Noise from Places other than Domestic Premises, Public Places or Construction Sites* (herein referred as TM), the type of the area where the Subject Site located is considered to be "Type (iii) Urban Area". Moreover, Ma Tau Wai Road (from Chi Kiang Street to Tin Kwong Road) and Chatham Road Flyover (from Ma Tau Kok to Chatham Road North) are in close proximity to the Subject Site, which consist of Annual Average Daily Traffic (AADT) of 36,770 and 77,910 respectively according to "The Annual Traffic Census 2014" published by Transport Department, are considered as Influencing Factors (IFs) that can affect the proposed development. As a result, the area sensitive rating (ASR) "B" or "C" is adopted as the assessment criteria depended on whether the selected NSR at such a location that noise generated by the IF is noticeable at the NSR according to the MLP. According to the observation during the site survey, there are different kinds of noise sources identified (such as road traffic noise from the surrounding roads with heavy traffic flows, fixed noise sources, etc.) and the proposed redevelopment project is in an urban area. Therefore, it is anticipated that the prevailing background noise levels should be higher than ANLs. Therefore, ANLs have been adopted as the noise criteria. **Table 2.1** below shows the details of the fixed noise impact assessment criteria for this study. It should be noted that the signed ASRs are for assessment purposed only and not to affect the Authority's discretion on the enforcement based on the temporary conditions.

Table 2.1 Industrial Noise Standard

Standards	NSRs	ASRs [1]	Criteria	Acceptable Noise Level (ANL) [2]	Noise Criteria in Current Assessment Leq (30mins), dB(A)
NCO	NA-M1, NA-J1, NA-G2, NC-A3, NC-D1	C	Leq (30 mins) (07:00 – 23:00)	70 dB(A)	70 dB(A)
			Leq (30 mins) (23:00 – 07:00)	60 dB(A)	60 dB(A)
	NC-E1	B	Leq (30 mins) (07:00 – 23:00)	65 dB(A)	65 dB(A)
			Leq (30 mins) (23:00 – 07:00)	55 dB(A)	55 dB(A)

Notes:

[1] Acceptable Noise Level for Area Sensitivity Ratings "B" & "C" stipulated in the Technical Memorandum for the Assessment from Places other than Domestic Premises, Public Places or Construction Sites. Under the "Urban Area" type of the area containing NSR, an ASR of "B" is assigned to the NSR "Not Affected" by IF, while an ASR of "C" is assigned to the NSR "Directly Affected" or "Indirectly Affected" by IF.

[2] The signed ASRs are for assessment purposed only and not to affect the Authority's discretion on the enforcement based on the temporary conditions.

- 2.4.4.2 Chiller and cooling tower of the central air-conditioning system for commercial and retail podium within the Subject Site are proposed in an enclosed indoor environment. Therefore, any potential fixed noise generated from these equipment would not directly affect the proposed redevelopment project or the surrounding NSRs. The inlet/outlet of the system would be equipped with acoustic louvre to eliminate any potential fixed noise impact from the system to the proposed redevelopment project or the surrounding NSRs.

2.4.5 Assessment Methodology

- 2.4.5.1 Standard acoustical principles were adopted for prediction of cumulative industrial noise impact. In accordance with the "Technical Memorandum On Noise From Construction Work Other Than Percussive Piling", a - 10dB shielding correction was adopted where the line of sight from the representative NSR would be completely blocked by buildings or barriers. A façade correction of + 3dB was assumed.

- 2.4.5.2 The fixed noise sources are mainly from the activities of garages, workshops for car repair, car washing and car painting and workshops for metal alloys. Since the activities should be free of the characteristics of tonality, no corrections for tonality was assumed. Moreover, as mentioned in S.2.4.2.3, there is no night-time operation from the identified fixed noise sources. Therefore, no correction of intermittency was applied for the calculations.

2.4.6 Assessment Result

- 2.4.6.1 A summary of predicted fixed noise levels during day time at selected NSRs is tabulated below in Table 2.2. Detailed calculations are shown in Appendix 2.6.

Table 2.2 Predicted Industrial Noise Level

Representative Noise Sensitive Receiver	Noise Criteria in Current Assessment, Leq (30mins), dB(A)	Predicted Noise Level (Day-time), Leq (30mins), dB(A)	Compliance
NA-M1	70	59	Yes
NA-J1	70	60	Yes
NA-G2	70	61	Yes
NC-A3	70	68	Yes
NC-D1	70	69	Yes
NC-E1	65	65	Yes

- 2.4.6.2 The assessment results indicate that the maximum predicted noise level for those selected NSRs with ASR of "C" is Leq(30 min) 68 dB(A) for day-time, which complies with the 70 dB(A) day-

Table 2 Hydraulic Capacity at Existing Sewers at Hung Fook Street / Ngan Hon Street, Kowloon City

Segment	Manhole Reference	Manhole Reference	Pipe Dia. mm	Pipe Length m	Invert Level 1 mPD	Invert Level 2 mPD	g m/s	k _s m	s 1/in	s 1/in	v m/s	V m/s	Area m ²	Q m ³ /s	Estimated Capacity L/s
S1-S2	FMH4024355	FMH4024356	300	9.0	3.20	3.15	9.81	0.006	180	0.006	0.000001	0.82	0.07	0.06	58
S2-S3	FMH4024356	FMH4024357	525	28.9	3.15	3.13	9.81	0.006	1443	0.001	0.000001	0.42	0.22	0.09	92
S3-S4	FMH4024357	FMH4024358	525	7.4	3.13	3.12	9.81	0.006	739	0.001	0.000001	0.59	0.22	0.13	128
S4-S5	FMH4024358	FMH4024359	525	12.6	3.12	3.10	9.81	0.006	630	0.002	0.000001	0.64	0.22	0.14	139
S5-S6	FMH4024359	FMH4025890	525	52.7	3.10	2.94	9.81	0.006	329	0.003	0.000001	0.89	0.22	0.19	192
S6-S7	FMH4025890	FMH4025891	525	23.4	2.94	2.74	9.81	0.006	117	0.009	0.000001	1.49	0.22	0.32	322
S7-S8	FMH4025891	FMH4025889	525	8.4	1.86	1.44	9.81	0.006	20	0.050	0.000001	3.61	0.22	0.78	782
S8-S9	FMH4025889	FMH4025769	525	53.1	1.31	1.00	9.81	0.006	171	0.006	0.000001	1.23	0.22	0.27	266

Notes (highlighted in red):

- (1) According to DSD Drainage Records, the incoming level of Manhole S2 (FMH4024356) is missing. Therefore the outgoing level of 3.15mPD is used instead.
- (2) According to DSD Drainage Records, both invert levels of Manhole S3 (FMH4024357) are listed as 3.17mPD. This would cause an upward slope from Manhole S2 and is disregarded. Therefore, they are amended to align with the downstream sewer pipes.
- (3) According to DSD Drainage Records, the invert levels of Manhole S4 (FMH4024358) and Manhole S5 (FMH4024359) are identical, at 3.12mPD. This would cause flat gradient and is disregarded. Therefore, the invert levels of Manhole S5 have been adjusted to align with Manhole S4 (from upstream) and Manhole S6 (to downstream).

Remarks: (1) g=gravitational acceleration; k_s=equivalent sand roughness; s=gradient; v=kinematic viscosity of water; V=mean velocity

(2) The value of k_s = 6mm is used for the calculation of concrete sewer (based on Table 5: Recommended roughness values in Sewerage Manual)

(3) The value of velocity (V) is referred to the Tables for the hydraulic design of pipes, sewers and channels (8th edition)

(4) Equation used:
$$v = \sqrt{(8gDs)} \log \left(\frac{k_s}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}} \right)$$

Table 4 Hydraulic Capacity of Existing Sewers for Sewerage generated from the Proposed Development only

Segment	Pipe Dia. (mm)	Pipe Length (m)	Gradient	Estimated Capacity (L/s)	Estimated Flow including the Proposed Development only (L/s)	Contributed by the Proposed Development only (%)	Status	Estimated Flow including the Proposed Development and Catchment Areas (L/s)	Contributed by the Proposed Development and Catchment Areas (%)	Status
S1-S2	300	9.0	0.0056	58	46.6	80.5%	OK	50.6	87.4%	OK
S2-S3	525	28.9	0.0007	92	46.6	50.9%	OK	56.6	61.8%	OK
S3-S4	525	7.4	0.0014	128	46.6	36.4%	OK	65.7	51.4%	OK
S4-S5	525	12.6	0.0016	139	46.6	33.6%	OK	65.7	47.4%	OK
S5-S6	525	52.7	0.0030	192	46.6	24.3%	OK	65.7	34.2%	OK
S6-S7	525	23.4	0.0085	322	46.6	14.5%	OK	65.7	20.4%	OK
S7-S8	525	8.4	0.0502	782	46.6	6.0%	OK	65.7	8.4%	OK
S8-S9	525	53.1	0.0058	266	46.6	17.5%	OK	90.0	33.8%	OK

市區重建局 鴻福街／銀漢街發展計劃 (KC-010)

鴻福街／啟明街發展項目 (KC-011) 榮光街發展項目 (KC-012)

(九龍城房建會文件第46/16號)

關注鴻福街、榮光街、銀漢街收購計劃 強烈要求儘快落實土瓜灣全區舊樓重建
(九龍城房建會文件第49/16號)

12. 市區重建局規劃及設計高級經理關以輝先生向委員介紹文件第46/16號。

13. 林博議員表示歡迎市區重建局(下文簡稱「市建局」)的重建項目，並提出以下查詢：(一) 若已向城規會呈交的KC-010發展計劃草圖不獲批核，局方將採取怎樣的應對措施；(二) 垂詢上述3個項目已接獲的反對意見數目，及局方是否有足夠信心成功展開有關項目；(三) 垂詢局方將如何協助業主應對相關部門發出的清拆令、消防令及解決「釘契」等問題，建議屋宇署及市建局酌情處理，派員到相關樓宇進行檢測，解除不會構成即時危險的維修令，並建議參考「樓宇更新大行動」的安排，由屋宇署直接安排承建商為有即時維修需要的樓宇進行維修；(四) 垂詢局方為何不收購在上述重建項目的周邊樓宇，包括愛華大廈、榮光街72-94號、土瓜灣道及馬頭圍道一帶樓齡高及已失修的唐樓，提供釐定重建項目優先次序的準則，及盡快落實各項重建項目的時間表；以及(五)希望局方在錄得45元億盈餘的情況下，增撥資源及落實整區的重建計劃。

14. 楊永杰議員表示鴻福街及啟明街為掘頭路及附近車房林立，認為市建局以「小區發展模式」開展KC-010/KC-011/KC-012項目不能有效改善區內的交通問題。他又對市建局的落實重建計劃的誠意表示懷疑，指出有關項目並無一併收購其周邊的樓宇，引致居民不滿，故建議局方一併收購愛華大廈、馬頭圍道、鴻福街及啟明街一帶的周邊樓宇作整體規劃，以徹底改善區內交通。

15. 李慧琼議員提出以下意見/查詢：(一) 局方公佈上述項目後，區內的榮光街72-94、96-118號及銀漢街44-45號法團已致函局方要求被納入重建範圍內，她提醒受影響的住戶必須以書面方式表達對重建項目的意見；(二) 若KC-010項目未能獲得城規會批准，局方將如何應對；(三) 相關部門將如何協助有需要的業主，申請擱置或暫緩處理由不同部門發出的維修令或消防令；(四) 部份業主雖然已盡力希望遵從屋宇署的維修命令，但卻無法召集足夠業權進行維修，故希望部門能夠承諾不起訴這些已作出努力的業主；(五) 居民有何上訴途徑對已進行的凍結人口調查結果表達不滿；(六) 垂詢居住在重建範圍內及被無理迫遷的租客將有何投訴途徑，及有關僱建商舖的權益；(七) 局方在未來應優先進行土瓜灣五街及十三街一帶的重建；以及(八) 垂詢局方於KC-010項目擬建的社區停車場總面積。

16. 勞超傑議員表示居民的意見與市建局提出的收購條款及範圍具互動性，若局方不檢討項目的收購條款及規劃範圍，將導致居民提出反對，令重建項目無法順利開展，故希望局方積極回應市民的意見及訴求。此外，他認為屋宇署除了負責發出命令及作出檢控外，亦應提供具體措施協助居民暫緩或遵辦法定命令。

17. 楊振宇議員表示歡迎局方提出的重建項目，並希望能盡快在土瓜灣五街及十三街一帶進行重建。他又指出局方未有將與項目周邊狀況相約的樓宇納入重建範圍內，影響整體規劃的協調性。此外，他要求局方詳述「小區發展模式」將如何改善區內規劃及交通。

18. 關浩洋議員垂詢KC-010項目中停車場除了讓私家車停泊外，會否因應本區的需要提供大型旅遊巴停泊位。

19. 市區重建局關以輝先生作綜合回應，重點如下：

- 在揀選重建範圍及項目方面，市建局會考慮樓宇狀況、樓齡、業權分佈、私人重建的可能性及市建局的能力等一籃子因素。
- 市民可於項目的諮詢期內以書面形式，要求被納入或剔除於重建範圍，或提出其他意見。KC-010項目的書面意見可向城規會提交，而KC-011/KC-012的書面意見則可向市建局提交。若上述項目附近樓宇的居民希望被納入重建範圍，需按法定要求於諮詢期內向市建局或城規會提出反對意見，他們的相關要求才會獲得考慮。然而，若當局收到太多不具廣泛代表性的意見，項目的批核程序可能需要延長。
- 無論是發展計劃或發展項目，若最終不獲通過，重建會被中止。市建局會研究有關原因，考慮是否修改項目。
- KC-010項目的停車場設於地庫，提供大約270個車位，實際的數目仍需待政府審批，主要用作服務KC-010/KC-011/KC-012/DL-8及附近的重建項目。市建局亦已應運輸署的要求，於停車場內設置最多4個旅遊巴車位。此外，KC-009項目內將另設其停車場。
- 市建局希望與區議會緊密合作，而市建局在區內的工作並未完結。

20. 市區重建局收購及遷置高級經理譚建強先生補充，重點如下：

- 重建項目需經過長時間的規劃程序，業主於期間仍有責任保養及維修他們的樓宇。市建局建議業主在未正式出售有關物業前，需遵從相關部門發出的法定命

令。然而，若業主在執行命令的7年內向市建局出售有關物業，市建局會根據其「發還修葺費用計劃」，在計算折舊後，向業主發還特定維修項目的合理開支。而若業主在出售物業前仍未執行法定命令，市建局會於收購價中扣起相應修葺費用作抵押金，以備有需要時完成有關法定命令的維修項目。

- 項目凍結人口調查三日的正式登記已經完成，隨後並正進行後補登記工作，若住戶認為已提交的資料有誤，或希望作出更正或提供補充資料，可以書面向市建局提出。市建局會將有關資料存檔，直至重建項目落實推行後才會正式核實已登記用戶的資料，及釐定收購基準。
- 市建局在收購過程中，會根據業權提出收購補償，而沒有業權的僭建物則不會獲得任何補償。
- 因重建被無理迫遷的租戶可聯絡市建局或由「市區更新基金」聘任的社工隊，要求提供協助。


21. 屋宇署高級屋宇測量師/E3嚴婉玲女士回覆，表示會於稍後向委員提供屋宇署將如何處理在市建局的收購項目中已發出的命令的補充資料。然而，她指出若有關樓宇構成即時危險，署方會即時採取執法行動，以確保公眾安全。

(會後補注：屋宇署於會議後提供以下補充資料：市建局於2016年3月刊憲公布庇利街／榮光街發展項目及於2016年6月刊憲公佈鴻福街／銀漢街發展計劃，鴻福街／啟明街發展項目及榮光街發展項目，根據本署紀錄，上述重建項目內的建築物仍有多張清拆命令及維修命令仍未遵從，本署會因應個別情況考慮暫緩執法，不過，就那些對生命財產明顯構成迫切危險、新建、嚴重危害健康及造成嚴重環境滋擾的僭建物，以及嚴重失修的樓宇，本署會繼續跟進，業主亦有責任妥善維修及保養其樓宇，以確保公眾安全。)

NOTATION 圖例

BOUNDARY OF DEVELOPMENT.....◆◆.....發展計劃範圍界線
SCHEME

RESIDENTIAL (GROUP A)8.....住宅(甲類)8

MAXIMUM BUILDING HEIGHT 100  最高建築物高度
(在水平基準上100米)
(100 METRES ABOVE PRINCIPAL DATUM)

夾附的<註釋>屬這份圖則的一部分

香港城市規劃委員會依據城市規劃條例擬備的市區重建局鴻福街／銀業街發展計劃圖則
TOWN PLANNING ORDINANCE, HONG KONG TOWN PLANNING BOARD
URBAN RENEWAL AUTHORITY HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN

依據市區重建局條例第25(3)(a)條擬備
PREPARED UNDER SECTION 25(3)(a) OF THE
URBAN RENEWAL AUTHORITY ORDINANCE

PLAN No. S/K9/URA2/A

2016年 月 日城市規劃委員會根據《城市規劃條例》第56(a)條及《土地法》第161條，對2016年 月1日生效的《土地法》第161條作出修訂，以符合《土地法》第161條的規定。

SECRETARY, TOWN PLANNING BOARD 城市規劃委員會秘書

SCALE	1:1000	比例尺
METRES 米	0 10 20 30 40 50	METRES 米

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the Purposes of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

NOTES

(N.B. These form part of the Plan)

- (1) These Notes show the uses or developments on land falling within the boundaries of the Plan which are always permitted and which may be permitted by the Town Planning Board, with or without conditions, on application. Where permission from the Town Planning Board for a use or development is required, the application for such permission should be made in a prescribed form. The application shall be addressed to the Secretary of the Town Planning Board, from whom the prescribed application form may be obtained.
- (2) Any use or development which is always permitted or may be permitted in accordance with these Notes must also conform to any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, as may be applicable.
- (3)
 - (a) No action is required to make the existing use of any land or building conform to this Plan until there is a material change of use or the building is redeveloped.
 - (b) Any material change of use or any other development (except minor alteration and/or modification to the development of the land or building in respect of the existing use which is always permitted) or redevelopment must be always permitted in terms of the Plan or, if permission is required, in accordance with the permission granted by the Town Planning Board.
 - (c) For the purposes of subparagraph (a) above, “existing use of any land or building” means –
 - (i) before the publication in the Gazette of the notice of the first statutory plan covering the land or building (hereafter referred as ‘the first plan’),
 - a use in existence before the publication of the first plan which has continued since it came into existence; or

- a use or a change of use approved under the Buildings Ordinance which relates to an existing building; and
- (ii) after the publication of the first plan,
- a use permitted under a plan which was effected during the effective period of that plan and has continued since it was effected; or
 - a use or a change of use approved under the Buildings Ordinance which relates to an existing building and permitted under a plan prevailing at the time when the use or change of use was approved.
- (4) Except as otherwise specified by the Town Planning Board, when a use or material change of use is effected or a development or redevelopment is undertaken, as always permitted in terms of the Plan or in accordance with a permission granted by the Town Planning Board, all permissions granted by the Town Planning Board in respect of the site of the use or material change of use or development or redevelopment shall lapse.
- (5) Road widths, road junctions and alignments of roads may be subject to minor adjustments as detailed planning proceeds.
- (6) Temporary uses (expected to be 5 years or less) of any land or building are always permitted as long as they comply with any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, and there is no need for these to conform to the zoned use or these Notes. For temporary uses expected to be over 5 years, the uses must conform to the zoned use or these Notes.
- (7) The following uses or developments are always permitted on land falling within the boundaries of the Plan except where the uses or developments are specified in Column 2 of the Schedule of Uses:
- (a) provision, maintenance or repair of plant nursery, amenity planting, open space, rain shelter, refreshment kiosk, road, bus/public light bus stop or lay-by, cycle track, Mass Transit Railway station entrance, Mass Transit Railway structure below ground level, taxi rank, nullah, public utility pipeline, electricity mast, lamp pole, telephone booth, telecommunications radio base station, automatic teller machine and shrine; and
 - (b) geotechnical works, local public works, road works, sewerage works, drainage works, environmental improvement works, marine related facilities, waterworks (excluding works on service reservoir) and such other public works co-ordinated or implemented by Government.

S/K9/URA2/A

- (8) Unless otherwise specified, all building, engineering and other operations incidental to and all uses directly related and ancillary to the permitted uses and developments within the same zone are always permitted and no separate permission is required.
- (9) In these Notes, "existing building" means a building, including a structure, which is physically existing and is in compliance with any relevant legislation and the conditions of the Government lease concerned.
- (10) Any development not compatible with the Urban Renewal Authority's Development Scheme for the area is prohibited by virtue of section 25(4) of the Urban Renewal Authority Ordinance.

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

Schedule of Uses

	<u>Page</u>
RESIDENTIAL (GROUP A) 8	1

RESIDENTIAL (GROUP A) 8

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Ambulance Depot	Commercial Bathhouse/ Massage
Flat	Establishment
Government Use (not elsewhere specified)	Eating Place
House	Education Institution
Library	Exhibition or Convention Hall
Market	Government Refuse Collection Point
Place of Recreation, Sports or Culture	Hospital
Public Clinic	Hotel
Public Transport Terminus or Station (excluding open-air terminus or station)	Institutional Use (not elsewhere specified)
Public Vehicle Park (coach parking only)	Mass Transit Railway Vent Shaft and/or Other Structure above Ground Level other than Entrances
Residential Institution	Office
School (in free-standing purpose-designed building only)	Petrol Filling Station
Social Welfare Facility	Place of Entertainment
Utility Installation for Private Project	Private Club
	Public Convenience
	Public Transport Terminus or Station (not elsewhere specified)
	Public Utility Installation
	Public Vehicle Park (excluding container vehicle) (not elsewhere specified)
	Religious Institution
	School (not elsewhere specified)
	Shop and Services
	Training Centre

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

In addition, the following uses are always permitted (a) on the lowest three floors of a building, taken to include basements; or (b) in the purpose-designed non-residential portion of an existing building, both excluding floors containing wholly or mainly car parking, loading / unloading bay and / or plant room:

Eating Place
Educational Institution
Institutional Use (not elsewhere specified)
Off-course Betting Centre
Office
Place of Entertainment
Private Club
Public Convenience
Recyclable Collection Centre
School
Shop and Services
Training Centre

Planning Intention

This zone is intended primarily for high-density residential developments with the provision of an underground car park, that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments commenced by the Urban Renewal Authority within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

Remarks

- (1) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in the plot ratio for the building upon development and/ or redevelopment in excess of 7.5 for a domestic building or 9.0 for a building that is partly domestic and partly non-domestic, or the plot ratio of the existing building, whichever is the greater. Except where the plot ratio is permitted to be exceed under paragraphs (8) and/or (9) hereof, under no circumstances shall the plot ratio for the domestic part of any building, to which this paragraph applies, exceed 7.5.

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

Remarks (Cont'd)

- (2) For a non-domestic building to be erected on the site, the maximum plot ratio shall not exceed 9.0 except where the plot ratio is permitted to be exceeded under paragraphs (8) and/or (9) hereof.
- (3) For the purposes of paragraph (1) above, no addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of the relevant maximum domestic and/or non-domestic plot ratio(s), or the domestic and/or non-domestic plot ratio(s) of the existing building, whichever is the greater, subject to, as applicable -
 - (i) the plot ratio(s) of the existing building shall apply only if any addition, alteration and/or modification to or redevelopment of an existing building is for the same type of building as the existing building, i.e. domestic, non-domestic, or partly domestic and partly non-domestic building; or
 - (ii) the maximum domestic and/or non-domestic plot ratio(s) stated in paragraph (1) above shall apply if any addition, alteration and/or modification to or redevelopment of an existing building is not for the same type of building as the existing building, i.e. domestic, non-domestic, or partly domestic and partly non-domestic building.
- (4) In determining the relevant maximum plot ratio for the purposes of paragraphs (1) and (2), area of any part of the site that is occupied or intended to be occupied by free-standing purpose-designed buildings (including both developed on ground and on podium level) solely for accommodating Government, institution or community facilities including school(s) as may be required by Government shall be deducted in calculating the relevant site area.
- (5) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of the maximum building height in terms of metres above Principal Datum (mPD) as stipulated on the Plan, or the height of the existing building, whichever is the greater.

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

Remarks (Cont'd)

- (6) In determining the relevant maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for use solely as plant room, caretaker's office, or caretaker's quarters and recreational facilities for the use and benefit of all the owners or occupiers of the domestic building or domestic part of the building, provided such uses and facilities are ancillary and directly related to the development or redevelopment, may be disregarded. An underground car park shall be provided. The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by government. In determining the maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for the use solely as the underground car park may be disregarded.
- (7) Where the permitted plot ratio as defined in Building (Planning) Regulations is permitted to be exceeded in circumstances as set out in Regulation 22(1) or (2) of the said Regulations, the plot ratio for the building on land to which paragraphs (1) or (2) above applies may be increased by the additional plot ratio by which the permitted plot ratio is permitted to be exceeded under and in accordance with the said Regulation 22(1) or (2), notwithstanding that the relevant maximum plot ratio specified in the paragraphs (1) or (2) above may thereby be exceeded.
- (8) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the plot ratio and building height restrictions as stated in paragraphs (1), (2) and (5) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

EXPLANATORY STATEMENT

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

	<u>Contents</u>	<u>Page</u>
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2.	AUTHORITY FOR THE PLAN AND PROCEDURES	1
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4.	NOTES OF THE PLAN	3
5.	AREA COVERED BY THE PLAN	3
6.	EXISTING CONDITIONS	3
7.	PLANNING AND LAND USE PROPOSALS	4
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DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET/NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the Purpose of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

EXPLANATORY STATEMENT

Note: For the purposes of the Town Planning Ordinance, this statement shall not be deemed to constitute a part of the Plan.

1. INTRODUCTION

This explanatory statement is intended to assist an understanding of the draft Urban Renewal Authority (URA) Hung Fook Street/Ngan Hon Street Development Scheme Plan (the Plan) No. S/K9/URA2/A. It reflects the planning intention and objectives of the Town Planning Board (the Board) for the area covered by the Plan.

2. AUTHORITY FOR THE PLAN AND PROCEDURES

2.1 In the URA's 15th Business Plan (2016/17) approved by the Financial Secretary in early 2016, Hung Fook Street/Ngan Hon Street Development Scheme (KC-010) was proposed to be processed as a Development Scheme under section 25 of the URA Ordinance (URAO).

2.2 On 3 June 2016, pursuant to section 23(1) of the URAO, the URA notified in the Government Gazette the commencement of implementation of the Hung Fook Street/Ngan Hon Street Development Scheme.

- 2.3 On 10 June 2016, the URA submitted the draft URA Hung Fook Street/Ngan Hon Street Development Scheme Plan for the Development Scheme to the Board under section 25(5) of the URAO.
- 2.4 On XXXX, the Board, under section 25(6)(a) of the URAO, deemed the draft URA Hung Fook Street/Ngan Hon Street Development Scheme Plan as being suitable for publication. Under section 25(7) of the URAO, the draft Development Scheme Plan, which the Board has deemed suitable for publication under section 25(6)(a) of the URAO, is deemed to be a draft plan prepared by the Board for the purposes of the Town Planning Ordinance (the Ordinance).
- 2.5 On XXXX, the draft Hung Fook Street/Ngan Hon Street Development Scheme Plan No. S/K9/URA2/1 (the Plan) was exhibited under section 5 of the Ordinance. By virtue of section 25(9) of the URAO, the Plan has from the date replaced the draft Hung Hom Outline Zoning Plan (OZP) No. S/K9/25 in respect of the area delineated and described herein.

3. OBJECT OF THE PLAN

The Plan illustrates that the Development Scheme Area (the Area) is designated as “Residential (Group A)8” (“R(A)8”). It is planned to be developed by means of the Development Scheme prepared under section 25 of the URAO. The Scheme intends to be primarily for high-density residential developments with the provision of an underground car park that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments commenced by URA within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street (the Wider Area) as demarcated on **Plan 1** as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

4. NOTES OF THE PLAN

- 4.1 Attached to the Plan is a set of Notes which shows the types of uses or developments which are always permitted within the Area in this zone and which may be permitted by the Board, with or without conditions, on application. The provision for application for planning permission under section 16 of the Ordinance allows greater flexibility in land use planning and control of development to meet changing needs.
- 4.2 For the guidance of the general public, a set of definitions that explains some of the terms used in the Notes may be obtained from the Technical Services Division of the Planning Department and can be downloaded from the Board's website at <http://www.info.gov.hk/tpb>.

5. AREA COVERED BY THE PLAN

- 5.1 The Scheme boundary which is shown in heavy broken line on the Plan, covers a total area of about 4,951m². The Scheme comprises tenement buildings which are bounded by Ngan Hon Street to the north, Wing Kwong Street to the east, Hung Fook Street to the south and a back lane of the adjacent buildings, nos. 17-31 To Kwa Wan Road, to the west.
- 5.2 The Scheme boundary has included Government lane and pavement area.
- 5.3 On the approved Hung Hom OZP No. S/K9/24, the Area is zoned "Residential (Group A)" before the exhibition of the Plan.

6. EXISTING CONDITIONS

- 6.1 The buildings within the Area are between 6 and 8 storeys and residential in nature with construction material retail shops, eating places, workshops and vehicle repair workshops on lower floors. Most of the existing buildings are in deteriorating or poor condition with

some sub-divided units on the upper floors. Staircase shops are found at the entrance of staircase in some buildings at ground floor.

- 6.2 The poor housing condition, the presence of illegal rooftop structures, temporary structures at back lanes, and a number of vehicle repair workshops at ground floor are sources of environmental nuisance in the area.

7. PLANNING AND LAND USE PROPOSALS

- 7.1 On the Plan, the Area has been zoned "R(A)8" and the Notes of the Plan indicated broadly the intended land use within the Area.

Uses

- 7.2 Development or redevelopment within the "R(A)8" zone is intended for high-density residential development with commercial uses on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.
- 7.3 The maximum plot ratio within the "R(A)8" zone is 9.0, or the plot ratio of the existing building(s), whichever is the greater. Except where the plot ratio is permitted to be exceeded under the Notes of the Plan or under Building (Planning) Regulations 22(1) or (2), under no circumstances shall the plot ratio for the domestic part of any development exceed 7.5.
- 7.4 To provide design flexibility, minor relaxation of the building height and plot ratio restrictions may be considered by the Board on application under section 16 of the Ordinance taking into account its individual planning and design merits.

Car Park

- 7.5 An underground car park will be provided within the Development Scheme. It will accommodate car parking spaces and loading/unloading bays ancillary to developments and/or

redevelopments commenced by URA within the Wider Area to meet the parking needs and avoid car park run in/outs at individual developments in the Wider Area. Coach parking spaces will also be provided to cater for the demand for coach parking in the district.

- 7.6 Three URA Development Projects are located within the Wider Area, i.e., Kai Ming Street (DL-8:KC), Hung Fook Street/Kai Ming Street (KC-011) and Wing Kwong Street (KC-012) to the immediate south of the Development Scheme. KC-011 and KC-012 are commenced by the URA under section 26 of the URAO on the same day as the Development Scheme (**Plan 2**). Limited on-site loading/unloading bays will be provided at each of the Development Projects while car parking spaces will not be provided. The underground car park within the Development Scheme will accommodate the remaining number of loading/unloading bays and car parking spaces provision for the three Development Projects.
- 7.7 The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by the Government and in any event there shall be not more than 274 car parking spaces, 20 loading/unloading bays and 4 coach parking spaces. The maximum number of car parking space and loading/unloading bay provisions are based on the relevant requirements under the Hong Kong Planning Standards and Guidelines with reference to the total gross floor area (GFA) of the Development Scheme, developments and/or redevelopments commenced by URA within the Wider Area; i.e. about 104,000m² GFA and about 2,000 residential units. The number of coach parking spaces is as required by the Government to cater for the need for coach parking in the district.
- 7.8 Among the sites within the Wider Area to be served by the underground car park, the site area of the Development Scheme is relatively large and more efficient car park layout can be achieved. Provision of a single underground car park in the Development Scheme avoids disruption to through traffic movements and pedestrians while maintaining continuous street frontages on other sites in the Wider Area.

- 7.9 The underground car park is for accommodating car parking and loading/unloading bays ancillary to the development and/or redevelopment commenced by URA within the Wider Area and the coach parking spaces are to cater for such need in the district. As such, in determining the maximum plot ratio for the Development Scheme, any floor space that is constructed or intended for the use solely as the underground car park, may be disregarded. Management of the underground car park will be arranged at the development stage.

Local Road Improvement

- 7.10 Through road connecting Hung Fook Street and Ngan Hon Street within the Development Scheme, which in turn forms part of the proposed road network extending from Wan On Street of URA Bailey Street/Wing Kwong Street Development Project KC-009 (commenced on 4 March 2016), URA Hung Fook Street/Kai Ming Street Development Project KC-011 and Wing Kwong Street Development Project KC-012 (commenced on 3 June 2016), will improve the accessibility of the vicinity.
- 7.11 **Plan 2** shows the proposed road network system extending from Wan On Street to Ngan Hon Street. It passes through the sites of the Development Scheme and other redevelopment projects KC-009, KC-011 and KC-012. Except a road section of Wan On Street between Wing Kwong Street and Bailey Street is two-way carriageway, the road section between Wing Kwong Street and Ngan Hon Street is a one-way carriageway.
- 7.12 With the proposed road network system, egress traffic from Wing Kwong Street heading to Hung Hom Road and the east direction can exit directly via Wan On Street. It minimizes the detouring of the traffic at Ma Tau Wai Road/Wing Kwong Street junction and Ma Tau Wai Road/Bailey Street junction.
- 7.13 Ingress traffic from Bailey Street to Ngan Hon Street could take a route through the proposed network system from Wan On Street via its extension instead of detouring via Bailey Street eastbound, Sung On

Street northbound, Lok Shan Road Westbound, and To Kwa Wan Road southbound.

- 7.14 Extension of Wan On Street facilitates egress traffic to the north via Lung Tak Street to Chi Kiang Street instead of detouring via Bailey Street eastbound and Sung On Street northbound.
- 7.15 A section of extension of Wan On Street between Wing Kwong Street and Ngan Hon Street involves resumption of land and is implemented by Chapter 124 Lands Resumption Ordinance. URA will make an application for resumption within 12 months after the Development Scheme Plan prepared under the Ordinance has been approved by the Chief Executive in Council in accordance with section 9 of the Ordinance.

8. IMPLEMENTATION OF THE SCHEME

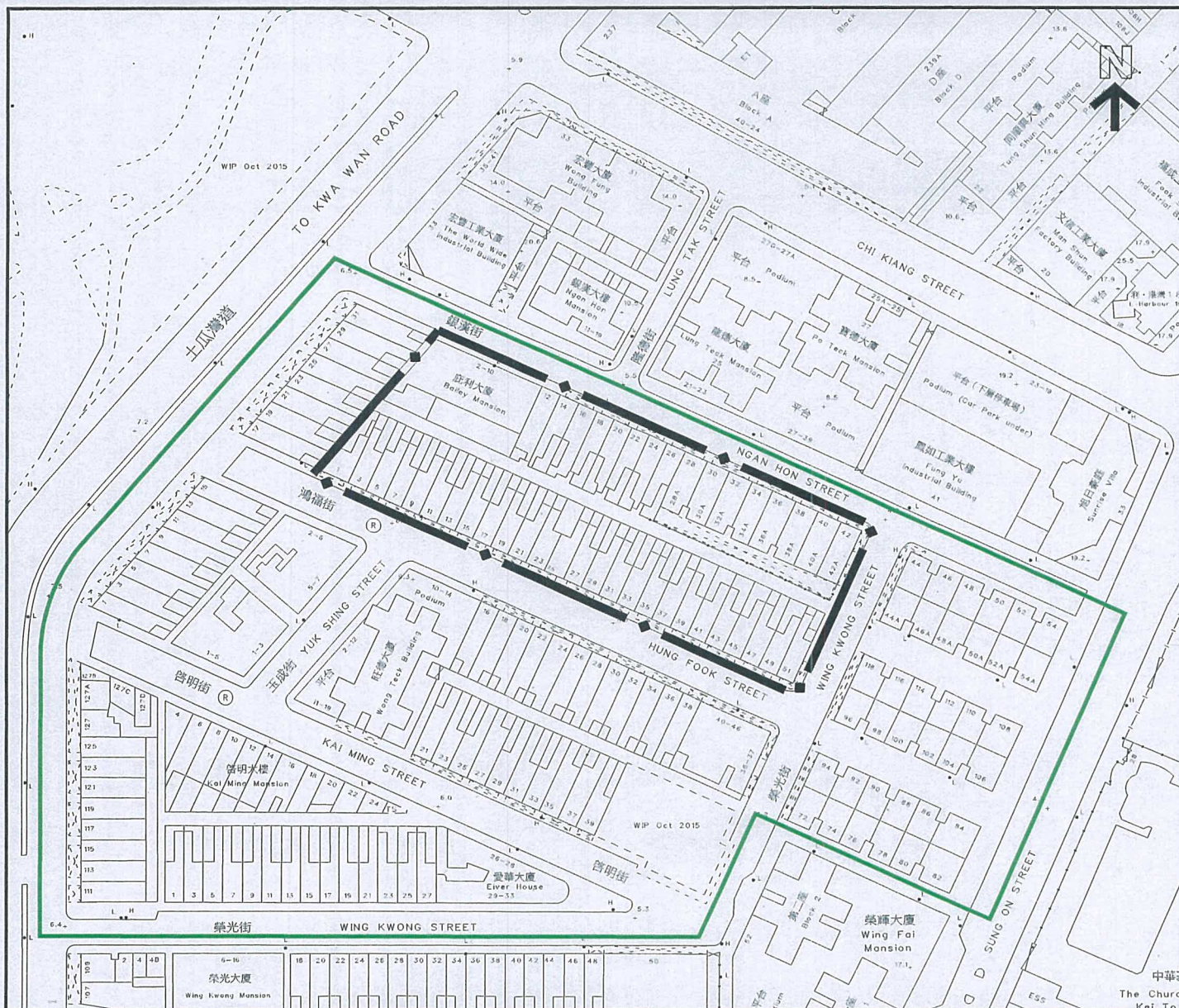
- 8.1 The proposals set out in the Plan form an integral part of the Development Scheme for the Area.
- 8.2 The URA does not own or lease any land within the boundaries of the Development Scheme. The URA intends to acquire the properties within the Area of the Development Scheme. With respect to any of such properties which cannot be acquired by purchase, the Secretary for Development would consider, upon the application of the URA, recommending to the Chief Executive in Council the resumption of properties under the Lands Resumption Ordinance, if necessary.
- 8.3 All eligible tenants will be offered an ex-gratia payment package in accordance with URA's policy. The URA has already entered into agreement with the Hong Kong Housing Society (HKHS) and the Hong Kong Housing Authority (HKHA) for the purpose of making available rehousing units by HKHS or HKHA to rehouse affected tenants who satisfy the eligibility criteria of HKHS or HKHA.
- 8.4 Non-domestic tenants of properties acquired by URA whose tenancies are terminated by URA due to implementation of the Development

Scheme may be offered an ex-gratia allowance to assist in their business relocation.

- 8.5 The URA may implement the Development Scheme on its own or in association with one or more joint venture partners.

TOWN PLANNING BOARD

XXXX 2016



圖例 NOTATION

BOUNDARY OF HUNG FOOK STREET/
NGAN HON STREET DEVELOPMENT
SCHEME

BOUNDARY OF WIDER AREA SERVED
BY UNDERGROUND CAR PARK

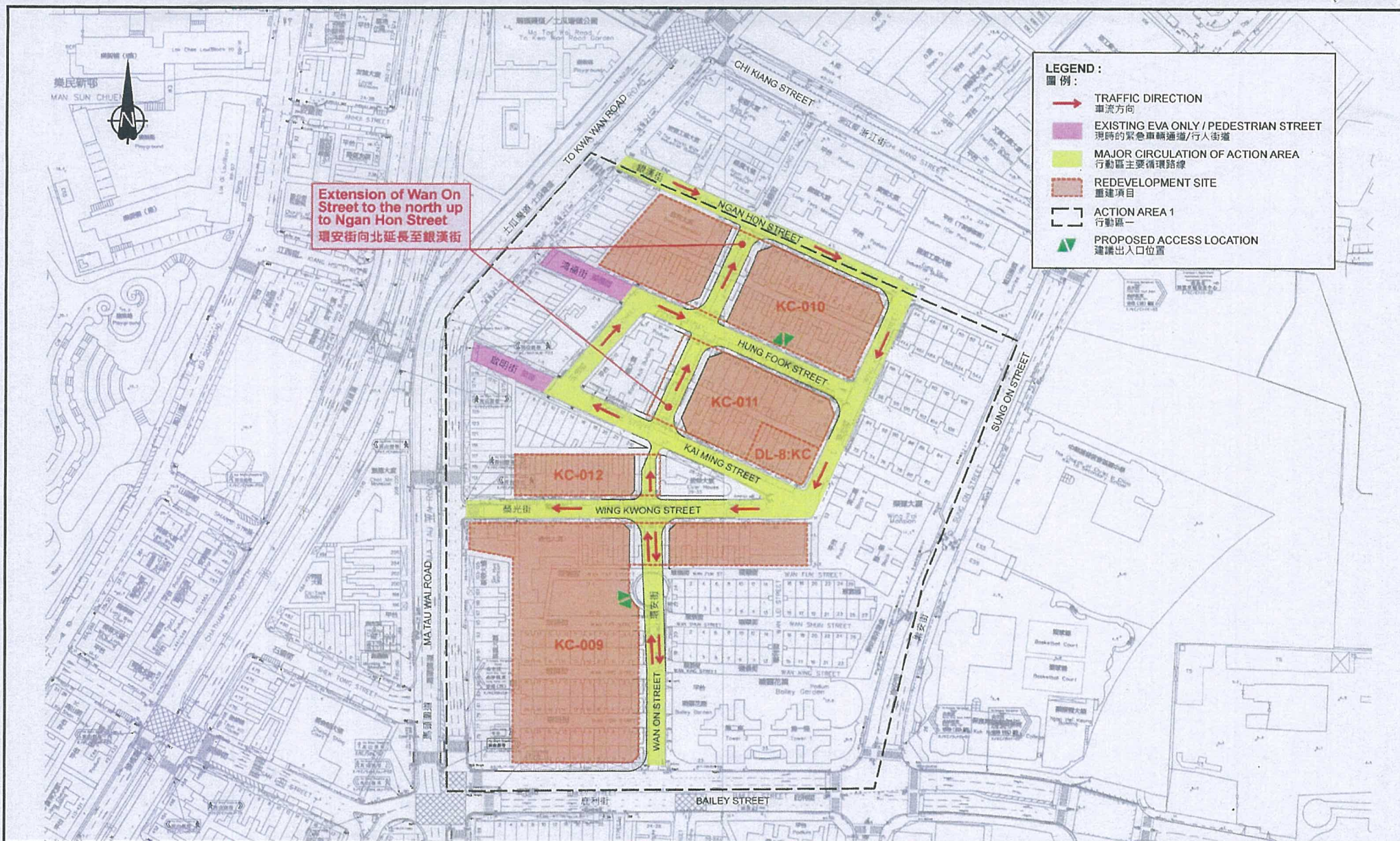
鴻福街/銀漢街
發展計劃範圍界線

地下停車場
提供服務的週邊
範圍界線

發展計劃內地下停車場提供服務的周邊範圍
WIDER AREA SERVED BY UNDERGROUND CAR PARK IN THE
DEVELOPMENT SCHEME

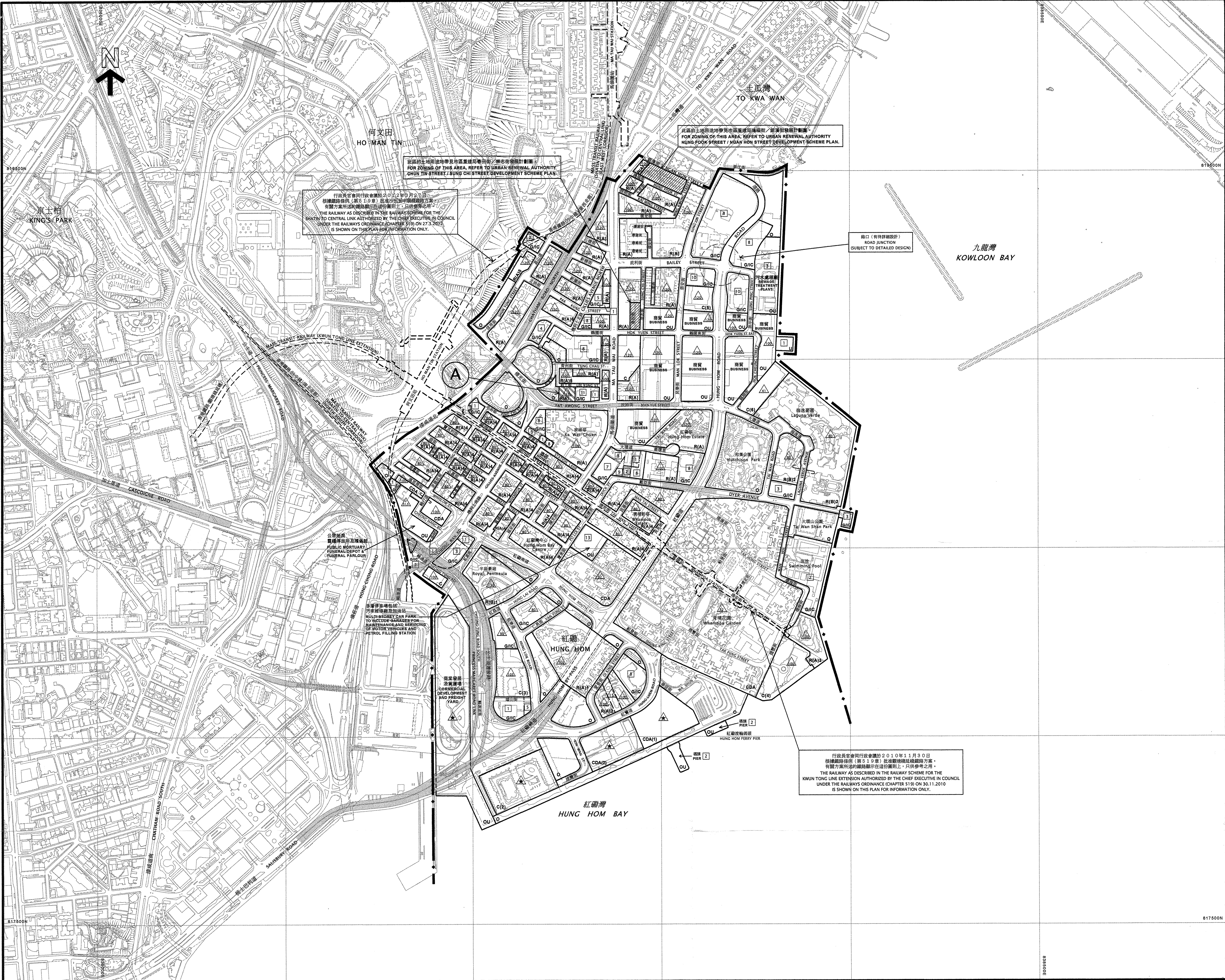


圖則編號
PLAN No. 1



擬議交通網絡
鴻福街/銀漢街發展計劃
PROPOSED TRAFFIC NETWORK

HUNG FOOK STREET / NGAN HON STREET DEVELOPMENT SCHEME



Annex H-1 of
TPB Paper No. 10184

圖例
NOTATION

ZONES

COMMERCIAL

COMPREHENSIVE DEVELOPMENT AREA

RESIDENTIAL (GROUP A)

RESIDENTIAL (GROUP B)

GOVERNMENT, INSTITUTION OR COMMUNITY

OPEN SPACE

OTHER SPECIFIED USES

UNDETERMINED

C

CDA

R(A)

R(B)

GIC

O

OU

U

地帶

商業

綜合發展區

住宅 (甲類)

住宅 (乙類)

政府、機構或社區

休憩用地

其他指定用途

未決定用途

COMMUNICATIONS

RAILWAY AND STATION
(UNDERGROUND)

MAJOR ROAD AND JUNCTION

ELEVATED ROAD

交通

鐵路及車站 (地下)

主要道路及路口

高架道路

MISCELLANEOUS

BOUNDARY OF PLANNING SCHEME

URBAN RENEWAL AUTHORITY
DEVELOPMENT SCHEME PLAN AREA

BUILDING HEIGHT CONTROL
ZONE BOUNDARY

MAXIMUM BUILDING HEIGHT
(IN METRES ABOVE PRINCIPAL DATUM)

MAXIMUM BUILDING HEIGHT
RESTRICTION AS STIPULATED ON THE NOTES

MAXIMUM BUILDING HEIGHT
(IN NUMBER OF STOREYS)

規劃範圍界線

市區重建局
發展計劃範圍

建築物高度管制區界線

最高建築物高度
(在主水平基準上若干米)

《註釋》內訂明最高建築物
高度限制

最高建築物高度
(樓層數目)

土地用途及面積一覽表
SCHEDULE OF USES AND AREAS

USES	大約面積及百分率 APPROXIMATE AREA & %		用途
	公頃 HECTARES	% 百分率	
COMMERCIAL	6.39	4.40	商業
COMPREHENSIVE DEVELOPMENT AREA	22.08	15.19	綜合發展區
RESIDENTIAL (GROUP A)	28.75	18.40	住宅 (甲類)
RESIDENTIAL (GROUP B)	6.63	4.56	住宅 (乙類)
GOVERNMENT, INSTITUTION OR COMMUNITY	12.58	8.65	政府、機構或社區
OPEN SPACE	13.40	9.22	休憩用地
OTHER SPECIFIED USES	15.23	10.48	其他指定用途
UNDETERMINED	0.17	0.12	未決定用途
MAJOR ROAD ETC.	41.48	28.53	主要道路等
URBAN RENEWAL AUTHORITY DEVELOPMENT SCHEME PLAN AREA	0.65	0.45	市區重建局 發展計劃範圍
TOTAL PLANNING SCHEME AREA	145.36	100.00	規劃範圍總面積

夾附的《註釋》屬這份圖則的一部分，
現經修訂並按照城市規劃條例第 5 條展示。
THE ATTACHED NOTES ALSO FORM PART OF THIS PLAN
AND HAVE BEEN AMENDED FOR EXHIBITION UNDER
SECTION 5 OF THE TOWN PLANNING ORDINANCE

核准圖編號 S / K 9 / 2 4 的修訂
AMENDMENT TO APPROVED PLAN No. S/K9/24

AMENDMENT EXHIBITED UNDER SECTION 5
OF THE TOWN PLANNING ORDINANCE

按照城市規劃條例第 5 條
展示的修訂

AMENDMENT ITEM A

修訂項目 A 項

(參看附表)
(SEE ATTACHED SCHEDULE)

香港城市規劃委員會依據城市規劃條例擬備的紅磡（九龍規劃區第 9 區）分區計劃大綱圖
TOWN PLANNING ORDINANCE, HONG KONG TOWN PLANNING BOARD
KOWLOON PLANNING AREA No. 9 - HUNG HOM - OUTLINE ZONING PLAN

SCALE 1:5000 比例尺
METRES 100 0 200 400 600 800 METRES 米

規劃署遵照城市規劃委員會指示擬備
PREPARED BY THE PLANNING DEPARTMENT UNDER
THE DIRECTION OF THE TOWN PLANNING BOARD

圖則編號
PLAN No. S/K9/24B

CE in C referred the approved Hung Hom OZP No. S/K9/22 to the Board for amendment under section 12(1)(b)(ii) of the Ordinance. Since then, the OZP was amended once and exhibited for public inspection under section 5 of the Ordinance.

- ~~2.13 On 30 June 2009, the CE in C referred the approved Hung Hom OZP No. S/K9/22 to the Board for amendment under section 12(1)(b)(ii) of the Ordinance. The reference of the OZP was notified in the Gazette on 10 July 2009 under section 12(2) of the Ordinance.~~
- ~~2.14 On 11 September 2009, the draft Hung Hom OZP No. S/K9/23, incorporating amendments to rezone a site at Wuhu Street from "Residential (Group A)4" to "Other Specified Uses" annotated "Hotel" was exhibited for public inspection under section 5 of the Ordinance. During the two month exhibition period, no representation was received.~~
- 2.151 On 5 October 2010, the CE in C, under section 9(1)(a) of the Ordinance, approved the draft Hung Hom OZP, which was subsequently renumbered as S/K9/24. ~~On 15 October 2010, the approved Hung Hom OZP No. S/K9/24 (the Plan) was exhibited under section 9(5) of the Ordinance.~~
- 2.12 *On 6 October 2015, the CE in C referred the approved Hung Hom OZP No. S/K9/24 to the Board for amendment under section 12(1)(b)(ii) of the Ordinance. The reference back of the OZP was notified in the Gazette on 16 October 2015 under section 12(2) of the Ordinance.*
- 2.13 *On _____ 2016, the draft Hung Hom OZP No. S/K9/25 (the Plan), incorporating amendments including (i) the rezoning of a site at Lee Kung Street from "Government, Institution or Community" ("G/IC") to "Residential (Group A)" ("R(A)"); (ii) inclusion of 'Art Studio (excluding those involving direct provision of services or goods)' as a Column 1 use in Schedule II for industrial or I-O building of the "Other Specified Uses" annotated "Business" ("OU(B)") zone; and (iii) excising the two areas designated as Urban Renewal Authority (URA) Chin Tin Street/ Sung Chi Street Development Scheme Plan (DSP) and URA Hung Fook Street/ Ngan Hon Street DSP from the OZP boundary; was exhibited for public inspection under section 5 of the Ordinance. In addition, the alignments of the Mass Transit Railway (MTR) Kwun Tong Line Extension (KTE) and Shatin to Central Link (SCL) authorised by the CE in C under the Railways Ordinance (Chapter 519) on 30.11.2010 and 27.3.2012 respectively are shown on the OZP for information.*

3. OBJECT OF THE PLAN

- 3.1 The object of the Plan is to indicate the broad land use zonings and major transport networks so that development and redevelopment within the Planning Scheme Area (the Area) can be put under statutory planning control.
- 3.2 The Plan is to illustrate the broad principles of development. It is a small-scale plan and the transport alignments and boundaries between the land use zones may be subject to minor alterations as detailed planning proceeds.

- 8.2.8 ~~A minor relaxation clause in respect of site coverage has been incorporated into the Notes of the Plan for sites with site coverage restriction in order to cater for development with special design merits. To provide design/architectural flexibility, minor relaxation of the plot ratio/GFA/site coverage restrictions may be considered by the Board on application under section 16 of the Ordinance taking into account its own merits.~~

8.3 "Residential (Group A)" ("R(A)") – Total Area ~~27.12~~ ~~27.29~~ 26.75ha

- 8.3.1 This zone is intended primarily for high-density residential developments. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building. Considering the traffic impact of office development, there is a general presumption against office use in "R(A)" zone. Thus, office development other than those along major transport route would not be supported.
- 8.3.2 Developments within this zone include private residential developments at Shun Yung Street and Ko Shan Road; on both sides of Ma Tau Wai Road and Gillies Avenue; and to the south of Fat Kwong Street/Man Yue Street; Ka Wai Chuen, a large public housing estate at Ma Tau Wai Road; and Hung Hom Estate ~~under redevelopment~~ at Man Yue Street.
- 8.3.3 In the consideration of the overall transport, environmental and infrastructural constraints, as well as the adequacy in the provision of community facilities envisioned in the Kowloon Density Study Review, completed in early 2002, developments or redevelopments within the "R(A)" zone and its sub-zones are subject to specific control on plot ratios except otherwise specified in the Notes, i.e. a maximum plot ratio of 7.5 for a domestic building or a maximum plot ratio of 9.0 for a partly domestic and partly non-domestic building. In calculating the GFA for these developments/redevelopments, the lands for free-standing purpose-designed buildings that are solely used for accommodating school or other GIC facilities, including those located on ground and on building podium, are not to be taken as parts of the site.
- 8.3.4 Development and redevelopment within the "R(A)", "R(A)3", "R(A)4" and "R(A)5" zones are covered by the building height bands of 80mPD, 100mPD, **110mPD** and 120mPD as stipulated on the Plan according to the overall building height concept of the Area as specified in paragraph 7 above.
- 8.3.5 A site zoned "R(A)1" near the junction of Hung Hom South Road and Hung Hom By-pass (HHBP) on the Reclamation Area has been developed for residential development, the Harbour Place, providing about 2,470 flats. The development includes a deck over the HHBP to mitigate the potential noise pollution problems arising from the HHBP. Taking into account the traffic capacity constraints identified by the Central Kowloon Traffic Study, the residential development on this site is restricted to a maximum domestic GFA of 144,300m² and a maximum non-domestic GFA of 4,500m². This site is also subject to a maximum building height of 105mPD to reflect the major building heights of the development.

9.3 Ferries

Two ferry piers are located at the waterfront of the Reclamation Area to provide passenger ferry services to ~~Central, Wanchai and~~ North Point.

9.4 Other Public Transport Facilities

The Area is well served by buses and green minibuses. A public transport interchange for bus, green minibus and taxi is proposed to be incorporated into the "CDA(1)" site south of Hung Luen Road to replace the existing temporary bus/coach terminus adjacent to the Hung Hom Ferry Pier.

9.5 Pedestrian Circulation

The Reclamation Area will be served by a comprehensive network of footbridges. Pedestrian links across the Princess Margaret Road Link are mainly through footbridges connecting from the "G/IC" site to the south of the "C(3)" site to the podium of the Metropolis. Pedestrian links across the Hung Hom By-pass are provided by the podium deck as part of the residential development above it. A grade separated walkway to the north of Royal Peninsula along Hung Ling Street links the MTR Hung Hom Station and bus terminus to the residential development in the existing built-up area in Hung Hom. Continuous public pedestrian access is provided along the waterfront promenade from Laguna Verde via the planned promenade at the Reclamation Area to Tsim Sha Tsui East.

10. URBAN RENEWAL AUTHORITY DEVELOPMENT SCHEME PLAN AREA

10.1 Two areas have been designated as "URA DSP Areas". The land use zonings of the areas are depicted on the relevant URA DSPs and they will be implemented by the URA.

10.2 The DSP for Chun Tin Street/Sung Chi Street covers an area of about 0.16ha. The site is intended for accommodating one residential tower over a commercial podium with basement car park. The DSP for Hung Fook Street/Ngan Hon Street Site covers an area of about 0.45ha. The site is intended for accommodating three residential towers over two 3-storey commercial podia with a basement car park.

10.11. UTILITY SERVICES

The Area is well served with piped water supply, drainage and sewerage systems, as well as gas, electricity and telephone services. No difficulties are anticipated in meeting the future requirements.

11.12. CULTURAL HERITAGE

Two historic structures including the The Kwun Yum Temple at Station Lane (Grade I) and the Pak Tai Temple at Ma Tau Wai Road are located within the Area are Grade II and Grade III historic buildings respectively. Prior consultation with the Antiquities and Monuments Office of the Leisure and Cultural Services Department should be made if any development or re-zoning proposals might affect these buildings and

~~structures, and their immediate environs.~~ In 2009, the Antiquities Advisory Board (AAB) released the list of 1,444 historic buildings, in which some buildings/structures within the Area have been accorded gradings. The AAB also released a number of new items which are subject to grading assessment by AAB. Details of the list of 1,444 historic buildings/structures have been uploaded onto the official website of the AAB at <http://www.aab.gov.hk>. Prior consultation with the Antiquities and Monuments Office of the Leisure and Cultural Services Department should be made if any development, redevelopment or rezoning proposals might affect the above graded/proposed historic buildings/structures, and their immediate environs.

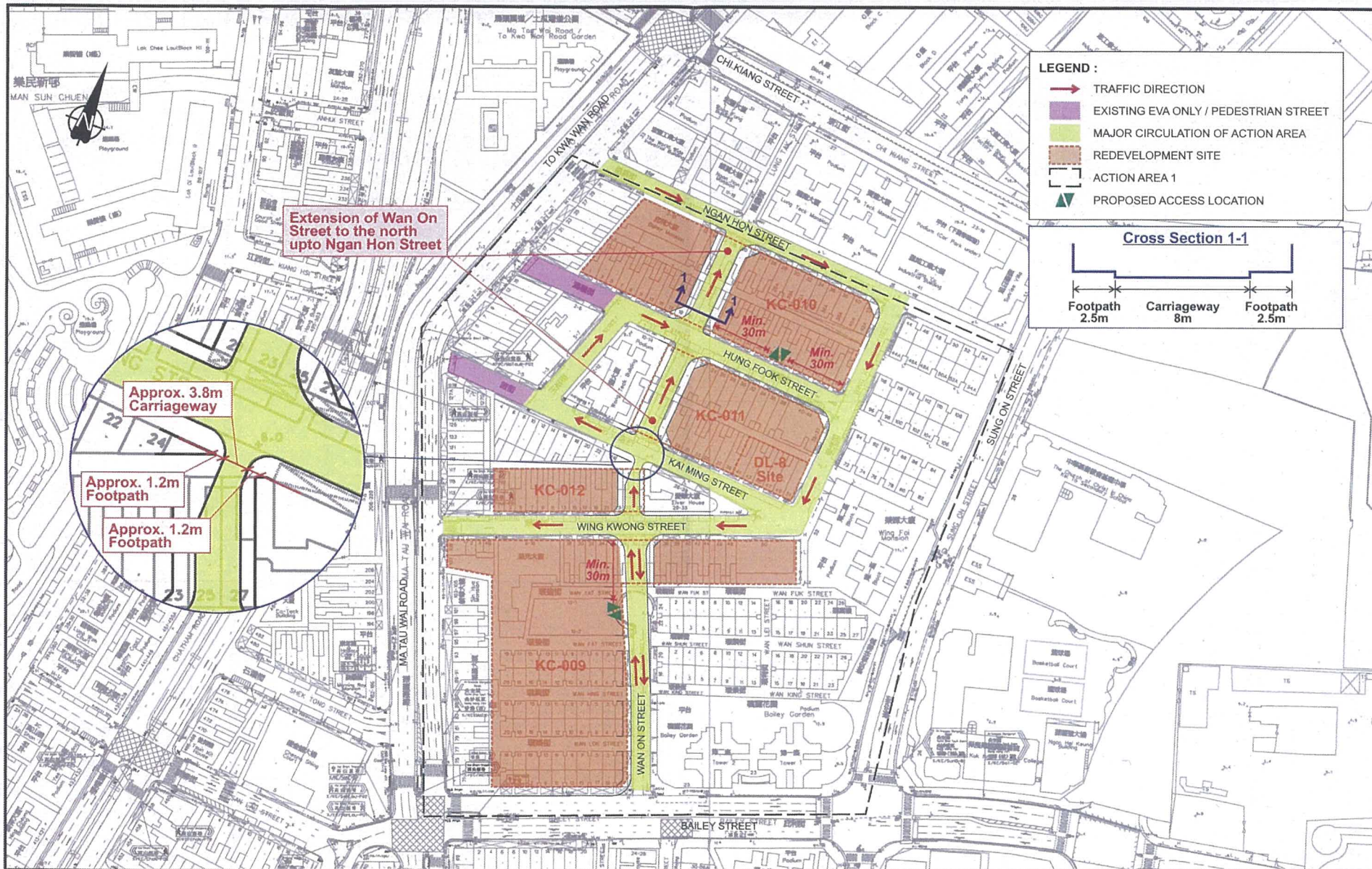
12.13. IMPLEMENTATION

- 123.2 Although existing uses non-conforming to the statutory zonings are tolerated, any material change of use and any other development/redevelopment must be always permitted in terms of the Plan or, if permission is required, in accordance with the permission granted by the Board. The Board has published a set of guidelines for the interpretation of existing use in the urban and new town areas. Any person who intends to claim an "existing use right" should refer to the guidelines and will need to provide sufficient evidence to support his claim. The enforcement of zonings mainly rests with the Buildings Department, the Lands Department and the various licensing authorities.
- 123.2 The Plan provides a broad land use framework within which more detailed non-statutory plans for the Area are prepared by the Planning Department. These detailed plans are used as the basis for public works planning and site reservation within the Government. Disposal of sites is undertaken by the Lands Department. Public works projects are co-ordinated by the Civil Engineering and Development Department in conjunction with the client departments and the works departments, such as the Highways Department and the Architectural Services Department. In the course of implementation of the Plan, the Kowloon City and Yau Tsim Mong District Councils would also be consulted as appropriate.
- 123.3 Planning applications to the Board will be assessed on individual merits. In general, the Board, in considering the planning applications, will take into account all relevant planning considerations which may include the departmental outline development plan and the guidelines published by the Board. The outline development plan is available for public inspection at the Planning Department. Guidelines published by the Board are available from the Board's website, the Secretariat of the Board and the Technical Services Division of the Planning Department. Application forms and guidance notes for planning applications can be downloaded from the Board's website and are available from the Secretariat of the Board, the Technical Services Division and the relevant District Planning Office of the Planning Department. Applications should be supported by such materials as the Board thinks appropriate to enable it to consider the applications.

TOWN PLANNING BOARD

OCTOBER 2010

***** 2016



Rev.	Description	Checked	Date
C	REVISED BASED ON TD'S COMMENT	KSC	24MAY16
B	REVISED BASED ON TD'S COMMENT	KSC	19MAY16
A	REVISED BASED ON TD'S COMMENT	KSC	11MAY16

Project Title
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

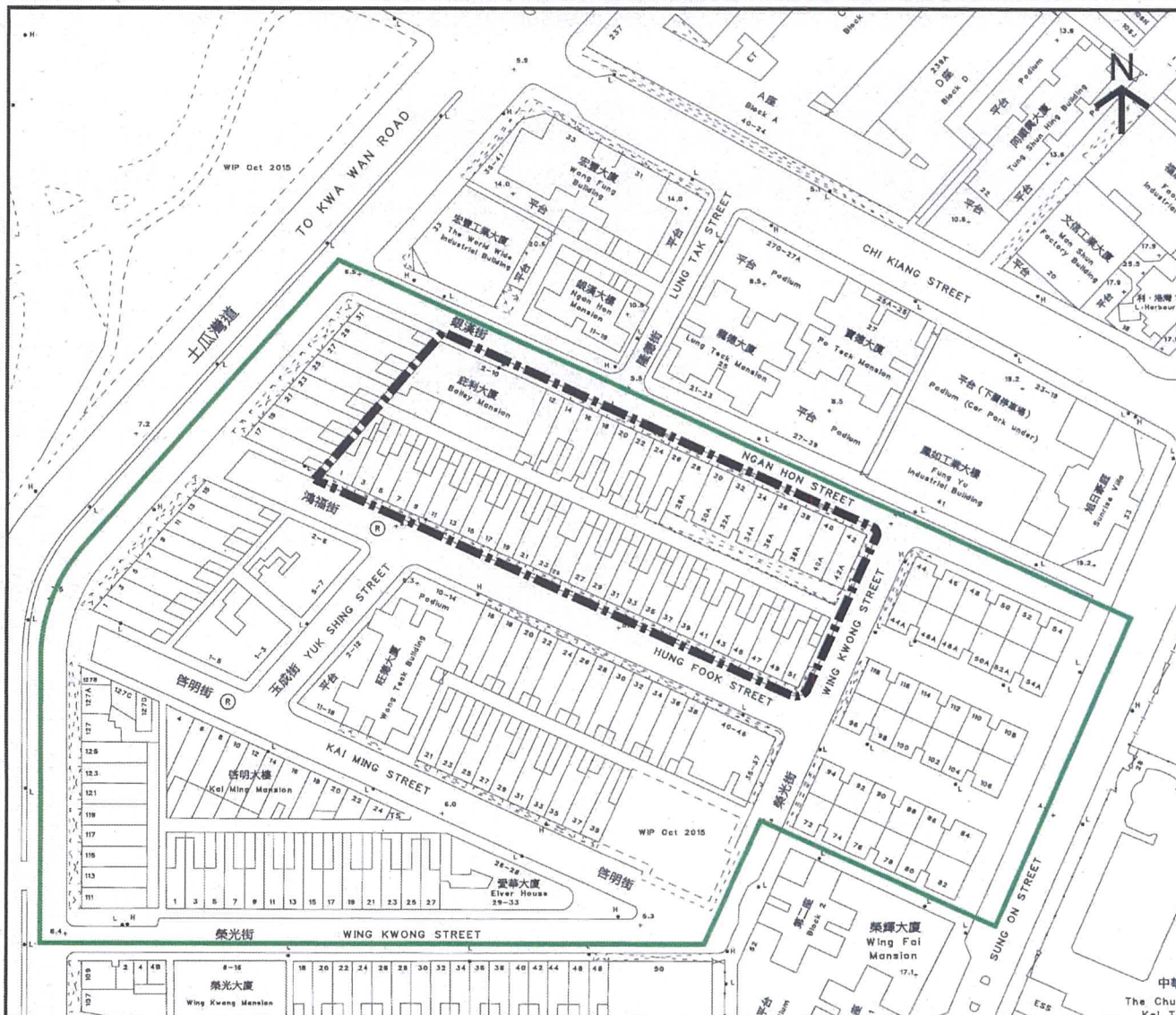
Drawing Title											
MASTER LAYOUT PLAN FOR ACTION AREA 1											
Designed	WHY	Checked	KSC	Scale	NTS	Date	APR 2016	Drawing No.	2.1	Rev.	C



參考編號
REFERENCE No.
M/K9/16/64

繪圖
DRAWING
1

(資料來源：由市區重建局提交)
(Source: Submitted by Urban Renewal Authority)



圖例 NOTATION

BOUNDARY OF HUNG FOOK STREET/NGAN HON STREET DEVELOPMENT SCHEME 鴻福街/銀漢街發展計劃範圍界線

BOUNDARY OF WIDER AREA 周邊範圍界線

發展計劃內地下停車場提供服務的周邊範圍
WIDER AREA SERVED BY UNDERGROUND CAR PARK IN THE DEVELOPMENT SCHEME

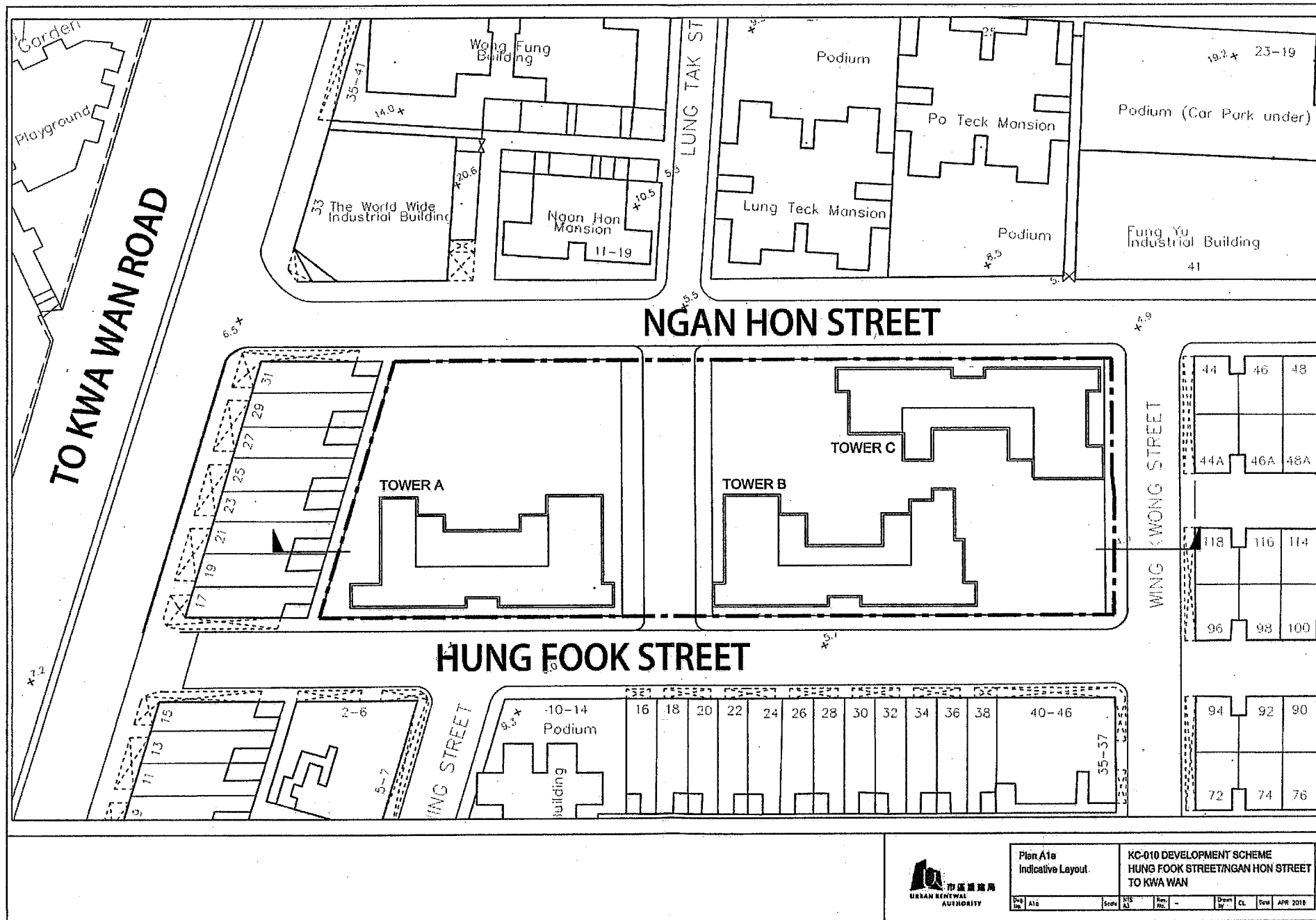


圖則編號
PLAN No. 1

參考編號
REFERENCE No.
M/K9/16/64

繪圖
DRAWING
2

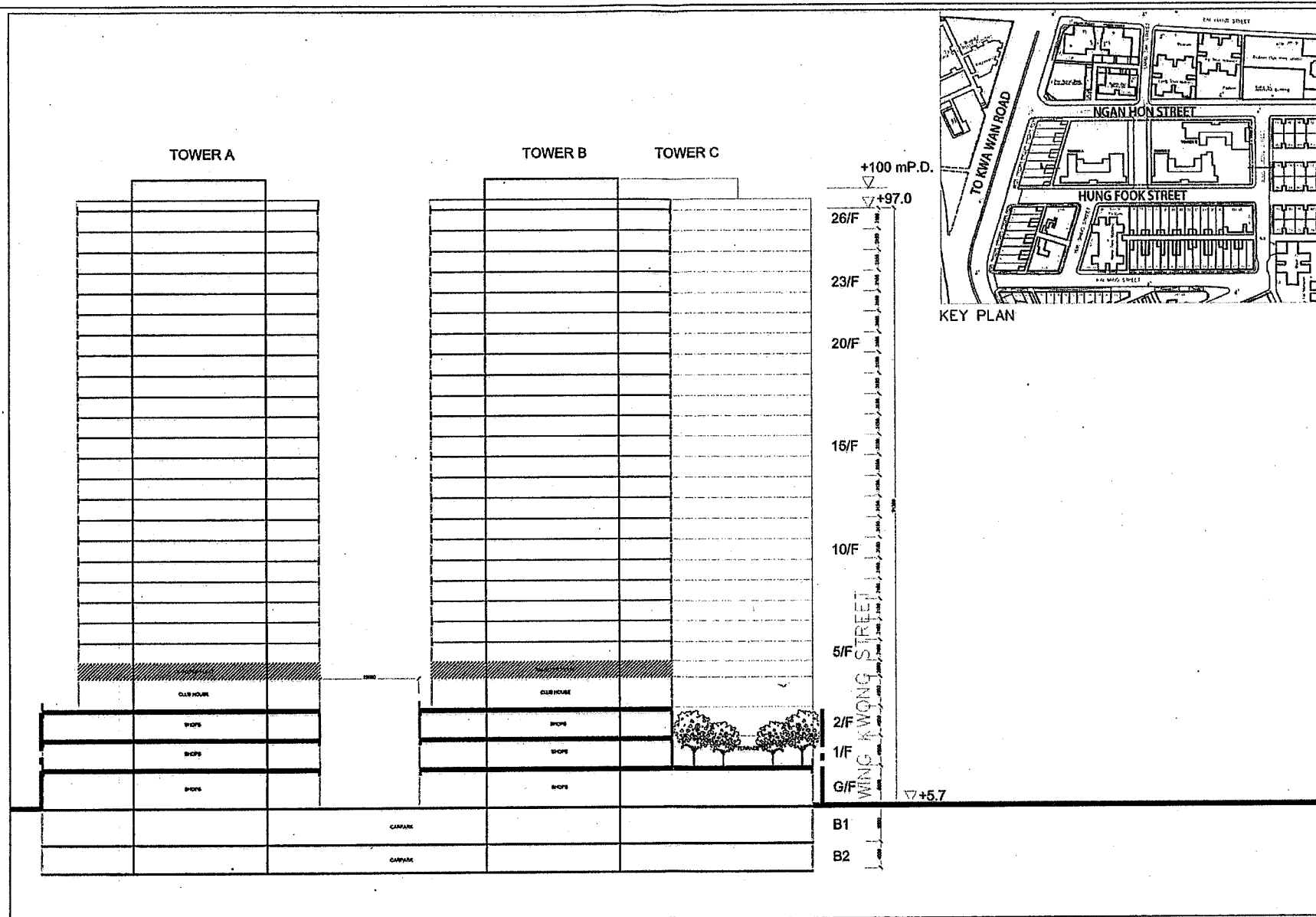
(資料來源：由市區重建局提交)
(Source: Submitted by Urban Renewal Authority)



(資料來源：由市區重建局提交)
(Source: Submitted by Urban Renewal Authority)

參考編號
REFERENCE No.
M/K9/16/64

繪圖
DRAWING
3

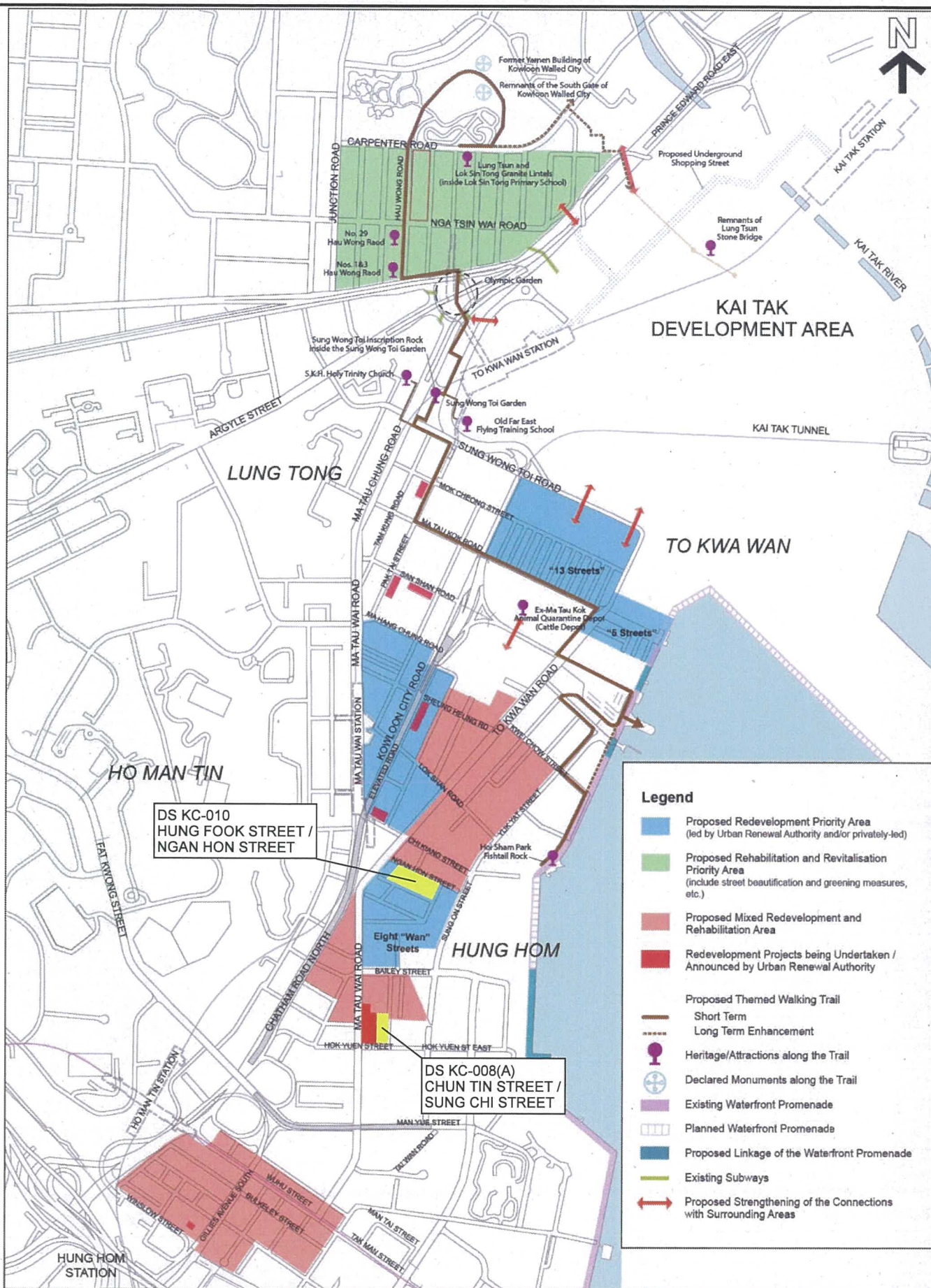


Plan A1b Section		KC-010 DEVELOPMENT SCHEME HUNG FOOK STREET/NGAN HON STREET TO KWA WAN			
Dep No	A1b	Scale	A1b A1	Rev No	—
Drawn by	CL	Date	APR 2018		

(資料來源：由市區重建局提交)
(Source: Submitted by Urban Renewal Authority)

參考編號
REFERENCE No.
M/K9/16/64

繪圖
DRAWING
4



本圖於2016年9月14日擬備
PLAN PREPARED ON 14.9.2016

RECOMMENDATION OF URBAN RENEWAL PLAN
FOR KOWLOON CITY

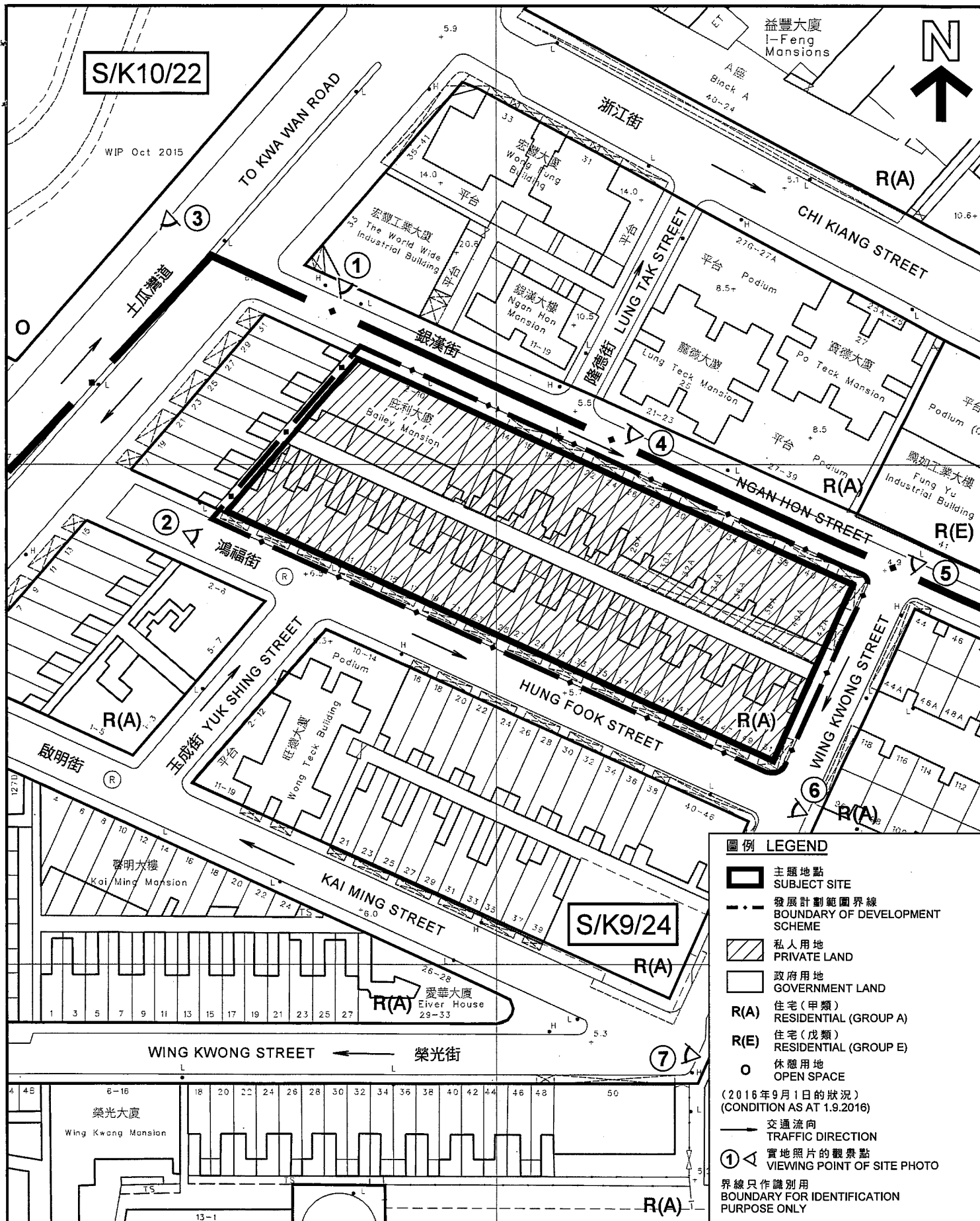
SCALE 1 : 13 000 比例尺
METRES 250 0 250 500 METRES

規劃署
PLANNING
DEPARTMENT



參考編號
REFERENCE No.
M/K9/16/64

圖 PLAN
1



圖例 LEGEND

- 主題地點
SUBJECT SITE
- 發展計劃範圍界線
BOUNDARY OF DEVELOPMENT SCHEME
- 私人用地
PRIVATE LAND
- 政府用地
GOVERNMENT LAND
- R(A)** 住宅(甲類)
RESIDENTIAL (GROUP A)
- R(E)** 住宅(戊類)
RESIDENTIAL (GROUP E)
- 休憩用地
OPEN SPACE

(2016年9月1日的狀況)
(CONDITION AS AT 1.9.2016)

交通流向
TRAFFIC DIRECTION

① 實地照片的觀景點
VIEWING POINT OF SITE PHOTO

界線只作識別用
BOUNDARY FOR IDENTIFICATION PURPOSE ONLY

平面圖 SITE PLAN

市區重建局鴻福街/銀漢街發展計劃
URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME

本摘要圖於2016年9月14日擬備，
所根據的資料為測量圖編號
11-NE-16C和11-NW-20D

EXTRACT PLAN PREPARED ON 14.9.2016
BASED ON SURVEY SHEETS No.
11-NE-16C & 11-NW-20D

SCALE 1:1 000 比例尺
METRES 20 0 20 40 METRES

規劃署
PLANNING
DEPARTMENT



參考編號
REFERENCE No.

M/K9/16/64

圖 PLAN

3



界線只作識別用
BOUNDARY FOR IDENTIFICATION PURPOSE ONLY

主題地點
SUBJECT SITE

實地照片 SITE PHOTO

本圖於2016年9月5日擬備，
所根據的資料為攝於
2016年9月1日的實地照片
PLAN PREPARED ON 5.9.2016
BASED ON SITE PHOTO
TAKEN ON 1.9.2016

市區重建局鴻福街／銀漢街發展計劃
URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME

規劃署
PLANNING
DEPARTMENT



參考編號
REFERENCE No.
M/K9/16/64

圖 PLAN
5



主題地點
SUBJECT SITE



界線只作識別用
BOUNDARY FOR IDENTIFICATION
PURPOSE ONLY

本圖於2016年9月5日擬備，
所根據的資料為攝於
2016年8月29日的實地照片
PLAN PREPARED ON 5.9.2016
BASED ON SITE PHOTOS
TAKEN ON 29.8.2016

實地照片 SITE PHOTOS

市區重建局鴻福街／銀漢街發展計劃
URBAN RENEWAL AUTHORITY
HUNG FOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME

規劃署
PLANNING
DEPARTMENT



參考編號
REFERENCE No.

M/K9/16/64

圖 PLAN

6



主題地點
SUBJECT SITE



主題地點
SUBJECT SITE

界線只作識別用
BOUNDARY FOR IDENTIFICATION
PURPOSE ONLY

本圖於2016年9月14日擬備，
所根據的資料為攝於
2016年8月29日及2016年9月1日
的實地照片

PLAN PREPARED ON 14.9.2016
BASED ON SITE PHOTOS
TAKEN ON 29.8.2016 AND 1.9.2016

實地照片 SITE PHOTOS

市區重建局鴻福街／銀漢街發展計劃
URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME

規劃署
PLANNING
DEPARTMENT

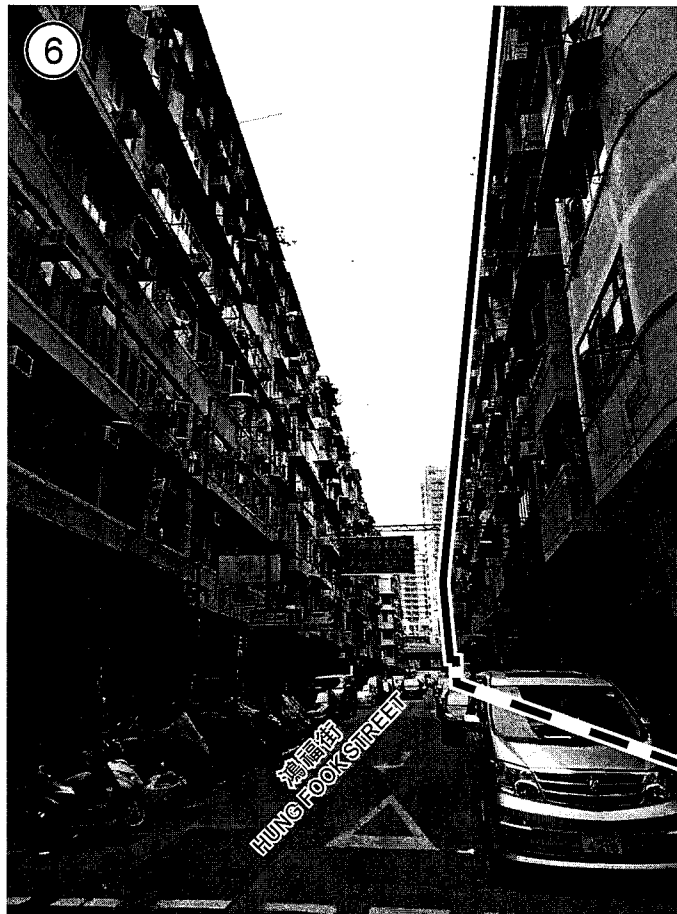


參考編號
REFERENCE No.

M/K9/16/64

圖 PLAN

7



主題地點
SUBJECT SITE



界線只作識別用
BOUNDARY FOR IDENTIFICATION
PURPOSE ONLY

本圖於2016年9月14日擬備，
所根據的資料為攝於
2016年8月29日及2016年9月1日
的實地照片

PLAN PREPARED ON 14.9.2016
BASED ON SITE PHOTOS
TAKEN ON 29.8.2016 AND 1.9.2016

實地照片 SITE PHOTOS

市區重建局鴻福街／銀漢街發展計劃
URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME

規劃署
PLANNING
DEPARTMENT

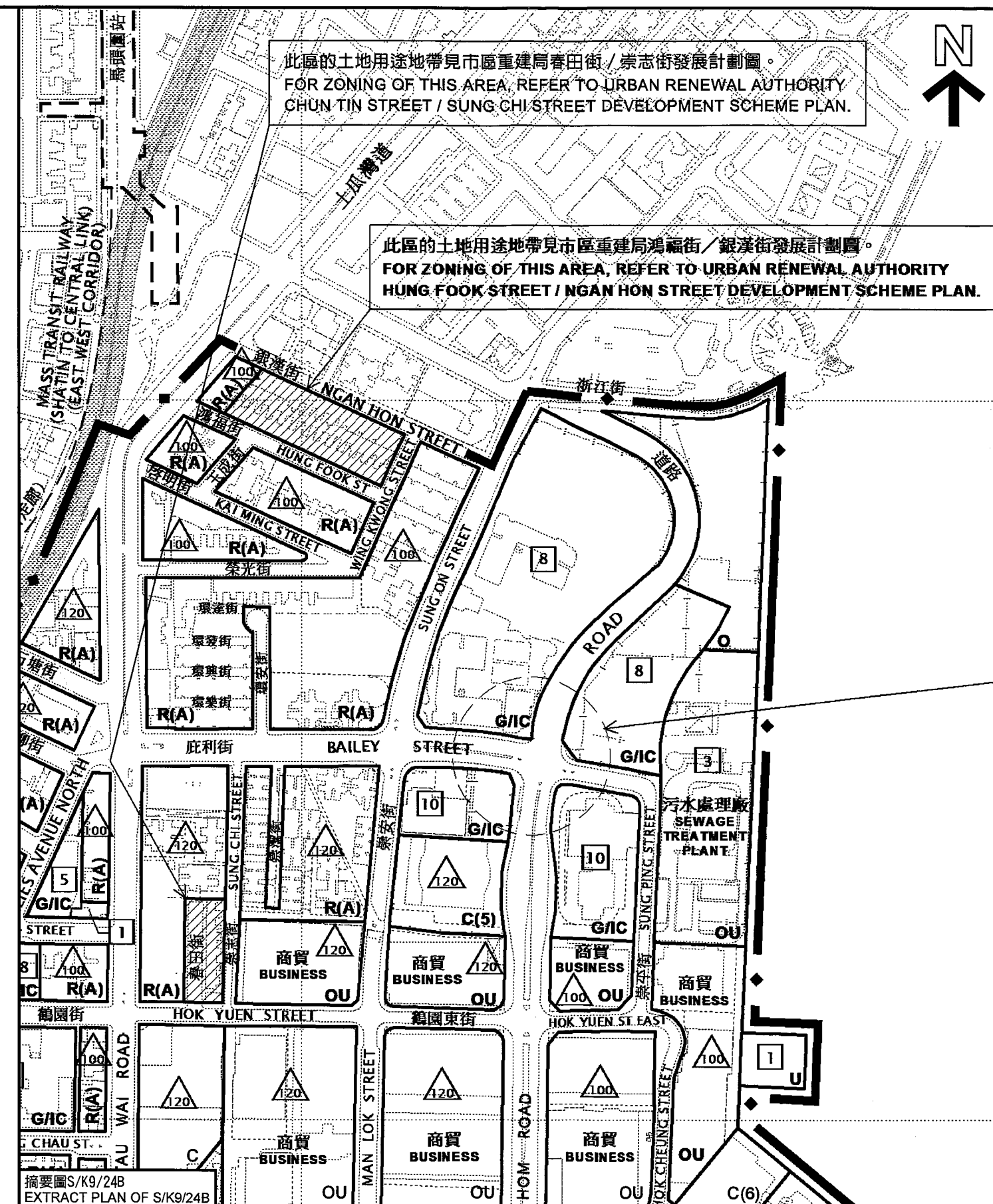
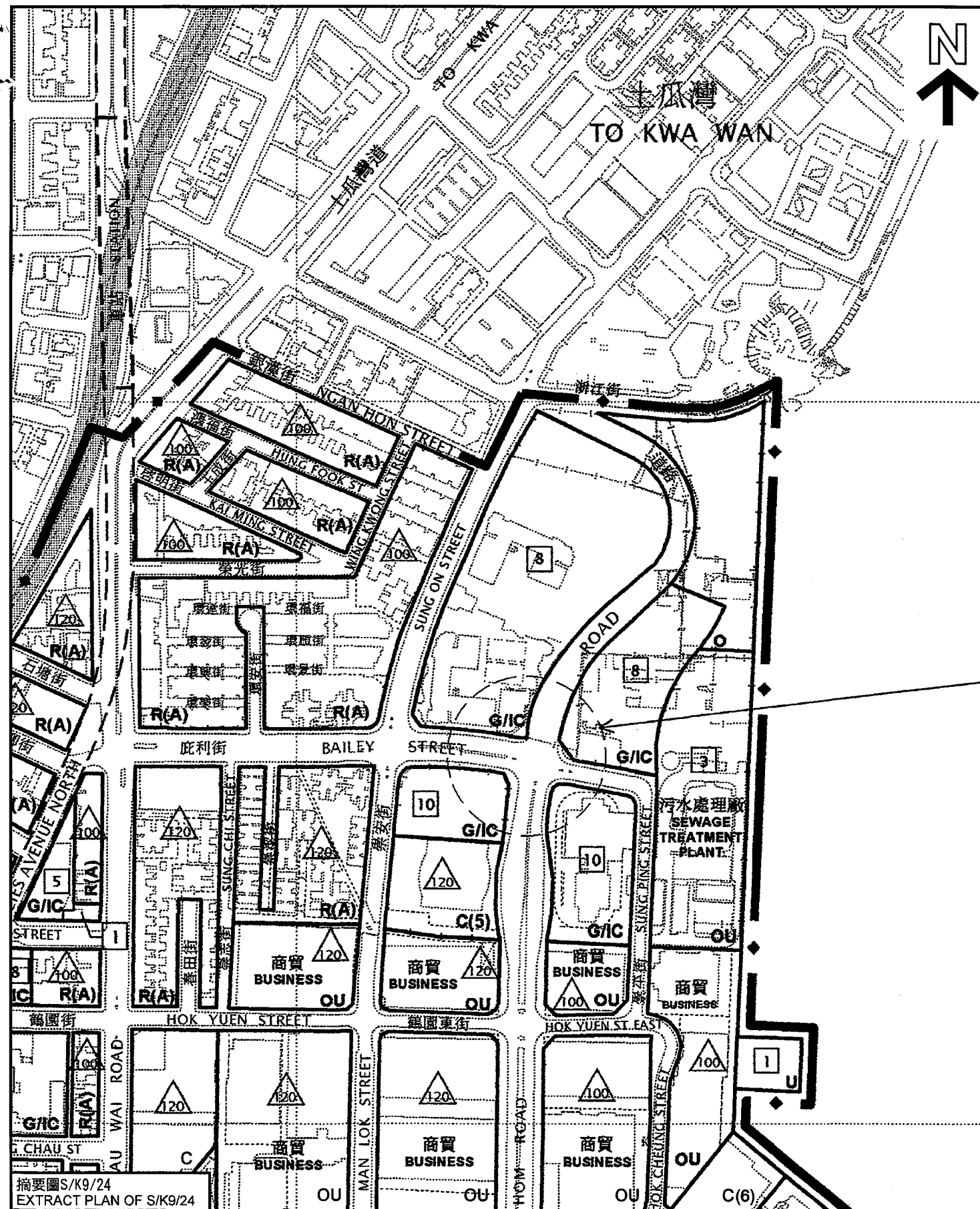


參考編號
REFERENCE No.

M/K9/16/64

圖 PLAN

8



位置圖 LOCATION PLAN

紅磡分區計劃大綱核准圖編號S/K9/24及紅磡分區計劃大綱草圖編號S/K9/24B之比較
COMPARISON OF THE APPROVED HUNG HOM OUTLINE ZONING PLAN No. S/K9/24
AND THE DRAFT HUNG HOM OUTLINE ZONING PLAN No. S/K9/24B

SCALE 1:3 500 比例尺
METRES 100 0 100 200 300 METRES

本摘要圖於2016年9月13日擬備，
所根據的資料為於2010年10月5日
核准的分區計劃大綱圖編號S/K9/24
EXTRACT PLAN PREPARED ON 13.9.2016
BASED ON OUTLINE ZONING PLAN No.
S/K9/24 APPROVED ON 5.10.2010

規劃署
PLANNING DEPARTMENT



參考編號
REFERENCE No.
M/K9/16/64

圖 PLAN
9

Urban Renewal Authority

DEVELOPMENT SCHEME

Prepared under Section 25 (3)

of the Urban Renewal Authority Ordinance

**HUNG FOOK STREET / NGAN HON STREET
KOWLOON CITY
(KC-010)**

PLANNING REPORT

June 2016



市區重建局

URBAN RENEWAL

AUTHORITY

Kowloon East's new plan district by adopting a comprehensive and
holistic approach to regenerate the district in local community.

EXECUTIVE SUMMARY

1. The Urban Renewal Authority (URA) submits this planning report to seek approval of the Town Planning Board for the submission of the draft Development Scheme Plan (DSP) No. S/K9/URA2/A under section 25(5) of the Urban Renewal Authority Ordinance (URAO). The DSP refers to the proposed Development Scheme designated as Hung Fook Street / Ngan Hon Street, Kowloon City, (KC-010) to be implemented by the way of development scheme under section 25 of the URAO. The Development Scheme involves a gross site area of about 4,951m², bounded by Ngan Hon Street to the North, Wing Kong Street to the east, Hung Fook Street to the south, , and a back lane of the adjacent buildings to the west. Pursuant to section 23(1) of the URAO, the URA notified the public in the Government Gazette (G.N. 3107) about the commencement of the Development Scheme on 3 June 2016.
2. The tenement buildings within the proposed Development Scheme were built between 1957 and 1960 with 6 -8 storeys in height and none is served by lift. Buildings condition is deteriorating and concrete spalling and cracks are found in the staircase of the buildings. There are sub-divided units in the buildings and suspected unauthorized building structures on the roofs of some buildings. The living condition in the Development Scheme is considered crowded and not satisfactory.
3. Accessibility of the area is poor. Ngan Hon Street is a 1-way local road running eastbound and is a dead-end road which serves as vehicular access to the old tenement buildings at the junction between Ngan Hon Street and Wing Kwong Street.
4. Hung Fook Street is also a 1-way local dead-end road running westbound serving traffic ingress/ egress from Wing Kwong Street via Kai Ming Street and Yuk Shing Street to Wing Kwong Street. Portion of Hung Fook Street to the west is vehicle access restricted pedestrian street for emergency use only.
5. Ngan Hon Street serves for ingress traffic from To Kwa Wan Road to Ngan Hon Street, Wing Kwong Street, Kai Ming Street, Yuk Shing Street, and Hung Fook Street while Wing Kwong Street serves egress traffic of these streets to Ma Tau Wai Road.
6. To improve living environment of an old urban district through master planning, the URA has conducted a Local Community Planning Study in Kowloon City/To Kwa Wan district by adopting a comprehensive and holistic approach to rejuvenate the district in local community planning

manner. It also aims at, through the study, redesigning more effective and environmentally-friendly local transport and road networks of the area.

7. The Development Scheme seeks to improve the living and environmental conditions, building condition and fire safety in the scheme area, through redevelopment of the existing dilapidated tenement blocks on the site to a quality residential development with modest commercial provisions on the lower floors.
8. An underground car park serving the local community will be provided in the Development Scheme. The underground car park will provide car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street (Wider Area) as well as coach parking spaces to cater for the need of coach parking in the district. Provision of an underground car park at one site within the Wider Area can reduce car park openings in the individual developments within the Wider Area and avoids disruption to the traffic movements and pedestrians while maintaining continuous street fronts of the Wider Area and the adjacent sites.
9. It is intended provide a through road connecting Hung Fook Street and Ngan Hon Street within the scheme area, which in turn forms part of the proposed road network extending from Wan On Street of URA Development Project KC-009 which was commenced on 4 March 2016, via the URA Development Project KC-011 and KC-012 which were commenced on the same date of the Development Scheme, to Ngan Hon Street. The proposed road network will improve traffic and pedestrian accessibility and alleviate traffic congestion of the district.
10. Supporting technical assessments such as Environmental Assessment (EA), Drainage Impact Assessment (DIA), Sewage Impact Assessment (SIA), and Traffic Impact Assessment (TIA) have been carried out and results of these assessments indicate that there will be no insurmountable problem or adverse impact in implementing the proposed Development Scheme.
11. All in all, the proposed Development Scheme will provide the following planning and environmental benefits:
 - removal of old building of over 50 years in deteriorating condition and demolition of sub-divided units;
 - the existing over-crowding and sharing households will be able to improve the living conditions through redevelopment;

- provision of more small to medium-sized flats in the urban area;
- satisfying needs of car parking spaces while lessening illegal parking in the area;
- maintaining existing continuous streetscape by avoiding the necessity for providing car park provisions in new developments of the area;
- lessening the conflicts between pedestrians and vehicular run-in/ outs in the Wider Area;
- enhancing street vibrancy of the Development Scheme by providing setback at the street level of the new development;
- enhancing road safety to road users;
- enhancing traffic and pedestrian accessibility in the area; and
- reducing the circulating traffic at the main public road network around the area.

行政摘要

1. 市區重建局(市建局)根據《市區重建局條例》第 25(5)條向城市規劃委員會提交發展計劃草圖(編號 S/K9/URA2/A)，以獲取城規會批准。該發展計劃草圖所建議的發展計劃名命為九龍城鴻福街及銀漢街發展計劃(KC-010)，建議依據《市區重建局條例》第 25 條以「發展計劃」方式施行。發展計劃的地盤總面積佔地約 4,951 平方米，地盤北面毗連銀漢街，東面毗連榮光街，南面毗連鴻福街，西面毗連鄰近大廈的後巷。按照《市區重建局條例》第 23 條，市建局於 2016 年 6 月 3 日出版的政府憲報(第 3107 號公告)刊載了公告，向公眾宣布啟動這個發展計劃。
2. 發展計劃內的樓宇於 1957 至 1960 年間落成，樓高 6-8 層，所有建築物均不設電梯。樓宇狀況不斷變差，樓梯的石屎剝落及出現裂痕。部份單位被分間為「劏房」，而部分樓宇的天台均發現疑似僭建物，居住環境擠迫及不理想。
3. 該區的交通可達性較差。銀漢街為單程的區內東行道路，亦是一條供車輛通往銀漢街及榮光街交界舊樓的掘頭路。
4. 鴻福街亦是往西行的區內單程掘頭路，供車輛由榮光街經啟明街及玉成街返回榮光街。西面的部分鴻福街為限制車輛使用的行人街，僅供車輛於緊急情況下使用。
5. 銀漢街主要供車輛由土瓜灣道進入銀漢街、榮光街、啟明街、玉成街，及鴻福街。榮光街則主要供車輛離開上述街道並通往馬頭圍道。
6. 為了通過總體規劃以改善舊區的居住環境，市建局於九龍城/土瓜灣區進以全面及整體方式進行「小區規劃研究」，並希望通過是次研究，以小區規劃方式為當區重新注入活力。研究還旨在通過重新設計，為當區帶來更有效率及環保的交通道路網絡。
7. 發展計劃的目標是透過重建，以高質素的住宅及於低層的商業用途發展去取代現有破落樓宇，從而提升發展計劃範圍內的居住及環境質素、建築物狀況及消防安全。
8. 發展計劃會附設一個地下停車場，提供附屬停車位及上落客貨設施給銀漢街、土瓜灣道、榮光街和崇安街(周邊地區)的發展及/或重建項目，以及提供旅遊巴泊位以應對區內對旅遊巴泊位的需求。於周邊地區內單獨一個地盤設置一個地下停車場的交通方案能避免於周

邊地區，因獨立發展而於每個發展項內提停車場出入口，因而令行車及行人受到阻礙，亦能保持周邊地區及鄰近地方的連續街道面。

9. 發展計劃擬議開闢一條新道路，以連貫市建局於 2016 年 3 月 4 日開展的 KC-009 發展項目中建議環安街的延伸，並途經與本發展計劃同日開展的市建局 KC-011 及 KC-012 發展項目，以連接銀漢街的道路網絡。該道路將增加發展計劃範圍內交通的可達性及減輕該區的交通擠塞。
10. 為擬備發展計劃已進行了一些技術評估，例如環境評估、渠務影響評估、污水系統影響評估，及交通影響評估等。這些技術評估結果顯示預期落實修訂計劃不會出現重大問題或對區內環境構成負面影響。
11. 總括而言，本發展計劃圖將帶來以下的規劃及環境裨益：
 - 清拆樓齡超過 50 年的破落樓宇及在內的「劏房」；
 - 改善現居住於過度擁擠和共用單位的家庭的居住情況；
 - 於市區提供更多中/小型住宅單位；
 - 滿足區內對停車泊位的需求以同時減少違例泊車；
 - 避免於區內因個別發展項目加設停車場而能以保持現有的連續街景；
 - 降低周邊地區內，行人和車輛出入口之間的衝突；
 - 通過在發展計劃中的新發展建築物於地面水平後移而提高街道活力；
 - 增加道路使用者的安全；
 - 加強交通和行人的可達性；及
 - 減少車量於區內主幹道繞行。

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PART 1

PLANNING REPORT

URBAN RENEWAL AUTHORITY**DEVELOPMENT SCHEME****HUNG FOOK STREET / NGAN HON STREET,
KOWLOON CITY
(KC-010)****1. INTRODUCTION**

- 1.1 To improve living environment of an old urban district through master planning, the Urban Renewal Authority (URA) has conducted a Community Planning Study (the Study) in Kowloon City/To Kwa Wan district by adopting a comprehensive and holistic approach to rejuvenate the district in local community planning manner. It also aims at, through the study, redesigning more effective and environmentally-friendly local transport and road networks of the area. **Plan 1** shows the commenced projects within the Study where the Development Scheme falls into.
- 1.2 The Development Scheme involves the rows of old tenement buildings at Nos. 1 - 51 Hung Fook Street (odd nos.) and Nos. 2 – 42A Ngan Hon Street (even nos.).
- 1.3 The Development Scheme is included in the URA's 15th Business Plan (2016/17) approved by the Financial Secretary recently. It is proposed to be processed as a Development Scheme under section 25 of the URA Ordinance (URAO). The URA's Board on 26 April 2016 approved the submission of the Development Scheme under section 25(5) of the URAO to the Town Planning Board (TPB). The draft Development Scheme Plan (DSP) No. S/K9/URA2/A is prepared for submission to the TPB.
- 1.4 Pursuant to section 23(1) of the URAO, the URA notified the public in the Government Gazette (G.N. 3107) about the commencement of the Development Scheme on 3 June 2016. The draft DSP is now submitted under section 25(5) of the URAO to the TPB for consideration.
- 1.5 This planning report (being Part 1 of the whole report) is prepared to provide the TPB with the necessary background information and the planning proposal to facilitate its consideration of the draft DSP, as contained in Part 2, submitted under section 25 of the URAO. Supplementary information, including the preliminary design of the proposed development, key technical assessments and social impact assessment (SIA) (Stage 1), and implementation approach are enclosed in Part 3 for reference.

2. THE DEVELOPMENT SCHEME PLAN AREA

- 2.1 The Development Scheme (the Scheme) is bounded by Hung Fook Street to the south, Wing Kwong Street to the east, Ngan Hon Street to the north and a back lane of the adjacent buildings to the west.
- 2.2 The Scheme boundary area is about 4,951m² which includes the pavement where the affected buildings overhang. The development site area for plot ratio calculation is about 4,562m² which is equivalent to area of "Residential (Group A)" ("R(A)") zone.
- 2.3 **Plan 2** shows the broad location of the Development Scheme while the Scheme area, information on the adjoining properties and streets are shown in **Plan 3**. As shown in **Plan 3**, the URA Hung Fook Street / Kai Ming Street Development Project (KC-011), the URA Kai Ming Street Development Project (DL8:KC), the URA Wing Kwong Street Development Project (KC-012), and the URA Bailey Street / Wing Kwong Street Development Project (KC-009) are located immediately to the south of the Scheme.
- 2.4 The Scheme is currently zoned "R(A)" on the approved Hung Hom Outline Zoning Plan (OZP) No. S/K9/24. An extract of the OZP is shown on **Plan 4**.
- 2.5 The Scheme boundary has been delineated based on several factors, including building conditions (building structure, fire safety and building services), building age, building height, local environmental conditions (**Plan 5, 6 and 7**).

3. BACKGROUND AND EXISTING CONDITIONS

- 3.1 On 3 June 2016, the URA published the Gazette Notice for the commencement of the Development Scheme under s.23 of URAO in the Government Gazette (G.N. 3107).

Historical Background

- 3.2 Historical map records indicate that the Scheme area was probably situated near the coastline before 1890s. There were village settlements and agricultural activities in that area in 1870s. Several rounds of reclamation had been undertaken in Ma Tau Kok Bay, To Kwa Wan Bay and Hung Hom since 1890s till 1960s to increase land at the coastline for expansion of population.
- 3.3 The surrounding of the Scheme area have been predominantly residential developments with neighbourhood retail activities on the ground floors. In the 1960s and 1970s, the area in To Kwa Wan and Hung Hom has been developed as industrial area with establishment of

many industrial buildings along the coastal area. The new reclaimed land was designated for more residential developments and industrial uses, such as manufacturing, weaving, bleaching and dyeing, printing and electroplating and car-repairing activities.

Existing Uses, Building and Living Conditions

- 3.4 The tenement buildings in the Scheme are 6 – 8 storeys in height, with some buildings having cocklofts in the ground floor units. All buildings were built between 1957 and 1960. None is served by lift. The buildings within the Scheme area are mainly residential in nature on the upper floors with commercial premises occupying the ground floor. These commercial premises are primarily engaging in business of retail of goods, services and workshops. Nearly a quarter of these commercial premises are vehicle repair workshops and a number of shops are retail of construction materials, restaurants and beauty salons. Staircase shops are found at the entrance of staircase in some buildings at ground floor.
- 3.5 Based on the URA's building condition survey carried out in May 2016, the buildings are in deteriorating condition (**Plan 7**); spalling concrete and cracks are found in the staircase of the buildings. There are sub-divided units in the buildings and unauthorized building structures on the roofs of many buildings (**Plans 10 to 16**). The living condition in the Scheme is considered crowded and not satisfactory.

Surrounding Land Uses

- 3.6 The street blocks adjacent to the Scheme are a mix of residential and commercial uses (**Plan 8**). Vehicle repair workshops are predominantly occupying ground floor shops in the area. Industrial uses of factory buildings are located to the northeast and northwest of Ngan Hon Street. Some of the industrial buildings appear not to be engaged in manufacturing activities but mainly for office, storage, workshop / showroom uses.

Existing Traffic Network

- 3.7 The Scheme is bounded by Ngan Hon Street to the north and Hung Fook Street to the south. Ngan Hon Street is a 1-way local road running eastbound and is a dead-end road which serves as vehicular access to the old tenement buildings at the junction between Ngan Hon Street and Wing Kwong Street.

- 3.8 Hung Fook Street is also a 1-way local dead-end road running westbound serving traffic ingress/ egress from Wing Kwong Street via Kai Ming Street and Yuk Shing Street to Wing Kwong Street. Portion of Hung Fook Street to the west is vehicle access restricted pedestrian street for emergency use only.
- 3.9 Ngan Hon Street serves for ingress traffic from To Kwa Wan Road to Ngan Hon Street, Wing Kwong Street, Kai Ming Street, Yuk Shing Street, and Hung Fook Street while Wing Kwong Street serves egress traffic of these streets to Ma Tau Wai Road.
- 3.10 Heavy traffic loading of these streets is aggravated by vehicle repair workshops predominately occupying shops at ground floor. Illegal parking in these streets is common although there are some on-street metered parking spaces provided in Hung Fook Street, Kai Ming Street and Wing Kwong Street.

Demographic Background

- 3.11 Population of the Scheme is estimated to be 1,500 in around 700 households. Detailed demographic information of the Scheme will be discussed in the SIA (Stage 2) report.

Ownership Pattern

- 3.12 All the buildings within the Scheme are under multiple ownerships, except No. 19 Hung Fook Street and No. 20 Ngan Hon Street, which are in single ownership (**Plan 9**).

4 PLANNING & LAND USE PROPOSALS

Objectives of the Scheme

- 4.1 The Scheme seeks to improve the living and environmental conditions, building condition and fire safety in the scheme area, through redevelopment of the existing dilapidated tenement blocks on the site to a quality residential development with modest commercial provisions on the lower floors.
- 4.2 An underground car park will be provided in the Scheme. The underground car park will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments within the area bounded by Ngan Hon Street, To Kwa

Wan Road, Wing Kwong Street and Sung On Street (the Wider Area) as demarcated on the **Plan 1** for **Explanatory Statement** in Part 2, as well as coach parking spaces to cater for the need of coach parking in the district. The provision of an underground car park at one site can maintain street fronts of the Scheme and the adjacent sites by reducing car park openings in the individual developments.

- 4.3 The Scheme is also proposes to provide a through road connecting Hung Fook Street and Ngan Hon Street within the scheme area. Provision of a through road within the Development Scheme area forms part of the proposed road network extending from Wan On Street of URA Project KC-009 which was commenced on 4 March 2016 under URAO Section 26 (see **Figure 3.2** of **Appendix 2**). The proposed road network will improve traffic accessibility and will alleviate traffic congestion of the scheme area. The underground car park is proposed underneath the through road in the surrounding.

Development Intensity

- 4.4 The Scheme which is proposed to be zoned as “R(A)8”, subject to a total plot ratio (PR) of 9 where the maximum domestic PR should not exceed 7.5. The Scheme is primarily for residential use with commercial/ retail uses on the podium three floors. Given the size and configuration of the site, three residential towers are proposed on top of the two 3-storey commercial podia with private residential clubhouse and podium garden on the podium roof. The proposed total Gross Floor Area (GFA) of the Scheme area is about 41,058m², which comprises domestic GFA of about 34,215m² and non-domestic GFA of about 6,843m². The proposed uses and development intensity conform to that of the original “R(A)” zoning covering the site prior to its rezoning as “R(A)8” on the DSP. The floor space of the underground car park is permitted as of right and will be disregarded from plot ratio calculation.

Conceptual Layout

- 4.5 **Appendix 1** (see **Plans A1a** and **A1b**) shows the block plan of the Scheme. Three residential towers on 3-level commercial/ retail podia are proposed based on the proposed development parameters for the Scheme. The proposed development parameters are set out in **Table 4.1** which is subject to minor adjustments in the detailed design stage.

Table 4.1 Proposed Development Parameters of the Scheme

Scheme Boundary Area	4,951m ²			
Net Site Area	4,562m ² (Subject to survey and detailed design)			
Proposed Zoning	“R(A)8”			
Maximum Domestic GFA	34,215m ² (PR = 7.5)			
Maximum Non-domestic GFA	6,843m ² (PR = 1.5)			
No. of Residential Tower	3 residential towers			
No. of Residential Floor	About 23 floors			
No. of Flats	About 750 flats			
Average Flat Size	About 45m ²			
Commercial Podium	3 storeys			
Maximum Building Height	100mPD			
Other Proposals in the Scheme				
Underground Car Park (to be as required by government and subject to maximum provisions)		Car Park Spaces (not more than)	Loading / Unloading Bays (not more than)	Coach Parking Spaces
	For the Site	110	12	4
	For Wider Area ¹	164	8	
Provision of Through Road	A through road connecting Hung Fook Street and Ngan Hon Street within the Scheme area is proposed.			
Notes:				
¹ Area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street				

Car Park

- 4.6 Currently, apart from the hourly parking spaces provided by the private residential development, Sunrise Villa, at Chi Kiang Street which is immediate to the north of the Scheme, the Scheme and the adjacent developments could rely on some on-street metered parking spaces at Hung Fook Street, Wing Kwong Street and Kai Ming Street for meeting the car parking need. According to the data of Transport Department (TD) as at 31 December 2015, there are 13, 23, and 8 parking spaces provided at Hung Fook Street, Wing Kwong Street and Kai Ming Street respectively. However, illegal parking is serious along the streets and creates issues of road safety and nuisances to the area.
- 4.7 On the same date of the Scheme commencement, two Development Projects, (i.e. Hung Fook Street/ Kai Ming Street: KC-011 and Wing Kwong Street: KC-012) immediate to the south of the Scheme, are commenced by the URA under section 26 of the URAO. The URA Kai Ming Street Development Project (DL-8:KC) is also located in the immediate south of the Scheme. Limited on-site loading/unloading bays will be provided at the three Development Projects while car parking spaces will not be provided. The underground car park at the Development Scheme serves the remaining required number of loading/unloading bays provision and car parking spaces provision for the three Development Projects.
- 4.8 However, in order to maintain existing continuous and active street frontages of the area, in determining the maximum provision of car park spaces, in addition to the sites above, due consideration has been given to the Wider Area bounded by Ngan Hon Street, To Kwa Wan Road, Kai Ming Street and Sung On Street as a whole. Instead of providing ancillary car parking spaces and loading/unloading facilities in individual sites, these facilities ancillary to the developments within the Wider Area can also be provided at the Scheme. Coach parking spaces will also be provided in the underground car park to meet the need of coach parking in the district. As a result, conflict between pedestrians and vehicles movement can be minimized by not opening up run-in/outs in all individual developments (**Plan 17**). Pedestrians can enjoy safety walk in the streets which in turn uphold street vibrancy in the locality.
- 4.9 As such, an underground car park accommodating car parking spaces and loading/ unloading bays ancillary to developments and/or redevelopments within the Wider Area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street as demarcated on **Plan 1** for **Explanatory Statement in Part 2**, will be provided to meet the parking needs in the Wider Area. Coach parking spaces will also be provided in the underground car park to meet need for coach parking in the district.

- 4.10 The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by Government and shall be not more than 274, 20 and 4 respectively. The number of car parking and loading/unloading bays provided above is derived from the parking and loading/unloading provision requirements under the current Hong Kong Planning Standards and Guidelines with reference to the total GFA of the Development Scheme and that of other developments and/or redevelopments in the Wider Area with about 104,000m² GFA in the provision of about 2000 residential units (see **Table A1** of **Appendix 2**). As the car parking spaces, and loading/unloading bays ancillary to the development and/or redevelopments in the Wider Area are met by the Scheme and the coach parking spaces are required by government to serve needs for coach parking in the district, in determining the maximum plot ratio for the Scheme, it is proposed that any floor space that is constructed or intended for the use solely as the underground car park, may be disregarded. Daily operation of the car park facilities will be arranged at the implementation stage.
- 4.11 Among the sites within the Wider Area to be served by the underground car park, site area of the Scheme is relatively large and more efficient car park layout can be achieved and the ingress and egress routings of the Scheme from both north and south directions are convenient with the proposed traffic scheme. Provision of an underground car park accommodates necessary parking needs generated by developments and/or redevelopment in the Wider Area and avoids disruption to the through traffic movements and pedestrians while maintaining continuous street fronts on other sites as well as meet the need for coach parking in the district.
- 4.12 URA has obtained no adverse comment from the TD on the proposal on parking provision, internal transport facilities, the related ingress/egress, and the traffic impact of the Scheme as stated in the Traffic Impact Assessment Report (TIA) (**Appendix 2**).

Local Road Improvement

- 4.13 As mentioned in para. 3.7 to 3.10 above, the traffic accessibility in the area is limited. Ingress traffic relies on the junction between To Kwa Wan Road and Ngan Hon Street while egress traffic relies on junction between Wing Kong Street and Ma Tau Wai Road. To enhance traffic accessibility in the area, a through road in the Scheme connected with a new road network in the area is proposed.

- 4.14 It is proposed to provide a through road connecting Hung Fook Street and Ngan Hon Street within the Scheme area, which in turn forms part of the proposed road network extending from Wan On Street of URA Development Project KC-009 which was commenced on 4 March 2016, via the URA Development Project KC-011 and KC-012 which were commenced on the same date of the Scheme, to Ngan Hon Street. The through road in the Scheme is about 13m wide of a 1-way carriageway with footpaths on both sides.
- 4.15 The proposed through road and traffic arrangement in the Scheme is shown in **Figure 3.2 of Appendix 2**.
- 4.16 With the proposed road network system, egress traffic from Wing Kwong Street heading to Hung Hom Road and the east direction can exit directly via Wan On Street. It minimizes the detouring of the traffic at Ma Tau Wai Road/Wing Kwong Street junction and Ma Tau Wai Road/Bailey Street junction.
- 4.17 Ingress traffic from Bailey Street to Ngan Hon Street could take a route of the proposed network system from Wan On Street via its extension instead of detouring via Bailey eastbound, Sung On Street northbound, Lok Shan Road Westbound, and To Kwa Wan Road southbound.
- 4.19 Extension of Wan On Street facilitates egress traffic to the north via Lung Tak Street to Chi Kiang Street instead of detouring via Bailey Street eastbound and Sung On Street northbound.
- 4.20 Road safety can be improved as egress traffic from Wan On Street can exit via Wing Kwong Street and Ma Tau Wai Road southbound carriageway instead of turning movements from Wan On Street to Bailey Street, where egress traffic has to cross 3 traffic lanes running to Ma Tau Wai Road and the west direction.
- 4.21 A section of extension of Wan On Street between Wing Kwong Street and Ngan Hon Street involves resumption of land and is implemented by Cap. 124 Land Resumption Ordinance through land resumption application of each site. URA will make an application for resumption within 12 months after the Development Projects, i.e. KC-011 and KC-012 have been authorized by the Secretary for Development while resumption application for the Scheme will be made within 12 months after the Scheme prepared under the Town Planning Ordinance (TPO) has been approved by Chief Executive in Council in accordance with section 9 of the Ordinance.

Pedestrian Circulation

- 4.22 Adequate podium setback at ground floor level from road of Hung Fook Street and Ngan Hon Street will be provided to create a 2.5m footpath to improve walking environment along the streets.

Implication on Air Ventilation

- 4.23 Based on the wind rose extracted from the PlanD- RAMS (Grid 83, 41), the annual prevailing winds are from the north-east, east and south-west where Victoria Harbour is located. Therefore, generally speaking, the area should be enjoying the sea breeze from these directions. The wind from the waterfront can penetrate via the north-south and east-west streets which orientation aligns well with the prevailing wind directions. However, with the present of dense and compact built environment, it is expected that narrows streets are not efficient air paths due to the friction and roughness of buildings along them. For those roads with wider width, such as Kowloon City Road and To Kwa Wan Road, it is considered to be important air paths for the study area.
- 4.24 During the summer period, the prevailing winds are from the southwest to east directions. From the wind rose of RAMS, the wind from southwest direction is also one of the dominate wind directions during summer period. It is also found that the winds from east to southeast direction are also dominated in summer wind direction. Since Victoria Harbour is also located to the east and southeast of the area, it is considered that the sea breeze from east to southeast direction would also be dominate of the area during summer period. After review the dominate wind flows and the compact built area, it is considered that the wider road, Chatham Road North and Kowloon City Road (align from northeast to southwest) would be the major air path of the area in summer direction. For the sea breeze, it will be similar to the annual condition.
- 4.25 The proposed development is not located at the upwind of the schools (such as CCC Kei To Secondary School), the proposed development would not have any signification impact on its air ventilation performance under this wind direction.
- 4.26 Since there are existing built developments with building height ranging from 8 to 35 storeys located to the east of the proposed development, it is expected that the wind will flow along the streets or path which is along the east to west array, such as Ngan Hon Street or the path

between the building along the Sung On Street. The proposed development will not affect the existing Ngan Hon Street, the wind flow along this street would not be significantly affected.

- 4.27 As the existing CCC Kei To Secondary is located at the upwind of the Hung Fook Street, this school has blocked the wind flow to this street. Similarly, there are existing medium rise buildings at the upwind of the Wing Kwong Street, the wind flow at this street is reduced. Since the proposed development is not located at the upwind of these streets, it is considered that the proposed development would not have any significant impact on the air ventilation performance of these streets.
- 4.28 As the proposed development conform with the existing R(A) development in terms of plot ratio and building height, it is considered that there would not have any significant adverse air ventilation impact upon the surrounding area under Annual Wind Condition and Summer Wind condition due to the proposed development.

Technical Assessment

Traffic Impact

- 4.29 A TIA (see **Appendix 2**) has been conducted to assess the traffic impact of the Scheme and the proposed through road within the Scheme area. The TIA demonstrated that the Scheme has no adverse traffic impact to the local traffic network. The proposed through road extending from Wan On Street to Ngan Hon Street enhances the vehicular and pedestrian accessibility of the Development Scheme area.

Environmental Aspect

- 4.30 An Environmental Assessment (EA) (see **Appendix 3**) was conducted to study any potential environmental impact/ benefit associated with the implementation of the Scheme. The study concluded that the impact on air quality, noise, and waste management was not insurmountable. The URA will ensure that satisfactory environmental standards are being met at the detailed design stage and during implementation.
- 4.31 **Drainage and Sewerage Impact**
A Drainage and Sewerage Impact Assessment (DSIA) was conducted (see **Appendix 4**). The DSIA report concluded that the drainage and sewerage impacts from the proposed development in the Scheme to the existing drainage and sewerage system are considered insignificant.

Social Impact

- 4.32 In accordance to the Urban Renewal Strategy (URS), a non-obtrusive SIA (Stage 1) has been conducted and the report is included as **Appendix 5**. The report highlights characteristics of the local population which will need to be prepared for and borne in mind during the implementation of the Scheme. The Stage 2 SIA Report is under preparation based on factual data and opinions collected as part of the freezing survey, which has been conducted on the commencement of the Development Scheme in accordance to the section 23(1) of the URAO. The Stage 2 SIA Report will be submitted to TPB separately.

5 PLANNING MERTIS

5.1 The Scheme will provide the following planning and environmental benefits:-

- removal of old building of over 50 years in deteriorating condition and demolition of sub-divided units;
- the existing over-crowding and sharing households will be able to improve the living conditions through redevelopment;
- providing more small to medium-sized flats in the urban area;
- satisfying needs of car parking spaces while lessening illegal parking in the area;
- maintaining existing continuous streetscape by avoiding the necessity for providing car park provisions in new developments of the Wider Area;
- lessening the conflicts between pedestrians and vehicular run-in/outs in the Development Scheme area;
- enhancing street vibrancy of the Scheme by providing setback at the street level of the new development;
- enhancing road safety to road users;
- enhancing traffic and pedestrian accessibility in the area; and
- reducing the circulating traffic at the public road network around the area.

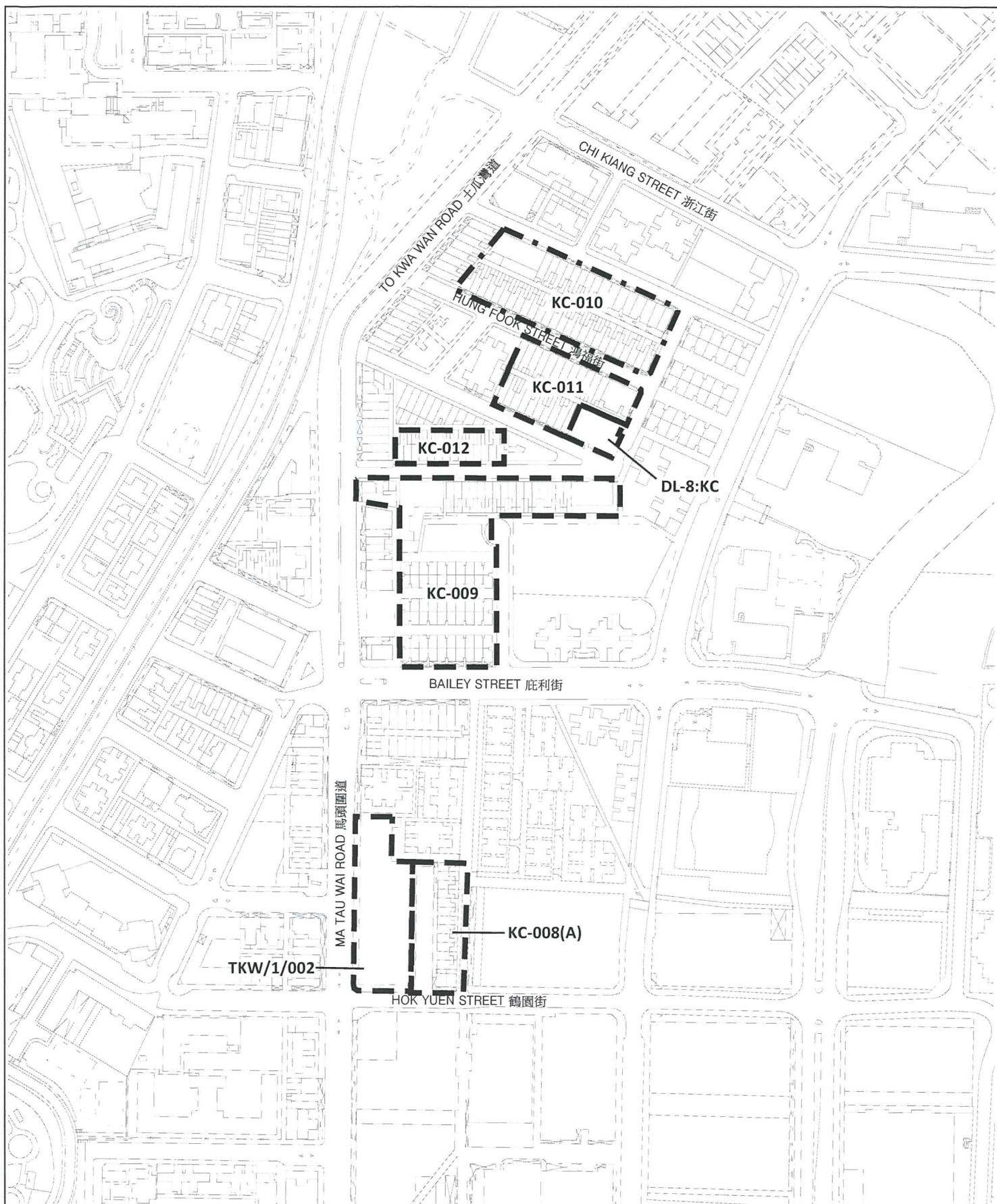
6 IMPLEMENTATION OF THE DEVELOPMENT SCHEME

6.1 The URA does not own or lease any land within the boundaries of the Scheme and will acquire all the private lots within the Development Scheme boundary. The URA intends to acquire the property within the Scheme boundary by purchase under the current acquisition policy. An owner of domestic and non-domestic property will receive the market value of his property (valued on vacant possession basis). As for the owner-occupier of domestic property, on top of the said market value, the URA will pay a Home Purchase Allowance so as to assist the owner in purchasing a 7-year old domestic flat of similar size within the same locality.

- 6.2 The Scheme provides an opportunity to offer flat-for-flat stock in the in-situ site or site in the vicinity for the affected owner occupiers.
- 6.3 Under current compensation policy, all eligible tenants who are affected by the URA redevelopment projects, and whose landlords sell the property to the URA subject to existing tenancies, will be offered rehousing. Tenants who are not allocated rehousing units due to various reasons, may receive ex-gratia payment.
- 6.4 Eligible domestic tenants required to move from properties affected by the URA projects will be offered a rehousing unit provided by the Hong Kong Housing Authority or the Hong Kong Housing Society. Tenants who are rehoused as a result of a URA project will be offered an ex-gratia removal allowance. Non-domestic tenants whose tenancies are terminated by their landlords are not entitled to any compensation or other payments. However, the URA is prepared to pay 3 times the rateable value of the affected premises as an ex-gratia allowance for non-domestic tenants if such premises are purchased by the URA subject to existing tenancies.
- 6.5 The URA may implement the Development Scheme in association another person/party or implementing the Scheme by itself alone.
- 6.6 Supplementary documents detailing the implementation programme for the Scheme and the URA's rehousing, compensation, acquisition and resumption principles are attached in **Appendices 6, 7 & 8**.
- 6.7 Any information contained in this document relating to compensation and re-housing benefits are with reference to the URA's prevailing policy on compensation and re-housing benefit ("Compensation Package") offered by the URA to owners/ tenants at the time of issuance of this document. The Compensation Package may be subject to change from time to time upon any review carried out by the URA. The Compensation Package to be offered by the URA to owners/ tenants affected by the URA's project shall be that Compensation Package prevailing at the time of offer. Nothing contained in this document shall constitute any representation or warranty on the part of the URA or give rise to any expectation that the Compensation Package contained in this document will not be changed at the time of implementation of the project.

URBAN RENEWAL AUTHORITY

JUNE 2016



THE SCHEME



SURROUNDING URA
PROJECTS

EXTRACT PLAN PREPARED ON 05.04.16
BASED ON SURVEY SHEET No.11-NW-20D,
11-NE-16C.

LOCAL COMMUNITY PLANNING IN
KOWLOON CITY
COMMENCED PROJECTS IN THE STUDY

SCALE 1 : 3000

METRES 30 0 30 60 90 120 150 METRES

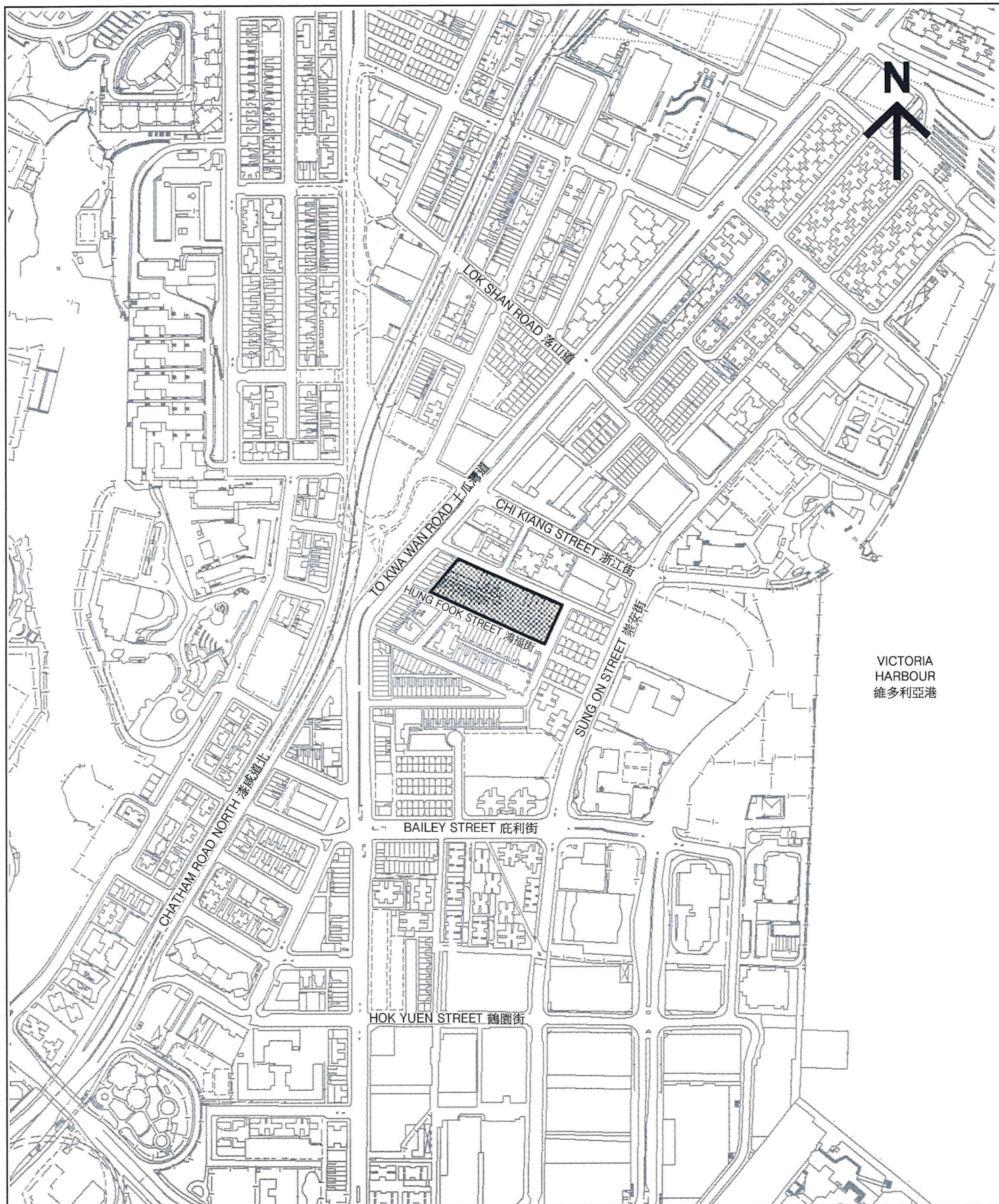


市區重建局
URBAN RENEWAL
AUTHORITY

KC-010

PLAN

1



THE SCHEME

EXTRACT PLAN PREPARED ON 05.04.16
BASED ON SURVEY SHEET No.11-NW-20D,
11-NE-16C.

LOCATION PLAN

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET

SCALE 1 : 5000

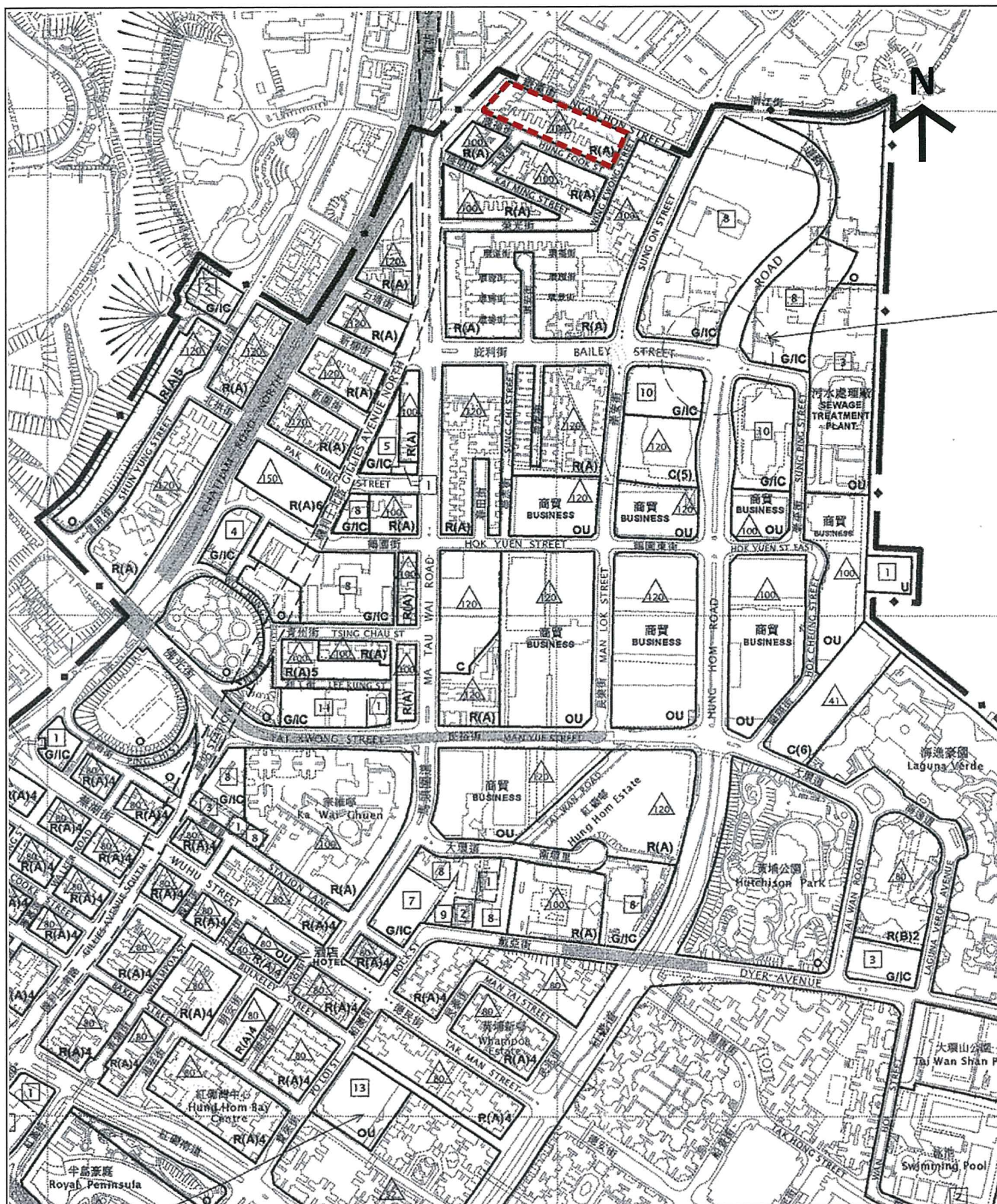
METRES 100 0 100 200 METRES



KC-010

PLAN

2



THE SCHEME

EXTRACT PLAN BASED ON
HUNG HOM OUTLINE ZONING PLAN
No.S/K9/24

EXISTING ZONING OF THE DEVELOPMENT SCHEME

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET

SCALE 1:5000

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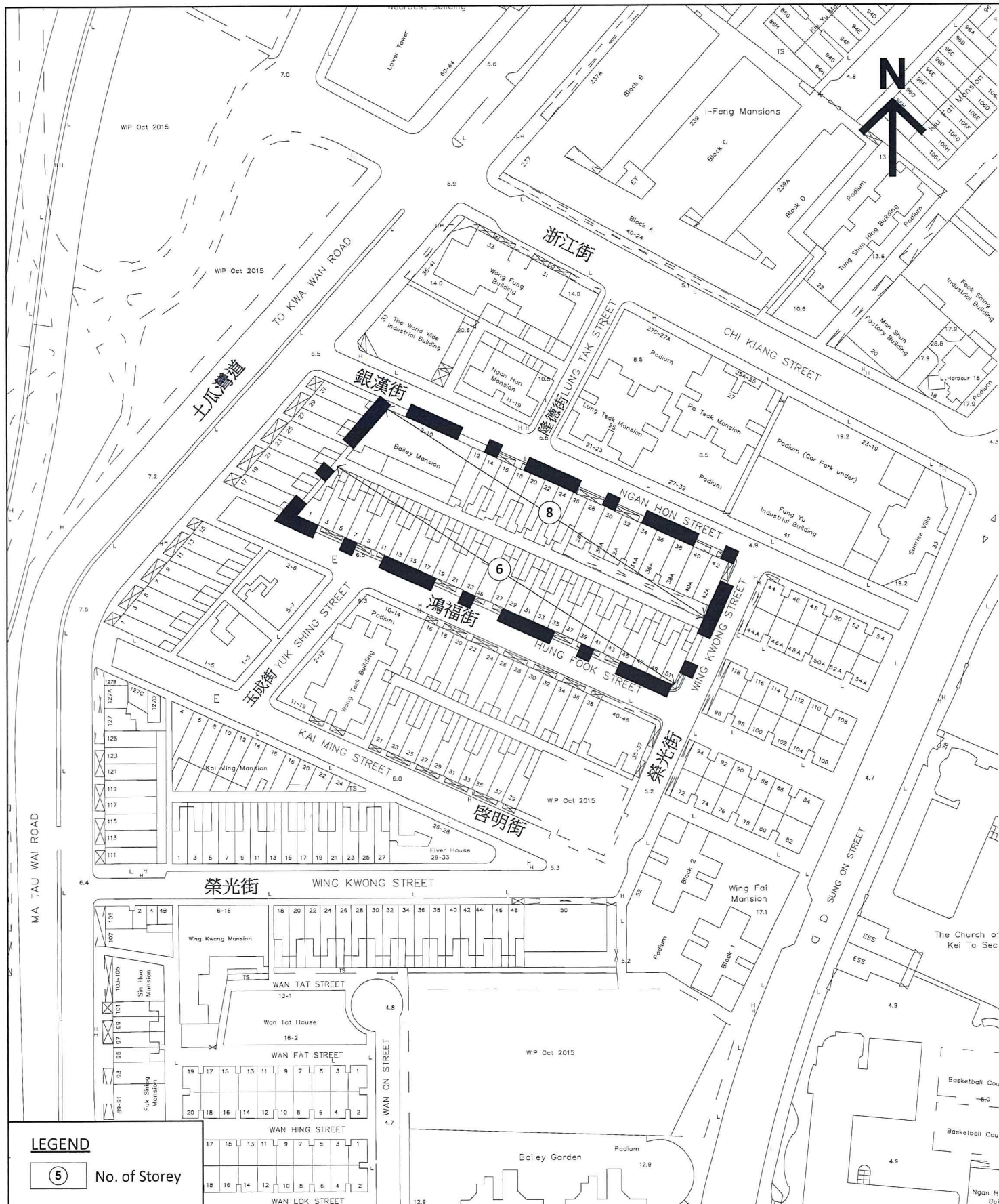


市區重建局
URBAN RENEWAL
AUTHORITY

KC-010

PLAN

4



LEGEND

⑤ No. of Storey

THE SCHEME

EXTRACT PLAN PREPARED ON 05.04.16
 BASED ON SURVEY SHEET No.11-NW-20D,
 11-NE-16C.

BUILDING HEIGHT

DEVELOPMENT SCHEME
 AT HUNG FOOK STREET / NGAN HON STREET

SCALE 1 : 1500

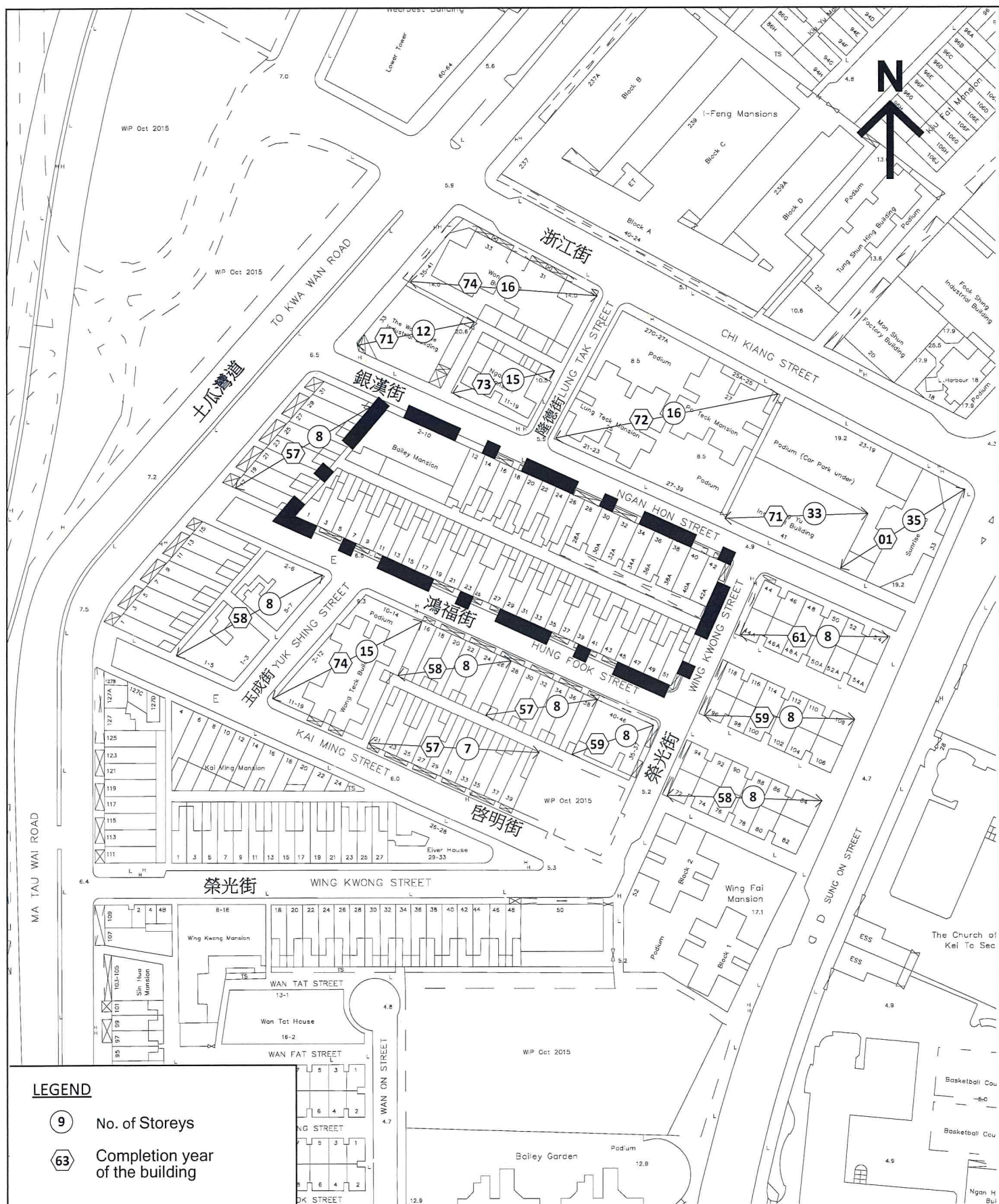
METRES 15 0 15 30 45 60 75 METRES



KC-010

PLAN

6



LEGEND

- 9 No. of Storeys
- 63 Completion year of the building



THE SCHEME

EXTRACT PLAN PREPARED ON 05.04.16
BASED ON SURVEY SHEET No.11-NW-20D,
11-NE-16C.

INFORMATION ON BUILDINGS IMMEDIATELY ADJACENT TO SCHEME AREA

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET

SCALE 1 : 1500

METRES 15 0 15 30 45 60 75 METRES

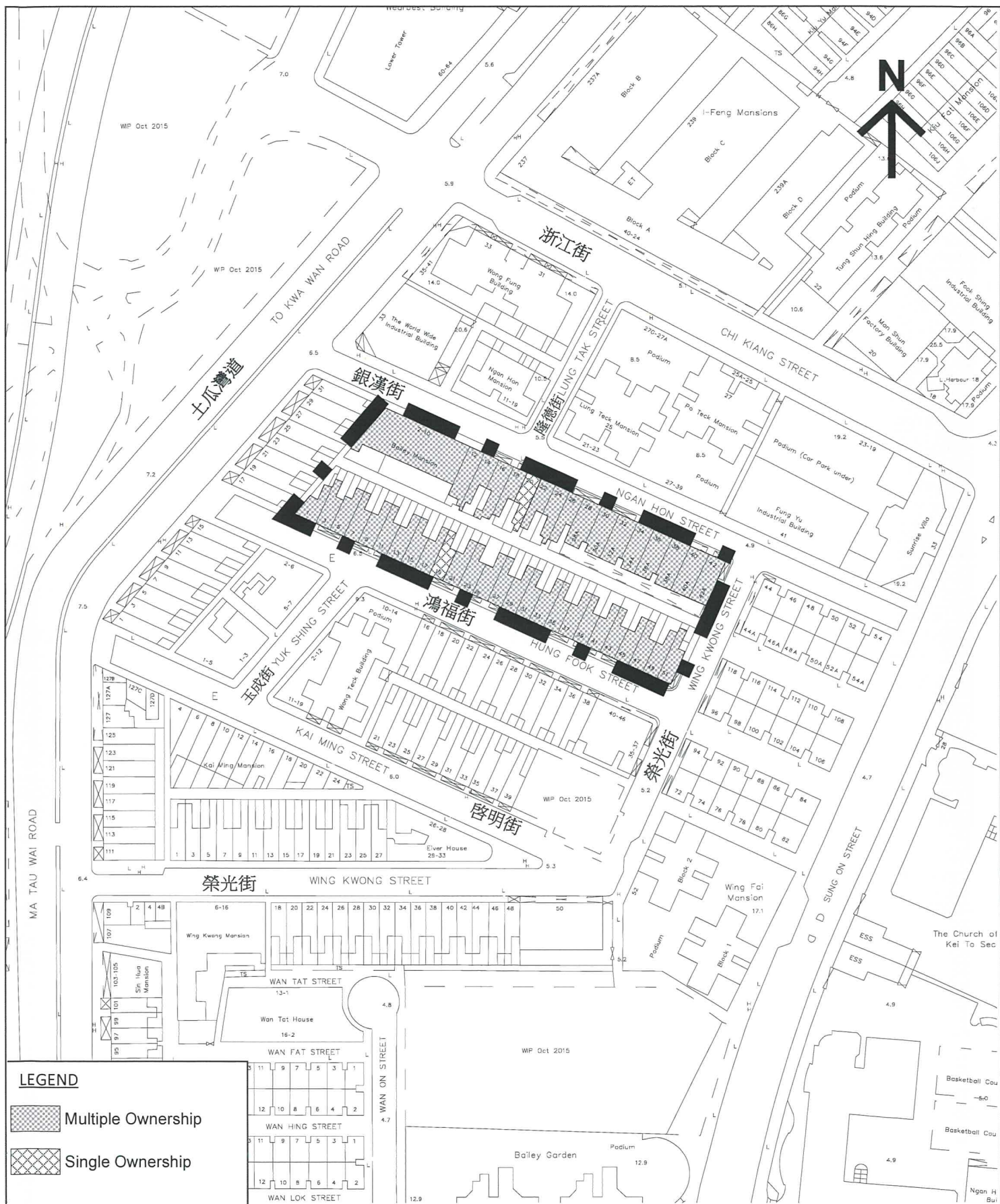


市區重建局
RENEWAL
AUTHORITY

KC-010

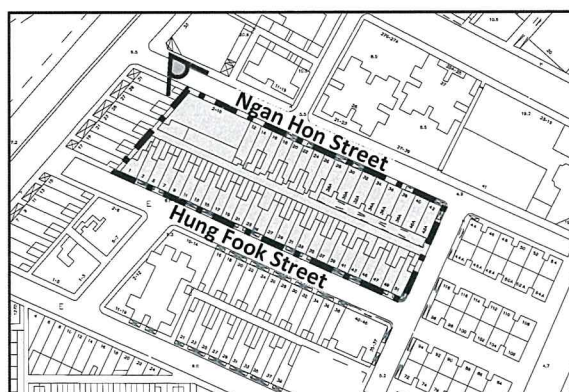
PLAN

8





Photos 1:
Overview of Hung Fook Street / Ngan Hon Street Development Scheme



PHOTOS TAKEN IN
MAY 2016

SITE PHOTOS

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET

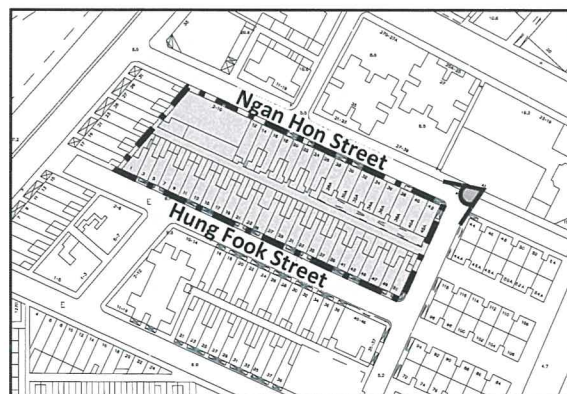


KC-010

PLAN
10



Photo 2:
View of the Site from Ngan Hon Street



PHOTOS TAKEN IN
APRIL 2016

SITE PHOTOS

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET

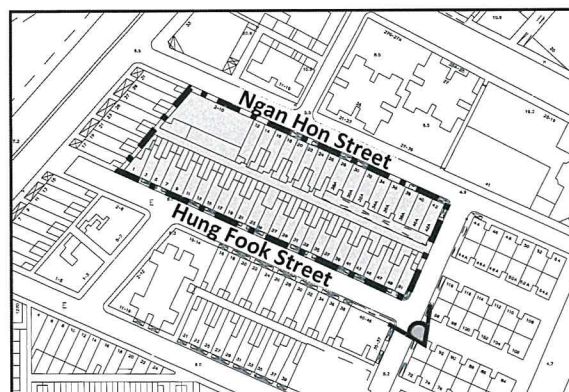
 市區重建局
URBAN RENEWAL
AUTHORITY

KC-010

PLAN
11



Photo 3:
View of the Site from Hung Fook Street



PHOTOS TAKEN IN
APRIL 2016

SITE PHOTOS

DEVELOPMENT SCHEME
AT HUNG FOOK STREET / NGAN HON STREET



KC-010

PLAN
12



Photo 4



Photo 5

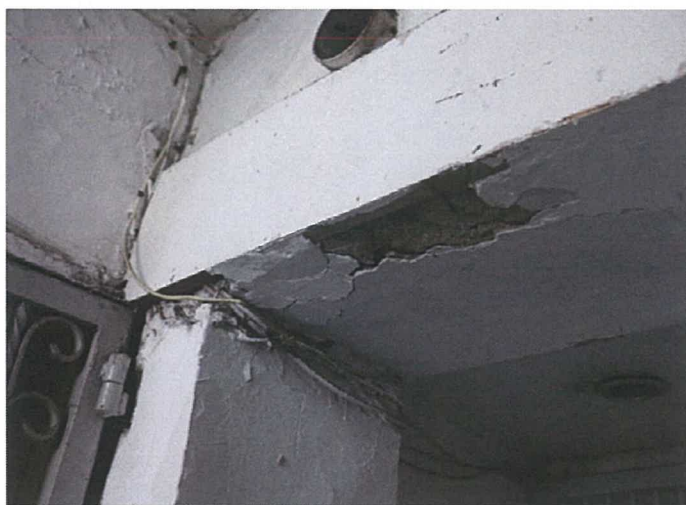


Photo 6



Photo 7

Photos 4-7:
Concrete spalling, cracks and exposed of steel bars in common areas

PHOTOS TAKEN IN NOBMEBER 2015	<p><u>SITE PHOTOS</u></p> <p>DEVELOPMENT SCHEME AT HUNG FOOK STREET / NGAN HON STREET</p>	 <p>市區重建局 URBAN RENEWAL AUTHORITY</p>	
		KC-010	<p>PLAN 13</p>



Photo 8



Photo 9



Photo 10

Photos 8-10:
Unauthorized structures are found at rooftops and rear of buildings in the Site.


PHOTOS TAKEN IN APRIL 2016	<p><u>SITE PHOTOS</u></p> <p>DEVELOPMENT SCHEME AT HUNG FOOK STREET / NGAN HON STREET</p>	 <p>市區重建局 URBAN RENEWAL AUTHORITY</p>	
		KC-010	PLAN 14



Photo 11



Photo 12

Photos 11 - 12:
Sub-division of residential units

PHOTOS TAKEN IN APRIL 2016	<u>SITE PHOTOS</u> DEVELOPMENT SCHEME AT HUNG FOOK STREET / NGAN HON STREET	 市區重建局 URBAN RENEWAL AUTHORITY	
		KC-010	PLAN 15



Photo 13



Photo 14



Photo 15

Photos 13-15:
G/F Shops at Hung Fook Street


PHOTOS TAKEN IN MAY 2016	<p><u>SITE PHOTOS</u></p> <p>DEVELOPMENT SCHEME AT HUNG FOOK STREET / NGAN HON STREET</p>	 市區重建局 URBAN RENEWAL AUTHORITY	
		KC-010	PLAN 16



Photo 1



Photo 2

Photos 1-2:

Car park entrance interferes with pedestrian safety and continuous street fronts

PHOTOS TAKEN IN
MAY 2016

SITE PHOTOS

Car Park Entrance of Individual Development

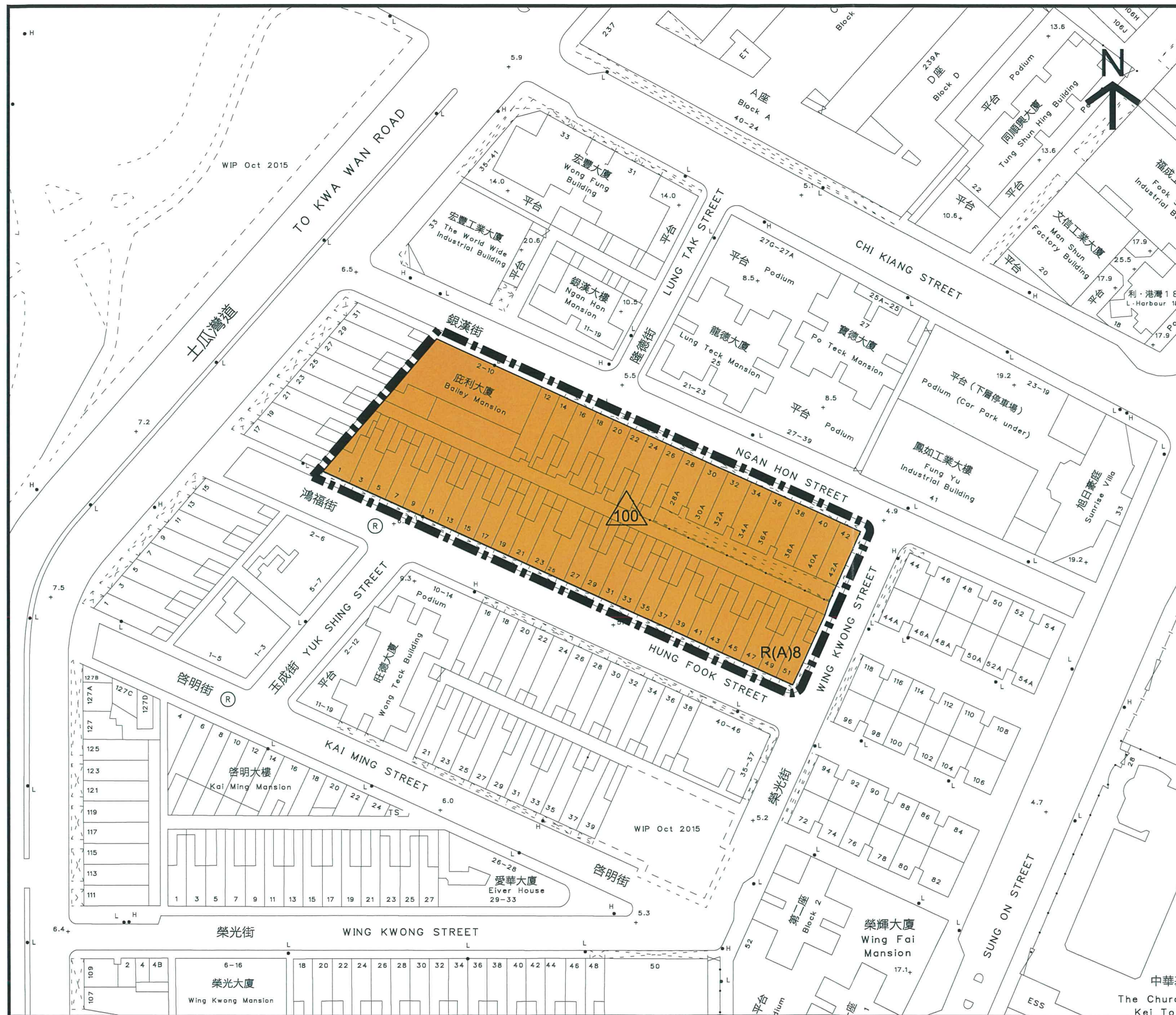


KC-010

PLAN
17

PART 2

THE DRAFT PLAN



圖例 NOTATION

BOUNDARY OF DEVELOPMENT.....發展計劃範圍界線
SCHEME

RESIDENTIAL (GROUP A)8.....住宅(甲類)8

MAXIMUM BUILDING HEIGHT.....最高建築物高度
(100 METRES ABOVE PRINCIPAL DATUM) (在主水平基準上100米)

夾附的<<註釋>>屬這份圖則的一部分

THE ATTACHED NOTES
ALSO FORM PART OF THIS PLAN

2016年 月 日城市規劃委員會根據市區重建局條例第25條(6)(a)認為該圖則適宜公布,並於2016年 月 日按照城市規劃條例第5條公開展示的圖則。
PLAN DEEMED SUITABLE BY THE TOWN PLANNING BOARD FOR PUBLICATION UNDER S.25(6) (a) OF THE URBAN RENEWAL AUTHORITY ORDINANCE ON THE DAY OF 2016 AND EXHIBITED UNDER SECTION 5 OF THE TOWN PLANNING ORDINANCE ON THE DAY OF 2016.

SECRETARY, TOWN PLANNING BOARD 城市規劃委員會秘書

香港城市規劃委員會依據城市規劃條例擬備的市區重建局鴻福街/銀漢街發展計劃圖則 TOWN PLANNING ORDINANCE, HONG KONG TOWN PLANNING BOARD URBAN RENEWAL AUTHORITY HUNG FOOK STREET / NGAN HON STREET DEVELOPMENT SCHEME PLAN

SCALE 1:1000 比例尺
米 METRES 10 0 10 20 30 40 50 METRES米

依據市區重建局條例第25(3)(a)條擬備
PREPARED UNDER SECTION 25(3)(a) OF THE URBAN RENEWAL AUTHORITY ORDINANCE

圖則編號
PLAN No. S/K9/URA2/A

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the Purposes of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

NOTES

(N.B. These form part of the Plan)

- (1) These Notes show the uses or developments on land falling within the boundaries of the Plan which are always permitted and which may be permitted by the Town Planning Board, with or without conditions, on application. Where permission from the Town Planning Board for a use or development is required, the application for such permission should be made in a prescribed form. The application shall be addressed to the Secretary of the Town Planning Board, from whom the prescribed application form may be obtained.
- (2) Any use or development which is always permitted or may be permitted in accordance with these Notes must also conform to any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, as may be applicable.
- (3)
 - (a) No action is required to make the existing use of any land or building conform to this Plan until there is a material change of use or the building is redeveloped.
 - (b) Any material change of use or any other development (except minor alteration and/or modification to the development of the land or building in respect of the existing use which is always permitted) or redevelopment must be always permitted in terms of the Plan or, if permission is required, in accordance with the permission granted by the Town Planning Board.
 - (c) For the purposes of subparagraph (a) above, "existing use of any land or building" means –
 - (i) before the publication in the Gazette of the notice of the first statutory plan covering the land or building (hereafter referred as 'the first plan'),
 - a use in existence before the publication of the first plan which has continued since it came into existence; or
 - a use or a change of use approved under the Buildings

Ordinance which relates to an existing building; and

(ii) after the publication of the first plan,

- a use permitted under a plan which was effected during the effective period of that plan and has continued since it was effected; or
 - a use or a change of use approved under the Buildings Ordinance which relates to an existing building and permitted under a plan prevailing at the time when the use or change of use was approved.
- (4) Except as otherwise specified by the Town Planning Board, when a use or material change of use is effected or a development or redevelopment is undertaken, as always permitted in terms of the Plan or in accordance with a permission granted by the Town Planning Board, all permissions granted by the Town Planning Board in respect of the site of the use or material change of use or development or redevelopment shall lapse.
- (5) Road widths, road junctions and alignments of roads may be subject to minor adjustments as detailed planning proceeds.
- (6) Temporary uses (expected to be 5 years or less) of any land or building are always permitted as long as they comply with any other relevant legislation, the conditions of the Government lease concerned, and any other Government requirements, and there is no need for these to conform to the zoned use or these Notes. For temporary uses expected to be over 5 years, the uses must conform to the zoned use or these Notes.
- (7) The following uses or developments are always permitted on land falling within the boundaries of the Plan except where the uses or developments are specified in Column 2 of the Schedule of Uses:
- (a) provision, maintenance or repair of plant nursery, amenity planting, open space, rain shelter, refreshment kiosk, road, bus/public light bus stop or lay-by, cycle track, Mass Transit Railway station entrance, Mass Transit Railway structure below ground level, taxi rank, nullah, public utility pipeline, electricity mast, lamp pole, telephone booth, telecommunications radio base station, automatic teller machine and shrine; and
 - (b) geotechnical works, local public works, road works, sewerage works, drainage works, environmental improvement works, marine related facilities, waterworks (excluding works on service reservoir) and such other public works co-ordinated or implemented by Government.
- (8) Unless otherwise specified, all building, engineering and other operations incidental to and all uses directly related and ancillary to the permitted uses

and developments within the same zone are always permitted and no separate permission is required.

- (9) In these Notes, “existing building” means a building, including a structure, which is physically existing and is in compliance with any relevant legislation and the conditions of the Government lease concerned.
- (10) Any development not compatible with the Urban Renewal Authority’s Development Scheme for the area is prohibited by virtue of section 25(4) of the Urban Renewal Authority Ordinance.

S/K9/URA2/A

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

Schedule of Uses

Page

RESIDENTIAL (GROUP A) 8

1

RESIDENTIAL (GROUP A) 8

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Ambulance Depot	Commercial Bathhouse/ Massage
Flat	Establishment
Government Use (not elsewhere specified)	Eating Place
House	Education Institution
Library	Exhibition or Convention Hall
Market	Government Refuse Collection Point
Place of Recreation, Sports or Culture	Hospital
Public Clinic	Hotel
Public Transport Terminus or Station (excluding open-air terminus or station)	Institutional Use (not elsewhere specified)
Public Vehicle Park (coach parking only)	Mass Transit Railway Vent Shaft and/or Other Structure above Ground Level other than Entrances
Residential Institution	Office
School (in free-standing purpose-designed building only)	Petrol Filling Station
Social Welfare Facility	Place of Entertainment
Utility Installation for Private Project	Private Club
	Public Convenience
	Public Transport Terminus or Station (not elsewhere specified)
	Public Utility Installation
	Public Vehicle Park (excluding container vehicle) (not elsewhere specified)
	Religious Institution
	School (not elsewhere specified)
	Shop and Services
	Training Centre

(Please see next page)

RESIDENTIAL (GROUP A) 8 (Cont'd)

In addition, the following uses are always permitted (a) on the lowest three floors of a building, taken to include basements; or (b) in the purpose-designed non-residential portion of an existing building, both excluding floors containing wholly or mainly car parking, loading / unloading bay and / or plant room:

Eating Place
Educational Institution
Institutional Use (not elsewhere specified)
Off-course Betting Centre
Office
Place of Entertainment
Private Club
Public Convenience
Recyclable Collection Centre
School
Shop and Services
Training Centre

Planning Intention

This zone is intended primarily for high-density residential developments with the provision of an underground car park, that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

Remarks

- (1) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in the plot ratio for the building upon development and/ or redevelopment in excess of 7.5 for a domestic building or 9.0 for a building that is partly domestic and partly non-domestic, or the plot ratio of the existing building, whichever is the greater. Except where the plot ratio is permitted to be exceed under paragraphs (6) and/or (7) hereof, under no circumstances shall the plot ratio for the domestic part of any building, to which this paragraph applies, exceed 7.5.
- (2) For a non-domestic building to be erected on the site, the maximum plot ratio shall not exceed 9.0 except where the plot ratio is permitted to be exceeded under paragraphs (6) and/or (7) hereof.

- (3) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of the maximum building height in terms of metres above Principal Datum (mPD) as stipulated on the Plan, or the height of the existing building, whichever is the greater.
- (4) In determining the relevant maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for use solely as plant room, caretaker's office, or caretaker's quarters and recreational facilities for the use and benefit of all the owners or occupiers of the domestic building or domestic part of the building, provided such uses and facilities are ancillary and directly related to the development or redevelopment, may be disregarded.
- (5) An underground car park shall be provided. The number of car parking spaces, loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by government. In determining the maximum plot ratio for the purposes of paragraphs (1) or (2) above, any floor space that is constructed or intended for the use solely as the underground car park may be disregarded.
- (6) Where the permitted plot ratio as defined in Building (Planning) Regulations is permitted to be exceeded in circumstances as set out in Regulation 22(1) or (2) of the said Regulations, the plot ratio for the building on land to which paragraphs (1) or (2) applies may be increased by the additional plot ratio by which the permitted plot ratio is permitted to be exceeded under and in accordance with the said Regulation 22(1) or (2), notwithstanding that the relevant maximum plot ratio specified in the paragraphs (1) or (2) above may thereby be exceeded.
- (7) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the restrictions on building height and plot ratio as stated in paragraphs (1) to (3) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

EXPLANATORY STATEMENT

DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

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DRAFT URBAN RENEWAL AUTHORITY
HUNG FOOK STREET / NGAN HON STREET
DEVELOPMENT SCHEME PLAN NO. S/K9/URA2/A

(Being a Draft Plan for the purpose of the Town Planning Ordinance prepared by the Urban Renewal Authority under section 25 of the Urban Renewal Authority Ordinance)

EXPLANATORY STATEMENT

Note: For the purposes of the Town Planning Ordinance, this statement shall not be deemed to constitute part of the Plan.

1. INTRODUCTION

This Explanatory Statement is intended to assist an understanding of the draft Urban Renewal Authority (URA) Hung Fook Street / Ngan Hon Street Development Scheme Plan (the Plan) No. S/K9/URA2/A. It reflects the planning intention and objectives of the Town Planning Board (the Board) for the area covered by the Plan.

2. AUTHORITY FOR THE PLAN AND PROCEDURES

- 2.1 In the URA's 15th Business Plan (2016/17) approved by the Financial Secretary recently, Hung Fook Street / Ngan Hon Street Development Scheme (KC-010) was proposed to be processed as a Development Scheme under section 25 of the URA Ordinance (URAO).
- 2.2 On 3 June 2016, pursuant to section 23(1) of the URAO, the URA notified in the Government Gazette the commencement of implementation of the Hung Fook Street / Ngan Hon Street Development Scheme.

- 2.3 On 10 June 2016, the URA submitted the draft URA Hung Fook Street / Ngan Hon Street Development Scheme Plan for the Development Scheme to the Board under section 25(5) of the URAO.
- 2.4 On XXXX, the Board, under section 25(6)(a) of the URAO, deemed the draft URA Hung Fook Street / Ngan Hon Street Development Scheme Plan as being suitable for publication. Under section 25(7) of the URAO, the draft Development Scheme Plan, which the Board has deemed suitable for publication under section 25(6)(a) of the URAO, is deemed to be a draft plan prepared by the Board for the purposes of the Town Planning Ordinance (the Ordinance).
- 2.5 On XXXX, the draft Hung Fook Street / Ngan Hon Street Development Scheme Plan No. S/K9/URA2/1 (the Plan) was exhibited under section 5 of the Ordinance. By virtue of section 25(9) of the URAO, the Plan has from the date replaced the draft Hung Hom Outline Zoning Plan (OZP) No. S/K9/25 in respect of the area delineated and described herein.

3. OBJECT OF THE PLAN

The Plan illustrates that the Development Scheme Area (the Area) is designated as “Residential (Group A)8” (“R(A)8”). It is planned to be developed by means of the Development Scheme prepared under section 25 of the URAO. The Scheme intends to be primarily for high-density residential developments with the provision of an underground car park that will accommodate car parking spaces and loading/unloading facilities ancillary to developments and/or redevelopments within the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street (the Wider Area) as demarcated on **Plan 1** as well as coach parking spaces to cater for the need of coach parking in the district. Commercial uses are always permitted on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.

4. NOTES OF THE PLAN

- 4.1 Attached to the Plan is a set of Notes which shows the types of uses or developments which are always permitted within the Area in this zone and which may be permitted by the Board, with or without conditions, on application. The provision for application for planning permission under section 16 of the Ordinance allows greater flexibility in land use planning and control of development to meet changing needs.
- 4.2 For the guidance of the general public, a set of definitions that explains some of the terms used in the Notes may be obtained from the Technical Services Division of the Planning Department and can be downloaded from the Board's website at <http://www.info.gov.hk/tpb>.

5. AREA COVERED BY THE PLAN

- 5.1 The Scheme boundary which is shown in heavy broken line on the Plan, covers a total area of about 4,951m². The Scheme comprises tenement buildings which are bounded by Ngan Hon Street to the north, Wing Kwong Street to the east, Hung Fook Street to the south and a back lane of the adjacent buildings to the west.
- 5.2 The Scheme boundary has included Government lane and pavement area.
- 5.3 On the approved Hung Hom OZP No. S/K9/24, the Area is zoned "Residential (Group A)" before the exhibition of the Plan.

6. EXISTING CONDITIONS

- 6.1 The buildings within the Area are between 6 and 8 storeys and residential in nature with construction material retail shops, eating places, workshops and vehicle repair workshops on lower floors. Most of the existing buildings are in deteriorating or poor condition with some sub-divided units on the upper floors. Staircase shops are found at the entrance of staircase in some buildings at ground floor.

- 6.2 The poor housing condition, the presence of illegal rooftop structures, temporary structures at back lanes, and a number of vehicle repair workshops at ground floor are sources of environmental nuisance in the area.

7. PLANNING AND LAND USE PROPOSALS

- 7.1 On the Plan, the Area has been zoned “R(A)8” and the Notes of the Plan indicated broadly the intended land use within the Area.

Uses

- 7.2 Development or redevelopment within the “R(A)8” zone is intended for high-density residential development with commercial uses on the lowest three floors of a building or in the purpose-designed non-residential portion of an existing building.
- 7.3 The maximum plot ratio within the “R(A)8” zone is 9.0, or the plot ratio of the existing building(s), whichever is the greater. Except where the plot ratio is permitted to be exceeded under the Notes of the Plan, under no circumstances shall the plot ratio for the domestic part of any development exceed 7.5.

Car Park

- 7.4 An underground car park will be provided within the Development Scheme. It will accommodate car parking spaces and loading/unloading bays ancillary to developments and/or redevelopments within the Wider Area to meet the parking needs and avoid car park run in/outs at individual developments in the Wider Area. Coach parking spaces will also be provided to cater for the demand for coach parking in the district.
- 7.5 Three URA Development Projects are located within the Wider Area, i.e., Kai Ming Street (DL-8:KC), Hung Fook Street/ Kai Ming Street (KC-011) and Wing Kwong Street (KC-012) to the immediate south of

the Development Scheme. KC-011 and KC-012 are commenced by the URA under section 26 of the URAO on the same date as the Development Scheme. Limited on-site loading/unloading bays will be provided at each of the Development Projects while car parking spaces will not be provided. The underground car park within the Development Scheme will accommodate the remaining number of loading/unloading bay and car parking spaces provision for the three Development Projects.

- 7.6 The number of car parking spaces and loading/unloading bays and coach parking spaces to be provided in the underground car park shall be as required by the Government and in any event there shall be not more than 274 car park spaces and not more than 20 loading/unloading bays and 4 coach parking spaces. The maximum number of car park spaces and loading/ unloading bays provisions are based on the relevant requirements under the Hong Kong Planning Standards and Guidelines with reference to the total gross floor area (GFA) of the Development Scheme, developments and/or redevelopments within the Wider Area; i.e. about 104,000m² GFA and about 2,000 residential units. The number of coach parking spaces is as required by the Government to cater for the need for coach parking in the district.
- 7.7 Among the sites within the Wider Area to be served by the underground car park, the site area of the Development Scheme is relatively large and more efficient car park layout can be achieved. Provision of a single underground car park in the Development Scheme avoids disruption to through traffic movements and pedestrians while maintaining continuous street frontages on other sites in the Wider Area.
- 7.8 The underground car park is for accommodating car parking and loading/unloading bays ancillary to the development and/or redevelopment within the Wider Area and the coach parking spaces are to cater for such need in the district. As such, in determining the maximum plot ratio for the Development Scheme, any floor space that is constructed or intended for the use solely as the underground car park, may be disregarded. Management of the underground car park will be arranged at the development stage.

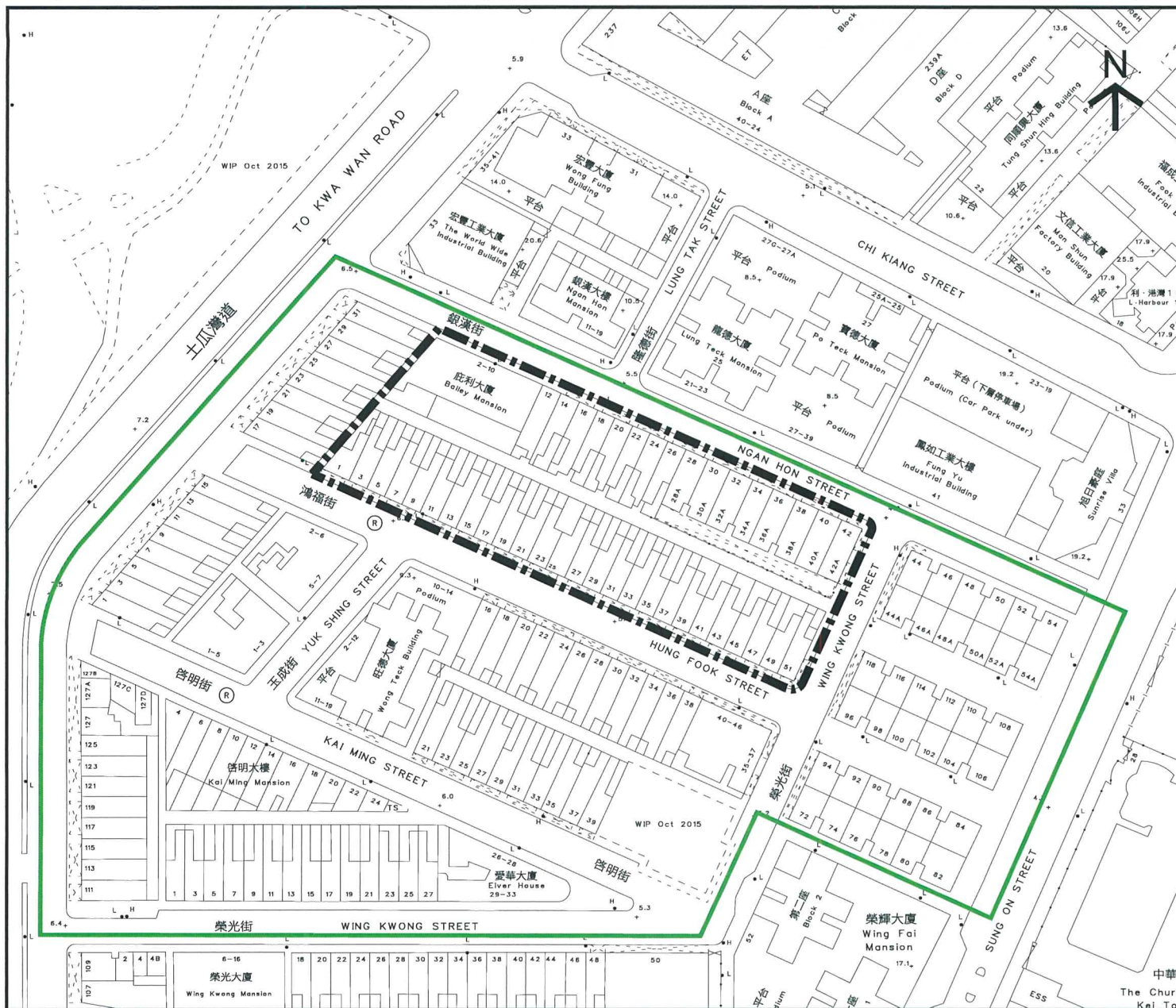
Local Road Improvement

- 7.9 Through road connecting Hung Fook Street and Ngan Hon Street within the Development Scheme, which in turn forms part of the proposed road network extending from Wan On Street of URA Development Project KC-009 (commenced on 4 March 2016), URA Development Project KC-011 and KC-012 (to be commenced on the same date of the Development Scheme under URAO), will be provided to improve the accessibility of the Development Scheme and the surrounding area.
- 7.10 **Plan 2** shows the proposed road network system extending from Wan On Street to Ngan Hon Street. It passes through the sites boundary of redevelopment projects KC-009, KC-010, KC-011 and KC-012. Except a road section of Wan On Street between Wing Kwong Street and Bailey Street is two-way carriageway, the road section between Kai Ming Street and Ngan Hon Street is a one-way carriageway.
- 7.11 With the proposed road network system, egress traffic from Wing Kwong Street heading Hung Hom Road and the east direction can exit directly via Wan On Street. It minimizes the detouring of the traffic at Ma Tau Wai Road/Wing Kwong Street junction and Ma Tau Wai Road/Bailey Street junction.
- 7.12 Ingress traffic from Bailey Street to Ngan Hon Street could take a route through the proposed network system from Wan On Street via its extension instead of detouring via Bailey Street eastbound, Sung On Street northbound, Lok Shan Road Westbound, and To Kwa Wan Road southbound.
- 7.13 Extension of Wan On Street facilitates egress traffic to the north via Lung Tak Street to Chi Kiang Street instead of detouring via Bailey Street eastbound and Sung On Street northbound.
- 7.14 A section of extension of Wan On Street between Wing Kwong Street and Ngan Hon Street involves resumption of land and is implemented by Cap 124 Lands Resumption Ordinance. URA will make an application for resumption within 12 months after the Development

Scheme Plan prepared under the Town Planning Ordinance (TPO) has been approved by the Chief Executive in Council in accordance with section 9 of the Ordinance.

8. IMPLEMENTATION OF THE SCHEME

- 8.1 The proposals set out in the Plan form an integral part of the Development Scheme for the Area.
- 8.2 The URA does not own or lease any land within the boundaries of the Development Scheme. The URA intends to acquire the property within the Area of the Development Scheme. With respect to any of such properties which cannot be acquired by purchase, the Secretary for Development would consider, upon the application of the URA, recommending to the Chief Executive in Council the resumption of properties under the Lands Resumption Ordinance, if necessary.
- 8.3 All eligible tenants will be offered an ex-gratia payment package in accordance with URA's policy. The URA has already entered into agreement with the Hong Kong Housing Society (HKHS) and the Hong Kong Housing Authority (HKHA) for the purpose of making available rehousing units by HKHS or HKHA to rehouse affected tenants who satisfy the eligibility criteria of HKHS or HKHA.
- 8.4 Non-domestic tenants of properties acquired by URA whose tenancies are terminated by URA due to implementation of the Development Scheme may be offered an ex-gratia allowance to assist in their business relocation.
- 8.5 The URA may implement the Development Scheme on its own or in association with one or more joint venture partners.

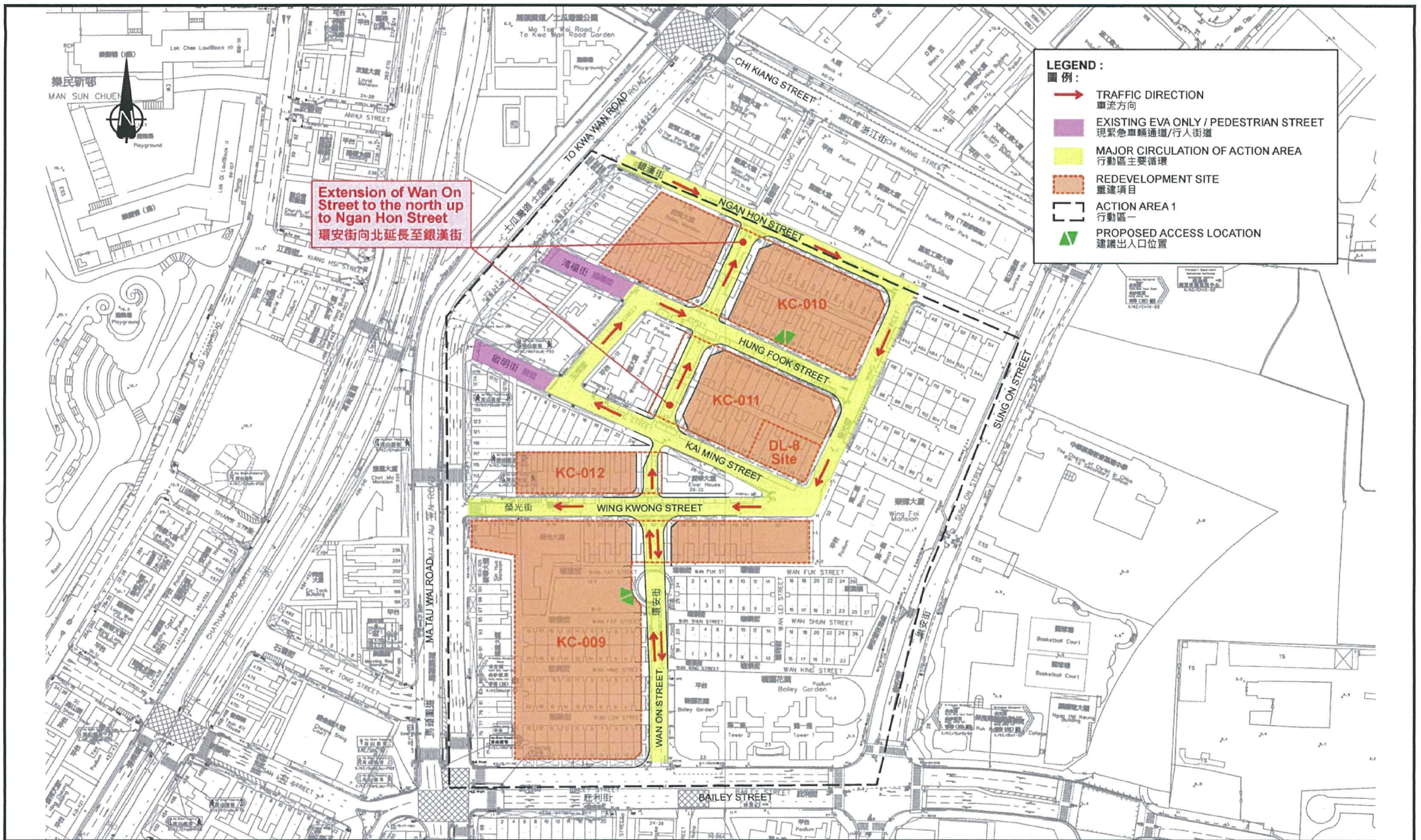


圖例 NOTATION

BOUNDARY OF HUNG FOOK STREET/.....鴻福街/銀漢街
NGAN HON STREET DEVELOPMENT SCHEME 發展計劃範圍界線

BOUNDARY OF WIDER AREA周邊範圍界線

發展計劃內地下停車場提供服務的周邊範圍
WIDER AREA SERVED BY UNDERGROUND CAR PARK IN THE
DEVELOPMENT SCHEME



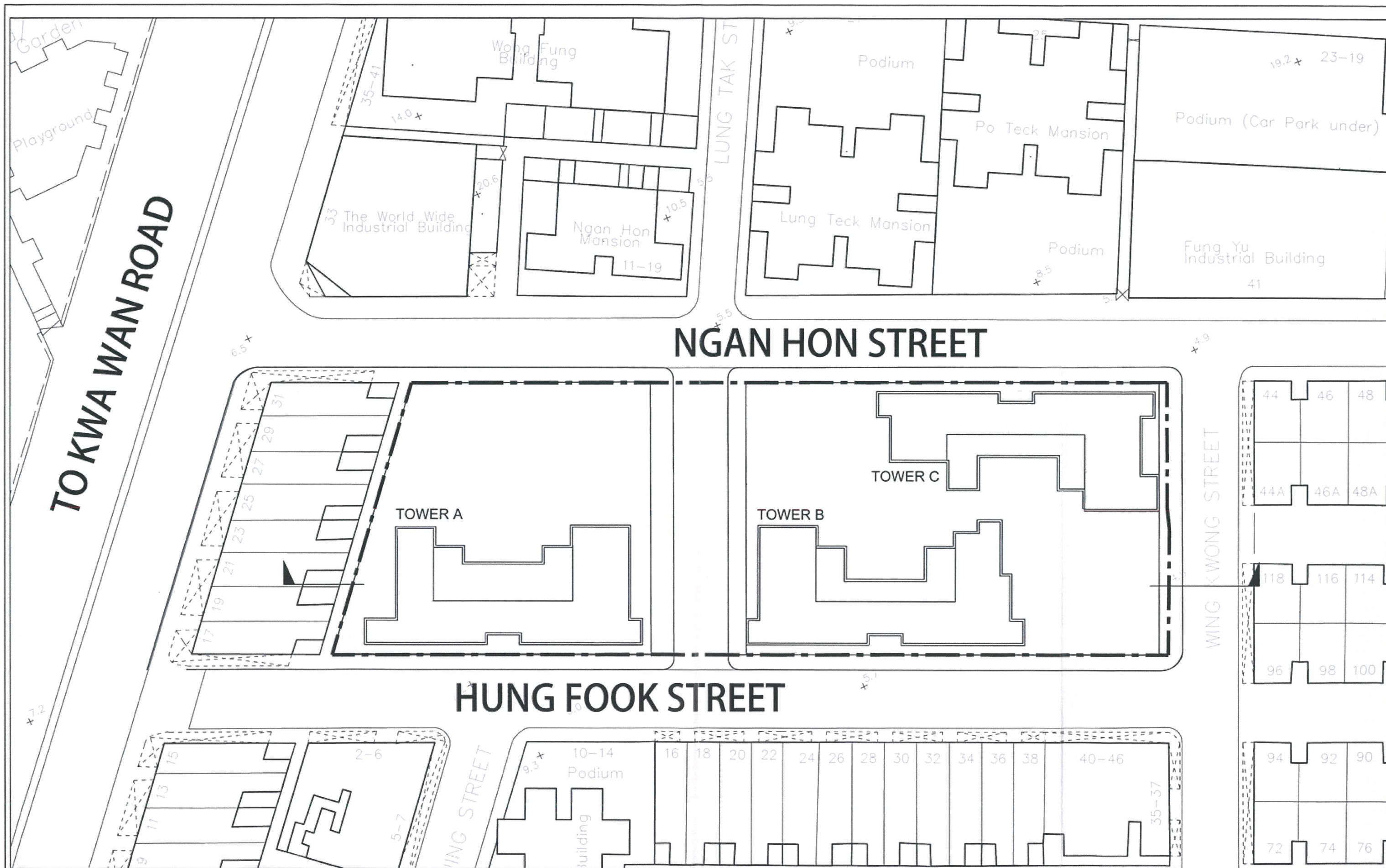
PROPOSED TRAFFIC NETWORK 擬議交通網絡
HUNG FOOK STREET / NGAN HON STREET DEVELOPMENT SCHEME 鴻福街/銀漢街發展計劃

PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 1

Preliminary Design



PART 3

**SUPPLEMENTARY
INFORMATION**

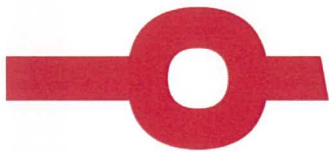
Appendix 2

Traffic Impact Assessment Report

02/06/2016

Reference number CHK50143110

COMPREHENSIVE AREA STUDY IN KOWLOON CITY
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF HUNG
FOOK STREET/NGAN HON STREET SITE WITHIN ACTION AREA 1



IDENTIFICATION TABLE

Client/Project owner	Urban Renewal Authority
Project	Comprehensive Area Study in Kowloon City
Type of document	Traffic impact Assessment for Redevelopment of Hung Fook Street/Ngan Hon Street Site within Action Area 1
Date	02/06/2016
File name	TIA for A+B Site 2Jun16(FINAL)
Reference number	CHK50143110

APPROVAL

Version	Name	Position	Date	Modifications
1	Author	Gary Ching	Associate	12/04/2016
	Checked by	Chapman Lam	Director	12/04/2016
	Approved by	Chapman Lam	Director	12/04/2016
2	Author	Gary Ching	Associate	11/05/2016
	Checked by	Chapman Lam	Director	11/05/2016
	Approved by	Chapman Lam	Director	11/05/2016
3	Author	Gary Ching	Associate	20/05/2016
	Checked by	Chapman Lam	Director	20/05/2016
	Approved by	Chapman Lam	Director	20/05/2016
4	Author	Gary Ching	Associate	25/05/2016
	Checked by	Chapman Lam	Director	25/05/2016
	Approved by	Chapman Lam	Director	25/05/2016
5	Author	Gary Ching	Associate	02/06/2016
	Checked by	Chapman Lam	Director	02/06/2016
	Approved by	Chapman Lam	Director	02/06/2016

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1. INTRODUCTION

- 1.1 Urban Renewal Authority (URA) has commissioned a Comprehensive Area Study in Kowloon City/To Kwa Wan district. Several “Action Areas” in Kowloon City/To Kwa Wan district will be studied under this project.
- 1.2 The study aims to adopt a comprehensive and holistic method to rejuvenate the district in an area-based manner and to review on restructuring and re-planning of the Action Areas; through master planning, to redesign more effective and environmentally-friendly local transport and road networks.
- 1.3 The area bounded by Ma Tau Wai Road, Bailey Street, Sung On Street and Ngan Hon Street is identified as Action Area 1 (AA1). This Traffic Impact Assessment (TIA) Report covers the traffic analysis for the redevelopment of Hung Fook Street/Ngan Hon Street Site (KC-010) (Subject Site) within AA1.
- 1.4 The main objectives of this Report are summarized as follow:
 - To present the proposed redevelopment plan of the Subject Site and the associated traffic scheme;
 - To review the existing traffic arrangement and traffic conditions in the vicinity of AA1;
 - To analyse the benefits of the proposed traffic scheme; and,
 - To assess the traffic impact of the proposed redevelopment plan.

2. PROPOSED MASTER PLAN

2.1 The Master Layout Plan (MLP) of AA1 are illustrated in **Figure 2.1** and the proposed redevelopment sites of the MLP are listed in **Table 2.1**.

Table 2.1 Development Sites in AA1

Site	Use	Remarks
KC-010 (Subject Site)	Residential/Commercial	Redevelopment initiated by URA
KC-011	Residential/Commercial	Redevelopment initiated by URA
KC-012	Residential/Commercial	Redevelopment initiated by URA
KC-009	Residential/Commercial	Redevelopment initiated by URA
DL-8 ⁽¹⁾	Residential/Commercial	URA Demand-Led Redevelopment Project
Other Development Site(s) ⁽²⁾	Residential/Commercial	Redevelopment initiated by URA

Note: (1) Project Committed in May 2014.
(2) Other Development Site(s) within AA1

2.2 The Subject Site is located at the northern part of AA1 bounded by Ngan Hon Street, Wing Kwong Street and Hung Fook Street. The development parameters of the proposed MLP of AA1 including the Subject Site are summarised in **Table 2.2**.

Table 2.2 Development Parameters of the Proposed MLP of AA1

Site	Site Area (m ²)	Domestic GFA (m ²)	Non-domestic GFA (m ²)	No. of Residential Units	Average Flat Size (m ² /unit)
KC-010 (Subject Site)	4,562	34,215	6,843	750	45.6
KC-011	2,440	18,300	3,660	400	45.8
KC-012	1,170	8,775	1,755	207	42.4
KC-009	7,403	55,522	11,105	1,152	48.2
DL-8 ⁽¹⁾	553	3,788	758	72	52.6
Other Development Site(s)	2,857	21,428	560	4,286	38.3

Note: (1) Project Committed in May 2014.

- 2.3 As shown in **Figure 2.1**, it is intended to extend Wan On Street to Ngan Hon Street upon the completion of the subject site and other proposed redevelopments along the extended road section (KC-009, KC011 and KC-012), which is tentatively to be year 2025.
- 2.4 As indicated in **Figure 2.1**, the access point for Subject Site is proposed to be provided at Hung Fook Street.
- 2.5 At-grade pedestrian crossings upto the standard will be provided, as appropriate, in the vicinity of the proposed redevelopment sites to enhance the pedestrian network in the Action Area. The proposed pedestrian crossings are illustrated in **Figure 2.2**.
- 2.6 Furthermore, a minimum of 2.5m pedestrian footpaths and corner sprays will be provided at the ground level of the redevelopment sites, as necessary and appropriate, to enhance the pedestrian walking environment and the visibility for vehicles.

Transport Provision for Subject Site

- 2.7 The transport provision requirements of the Subject Site are estimated with reference to the Hong Kong Planning Standards and Guidelines (HKPSG). The provision requirements are summarized in **Tables 2.3 – 2.5**.

Table 2.3 Car Parking Provision Requirements for Subject Site

Use		HKPSG Provision Rate				Parameters	Car Parking Spaces
Domestic	Private Housing	1 car space per 6-9 flats ⁽¹⁾	Adjustment Ratio			750 units	40 – 60
			R1 ⁽¹⁾	R2 ⁽²⁾	R3 ⁽³⁾		
			0.7	0.75	0.9		
	Visitor	1-5 visitor spaces per block with more than 75 units per block				3 blocks ⁽⁴⁾	3 – 15
Sub-total						43-75	
Non-Domestic		1 car space per 200-300m ² GFA				6,843 m ²	23 – 35
Total							66 – 110

- Note : (1) Assume unit size of all residential units are within 40m² -70m² GFA
 (2) Subject Site is within a 500m-radius of rail station, the 500m-radius coverage is illustrated in **Figure 2.3**.
 (3) Subject Site domestic plot ratio within 5-8
 (4) Assume residential units are distributed in 3 residential blocks

Table 2.4 Motorcycle Parking Provision Requirements for Subject Site

Use	HKPSG Provision Rate	Parameters	Motorcycle Parking Spaces
Domestic	1 space per 100-150 flats	750 units	5 – 8
Non-Domestic	5 to 10% of the total provision for private cars	23 – 35 nos.	2 – 4
Total			7 – 12

Table 2.5 Loading/Unloading Bay Provision Requirements for Subject Site

Use	HKPSG Provision Rate	Parameters	Loading/Unloading Bay
Domestic	Minimum of 1 bay for every 800 flats or part thereof, subject to a minimum of 1 bay for each housing block	3 blocks ⁽¹⁾	3 ⁽²⁾
Non-Domestic	1 bay for every 800 to 1,200m ² GFA or part thereof.	6,843 m ²	6 – 9 ⁽²⁾
Total			9 – 12 ⁽²⁾

Note : (1) Assume residential units are distributed in 3 residential blocks
 (2) 65% to be LGV and 35% to be HGV based on HKPSG

- 2.8 It is recommended in the DURF Report and related working documents to provide a “Communal Car Park” in one of the new development sites within the Wider Area to accommodate the necessary parking requirements generated by the new and existing developments within the few adjacent street blocks in the Wider Area, rather than to separately provide them inside individual developments. This can retain street fronts by reducing carpark entrances to individual developments.
- 2.9 To be specific, a Communal Car Park will serve the area bounded by To Kwa Wan Road, Ngan Hon Street, Sung On Street and Wing Kwong Street (Wider Area) where the boundary is same as **Plan 1** for the **Explanatory Statement** in Part 2 of the Planning Report.
- 2.10 Considering the size of development sites within AA1, it is anticipated that some of the sites are unlikely to provide any transport facilities within the site, or the sites undergo rehab and revitalize only and therefore no transport facilities can be provided. Therefore, it is proposed to provide a “Communal Car Park” in the Subject Site to accommodate the necessary parking requirements generated by the new and existing developments within the few adjacent street

blocks in the Wider Area, rather than to separately provide them inside individual developments.

2.11 This can reduce the illegal parking along the local roads and its disruption to the through traffic movements and pedestrians in the Wider Area. Moreover, this will cut down the number of car park openings on the streets to ensure continuous street fronts, and lessen the conflicts between footpath and vehicular run-in/outs.

2.12 The Subject Site is selected for the proposed Communal Car Park with the following considerations:

- The site area of the Subject Site is relatively large and a more efficient car park layout can be achieved;
- Most of the development sites within Wider Area are located within close proximity from the Subject Site;
- The ingress and egress routings of the Subject Site from both north and south directions are convenient, with the proposed traffic scheme.

2.13 The transport provision requirements of the Subject Site are summarized in Table 2.6.

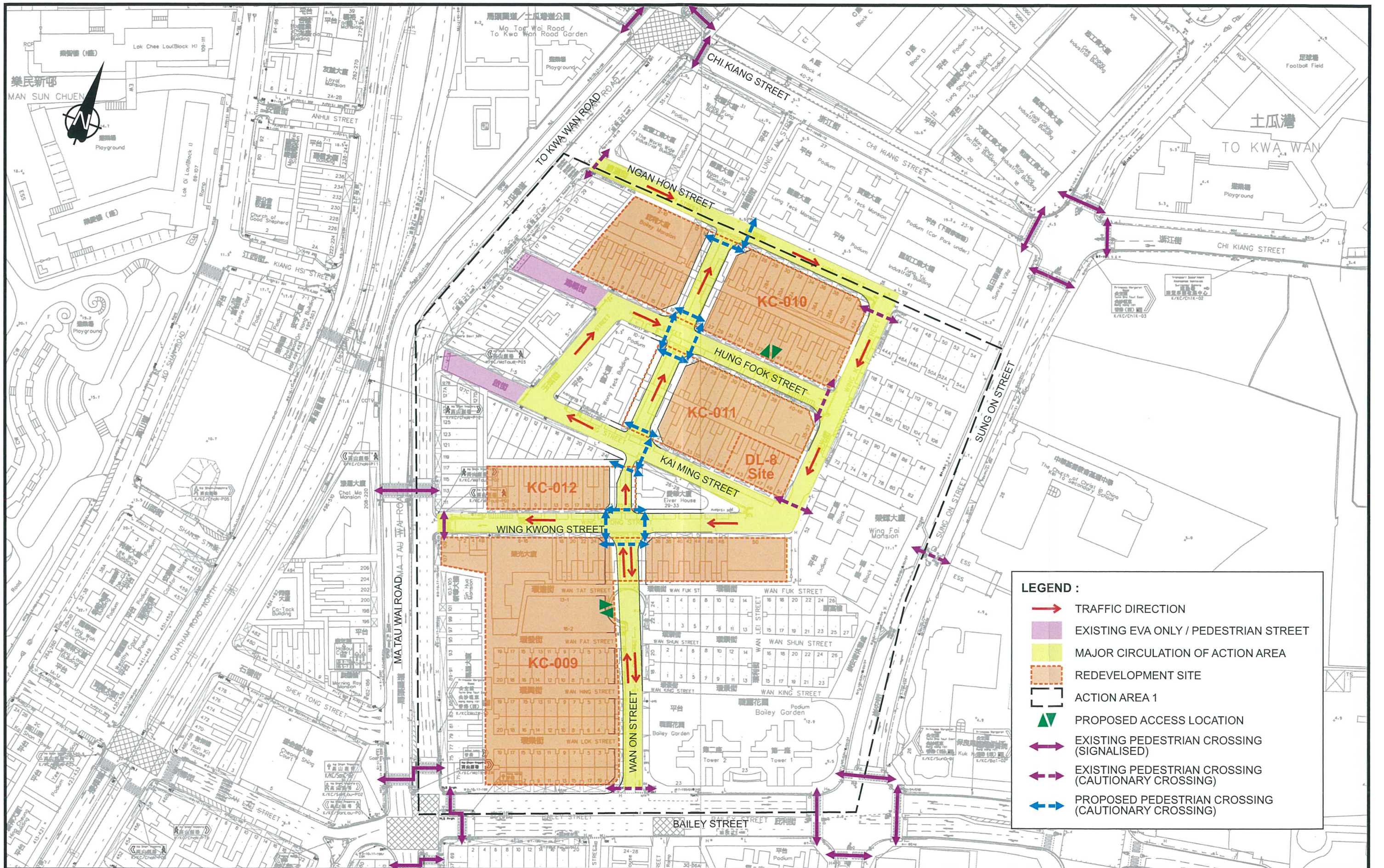
Table 2.6 Transport Provision Requirements

	Car Parking Spaces	Motorcycle Parking Spaces	Loading/Unloading Bays
Provision Requirement for Subject Site as per HKPSG	66 - 110	7 - 12	9 – 12 ⁽¹⁾
Provision Requirement for URA projects in the neighbourhood ⁽³⁾	98-164	14-21	16 - 20 ⁽¹⁾
Total Provision	274 ⁽⁴⁾	33 ⁽⁴⁾	26 ^{(1) (2) (5)}

- Note :
- (1) 65% to be LGV and 35% to be HGV based on HKPSG
 - (2) 6 nos. L/UL provisions will be provided in such form as layby within the project sites and 20 nos. L/UL bays will be provided in the proposed Communal Carpark in Subject Site.
 - (3) Development parameters adopted in the calculation of Transport Facilities and detailed provision calculations are illustrated in **Annex A**.
 - (4) Car and motorcycle parking spaces are provided based on the upper end of provision requirements.
 - (5) Loading / unloading bays are provided slightly above the lower end of provision requirement.

2.14 In views of the demand on coach parking spaces in the vicinity, it is proposed to provide 4 nos. coach parking space in the proposed Communal Carpark.

- 2.15 Car and motorcycle parking spaces has adopted the upper end provision requirements. It is due to the following considerations:
- The assessed domestic private car parking situation in Kowloon City is identified to be Category A, which require highest rate of parking provision, based on Transport Planning and Design Manual (TPDM);
 - A very high usage rate for parking spaces in this area is observed; and,
 - Illegal parking in this area are commonly observed.
- 2.16 Loading / unloading bays are provided slightly above the lower end of provision requirements considering the provision can be share-used and therefore the peak demand can be flattened.



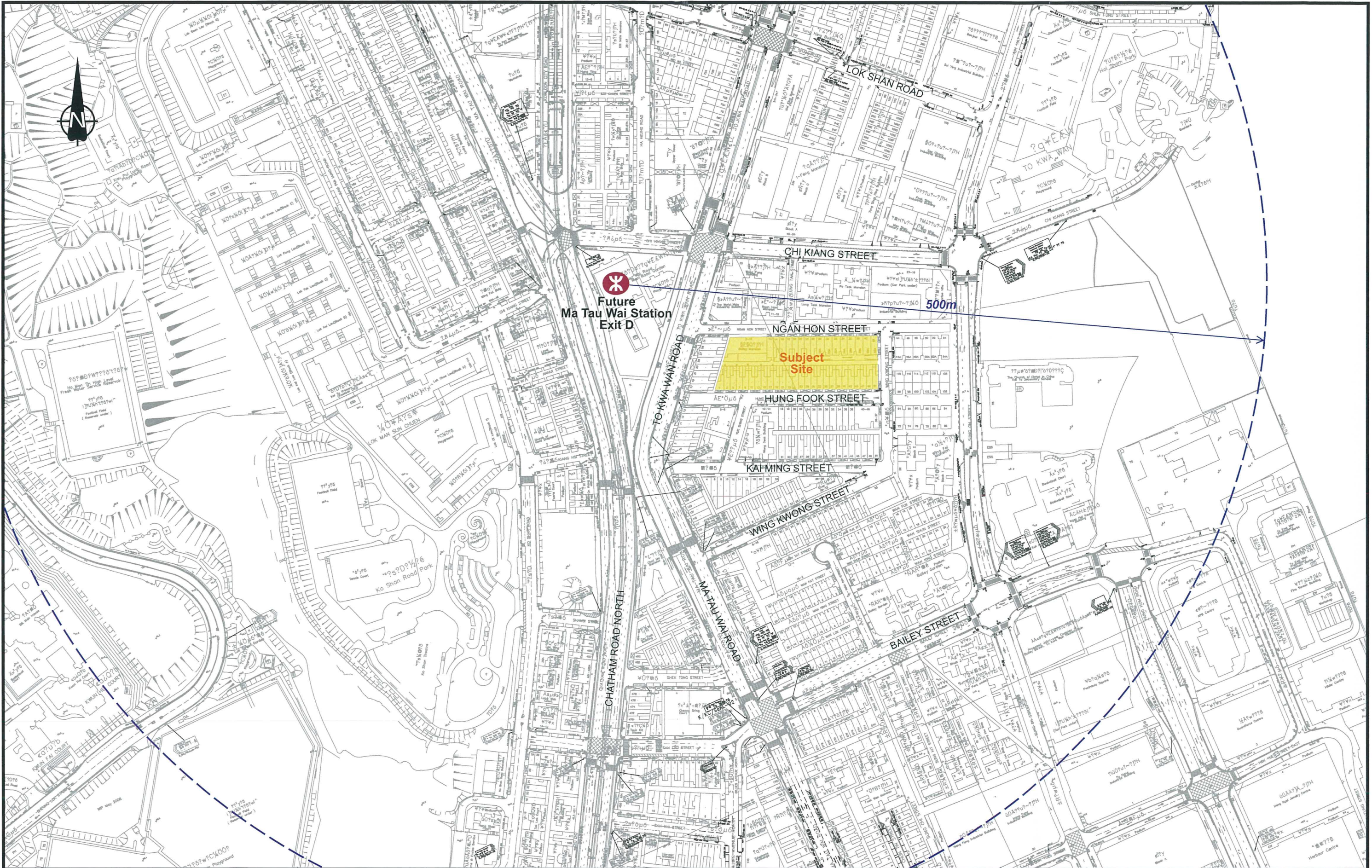
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Rev.	Description	Checked	Date

CHK50143110/AA1-A+B/F22-A.CDR/LLH/24MAY16

Project Title
TRAFFIC IMPACT ASSESSMENT FOR
REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title EXISTING AND PROPOSED PEDESTRIAN CROSSING FACILITIES			
Designed	WHY	Checked	KSC
Scale	NTS	Date	MAY 2016
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Rev.	Description	Checked	Date

Project Title
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title											
FUTURE MA TAU WAI MTR STATION											
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3. TRAFFIC ANALYSIS ON PROPOSED TRAFFIC SCHEMES

- 3.1 AA1 is bounded by Ma Tau Wai Road/To Kwa Wan Road, Bailey Street, Sung On Street and Ngan Hon Street, where Ma Tau Wai Road/To Kwa Wan Road are dual-carriageway district distributor running north-south direction in the western side of AA1; Bailey Street and is also district distributor running east-west direction; Ngan Hon Street is a 1-way local road running eastbound; and Sung On Street is a 2-way road which runs north-south direction at the east side of AA1.
- 3.2 The existing road network inside AA1 generally operates with a one-way system, except Wan On Street, which is a cul-de-sac road which connects Bailey Street. General pickup/drop-off layby or on-street metered parking spaces are provided on the kerbside at most road sections in AA1. Nevertheless, illegal parking is often observed within AA1.
- 3.3 The existing accessibility of the development sites within the AA1 are relatively poor due to limited number of ingress/egress points to the area. The development sites at Wan On Street can be accessed via Bailey Street/Wan On Street junction only. Similarly, the development sites at Wing Kwong Street/Kai Ming Street/Hung Fook Street/Ngan Hon Street can be accessed via To Kwa Wan Road/Ngan Hon Street junction from the north and exit via Ma Tau Wai Road/Wing Kwong Street junction to the south only. Detouring are required for the access traffic from the south and egress traffic to the north based on the existing road network. Such detouring not only increase the travelling time of access traffic, but also increase traffic loading at the adjacent road links and junctions.
- 3.4 Although there is no public transportation which passes through the inner street of the AA1, different public transport modes could also be found at the close vicinity of the AA1. A number of buses and minibuses going to different parts of Hong Kong, Kowloon and New Territories made their journey through and stop at Ma Tau Wai Road and To Kwa Wan Road. The proposed Ma Tau Wai Station of the Future Shatin-to-Central Line will be located at the immediate northwest of the Action Area. The AA1 will be well-connected and easily accessed by people from different parts of the territory after completion of Shatin-to-Central Line.
- 3.5 Currently, there is a taxi stand located at the eastbound carriageway of Bailey Street at the fringe of AA1
- 3.6 The existing traffic arrangement is illustrated in **Figure 3.1**. Analysis on the accessibility of the area is illustrated in **Figure 3.2**.
- 3.7 It is intended to extend Wan On Street upto Ngan Hon Street upon the completion of the MLP, to improve the accessibility of AA1. The proposed traffic scheme and the benefits of the scheme are illustrated in **Figure 3.3** and summarized as follow:
 - The egress traffic from the development sites at Wan On Street can exit via Wing Kwong Street and Ma Tau Wai Road southbound carriageway. This routing can replace the right turn movements from Wan On Street to Bailey Street, where egress traffic heading to Ma Tau Wai Road and the west direction has to cross 3 traffic lanes. Provision of the alternative routing with the proposed scheme can improve the road safety.

- The egress traffic from Wing Kwong Street heading to Hung Hom Road and the east direction can exit directly via Wan On Street with the proposed traffic scheme. This can minimize the detouring of the traffic and Ma Tau Wai Road/Wing Kwong Street junction and Ma Tau Wai Road/Bailey Street junction.
 - The extension of Wan On Street upto Ngan Hon Street can enhance the ingress traffic routing from the South to Wing Kwong Street area via Wan On Street and its extension, instead of detouring via Bailey Street eastbound, Sung On Street northbound, Lok Shan Road westbound, and To Kwa Wan Road southbound.
 - In addition, the extension of Wan On Street can facilitate the egress traffic from AA1 to the North via Lung Tak Street to Chi Kiang Street, instead of detouring via Bailey Street eastbound and Sung On Street northbound.
- 3.8 The opening up of Wan On Street to Wing Kwong Street has been proposed during commencement of the URA Project KC-009 (Bailey Street/Wing Kwong Street Project) and the proposal was generally supported by TD.
- 3.9 Swept path analysis has been conducted for the proposed road network and the results are illustrated in **Figure 3.4**. Due to the narrow width at the Wan On Street Extension near its junction with Kai Ming Street, it is proposed to ban the left turning movement for long vehicles over 11m at the junction. It is considered that the banning of left turning movement for long vehicles over 11m does not have any adverse traffic impact due to the following considerations:
- There are no major developments and access points at Yuk Shing Street;
 - Vehicles over 11m can access via the junction of Wing Kwong Street / Kai Ming Street, if necessary.
- 3.10 The opening up of Wan On Street to Wing Kwong Street has been proposed during commencement of the URA Project KC-009 (Bailey Street/Wing Kwong Street Project) and the proposal was generally supported by TD.

Analysis on On-Street Parking Spaces with Proposed MLP

- 3.11 With the extension of Wan On Street upto Ngan Hon Street as proposed in the MLP, some existing on-street parking spaces will be deleted. In addition, it is proposed to provide layby to cater for the loading / unloading purpose for some of the redevelopment sites.
- 3.12 **Figure 3.5** illustrates the affected on-street parking spaces and **Table 3.1** summarised the on-street public parking spaces provision.

Table 3.1 On-Street Public Parking Spaces Provision

	Existing Provision	Provision with Proposed MLP	Percentage of Spaces Remaining
Car Parking Space	45 nos. ⁽¹⁾	29 nos. ⁽¹⁾	67%
Motorcycle Parking Space	20 nos.	20 nos.	100%

Note (1) : Car Parking Space included 1 parking space for persons with disabilities

- 3.13 Among the existing 45 nos. spaces, it is anticipated that approximately 64% of the on-street car parking spaces will be remained with the proposed MLP.
- 3.14 The percentage of remaining on-street car parking spaces is reviewed against the development sites within AA1 that is not going to be redeveloped by URA in this MLP. The analysis of the site area are illustrated in **Tables 3.2**.

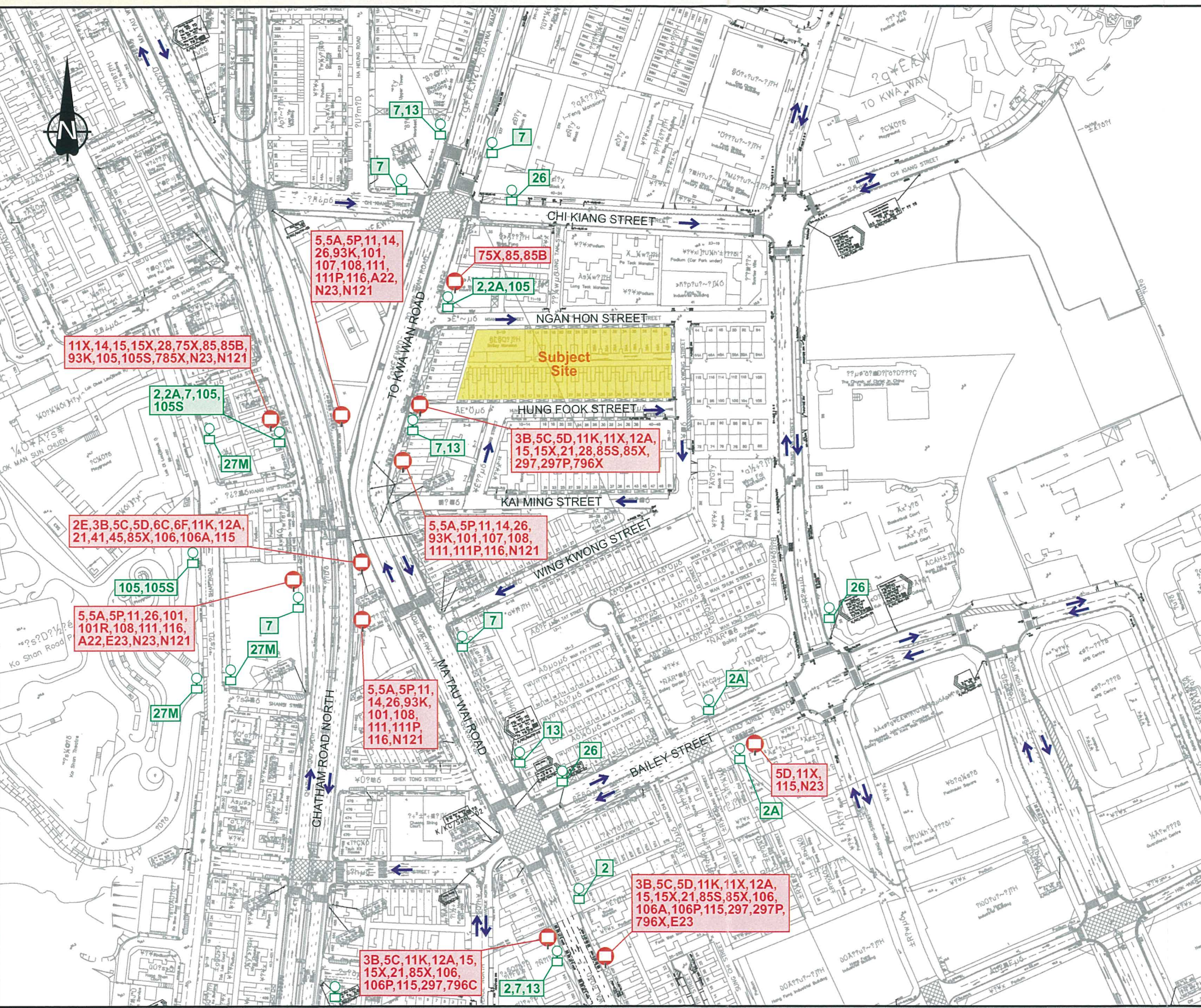
Table 3.2 Analysis on Site Area

	Redevelopment Sites in this MLP	Other Sites ⁽¹⁾	Total ⁽¹⁾
Site Area (m²)	18,985	6,575	25,560
Percentage	74%	26%	100%

Note:

(1) Existing developments / developments under construction which has its own car park are excluded.

- 3.15 Based on the site area of the study area, it is deemed that 26% will not be redeveloped.
- 3.16 For the URA re-development sites, the car parking provision will be provided in accordance with Hong Kong Planning Standards and Guidelines (HKPSG).
- 3.17 Therefore, the car parking provision within the Action Area will be increased. The demand on the on-street public car parking spaces will be reduced with the redevelopment of the sites.
- 3.18 The percentage of remaining on-street parking spaces is approximately 64% of the existing provision, while the percentage of site area of the remaining sites is approximately 26%.
- 3.19 Based on the analysis above, it is expected that the car parking provision shall meet the parking demand in the area.



LEGEND :

- FRANCHISED BUS STOP
- GREEN MINIBUS STOP
- TRAFFIC DIRECTION

Rev.	Description	Checked	Date
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A	REVISED BASED ON TD'S COMMENT	KSC	19MAY16

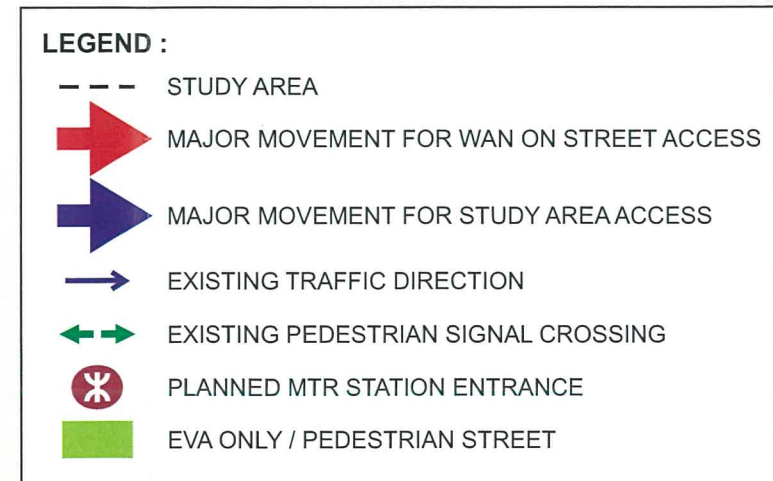
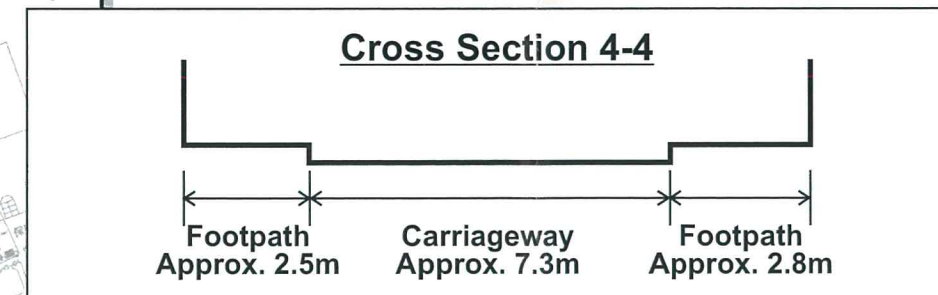
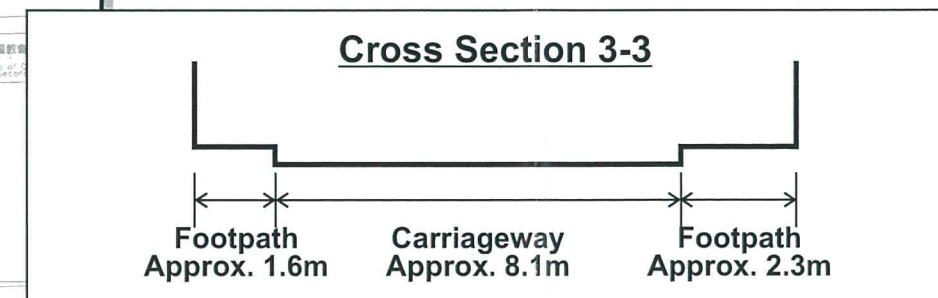
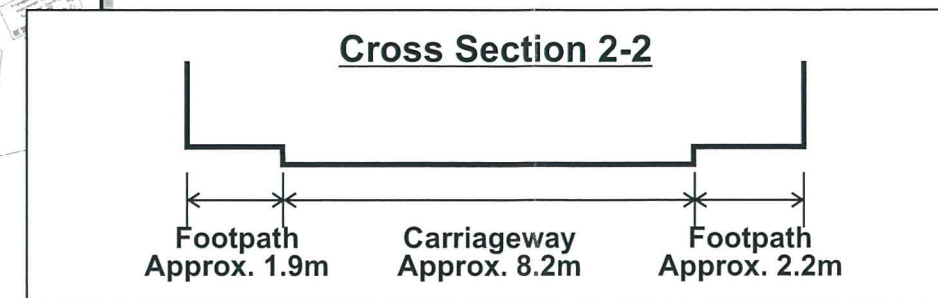
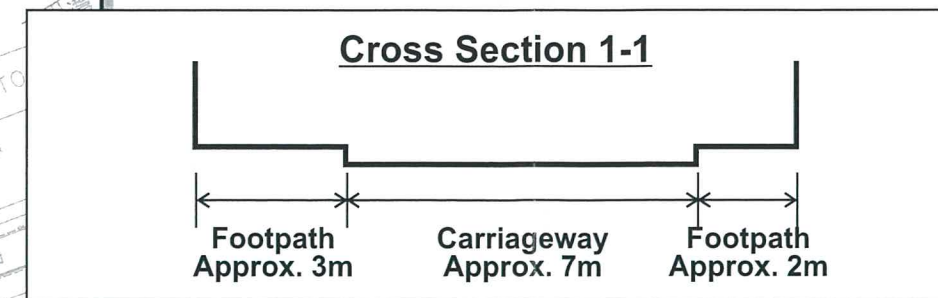
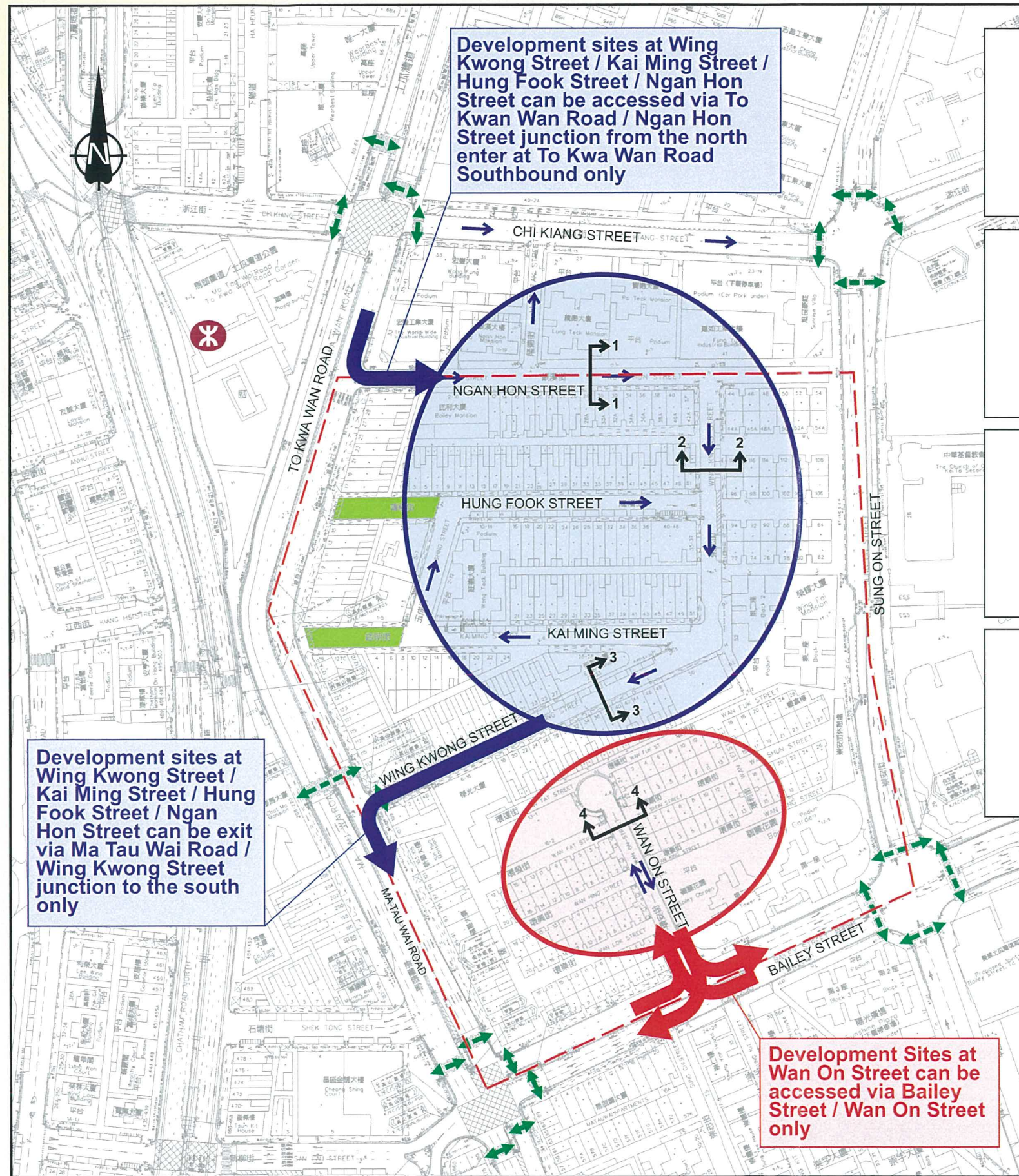
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TRAFFIC IMPACT ASSESSMENT FOR
REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title

EXISTING TRAFFIC ARRANGEMENT

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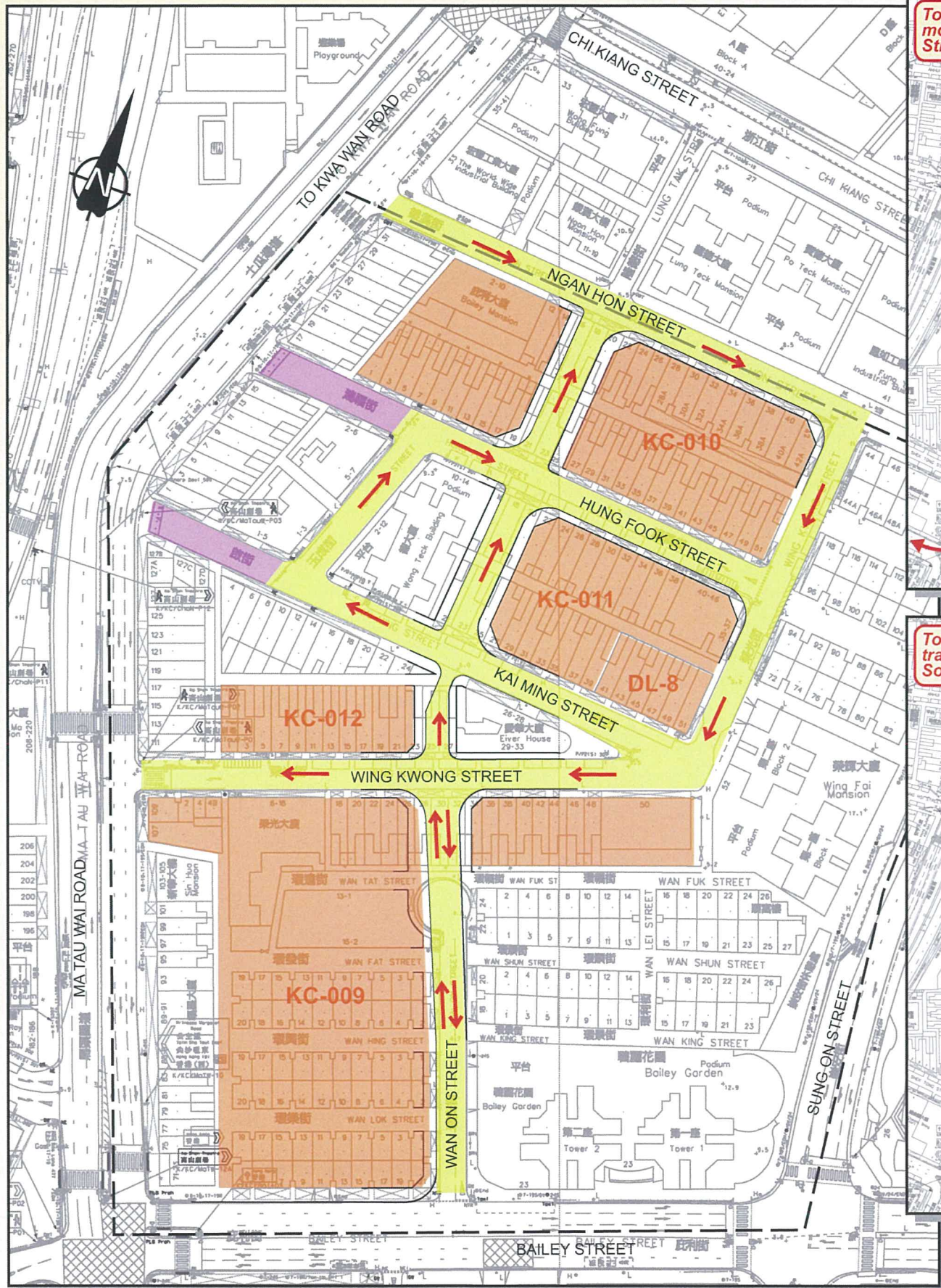
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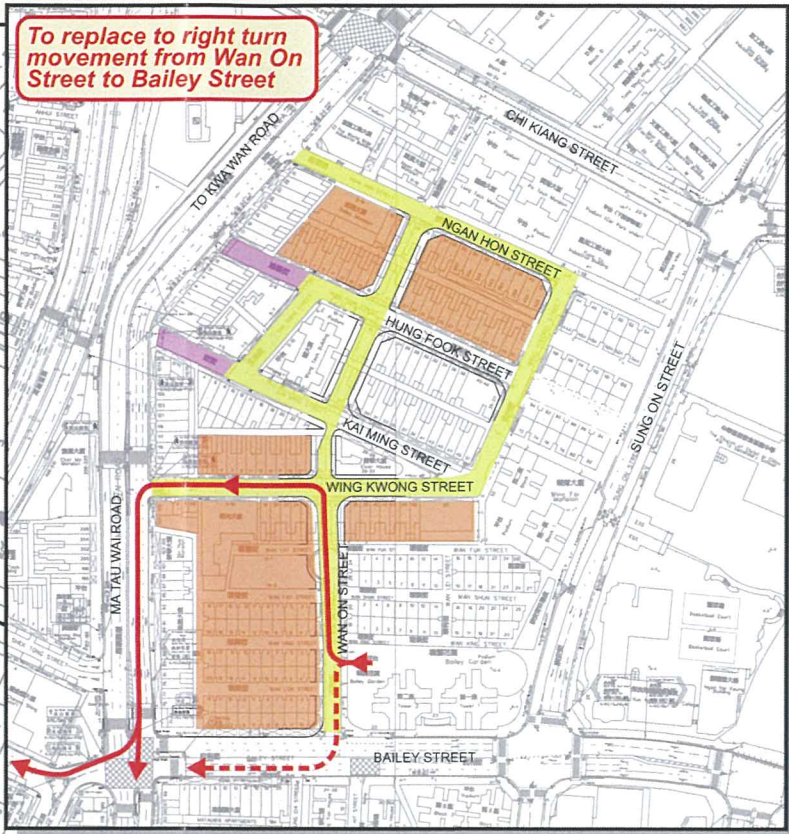
ANALYSIS OF EXISTING TRAFFIC ARRANGEMENT

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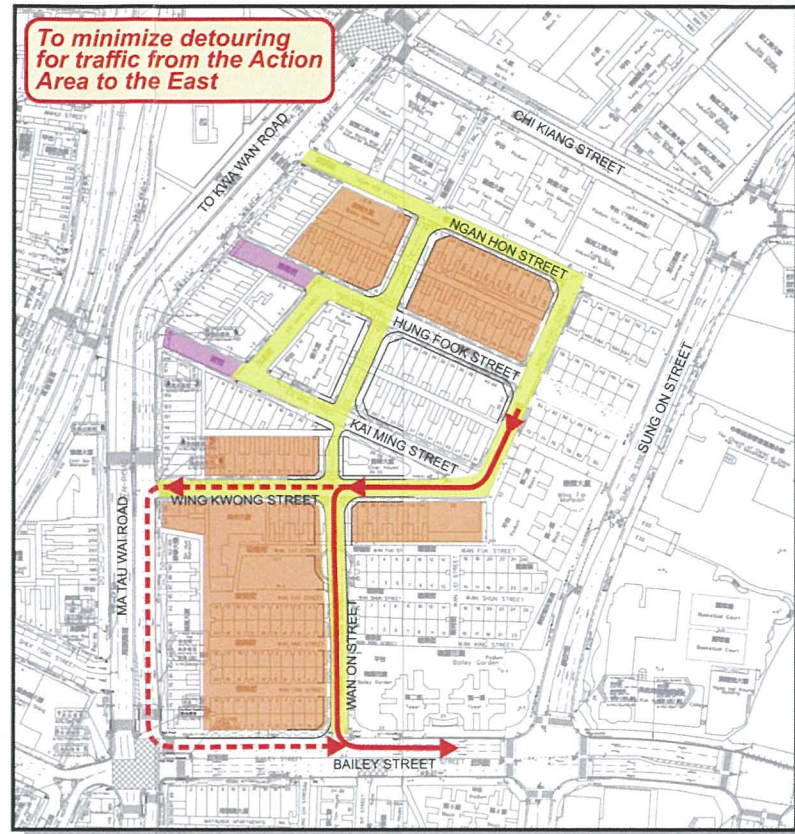




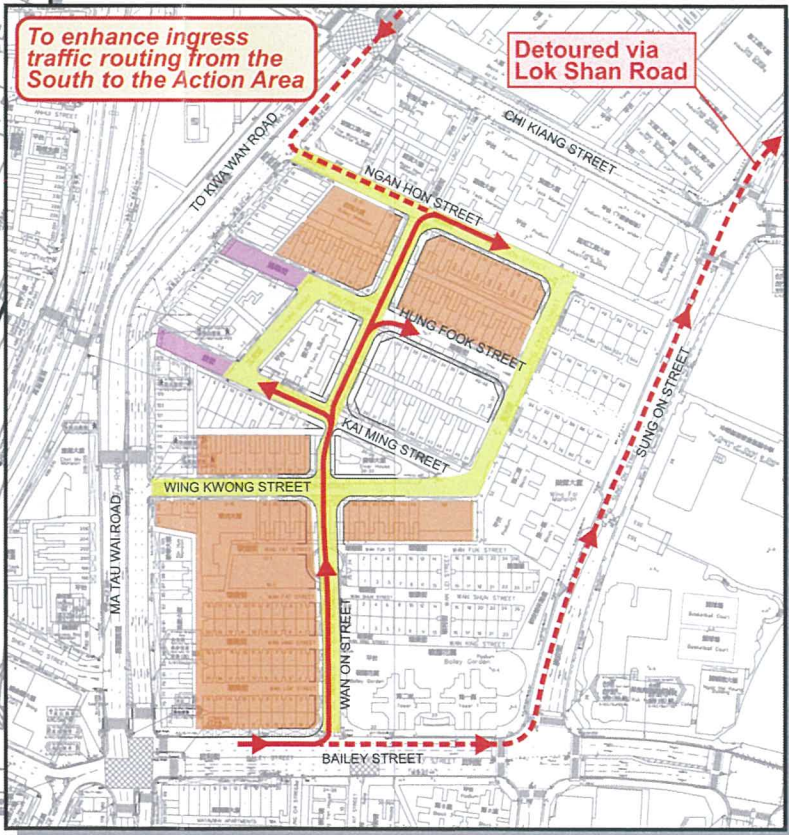
To replace to right turn movement from Wan On Street to Bailey Street



To minimize detouring for traffic from the Action Area to the East

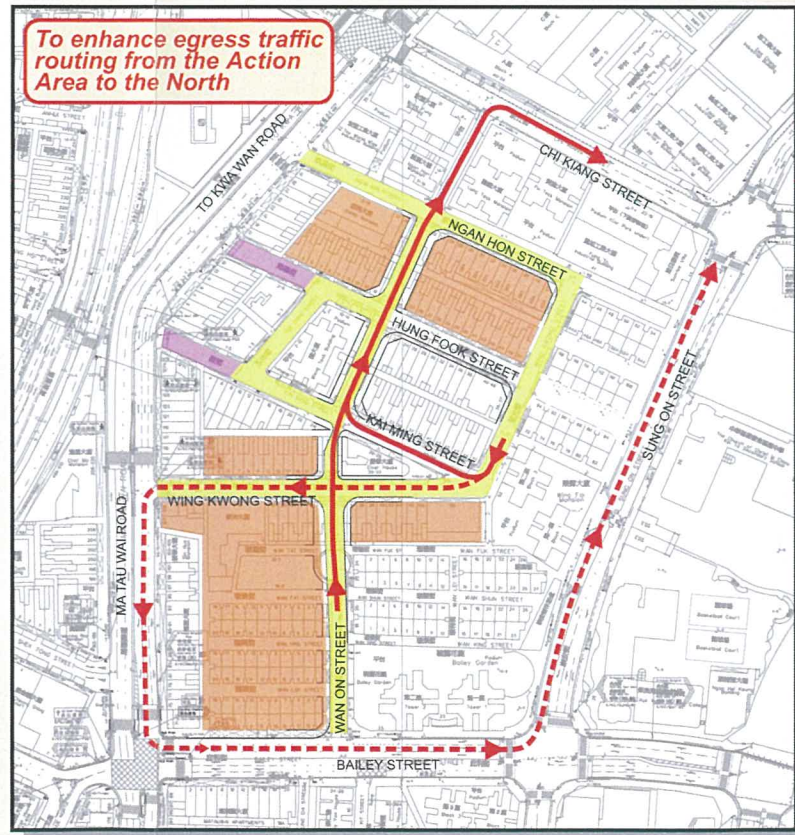


To enhance ingress traffic routing from the South to the Action Area



Detoured via Lok Shan Road

To enhance egress traffic routing from the Action Area to the North



LEGEND :

--- ORIGINAL ROUTING --- NEW ROUTING

-	-	-	-
-	-	-	-
-	-	-	-
A	REVISED BASED ON TD'S COMMENT	KSC	24MAY16
Rev.	Description	Checked	Date

CHK50143110/AA1-A+B/F33-A.CDR/LH/24MAY16

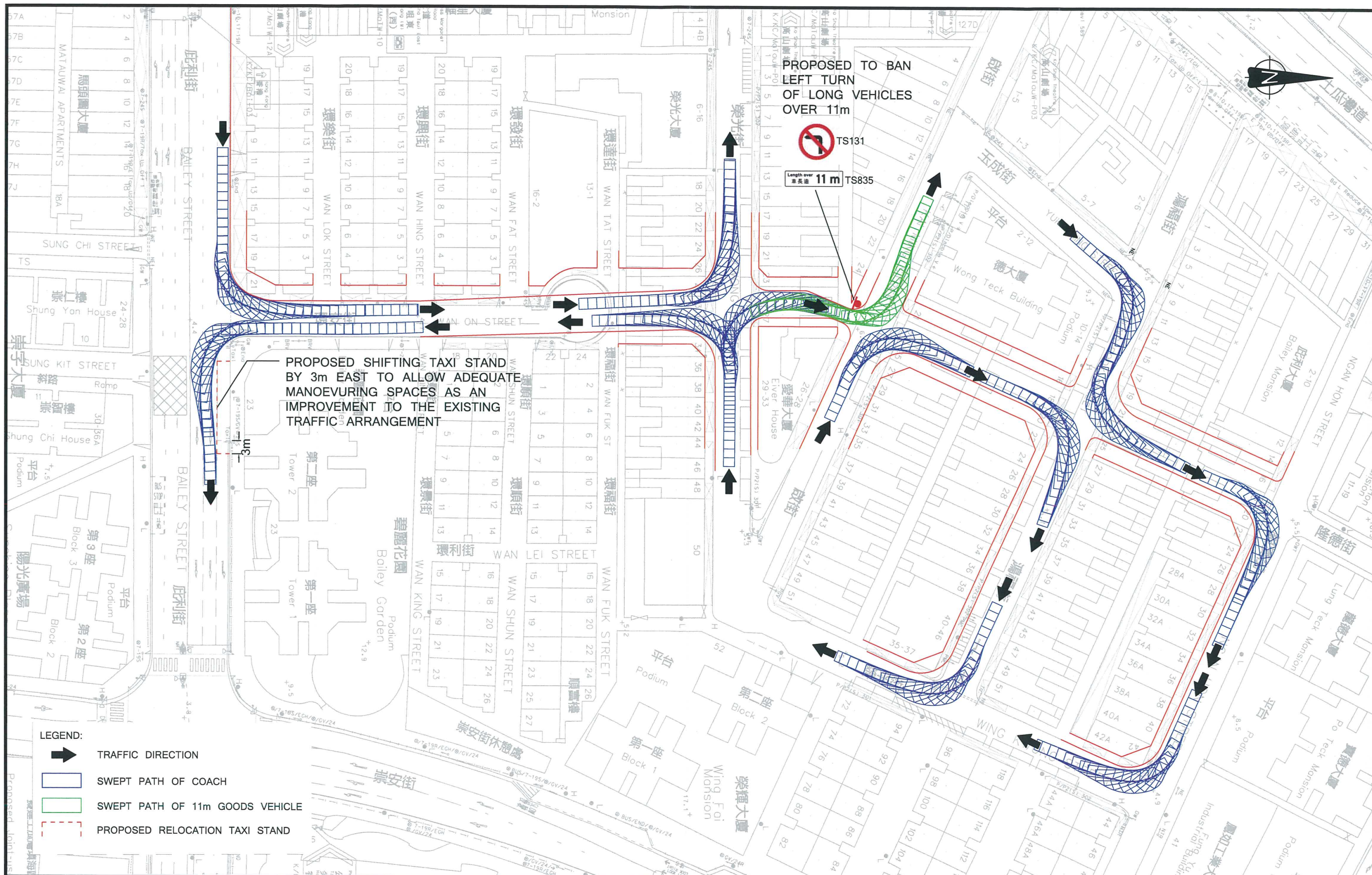
Project Title
TRAFFIC IMPACT ASSESSMENT FOR
REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title

ANALYSIS OF TRAFFIC SCHEME

Designed WHY Checked KSC Scale NTS Date APR 2016 Drawing No. 3.3 Rev. A





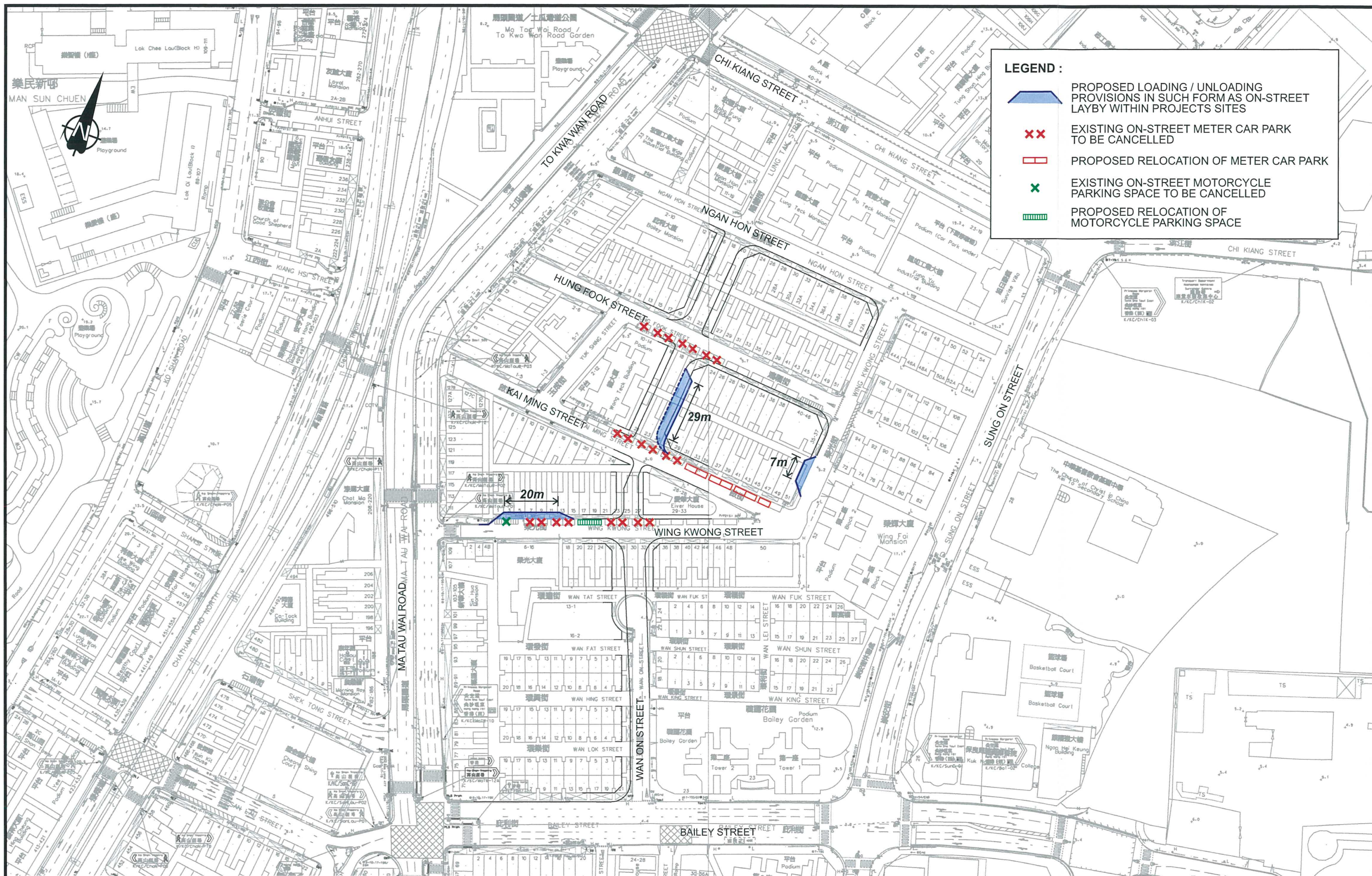
- LEGEND:
- TRAFFIC DIRECTION
 - SWEPT PATH OF COACH
 - SWEPT PATH OF 11m GOODS VEHICLE
 - PROPOSED RELOCATION TAXI STAND

A	REVISED BASED ON TD'S COMMENT	KSC	24MAY16		
Rev.	Description	Checked	Date		

Project Title
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title											
SWEPT PATH ANALYSIS											
Designed	HCY	Checked	KSC	Scale	1:800(A3)	Date	MAY 2016	Drawing No.	3-4	Rev.	A





Rev.	Description	Checked	Date
-	-	-	-
-	-	-	-
-	-	-	-
-	-	-	-

Project Title
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title											
PROPOSED ON STREET PARKING AND LOADING / UNLOADING LAY-BY											
Designed	WHY	Checked	KSC	Scale	NTS	Date	MAY 2016	Drawing No.	3.5	Rev.	



4. TRAFFIC FORECAST AND TRAFFIC IMPACT ASSESSMENT

- 4.1 Manual classified traffic count survey has been conducted in the vicinity of the Action Area to collect the observed traffic flow to form the basis of traffic forecast and traffic assessment. The traffic survey was conducted on morning and evening peak hours of a typical week day in September 2015. The observed traffic flows are illustrated in **Figure 4.1**.
- 4.2 The development is tentatively estimated to complete by year 2025 and the design year for the traffic impact assessment is set to be year 2028, which is 3 years after the completion.
- 4.3 To assess the potential traffic impact arising from the proposed MLP, the future traffic flows for the surrounding road network is developed. The 2028 traffic forecast is derived by using a growth factor method, whereby the observed 2015 traffic flows were factored to the design year 2028 using a growth factor.
- 4.4 The growth factor is derived by reviewing the historical traffic growth and also the latest planning data. The historical traffic flows extracted from the Annual Traffic Census (ATC) from year 2008 to 2014 are shown in **Table 4.1**, and the 2011-based TPEDM are shown in **Table 4.2**.

Table 4.1 Historical Annual Average Daily Traffic (AADT) Flows

Station No.	Road Name	From	To	AADT Flow (veh./day)							Annual Growth Rate (p.a.)
				2008	2009	2010	2011	2012	2013	2014	
3014	Ma Tau Wai Rd & To Kwa Wan Rd	San Lau St & Bailey St	Chi Kiang St	22,630	21,580	22,740	23,830	24,320	23,420	26,640	2.76%
3245	To Kwa Wan Rd	Kwei Chow St	Chi Kiang St	20,900	22,620	23,320	23,510	23,930	21,040	21,550	0.51%
3290	Hung Hom Rd	Man Yue St	Bailey St	12,340	12,000	12,380	12,470	12,700	13,280	13,880	1.98%
4040	Ma Tau Wai Rd	San Lau St & Bailey St	Fat Kwong St	18,120	17,790	18,340	18,480	15,160	16,740	16,950	-1.11%
4077	San Lau St	Ma Tau Wai Rd	Chatham Rd N	11,250	11,040	11,380	11,480	9,320	6,420	6,500	-8.74%
4212	Chatham Rd N & Ma Tau Wai Rd	San Lau St	Chi Kiang St	35,410	33,990	34,960	34,180	34,730	34,060	34,900	-0.24%
Total				120,650	119,020	123,120	123,950	120,160	114,960	120,420	-0.03%

Source: Annual Traffic Census

Table 4.2 Planning Data for Population and Employment

Zone 71 + 75 + 76	2011	2026	Average Annual Growth Rate (p.a.)
Population	106,950	105,050	-0.12%
Employment	54,750	58,900	0.49%

Source: Planning Data is based on "2011-Based TPEDM Scenario I"

- 4.5 The growth rate is -0.03% per annum based on the historical traffic trend on the road links near the Action Area, and according to planning data, the growth rates based on population and employment are -0.12% and +0.49% per annum respectively. To be more conservative in the traffic forecast, a growth factor of +1.0% per annum is adopted.
- 4.6 Furthermore, committed new developments in the vicinity of the Action Area are also taken into account in the traffic forecast. The development parameters of new developments adopted in the traffic forecast are listed in **Annex B**.
- 4.7 The development traffic is derived using the mean value traffic generation rates stipulated in the Transport Planning and Design Manual (TPDM). The traffic generation rates adopted and the development traffic are summarized in **Tables 4.3 and 4.4** respectively.

Table 4.3 Traffic Generation Rates

Development Density/OZP Zoning	Average Flat Size	Unit	AM		PM	
			Generation	Attraction	Generation	Attraction
Private Housing : High Density/R(A)	60	pcu/hr/unit	0.0718	0.0425	0.0286	0.037
Retail/Shopping Complex	-	pcu/hr/100sq.m. GFA	0.2296	0.2434	0.3100	0.3563

Table 4.4 Traffic Generation of Proposed Redevelopment Site

Site	Trip Ends (pcu/hr)			
	AM		PM	
	Generation	Attraction	Generation	Attraction
KC-010 (Subject Site)	70	49	44	53
KC-011	38	26	24	29
KC-012	27	18	15	20
KC-009	109	77	68	83
DL-8 ⁽¹⁾	8	6	6	6
Other Development Site(s)	51	35	31	37
Total	303	211	188	228

Note: (1) Project Committed in May 2014 and the development traffic also included in Reference Scenarios.

4.8 Traffic forecasts for year 2028 have been developed and the traffic impact assessment for the proposed MLP has been conducted. The traffic forecast are illustrated in **Figure 4.2 – 4.4**.

4.9 The results of traffic impact assessment of the Subject Site development are summarized in **Table 4.5** and detailed calculations are included in **Annex C**.

Table 4.5 Results of Junction Assessments

Junction	Reserve Capacity/Design Flow Ratio							
	Year 2015 Existing		Year 2028 Reference		Year 2028 with Development		Year 2028 Design Scenario (with Development & Traffic Scheme)	
	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak
Sung On Street/ Bailey Street	>100%	79%	>100%	78%	59%	52%	59%	52%
Ma Tau Wai Road/ Bailey Street	48%	39%	35%	34%	16%	15%	37%	32%
Bailey Street/ Wan On Street	0.10	0.09	0.32	0.44	0.67	0.44	0.59	0.37
Ma Tau Wai Road/ Wing Kwong Street	>100%	>100%	>100%	>100%	>100%	>100%	>100%	>100%
To Kwa Wan Road/ Chi Kiang Street	>100%	>100%	91%	>100%	75%	83%	83%	95%
Chi Kiang Street/ Lung Tak Street	0.02	0.04	0.03	0.06	0.03	0.06	0.16	0.15
Chi Kiang Street/ Yuk Yat Street/Sung On Street	>100%	>100%	86%	89%	42%	41%	56%	62%
Wing Kwong Street/ Hung Fook Street	0.06	0.12	0.08	0.13	0.15	0.18	0.08	0.13
Bailey Street/Hung Hom Road	>100%	>100%	89%	72%	67%	63%	67%	63%
Wing Kwong Street/ Wan On Street Extension	-	-	-	-	-	-	0.25	0.21
Kai Ming Street/ Wan On Street Extension	-	-	-	-	-	-	0.14	0.14
Hung Fook Street/ Wan On Street Extension	-	-	-	-	-	-	0.20	0.19
Ngan Hon Street/ Wan On Street Extension	-	-	-	-	-	-	0.16	0.14

Note: A single 2-lane road (L19) connecting Chi Kiang Street/ Yuk Yat Street/Sung On Street junction and Bailey Street/Hung Hom Road junction as planned are incorporated in the assessment. The road layout is shown in **Annex D**.

4.10 The junction assessment results indicate :

- All the junctions will be operating within capacity for both year 2028 with MLP development Scenario and Design Scenario (with MLP development & proposed traffic scheme);

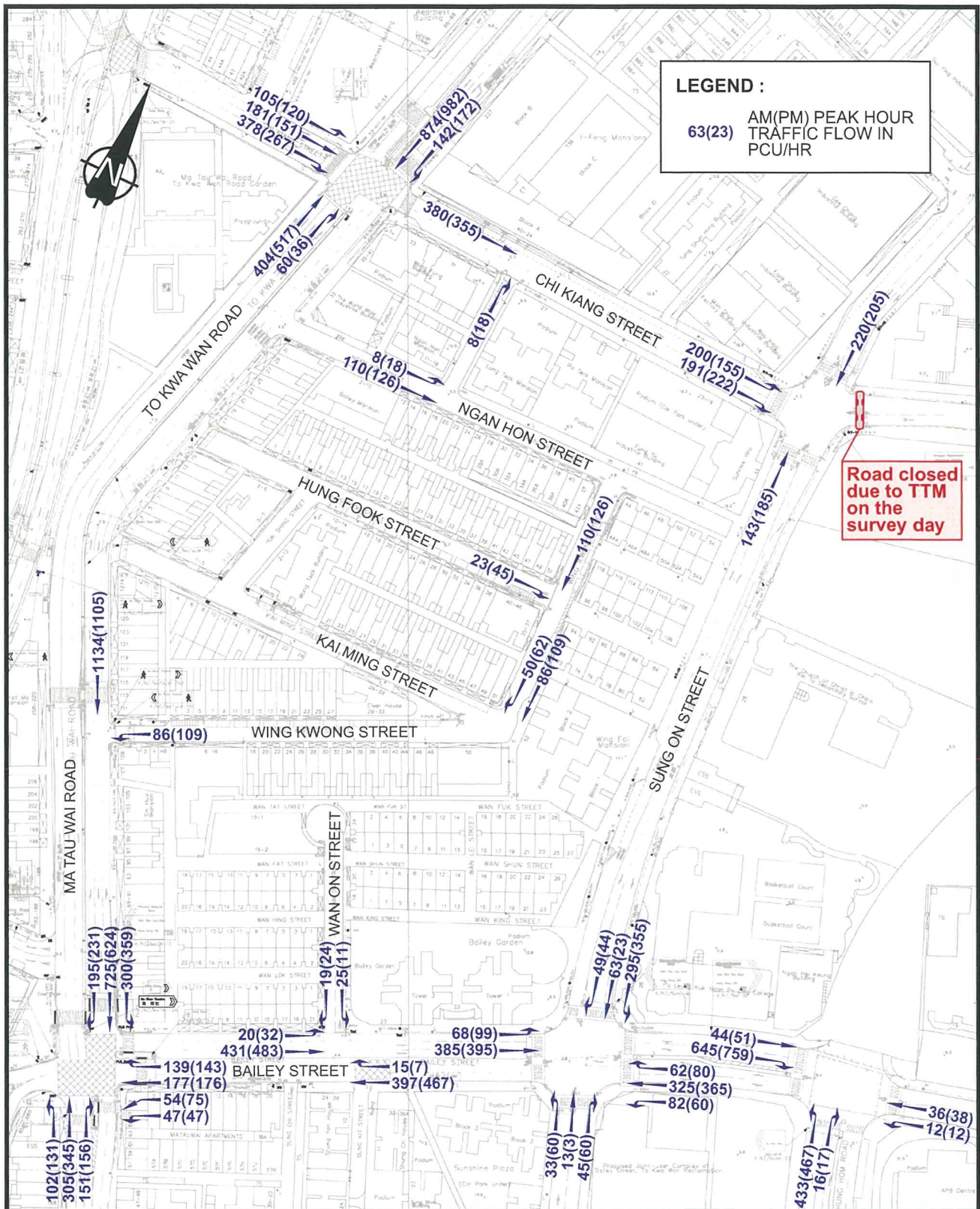
- Junction of Ma Tau Wai Road/Bailey Street operate with limited spare capacity in year 2028 with MLP development Scenario. With the proposed traffic scheme, some of the access traffic no longer require to detour via this junction. As the traffic volume reduced at this junction, the junction performance will be improved with the proposed traffic scheme.
- Junction of Bailey Street/Wing Kwong Street operated with limited space capacity in year 2028 with MLP development Scenario. The proposed traffic scheme can reduce the traffic from Wan On Street to turn right and cross 3 opposing eastbound lanes to Bailey Street to the west. The proposed traffic scheme improve the junction's performance as well as the safety.
- The new junctions under the proposed traffic schemes, including, junction of Wing Kwong Street/Wan On Street Extension and junction of Ngan Hon Street/Wan On Street Extension will be operating with ample capacity as the predicted traffic volume is relatively low.

4.11 Therefore, it is anticipated that there is no adverse traffic impact with the proposed MLP.

Benefits of the Proposed Scheme

4.12 The benefits of the proposed scheme are summarised as following:

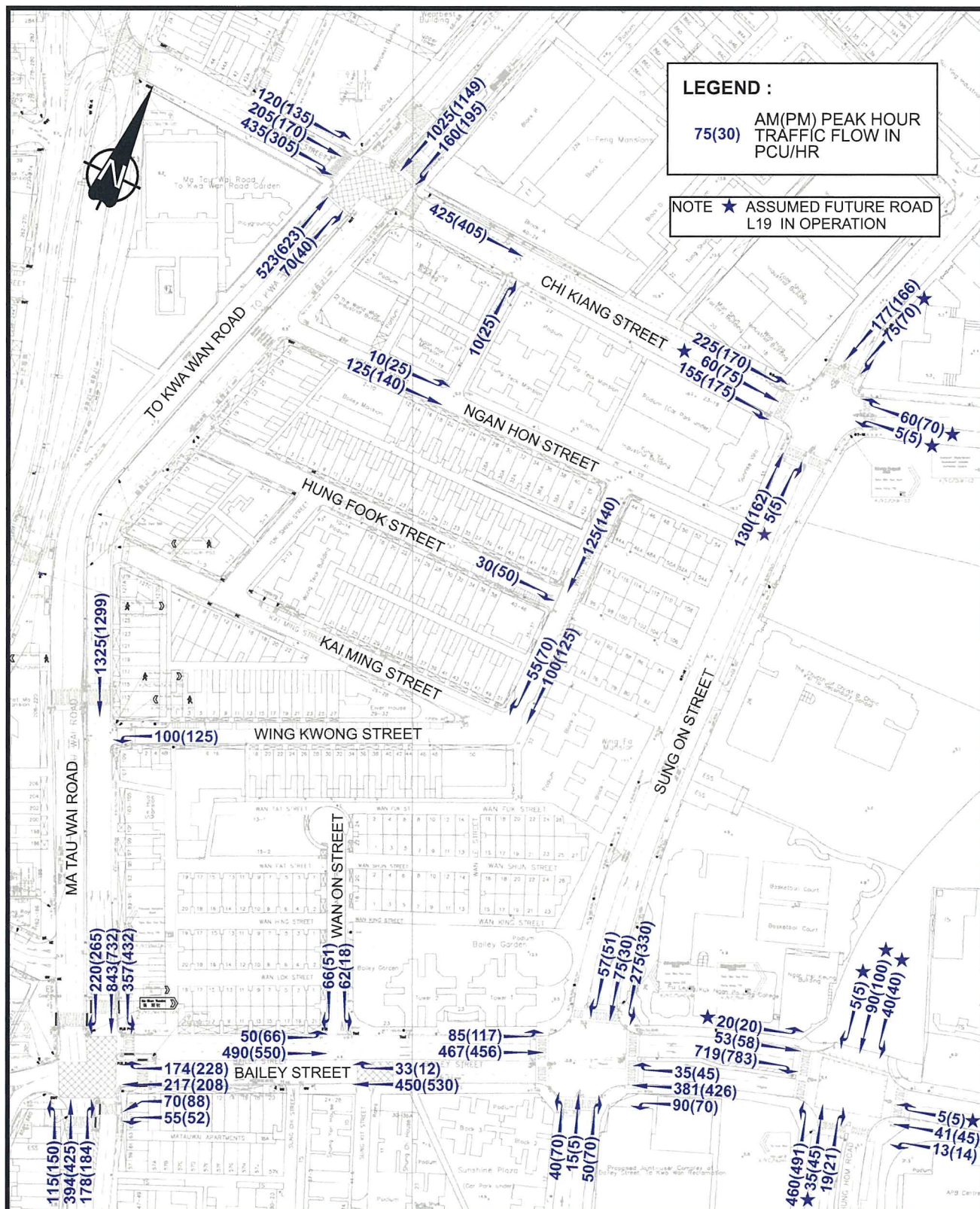
- Enhance the vehicular accessibility of the Action Area;
- Reduce the circulating traffic at the public road network around the Action Area;
- Improve road safety by providing an alternative routing to replace the existing right turning movements from Wan On Street to Bailey Street westbound carriageway;
- At-grade pedestrian crossings upto the standard will be provided, as appropriate, in the vicinity of the proposed redevelopment sites to enhance the pedestrian network;
- A minimum of 2.5m pedestrian footpaths and corner sprays will be provided at the ground level of the redevelopment sites, as necessary and appropriate, to enhance the pedestrian walking environment and the visibility for vehicles;
- Provide transport facilities upto the prevailing standards for the redevelopment sites within the development site boundary;
- Provide 4 nos. coach parking space in the proposed Communal Carpark.



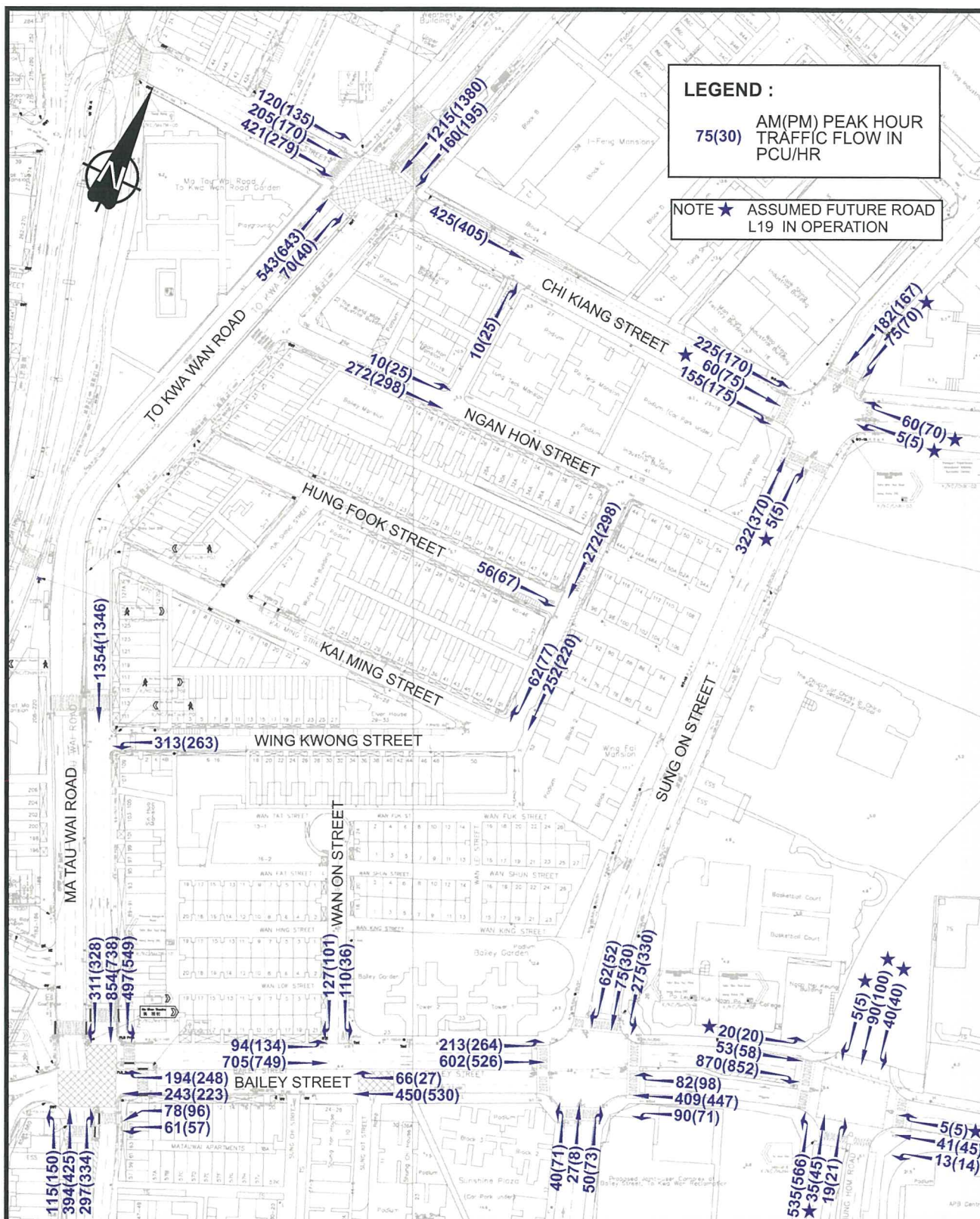
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Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Traffic Impact Assessment for Redevelopment of A+B Site at Section Area 1			
Drawing Title				YEAR 2015 OBSERVED TRAFFIC FLOW			
Designed	HCY	Checked	KSC	Scale	NTS	Date	MAY 2016
Drawing No.		4.1		Rev.		A	

CHK50143110/AA1-A+B/F41-A.CDR/LLH/24MAY16





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A	BASED ON TD'S COMMENT	KSC	24MAY16	-	-	-	-
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1			
Drawing Title				YEAR 2028 REFERENCE TRAFFIC FORECAST			
Designed	HCY	Checked	KSC	Scale	NTS	Date	MAY 2016
Drawing No.	4.2			Rev.	A		



-	-	-	-	-	-	-	-
A	BASED ON TD'S COMMENT	KSC	24MAY16	-	-	-	-
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Traffic Impact Assessment for Redevelopment of A+B Site at Section Area 1			
Drawing Title				YEAR 2028 REFERENCE TRAFFIC FORECAST WITH DEVELOPMENT			
Designed	HCY	Checked	KSC	Scale	NTS	Date	MAY 2016
Drawing No.	4.3	Rev.	A				

5. SUMMARY OF FINDINGS

- 5.1 Urban Renewal Authority (URA) has commissioned a Comprehensive Area Study in Kowloon City/To Kwa Wan district. Several “Action Areas” in Kowloon City/To Kwa Wan district will be studied under this project.
- 5.2 This Traffic Impact Assessment (TIA) Report covers the traffic analysis for the redevelopment of the Subject Site at AA1.
- 5.3 In addition to the transport provision demand of its own Site, it is proposed to provide a “Communal Car Park” in the Subject Site to accommodate the necessary parking requirements generated by the new and existing developments in the neighbourhood within the Wider Area.
- 5.4 It is intended to extend Wan On Street to Ngan Hon Street upon the completion of the proposed MLP to enhance the vehicular and pedestrian accessibility of the Action Area.
- 5.5 Traffic forecasts for year 2028 have been developed and the traffic impact assessment for the proposed MLP has been conducted. The junction assessment results indicate that all the affected junctions will be still operating within capacity in year 2028.
- 5.6 Based on the analysis above, it is concluded that the proposed traffic scheme can benefit the Action Area by improving the traffic and pedestrian accessibility. It is also anticipated that there will be no adverse traffic impact with the Subject Site and the MLP.

Annex A

Transport Provision Requirement Calculations

Table A1 Area Schedule

Site	Use	GFA	Flat Size	No. of Units	No. of Tower
Subject Site KC-010	Domestic	34,215	<=40		3
			40<FS<=70	750	
	Non-Domestic	6,843	-		
	Sub-Total	41,058	-	750	3
KC-011	Domestic	18,300	<=40	0	2
			40<FS<=70	400	
	Non-Domestic	3,660	-		
	Sub-Total	21,960	-	400	2
KC-012	Domestic	8,775	<=40		1
			40<FS<=70	207	
	Non-Domestic	1,755	-		
	Sub-Total	10,530	-	207	1
DL-8	Domestic	3,788	<=40	24	1
			40<FS<=70	48	
	Non-Domestic	758	-		
	Sub-Total	4,546	-	72	1
Other Development Site(s) ⁽¹⁾	Domestic	21,428	<=40	450	1
			40<FS<=70	110	
	Non-Domestic	4,286	-		
	Sub-Total	25,714	-	560	1
Total		103,808	-	1,989	8

Note: (1) Other Development Site(s) within the Wider Area.

Table A2 Car and Motorcycle Provision for Domestic Use

Site	Flat Size	Car Parking Spaces					Motorcycle Parking Spaces	
		Provision Rate				Car Parking Requirement	Provision Rate	Motorcycle Parking Requirement
		Global Parking Standard (GPS)	Demand Adjust-ment Ratio ⁽²⁾ (R1)	Accessibility Adjustment Ratio ⁽³⁾ (R2)	Development Intensity Adjustment Ratio ⁽⁴⁾ (R3)			
Subject Site KC-010	⁽²⁾ FS<=40	1 car space per 6-9 flats	0.4	0.75	0.9	0 - 0	1 motorcycle parking space per 100-150 flats	5 - 8
	⁽²⁾ 40<FS<=70		0.7			40 - 60		
	⁽⁵⁾ Visitor	1-5 visitor spaces per block				3 - 15		
Sub-Total						43 - 75		5 - 8
KC-011	⁽²⁾ FS<=40	1 car space per 6-9 flats	0.4	0.75	0.9	0 - 0		3 - 4
	⁽²⁾ 40<FS<=70		0.7			21 - 32		
	⁽⁵⁾ Visitor	1-5 visitor spaces per block				2 - 10		
Sub-Total						23 - 42		3 - 4
KC-012	⁽²⁾ FS<=40	1 car space per 6-9 flats	0.4	0.75	0.9	0 - 0		2 - 3
	⁽²⁾ 40<FS<=70		0.7			11 - 17		
	⁽⁵⁾ Visitor	1-5 visitor spaces per block				1 - 5		
Sub-Total						12 - 22	2 - 3	
DL-8	⁽²⁾ FS<=40	1 car space per 6-9 flats	0.4	0.75	0.9	1 - 2	1 - 1	
	⁽²⁾ 40<FS<=70		0.7			3 - 4		
	⁽⁵⁾ Visitor	1-5 visitor spaces per block				1 - 5		
Sub-Total						5 - 11	1 - 1	
Other Develop-ment Site(s)	⁽²⁾ FS<=40	1 car space per 6-9 flats	0.4	0.75	0.9	14 - 21	4 - 6	
	⁽²⁾ 40<FS<=70		0.7			6 - 9		
	⁽⁵⁾ Visitor	1-5 visitor spaces per block				1 - 5		
Sub-Total						21 - 35	4 - 6	
Total						104 - 185	15 - 22	

Note: (2) R1 = 0.4 for Flat size <=40 and 0.7 for 40<FS<=70

(3) R2 = 0.75 for site within a 500m-radius of rail station

(4) R3 = 0.9 for plot ratio 5<PR<=8

Table A3 Car and Motorcycle Provision for Non-Domestic Use

Site	Car Parking Spaces			Motorcycle Parking Spaces				
	Provision Rate	Provision Requirement		Provision Rate	Provision Requirement			
Subject Site KC-010	1 car space per 200-300m ² GFA	23	-	35	5 to 10% of the total provision for private cars	2	-	4
KC-011		13	-	19		1	-	2
KC-012		6	-	9		1	-	1
DL-8		3	-	4		1	-	1
Other Development Site(s)		15	-	22		1	-	3
Total	-	60	-	89	-	6	-	11

Table A4 Total Car and Motorcycle Provision

Site	Use	Car Parking Spaces ⁽⁶⁾			Motorcycle Parking Spaces		
Subject Site KC-010	Domestic	43	-	75	5	-	8
	Non-Domestic	23	-	35	2	-	4
	Sub-Total	66	-	110	7	-	12
KC-011	Domestic	23	-	42	3	-	4
	Non-Domestic	13	-	19	1	-	2
	Sub-Total	36	-	61	4	-	6
KC-012	Domestic	12	-	22	2	-	3
	Non-Domestic	6	-	9	1	-	1
	Sub-Total	18	-	31	3	-	4
DL-8	Domestic	5	-	11	1	-	1
	Non-Domestic	3	-	4	1	-	1
	Sub-Total	8	-	15	2	-	2
Other Development Site(s)	Domestic	21	-	35	4	-	6
	Non-Domestic	15	-	22	1	-	3
	Sub-Total	36	-	57	5	-	9
Total		164	-	274	21	-	33

Note: (6) Included parking spaces for persons with disabilities

Table A5 Loading / Unloading Provision

Site	Use	Loading / Unloading Bay		
		Provision Rate		Provision
		1 L/UL bay per block	1 L/UL bay per GFA(sqm)	
Subject Site KC-010	Domestic	1	-	3
	Non-Domestic	-	800 - 1200	6 - 9
	Sub-Total ⁽⁷⁾			9 - 12
KC-011	Domestic	1	-	2
	Non-Domestic	-	800 - 1200	4 - 5
	Sub-Total ⁽⁷⁾			6 - 7
KC-012	Domestic	1	-	1
	Non-Domestic	-	800 - 1200	2 - 3
	Sub-Total ⁽⁷⁾			3 - 4
DL-8	Domestic	1	-	1
	Non-Domestic	-	800 - 1200	1 - 1
	Sub-Total ⁽⁷⁾			2 - 2
Other Development Site(s)	Domestic	1	-	1
	Non-Domestic	-	800 - 1200	4 - 6
	Sub-Total ⁽⁷⁾			5 - 7
Total ⁽⁷⁾				25 - 32

Note: (7) Goods vehicle provision is divided into 65 % LGV and 35 % HGV.

Annex B

Development Parameters of New Developments Adopted in Traffic Forecast

Table B1 Development Parameters of New Development adopted in the Traffic Forecast

Site	Non-domestic GFA (m ²)	No. of Residential Units	Average Flat Size (m ² /unit)	Institution or Community use GFA (m ²)
Upper East	11,250	1,436	39	-
4-24 Kai Ming Street	1,155	100	58	-
URA Chun Tin Street/ Sung Chi Street Development	2,454	310	40	-
URA Ma Tau Wai Road Development	3,045	493	41	1,000
La Lumiere	-	216	40	-
Hong Kong Housing Society Senior Citizen Residences Scheme (Lee Kung Street)	-	300	32	-

Table B2 Traffic Generation Rates

Development Density/OZP Zoning	Average Flat Size	Unit	AM		PM	
			Generation	Attraction	Generation	Attraction
Private Housing : High Density/R(A)	60	pcu/hr/unit	0.0718	0.0425	0.0286	0.037
HOS/PSPS	50	pcu/hr/unit	0.0622	0.0426	0.0297	0.0401
Retail/Shopping Complex	-	pcu/hr/100sq.m . GFA	0.2296	0.2434	0.3100	0.3563
Community Hall / Social Welfare Facilities	-	pcu/hr/100sq.m . GFA	0.0300	0.0070	0.0410	0.0510

Table B3 Traffic Generation of New Development adopted in the Traffic Forecast

Site	Trip Ends (pcu/hr)			
	AM		PM	
	Generation	Attraction	Generation	Attraction
Upper East	130	90	77	95
4-24 Kai Ming Street	11	8	7	9
URA Chun Tin Street/ Sung Chi Street Development	31	21	19	23
URA Ma Tau Wai Road Development	44	30	26	31
La Lumiere	16	10	7	8
Hong Kong Housing Society Senior Citizen Residences Scheme (Lee Kung Street)	19	13	9	13
Total	251	172	145	179

Annex C

Detailed Junction Calculations

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: Bailey Street / Sung On Street

Design Year: 2015

Description: Observed

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Sung On Street NB	↔	A	4	4.500	30	25		36% / 49%	49% / 49%	1970	1960	91	0.046	0.046	123	0.063	0.063
Bailey Street EB	↘	B	1	3.100	10					1675	1675	68	0.041		99	0.059	
	↓	C	1	3.100						2065	2065	193	0.093	0.093	198	0.096	0.096
	↙	C	1	3.100						2065	2065	192	0.093		197	0.095	
Sung On Street SB	↘	D	3	3.300	22.5					1825	1825	171	0.094		182	0.100	
	↓	D	3	3.300	20			66%	88%	1985	1955	187	0.094	0.094	196	0.100	0.100
	↙	E	3	3.300		15				1895	1895	49	0.026		44	0.023	
Bailey Street WB	↘	F	1,2	3.000	30			42%	30%	1875	1885	194	0.103		203	0.108	
	↓	F	1,2	3.000						2055	2055	213	0.104		222	0.108	
	↙	G	2	3.000		15				1870	1870	62	0.033	0.033	80	0.043	0.043
Pedestrian Crossing		Ep	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Fp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Gp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Hp	5	MIN GREEN + FLASH =		11	+	11	=	22							

Notes: * Right Turning Pocket is provided.	Flow: (pcu/hr)			Group	F.D.A.Fp	C.G.D.A.Fp	Group	F.D.A.Fp	C.G.D.A.Fp
	y			y	0.244	0.267	y	0.271	0.302
	L (sec)	41		L (sec)	41	52	L (sec)	41	52
	C (sec)	130		C (sec)	130	130	C (sec)	130	130
	y pract.	0.616		y pract.	0.616	0.540	y pract.	0.616	0.540
	R.C. (%)	152%		R.C. (%)	152%	102%	R.C. (%)	127%	79%

Stage / Phase Diagrams				
1.	2.	3.	4.	5.
I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13
I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13

Date: MAY, 2016 Junction: Bailey Street / Sung On Street (J3)

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED











Junction: Bailey Street / Sung On Street

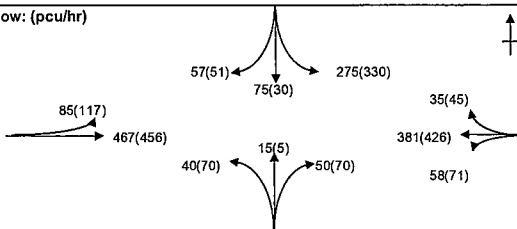
Design Year: 2028

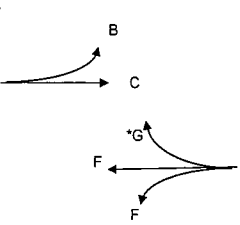
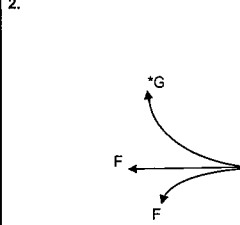
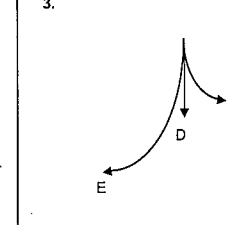
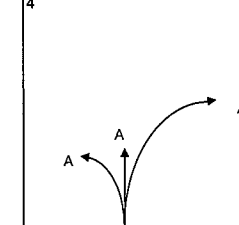
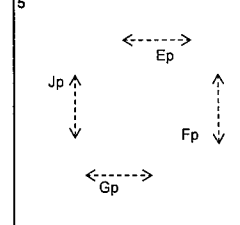
Description: Reference

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Sung On Street NB		A	4	4.500	30	25		38% / 48%	48% / 48%	1970	1960	105	0.053	0.053	145	0.074	0.074
Bailey Street EB		B	1	3.100	10					1675	1675	85	0.051		117	0.070	
		C	1	3.100						2065	2065	234	0.113	0.113	228	0.110	0.110
		C	1	3.100						2065	2065	233	0.113		228	0.110	
Sung On Street SB		D	3	3.300	22.5					1825	1825	167	0.092		174	0.095	0.095
		D	3	3.300	20			59%	84%	1995	1960	183	0.092	0.092	186	0.095	
		E	3	3.300		15				1895	1895	57	0.030		51	0.027	
Bailey Street WB		F	1,2	3.000	30			28%	30%	1890	1885	210	0.111		238	0.126	
		F	1,2	3.000						2055	2055	229	0.111		259	0.126	
		G	2	3.000		15				1870	1870	35	0.019	0.019	45	0.024	0.024
Pedestrian Crossing		Ep	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Fp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Gp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Hp	5	MIN GREEN + FLASH =		11	+	11	=	22							

Notes: * Right Turning Pocket is provided.	Flow: (pcu/hr) 	Group	F,D,A,Fp	C,G,D,A,Fp	Group	F,D,A,Fp	C,G,D,A,Fp
		y	0.256	0.258	y	0.296	0.304
		L (sec)	41	52	L (sec)	41	52
		C (sec)	130	130	C (sec)	130	130
		y pract.	0.616	0.540	y pract.	0.616	0.540
		R.C. (%)	140%	109%	R.C. (%)	108%	78%

Stage / Phase Diagrams				
1. 	2. 	3. 	4. 	5. 
I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13
I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13

Date: MAY, 2016 Junction: Bailey Street / Sung On Street (J3)

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: Bailey Street / Sung On Street

Design Year: 2028

Description: Reference (With Development)

Designed By: ALX

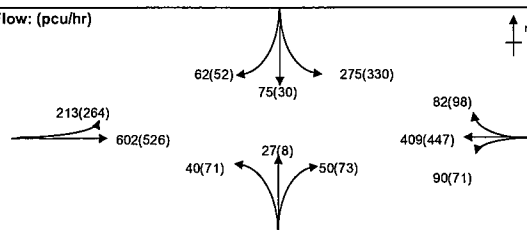
Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Sung On Street NB	↔	A	4	4.500	30	25		34% / 43%	47% / 48%	1980	1965	117	0.059	0.059	152	0.077	0.077
Bailey Street EB	↗	B	1	3.100	10					1675	1675	213	0.127		264	0.158	
	↘	C	1	3.100						2065	2065	301	0.146	0.146	263	0.127	0.127
	↔	C	1	3.100						2065	2065	301	0.146		263	0.127	
Sung On Street SB	↖	D	3	3.300	22.5					1825	1825	167	0.092		174	0.095	0.095
	↗	D	3	3.300	20			59%	84%	1995	1960	183	0.092	0.092	186	0.095	
	↘	E	3	3.300		15				1895	1895	62	0.033		52	0.027	
Bailey Street WB	↖	F	1,2	3.000	30			38%	29%	1880	1890	238	0.127		248	0.131	
	↗	F	1,2	3.000						2055	2055	261	0.127		270	0.131	
	↔	G	2	3.000		15				1870	1870	82	0.044	0.044	98	0.052	0.052
Pedestrian Crossing		Ep	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Fp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Gp	5	MIN GREEN + FLASH =		11	+	11	=	22							
		Hp	5	MIN GREEN + FLASH =		11	+	11	=	22							

Notes:

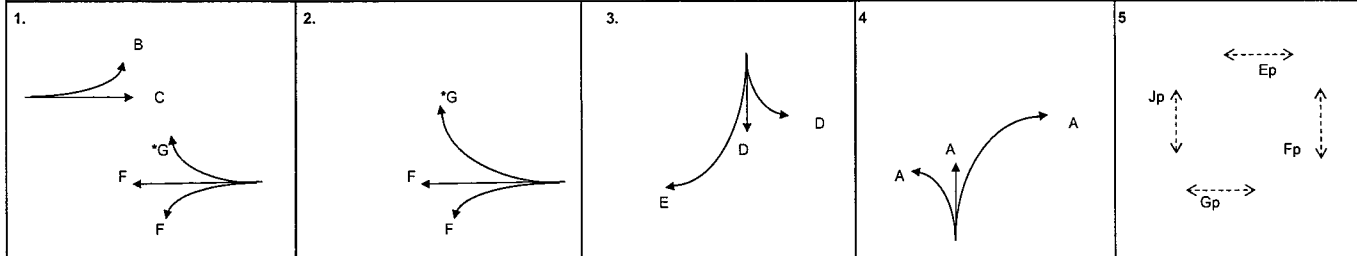
* Right Turning Pocket is provided.

Flow: (pcu/hr)



Group	F,D,A,Fp	C,G,D,A,Fp	Group	F,D,A,Fp	C,G,D,A,Fp
y	0.278	0.340	y	0.304	0.352
L (sec)	41	52	L (sec)	41	52
C (sec)	130	130	C (sec)	130	130
y pract.	0.616	0.540	y pract.	0.616	0.540
R.C. (%)	122%	59%	R.C. (%)	103%	53%

Stage / Phase Diagrams



I/G= 3		I/G= 5		I/G= 5		I/G= 8		I/G= 13	22
I/G= 3		I/G= 5		I/G= 5		I/G= 8		I/G= 13	22
Date: MAY, 2016								Junction: Bailey Street / Sung On Street	

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: Bailey Street / Sung On Street

Design Year: 2028

Description: Design

Designed By: ALX

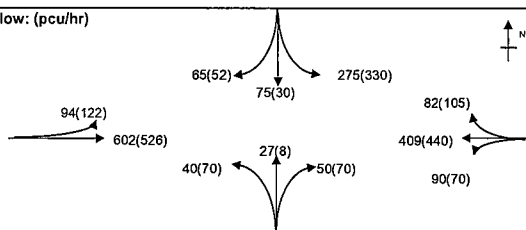
Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Sung On Street NB	↔	A	4	4.500	30	25		34% / 43%	47% / 47%	1980	1965	117	0.059	0.059	148	0.075	0.075
Bailey Street EB	↘	B	1	3.100	10					1675	1675	94	0.056		122	0.073	
	↔	C	1	3.100						2065	2065	301	0.146	0.146	263	0.127	0.127
	↗	C	1	3.100						2065	2065	301	0.146		263	0.127	
Sung On Street SB	↙	D	3	3.300	22.5					1825	1825	167	0.092		174	0.095	0.095
	↔	D	3	3.300	20			59%	84%	1995	1960	183	0.092	0.092	186	0.095	
	↘	E	3	3.300		15				1895	1895	65	0.034		52	0.027	
Bailey Street WB	↙	F	1,2	3.000	30			38%	29%	1880	1890	238	0.127		244	0.129	
	↔	F	1,2	3.000						2055	2055	261	0.127		266	0.129	
	↗	G	2	3.000		15				1870	1870	82	0.044	0.044	105	0.056	0.056
Pedestrian Crossing		Ep	5	MIN GREEN + FLASH =			11	+	11	=	22						
		Fp	5	MIN GREEN + FLASH =			11	+	11	=	22						
		Gp	5	MIN GREEN + FLASH =			11	+	11	=	22						
		Hp	5	MIN GREEN + FLASH =			11	+	11	=	22						

Notes:

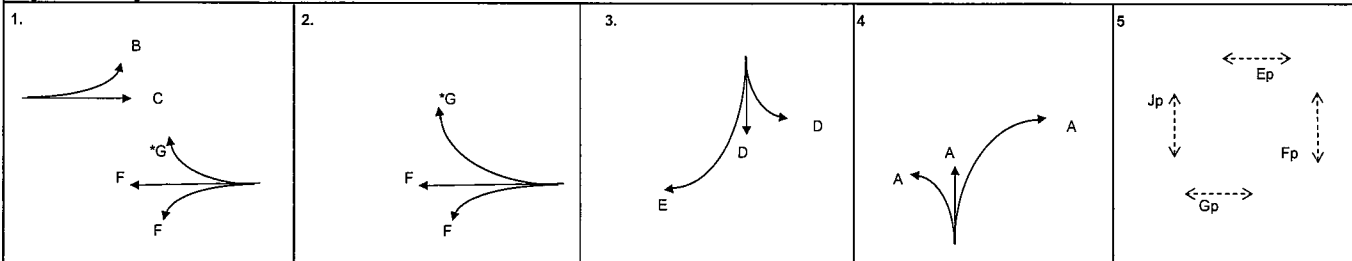
* Right Turning Pocket is provided.

Flow: (pcu/hr)



Group	F,D,A,Fp	C,G,D,A,Fp	Group	F,D,A,Fp	C,G,D,A,Fp
y	0.278	0.340	y	0.300	0.354
L (sec)	41	52	L (sec)	41	52
C (sec)	130	130	C (sec)	130	130
y pract.	0.616	0.540	y pract.	0.616	0.540
R.C. (%)	122%	59%	R.C. (%)	105%	52%

Stage / Phase Diagrams



I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13	22
I/G= 3	I/G= 5	I/G= 5	I/G= 8	I/G= 13	22
Date: MAY, 2016				Junction: Bailey Street / Sung On Street	

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: Ma Tau Wai Road / Sun Lau Street

Design Year: 2015

Description: Observed

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB		A	1	2.400	12			60%	69%	1725	1710	169	0.098		190	0.111	
		A	1	3.000						2055	2055	202	0.098		229	0.111	0.111
		A	1	3.000		15		81%	73%	1900	1915	187	0.098	0.098	213	0.111	
Ma Tau Wai Road SB		B	2	3.000	10			64%	80%	1750	1710	471	0.269		446	0.261	
		B	2	3.000						2055	2055	554	0.270	0.270	537	0.261	0.261
		D	2	3.000		15				1870	1870	195	0.104		231	0.124	
Sun Lau Street WB		C	3	3.000		15		44%	52%	1970	1955	316	0.160	0.160	369	0.189	0.189
		C	3	3.300						2085	2085	54	0.026		75	0.036	
		C	3	3.300	10					1690	1690	47	0.028		47	0.028	
Pedestrian Crossing		Ep	1,3	MIN GREEN + FLASH =		5	+	9	=	14							
		Fp	1,2	MIN GREEN + FLASH =		5	+	8	=	13							
		Gp	1	MIN GREEN + FLASH =		5	+	6	=	11							
		Hp	2,3	MIN GREEN + FLASH =		5	+	8	=	13							
		Ip	2	MIN GREEN + FLASH =		5	+	9	=	14							
		Jp	3	MIN GREEN + FLASH =		5	+	7	=	12							

Notes: * 0.6m is deducted from the carriageway width due to the bus stop at 35m before the stop line	Flow: (pcu/hr) <div>Diagram description: The diagram shows three approach flows. Top: Northbound Ma Tau Wai Road with left-turn flow 195(231), through flow 725(624), and right-turn flow 300(359). Bottom: Southbound Ma Tau Wai Road with left-turn flow 102(131), through flow 305(345), and right-turn flow 151(156). Right: Westbound Sun Lau Street with left-turn flow 54(75), through flow 139(193), and right-turn flow 177(176). A north arrow points to the right.</div>	Group	A,B,Jp	A,B,C	Group	A,D,C	A,B,C
		y	0.368	0.528	y	0.424	0.561
		L (sec)	26	17	L (sec)	12	17
		C (sec)	130	130	C (sec)	130	130
		y pract.	0.720	0.782	y pract.	0.817	0.782
		R.C. (%)	96%	48%	R.C. (%)	93%	39%

Stage / Phase Diagrams				
1.	2.	3.	4.	5.

I/G= 6		I/G= 8		I/G= 6		I/G=		I/G=	
I/G= 6		I/G= 8		I/G= 6		I/G=		I/G=	
Date: MAY, 2016					Junction: Ma Tau Wai Road / Sun Lau Street				

J1 (CTIA J14)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED




Junction: Ma Tau Wai Road / Sun Lau Street

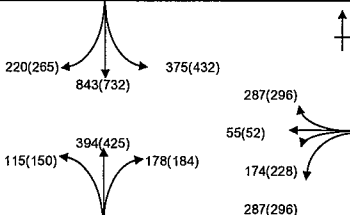
Design Year: 2028

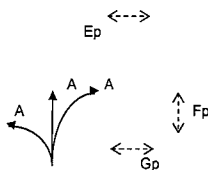
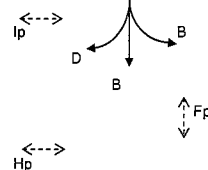
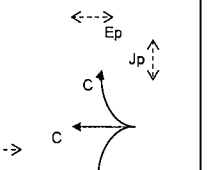
Description: Reference

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB		A	1	2.400	12			55%	66%	1735	1715	209	0.120		229	0.134	
		A	1	3.000						2055	2055	248	0.121		274	0.133	
		A	1	3.000		15		77%	72%	1905	1915	230	0.121	0.121	256	0.134	0.134
Ma Tau Wai Road SB		B	2	3.000	10			67%	82%	1740	1705	558	0.321		528	0.310	0.310
		B	2	3.000						2055	2055	660	0.321	0.321	636	0.309	
		D	2	3.000		15				1870	1870	220	0.118		265	0.142	
Sun Lau Street WB		C	3	3.300	10					1690	1690	55	0.033		52	0.031	
		C	3	3.300						2085	2085	287	0.138	0.138	296	0.142	0.142
		C	3	3.300		15				1895	1895	174	0.092		228	0.120	
Pedestrian Crossing		Ep	1,3	MIN GREEN + FLASH =		5	+	9	=	14							
		Fp	1,2	MIN GREEN + FLASH =		5	+	8	=	13							
		Gp	1	MIN GREEN + FLASH =		5	+	6	=	11							
		Hp	2,3	MIN GREEN + FLASH =		5	+	8	=	13							
		Ip	2	MIN GREEN + FLASH =		5	+	9	=	14							
		Jp	3	MIN GREEN + FLASH =		5	+	7	=	12							

Notes: * 0.6m is deducted from the carriageway width due to the bus stop at 35m before the stop line	Flow: (pcu/hr) 	Group	A,B,Jp	A,B,C	Group	A,B,Jp	A,B,C
		y	0.442	0.580	y	0.443	0.585
		L (sec)	26	17	L (sec)	26	17
		C (sec)	130	130	C (sec)	130	130
		y pract.	0.720	0.782	y pract.	0.720	0.782
		R.C. (%)	63%	35%	R.C. (%)	62%	34%

Stage / Phase Diagrams				
1. 	2. 	3. 	4.	5.

I/G= 6		I/G= 8		I/G= 6		I/G=		I/G=	
I/G= 6		I/G= 8		I/G= 6		I/G=		I/G=	

Date: MAY, 2016 Junction: Ma Tau Wai Road / Sun Lau Street J1 (CTIA J14)

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED










Junction: Ma Tau Wai Road / Sun Lau Street

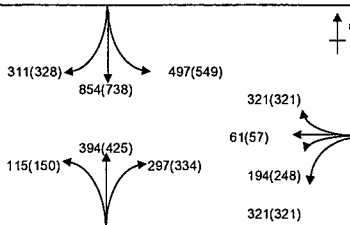
Design Year: 2028

Description: Reference (With Development)

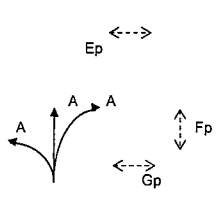
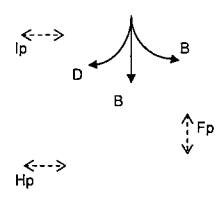
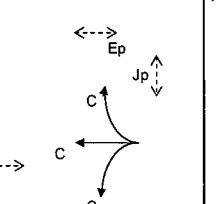
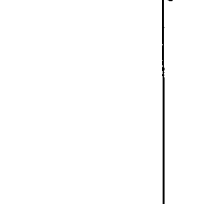

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB		A	1	2.400	12			49%	57%	1750	1730	234	0.134		263	0.152	
		A	1	3.000						2055	2055	275	0.134		312	0.152	
		A	1	3.000		15		100%	100%	1870	1870	297	0.159	0.159	334	0.179	0.179
Ma Tau Wai Road SB		B	2	3.000	10			81%	95%	1705	1675	613	0.360	0.360	578	0.345	0.345
		B	2	3.000						2055	2055	738	0.359		709	0.345	
		D	2	3.000		15				1870	1870	311	0.166		328	0.175	
Sun Lau Street WB		C	3	3.300	10					1690	1690	61	0.036		57	0.034	
		C	3	3.300						2085	2085	321	0.154	0.154	321	0.154	0.154
		C	3	3.300		15				1895	1895	194	0.102		248	0.131	
Pedestrian Crossing	Ep	1,3	MIN GREEN + FLASH =				5	+	9	=	14						
	Fp	1,2	MIN GREEN + FLASH =				5	+	8	=	13						
	Gp	1	MIN GREEN + FLASH =				5	+	6	=	11						
	Hp	2,3	MIN GREEN + FLASH =				5	+	8	=	13						
	Ip	2	MIN GREEN + FLASH =				5	+	9	=	14						
	Jp	3	MIN GREEN + FLASH =				5	+	7	=	12						

Notes:	Flow: (pcu/hr)		Group	A,B,Jp	A,B,C	Group	A,B,Jp	A,B,C
* 0.6m is deducted from the carriageway width due to the bus stop at 35m before the stop line			y	0.518	0.672	y	0.524	0.678
			L (sec)	26	17	L (sec)	26	17
			C (sec)	130	130	C (sec)	130	130
			y pract.	0.720	0.782	y pract.	0.720	0.782
			R.C. (%)	39%	16%	R.C. (%)	37%	15%

Stage / Phase Diagrams

1. 	2. 	3. 	4. 	5. 
--	--	---	--	--

I/G= 6	I/G= 8	I/G= 6	I/G=	I/G=
I/G= 6	I/G= 8	I/G= 6	I/G=	I/G=
Date: MAY, 2016			Junction: Ma Tau Wai Road / Sun Lau Street	

J1 (CTIA J14

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED




Junction: **Ma Tau Wai Road / Sun Lau Street**

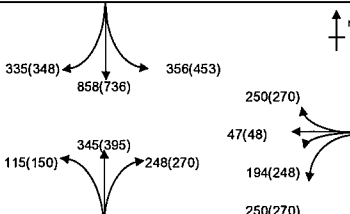
Design Year: **2028**

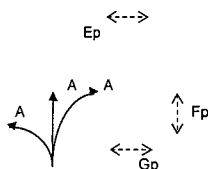
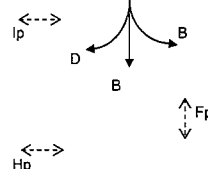
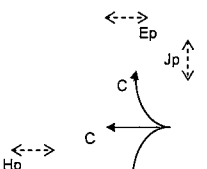
Description: **Design**

Designed By: **ALX**

Checked By: **KSC**

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB		A	1	2.400	12			55%	60%	1735	1725	211	0.122		249	0.144	
		A	1	3.000						2055	2055	249	0.121		296	0.144	
		A	1	3.000		15		100%	100%	1870	1870	248	0.133	0.133	270	0.144	0.144
Ma Tau Wai Road SB		B	2	3.000	10			64%	84%	1750	1700	558	0.319		538	0.316	
		B	2	3.000						2055	2055	656	0.319	0.319	651	0.317	0.317
		D	2	3.000		15				1870	1870	335	0.179		348	0.186	
Sun Lau Street WB		C	3	3.300	10					1690	1690	47	0.028		48	0.028	
		C	3	3.300						2085	2085	250	0.120	0.120	270	0.129	
		C	3	3.300		15				1895	1895	194	0.102		248	0.131	0.131
Pedestrian Crossing		Ep	1,3	MIN GREEN + FLASH =		5	+	9	=	14							
		Fp	1,2	MIN GREEN + FLASH =		5	+	8	=	13							
		Gp	1	MIN GREEN + FLASH =		5	+	6	=	11							
		Hp	2,3	MIN GREEN + FLASH =		5	+	8	=	13							
		Ip	2	MIN GREEN + FLASH =		5	+	9	=	14							
		Jp	3	MIN GREEN + FLASH =		5	+	7	=	12							

Notes: * 0.6m is deducted from the carriageway width due to the bus stop at 35m before the stop line	Flow: (pcu/hr) 	Group	A,B,Jp	A,B,C	Group	A,B,Jp	A,B,C
		y	0.452	0.572	y	0.461	0.592
		L (sec)	26	17	L (sec)	26	17
		C (sec)	130	130	C (sec)	130	130
		y pract.	0.720	0.782	y pract.	0.720	0.782
		R.C. (%)	59%	37%	R.C. (%)	56%	32%

Stage / Phase Diagrams							
1.	2.	3.	4.	5.			
							
I/G= 6	I/G= 8	I/G= 6	I/G=	I/G=			
I/G= 6	I/G= 8	I/G= 6	I/G=	I/G=			

Date: **MAY, 2016** Junction: **Ma Tau Wai Road / Sun Lau Street** J1 (CTIA J14)

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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Run with file:- "O:\sel\Updated\J2_obs_AM.vpi" at 15:14:11 on Tuesday, 24 May 2016

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RUN TITLE
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J2_obs_AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I (W	13.50 M.	I
I	CENTRAL RESERVE WIDTH	I (WCR	0.50 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I (WC-B)	3.00 M.	I
I	- VISIBILITY	I (VC-B)	250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I (VB-C)	5.0 M.	I
I	- VISIBILITY TO RIGHT	I (VB-A)	5.0 M.	I
I	- LANE 1 WIDTH	I (WB-C)	3.50 M.	I
I	- LANE 2 WIDTH	I (WB-A)	0.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	RATE OF FLOW (VEH/MIN) AT TOP OF PEAK
ARM A	15.00	45.00	5.64
ARM B	15.00	45.00	0.55
ARM C	15.00	45.00	7.73

I		I	TURNING PROPORTIONS						I			
I		I	TURNING COUNTS (VEH/HR)						I			
I		I	(PERCENTAGE OF H.V.S)						I			
I		I										I
I	TIME	I	FROM/TO	I	ARM A	I	ARM B	I	ARM C	I		
I	08.00 ~ 09.30	I		I		I		I		I		
I		I	ARM A	I	0.000	I	0.044	I	0.956	I		
I		I		I	0.0	I	20.0	I	431.0	I		
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I		
I		I		I		I		I		I		
I		I	ARM B	I	0.432	I	0.000	I	0.568	I		
I		I		I	19.0	I	0.0	I	25.0	I		
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I		
I		I		I		I		I		I		
I		I	ARM C	I	0.964	I	0.036	I	0.000	I		
I		I		I	397.0	I	15.0	I	0.0	I		
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I		
I		I		I		I		I		I		

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	0.55	8.53	0.064		0.0	0.1	1.0	
C-A	4.96							
C-B	0.19	11.87	0.016		0.0	0.0	0.2	
A-B	0.25							
A-C	5.39							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-AC	0.102	0.006	0.013	0.005		0.007		
C-B	0.119	0.007		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	0.66	8.29	0.079		0.1	0.1	1.3	
C-A	5.93							
C-B	0.22	11.65	0.019		0.0	0.0	0.3	
A-B	0.30							
A-C	6.43							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY		
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR) (M)	TO RIGHT (M)		
B-AC		0.098	0.007	0.013	0.005	0.007		
C-B		0.117	0.009		0.009			

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH./MIN/	(VEH./MIN/
I				(RFC)	(PDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

I	08.30-08.45							I
I	B-AC	0.80	7.95	0.101	0.1	0.1	1.6	I
I	C-A	7.26						I
I	C-B	0.27	11.34	0.024	0.0	0.0	0.4	I
I	A-B	0.37						I
I	A-C	7.88						I
I								I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:						I
I		MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY	I
I		CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT	I
I				(.1M)	(.1M)	(M)	(M)	I
I		B-AC	0.093	0.009	0.013	0.004	0.007	I
I		C-B	0.113	0.011		0.009		I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	08.45-09.00									I
I	B-AC	0.80	7.95	0.101		0.1	0.1	1.7		I
I	C-A	7.26								I
I	C-B	0.27	11.34	0.024		0.0	0.0	0.4		I
I	A-B	0.37								I
I	A-C	7.88								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I		MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I		CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I				(.1M)	(.1M)	(M)	(M)			I
I		B-AC	0.093	0.009	0.013	0.004	0.007			I
I		C-B	0.113	0.011		0.009				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.00-09.15									I
I	B-AC	0.66	8.29	0.079		0.1	0.1	1.3		I
I	C-A	5.93								I
I	C-B	0.22	11.65	0.019		0.0	0.0	0.3		I
I	A-B	0.30								I
I	A-C	6.43								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I		MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I		CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I				(.1M)	(.1M)	(M)	(M)			I
I		B-AC	0.098	0.007	0.013	0.005	0.007			I
I		C-B	0.117	0.009		0.009				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.15-09.30									I
I	B-AC	0.55	8.53	0.064		0.1	0.1	1.1		I
I	C-A	4.96								I
I	C-B	0.19	11.87	0.016		0.0	0.0	0.2		I
I	A-B	0.25								I
I	A-C	5.39								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I		MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I		CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I				(.1M)	(.1M)	(M)	(M)			I
I		B-AC	0.102	0.006	0.013	0.005	0.007			I
I		C-B	0.119	0.007		0.010				I

QUEUE FOR STREAM B-AC

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1

09.15	0.1
09.30	0.1

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	I	I	I	I	I	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-AC	I	60.3	I	40.2	I	7.9	I
I	C-A	I	544.4	I	362.9	I		I
I	C-B	I	20.6	I	13.7	I	1.8	I
I	A-B	I	27.4	I	18.3	I		I
I	A-C	I	591.0	I	394.0	I		I
I	ALL	I	1243.7	I	829.1	I	9.7	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
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Run with file:- "O:\sel\Updated\J2 obs PM.vpi" at 15:14:17 on Tuesday, 24 May 2016

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RUN TITLE
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J2 obs PM

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MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 13.50 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.50 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 3.00 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 5.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 5.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 3.50 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	AT OF PEAK	AFTER PEAK
ARM A	15.00	45.00	75.00	6.44	9.66	6.44
ARM B	15.00	45.00	75.00	0.44	0.66	0.44
ARM C	15.00	45.00	75.00	5.93	8.89	5.93

I	I	TURNING PROPORTIONS			I
		TURNING COUNTS (VEH/HR) (PERCENTAGE OF H.V.S.)			
TIME	FROM/TO	ARM A	ARM B	ARM C	
08.00 - 09.30					
	ARM A	0.000	0.062	0.938	
		0.0	32.0	483.0	
		(0.0)	(0.0)	(0.0)	
	ARM B	0.686	0.000	0.314	
		24.0	0.0	11.0	
		(0.0)	(0.0)	(0.0)	
	ARM C	0.985	0.015	0.000	
		467.0	7.0	0.0	
		(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	0.44	7.68	0.057		0.0	0.1	0.9	
C-A	5.84							
C-B	0.09	11.71	0.007		0.0	0.0	0.1	
A-B	0.40							
A-C	6.04							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-AC	0.100	0.007	0.013	0.005	0.007			
C-B	0.117	0.009		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	0.52	7.38	0.071		0.1	0.1	1.1	
C-A	6.97							
C-B	0.10	11.45	0.009		0.0	0.0	0.1	
A-B	0.48							
A-C	7.21							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)		TO RIGHT (M)		
B-AC	0.095	0.008	0.013	0.004		0.007		
C-B	0.115	0.010		0.009				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/
I				(RPC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH./MIN/	(VEH./MIN/
I				(RFC)	(PDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

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QUEUE FOR STREAM C-B

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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Run with file:- "O:\sel\Updated\J2 Ref PM.vpi" at 15:14:38 on Tuesday, 24 May 2016

RUN TITLE

J2 Ref PM

MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 13.50 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.50 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 3.00 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 5.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 5.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 3.50 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	I	NUMBER OF	MINUTES FROM	START WHEN	I	RATE	OF FLOW (VEH/MIN)	I
I	ARM	I	FLOW STARTS	I	TOP OF PEAK	I	BEFORE	I	I
I	I	I	TO RISE	I	IS REACHED	I	PEAK	I	I
I	I	I	I	I	I	I	I	I	I
I	I	I	I	I	I	I	I	I	I
I	ARM A	I	15.00	I	45.00	I	11.04	I	11.04
I	ARM B	I	15.00	I	45.00	I	1.71	I	1.71
I	ARM C	I	15.00	I	45.00	I	6.96	I	6.96

		TURNING PROPORTIONS				TURNING COUNTS (VEH/HR)				(PERCENTAGE OF H.V.S)			
TIME		FROM/TO	ARM A	ARM B	ARM C	FROM/TO	ARM A	ARM B	ARM C	FROM/TO	ARM A	ARM B	ARM C
08.00 - 09.30		ARM A	0.000	0.152	0.848								
			0.0	134.0	749.0								
		(0.0)	(0.0)	(0.0)	(0.0)								
		ARM B	0.737	0.000	0.263								
			101.0	0.0	36.0								
		(0.0)	(0.0)	(0.0)	(0.0)								
		ARM C	0.952	0.048	0.000								
			530.0	27.0	0.0								
		(0.0)	(0.0)	(0.0)	(0.0)								

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	1.71	6.81	0.251		0.0	0.3	4.7	
C-A	6.63							
C-B	0.34	10.77	0.031		0.0	0.0	0.5	
A-B	1.67							
A-C	9.36							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
		(.1M)	(.1M)	(M)	(M)			
B-AC	0.089	0.010	0.013	0.004	0.006			
C-B	0.108	0.015		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	2.04	6.37	0.321		0.3	0.5	6.7	
C-A	7.91							
C-B	0.40	10.33	0.039		0.0	0.0	0.6	
A-B	2.00							
A-C	11.18							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)		TO RIGHT (M)		
B-AC	0.083	0.012	0.013	0.004		0.006		
C-B	0.103	0.017		0.008				

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QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

END OF JOB

(C) COPYRIGHT 1998

CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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Run with file:- "O:\sel\Updated\J2_RefDev_AM.vpi" at 15:14:46 on Tuesday, 24 May 2016

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RUN TITLE
*****
J2 RefDev AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
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MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 13.50 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.50 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 3.00 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 5.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 5.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 3.50 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF MINUTES FROM START WHEN				I	RATE OF FLOW (VEH/MIN)				I			
I	ARM	I	FLOW STARTS	I	TOP OF PEAK	I	FLOW STOPS	I	BEFORE	I	AT TOP	I	AFTER	I
I	I	I	TO RISE	I	IS REACHED	I	FALLING	I	PEAK	I	OF PEAK	I	PEAK	I
I	ARM A	I	15.00	I	45.00	I	75.00	I	9.99	I	14.98	I	9.99	I
I	ARM B	I	15.00	I	45.00	I	75.00	I	2.96	I	4.44	I	2.96	I
I	ARM C	I	15.00	I	45.00	I	75.00	I	6.45	I	9.67	I	6.45	I

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME		FROM/TO	ARM A	ARM B	ARM C		
08.00 - 09.30		ARM A	0.000	0.118	0.882		
			0.0	94.0	705.0		
			(0.0)	(0.0)	(0.0)		
		ARM B	0.536	0.000	0.464		
			127.0	0.0	110.0		
			(0.0)	(0.0)	(0.0)		
		ARM C	0.872	0.128	0.000		
			450.0	66.0	0.0		
			(0.0)	(0.0)	(0.0)		

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	2.96	7.46	0.397		0.0	0.6	9.0	
C-A	5.63							
C-B	0.82	10.99	0.075		0.0	0.1	1.2	
A-B	1.17							
A-C	8.81							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY		
MARGINAL	LANE WIDTH		WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT		
CHANGE:	(.1M)		(.1M)	(.1M)	(M)	(M)		
B-AC	0.091	0.009	0.013	0.004	0.006			
C-B	0.110	0.013		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	3.54	7.05	0.502		0.6	1.0	13.8	
C-A	6.72							
C-B	0.99	10.59	0.093		0.1	0.1	1.5	
A-B	1.40							
A-C	10.52							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR)		VISIBILITY TO RIGHT (M)		
B-AC	0.084	0.011	0.013	0.004		0.006		
C-B	0.106	0.016		0.009				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH./MIN/	(VEH./MIN/
I				(RFC)	(PDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

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QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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Run with file:- "O:\sel\Updated\J2_RefDev_PM.vpi" at 15:15:02 on Tuesday, 24 May 2016

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RUN TITLE
*****
J2_RefDev_PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 13.50 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.50 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 3.00 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 5.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 5.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 3.50 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I

.TRAFFIC DEMAND DATA

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF MINUTES FROM START WHEN					I	RATE OF FLOW (VEH/MIN)				I	
I	ARM	I	FLOW STARTS	I	TOP OF PEAK	I	FLOW STOPS	I	BEFORE	I	AT TOP	I	AFTER
I	I	I	TO RISE	I	IS REACHED	I	FALLING	I	PEAK	I	OF PEAK	I	PEAK
I	ARM A	I	15.00	I	45.00	I	75.00	I	11.04	I	16.56	I	11.04
I	ARM B	I	15.00	I	45.00	I	75.00	I	1.71	I	2.57	I	1.71
I	ARM C	I	15.00	I	45.00	I	75.00	I	6.96	I	10.44	I	6.96

		TURNING PROPORTIONS TURNING COUNTS (VEH/HR) (PERCENTAGE OF H.V.S)					
TIME		FROM/TO	ARM A	ARM B	ARM C		
08.00 - 09.30		ARM A	0.000 0.0 (0.0)	0.152 134.0 (0.0)	0.848 749.0 (0.0)		
		ARM B	0.737 101.0 (0.0)	0.000 0.0 (0.0)	0.263 36.0 (0.0)		
		ARM C	0.952 530.0 (0.0)	0.048 27.0 (0.0)	0.000 0.0 (0.0)		

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	1.71	6.81	0.251		0.0	0.3	4.7	
C-A	6.63							
C-B	0.34	10.77	0.031		0.0	0.0	0.5	
A-B	1.67							
A-C	9.36							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR) (M)	VISIBILITY TO RIGHT (M)			
B-AC	0.089	0.010	0.013	0.004	0.006			
C-B	0.108	0.015		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	2.04	6.37	0.321		0.3	0.5	6.7	
C-A	7.91							
C-B	0.40	10.33	0.039		0.0	0.0	0.6	
A-B	2.00							
A-C	11.18							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-AC	0.083	0.012	0.013	0.004	0.006			
C-B	0.103	0.017		0.008				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH./MIN/	(VEH./MIN/
I				(RFC)	(PDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

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QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.1
09.00	0.1
09.15	0.0
09.30	0.0

STREAM	TOTAL DEMAND	* QUEUEING * * DELAY *	* INCLUSIVE QUEUEING * * DELAY *
	(VEH)	(VEH/H) (MIN)	(MIN/VEH) (MIN/VEH)
B-AC	187.9	125.2	46.2 0.25
C-A	726.7	484.5	46.2 0.25
C-B	37.0	24.7	3.8 0.10
A-B	183.7	122.5	46.2 0.25
A-C	1027.0	684.7	46.2 0.25
ALL	2162.4	1441.6	49.9 0.02

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

I	08.30-08.45								I
I	B-AC	5.08	8.60	0.591		0.8	1.4	19.4	I
I	C-A	8.23							I
I	C-B	1.21	10.75	0.112		0.1	0.1	1.8	I
I	A-B	2.72							I
I	A-C	8.41							I
I									I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:							I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			I
I									I
I	B-AC	0.085	0.011	0.013	0.004	0.006			I
I	C-B	0.108	0.015		0.009				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	08.45-09.00									I
I	B-AC	5.08	8.60	0.591		1.4	1.4	21.0		I
I	C-A	8.23								I
I	C-B	1.21	10.75	0.112		0.1	0.1	1.9		I
I	A-B	2.72								I
I	A-C	8.41								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-AC	0.085	0.011	0.013	0.004	0.006				I
I	C-B	0.108	0.015		0.009					I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.00-09.15									I
I	B-AC	4.15	8.96	0.463		1.4	0.9	14.0		I
I	C-A	6.72								I
I	C-B	0.99	11.17	0.088		0.1	0.1	1.5		I
I	A-B	2.22								I
I	A-C	6.87								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-AC	0.092	0.009	0.013	0.004	0.006				I
I	C-B	0.112	0.012		0.009					I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.15-09.30									I
I	B-AC	3.47	9.22	0.377		0.9	0.6	9.6		I
I	C-A	5.63								I
I	C-B	0.82	11.47	0.072		0.1	0.1	1.2		I
I	A-B	1.86								I
I	A-C	5.75								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-AC	0.097	0.008	0.013	0.004	0.007				I
I	C-B	0.115	0.010		0.009					I

QUEUE FOR STREAM B-AC

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.6
08.30	0.8
08.45	1.4
09.00	1.4

09.15	0.9	*
09.30	0.6	*

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	I	I	I	I	I	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-AC	I	381.2	I	254.1	I	84.5	I
I	C-A	I	617.0	I	411.4	I	I	I
I	C-B	I	90.5	I	60.3	I	9.0	I
I	A-B	I	204.3	I	136.2	I	I	I
I	A-C	I	630.8	I	420.5	I	I	I
I	ALL	I	1923.8	I	1282.5	I	93.4	I
I		I		I		I		I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

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RUN TITLE
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J2 Des PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Bailey Street EB
ARM B IS Wan On Street SB
ARM C IS Bailey Street WB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 13.50 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.50 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 3.00 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 5.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 5.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 3.50 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	MINUTES FROM START WHEN FLOW STOPS FALLING	RATE OF FLOW BEFORE PEAK	RATE OF FLOW AT PEAK	RATE OF FLOW AFTER PEAK
ARM A	15.00	45.00	75.00	9.04	13.56	9.04
ARM B	15.00	45.00	75.00	2.00	3.00	2.00
ARM C	15.00	45.00	75.00	6.96	10.44	6.96

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME	FROM/TO	ARM A	ARM B	ARM C			
08.00 - 09.30	ARM A	0.000	0.275	0.725			
		0.0	199.0	524.0			
		(0.0)	(0.0)	(0.0)			
	ARM B	0.256	0.000	0.744			
		41.0	0.0	119.0			
		(0.0)	(0.0)	(0.0)			
	ARM C	0.952	0.048	0.000			
		530.0	27.0	0.0			
		(0.0)	(0.0)	(0.0)			

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-AC	2.00	8.65	0.231		0.0	0.3	4.3	
C-A	6.63							
C-B	0.34	11.18	0.030		0.0	0.0	0.5	
A-B	2.49							
A-C	6.55							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY		
MARGINAL	LANE WIDTH		WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT		
CHANGE:	(.1M)		(.1M)	(.1M)	(M)	(M)		
B-AC	0.095	0.008	0.013	0.004	0.007			
C-B	0.112	0.012		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-AC	2.39	8.34	0.286		0.3	0.4	5.8	
C-A	7.91							
C-B	0.40	10.82	0.037		0.0	0.0	0.6	
A-B	2.97							
A-C	7.82							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-AC	0.089	0.010	0.013	0.004		0.006		
C-B	0.108	0.014		0.009				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

08.30-08.45						
B-AC	2.92	7.90	0.370		0.4	0.6
C-A	9.69					8.3
C-B	0.49	10.33	0.048		0.0	0.0
A-B	3.64					0.7
A-C	9.58					
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:						
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)
B-AC	0.082	0.012	0.013	0.004		0.006
C-B	0.103	0.017		0.008		

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.45-09.00								
B-AC	2.92	7.90	0.370		0.6	0.6	8.7	
C-A	9.69							
C-B	0.49	10.33	0.048		0.0	0.1	0.7	
A-B	3.64							
A-C	9.58							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-AC	0.082	0.012	0.013	0.004	0.006			
C-B	0.103	0.017		0.008				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
09.00-09.15								
B-AC	2.39	8.34	0.286		0.6	0.4	6.3	
C-A	7.91							
C-B	0.40	10.82	0.037		0.1	0.0	0.6	
A-B	2.97							
A-C	7.82							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-AC	0.089	0.010	0.013	0.004	0.006			
C-B	0.108	0.014		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
09.15-09.30								
B-AC	2.00	8.65	0.231		0.4	0.3	4.7	
C-A	6.63							
C-B	0.34	11.18	0.030		0.0	0.0	0.5	
A-B	2.49							
A-C	6.55							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-AC	0.095	0.008	0.013	0.004	0.007			
C-B	0.112	0.012		0.009				

QUEUE FOR STREAM B-AC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.3
08.30	0.4
08.45	0.6
09.00	0.6

09.15	0.4
09.30	0.3

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.1
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	TOTAL DEMAND		* QUEUEING * * DELAY *		* INCLUSIVE QUEUEING * * DELAY *	
		(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-AC	I 219.4	I 146.3	I 38.1	I 0.17	I 38.1	I 0.17
I	C-A	I 726.7	I 484.5	I	I	I	I
I	C-B	I 37.0	I 24.7	I 3.6	I 0.10	I 3.6	I 0.10
I	A-B	I 272.9	I 181.9	I	I	I	I
I	A-C	I 718.5	I 479.0	I	I	I	I
I	ALL	I 1974.5	I 1316.4	I 41.6	I 0.02	I 41.6	I 0.02

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: **Ma Tau Wan Road / Wing Kwong Street**

Design Year: **2015**

Description: **Observed**

Designed By: **ALX**

Checked By: **KSC**

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB	↕	A	1	3,500						1965	1965	148	0.075		174	0.089	
		A	1	3,500						2105	2105	158	0.075		187	0.089	
	↕	A	1	3,500						2105	2105	158	0.075		187	0.089	
Ma Tau Wai Road SB	↕	B	1	3,000						2055	2055	387	0.188	0.188	377	0.183	0.183
		B	1	3,000						2055	2055	387	0.188		377	0.183	
	↕	B	1	3,000						1915	1915	360	0.188		351	0.183	
Wing Kwong Street WB	↙	C	2	5,500	10					1885	1885	86	0.046	0.046	109	0.058	0.058
Pedestrian Crossing																	
Ep	1	MIN GREEN + FLASH =	5	+	6	=	11										
Fp	2	MIN GREEN + FLASH =	5	+	10	=	15										
Notes:																	
	Flow: (pcu/hr)		<div><div><div>1134(105)</div><div>484(548)</div><div>86(109)</div></div><div><div>→</div><div>↘</div><div>↗</div><div>←</div><div>↖</div><div>↙</div><div>↘</div><div>↗</div><div>←</div><div>↖</div><div>↙</div><div>↘</div><div>↗</div><div>←</div><div>↖</div><div>↙</div></div></div>														
			<div><div><div>Group</div><div>y</div><div>L (sec)</div><div>C (sec)</div><div>y pract.</div><div>R.C. (%)</div></div><div>0.188</div><div>22</div><div>130</div><div>0.748</div><div>297%</div></div> <div><div><div>B/C</div><div>0.234</div><div>9</div><div>130</div><div>0.838</div><div>258%</div></div></div> <div><div><div>Group</div><div>y</div><div>L (sec)</div><div>C (sec)</div><div>y pract.</div><div>R.C. (%)</div></div><div>0.183</div><div>22</div><div>130</div><div>0.748</div><div>308%</div></div> <div><div><div>B/C</div><div>0.241</div><div>9</div><div>130</div><div>0.838</div><div>247%</div></div></div>														
Stage / Phase Diagrams																	
1.	2.		3.		4		5										
<div><div><div><div>A</div><div>B</div><div>Ep</div></div><div><div>←</div><div>→</div><div>↖</div><div>↗</div><div>C</div></div></div><div><div><div>I/G= 5</div><div>I/G= 6</div><div>I/G= 6</div><div>I/G=</div><div>I/G=</div><div>I/G=</div></div><div><div><div>I/G= 5</div><div>I/G= 6</div><div>I/G= 6</div><div>I/G=</div><div>I/G=</div><div>I/G=</div></div><div><div><div>Date: MAY 2016</div><div>Junction: Ma Tau Wan Road / Wing Kwong Street</div><div>(35)</div></div></div></div></div></div>																	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Ma Tau Wan Road / Wing Kwong Street

Design Year: 2028

Description: Reference

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB	↕	A	1	3,500						1965	1965	189	0.096		211	0.107	
	↕	A	1	3,500						2105	2105	202	0.096		226	0.107	
	↕	A	1	3,500						2105	2105	202	0.096		226	0.107	
Ma Tau Wai Road SB	↕	B	1	3,000						2055	2055	452	0.220	0.220	443	0.216	
	↕	B	1	3,000						2055	2055	452	0.220		443	0.216	
	↕	B	1	3,000						1915	1915	421	0.220		413	0.216	0.216
Wing Kwong Street WB	↕	C	2	5,500	10					1885	1885	100	0.053	0.053	125	0.066	0.066
	↕	C	2	5,500	10					1885	1885	100	0.053	0.053	125	0.066	0.066
Pedestrian Crossing																	
Ep		1	MIN GREEN + FLASH =		5	+	6	=	11								
Fp		2	MIN GREEN + FLASH =		5	+	10	=	15								
Notes:																	
Flow: (pcu/hr)		<div><div><div>1325(1299)</div><div>593(663)</div><div>100(125)</div></div><div><div>↑</div><div>N</div></div></div>															
Group		B/Fp	B/C	Group		B/Fp	B/C										
y		0.220	0.273	y		0.216	0.282										
L (sec)		22	9	L (sec)		22	9										
C (sec)		130	130	C (sec)		130	130										
y pract.		0.748	0.838	y pract.		0.748	0.838										
R.C. (%)		240%	207%	R.C. (%)		247%	197%										
Stage / Phase Diagrams																	
1.		2.		3.		4.		5.									
I/G=5		I/G=6		I/G=		I/G=		I/G=									
I/G=5		I/G=6		I/G=		I/G=		I/G=									
Date: MAY, 2016																	
Junction: Ma Tau Wan Road / Wing Kwong Street																	
(15)																	

TRAFFIC SIGNALS CALCULATION

Job No.: **CHKSD143110**

MVA HONG KONG LIMITED

Junction: **Ma Tau Wan Road / Wing Kwong Street**

Design Year: **2028**

Description: **Reference (With Development)**

Designed By: **ALX**

Checked By: **KSC**

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB	A	A	1	3,500						1965	1965	195	0.099		217	0.110	
		A	1	3,500						2105	2105	209	0.099		233	0.111	
	A	A	1	3,500						2105	2105	209	0.099		233	0.111	
		A	1	3,500						2105	2105	209	0.099		233	0.111	
Ma Tau Wai Road SB	B	B	1	3,000						2055	2055	462	0.225	0.225	459	0.223	
		B	1	3,000						2055	2055	462	0.225		459	0.223	
	B	B	1	3,000						1915	1915	430	0.225		428	0.223	
		B	1	3,000						1915	1915	430	0.225		428	0.223	0.223
Wing Kwong Street WB	C	C	2	5,500	10					1885	1885	313	0.166	0.166	263	0.140	0.140
		C	2	5,500	10					1885	1885	313	0.166	0.166	263	0.140	0.140
Pedestrian Crossing																	
Ep	1	MIN GREEN + FLASH =		5	+	6	=	11									
Fp	2	MIN GREEN + FLASH =		5	+	10	=	15									
Notes:																	
				Flow: (pcu/hr)													
				Group		A/C	B/C	Group		B/Fp	B/C						
				y		0.265	0.391	y		0.223	0.363						
				L (sec)		9	9	L (sec)		22	9						
				C (sec)		130	130	C (sec)		130	130						
				y pract.		0.838	0.838	y pract.		0.748	0.838						
				R.C. (%)		216%	114%	R.C. (%)		235%	131%						
Stage / Phase Diagrams																	
1.		2.		3.		4.		5.									
I/G= 5 I/G= 5		I/G= 6 I/G= 6		I/G= 5 I/G= 5		I/G= 6 I/G= 6		I/G= 5 I/G= 5		I/G= 6 I/G= 6		I/G= 5 I/G= 5		I/G= 6 I/G= 6		I/G= 5 I/G= 5	
Date: MAY, 2016																	
Junction: Ma Tau Wan Road / Wing Kwong Street																	
(15)																	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MYA HONG KONG LIMITED

Junction: Ma Tau Wan Road / Wing Kwong StreetDesign Year: 2028Description: DesignDesigned By: ALXChecked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Ma Tau Wai Road NB	↕	A	1	3,500						1965	1965	195	0.099		217	0.110	
	↕	A	1	3,500						2105	2105	209	0.099		233	0.111	
	↕	A	1	3,500						2105	2105	209	0.099		233	0.111	
Ma Tau Wai Road SB	↕	B	1	3,000						2055	2055	453	0.220		451	0.219	0.219
	↕	B	1	3,000						2055	2055	454	0.221	0.221	450	0.219	
	↕	B	1	3,000						1915	1915	422	0.220		420	0.219	
Wing Kwong Street WB	↙	C	2	5,500	10					1885	1885	203	0.108	0.108	196	0.104	0.104
<p>Pedestrian Crossing</p> <p>Ep 1 MIN GREEN + FLASH = 5 + 6 = 11</p> <p>Fp 2 MIN GREEN + FLASH = 5 + 10 = 15</p>																	
<p>Notes:</p> <p>Flow: (pcu/hr)</p> <p>1329(1321) ↗</p> <p>613(663) ↘</p> <p>203(196) ↘</p> <p>↑ N</p>																	
<p>Stage / Phase Diagrams</p> <p>1. 2. 3. 4. 5</p> <p>Diagram 1: Approach A, Movements B, Ep, Fp</p> <p>Diagram 2: Approach Fp, Movements C</p> <p>Diagram 3: Approach C, Movements A, B, Ep</p> <p>Diagram 4: Approach A, Movements B, Ep, Fp</p> <p>Diagram 5: Approach Fp, Movements C</p>																	
I/G=5				I/G=6				I/G=			I/G=		I/G=		I/G=		
I/G=5				I/G=6				I/G=			I/G=		I/G=		I/G=		
<p>Date: <u>MAY, 2016</u></p> <p>Junction: <u>Ma Tau Wan Road / Wing Kwong Street</u></p> <p>(J5)</p>																	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Ma Tau Wai Road / To Kwa Wan Road / Chi Kiang Street

Design Year: 2015

Description: Observed

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)		Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak																																						
					Left	Right			AM	PM	AM	PM	Flow (pcu/hr)	Y Value	Critical Y	Flow (pcu/hr)	Y Value	Critical Y																																				
To Kwa Wan Road NB	→	A	1,2	3,500							1965	1965	195	0.099		250	0.127																																					
	→	A	1,2	3,500							2105	2105	209	0.099		267	0.127																																					
	↘	B	2	3,500		20					1960	1960	60	0.031	0.031	36	0.018	0.018																																				
To Kwa Wan Road SB	←	C	1	1,900	8		49%	52%			1655	1645	292	0.176	0.176	330	0.201	0.201																																				
	←	C	1	3,000							2055	2055	362	0.176		412	0.200																																					
				3,000							2055	2055	362	0.176		412	0.200																																					
Chi Kiang Street EB	↓	D	4	3,100	12						1710	1710	105	0.061		120	0.070																																					
	↓	F	4	3,100							2065	2065	181	0.088	0.088	151	0.073	0.073																																				
	↓	E	3,4	3,100		8					1740	1740	193	0.111		136	0.078																																					
	↓	E	3,4	3,100		10					1675	1675	185	0.110		131	0.078																																					
Pedestrian Crossing																																																						
Gp		1,2	MIN GREEN + FLASH =		6	+	11	=	17																																													
Hp		3	MIN GREEN + FLASH =		5	+	8	=	13																																													
Ip		2,3,4	MIN GREEN + FLASH =		5	+	9	=	14																																													
Jp		3	MIN GREEN + FLASH =		5	+	8	=	13																																													
Notes:																																																						
* 1.1m is deducted from the carriageway width due to the bus stop at 35m before the stop line				Flow: (pcu/hr)																																																		
				<table><tr><td>Group</td><td>C.B.,Jp,D</td><td>C.B.,Jp,F</td><td>Group</td><td>C.B.,Jp,D</td><td>C.B.,Jp,F</td></tr><tr><td>Y</td><td>0.268</td><td>0.295</td><td>Y</td><td>0.271</td><td>0.274</td></tr><tr><td>L (sec)</td><td>30</td><td>30</td><td>L (sec)</td><td>30</td><td>30</td></tr><tr><td>C (sec)</td><td>108</td><td>108</td><td>C (sec)</td><td>108</td><td>108</td></tr><tr><td>Y pract.</td><td>0.650</td><td>0.650</td><td>Y pract.</td><td>0.650</td><td>0.650</td></tr><tr><td>R.C. (%)</td><td>142%</td><td>121%</td><td>R.C. (%)</td><td>140%</td><td>137%</td></tr></table>															Group	C.B.,Jp,D	C.B.,Jp,F	Group	C.B.,Jp,D	C.B.,Jp,F	Y	0.268	0.295	Y	0.271	0.274	L (sec)	30	30	L (sec)	30	30	C (sec)	108	108	C (sec)	108	108	Y pract.	0.650	0.650	Y pract.	0.650	0.650	R.C. (%)	142%	121%	R.C. (%)	140%	137%
Group	C.B.,Jp,D	C.B.,Jp,F	Group	C.B.,Jp,D	C.B.,Jp,F																																																	
Y	0.268	0.295	Y	0.271	0.274																																																	
L (sec)	30	30	L (sec)	30	30																																																	
C (sec)	108	108	C (sec)	108	108																																																	
Y pract.	0.650	0.650	Y pract.	0.650	0.650																																																	
R.C. (%)	142%	121%	R.C. (%)	140%	137%																																																	
Stage / Phase Diagrams																																																						
1.		2.		3.		4.		5.																																														
I/G= 6	I/G= 6	I/G= 6	I/G= 5	13	I/G= 3		I/G=																																															
I/G= 6	I/G= 6			13	I/G= 3		I/G=																																															
Date: MAY, 2016										Junction: J7 (CTIA, J15)																																												
Ma Tau Wai Road / To Kwa Wan Road / Chi Kiang Street																																																						

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: Ma Tau Wai Road / To Kwa Wan Road / Chi Kiang Street

Design Year: 2028

Description: Reference (With Development)

Designed By: ALX

Checked By: KSC

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
To Kwa Wan Road NB	→	A	1,2	3,500													
	→	A	1,2	3,500													
	↘	B	2	3,500		20											
To Kwa Wan Road SB	*↖	C	1	1,900	8		40%	43%									
	←	C	1	3,000													
	←	C	1	3,000													
Chi Kiang Street EB	↘	D	4	3,100	12												
	↓	F	4	3,100													
	↓	E	3,4	3,100		8											
	↓	E	3,4	3,100		10											

Pedestrian Crossing	GP	1,2	MIN GREEN + FLASH =	6	+	11	=	17
	Hp	3	MIN GREEN + FLASH =	5	+	8	=	13
	Ip	2,3,4	MIN GREEN + FLASH =	5	+	9	=	14
	Jp	3	MIN GREEN + FLASH =	5	+	8	=	13

Notes:

* 1.1m is deducted from the carriageway width due to the bus stop at 35m before the stop line

Flow: (pcu/hr)	Group		Group		Group		Group	
	y	C (sec)	y pract.	R.C. (%)	y	C (sec)	y pract.	R.C. (%)
421(279) 206(170) 120(135)	0.343	30	0.650	89%	0.372	30	0.650	75%
543(643) 70(40)	0.351	30	0.650	85%	0.355	30	0.650	83%
1215(1380) 160(195)								

Stage / Phase Diagrams				
1.	2.	3.	4.	5
I/G=6	I/G=6	I/G=5	I/G=3	I/G=
I/G=6	I/G=6	I/G=5	I/G=3	I/G=
Date: <u>MAY, 2016</u> Junction: <u>J7 (CTIA) J15</u> <u>Ma Tau Wai Road / To Kwa Wan Road / Chi Kiang Street</u>				

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Ma Tau Wai Road / To Kwa Wan Road / Chi Kiang Street

Design Year: 2028

Description: _____ Design _____

Designed By: ALX

Checked By: KSC

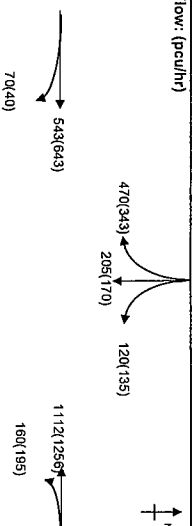
[illegible]

Pedestrian Crossing

Gp	1,2	MIN GREEN + FLASH =	6	+	11	=	17
Hp	3	MIN GREEN + FLASH =	5	+	8	=	13
lp	2,3,4	MIN GREEN + FLASH =	5	+	9	=	14
lp	3	MIN GREEN + FLASH =	5	+	8	=	13

Notes:

* 1.1m is deducted from the carriageway width due to the bus stop at 35m before the stop line



Group		Group			
	C,B,E	C,B,D	C,B,F		
y	0.394	0.355	y	0.331	0.334
L (sec)	14	30	L (sec)	30	30
C (sec)	108	108	C (sec)	108	108
y pract.	0.783	0.650	y pract.	0.650	0.650
R.C. (%)	99%	83%	R.C. (%)	97%	95%

Stage / Phase Diagrams

1.	
2.	
3.	
4.	
5.	

I/G=6		I/G=5	I/G=3	I/G=
I/G=6	I/G=6	13	I/G=3	I/G=
I/G=6	I/G=6	13	I/G=3	I/G=
			Date:	J7 (CT) J15
			MAY, 2016	
			Ma Tau Wai Road / To Kwa Wan Road / Chi Kwang Street	

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J8 Obs AM

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MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Chi Kiang Street WB
ARM B IS Lung Tak Street SB
ARM C IS Chi Kiang Street EB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 14.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 6.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 8.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 3.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

ARM			NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	MINUTES FROM START WHEN FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	RATE OF FLOW (VEH/MIN) AT TOP OF PEAK	RATE OF FLOW (VEH/MIN) AFTER PEAK
ARM A			15.00	45.00	75.00	0.00	0.00	0.00
ARM B			15.00	45.00	75.00	0.10	0.15	0.10
ARM C			15.00	45.00	75.00	4.75	7.13	4.75

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF VEH.S)			
TIME	FROM/TO	ARM A	ARM B	ARM C	
08.00 - 09.30					
	ARM A	0.000	0.000	0.000	
		I???????	I???????	I???????	
		I (0.0)	I (0.0)	I (0.0)	
	ARM B	1.000	0.000	0.000	
		8.0	0.0	0.0	
		I (0.0)	I (0.0)	I (0.0)	
	ARM C	1.000	0.000	0.000	
		380.0	0.0	0.0	
		I (0.0)	I (0.0)	I (0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.74	0.000		0.0	0.0	0.0	
B-A	0.10	7.63	0.013		0.0	0.0	0.2	
C-A	4.75							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.105	0.000				0.010		
B-A	0.076	0.003	0.018	0.005		0.008		
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.73	0.000		0.0	0.0	0.0	
B-A	0.12	7.54	0.016		0.0	0.0	0.2	
C-A	5.67							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.104	0.000				0.010		
B-A	0.075	0.003	0.018	0.005		0.008		
C-B	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.72	0.000	0.0	0.0	0.0		I
I	B-A	0.15	7.43	0.020	0.0	0.0	0.3		I
I	C-A	6.95							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I	B-C	0.104	0.000			0.010			I
I	B-A	0.074	0.004	0.018	0.005	0.007			I
I	C-B	0.130	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH_MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH_MIN/ TIME SEGMENT)
	08.45-09.00								
	B-C	0.00	8.72	0.000		0.0	0.0	0.0	
	B-A	0.15	7.43	0.020		0.0	0.0	0.3	
	C-A	6.95							
	C-B	0.00	10.89	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-C	0.104	0.000					0.010	
	B-A	0.074	0.004	0.018	0.005			0.007	
	C-B	0.130	0.000		0.010				

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)		
I	09.00-09.15									I	
I	B-C	0.00	8.73	0.000		0.0	0.0	0.0		I	
I	B-A	0.12	7.54	0.016		0.0	0.0	0.2		I	
I	C-A	5.67								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.104	0.000					0.010		I	
I	B-A	0.075	0.003	0.018	0.005	0.008				I	
I	C-B	0.130	0.000		0.010					I	

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH_MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH_MIN/ TIME SEGMENT)		
I	09.15-09.30									I	
I	B-C	0.00	8.74	0.000		0.0	0.0	0.0		I	
I	B-A	0.10	7.63	0.013		0.0	0.0	0.2		I	
I	C-A	4.75								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.105	0.000					0.010		I	
I	B-A	0.076	0.003	0.018	0.005	0.008				I	
I	C-B	0.130	0.000		0.010					I	

QUEUE FOR STREAM B-C

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	0.0	I	0.0	I	0.0
I	B-A	I	11.0	7.3	I	1.5	I	0.13
I	C-A	I	521.1	347.4	I		I	
I	C-B	I	0.0	0.0	I	0.0	I	0.00
I	A-B	I	0.0	0.0	I		I	
I	A-C	I	0.0	0.0	I		I	
I	ALL	I	532.0	354.7	I	1.5	I	0.00

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
---	------	--------	----------	---------	------------	-------	-----	-------	------------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
I 08.30-08.45									I
I B-C	0.00	8.67	0.000		0.0	0.0	0.0		I
I B-A	0.33	7.47	0.044		0.0	0.0	0.7		I
I C-A	6.49								I
I C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I B-C		0.104	0.000				0.010		I
I B-A		0.075	0.004	0.018	0.005	0.007			I
I C-B		0.130	0.000		0.010				I

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 08.45-09.00									I
I B-C	0.00	8.67	0.000			0.0	0.0	0.0	I
I B-A	0.33	7.47	0.044			0.0	0.0	0.7	I
I C-A	6.49								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I B-C		0.104	0.000					0.010	I
I B-A		0.075	0.004	0.018	0.005	0.007			I
I C-B		0.130	0.000		0.010				I

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 09.00-09.15									I
I B-C	0.00	8.69	0.000			0.0	0.0	0.0	I
I B-A	0.27	7.58	0.035			0.0	0.0	0.6	I
I C-A	5.30								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I B-C		0.104	0.000					0.010	I
I B-A		0.076	0.003	0.018	0.005	0.008			I
I C-B		0.130	0.000		0.010				I

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 09.15-09.30									I
I B-C	0.00	8.70	0.000			0.0	0.0	0.0	I
I B-A	0.22	7.66	0.029			0.0	0.0	0.5	I
I C-A	4.44								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I B-C		0.104	0.000					0.010	I
I B-A		0.077	0.003	0.018	0.005	0.008			I
I C-B		0.130	0.000		0.010				I

QUEUE FOR STREAM B-C

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	I	0.0	I	0.00	I
I	B-A	I	24.7	I	16.5	I	3.4	I
I	C-A	I	486.8	I	324.5	I		I
I	C-B	I	0.0	I	0.0	I	0.00	I
I	A-B	I	0.0	I	0.0	I		I
I	A-C	I	0.0	I	0.0	I		I
I	ALL	I	511.5	I	341.0	I	3.4	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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RUN TITLE
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J8 Ref AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Chi Kiang Street WB
ARM B IS Lung Tak Street SB
ARM C IS Chi Kiang Street EB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 14.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 6.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 8.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 3.00 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	MINUTES FROM START WHEN FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	RATE OF FLOW (VEH/MIN) AT TOP OF PEAK	RATE OF FLOW (VEH/MIN) AFTER PEAK
ARM A	15.00	45.00	75.00	0.00	0.00	0.00
ARM B	15.00	45.00	75.00	0.13	0.19	0.13
ARM C	15.00	45.00	75.00	5.31	7.97	5.31

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF H.V.S.)			
TIME	FROM/TO	ARM A	ARM B	ARM C	
08.00 - 09.30	ARM A	0.000	0.000	0.000	
		1.000	1.000	1.000	
		(0.0)	(0.0)	(0.0)	
	ARM B	1.000	0.000	0.000	
		10.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	
	ARM C	1.000	0.000	0.000	
		425.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.73	0.000		0.0	0.0	0.0	
B-A	0.13	7.58	0.017		0.0	0.0	0.2	
C-A	5.31							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.104	0.000				0.010		
B-A	0.076	0.003	0.018	0.005		0.008		
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.72	0.000		0.0	0.0	0.0	
B-A	0.15	7.48	0.020		0.0	0.0	0.3	
C-A	6.34							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.104	0.000				0.010		
B-A	0.075	0.004	0.018	0.005		0.007		
C-B	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.71	0.000	0.0	0.0	0.0		I
I	B-A	0.18	7.35	0.025	0.0	0.0	0.4		I
I	C-A	7.77							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
I	B-C	0.104	0.000			0.010			I
I	B-A	0.074	0.005	0.018	0.005	0.007			I
I	C-B	0.130	0.000		0.010				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH_MIN/	(VEH_MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	08.45-09.00									I
I	B-C	0.00	8.71	0.000		0.0	0.0	0.0		I
I	B-A	0.18	7.35	0.025		0.0	0.0	0.4		I
I	C-A	7.77								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-C	0.104	0.000					0.010		I
I	B-A	0.074	0.005	0.018	0.005	0.007				I
I	C-B	0.130	0.000		0.010					I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
	09.00-09.15								
	B-C	0.00	8.72	0.000		0.0	0.0	0.0	
	B-A	0.15	7.48	0.020		0.0	0.0	0.3	
	C-A	6.34							
	C-B	0.00	10.89	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
	B-C	0.104	0.000			0.010			
	B-A	0.075	0.004	0.018	0.005	0.007			
	C-B	0.130	0.000		0.010				

I	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)	I
I	09.15-09.30									I
I	B-C	0.00	8.73	0.000		0.0	0.0	0.0		I
I	B-A	0.13	7.58	0.017		0.0	0.0	0.3		I
I	C-A	5.31								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I	B-C	0.104	0.000					0.010		I
I	B-A	0.076	0.003	0.018	0.005			0.008		I
I	C-B	0.130	0.000		0.010					I

QUEUE FOR STREAM B-C

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	I	(MIN)	I	(MIN)	I
I	I	I	(VEH/H)	I	(MIN/VEH)	I	(MIN/VEH)	I
I	B-C	I	0.0	I	0.0	I	0.0	I
I	B-A	I	13.7	I	1.9	I	1.9	I
I	C-A	I	582.8	I	388.5	I		I
I	C-B	I	0.0	I	0.0	I	0.0	I
I	A-B	I	0.0	I	0.0	I		I
I	A-C	I	0.0	I	0.0	I		I
I	ALL	I	596.5	I	1.9	I	1.9	I

- * DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
- * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
- * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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RUN TITLE
*****
J8 Ref PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Chi Kiang Street WB
ARM B IS Lung Tak Street SB
ARM C IS Chi Kiang Street EB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 14.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 6.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 8.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 3.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM			NUMBER OF MINUTES FROM START WHEN FLOW STARTS TO RISE	TOP OF PEAK IS REACHED	FLOW STOPS FALLING	BEFORE PEAK	AT TOP OF PEAK	AFTER PEAK
ARM A			15.00	45.00	75.00	0.00	0.00	0.00
ARM B			15.00	45.00	75.00	0.31	0.47	0.31
ARM C			15.00	45.00	75.00	5.06	7.59	5.06

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF H. V. S)			
TIME	FROM/TO	ARM A	ARM B	ARM C	
08.00 - 09.30					
	ARM A	0.000	0.000	0.000	
		1.000	1.000	1.000	
		(0.0)	(0.0)	(0.0)	
	ARM B	1.000	0.000	0.000	
		25.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	
	ARM C	1.000	0.000	0.000	
		405.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.68	0.000		0.0	0.0	0.0	
B-A	0.31	7.60	0.041		0.0	0.0	0.6	
C-A	5.06							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.104	0.000			0.010			
B-A	0.076	0.003	0.018	0.005	0.008			
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.66	0.000		0.0	0.0	0.0	
B-A	0.37	7.51	0.050		0.0	0.1	0.8	
C-A	6.05							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.104	0.000				0.010		
B-A	0.075	0.004	0.018	0.005		0.008		
C-B	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.63	0.000	0.0	0.0	0.0		I
I	B-A	0.46	7.38	0.062	0.1	0.1	1.0		I
I	C-A	7.40							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			
B-C	0.103	0.000				0.009			
B-A	0.074	0.005	0.018	0.005		0.007			
C-B	0.130	0.000		0.010					

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	
I	08.45-09.00								I
I	B-C	0.00	8.63	0.000	0.0	0.0	0.0		I
I	B-A	0.46	7.38	0.062	0.1	0.1	1.0		I
I	C-A	7.40							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			
B-C	0.103	0.000				0.009			
B-A	0.074	0.005	0.018	0.005		0.007			
C-B	0.130	0.000		0.010					

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	
I	09.00-09.15								I
I	B-C	0.00	8.66	0.000	0.0	0.0	0.0		I
I	B-A	0.37	7.51	0.050	0.1	0.1	0.8		I
I	C-A	6.05							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			
B-C	0.104	0.000				0.010			
B-A	0.075	0.004	0.018	0.005		0.008			
C-B	0.130	0.000		0.010					

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	
I	09.15-09.30								I
I	B-C	0.00	8.68	0.000	0.0	0.0	0.0		I
I	B-A	0.31	7.60	0.041	0.1	0.0	0.7		I
I	C-A	5.06							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			
B-C	0.104	0.000				0.010			
B-A	0.076	0.003	0.018	0.005		0.008			
C-B	0.130	0.000		0.010					

QUEUE FOR STREAM B-C

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	0.0	I	0.0	I	0.0
I	B-A	I	34.3	22.9	I	4.8	I	0.14
I	C-A	I	555.3	370.2	I		I	
I	C-B	I	0.0	0.0	I	0.0	I	0.00
I	A-B	I	0.0	0.0	I		I	
I	A-C	I	0.0	0.0	I		I	
I	ALL	I	589.6	393.1	I	4.8	I	0.01

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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Visual PICADY 4 ANALYSIS PROGRAM
RELEASE 2.1 (DEC 1998)

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RUN TITLE
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J8_RefDev_AM

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INPUT DATA

I
I
I
I
I
I
MINOR ROAD (ARM B)

STREAM LABELLING CONVENTION

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 14.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 6.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 8.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 3.00 M.	I

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

I	I	NUMBER OF	MINUTES FROM START WHEN	I	RATE OF FLOW (VEH/MIN)	I
I	ARM	FLOW STARTS	I TOP OF PEAK	I FLOW STOPS	I BEFORE	I AFTER
I	I	TO RISE	I IS REACHED	I FALLING	I PEAK	I PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I 0.00	I 0.00
I	ARM B	I 15.00	I 45.00	I 75.00	I 0.13	I 0.19
I	ARM C	I 15.00	I 45.00	I 75.00	I 5.31	I 7.97

I		I	TURNING PROPORTIONS						I	
I		I	TURNING COUNTS (VEH/HR)						I	
I		I	(PERCENTAGE OF H.V.S.)						I	
I	TIME	I	FROM/TO	I	ARM A	I	ARM B	I	ARM C	I
I	08.00 - 09.30	I		I		I		I		I
I		I	ARM A	I	0.000	I	0.000	I	0.000	I
I		I		I	1???????	I	1???????	I	1???????	I
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I
I		I		I		I		I		I
I		I	ARM B	I	1.000	I	0.000	I	0.000	I
I		I		I	10.0	I	0.0	I	0.0	I
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I
I		I		I		I		I		I
I		I	ARM C	I	1.000	I	0.000	I	0.000	I
I		I		I	425.0	I	0.0	I	0.0	I
I		I		I	(0.0)	I	(0.0)	I	(0.0)	I
I		I		I		I		I		I

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.73	0.000		0.0	0.0	0.0	
B-A	0.13	7.58	0.017		0.0	0.0	0.2	
C-A	5.31							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR)	VISIBILITY TO RIGHT (M)			
B-C	0.104	0.000					0.010	
B-A	0.076	0.003	0.018	0.005			0.008	
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.72	0.000		0.0	0.0	0.0	
B-A	0.15	7.48	0.020		0.0	0.0	0.3	
C-A	6.34							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.104	0.000			0.010			
B-A	0.075	0.004	0.018	0.005	0.007			
C-B	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC	DELAYI
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	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.71	0.000	0.0	0.0	0.0		I
I	B-A	0.18	7.35	0.025	0.0	0.0	0.4		I
I	C-A	7.77							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I	B-C	0.104	0.000			0.010			I
I	B-A	0.074	0.005	0.018	0.005	0.007			I
I	C-B	0.130	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
	08.45-09.00								
	B-C	0.00	8.71	0.000		0.0	0.0	0.0	
	B-A	0.18	7.35	0.025		0.0	0.0	0.4	
	C-A	7.77							
	C-B	0.00	10.89	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:							
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-C	0.104	0.000					0.010	
	B-A	0.074	0.005	0.018	0.005			0.007	
	C-B	0.130	0.000		0.010				

I	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I	09.00-09.15									I
I	B-C	0.00	8.72	0.000		0.0	0.0	0.0		I
I	B-A	0.15	7.48	0.020		0.0	0.0	0.3		I
I	C-A	6.34								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-C	0.104	0.000					0.010		I
I	B-A	0.075	0.004	0.018	0.005			0.007		I
I	C-B	0.130	0.000		0.010					I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	
I	09.15-09.30									I
I	B-C	0.00	8.73	0.000		0.0	0.0	0.0		I
I	B-A	0.13	7.58	0.017		0.0	0.0	0.3		I
I	C-A	5.31								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
I				MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I	B-C	0.104	0.000				0.010			I
I	B-A	0.076	0.003	0.018	0.005	0.008				I
I	C-B	0.130	0.000		0.010					I

QUEUE FOR STREAM	B-C
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM	B-A
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM	C-B
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I		I		I		I		I	
I		I		I		I		I	
I		I		I		I		I	
I		I		I		I		I	
I		I		I		I		I	
I		I		I		I		I	
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* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
08.30-08.45									
B-C	0.00	8.63	0.000		0.0	0.0	0.0		
B-A	0.46	7.38	0.062		0.1	0.1	1.0		
C-A	7.40								
C-B	0.00	10.89	0.000		0.0	0.0	0.0		
A-B	0.00								
A-C	0.00								
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:		(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
B-C	0.103	0.000				0.009			
B-A	0.074	0.005		0.018	0.005	0.007			
C-B	0.130	0.000			0.010				

TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	
	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
08.45-09.00									
B-C	0.00	8.63	0.000		0.0	0.0	0.0		
B-A	0.46	7.38	0.062		0.1	0.1	1.0		
C-A	7.40								
C-B	0.00	10.89	0.000		0.0	0.0	0.0		
A-B	0.00								
A-C	0.00								
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:		(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
B-C	0.103	0.000				0.009			
B-A	0.074	0.005		0.018	0.005	0.007			
C-B	0.130	0.000			0.010				

TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	
	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
09.00-09.15									
B-C	0.00	8.66	0.000		0.0	0.0	0.0		
B-A	0.37	7.51	0.050		0.1	0.1	0.8		
C-A	6.05								
C-B	0.00	10.89	0.000		0.0	0.0	0.0		
A-B	0.00								
A-C	0.00								
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:		(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
B-C	0.104	0.000				0.010			
B-A	0.075	0.004		0.018	0.005	0.008			
C-B	0.130	0.000			0.010				

TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	
	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
09.15-09.30									
B-C	0.00	8.68	0.000		0.0	0.0	0.0		
B-A	0.31	7.60	0.041		0.1	0.0	0.7		
C-A	5.06								
C-B	0.00	10.89	0.000		0.0	0.0	0.0		
A-B	0.00								
A-C	0.00								
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:		(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
B-C	0.104	0.000				0.010			
B-A	0.076	0.003		0.018	0.005	0.008			
C-B	0.130	0.000			0.010				

QUEUE FOR STREAM B-C

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

STREAM	TOTAL DEMAND	* QUEUEING *	* INCLUSIVE QUEUEING *
		* DELAY *	* DELAY *
	(VEH)	(MIN)	(MIN)
	(VEH/H)	(MIN/VEH)	(MIN/VEH)
B-C	0.0	0.0	0.0
B-A	34.3	4.8	4.8
C-A	555.3	370.2	
C-B	0.0	0.0	0.0
A-B	0.0	0.0	
A-C	0.0		
ALL	589.6	4.8	4.8

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END OF JOB

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RELEASE 2.1 (DEC 1998)

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)
|
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Chi Kiang Street WB
ARM B IS Lung Tak Street SB
ARM C IS Chi Kiang Street EB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 14.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 6.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 8.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 3.00 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

I	I	NUMBER OF	MINUTES FROM	START WHEN	I	RATE OF	FLOW (VEH/MIN)	I
I	ARM	I FLOW STARTS	I TOP OF PEAK	I FLOW STOPS	I	BEFORE	I AT TOP	I AFTER
I	I	I TO RISE	I IS REACHED	I FALLING	I	PEAK	I OF PEAK	I PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I	0.00	I 0.00	I 0.00
I	ARM B	I 15.00	I 45.00	I 75.00	I	0.79	I 1.18	I 0.79
I	ARM C	I 15.00	I 45.00	I 75.00	I	5.31	I 7.97	I 5.31

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME	FROM/TO	ARM A	ARM B	ARM C			
08.00 - 09.30	ARM A	0.000	0.000	0.000			
		I???????	I???????	I???????			
		I (0.0)	I (0.0)	I (0.0)			
	ARM B	1.000	0.000	0.000			
		63.0	0.0	0.0			
		I (0.0)	I (0.0)	I (0.0)			
	ARM C	1.000	0.000	0.000			
		425.0	0.0	0.0			
		I (0.0)	I (0.0)	I (0.0)			

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.54	0.000		0.0	0.0	0.0	
B-A	0.79	7.58	0.104		0.0	0.1	1.7	
C-A	5.31							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR) (M)	TO RIGHT (M)			
B-C	0.102	0.000			0.009			
B-A	0.076	0.003	0.018	0.005	0.008			
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.49	0.000		0.0	0.0	0.0	
B-A	0.94	7.48	0.126		0.1	0.1	2.1	
C-A	6.34							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.102	0.000					0.009	
B-A	0.075	0.004	0.018	0.005			0.007	
C-B	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.45-09.00								
B-C	0.00	8.42	0.000		0.0	0.0	0.0	
B-A	1.15	7.35	0.157		0.2	0.2	2.8	
C-A	7.77							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.101	0.000			0.009			
B-A	0.074	0.005	0.018	0.005	0.007			
C-B	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
09.15-09.30								
B-C	0.00	8.54	0.000		0.0	0.0	0.0	
B-A	0.79	7.58	0.104		0.1	0.1	1.8	
C-A	5.31							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.102	0.000				0.009		
B-A	0.076	0.003	0.018	0.005		0.008		
C-B	0.130	0.000		0.010				

QUEUE FOR STREAM C-B	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

I	I	TOTAL DEMAND		* QUEUEING *		* INCLUSIVE QUEUEING *	
		I	I	* DELAY *	I	* DELAY *	I
I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	I	0.0	I	0.00
I	B-A	I	86.4	I	57.6	I	13.2
I	C-A	I	582.8	I	388.5	I	0.15
I	C-B	I	0.0	I	0.0	I	0.00
I	A-B	I	0.0	I	0.0	I	0.00
I	A-C	I	0.0	I	0.0	I	0.00
I	ALL	I	669.2	I	446.1	I	13.2
I		I		I		I	0.02

END OF JOB

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.43	0.000	0.0	0.0	0.0		I
I	B-A	1.12	7.38	0.151	0.1	0.2	2.6		I
I	C-A	7.40							I
I	C-B	0.00	10.89	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
			(.1M)	(.1M)	(M)	(M)			
I	B-C	0.101	0.000			0.009			I
I	B-A	0.074	0.005	0.018	0.005	0.007			I
I	C-B	0.130	0.000		0.010				I

I	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I	08.45-09.00									I
I	B-C	0.00	8.43	0.000		0.0	0.0	0.0		I
I	B-A	1.12	7.38	0.151		0.2	0.2	2.6		I
I	C-A	7.40								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I	B-C	0.101	0.000					0.009		I
I	B-A	0.074	0.005	0.018	0.005			0.007		I
I	C-B	0.130	0.000		0.010					I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)		
I	09.00-09.15									I	
I	B-C	0.00	8.50	0.000		0.0	0.0	0.0		I	
I	B-A	0.91	7.51	0.121		0.2	0.1	2.1		I	
I	C-A	6.05								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.102	0.000					0.009		I	
I	B-A	0.075	0.004	0.018	0.005			0.008		I	
I	C-B	0.130	0.000		0.010					I	

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH_MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH_MIN/ TIME SEGMENT)		
I	09.15-09.30									I	
I	B-C	0.00	8.54	0.000		0.0	0.0	0.0		I	
I	B-A	0.76	7.60	0.100		0.1	0.1	1.7		I	
I	C-A	5.06								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.102	0.000					0.009		I	
I	B-A	0.076	0.003	0.018	0.005	0.008				I	
I	C-B	0.130	0.000		0.010					I	

QUEUE FOR STREAM B-C

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.1
08.30	0.1
08.45	0.2
09.00	0.2
09.15	0.1
09.30	0.1

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING	*	I	* INCLUSIVE	QUEUEING	*	I
I	I	I	I	I	* DELAY *	I	I	* DELAY *	I	I	I
I	I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	I	(MIN)	(MIN/VEH)	I
I	B-C	I	0.0	I 0.0	I	0.0	I 0.00	I	0.0	I 0.00	I
I	B-A	I	83.6	I 55.8	I	12.7	I 0.15	I	12.7	I 0.15	I
I	C-A	I	555.3	I 370.2	I		I	I		I	I
I	C-B	I	0.0	I 0.0	I	0.0	I 0.00	I	0.0	I 0.00	I
I	A-B	I	0.0	I 0.0	I		I	I		I	I
I	A-C	I	0.0	I 0.0	I		I	I		I	I
I	ALL	I	639.0	I 426.0	I	12.7	I 0.02	I	12.7	I 0.02	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Chi Kiang St / Sung On St

Design Year: 2016

Description: Existing

Designed By: SEL

Checked By: SEL

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)		Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right			AM	PM	AM	PM	Flow (pcu/hr)	Y Value	Critical Y	Flow (pcu/hr)	Y Value	Critical Y
Sung On St NB	↑	C	2	3,300							1945	1945	143	0.074	0.074	185	0.095	0.095
Chi Kiang St EB	↑	A	1	3,300	15						1770	1770	200	0.113	0.113	155	0.088	
	↑	A	1	3,500		12					1870	1870	191	0.102		222	0.119	0.119
Shun On St SB	↑	B	2	3,500							1965	1965	106	0.054		99	0.050	
	↑	B	2	3,500							2105	2105	114	0.054		106	0.050	

Pedestrian Crossing
 Dp 3 MIN GREEN + FLASH = 8 + 9 = 17
 Ep 3 MIN GREEN + FLASH = 8 + 8 = 16
 Fp 3 MIN GREEN + FLASH = 8 + 8 = 16

Notes:	Flow: (pcu/hr)	→	←
--------	----------------	---	---

 200(165) 191(222) 220(205) 143(186)							
y	0.167	0.187	y	0.169	0.214		
	L (sec)	34		34	L (sec)	34	34
	C (sec)	90		90	C (sec)	90	90
	y pract.	0.560		0.560	y pract.	0.560	0.560
	R.C. (%)	235%		200%	R.C. (%)	231%	162%

Stage / Phase Diagrams

1.		2.		3.		4.		5.	
----	--	----	--	----	--	----	--	----	--

I/G= 3	I/G= 6	I/G= 11	16	I/G=	I/G=
I/G= 3	I/G= 6	I/G= 11	16	I/G=	I/G=
				Date: MAY, 2016	Junction: Chi Kiang St / Sung On St

TRAFFIC SIGNALS CALCULATION

Job No.: **CHK50143110**

MVA HONG KONG LIMITED

Junction: **Chi Kiang St/ Sung On St**

Design Year: **2016**

Description: **Year 2028 Reference**

Designed By: **SEL**





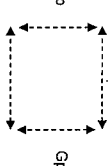
Checked By: **SEL**

Approach	Movements	Phase	Stage	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
				Width (m)			AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Chi Kiang St WB	↑	D	4	4.000	12	12	8% / 92%	7% / 93%	1790	1790	65	0.036	0.036	75	0.042	0.042
Sung On St NB	↑	C	3	3.500					1965	1965	130	0.066	0.066	162	0.082	0.082
	↑	C	3	4.000		25			2035	2035	5	0.002		5	0.002	
Chi Kiang St EB	↑	A	1	4.500	15		79%	69%	1915	1930	285	0.149	0.149	245	0.127	0.127
	↑	A	1	5.000		12			2005	2005	155	0.077		175	0.087	
Sung On St SB	↑	B	2	3.000	6		67%	67%	1640	1640	112	0.068	0.068	105	0.064	0.064
	↑	B	2	3.000					2055	2055	140	0.068		131	0.064	

Pedestrian Crossing

Ep 5 MIN GREEN + FLASH = 8 + 8 = 16
 Fp 5 MIN GREEN + FLASH = 8 + 8 = 16
 Gp 5 MIN GREEN + FLASH = 8 + 8 = 16
 Hp 5 MIN GREEN + FLASH = 8 + 9 = 17

Notes:	Flow: (pcu/hr)		Group	A,B,C,D,Fp	Group	A,B,C,D,Fp
			y		y	
			L (sec) C (sec) y pract. R.C. (%)	0.320 44 130 0.595 86%	L (sec) C (sec) y pract. R.C. (%)	0.315 44 130 0.595 89%

Stage / Phase Diagrams									
1.		2.		3.		4.		5.	
									
I/G=3		I/G=6		I/G=6		I/G=6		I/G=11	16
I/G=3		I/G=6		I/G=6		I/G=6		I/G=11	16
Date: MAY, 2016					Junction: Chi Kiang St/ Sung On St				

TRAFFIC SIGNALS CALCULATION

Job No.: **CHKS0143110**

MVA HONG KONG LIMITED

Junction: **Chi Kiang St / Sung On St**

Design Year: **2028**

Description: **Year 2028 with Development**

Designed By: **SEL**

Checked By: **SEL**

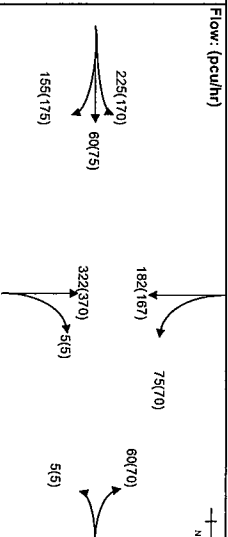
Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	Y Value	Critical Y	Flow (pcu/hr)	Y Value	Critical Y
Chi Kiang St WB	↑	D	4	4.000	12	12	8% / 92%	7% / 93%		1790	1790	65	0.036	0.036	75	0.042	0.042
Sung On St NB	↑	C	3	3.500						1965	1965	322	0.164	0.164	370	0.188	0.188
	↑	C	3	4.000		25				2035	2035	5	0.002		5	0.002	
Chi Kiang St EB	↑	A	1	4.500	15		79%	69%		1915	1930	285	0.149	0.149	245	0.127	0.127
	↑	A	1	5.000		12				2005	2005	155	0.077		175	0.087	
Sung On St SB	↑	B	2	3.000	6		66%	67%		1645	1640	114	0.069		105	0.064	
	↑	B	2	3.000						2055	2055	143	0.070	0.070	132	0.064	0.064

Pedestrian Crossing

Ep 5 MIN GREEN + FLASH = 8 + 8 = 16
 Fp 5 MIN GREEN + FLASH = 8 + 8 = 16
 Gp 5 MIN GREEN + FLASH = 8 + 8 = 16
 Hp 5 MIN GREEN + FLASH = 8 + 9 = 17

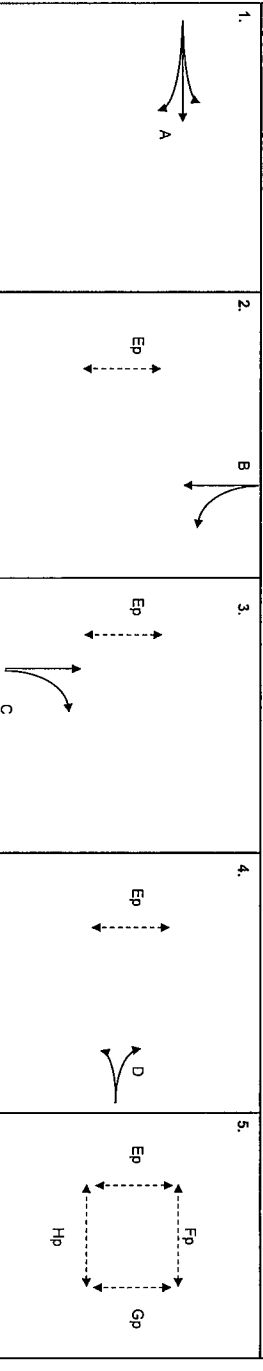
Notes:

Flow: (pcu/hr)



Group		A,B,C,D,Fp	Group		A,B,C,D,Fp
Y			Y		
L (sec)	0.419		L (sec)		0.421
C (sec)	44		C (sec)		44
Y pract.	130		Y pract.		130
R.C. (%)	0.595		R.C. (%)		0.595
	42%				41%

Stage / Phase Diagrams



I/G= 3	I/G= 6	I/G= 6	I/G= 6	I/G= 11	16
I/G= 3	I/G= 6	I/G= 6	I/G= 6	I/G= 11	16
Date: MAY, 2016			Junction: Chi Kiang St / Sung On St		

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Chi Kiang St / Sung On StDesign Year: 2028Description: Year 2028 Design ScenarioDesigned By: SELChecked By: SEL

Approach	Movements	Phase	Stage	Radius (m)		Gradient (%)		Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
				Width (m)		Left	Right	AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Chi Kiang St WB	↑	D	4	4.000	12	12		8% / 92%	7% / 93%	1790	1790	65	0.036	0.036	75	0.042	0.042
Sung On St NB	↑	C	3	3.500						1965	1965	198	0.101	0.101	230	0.117	0.117
	↑	C	3	4.000	25					2035	2035	5	0.002		5	0.002	
Chi Kiang St EB	↑	A	1	4.500	15			82%	73%	1910	1925	334	0.175	0.175	277	0.144	0.144
	↑	A	1	5.000	12					2005	2005	155	0.077		175	0.087	
Sung On St SB	↑	B	2	3.000	6			66%	67%	1645	1640	114	0.069		105	0.064	
	↑	B	2	3.000						2055	2055	143	0.070	0.070	132	0.064	0.064

Pedestrian Crossing	Ep	5	MIN GREEN + FLASH =	8	+	8	=	16
	Fp	5	MIN GREEN + FLASH =	8	+	8	=	16
	Gp	5	MIN GREEN + FLASH =	8	+	8	=	16
	Hp	5	MIN GREEN + FLASH =	8	+	9	=	17

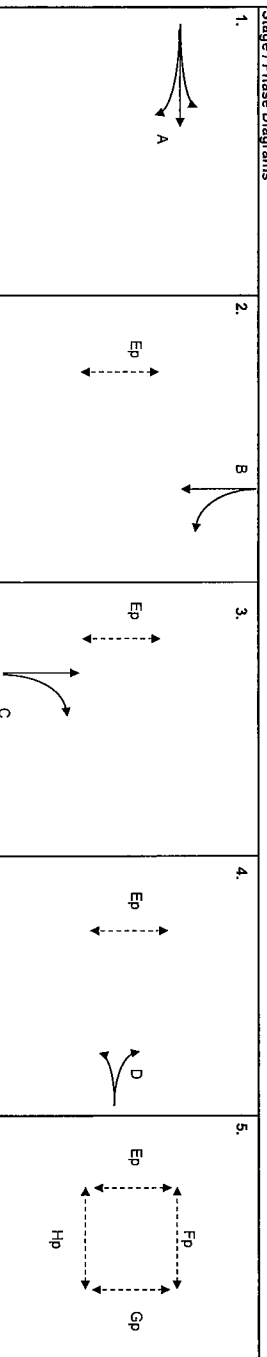
Notes:

Flow: (pcu/hr)



Group		ABCD/Fp	Group		ABCD/Fp
y			y		
L (sec)	0.382		L (sec)		0.367
C (sec)	44		C (sec)		44
y pract.	130		y pract.		130
R.C. (%)	0.595		R.C. (%)		0.595
	56%				62%

Stage / Phase Diagrams



I/G = 3	I/G = 6	I/G = 6	I/G = 6	I/G = 6	I/G = 11	16
I/G = 3	I/G = 6	I/G = 6	I/G = 6	I/G = 6	I/G = 11	16
Date: <u>MAY, 2016</u>			Junction: <u>Chi Kiang St / Sung On St</u>			

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RUN TITLE
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X2 Obs AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

```

INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM			NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	MINUTES FROM FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	RATE OF FLOW (VEH/MIN) AT TOP OF PEAK	RATE OF FLOW (VEH/MIN) AFTER PEAK
ARM A			15.00	45.00	75.00	0.00	0.00	0.00
ARM B			15.00	45.00	75.00	0.29	0.43	0.29
ARM C			15.00	45.00	75.00	1.38	2.06	1.38

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME		FROM/TO	ARM A	ARM B	ARM C		
08.00 - 09.30	ARM A	0.000	0.000	0.000			
		1.000	0.000	0.000			
		(0.0)	(0.0)	(0.0)			
	ARM B	1.000	0.000	0.000			
		23.0	0.0	0.0			
		(0.0)	(0.0)	(0.0)			
	ARM C	1.000	0.000	0.000			
		110.0	0.0	0.0			
		(0.0)	(0.0)	(0.0)			

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.61	0.000		0.0	0.0	0.0	
B-A	0.29	7.27	0.040		0.0	0.0	0.6	
C-A	1.38							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.103	0.000				0.010		
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.59	0.000		0.0	0.0	0.0	
B-A	0.34	7.24	0.047		0.0	0.0	0.7	
C-A	1.64							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.103	0.000				0.010		
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
---	------	--------	----------	---------	------------	-------	-----	-------	------------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.57	0.000	0.0	0.0	0.0		I
I	B-A	0.42	7.21	0.058	0.0	0.1	0.9		I
I	C-A	2.01							I
I	C-B	0.00	8.70	0.000	0.0	0.0	0.0		I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			I
I	B-C	0.103	0.000				0.010		I
I	B-A	0.078	0.001	0.017	0.005	0.007			I
I	C-B	0.104	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
	08.45-09.00								
	B-C	0.00	8.57	0.000		0.0	0.0	0.0	
	B-A	0.42	7.21	0.058		0.1	0.1	0.9	
	C-A	2.01							
	C-B	0.00	8.70	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-C	0.103	0.000			0.010			
	B-A	0.078	0.001	0.017	0.005	0.007			
	C-B	0.104	0.000		0.010				

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
	09.00-09.15								
	B-C	0.00	8.59	0.000		0.0	0.0	0.0	
	B-A	0.34	7.24	0.047		0.1	0.1	0.8	
	C-A	1.64							
	C-B	0.00	8.70	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-C	0.103	0.000				0.010		
	B-A	0.078	0.001	0.017	0.005	0.007			
	C-B	0.104	0.000		0.010				

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
	09.15-09.30								
	B-C	0.00	8.61	0.000		0.0	0.0	0.0	
	B-A	0.29	7.27	0.040		0.1	0.0	0.6	
	C-A	1.38							
	C-B	0.00	8.70	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-C	0.103	0.000				0.010		
	B-A	0.078	0.001	0.017	0.005	0.007			
	C-B	0.104	0.000		0.010				

QUEUE FOR STREAM	B-C
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM	B-A
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.0

QUEUE FOR STREAM	C-B
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	I	(MIN)	I	(MIN)	I
I	I	I	(VEH/H)	I	(MIN/VEH)	I	(MIN/VEH)	I
I	B-C	I	0.0	I	0.0	I	0.0	I
I	B-A	I	31.5	I	21.0	I	4.5	I
I	C-A	I	150.8	I	100.6	I		I
I	C-B	I	0.0	I	0.0	I	0.0	I
I	A-B	I	0.0	I	0.0	I		I
I	A-C	I	0.0	I		I		I
I	ALL	I	182.4	I	121.6	I	4.5	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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RUN TITLE
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X2 Obs PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I

MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF	MINUTES FROM START WHEN	I	RATE	OF FLOW (VEH/MIN)	I
I	ARM	I FLOW STARTS	I TOP OF PEAK	I FLOW STOPS	I BEFORE	I AT TOP	I AFTER
I	I	I TO RISE	I IS REACHED	I FALLING	I PEAK	I OF PEAK	I PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I 0.00	I 0.00	I 0.00
I	ARM B	I 15.00	I 45.00	I 75.00	I 0.56	I 0.84	I 0.56
I	ARM C	I 15.00	I 45.00	I 75.00	I 1.58	I 2.36	I 1.58

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME		FROM/TO	ARM A	ARM B	ARM C		
17.00 - 18.30							
	ARM A	0.000	0.000	0.000			
		1.000	1.000	1.000			
		(0.0)	(0.0)	(0.0)			
	ARM B	1.000	0.000	0.000			
		45.0	0.0	0.0			
		(0.0)	(0.0)	(0.0)			
	ARM C	1.000	0.000	0.000			
		126.0	0.0	0.0			
		(0.0)	(0.0)	(0.0)			

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
17.00-17.15								
B-C	0.00	8.53	0.000		0.0	0.0	0.0	
B-A	0.56	7.25	0.078		0.0	0.1	1.2	
C-A	1.58							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR)		VISIBILITY TO RIGHT		
					(M)		(M)	
B-C	0.102	0.000					0.009	
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.15-17.30								
B-C	0.00	8.49	0.000		0.0	0.0	0.0	
B-A	0.67	7.22	0.093		0.1	0.1	1.5	
C-A	1.88							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.102	0.000				0.009		
B-A	0.078	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
18.15-18.30								
B-C	0.00	8.53	0.000		0.0	0.0	0.0	
B-A	0.56	7.25	0.078		0.1	0.1	1.3	
C-A	1.58							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.102	0.000			0.009			
B-A	0.078	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

I	I	I	TOTAL DEMAND		* QUEUEING *		* INCLUSIVE QUEUEING *	
			I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	0.0	0.0	0.00	I	0.00
I	B-A	I	61.7	41.1	9.4	0.15	I	0.15
I	C-A	I	172.8	115.2	I	I	I	I
I	C-B	I	0.0	0.0	0.0	0.00	I	0.00
I	A-B	I	0.0	0.0	I	I	I	I
I	A-C	I	0.0	0.0	I	I	I	I
I	ALL	I	234.5	156.3	9.4	0.04	I	0.04

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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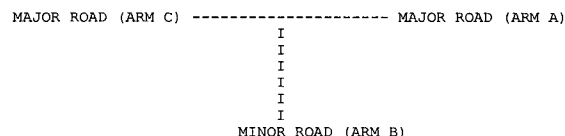
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RUN TITLE
*****
X2 Ref AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA



ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	I	NUMBER OF MINUTES FROM START WHEN			I	RATE OF FLOW (VEH/MIN)				I			
I	ARM	I	FLOW STARTS	I	TOP OF PEAK	I	FLOW STOPS	I	BEFORE	I	AT	I	AFTER	I
I	I	I	TO RISE	I	IS REACHED	I	FALLING	I	PEAK	I	OF	I	PEAK	I
I	ARM A	I	15.00	I	45.00	I	75.00	I	0.00	I	0.00	I	0.00	I
I	ARM B	I	15.00	I	45.00	I	75.00	I	0.38	I	0.56	I	0.38	I
I	ARM C	I	15.00	I	45.00	I	75.00	I	1.56	I	2.34	I	1.56	I

TIME		FROM/TO	ARM A	ARM B	ARM C
08.00 - 09.30					
	ARM A	0.000	0.000	0.000	
		(0.0)	(0.0)	(0.0)	
	ARM B	1.000	0.000	0.000	
		30.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	
	ARM C	1.000	0.000	0.000	
		125.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.58	0.000		0.0	0.0	0.0	
B-A	0.38	7.25	0.052		0.0	0.1	0.8	
C-A	1.56							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.103	0.000					0.010	
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.56	0.000		0.0	0.0	0.0	
B-A	0.45	7.22	0.062		0.1	0.1	1.0	
C-A	1.87							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.102	0.000				0.009		
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------	-------

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.45-09.00								
B-C	0.00	8.53	0.000		0.0	0.0	0.0	
B-A	0.55	7.18	0.076		0.1	0.1	1.2	
C-A	2.29							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.102	0.000			0.009			
B-A	0.077	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
09.00-09.15								
B-C	0.00	8.56	0.000		0.0	0.0	0.0	
B-A	0.45	7.22	0.062		0.1	0.1	1.0	
C-A	1.87							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(.1M)	(M)	(M)		
B-C	0.102	0.000				0.009		
B-A	0.078	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
09.15-09.30								
B-C	0.00	8.58	0.000		0.0	0.0	0.0	
B-A	0.38	7.25	0.052		0.1	0.1	0.8	
C-A	1.56							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.103	0.000			0.010			
B-A	0.078	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

QUEUE FOR STREAM B-A	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

QUEUE FOR STREAM C-B	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	I	I	TOTAL DEMAND		* QUEUEING * * DELAY *		* INCLUSIVE QUEUEING * * DELAY *		I
			(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)	
I	B-C	I	0.0	0.0	0.0	0.00	0.0	0.00	I
I	B-A	I	41.1	27.4	6.0	0.15	6.0	0.15	I
I	C-A	I	171.4	114.3	I	I	I	I	I
I	C-B	I	0.0	0.0	0.0	0.00	0.0	0.00	I
I	A-B	I	0.0	0.0	I	I	I	I	I
I	A-C	I	0.0	0.0	I	I	I	I	I
I	ALL	I	212.5	141.7	6.0	0.03	6.0	0.03	I

- * DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
- * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
- * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD

END OF JOB

TRANSPORT RESEARCH LABORATORY

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

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RUN TITLE

X2_Ref_PM

.MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

```
MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)
                        I
                        I
                        I
                        I
                        I
                        I
MINOR ROAD (ARM B)
```

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

DATA ITEM	MINOR ROAD B
TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	(W) 11.40 M.
CENTRAL RESERVE WIDTH	(WCR) 0.00 M.
MAJOR ROAD RIGHT TURN - WIDTH	(WC-B) 2.20 M.
- VISIBILITY	(VC-B) 0.0 M.
- BLOCKS TRAFFIC	NO
MINOR ROAD - VISIBILITY TO LEFT	(VB-C) 0.0 M.
- VISIBILITY TO RIGHT	(VB-A) 0.0 M.
- LANE 1 WIDTH	(WB-C) 2.20 M.
- LANE 2 WIDTH	(WB-A) 2.30 M.

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF MINUTES FROM START WHEN FLOW STARTS	TOP OF PEAK IS REACHED	FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	AT TOP OF PEAK	AFTER PEAK
ARM A	15.00	45.00	75.00	0.00	0.00	0.00
ARM B	15.00	45.00	75.00	0.63	0.94	0.63
ARM C	15.00	45.00	75.00	1.75	2.63	1.75

TIME	FROM/TO	ARM A	ARM B	ARM C
17.00 - 18.30	ARM A	0.000	0.000	0.000
	ARM B	1.000	0.000	0.000
	ARM C	1.000	0.000	0.000

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.00-17.15								
B-C	0.00	8.51	0.000		0.0	0.0	0.0	
B-A	0.63	7.23	0.086		0.0	0.1	1.3	
C-A	1.75							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							

MARGINAL CHANGE:	LANE WIDTH (.1M)	EFFECT ON CAPACITY (PCU/MIN) MAJOR RD. WIDTH (.1M)	OF MARGINAL CHANGES IN: CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR) (M)	TO RIGHT (M)
B-C	0.102	0.000			0.009
B-A	0.078	0.001	0.017	0.005	0.007
C-B	0.104	0.000		0.010	

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.15-17.30								
B-C	0.00	8.47	0.000		0.0	0.0	0.0	
B-A	0.75	7.20	0.104		0.1	0.1	1.7	
C-A	2.09							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							

MARGINAL CHANGE:	LANE WIDTH (.1M)	EFFECT ON CAPACITY (PCU/MIN) MAJOR RD. WIDTH (.1M)	OF MARGINAL CHANGES IN: CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR) (M)	TO RIGHT (M)
B-C	0.101	0.000			0.009
B-A	0.077	0.001	0.017	0.005	0.007
C-B	0.104	0.000		0.010	

TIME	DEMAND	CAPACITY	DEMAND/CAPACITY	PEDESTRIAN FLOW	START QUEUE	END QUEUE	DELAY	GEOMETRIC DELAY
------	--------	----------	-----------------	-----------------	-------------	-----------	-------	-----------------

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
17.45-18.00								
B-C	0.00	8.42	0.000		0.0	0.0	0.0	
B-A	0.91	7.15	0.128		0.1	0.1	2.2	
C-A	2.56							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.101	0.000			0.009			
B-A	0.077	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
18.00-18.15								
B-C	0.00	8.47	0.000		0.0	0.0	0.0	
B-A	0.75	7.20	0.104		0.1	0.1	1.8	
C-A	2.09							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.101	0.000			0.009			
B-A	0.077	0.001	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH_MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH_MIN/ TIME SEGMENT)
18.15-18.30								
B-C	0.00	8.51	0.000		0.0	0.0	0.0	
B-A	0.63	7.23	0.086		0.1	0.1	1.5	
C-A	1.75							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
B-C	0.102	0.000				0.009		
B-A	0.078	0.001	0.017	0.005		0.007		
C-B	0.104	0.000		0.010				

QUEUE FOR STREAM B-C	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.1
17.30	0.1
17.45	0.1
18.00	0.1
18.15	0.1
18.30	0.1

QUEUE FOR STREAM C-B	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	0.0	0.0	0.00	0.0	0.00
I	B-A	I	68.6	45.7	10.6	0.15	10.6	0.15
I	C-A	I	192.0	128.0	I	I	I	I
I	C-B	I	0.0	0.0	0.0	0.00	0.0	0.00
I	A-B	I	0.0	0.0	I	I	I	I
I	A-C	I	0.0	0.0	I	I	I	I
I	ALL	I	260.5	173.7	10.6	0.04	10.6	0.04

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD

END OF JOB

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RUN TITLE
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X2_RefDev_AM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF	MINUTES FROM START WHEN	I	RATE	OF FLOW (VEH/MIN)	I
I	ARM	FLOW STARTS	I TOP OF PEAK	I FLOW STOPS	I BEFORE	I AT TOP	I AFTER
I	I	TO RISE	I IS REACHED	I FALLING	I PEAK	I OF PEAK	I PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I 0.00	I 0.00	I 0.00
I	ARM B	I 15.00	I 45.00	I 75.00	I 0.70	I 1.05	I 0.70
I	ARM C	I 15.00	I 45.00	I 75.00	I 3.40	I 5.10	I 3.40

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF H.V.S)			
TIME		FROM/TO	ARM A	ARM B	ARM C
08.00 - 09.30	ARM A	1 0.000	1 0.000	1 0.000	1
		1 0.000	1 0.000	1 0.000	1
		1 (0.0)	1 (0.0)	1 (0.0)	1
	ARM B	1 1.000	1 0.000	1 0.000	1
		1 56.0	1 0.0	1 0.0	1
		1 (0.0)	1 (0.0)	1 (0.0)	1
	ARM C	1 1.000	1 0.000	1 0.000	1
		1 272.0	1 0.0	1 0.0	1
		1 (0.0)	1 (0.0)	1 (0.0)	1

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA.

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.48	0.000		0.0	0.0	0.0	
B-A	0.70	7.07	0.099		0.0	0.1	1.6	
C-A	3.40							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY	
MARGINAL	LANE WIDTH		WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT	
CHANGE:	(.1M)		(.1M)	(.1M)	(M)		(M)	
B-C	0.101		0.000				0.009	
B-A	0.076		0.002	0.017	0.005		0.007	
C-B	0.104		0.000		0.010			

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.43	0.000		0.0	0.0	0.0	
B-A	0.84	7.01	0.119		0.1	0.1	2.0	
C-A	4.06							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR) (M)	VISIBILITY TO RIGHT (M)			
B-C	0.101	0.000					0.009	
B-A	0.075	0.002	0.017	0.005			0.007	
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------	-------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I	08.30-08.45								I
I	B-C	0.00	8.37	0.000		0.0	0.0	0.0	I
I	B-A	1.02	6.92	0.148		0.1	0.2	2.5	I
I	C-A	4.97							I
I	C-B	0.00	8.70	0.000		0.0	0.0	0.0	I
I	A-B	0.00							I
I	A-C	0.00							I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			I
I	B-C	0.100	0.000					0.009	I
I	B-A	0.074	0.003	0.017	0.005	0.007			I
I	C-B	0.104	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
	08.45-09.00								
	B-C	0.00	8.37	0.000		0.0	0.0	0.0	
	B-A	1.02	6.92	0.148		0.2	0.2	2.6	
	C-A	4.97							
	C-B	0.00	8.70	0.000		0.0	0.0	0.0	
	A-B	0.00							
	A-C	0.00							
	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR) (M)	TO RIGHT (M)			
	B-C	0.100	0.000					0.009	
	B-A	0.074	0.003	0.017	0.005	0.007			
	C-B	0.104	0.000		0.010				

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)		
I	09.00-09.15									I	
I	B-C	0.00	8.43	0.000		0.0	0.0	0.0		I	
I	B-A	0.84	7.01	0.119		0.2	0.1	2.1		I	
I	C-A	4.06								I	
I	C-B	0.00	8.70	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.101	0.000					0.009		I	
I	B-A	0.075	0.002	0.017	0.005	0.007				I	
I	C-B	0.104	0.000		0.010					I	

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)		
I	09.15-09.30									I	
I	B-C	0.00	8.48	0.000		0.0	0.0	0.0		I	
I	B-A	0.70	7.07	0.099		0.1	0.1	1.7		I	
I	C-A	3.40								I	
I	C-B	0.00	8.70	0.000		0.0	0.0	0.0		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I	
I	B-C	0.101	0.000					0.009		I	
I	B-A	0.076	0.002	0.017	0.005	0.007				I	
I	C-B	0.104	0.000		0.010					I	

QUEUE FOR STREAM B-C

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-A

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.1
08.30	0.1
08.45	0.2
09.00	0.2
09.15	0.1
09.30	0.1

QUEUE FOR STREAM C-B

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	I	I	I	I	I	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-C	I	0.0	I 0.0	I 0.0	I 0.00	I 0.0	I 0.00
I	B-A	I	76.8	I 51.2	I 12.4	I 0.16	I 12.4	I 0.16
I	C-A	I	373.0	I 248.6	I	I	I	I
I	C-B	I	0.0	I 0.0	I 0.0	I 0.00	I 0.0	I 0.00
I	A-B	I	0.0	I 0.0	I	I	I	I
I	A-C	I	0.0	I 0.0	I	I	I	I
I	ALL	I	449.8	I 299.8	I 12.4	I 0.03	I 12.4	I 0.03

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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RUN TITLE
*****
X2 RefDev PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM			NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	MINUTES FROM START WHEN FLOW STOPS FALLING	RATE OF FLOW (VEH/MIN) BEFORE PEAK	RATE OF FLOW (VEH/MIN) AT TOP OF PEAK	RATE OF FLOW (VEH/MIN) AFTER PEAK
ARM A			15.00	45.00	75.00	0.00	0.00	0.00
ARM B			15.00	45.00	75.00	0.84	1.26	0.84
ARM C			15.00	45.00	75.00	3.72	5.59	3.72

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF H.V.S)			
TIME	FROM/TO	ARM A	ARM B	ARM C	
17.00 - 18.30	ARM A	0.000	0.000	0.000	
		1.000	1.000	1.000	
		(0.0)	(0.0)	(0.0)	
	ARM B	1.000	0.000	0.000	
		67.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	
	ARM C	1.000	0.000	0.000	
		298.0	0.0	0.0	
		(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
17.00-17.15								
B-C	0.00	8.44	0.000		0.0	0.0	0.0	
B-A	0.84	7.04	0.119		0.0	0.1	1.9	
C-A	3.72							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.101	0.000			0.009			
B-A	0.076	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.15-17.30								
B-C	0.00	8.38	0.000		0.0	0.0	0.0	
B-A	1.00	6.97	0.144		0.1	0.2	2.4	
C-A	4.45							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR) (M)				
				VISIBILITY TO RIGHT (M)				
B-C	0.100	0.000					0.009	
B-A	0.075	0.002	0.017		0.005		0.007	
C-B	0.104	0.000			0.010			

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------	-------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I 17.30-17.45									I
I B-C	0.00	8.31	0.000		0.0	0.0	0.0		I
I B-A	1.22	6.87	0.178		0.2	0.2	3.1		I
I C-A	5.45								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				
B-C	0.099	0.000				0.009			
B-A	0.074	0.003	0.017	0.005	0.007				
C-B	0.104	0.000		0.010					

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 17.45-18.00									I
I B-C	0.00	8.30	0.000		0.0	0.0	0.0		I
I B-A	1.22	6.87	0.178		0.2	0.2	3.2		I
I C-A	5.45								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				
B-C	0.099	0.000				0.009			
B-A	0.074	0.003	0.017	0.005	0.007				
C-B	0.104	0.000		0.010					

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 18.00-18.15									I
I B-C	0.00	8.38	0.000		0.0	0.0	0.0		I
I B-A	1.00	6.97	0.144		0.2	0.2	2.6		I
I C-A	4.45								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				
B-C	0.100	0.000				0.009			
B-A	0.075	0.002	0.017	0.005	0.007				
C-B	0.104	0.000		0.010					

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 18.15-18.30									I
I B-C	0.00	8.43	0.000		0.0	0.0	0.0		I
I B-A	0.84	7.04	0.119		0.2	0.1	2.1		I
I C-A	3.72								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
		MAJOR RD.		CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				
B-C	0.101	0.000				0.009			
B-A	0.076	0.002	0.017	0.005	0.007				
C-B	0.104	0.000		0.010					

QUEUE FOR STREAM	B-C
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUE FOR STREAM	B-A
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.1
17.30	0.2
17.45	0.2
18.00	0.2
18.15	0.2
18.30	0.1

QUEUE FOR STREAM	C-B
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I B-C	I	0.0	0.0	0.0	0.00	0.0	0.00
I B-A	I	91.9	61.2	15.4	0.17	15.4	0.17
I C-A	I	408.6	272.4				
I C-B	I	0.0	0.0	0.0	0.00	0.0	0.00
I A-B	I	0.0	0.0				
I A-C	I	0.0	0.0				
I ALL	I	500.5	333.7	15.4	0.03	15.4	0.03

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .

* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.

* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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RUN TITLE
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X2 Des AM

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MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I (W)	11.40 M.	I
I	CENTRAL RESERVE WIDTH	I (WCR)	0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I (WC-B)	2.20 M.	I
I	- VISIBILITY	I (VC-B)	0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I (VB-C)	0.0 M.	I
I	- VISIBILITY TO RIGHT	I (VB-A)	0.0 M.	I
I	- LANE 1 WIDTH	I (WB-C)	2.20 M.	I
I	- LANE 2 WIDTH	I (WB-A)	2.30 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM			NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	WHEN FLOW STOPS FALLING	RATE BEFORE PEAK	OF FLOW AT PEAK	(VEH/MIN) AFTER PEAK
ARM A			15.00	45.00	75.00	0.00	0.00	0.00
ARM B			15.00	45.00	75.00	0.38	0.56	0.38
ARM C			15.00	45.00	75.00	3.19	4.78	3.19

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME	FROM/TO	ARM A	ARM B	ARM C			
08.00 - 09.30	ARM A	0.000	0.000	0.000			
		I???????	I???????	I???????			
		I (0.0)	I (0.0)	I (0.0)			
		I	I	I			
	ARM B	1.000	0.000	0.000			
		I 30.0	I 0.0	I 0.0			
		I (0.0)	I (0.0)	I (0.0)			
		I	I	I			
	ARM C	1.000	0.000	0.000			
		I 255.0	I 0.0	I 0.0			
		I (0.0)	I (0.0)	I (0.0)			
		I	I	I			

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-C	0.00	8.58	0.000		0.0	0.0	0.0	
B-A	0.38	7.09	0.053		0.0	0.1	0.8	
C-A	3.19							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.103	0.000			0.010			
B-A	0.076	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.15-08.30								
B-C	0.00	8.56	0.000		0.0	0.0	0.0	
B-A	0.45	7.03	0.064		0.1	0.1	1.0	
C-A	3.81							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.102	0.000			0.009			
B-A	0.076	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC	DELAYI
---	------	--------	----------	---------	------------	-------	-----	-------	-----------	--------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I 08.30-08.45									I
I B-C	0.00	8.52	0.000		0.0	0.0	0.0		I
I B-A	0.55	6.95	0.079		0.1	0.1	1.2		I
I C-A	4.66								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
I MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			I
I B-C	0.102	0.000				0.009			I
I B-A	0.075	0.003	0.017	0.005	0.007				I
I C-B	0.104	0.000		0.010					I

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 08.45-09.00									I
I B-C	0.00	8.52	0.000		0.0	0.0	0.0		I
I B-A	0.55	6.95	0.079		0.1	0.1	1.3		I
I C-A	4.66								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
I MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			I
I B-C	0.102	0.000				0.009			I
I B-A	0.075	0.003	0.017	0.005	0.007				I
I C-B	0.104	0.000		0.010					I

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 09.00-09.15									I
I B-C	0.00	8.56	0.000		0.0	0.0	0.0		I
I B-A	0.45	7.03	0.064		0.1	0.1	1.1		I
I C-A	3.81								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
I MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			I
I B-C	0.102	0.000				0.009			I
I B-A	0.076	0.002	0.017	0.005	0.007				I
I C-B	0.104	0.000		0.010					I

I TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I 09.15-09.30									I
I B-C	0.00	8.58	0.000		0.0	0.0	0.0		I
I B-A	0.38	7.09	0.053		0.1	0.1	0.9		I
I C-A	3.19								I
I C-B	0.00	8.70	0.000		0.0	0.0	0.0		I
I A-B	0.00								I
I A-C	0.00								I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
I MARGINAL	LANE WIDTH	WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)	(M)			I
I B-C	0.103	0.000				0.010			I
I B-A	0.076	0.002	0.017	0.005	0.007				I
I C-B	0.104	0.000		0.010					I

QUEUE FOR STREAM	B-C
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM	B-A
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

QUEUE FOR STREAM	C-B
TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I B-C	I	0.0	0.0	0.0	0.00	0.0	0.00
I B-A	I	41.1	27.4	6.2	0.15	6.2	0.15
I C-A	I	349.7	233.1	I	I	I	I
I C-B	I	0.0	0.0	0.0	0.00	0.0	0.00
I A-B	I	0.0	0.0	I	I	I	I
I A-C	I	0.0	0.0	I	I	I	I
I ALL	I	390.8	260.5	6.2	0.02	6.2	0.02

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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RUN TITLE
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X2_Des_PM

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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA

MAJOR ROAD (ARM C) ----- MAJOR ROAD (ARM A)

I
I
I
I
I
I
I
MINOR ROAD (ARM B)

ARM A IS Wing Kwong Street NB
ARM B IS Hung Fook Street EB
ARM C IS Wing Kwong Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B
ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.40 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 0.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 0.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.30 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

I	I	NUMBER OF	MINUTES FROM	START WHEN	I	RATE	OF FLOW (VEH/MIN)	I
I	ARM	I FLOW STARTS	I TOP OF PEAK	I FLOW STOPS	I	I BEFORE	I AT	I AFTER
I	I	I TO RISE	I IS REACHED	I FALLING	I	I PEAK	I OF PEAK	I PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I	I 0.00	I 0.00	I 0.00
I	ARM B	I 15.00	I 45.00	I 75.00	I	I 0.63	I 0.94	I 0.63
I	ARM C	I 15.00	I 45.00	I 75.00	I	I 3.46	I 5.19	I 3.46

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME		FROM/TO	ARM A	ARM B	ARM C		
17.00 - 18.30		ARM A	0.000	0.000	0.000		
			1.000	0.000	0.000		
			50.0	0.0	0.0		
			(0.0)	(0.0)	(0.0)		
		ARM B	1.000	0.000	0.000		
			50.0	0.0	0.0		
			(0.0)	(0.0)	(0.0)		
		ARM C	1.000	0.000	0.000		
			277.0	0.0	0.0		
			(0.0)	(0.0)	(0.0)		

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.00-17.15								
B-C	0.00	8.50	0.000		0.0	0.0	0.0	
B-A	0.63	7.07	0.088		0.0	0.1	1.4	
C-A	3.46							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.102	0.000			0.009			
B-A	0.076	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
17.15-17.30								
B-C	0.00	8.46	0.000		0.0	0.0	0.0	
B-A	0.75	7.00	0.107		0.1	0.1	1.7	
C-A	4.13							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			
B-C	0.101	0.000			0.009			
B-A	0.075	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH·MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH·MIN/ TIME SEGMENT)
18.15-18.30								
B-C	0.00	8.50	0.000		0.0	0.0	0.0	
B-A	0.63	7.07	0.088		0.1	0.1	1.5	
C-A	3.46							
C-B	0.00	8.70	0.000		0.0	0.0	0.0	
A-B	0.00							
A-C	0.00							
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-C	0.102	0.000			0.009			
B-A	0.076	0.002	0.017	0.005	0.007			
C-B	0.104	0.000		0.010				

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
* INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
* THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Bailey St / Hung Hom Rd

Design Year: 2016


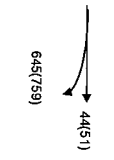
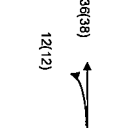
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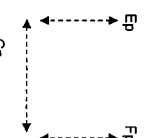

Designed By: SEL

Checked By: SEL

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Bailey St WB	←	D	3	4,000	15			25%	24%	1965	1970	48	0.024	0.024	50	0.025	0.025
Hung Hom Rd	↗	A	1,2	4,000	15					1830	1830	212	0.116		228	0.125	
	↘	A	1,2	3,300	17					1915	1915	221	0.115		239	0.125	
	↗	B	2	2,800		20				1895	1895	16	0.008		17	0.009	
	↘	B	2	2,800						1895	1895						
Bailey St EB	↗	C	1	4,500		23		88%	88%	2085	2085	364	0.175	0.175	428	0.205	0.205
	↘	C	1	4,500		25				1865	1865	325	0.174		382	0.205	

Pedestrian Crossing
 Ep 4 MIN GREEN + FLASH = 10 + 9 = 19
 Fp 4 MIN GREEN + FLASH = 7 + 6 = 13
 Gp 4 MIN GREEN + FLASH = 11 + 10 = 21

Notes:	Flow: (pcu/hr)		Group			Group		
			y	A.DEP	C.B.DEP	y	A.DEP	C.B.DEP
								
								
			L (sec) C (sec) y pract. R.C. (%)			L (sec) C (sec) y pract. R.C. (%)		
			34 130 0.665 374%		46 130 0.582 192%	34 130 0.665 343%		0.231 46 130 0.582 152%









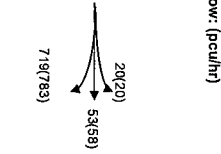
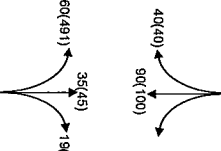
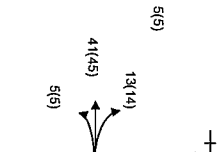
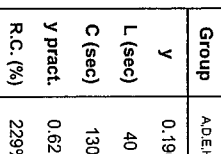




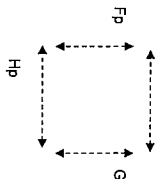
Stage / Phase Diagrams									
1.		2.		3.		4.		5.	
									
I/G= 3		I/G= 7		I/G= 5		I/G= 9		I/G=	
I/G= 3		I/G= 7	5	I/G= 5		I/G= 9	19	I/G=	
Date: MAY, 2016						Junction: Bailey St / Hung Hom Rd			
									

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Bailey St / Hung Hom RdDesign Year: 2016Description: Year 2028 ReferenceDesigned By: SELChecked By: SEL

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak				
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Bailey St WB		D	3	3,000	15	20	8% / 22%	8% / 22%	1870	1870	59	0.032	0.032	64	0.034	0.034			
Hung Hom Rd NB		A	1.2	3,500	15				1785	1785	221	0.124		236	0.132				
		A	1.2	3,500	17				1935	1935	239	0.124		255	0.132				
		B	2	3,000		20	35%	32%	1865	1870	54	0.029	0.029	66	0.035	0.035			
Bailey St EB		C	1	3,500	15	23	5% / 82%	5% / 82%	1855	1855	396	0.213	0.213	431	0.232	0.232			
		C	1	3,500		25			1855	1855	396	0.213		430	0.232				
Hung Hom Rd SB		E	4	3,500	15		8%	7%	1950	1950	66	0.034		71	0.036				
		E	4	3,500		20	59%	54%	2015	2025	69	0.034	0.034	74	0.037	0.037			
Pedestrian Crossing																			
	Fp	5	MIN GREEN + FLASH =	10	+	9	=	19											
	Gp	5	MIN GREEN + FLASH =	7	+	6	=	13											
	Hp	5	MIN GREEN + FLASH =	11	+	10	=	21											
	Ip	5	MIN GREEN + FLASH =	11	+	10	=	21											
Notes:																			
				Flow: (pcu/hr)															
																			
																			
																			
																			
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
																			
I/G=3		I/G=7		I/G=7		I/G=5		I/G=5		I/G=5		I/G=5		I/G=9		21			
I/G=3		I/G=7		I/G=5		I/G=5		I/G=5		I/G=5		I/G=5		I/G=9		21			
Date: MAY / 2016																			
Junction: Bailey St / Hung Hom Rd																			

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Bailey St / Hung Hom Rd

Design Year: 2028

Description: Year 2028 with Development

Designed By: SEL

Checked By: SEL

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)		Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak																							
					Left	Right	AM	PM	AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																					
Bailey St WB	←	D	3	3,000	15	20	8% / 22% 8% / 22%				1870	1870	59	0.032	0.032	64	0.034	0.034																					
Hung Hom Rd NB	↑	A	1,2	3,500	15						1785	1785	257	0.144		272	0.152																						
	↑	A	1,2	3,500	17						1935	1935	278	0.144		294	0.152																						
	→	B	2	3,000		20	35% 32%				1865	1870	54	0.029	0.029	66	0.035	0.035																					
Bailey St EB	↗	C	1	3,500	15	23	4% / 85% 4% / 83%				1855	1855	471	0.254	0.254	465	0.251	0.251																					
	↑	C	1	3,500		25					1855	1855	472	0.254	0.254	465	0.251																						
Hung Hom Rd SB	↑	E	4	3,500	15		8% 7%				1950	1950	66	0.034		71	0.036																						
	↑	E	4	3,500		20	58% 54%				2015	2025	69	0.034	0.034	74	0.037	0.037																					
Pedestrian Crossing																																							
Fp	5	MIN GREEN + FLASH =	10	+	9	=	19											*																					
Gp	5	MIN GREEN + FLASH =	7	+	6	=	13																																
Hp	5	MIN GREEN + FLASH =	11	+	10	=	21																																
Ip	5	MIN GREEN + FLASH =	11	+	10	=	21																																
Notes:																																							
				Flow: (pcu/hr)																																			
Stage / Phase Diagrams																																							
1.								2.								3.								4.								5.							
I/G=3				I/G=7				I/G=5				I/G=5				I/G=9				21																			
I/G=3				I/G=7				I/G=5				I/G=5				I/G=9				21																			
Date: MAY, 2016 Junction: Bailey St / Hung Hom Rd																																							

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50143110

MVA HONG KONG LIMITED

Junction: Bailey St / Hung Hom RdDesign Year: 2028Description: Year 2028 Design ScenarioDesigned By: SELChecked By: SEL

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		AM Peak			PM Peak		
					Left	Right		AM	PM	AM	PM	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Bailey St WB	←	D	3	3.000	15	20		8% / 22%	8% / 22%	1870	1870	59	0.032	0.032	64	0.034	0.034
Hung Hom Rd NB	↖	A	1,2	3.500	15					1785	1785	257	0.144		272	0.152	
	↗	A	1,2	3.500	17					1935	1935	278	0.144		294	0.152	
	←	B	2	3.000		20	35%		32%	1865	1870	54	0.029	0.029	66	0.035	0.035
Bailey St EB	↖	C	1	3.500	15	23	4% / 85%	4% / 83%		1855	1855	471	0.254	0.254	465	0.251	0.251
	↗	C	1	3.500		25				1855	1855	472	0.254	0.254	465	0.251	
Hung Hom Rd SB	↖	E	4	3.500	15		8%	7%		1950	1950	66	0.034		71	0.036	
	↗	E	4	3.500		20	58%	54%		2015	2025	69	0.034	0.034	74	0.037	0.037
<p>Notes:</p> <p>Pedestrian Crossing</p> <p>Fp 5 MIN GREEN + FLASH = 10 + 9 = 19</p> <p>Gp 5 MIN GREEN + FLASH = 7 + 6 = 13</p> <p>Hp 5 MIN GREEN + FLASH = 11 + 10 = 21</p> <p>Ip 5 MIN GREEN + FLASH = 11 + 10 = 21</p>																	
Flow: (pcu/hr)																	
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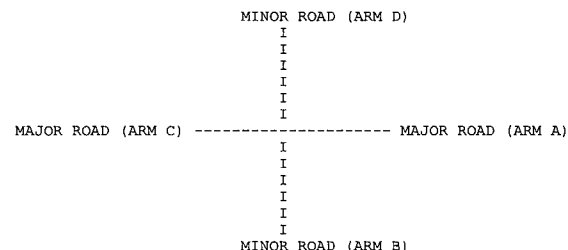
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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA



ARM A IS Wing Kwong Street WB
ARM B IS Wan On Street NB
ARM C IS Wing Kwong Street EB
ARM D IS Wan On Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM A-BC CONTAINS TRAFFIC GOING FROM ARM A TO ARM B AND TO ARM C

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.00 M.	I	(W) 11.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I		I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I		I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.25 M.	I	(WD-A) 2.25 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.25 M.	I	(WD-C) 2.25 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.

LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I		NUMBER OF MINUTES FROM START WHEN				RATE OF FLOW (VEH/MIN)							
I	ARM	I	FLOW STARTS	I	TOP OF PEAK	I	FLOW STOPS	I	BEFORE	I	AT	I	AFTER
I	I	I	TO RISE	I	IS REACHED	I	FALLING	I	PEAK	I	OF PEAK	I	PEAK
I	ARM A	I	15.00	I	45.00	I	75.00	I	3.70	I	5.55	I	3.70
I	ARM B	I	15.00	I	45.00	I	75.00	I	1.75	I	2.63	I	1.75
I	ARM C	I	15.00	I	45.00	I	75.00	I	0.00	I	0.00	I	0.00
I	ARM D	I	15.00	I	45.00	I	75.00	I	0.00	I	0.00	I	0.00

TIME		FROM/TO	ARM A	ARM B	ARM C	ARM D
08.00 - 09.30	ARM A	0.000	0.426	0.395	0.179	
		0.0	126.0	117.0	53.0	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM B	0.000	0.000	0.657	0.343	
		0.0	0.0	92.0	48.0	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM C	0.000	0.000	0.000	0.000	
		???????	???????	???????	???????	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM D	0.000	0.000	0.000	0.000	
		???????	???????	???????	???????	
		(0.0)	(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-CD	1.45	8.76	0.165		0.0	0.2	2.8	
B-AD	0.30	7.16	0.042		0.0	0.0	0.6	
A-B	1.58							
A-C	1.46							
A-D	0.66	11.98	0.055		0.0	0.1	0.9	
D-AB	0.00	10.95	0.000		0.0	0.0	0.0	
D-BC	0.00	9.00	0.000		0.0	0.0	0.0	
C-D	0.00							
C-A	0.00							
C-B	0.00	10.10	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY	
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT	
			(.1M)	(.1M)	(M)		(M)	
B-CD		0.095	0.002	0.005	0.001		0.009	
B-AD		0.077	0.003	0.017	0.005		0.007	
C-B		0.121	0.005		0.009			
D-AB		0.130	0.000	0.000	0.000		0.010	
D-BC		0.107	0.005	0.024	0.006		0.008	
A-D		0.130	0.000		0.010			

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC	DELAYI
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	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I 08.15-08.30									I
I B-CD	1.73	8.67	0.200		0.2	0.2	3.6		I
I B-AD	0.36	7.07	0.051		0.0	0.1	0.8		I
I A-B	1.88								I
I A-C	1.75								I
I A-D	0.79	11.98	0.066		0.1	0.1	1.0		I
I D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I
I D-BC	0.00	8.85	0.000		0.0	0.0	0.0		I
I C-D	0.00								I
I C-A	0.00								I
I C-B	0.00	9.95	0.000		0.0	0.0	0.0		I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	(M)	TO RIGHT	(M)	
I B-CD		0.094	0.003	0.005	0.001		0.008		I
I B-AD		0.077	0.003	0.017	0.005		0.007		I
I C-B		0.119	0.006		0.009				I
I D-AB		0.130	0.000	0.000	0.000		0.010		I
I D-BC		0.105	0.006	0.024	0.006		0.008		I
I A-D		0.130	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)		
I	08.30-08.45									I	
I	B-CD	2.12	8.53	0.249		0.2	0.3	4.8		I	
I	B-AD	0.44	6.95	0.063		0.1	0.1	1.0		I	
I	A-B	2.30								I	
I	A-C	2.14								I	
I	A-D	0.97	11.98	0.081		0.1	0.1	1.3		I	
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I	
I	D-BC	0.00	8.65	0.000		0.0	0.0	0.0		I	
I	C-D	0.00								I	
I	C-A	0.00								I	
I	C-B	0.00	9.73	0.000		0.0	0.0	0.0		I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY			I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT			I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)			I	
I	B-CD	0.092	0.003	0.005	0.001		0.008			I	
I	B-AD	0.075	0.004	0.017	0.005		0.007			I	
I	C-B	0.116	0.007		0.009					I	
I	D-AB	0.130	0.000	0.000	0.000		0.010			I	
I	D-BC	0.103	0.007	0.024	0.005		0.008			I	
I	A-D	0.130	0.000		0.010					I	

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
I	08.45-09.00								
I	B-CD	2.12	8.53	0.249		0.3	0.3	4.9	
I	B-AD	0.44	6.95	0.063		0.1	0.1	1.0	
I	A-B	2.30							
I	A-C	2.14							
I	A-D	0.97	11.98	0.081		0.1	0.1	1.3	
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0	
I	D-BC	0.00	8.64	0.000		0.0	0.0	0.0	
I	C-D	0.00							
I	C-A	0.00							
I	C-B	0.00	9.73	0.000		0.0	0.0	0.0	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
I			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)		
I	B-CD	0.092	0.003	0.005	0.001		0.008		
I	B-AD	0.075	0.004	0.017	0.005		0.007		
I	C-B	0.116	0.007		0.009				
I	D-AB	0.130	0.000	0.000	0.000		0.010		
I	D-BC	0.103	0.007	0.024	0.005		0.008		
I	A-D	0.130	0.000		0.010				

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	
I	09.00-09.15									I
I	B-CD	1.73	8.67	0.200		0.3	0.3	3.9		I
I	B-AD	0.36	7.07	0.051		0.1	0.1	0.8		I
I	A-B	1.88								I
I	A-C	1.75								I
I	A-D	0.79	11.98	0.066		0.1	0.1	1.1		I
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	8.85	0.000		0.0	0.0	0.0		I
I	C-D	0.00								I
I	C-A	0.00								I
I	C-B	0.00	9.95	0.000		0.0	0.0	0.0		I
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	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)		
I	09.15-09.30									I	
I	B-CD	1.45	8.76	0.165		0.3	0.2	3.1		I	
I	B-AD	0.30	7.16	0.042		0.1	0.0	0.7		I	
I	A-B	1.58								I	
I	A-C	1.46								I	
I	A-D	0.66	11.98	0.055		0.1	0.1	0.9		I	
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I	
I	D-BC	0.00	9.00	0.000		0.0	0.0	0.0		I	
I	C-D	0.00								I	
I	C-A	0.00								I	
I	C-B	0.00	10.10	0.000		0.0	0.0	0.0		I	
I										I	
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I		MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		I	
I		CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		(M)		I	
I				(.1M)	(.1M)	(M)				I	
I	B-CD	0.095	0.002	0.005	0.001	0.009				I	
I	B-AD	0.077	0.003	0.017	0.005	0.007				I	
I	C-B	0.121	0.005		0.009					I	
I	D-AB	0.130	0.000	0.000	0.000	0.010				I	
I	D-BC	0.107	0.005	0.024	0.006	0.008				I	
I	A-D	0.130	0.000		0.010					I	

QUEUE FOR STREAM B-CD

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.2
08.30	0.2
08.45	0.3
09.00	0.3
09.15	0.3
09.30	0.2

QUEUE FOR STREAM B-AD

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.0
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.0

QUEUE FOR STREAM A-D

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	

ENDING	VEHICLES IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

 QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	I	I	I	I	I	I
I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-CD	I	159.1	I	106.0	I	23.1	I
I	B-AD	I	32.9	I	21.9	I	4.9	I
I	A-B	I	172.8	I	115.2	I	I	I
I	A-C	I	160.4	I	107.0	I	I	I
I	A-D	I	72.7	I	48.4	I	6.5	I
I	D-AB	I	0.0	I	0.0	I	0.0	I
I	D-BC	I	0.0	I	0.0	I	0.0	I
I	C-D	I	0.0	I	0.0	I	I	I
I	C-A	I	0.0	I	0.0	I	I	I
I	C-B	I	0.0	I	0.0	I	0.0	I
I	ALL	I	597.8	I	398.6	I	34.5	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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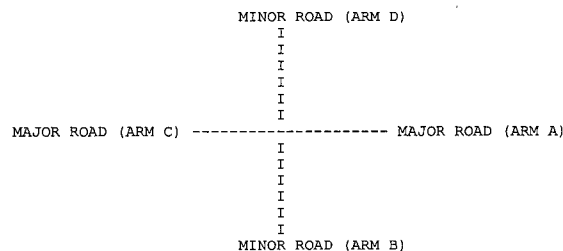
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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
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INPUT DATA



ARM A IS Wing Kwong Street WB
ARM B IS Wan On Street NB
ARM C IS Wing Kwong Street EB
ARM D IS Wan On Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM A-BC CONTAINS TRAFFIC GOING FROM ARM A TO ARM B AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 11.00 M.	I	(W) 11.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 250.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.25 M.	I	(WD-A) 2.25 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.25 M.	I	(WD-C) 2.25 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I		NUMBER OF MINUTES FROM START WHEN			RATE OF FLOW (VEH/MIN)		
I	ARM	FLOW STARTS	TOP OF PEAK	FLOW STOPS	BEFORE	AT	AFTER
I	I	TO RISE	IS REACHED	FALLING	PEAK	OF PEAK	PEAK
I	ARM A	I 15.00	I 45.00	I 75.00	I 3.26	I 4.89	I 3.26
I	ARM B	I 15.00	I 45.00	I 75.00	I 1.56	I 2.34	I 1.56
I	ARM C	I 15.00	I 45.00	I 75.00	I 0.00	I 0.00	I 0.00
I	ARM D	I 15.00	I 45.00	I 75.00	I 0.00	I 0.00	I 0.00

		TURNING PROPORTIONS TURNING COUNTS (VEH/HR) (PERCENTAGE OF H.V.S)				
TIME	FROM/TO	ARM A	ARM B	ARM C	ARM D	
08.00 - 09.30	ARM A	0.000	0.318	0.536	0.146	
		0.0	83.0	140.0	38.0	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM B	0.000	0.000	0.544	0.456	
		0.0	0.0	68.0	57.0	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM C	0.000	0.000	0.000	0.000	
		I???????	I???????	I???????	I???????	
		(0.0)	(0.0)	(0.0)	(0.0)	
	ARM D	0.000	0.000	0.000	0.000	
		I???????	I???????	I???????	I???????	
		(0.0)	(0.0)	(0.0)	(0.0)	

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-CD	1.21	8.52	0.142		0.0	0.2	2.4	
B-AD	0.36	7.18	0.050		0.0	0.1	0.7	
A-B	1.04							
A-C	1.75							
A-D	0.47	11.98	0.040		0.0	0.0	0.6	
D-AB	0.00	10.95	0.000		0.0	0.0	0.0	
D-BC	0.00	9.06	0.000		0.0	0.0	0.0	
C-D	0.00							
C-A	0.00							
C-B	0.00	10.20	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR)		VISIBILITY TO RIGHT (M)		
B-CD	0.092	0.002	0.007	0.002		0.008		
B-AD	0.078	0.002	0.017	0.005		0.007		
C-B	0.122	0.004		0.009				
D-AB	0.130	0.000	0.000	0.000		0.010		
D-BC	0.108	0.004	0.024	0.006		0.008		
A-D	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
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I	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/	I
I			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	08.15-08.30								I
I	B-CD	1.44	8.42	0.171	0.2	0.2	3.0		I
I	B-AD	0.43	7.10	0.060	0.1	0.1	0.9		I
I	A-B	1.24							I
I	A-C	2.09							I
I	A-D	0.57	11.98	0.047	0.0	0.0	0.7		I
I	D-AB	0.00	10.95	0.000	0.0	0.0	0.0		I
I	D-BC	0.00	8.92	0.000	0.0	0.0	0.0		I
I	C-D	0.00							I
I	C-A	0.00							I
I	C-B	0.00	10.07	0.000	0.0	0.0	0.0		I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			I
I	B-CD	0.091	0.003	0.007	0.002	0.008			I
I	B-AD	0.077	0.003	0.017	0.005	0.007			I
I	C-B	0.120	0.005		0.009				I
I	D-AB	0.130	0.000	0.000	0.000	0.010			I
I	D-BC	0.106	0.005	0.024	0.006	0.008			I
I	A-D	0.130	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
	08.30-08.45								
	B-CD	1.76	8.28	0.213		0.2	0.3	3.9	
	B-AD	0.52	6.98	0.075		0.1	0.1	1.2	
	A-B	1.52							
	A-C	2.56							
	A-D	0.69	11.98	0.058		0.0	0.1	0.9	
	D-AB	0.00	10.95	0.000		0.0	0.0	0.0	
	D-BC	0.00	8.73	0.000		0.0	0.0	0.0	
	C-D	0.00							
	C-A	0.00							
	C-B	0.00	9.88	0.000		0.0	0.0	0.0	
	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
	B-CD	0.090	0.003	0.007	0.002	0.008			
	B-AD	0.076	0.004	0.017	0.005	0.007			
	C-B	0.118	0.006		0.009				
	D-AB	0.130	0.000	0.000	0.000	0.010			
	D-BC	0.104	0.006	0.024	0.005	0.008			
	A-D	0.130	0.000		0.010				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.45-09.00								
B-CD	1.76	8.28	0.213		0.3	0.3	4.0	
B-AD	0.52	6.98	0.075		0.1	0.1	1.2	
A-B	1.52							
A-C	2.56							
A-D	0.69	11.98	0.058		0.1	0.1	0.9	
D-AB	0.00	10.95	0.000		0.0	0.0	0.0	
D-BC	0.00	8.73	0.000		0.0	0.0	0.0	
C-D	0.00							
C-A	0.00							
C-B	0.00	9.88	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-CD	0.090	0.003	0.007	0.002	0.008			
B-AD	0.076	0.004	0.017	0.005	0.007			
C-B	0.118	0.006		0.009				
D-AB	0.130	0.000	0.000	0.000	0.010			
D-BC	0.104	0.006	0.024	0.005	0.008			
A-D	0.130	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.00-09.15									I
I	B-CD	1.44	8.42	0.171		0.3	0.2	3.2		I
I	B-AD	0.43	7.10	0.060		0.1	0.1	1.0		I
I	A-B	1.24								I
I	A-C	2.09								I
I	A-D	0.57	11.98	0.047		0.1	0.0	0.8		I
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	8.92	0.000		0.0	0.0	0.0		I
I	C-D	0.00								I
I	C-A	0.00								I
I	C-B	0.00	10.07	0.000		0.0	0.0	0.0		I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I	B-CD	0.091	0.003	0.007	0.002	0.008				I
I	B-AD	0.077	0.003	0.017	0.005	0.007				I
I	C-B	0.120	0.005		0.009					I
I	D-AB	0.130	0.000	0.000	0.000	0.010				I
I	D-BC	0.106	0.005	0.024	0.006	0.008				I
I	A-D	0.130	0.000		0.010					I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	09.15-09.30									I
I	B-CD	1.21	8.52	0.142		0.2	0.2	2.6		I
I	B-AD	0.36	7.18	0.050		0.1	0.1	0.8		I
I	A-B	1.04								I
I	A-C	1.75								I
I	A-D	0.47	11.98	0.040		0.0	0.0	0.6		I
I	D-AB	0.00	10.95	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	9.06	0.000		0.0	0.0	0.0		I
I	C-D	0.00								I
I	C-A	0.00								I
I	C-B	0.00	10.20	0.000		0.0	0.0	0.0		I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I	B-CD	0.092	0.002	0.007	0.002	0.008				I
I	B-AD	0.078	0.002	0.017	0.005	0.007				I
I	C-B	0.122	0.004		0.009					I
I	D-AB	0.130	0.000	0.000	0.000	0.010				I
I	D-BC	0.108	0.004	0.024	0.006	0.008				I
I	A-D	0.130	0.000		0.010					I

QUEUE FOR STREAM B-CD

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.2
08.30	0.2
08.45	0.3
09.00	0.3
09.15	0.2
09.30	0.2

QUEUE FOR STREAM B-AD

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

QUEUE FOR STREAM A-D

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.1
09.00	0.1
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I	
I	I	I	I	I	* DELAY *	I	* DELAY *	I	
I	I	I	I	I	I	I	I	I	
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)	
I	B-CD	I	132.3	I	88.2	I	19.0	I	0.14
I	B-AD	I	39.1	I	26.1	I	5.8	I	0.15
I	A-B	I	113.8	I	75.9	I	I	I	I
I	A-C	I	192.0	I	128.0	I	I	I	I
I	A-D	I	52.1	I	34.7	I	4.5	I	0.09
I	D-AB	I	0.0	I	0.0	I	0.0	I	0.00
I	D-BC	I	0.0	I	0.0	I	0.0	I	0.00
I	C-D	I	0.0	I	0.0	I	I	I	I
I	C-A	I	0.0	I	0.0	I	I	I	I
I	C-B	I	0.0	I	0.0	I	0.0	I	0.00
I	ALL	I	529.3	I	352.9	I	29.4	I	0.06

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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QUEUE FOR STREAM A-D

QUEUE FOR STREAM D-AB

QUEUE FOR STREAM D-BC

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I	
I	I	I	I	I	* DELAY *	I	* DELAY *	I	
I	I	I	I	I	I	I	I	I	
I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	I	(MIN)	(MIN/VEH)
I	B-ACD	I	138.5	I	92.3	I	18.0	I	0.13
I	A-B	I	0.0	I	0.0	I		I	
I	A-C	I	54.8	I	36.6	I		I	
I	A-D	I	20.6	I	13.7	I	2.2	I	0.11
I	D-AB	I	0.0	I	0.0	I	0.0	I	0.00
I	D-BC	I	0.0	I	0.0	I	0.0	I	0.00
I	C-D	I	0.0	I	0.0	I		I	
I	C-A	I	0.0	I	0.0	I		I	
I	C-B	I	0.0	I	0.0	I	0.0	I	0.00
I	ALL	I	213.9	I	142.6	I	20.2	I	0.09

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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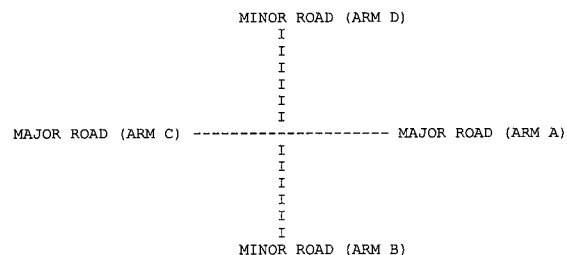
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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
*****

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INPUT DATA



ARM A IS Kai Ming Street WB
ARM B IS Wan on Street NB
ARM C IS Kai Ming Street EB
ARM D IS Wan On Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM B-AC CONTAINS TRAFFIC GOING FROM ARM B TO ARM A AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 9.00 M.	I	(W) 9.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I		I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I		I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 4.00 M.	I	(WD-A) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 0.00 M.	I	(WD-C) 2.20 M.	I

.TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I		I		NUMBER OF		MINUTES FROM START WHEN		I		RATE OF FLOW (VEH/MIN)		I	
I	ARM	I	FLOW	I	STARTS	I	TOP OF	I	PEAK	I	FLOWS	I	AT
I	I	I	TO	I	TO RISE	I	IS	I	REACHED	I	TOPS	I	FALLING
I	ARM A	I	15.00	I	45.00	I	75.00	I	0.88	I	1.31	I	0.88
I	ARM B	I	15.00	I	45.00	I	75.00	I	1.19	I	1.78	I	1.19
I	ARM C	I	15.00	I	45.00	I	75.00	I	0.00	I	0.00	I	0.00
I	ARM D	I	15.00	I	45.00	I	75.00	I	0.00	I	0.00	I	0.00

		TURNING PROPORTIONS					
		TURNING COUNTS (VEH/HR)					
		(PERCENTAGE OF H.V.S)					
TIME	FROM/TO	ARM A	ARM B	ARM C	ARM D		
17.00 - 18.30							
	ARM A	0.000	0.000	0.786	0.214		
		0.0	0.0	55.0	15.0		
		(0.0)	(0.0)	(0.0)	(0.0)		
	ARM B	0.000	0.000	0.211	0.789		
		0.0	0.0	20.0	75.0		
		(0.0)	(0.0)	(0.0)	(0.0)		
	ARM C	0.000	0.000	0.000	0.000		
		I???????	I???????	I???????	I???????		
		(0.0)	(0.0)	(0.0)	(0.0)		
	ARM D	0.000	0.000	0.000	0.000		
		I???????	I???????	I???????	I???????		
		(0.0)	(0.0)	(0.0)	(0.0)		

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.00-17.15								
B-ACD	1.19	9.32	0.127		0.0	0.1	2.1	
A-B	0.00							
A-C	0.69							
A-D	0.19	9.57	0.020		0.0	0.0	0.3	
D-AB	0.00	10.89	0.000		0.0	0.0	0.0	
D-BC	0.00	9.27	0.000		0.0	0.0	0.0	
C-D	0.00							
C-A	0.00							
C-B	0.00	10.68	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
		MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)			
B-ACD	0.127	0.001	0.013	0.006	0.009			
C-B	0.128	0.001		0.009				
D-AB	0.130	0.000	0.000	0.000	0.010			
D-BC	0.111	0.002	0.024	0.006	0.008			
A-D	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH./MIN/	(VEH./MIN/
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)

I	17.15-17.30								I
I	B-ACD	1.42	9.28	0.153	0.1	0.2	2.6		I
I	A-B	0.00							I
I	A-C	0.82							I
I	A-D	0.22	9.57	0.023	0.0	0.0	0.4		I
I	D-AB	0.00	10.89	0.000	0.0	0.0	0.0		I
I	D-BC	0.00	9.19	0.000	0.0	0.0	0.0		I
I	C-D	0.00							I
I	C-A	0.00							I
I	C-B	0.00	10.64	0.000	0.0	0.0	0.0		I
I									I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:							I
I									I
I									I
I	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		I
I	CHANGE:	(.1M)	(.1M)	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		I
I				(.1M)	(M)		(M)		I
I									I
I	B-ACD	0.126	0.001	0.013	0.006		0.009		I
I	C-B	0.127	0.001		0.009				I
I	D-AB	0.130	0.000	0.000	0.000		0.010		I
I	D-BC	0.110	0.003	0.024	0.006		0.008		I
I	A-D	0.104	0.000		0.010				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	17.30-17.45									I
I	B-ACD	1.74	9.23	0.188		0.2	0.2	3.4		I
I	A-B	0.00								I
I	A-C	1.01								I
I	A-D	0.27	9.57	0.029		0.0	0.0	0.4		I
I	D-AB	0.00	10.89	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	9.07	0.000		0.0	0.0	0.0		I
I	C-D	0.00								I
I	C-A	0.00								I
I	C-B	0.00	10.58	0.000		0.0	0.0	0.0		I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I			MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY				I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT				I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)	(M)				I
I										I
I	B-ACD	0.126	0.001	0.013	0.006	0.009				I
I	C-B	0.127	0.002		0.009					I
I	D-AB	0.130	0.000	0.000	0.000	0.010				I
I	D-BC	0.109	0.004	0.024	0.006	0.008				I
I	A-D	0.104	0.000		0.010					I

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
17.45-18.00								
B-ACD	1.74	9.23	0.188		0.2	0.2	3.5	
A-B	0.00							
A-C	1.01							
A-D	0.27	9.57	0.029		0.0	0.0	0.4	
D-AB	0.00	10.89	0.000		0.0	0.0	0.0	
D-BC	0.00	9.07	0.000		0.0	0.0	0.0	
C-D	0.00							
C-A	0.00							
C-B	0.00	10.58	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY	
MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)	(M)		TO RIGHT (M)	
B-ACD	0.126	0.001	0.013	0.006		0.009		
C-B	0.127	0.002		0.009				
D-AB	0.130	0.000	0.000	0.000		0.010		
D-BC	0.108	0.004	0.024	0.006		0.008		
A-D	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	18.00-18.15									I
I	B-ACD	1.42	9.28	0.153		0.2	0.2	2.8		I
I	A-B	0.00								I
I	A-C	0.82								I

I	A-D	0.22	9.57	0.023		0.0	0.0	0.4	I
I	D-AB	0.00	10.89	0.000		0.0	0.0	0.0	I
I	D-BC	0.00	9.18	0.000		0.0	0.0	0.0	I
I	C-D	0.00							I
I	C-A	0.00							I
I	C-B	0.00	10.64	0.000		0.0	0.0	0.0	I
I									I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:							I
I									I
I									I
I	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		I
I	CHANGE:	(.1M)	(.1M)	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		I
I				(.1M)	(M)		(M)		I
I									I
I	B-ACD	0.126	0.001	0.013	0.006		0.009		I
I	C-B	0.127	0.001		0.009				I
I	D-AB	0.130	0.000	0.000	0.000		0.010		I
I	D-BC	0.110	0.003	0.024	0.006		0.008		I
I	A-D	0.104	0.000		0.010				I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
I		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH. MIN/	(VEH. MIN/	I
I				(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	I
I	18.15-18.30									I
I	B-ACD	1.19	9.32	0.127		0.2	0.1	2.3		I
I	A-B	0.00								I
I	A-C	0.69								I
I	A-D	0.19	9.57	0.020		0.0	0.0	0.3		I
I	D-AB	0.00	10.89	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	9.27	0.000		0.0	0.0	0.0		I
I	C-D	0.00								I
I	C-A	0.00								I
I	C-B	0.00	10.68	0.000		0.0	0.0	0.0		I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I										I
I										I
I	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY			I
I	CHANGE:	(.1M)	(.1M)	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT			I
I				(.1M)	(M)		(M)			I
I										I
I	B-ACD	0.127	0.001	0.013	0.006		0.009			I
I	C-B	0.128	0.001		0.009					I
I	D-AB	0.130	0.000	0.000	0.000		0.010			I
I	D-BC	0.111	0.002	0.024	0.006		0.008			I
I	A-D	0.104	0.000		0.010					I

QUEUE FOR STREAM B-ACD

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.1
17.30	0.2
17.45	0.2
18.00	0.2
18.15	0.2
18.30	0.1

QUEUE FOR STREAM A-D

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUE FOR STREAM D-AB

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUE FOR STREAM D-BC

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	I	I	I	I	I	I
I	I	I	(VEH) (VEH/H)	I	(MIN) (MIN/VEH)	I	(MIN) (MIN/VEH)	I
I	B-ACD	I	130.3 I 86.8	I	16.6 I 0.13	I	16.6 I 0.13	I
I	A-B	I	0.0 I 0.0	I	I	I	I	I
I	A-C	I	75.4 I 50.3	I	I	I	I	I
I	A-D	I	20.6 I 13.7	I	2.2 I 0.11	I	2.2 I 0.11	I
I	D-AB	I	0.0 I 0.0	I	0.0 I 0.00	I	0.0 I 0.00	I
I	D-BC	I	0.0 I 0.0	I	0.0 I 0.00	I	0.0 I 0.00	I
I	C-D	I	0.0 I 0.0	I	I	I	I	I
I	C-A	I	0.0 I 0.0	I	I	I	I	I
I	C-B	I	0.0 I 0.0	I	0.0 I 0.00	I	0.0 I 0.00	I
I	ALL	I	226.2 I 150.8	I	18.8 I 0.08	I	18.8 I 0.08	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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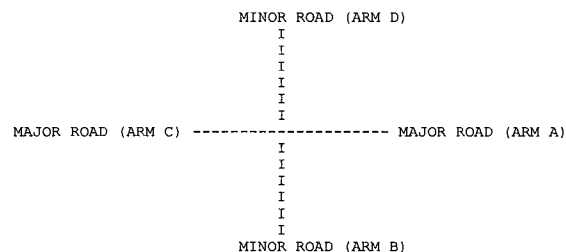
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.MAJOR/MINOR JUNCTION CAPACITY AND DELAY
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INPUT DATA



ARM A	IS	Hung Fook Street	WB
ARM B	IS	Wan on Street	NB
ARM C	IS	Hung Fook Street	EB
ARM D	IS	Wan On Street	SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM A-BC CONTAINS TRAFFIC GOING FROM ARM A TO ARM B AND TO ARM C

ETC.

.GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 9.00 M.	I	(W) 9.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I		I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I		I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.25 M.	I	(WB-A) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.25 M.	I	(WB-D) 2.20 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF MINUTES FROM START WHEN			I	RATE OF FLOW (VEH/MIN)				I
I	ARM	FLOW STARTS	TOP OF PEAK	FLOW STOPS	I	BEFORE	AT	AFTER	I	
I	I	TO RISE	IS REACHED	FALLING	I	PEAK	PEAK	PEAK	I	
I	ARM A	I 15.00	I 45.00	I 75.00	I	0.00	I 0.00	I 0.00	I	
I	ARM B	I 15.00	I 45.00	I 75.00	I	1.26	I 1.89	I 1.26	I	
I	ARM C	I 15.00	I 45.00	I 75.00	I	0.24	I 0.36	I 0.24	I	
I	ARM D	I 15.00	I 45.00	I 75.00	I	0.00	I 0.00	I 0.00	I	

		TURNING PROPORTIONS			
		TURNING COUNTS (VEH/HR)			
		(PERCENTAGE OF H.V.S)			
TIME	FROM/TO	ARM A	ARM B	ARM C	ARM D
08.00 - 09.30	ARM A	0.000	0.000	0.000	0.000
		I???????	I???????	I???????	I???????
		I (0.0)	I (0.0)	I (0.0)	I (0.0)
		I	I	I	I
	ARM B	0.198	0.000	0.000	0.802
		I 20.0	I 0.0	I 0.0	I 81.0
		I (0.0)	I (0.0)	I (0.0)	I (0.0)
		I	I	I	I
	ARM C	0.526	0.000	0.000	0.474
		I 10.0	I 0.0	I 0.0	I 9.0
		I (0.0)	I (0.0)	I (0.0)	I (0.0)
		I	I	I	I
	ARM D	0.000	0.000	0.000	0.000
		I???????	I???????	I???????	I???????
		I (0.0)	I (0.0)	I (0.0)	I (0.0)
		I	I	I	I

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.00-08.15								
B-CD	0.52	7.40	0.071		0.0	0.1	1.1	
B-AD	0.74	7.59	0.097		0.0	0.1	1.5	
A-B	0.00							
A-C	0.00							
A-D	0.00	8.65	0.000		0.0	0.0	0.0	
D-AB	0.00	10.85	0.000		0.0	0.0	0.0	
D-BC	0.00	9.41	0.000		0.0	0.0	0.0	
C-D	0.11							
C-A	0.13							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL CHANGE:	LANE WIDTH (.1M)	MAJOR RD. WIDTH (.1M)	CENT RES WIDTH (.1M)	VIS TO LEFT (AHEAD FOR MAJOR)	VISIBILITY TO RIGHT (M)			
B-CD	0.080	0.000	0.017	0.005	0.007			
B-AD	0.082	0.000	0.017	0.005	0.008			
C-B	0.130	0.000		0.010				
D-AB	0.130	0.000	0.000	0.000	0.010			
D-BC	0.113	0.002	0.024	0.006	0.008			
A-D	0.104	0.000		0.010				

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
---	------	--------	----------	---------	------------	-------	-----	-------	------------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)	
I 08.15-08.30									I
I B-CD	0.63	7.36	0.085		0.1	0.1	1.4		I
I B-AD	0.88	7.59	0.116		0.1	0.1	1.9		I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	8.65	0.000		0.0	0.0	0.0		I
I D-AB	0.00	10.85	0.000		0.0	0.0	0.0		I
I D-BC	0.00	9.35	0.000		0.0	0.0	0.0		I
I C-D	0.13								I
I C-A	0.15								I
I C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)		TO RIGHT		
					(M)		(M)		
I B-CD		0.080	0.000	0.017	0.005		0.007		I
I B-AD		0.082	0.000	0.017	0.005		0.008		I
I C-B		0.130	0.000		0.010				I
I D-AB		0.130	0.000	0.000	0.000		0.010		I
I D-BC		0.112	0.002	0.024	0.006		0.008		I
I A-D		0.103	0.000		0.010				I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
	08.30-08.45								
	B-CD	0.78	7.30	0.106		0.1	0.1	1.7	
	B-AD	1.07	7.58	0.141		0.1	0.2	2.4	
	A-B	0.00							
	A-C	0.00							
	A-D	0.00	8.64	0.000		0.0	0.0	0.0	
	D-AB	0.00	10.84	0.000		0.0	0.0	0.0	
	D-BC	0.00	9.27	0.000		0.0	0.0	0.0	
	C-D	0.16							
	C-A	0.18							
	C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
			MAJOR RD.		CENT RES	VIS TO LEFT		VISIBILITY	
	MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)		(AHEAD FOR MAJOR) (M)		TO RIGHT (M)	
	B-CD	0.079	0.000	0.017		0.005		0.007	
	B-AD	0.082	0.000	0.017		0.005		0.007	
	C-B	0.130	0.000			0.010			
	D-AB	0.130	0.000	0.000		0.000		0.010	
	D-BC	0.111	0.002	0.024		0.006		0.008	
	A-D	0.103	0.000			0.010			

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)		
I	08.45-09.00									I	
I	B-CD	0.78	7.30	0.106		0.1	0.1	1.8		I	
I	B-AD	1.07	7.58	0.141		0.2	0.2	2.4		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	A-D	0.00	8.64	0.000		0.0	0.0	0.0		I	
I	D-AB	0.00	10.84	0.000		0.0	0.0	0.0		I	
I	D-BC	0.00	9.27	0.000		0.0	0.0	0.0		I	
I	C-D	0.16								I	
I	C-A	0.18								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY			I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT			I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)			I	
I										I	
I	B-CD	0.079	0.000	0.017		0.005		0.007		I	
I	B-AD	0.082	0.000	0.017		0.005		0.007		I	
I	C-B	0.130	0.000			0.010				I	
I	D-AB	0.130	0.000	0.000		0.000		0.010		I	
I	D-BC	0.111	0.002	0.024		0.006		0.008		I	
I	A-D	0.103	0.000			0.010				I	

I	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I	09.00-09.15									I
I	B-CD	0.63	7.36	0.085		0.1	0.1	1.5		I
I	B-AD	0.88	7.59	0.116		0.2	0.1	2.0		I
I	A-B	0.00								I
I	A-C	0.00								I
I	A-D	0.00	8.65	0.000		0.0	0.0	0.0		I
I	D-AB	0.00	10.85	0.000		0.0	0.0	0.0		I
I	D-BC	0.00	9.35	0.000		0.0	0.0	0.0		I
I	C-D	0.13								I
I	C-A	0.15								I
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									I
I			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY			I
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT			I
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)			I
I	B-CD	0.080	0.000	0.017	0.005		0.007			I
I	B-AD	0.082	0.000	0.017	0.005		0.008			I
I	C-B	0.130	0.000		0.010					I
I	D-AB	0.130	0.000	0.000	0.000		0.010			I
I	D-BC	0.112	0.002	0.024	0.006		0.008			I
I	A-D	0.103	0.000		0.010					I

	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)		
I	09.15-09.30									I	
I	B-CD	0.52	7.40	0.071		0.1	0.1	1.2		I	
I	B-AD	0.74	7.59	0.097		0.1	0.1	1.7		I	
I	A-B	0.00								I	
I	A-C	0.00								I	
I	A-D	0.00	8.65	0.000		0.0	0.0	0.0		I	
I	D-AB	0.00	10.85	0.000		0.0	0.0	0.0		I	
I	D-BC	0.00	9.41	0.000		0.0	0.0	0.0		I	
I	C-D	0.11								I	
I	C-A	0.13								I	
I	C-B	0.00	10.89	0.000		0.0	0.0	0.0		I	
I	EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:										I
I			MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY			I	
I	MARGINAL	LANE WIDTH	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT			I	
I	CHANGE:	(.1M)	(.1M)	(.1M)	(M)		(M)			I	
I	B-CD	0.080	0.000	0.017	0.005		0.007			I	
I	B-AD	0.082	0.000	0.017	0.005		0.008			I	
I	C-B	0.130	0.000		0.010					I	
I	D-AB	0.130	0.000	0.000	0.000		0.010			I	
I	D-BC	0.113	0.002	0.024	0.006		0.008			I	
I	A-D	0.104	0.000		0.010					I	

QUEUE FOR STREAM B-CD

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.1
08.30	0.1
08.45	0.1
09.00	0.1
09.15	0.1
09.30	0.1

QUEUE FOR STREAM B-AD

TIME SEGMENT	NO. OF VEHICLES IN QUEUE
ENDING	
08.15	0.1
08.30	0.1
08.45	0.2
09.00	0.2
09.15	0.1
09.30	0.1

QUEUE FOR STREAM A-D

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I		
I	I	I	I	I	* DELAY *	I	* DELAY *	I		
I	I	I	I	I	I	I	I	I		
I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	I	(MIN)		
I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	I	(MIN)		
I	B-CD	I	57.8	I	38.6	I	8.6	I	0.15	I
I	B-AD	I	80.7	I	53.8	I	12.0	I	0.15	I
I	A-B	I	0.0	I	0.0	I		I		I
I	A-C	I	0.0	I	0.0	I		I		I
I	A-D	I	0.0	I	0.0	I	0.0	I	0.00	I
I	D-AB	I	0.0	I	0.0	I	0.0	I	0.00	I
I	D-BC	I	0.0	I	0.0	I	0.0	I	0.00	I
I	C-D	I	12.3	I	8.2	I		I		I
I	C-A	I	13.7	I	9.1	I		I		I
I	C-B	I	0.0	I	0.0	I	0.0	I	0.00	I
I	ALL	I	164.5	I	109.7	I	20.5	I	0.12	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

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RELEASE 2.1 (DEC 1998)

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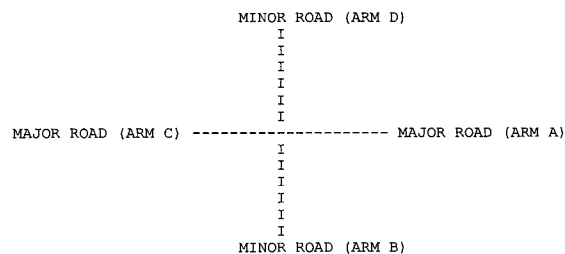
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RUN TITLE
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X4A Des PM

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MAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA



ARM A IS Hung Fook Street WB
ARM B IS Wan on Street NB
ARM C IS Hung Fook Street EB
ARM D IS Wan On Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM A-BC CONTAINS TRAFFIC GOING FROM ARM A TO ARM B AND TO ARM C

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 9.00 M.	I	(W) 9.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I		I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I		I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.25 M.	I	(WD-A) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.25 M.	I	(WD-C) 2.20 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 17.00 AND ENDS 18.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

ARM	NUMBER OF FLOW STARTS TO RISE	MINUTES FROM START WHEN TOP OF PEAK IS REACHED	FLOW STOPS FALLING	RATE OF FLOW BEFORE PEAK	OF FLOW AT TOP OF PEAK	(VEH/MIN) AFTER PEAK
ARM A	15.00	45.00	75.00	0.00	0.00	0.00
ARM B	15.00	45.00	75.00	1.13	1.69	1.13
ARM C	15.00	45.00	75.00	0.44	0.66	0.44
ARM D	15.00	45.00	75.00	0.00	0.00	0.00

		TURNING PROPORTIONS							
		TURNING COUNTS (VEH/HR)							
		(PERCENTAGE OF H. V. S.)							
TIME		FROM/TO	ARM A	ARM B	ARM C	ARM D			
17.00 - 18.30	ARM A	0.000	0.000	0.000	0.000				
		I 2???????	I 2???????	I 2???????	I 2???????				
		I (0.0)	I (0.0)	I (0.0)	I (0.0)				
	ARM B	0.333	0.000	0.000	0.667				
		I 30.0	I 0.0	I 0.0	I 60.0				
		I (0.0)	I (0.0)	I (0.0)	I (0.0)				
	ARM C	0.571	0.000	0.000	0.429				
		I 20.0	I 0.0	I 0.0	I 15.0				
		I (0.0)	I (0.0)	I (0.0)	I (0.0)				
	ARM D	0.000	0.000	0.000	0.000				
		I 2???????	I 2???????	I 2???????	I 2???????				
		I (0.0)	I (0.0)	I (0.0)	I (0.0)				

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
17.00-17.15								
B-CD	0.39	7.38	0.053		0.0	0.1	0.8	
B-AD	0.73	7.57	0.097		0.0	0.1	1.5	
A-B	0.00							
A-C	0.00							
A-D	0.00	8.62	0.000		0.0	0.0	0.0	
D-AB	0.00	10.82	0.000		0.0	0.0	0.0	
D-BC	0.00	9.42	0.000		0.0	0.0	0.0	
C-D	0.19							
C-A	0.25							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY	
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)		TO RIGHT	
					(M)		(M)	
B-CD		0.080	0.000	0.017	0.005		0.007	
B-AD		0.082	0.000	0.017	0.005		0.007	
C-B		0.130	0.000		0.010			
D-AB		0.129	0.000	0.000	0.000		0.010	
D-BC		0.113	0.001	0.024	0.006		0.008	
A-D		0.103	0.000		0.010			

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY (RFC)	FLOW (PEDS/MIN)	QUEUE (VEHS)	QUEUE (VEHS)	(VEH.MIN/ TIME SEGMENT)	(VEH.MIN/ TIME SEGMENT)
17.15-17.30								
B-CD	0.47	7.34	0.065		0.1	0.1	1.0	
B-AD	0.87	7.57	0.115		0.1	0.1	1.9	
A-B	0.00							
A-C	0.00							
A-D	0.00	8.60	0.000		0.0	0.0	0.0	
D-AB	0.00	10.80	0.000		0.0	0.0	0.0	
D-BC	0.00	9.36	0.000		0.0	0.0	0.0	
C-D	0.22							
C-A	0.30							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY	
	CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR) (M)		TO RIGHT (M)	
B-CD		0.079	0.000	0.017	0.005		0.007	
B-AD		0.082	0.000	0.017	0.005		0.007	
C-B		0.130	0.000		0.010			
D-AB		0.129	0.000	0.000	0.000		0.010	
D-BC		0.112	0.002	0.024	0.006		0.008	
A-D		0.103	0.001		0.010			

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
18.00-18.15								
B-CD	0.47	7.33	0.065		0.1	0.1	1.1	
B-AD	0.87	7.57	0.115		0.2	0.1	2.0	
A-B	0.00							
A-C	0.00							
A-D	0.00	8.60	0.000		0.0	0.0	0.0	
D-AB	0.00	10.80	0.000		0.0	0.0	0.0	
D-BC	0.00	9.36	0.000		0.0	0.0	0.0	
C-D	0.22							
C-A	0.30							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT		
			(.1M)	(.1M)	(M)	(M)		
B-CD	0.079	0.000	0.017	0.005	0.007			
B-AD	0.082	0.000	0.017	0.005	0.007			
C-B	0.130	0.000		0.010				
D-AB	0.129	0.000	0.000	0.000	0.010			
D-BC	0.112	0.002	0.024	0.006	0.008			
A-D	0.103	0.001		0.010				

QUEUE FOR STREAM B-CD	
TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.1
17.30	0.1
17.45	0.1
18.00	0.1
18.15	0.1
18.30	0.1

QUEUE FOR STREAM A-D	
TIME SEGMENT	NO. OF

ENDING	VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

 QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

 QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
17.15	0.0
17.30	0.0
17.45	0.0
18.00	0.0
18.15	0.0
18.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I
I	I	I	I	I	* DELAY *	I	* DELAY *	I
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)
I	B-CD	I	43.7	I	29.1	I	6.3	I
I	B-AD	I	79.7	I	53.1	I	11.9	I
I	A-B	I	0.0	I	0.0	I		I
I	A-C	I	0.0	I	0.0	I		I
I	A-D	I	0.0	I	0.0	I	0.00	I
I	D-AB	I	0.0	I	0.0	I	0.00	I
I	D-BC	I	0.0	I	0.0	I	0.00	I
I	C-D	I	20.6	I	13.7	I		I
I	C-A	I	27.4	I	18.3	I		I
I	C-B	I	0.0	I	0.0	I	0.00	I
I	ALL	I	171.4	I	114.3	I	18.2	I

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

TRANSPORT RESEARCH LABORATORY

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CAPACITIES, QUEUES, AND DELAYS AT 3 OR 4-ARM MAJOR/MINOR PRIORITY JUNCTIONS

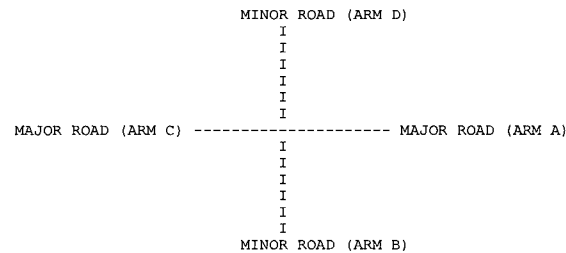
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RUN TITLE

X1_Des_AMMAJOR/MINOR JUNCTION CAPACITY AND DELAY

INPUT DATA

ARM A IS Ngan Hon Street WB
ARM B IS Lung Tak Street NB
ARM C IS Ngan Hon Street EB
ARM D IS Lung Tak Street SB

STREAM LABELLING CONVENTION

STREAM A-B CONTAINS TRAFFIC GOING FROM ARM A TO ARM B

STREAM A-BC CONTAINS TRAFFIC GOING FROM ARM A TO ARM B AND TO ARM C

ETC.

GEOMETRIC DATA

I	DATA ITEM	I	MINOR ROAD B	I	MINOR ROAD D	I
I	TOTAL MAJOR ROAD CARRIAGEWAY WIDTH	I	(W) 9.00 M.	I	(W) 9.00 M.	I
I	CENTRAL RESERVE WIDTH	I	(WCR) 0.00 M.	I	(WCR) 0.00 M.	I
I		I		I		I
I	MAJOR ROAD RIGHT TURN - WIDTH	I	(WC-B) 2.20 M.	I	(WA-D) 2.20 M.	I
I	- VISIBILITY	I	(VC-B) 250.0 M.	I	(VA-D) 0.0 M.	I
I	- BLOCKS TRAFFIC	I	NO	I	NO	I
I		I		I		I
I	MINOR ROAD - VISIBILITY TO LEFT	I	(VB-C) 20.0 M.	I	(VD-A) 250.0 M.	I
I	- VISIBILITY TO RIGHT	I	(VB-A) 20.0 M.	I	(VD-C) 250.0 M.	I
I	- LANE 1 WIDTH	I	(WB-C) 2.25 M.	I	(WD-A) 2.20 M.	I
I	- LANE 2 WIDTH	I	(WB-A) 2.25 M.	I	(WD-C) 2.20 M.	I

TRAFFIC DEMAND DATA

TIME PERIOD BEGINS 08.00 AND ENDS 09.30

LENGTH OF TIME PERIOD - 90 MINUTES.
LENGTH OF TIME SEGMENT - 15 MINUTES.

DEMAND FLOW PROFILES ARE SYNTHESISED FROM TURNING COUNT DATA

I	I	NUMBER OF MINUTES FROM START WHEN	I	RATE OF FLOW (VEH/MIN)	I
I	ARM	I FLOW STARTS I TOP OF PEAK I FLOW STOPS I BEFORE I AT TOP I AFTER I	I	I	I
I	I	I TO RISE I IS REACHED I FALLING I PEAK I OF PEAK I PEAK I	I	I	I
I	ARM A	I 15.00 I 45.00 I 75.00 I 0.00 I 0.00 I 0.00 I	I	I	I
I	ARM B	I 15.00 I 45.00 I 75.00 I 1.13 I 1.69 I 1.13 I	I	I	I
I	ARM C	I 15.00 I 45.00 I 75.00 I 2.85 I 4.27 I 2.85 I	I	I	I
I	ARM D	I 15.00 I 45.00 I 75.00 I 0.00 I 0.00 I 0.00 I	I	I	I

I	I	TURNING PROPORTIONS	I
I	I	TURNING COUNTS (VEH/HR)	I
I	I	(PERCENTAGE OF H.V.S)	I
I	TIME	I FROM/TO I ARM A I ARM B I ARM C I ARM D I	I
I	08.00 - 09.30	I I I I I I I	I
I		I ARM A I 0.000 I 0.000 I 0.000 I 0.000 I	I
I		I I?????? I?????? I?????? I?????? I	I
I		I (0.0)I (0.0)I (0.0)I (0.0)I	I
I		I I I I I I I	I
I		I ARM B I 0.411 I 0.000 I 0.000 I 0.589 I	I
I		I I 37.0 I 0.0 I 0.0 I 53.0 I	I
I		I (0.0)I (0.0)I (0.0)I (0.0)I	I
I		I I I I I I I	I
I		I ARM C I 0.956 I 0.000 I 0.000 I 0.044 I	I
I		I I 218.0 I 0.0 I 0.0 I 10.0 I	I
I		I (0.0)I (0.0)I (0.0)I (0.0)I	I
I		I I I I I I I	I
I		I ARM D I 0.000 I 0.000 I 0.000 I 0.000 I	I
I		I I?????? I?????? I?????? I?????? I	I
I		I (0.0)I (0.0)I (0.0)I (0.0)I	I
I		I I I I I I I	I

TURNING PROPORTIONS ARE CALCULATED FROM TURNING COUNT DATA

I	TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)	I
I	08.00-08.15									I
I		B-CD 0.35	7.09	0.050		0.0	0.1	0.7		I
I		B-AD 0.77	7.29	0.106		0.0	0.1	1.7		I
I		A-B 0.00								I
I		A-C 0.00								I
I		A-D 0.00	8.20	0.000		0.0	0.0	0.0		I
I		D-AB 0.00	10.28	0.000		0.0	0.0	0.0		I
I		D-BC 0.00	8.86	0.000		0.0	0.0	0.0		I
I		C-D 0.13								I
I		C-A 2.72								I
I		C-B 0.00	10.89	0.000		0.0	0.0	0.0		I
I										I
I		EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								I
I		MAJOR RD. CENT RES VIS TO LEFT VISIBILITY								I
I	MARGINAL CHANGE:	LANE WIDTH (.1M)	WIDTH (.1M)	WIDTH (.1M)	(M)	(AHEAD FOR MAJOR)	TO RIGHT (M)			I
I		B-CD 0.077	0.002	0.017	0.005		0.007			I
I		B-AD 0.079	0.002	0.017	0.005		0.007			I
I		C-B 0.130	0.000		0.010					I
I		D-AB 0.123	0.003	0.000			0.009			I
I		D-BC 0.106	0.004	0.024	0.006		0.008			I
I		A-D 0.098	0.003		0.009					I

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY	I
---	------	--------	----------	---------	------------	-------	-----	-------	-----------------	---

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH. MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH. MIN/ TIME SEGMENT)
08.45-09.00								
B-CD	0.53	6.85	0.077		0.1	0.1	1.2	
B-AD	1.11	7.15	0.156		0.2	0.2	2.7	
A-B	0.00							
A-C	0.00							
A-D	0.00	7.96	0.000		0.0	0.0	0.0	
D-AB	0.00	10.00	0.000		0.0	0.0	0.0	
D-BC	0.00	8.46	0.000		0.0	0.0	0.0	
C-D	0.18							
C-A	3.99							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
CHANGE:	(.1M)	WIDTH (.1M)	WIDTH (.1M)	(AHEAD FOR MAJOR)		(M) TO RIGHT (M)		
B-CD	0.074	0.002	0.017	0.004		0.007		
B-AD	0.077	0.002	0.017	0.005		0.007		
C-B	0.130	0.000		0.010				
D-AB	0.120	0.005	0.000	0.000		0.009		
D-BC	0.101	0.006	0.024	0.005		0.007		
A-D	0.095	0.004		0.009				

TIME	DEMAND (VEH/MIN)	CAPACITY (VEH/MIN)	DEMAND/ CAPACITY (RFC)	PEDESTRIAN FLOW (PEDS/MIN)	START QUEUE (VEHS)	END QUEUE (VEHS)	DELAY (VEH.MIN/ TIME SEGMENT)	GEOMETRIC DELAY (VEH.MIN/ TIME SEGMENT)
09.15-09.30								
B-CD	0.35	7.09	0.050		0.1	0.1	0.8	
B-AD	0.77	7.29	0.106		0.1	0.1	1.8	
A-B	0.00							
A-C	0.00							
A-D	0.00	8.20	0.000		0.0	0.0	0.0	
D-AB	0.00	10.28	0.000		0.0	0.0	0.0	
D-BC	0.00	8.86	0.000		0.0	0.0	0.0	
C-D	0.13							
C-A	2.72							
C-B	0.00	10.89	0.000		0.0	0.0	0.0	
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:								
MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT	VISIBILITY			
CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)	TO RIGHT			
		(.1M)	(.1M)	(M)	(M)			
B-CD	0.077	0.002	0.017	0.005	0.007			
B-AD	0.079	0.002	0.017	0.005	0.007			
C-B	0.130	0.000		0.010				
D-AB	0.123	0.003	0.000	0.000	0.009			
D-BC	0.106	0.004	0.024	0.006	0.008			
A-D	0.098	0.003		0.009				

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

 QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I	
I	I	I	I	I	* DELAY *	I	* DELAY *	I	
I	I	I	I	I	I	I	I	I	
I	I	(VEH)	(VEH/H)	I	(MIN)	(MIN/VEH)	I	(MIN)	(MIN/VEH)
I	B-CD	I	39.3	I	26.2	I	6.0	I	0.15
I	B-AD	I	84.1	I	56.1	I	13.3	I	0.16
I	A-B	I	0.0	I	0.0	I		I	
I	A-C	I	0.0	I	0.0	I		I	
I	A-D	I	0.0	I	0.0	I	0.0	I	0.00
I	D-AB	I	0.0	I	0.0	I	0.0	I	0.00
I	D-BC	I	0.0	I	0.0	I	0.0	I	0.00
I	C-D	I	13.7	I	9.1	I		I	
I	C-A	I	298.9	I	199.3	I		I	
I	C-B	I	0.0	I	0.0	I	0.0	I	0.00
I	ALL	I	436.0	I	290.7	I	19.3	I	0.04

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

I	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAYI
---	------	--------	----------	---------	------------	-------	-----	-------	------------------

	(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/	
			(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)	
I 08.15-08.30									I
I B-CD	0.29	6.95	0.042		0.0	0.0	0.6		I
I B-AD	0.83	7.19	0.115		0.1	0.1	1.9		I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	8.01	0.000		0.0	0.0	0.0		I
I D-AB	0.00	10.08	0.000		0.0	0.0	0.0		I
I D-BC	0.00	8.66	0.000		0.0	0.0	0.0		I
I C-D	0.37								I
I C-A	3.55								I
I C-B	0.00	10.89	0.000		0.0	0.0	0.0		I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
			(.1M)	(.1M)	(M)		(M)		
I B-CD	0.075	0.002	0.017	0.005	0.007				
I B-AD	0.078	0.002	0.017	0.005	0.007				
I C-B	0.130	0.000	0.010						
I D-AB	0.121	0.004	0.000	0.000	0.009				
I D-BC	0.104	0.005	0.024	0.005	0.008				
I A-D	0.096	0.004	0.009						

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
			(RFC)	(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 08.30-08.45									I
I B-CD	0.36	6.81	0.053			0.0	0.1	0.8	I
I B-AD	1.01	7.10	0.142			0.1	0.2	2.4	I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	7.85	0.000			0.0	0.0	0.0	I
I D-AB	0.00	9.89	0.000			0.0	0.0	0.0	I
I D-BC	0.00	8.42	0.000			0.0	0.0	0.0	I
I C-D	0.46								I
I C-A	4.35								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
			(.1M)	(.1M)	(M)		(M)		
I B-CD	0.074	0.003	0.017	0.004	0.006				
I B-AD	0.077	0.003	0.017	0.005	0.007				
I C-B	0.130	0.000	0.010						
I D-AB	0.118	0.005	0.000	0.000	0.009				
I D-BC	0.101	0.007	0.024	0.005	0.007				
I A-D	0.094	0.005	0.009						

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
			(RFC)	(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 08.45-09.00									I
I B-CD	0.36	6.81	0.053			0.1	0.1	0.8	I
I B-AD	1.01	7.10	0.142			0.2	0.2	2.5	I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	7.85	0.000			0.0	0.0	0.0	I
I D-AB	0.00	9.89	0.000			0.0	0.0	0.0	I
I D-BC	0.00	8.42	0.000			0.0	0.0	0.0	I
I C-D	0.46								I
I C-A	4.35								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
			(.1M)	(.1M)	(M)		(M)		
I B-CD	0.074	0.003	0.017	0.004	0.006				
I B-AD	0.077	0.003	0.017	0.005	0.007				
I C-B	0.130	0.000	0.010						
I D-AB	0.118	0.005	0.000	0.000	0.009				
I D-BC	0.101	0.007	0.024	0.005	0.007				
I A-D	0.094	0.005	0.009						

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
			(RFC)	(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 09.00-09.15									I
I B-CD	0.29	6.95	0.042			0.1	0.0	0.7	I
I B-AD	0.83	7.19	0.115			0.2	0.1	2.0	I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	8.01	0.000			0.0	0.0	0.0	I
I D-AB	0.00	10.08	0.000			0.0	0.0	0.0	I
I D-BC	0.00	8.66	0.000			0.0	0.0	0.0	I
I C-D	0.37								I
I C-A	3.55								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
			(.1M)	(.1M)	(M)		(M)		
I B-CD	0.075	0.002	0.017	0.005	0.007				
I B-AD	0.078	0.002	0.017	0.005	0.007				
I C-B	0.130	0.000	0.010						
I D-AB	0.121	0.004	0.000	0.000	0.009				
I D-BC	0.104	0.005	0.024	0.005	0.008				
I A-D	0.096	0.004	0.009						

	TIME	DEMAND	CAPACITY	DEMAND/	PEDESTRIAN	START	END	DELAY	GEOMETRIC DELAY
		(VEH/MIN)	(VEH/MIN)	CAPACITY	FLOW	QUEUE	QUEUE	(VEH.MIN/	(VEH.MIN/
			(RFC)	(RFC)	(PEDS/MIN)	(VEHS)	(VEHS)	TIME SEGMENT)	TIME SEGMENT)
I 09.15-09.30									I
I B-CD	0.24	7.06	0.034			0.0	0.0	0.5	I
I B-AD	0.70	7.26	0.096			0.1	0.1	1.6	I
I A-B	0.00								I
I A-C	0.00								I
I A-D	0.00	8.12	0.000			0.0	0.0	0.0	I
I D-AB	0.00	10.21	0.000			0.0	0.0	0.0	I
I D-BC	0.00	8.83	0.000			0.0	0.0	0.0	I
I C-D	0.31								I
I C-A	2.97								I
I C-B	0.00	10.89	0.000			0.0	0.0	0.0	I
EFFECT ON CAPACITY (PCU/MIN) OF MARGINAL CHANGES IN:									
	MARGINAL	LANE WIDTH	MAJOR RD.	CENT RES	VIS TO LEFT		VISIBILITY		
	CHANGE:	(.1M)	WIDTH	WIDTH	(AHEAD FOR MAJOR)		TO RIGHT		
			(.1M)	(.1M)	(M)		(M)		
I B-CD	0.076	0.002	0.017	0.005	0.007				
I B-AD	0.079	0.002	0.017	0.005	0.007				
I C-B	0.130	0.000	0.010						
I D-AB	0.122	0.004	0.000	0.000	0.009				
I D-BC	0.106	0.005	0.024	0.005	0.008				
I A-D	0.097	0.003	0.009						

QUEUE FOR STREAM B-CD

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.1
09.00	0.1
09.15	0.0
09.30	0.0

QUEUE FOR STREAM B-AD

TIME SEGMENT	NO. OF
ENDING	VEHICLES
	IN QUEUE
08.15	0.1
08.30	0.1
08.45	0.2
09.00	0.2
09.15	0.1
09.30	0.1

QUEUE FOR STREAM A-D

TIME SEGMENT	NO. OF
--------------	--------

ENDING	VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM D-AB

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM D-BC

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUE FOR STREAM C-B

TIME SEGMENT ENDING	NO. OF VEHICLES IN QUEUE
08.15	0.0
08.30	0.0
08.45	0.0
09.00	0.0
09.15	0.0
09.30	0.0

QUEUEING DELAY INFORMATION OVER WHOLE PERIOD

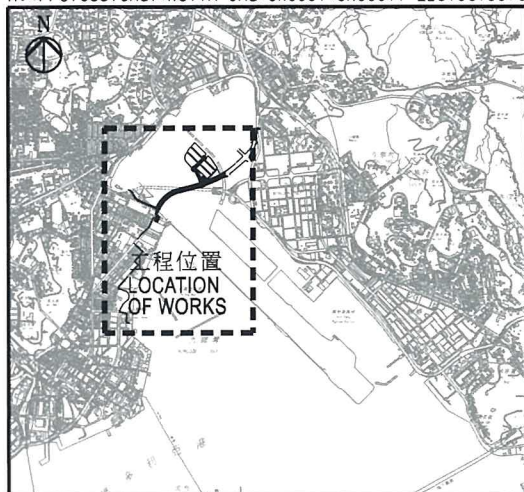
I	STREAM	I	TOTAL DEMAND	I	* QUEUEING *	I	* INCLUSIVE QUEUEING *	I	
I	I	I	I	I	* DELAY *	I	* DELAY *	I	
I	I	I	(VEH)	(VEH/H)	(MIN)	(MIN/VEH)	(MIN)	(MIN/VEH)	
I	B-CD	I	26.8	I	17.9	I	4.0	I	0.15
I	B-AD	I	76.0	I	50.7	I	11.9	I	0.16
I	A-B	I	0.0	I	0.0	I	I	I	I
I	A-C	I	0.0	I	0.0	I	I	I	I
I	A-D	I	0.0	I	0.0	I	0.0	I	0.00
I	D-AB	I	0.0	I	0.0	I	0.0	I	0.00
I	D-BC	I	0.0	I	0.0	I	0.0	I	0.00
I	C-D	I	34.3	I	22.9	I	I	I	I
I	C-A	I	326.3	I	217.6	I	I	I	I
I	C-B	I	0.0	I	0.0	I	0.0	I	0.00
I	ALL	I	463.5	I	309.0	I	15.9	I	0.03

* DELAY IS THAT OCCURRING ONLY WITHIN THE TIME PERIOD .
 * INCLUSIVE DELAY INCLUDES DELAY SUFFERED BY VEHICLES WHICH ARE STILL QUEUEING AFTER THE END OF THE TIME PERIOD.
 * THESE WILL ONLY BE SIGNIFICANTLY DIFFERENT IF THERE IS A LARGE QUEUE REMAINING AT THE END OF THE TIME PERIOD.

END OF JOB

Annex D

Road Layout of Planned Road (L19)



位置平面圖
KEY PLAN



啟德隧道
KAI TAK TUNNEL

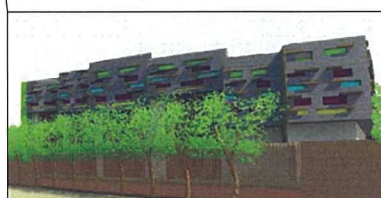
擬建的L6路
PROPOSED ROAD L6

連接將來的排水系統
(工務計劃項目第469CL號)
TO CONNECT DRAINAGE
SYSTEM IN FUTURE
(PWP ITEM NO. 469CL)

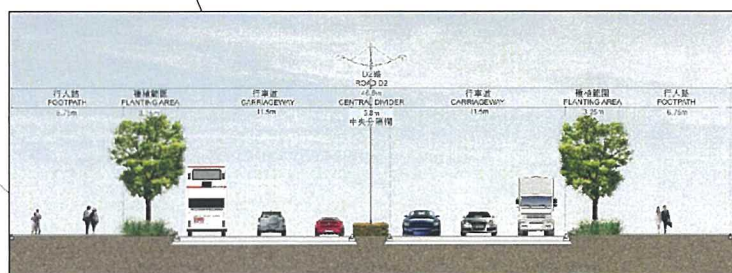
擬建的D2路
PROPOSED ROAD D2

連接擬建的D2路排水系統
IN PROPOSED ROAD D2

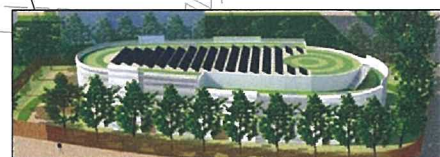
九龍東旱流污水截流泵房
KOWLOON EAST DRY WEATHER FLOW
INTERCEPTOR PUMPING STATION



擬建的污水泵站 NPS 構想圖
ARTIST'S IMPRESSION OF PROPOSED SEWAGE PUMPING STATION NPS



擬建的 D2 路構想圖
ARTIST'S IMPRESSION OF PROPOSED ROAD D2



擬建的污水泵站 PS2 構想圖
ARTIST'S IMPRESSION OF PROPOSED SEWAGE PUMPING STATION PS2

連接即將興建的排水系統
(工務計劃項目第167CD號)
TO CONNECT DRAINAGE SYSTEM
SOON UNDER CONSTRUCTION
(PWP ITEM NO. 167CD)

建造中的道路工程
(工務計劃項目第746CL號)
ROAD WORKS UNDER
CONSTRUCTION
(PWP ITEM NO. 746CL)

往來九龍灣交通方向
TO AND FROM
KOWLOON BAY

圖例
LEGEND:

- 擬建的行車道 / 現有的行車道將予重建或擴闊
PROPOSED CARRIAGEWAY / EXISTING CARRIAGEWAY TO BE RECONSTRUCTED OR WIDENED
- 擬建的 / 重建的行人路, 中央分隔帶, 安全島或種植範圍
PROPOSED / RECONSTRUCTED FOOTPATH, CENTRAL MEDIAN, TRAFFIC ISLAND OR PLANTING AREA
- 擬建的污水泵站
PROPOSED SEWAGE PUMPING STATION
- 在現有行車道和擬建的L19路上鋪設無壓力污水管 / 加壓污水管
PROPOSED GRAVITY SEWER / RISING MAIN TO BE LAID IN EXISTING ROAD AND PROPOSED ROAD L19
- 擬建的箱形暗渠
PROPOSED BOX CULVERT
- 行車道的行車線 (每一箭頭表示一條行車線)
TRAFFIC LANE FOR CARRIAGEWAY (ONE ARROW REPRESENTS ONE LANE)
- 住宅發展用地
SITE FOR RESIDENTIAL DEVELOPMENT

圖則名稱 plan title

啟德發展計劃
前北面停機坪第4期基礎設施
AT FORMER NORTH APRON AREA

圖則編號 drawing no.

SK0001

比例 scale
不按比例
NOT TO SCALE

辦事處 office

九龍拓展處
KOWLOON DEVELOPMENT OFFICE

土木工程拓展署
CIVIL ENGINEERING
AND DEVELOPMENT DEPARTMENT

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PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 3

Environmental Assessment Report



**Term Environmental Consultancy Services
For a Project at
Hung Fook Street / Ngan Hon Street, Kowloon City District
(KC-010)**

Environmental Assessment

Prepared for:
Urban Renewal Authority

Prepared by:
Ramboll Environ Hong Kong Limited

Date:
June 2016

Reference Number:
R5152_V1.0_20160601

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1. Introduction

1.1 Background and Objectives

- 1.1.1 The Urban Renewal Authority (URA) has proposed the Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street in Kowloon City.
- 1.1.2 Ramboll Environ Hong Kong Limited has been appointed by the Urban Renewal Authority (URA) to conduct this Environmental Assessment for submission to the Town Planning Board for approval.

1.2 Subject Site and its Environs

- 1.2.1 The Subject Site is bounded by Hung Fook Street to the South, Wing Kwong Street to the East, Ngan Hon Street to the North and a back lane of the adjacent buildings to the West. The existing buildings within the Subject Site were built between 1957 and 1960, and range from 6 storeys to 8 storeys high. The buildings are mainly residential in nature with commercial premises at the ground floor.
- 1.2.2 **Figure 1** shows the location of the Subject Site and its environs.

1.3 Proposed Redevelopment

- 1.3.1 The proposed redevelopment to be built in the Subject Site will be built on the area zoned Residential (Group A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. The adjacent public pavement area shown as "Road", will not be built upon and it will be excluded from calculation for plot ratio purposes.
- 1.3.2 In line with the "Residential (Group A)" zoning, the redevelopment intention includes both residential and commercial uses. The subject redevelopment covers a gross site area of approximately 4,951m², with an approximate domestic gross floor area of 34,215m² and non-domestic gross floor area of 6,843m².
- 1.3.3 Three 23-storey residential towers are proposed with a total of 759 units, and a common clubhouse will be shared among the residential towers.

2. Noise Impact Assessment

2.1 Scope of Work

- 2.1.1 The Subject Site is zoned as R(A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. Under the R(A) zone, residential development is always permitted for the Subject Site.
- 2.1.2 The aim of this study is to review the potential noise impacts (i.e. road traffic noise impact and fixed noise impact) on the noise sensitive uses of the proposed redevelopment, and to recommend mitigation measures where practicable to attenuate the impact.
- 2.1.3 There is no railway noise source identified in the surrounding so that no impact is anticipated on the proposed development.

2.2 Road Traffic Noise Impact Assessment

2.2.1 Assessment Criteria

- 2.2.1.1 Noise standards are recommended by the Hong Kong Planning Standards and Guideline (HKPSG) for planning against possible noise impact from road traffic. Under the HKPSG, the criterion for road traffic noise impact on domestic premises (habitable rooms) is $L_{10(1\text{-hour})}$ 70dB(A). This criterion applies to uses which rely on opened windows for ventilation.

2.2.2 Discussion

- 2.2.2.1 The roads surrounding the Subject Site, namely To Kwa Wan Road, Ngan Hon Street, Wing Kwong Street, and Hung Fook Street, are considered to be the dominant sources contributing to traffic noise impact on the NSRs. Other roads in the vicinity are also considered to affect the Subject Site.
- 2.2.2.2 Under the traffic noise criteria of 70dB(A), many of the residential units are expected to experience noise levels that exceed the noise criterion. In order to reduce the road traffic noise impacts, the following generic mitigation measures would be considered for the redevelopment:

Non-Noise Sensitive Use as Noise Barrier

- 2.2.2.3 As proposed, there shall be three floors for retail purpose and also space for the provision of a clubhouse. These retail and clubhouse areas are considered as non-noise sensitive use and shall be located in the podium (lower floors) of the building to act as a noise shield structure. At the same time, noise tolerant portions (including bathrooms, stairwells and lift cores) shall be arranged to face the noise source.

Increase of Buffer Distance

- 2.2.2.4 Residential blocks shall be placed above the podium, including retail and clubhouse facilities, so as to increase the buffer distance from the residential floors to the nearby traffic noise source.

Orientation

- 2.2.2.5 The openable windows of the façade facing the major roads (i.e. To Kwa Wan Road, Ngan Hon Street, Wing Kwong Street, and Hung Fook Street) shall be oriented at an angle to minimize exposure to road traffic noise.

Architectural Fin

- 2.2.2.6 Full-height vertical acoustic fin shall be applied to the opening of certain habitable rooms to further reduce the view angle to the traffic noise source.

Openable Window for Habitable Room/ Recessed Window

- 2.2.2.7 Openable windows for ventilation shall be located on the side with lower noise levels while fixed windows are positioned at protruded rooms facing major traffic noise source. Recessed windows shall also be adopted for habitable rooms to effectively resist noise from entering residential units directly.
- 2.2.2.8 A detailed quantitative traffic noise impact assessment will be conducted and the project specific mitigation measures will be proposed once traffic data is available, to demonstrate that the proposed redevelopment would not be subject to adverse traffic noise impact.

2.3 Fixed Noise Impact Assessment

2.3.1 Introduction

- 2.3.1.1 The aim of this study is to review potential noise impacts of the fixed noise sources within 300m on the proposed development. Practicable noise mitigation measures would be recommended, where necessary.

2.3.2 Assessment Criteria

- 2.3.2.1 The subject site is located in an urban area and surrounded by To Kwa Wan Road to the northwest, Ngan Hon Street to the northeast, Wing Kong Street to the southeast and Hung Fook Street to the southwest. Moreover, Ma Tau Wai Road (from Chi Kiang Street to Tin Kwong Road) and East Kowloon Corridor Flyover (from Ma Tau Kok to Chatham Road North) are in close proximity to the subject site, which consist of Annual Average Daily Traffic (AADT) of 36,770 and 77,910 respectively, thus are considered as influencing factors that are directly affecting the proposed development. As a result, the area sensitive rating (ASR) "C" is adopted as the assessment criteria. **Table 1** below shows the details of the fixed noise impact assessment criteria for this study.

Table 1 Industrial Noise Standard

Standards	Criteria	Acceptable Noise Level (ANL)*	Noise Criteria in Current Assessment
NCO	Leq (30 minutes) (07:00 – 23:00)	70 dB(A)	70 dB(A)
NCO	Leq (30 minutes) (23:00 – 07:00)	60 dB(A)	60 dB(A)

* Acceptable Noise Level for Area Sensitivity Ratings "C" stipulated in the Technical Memorandum for the Assessment from Places other than Domestic Premises, Public Places or Construction Sites.

2.3.1 Fixed Noise Sources

- 2.3.1.1 Existing noise sources along Ngan Hon Street and Wing Kwong Street mainly include garages, workshops for car washing and car painting and metal workshops. There are also two industrial buildings at both ends of the Ngan Hon Street, including the World Wide Industrial Building at the junction of the Ngan Hon Street and To Kwa Wan Road and the Fung Yu Industrial Building behind the Sunrise Villa. Site visits were carried out on 26 Apr 16 and 05 May 16 to measure the noise level of these noise sources. From the site visits, there is no industrial noise sources identified at Fung Yu Industrial Building and World Wide Industrial Building. The two industrial buildings are totally enclosed and only equipped with ordinary window type air conditioners (L1 and L2 in **Appendix A2**). No cooling towers or chillers were observed. For the garages, workshops for car washing and car painting and metal workshops (hereafter named as noise sources), no night-time operation of these noise sources were observed during the site visits. During the site visits, there were no noisy activities of these noise sources. Therefore, for the assessment of the potential noise impact from these noise sources to the proposed development, the sound power levels of these noise sources are referenced from the Environmental Assessment Report (EAS) for the proposed residential development at Hong Ting Road (Town Planning Board Application No. A/SK-SK/10 which was approved on 17 Apr 2015) where the noise sources are of similar nature and size. The following table (**Table 2**) shows the comparison of the typical workshop at Hong Ting Road and that along Ngan Hon Street and Wing Kwong Street. The maximum sound power level of the Auto Services / Workshops adopted for the Fixed Noise Impact Assessment for the Proposed Development at Hong Ting Road were between 85.2 dB(A) and 89.1 dB(A). For conservatism, the maximum sound power level of 89.1 dB(A) is adopted for all the other Auto Services / Workshops for the Fixed Noise Assessment of this subject site as a worst case scenario. The locations of fixed noise sources at Ngan Hon Street and Wing Kwong Street are indicated in **Figure 2**. Site survey photo record can be found in **Appendix A1**.

Table 2 Comparison between the Types of Auto Services / Workshops nearby the Subject Site and for the Proposed Residential Development at Hong Ting Road

Typical Auto Service / Workshop along Ngan Hon Street and Wing Kwong Street	Auto Service / Workshop identified for the EAS for the Proposed Residential Development at Hong Ting Road
	

2.3.2 Assessment Methodology

- 2.3.2.1 Standard acoustical principles were adopted for prediction of cumulative industrial noise impact. In accordance with the “*Technical Memorandum On Noise From Construction Work Other Than Percussive Piling*”, a – 10dB shielding correction was adopted where the line of sight from the representative NSR would be completely blocked by buildings or barriers. A façade correction of + 3dB was assumed. Corrections for tonality, intermittency or impulsiveness shall be applied, where necessary.

2.3.3 Noise Sensitive Receivers

- 2.3.3.1 Representative Noise Sensitive Receivers (NSRs) (NA-M1, NA-J1, NA-G4, NC-A4, NC-D1 and NC-E1) of residential towers nearest to the identified noise sources have been selected for the assessment. **Figure 3** shows the locations of the representative NSRs.

2.3.4 Assessment Result

- 2.3.4.1 A summary of predicted fixed noise levels during day time at selected NSRs is tabulated below. Detailed calculations are shown in **Appendix A2**.

Table 3 Predicted Industrial Noise Level

Representative Noise Sensitive Receiver	Predicted Noise Level, dB(A) (Day-time)
NA-M1	56 dB(A)
NA-J1	58 dB(A)
NA-G4	59 dB(A)
NC-A4	65 dB(A)

Representative Noise Sensitive Receiver	Predicted Noise Level, dB(A) (Day-time)
NC-D1	65 dB(A)
NC-E1	58 dB(A)

2.3.4.2 The assessment results indicate that the maximum predicted noise levels are 66 dB(A) for day-time, which is well below the 70 dB(A) day-time criterion.

2.3.5 Conclusion

2.3.5.1 The potential noise impact from fixed noise sources has been assessed based on the proposed development scheme.

2.3.5.2 According to the assessment results, the proposed development will not be subject to adverse noise impact from fixed noise sources.

3. Air Quality Impact Assessment

3.1 Scope of Work

- 3.1.1 The aim of this study is to assess the potential air quality impact arising from traffic emissions along the road carriageways and chimney emission from industrial stack surrounding the subject site during the operation of the proposed redevelopment.

3.2 Assessment Criteria

- 3.2.1 The Hong Kong Planning Standards and Guidelines (HKPSG) has provided a set of guidelines to assess the potential air quality impacts generated from traffic. **Table 4** below is extracted from Table 3.1 in Chapter 9 of HKPSG, where the minimum horizontal buffer distance between kerb side of roads and sensitive uses for various types of road is given.

Table 4 Guidelines on Usage of Open Space Site (HKPSG Chapter 9: Environment)

Pollution Source	Parameter	Buffer Distance	Permitted Uses
Road and Highways	<i>Type of Road</i>		
	Trunk Road and Primary Distributor	>20 m	Active and passive recreation uses
		3 – 20 m	Passive recreational uses
		<3 m	Amenity areas
	District Distributor	>10 m	Active and passive recreation uses
		<10 m	Passive recreational uses
	Local Distributor	>5 m	Active and passive recreation uses
		<5 m	Passive recreational uses
	Under Flyovers		Passive recreational uses
Industrial Areas	<i>Difference in Height between Industrial Chimney Exit and the Site</i>		
	<20 m	>200 m	Active and passive recreation uses
		5 – 200 m	Passive recreational uses
	20 – 30 m (*)	>100 m	Active and passive recreation uses
		5 – 100 m	Passive recreational uses
	30 – 40 m	>50 m	Active and passive recreation uses
		5 – 50 m	Passive recreational uses
	>40 m	>10 m	Active and passive recreation uses
Construction and earth moving Activities	-	<50 m	Passive recreational uses
		>50 m	Active and passive recreational uses

Remarks :

- (a) According to the Appendix 3.2: Principal Framework for Planning against Air Pollution of Chapter 9 Environment of Hong Kong Planning Standard and Guideline, Air Sensitive uses include: residential areas, child care centres, residential care homes for the elderly, hospitals and clinics, schools and active recreational activities.
- (b) In situations where the height of chimneys is not known, use the set of guidelines marked

with an asterisk for preliminary planning purpose and refine as and when more information is available.

- (d) The buffer distance is the horizontal, shortest distance from the boundary of the industrial lot, the position of existing chimneys or the edge of road kerb, to the boundary of open space sites.
- (e) The guidelines are generally applicable to major industrial areas but NOT individual large industrial establishments which are likely to be significant air pollution sources. Consult EPD when planning open space sites close to such establishments.
- (f) Amenity areas are permitted in any situation.

3.3 Air Quality Impact Assessment Methodology

3.3.1 The Subject Site is bounded three roads, which are Ngan Hon Street, Wing Kwong Street, and Hung Fook Street, which are all listed as Local Distributors.

3.3.2 According to Table 3 in Chapter 9 of HKPSG, a horizontal buffer separation of >5m is recommended between kerb side of the Local Distributor (LD) and the air sensitive uses.

3.3.3 For all three roads, the buffer distance between the road and Towers B and C of the proposed redevelopment is slightly less than the HKPSG's recommendation. However, it must be emphasized that there is a podium which consists of three storeys of retail and one storey of clubhouse underneath the residential block. Also, the lowest residential unit is at 27.55mPD, while Ngan Hon Street, Wing Kwong Street, and Hung Fook Street are at ~5.05mPD, i.e. there is at least ~22m vertical distance from the road.

3.3.4 It is anticipated that the concentration of the air pollutant from the vehicular emission will be reduced with the increase of buffer distance, in particular vertical buffer distance. Therefore, taking into account of the sufficient vertical buffer distance which is longer than that of the HKPSG horizontal buffer distance, it is anticipated that the residential units of the proposed redevelopment would not be subject to unacceptable vehicular emission impact.

3.4 Summary

3.4.1 Since the buffer distance of the proposed redevelopment comply with the relevant vehicular emission buffer distance as stipulate din Chapter 9 – Environment of HKPSG, it is anticipated that the proposed redevelopment would not be subject to unacceptable vehicular emission impact.

4. Land Contamination Review

4.1 Scope of Work

- 4.1.1 The aim of this study is to assess the potential land contamination impact at the Subject Site due to previous land uses and/or the existing operations.

4.2 Assessment Criteria

- 4.2.1 The following guidelines published by EPD have been followed:

- Guidance Manual for Use of Risk-Based Remediation Goals for Contaminated Land Management (RBRGs), dated December 2007;
- Guidance Note for Contaminated Land Assessment and Remediation, dated 15 August 2007; and
- Practice Guide for Investigation and Remediation of Contaminated Land (EPD's Practice Guide), dated August 2011.

- 4.2.2 As the RBRGs and the Practice Guide are the latest guidelines promulgated for use in August 2007 and August 2011 respectively, the RBRGs criteria and the requirements stated in the Practice Guide will be adopted in this Land Contamination Review.

4.3 Review of Historical and Current Available Information

Landuses

- 4.3.1 According to the Hong Kong Geological Survey Map (Series: HGM20) – Sheet No. 11 (1:20,000 scale) on the solid and superficial geology in To Kwa Wan district, the Subject Site was reclaimed in 1924. The Subject Site is comprised mainly of marine sand. The Geological Survey Map for the Subject Site is captured and presented in **Appendix B1**.
- 4.3.2 The Subject Site is currently located in an area consisting of low to mid-rise residential buildings. According to the aerial photograph taken in 1945 by Lands Department, the Subject Site is situated in an urban built area with buildings.
- 4.3.3 In the aerial photograph taken in 1967 by lands Department, buildings were already visible within the Subject Site, and such buildings have remained unchanged till present day.
- 4.3.4 **Table 4** below presents the landuse summary of the Subject Site. The aerial photos of Year 1945, 1967, 1995, and 2016 are attached in **Appendix B2**.

Table 5 Landuse Summary of the Subject Site

Period / Year	Landuse / Description	Owner or occupier	Source(s) of Information	Off-site Property Affected?
Before 1945	Reclamation work was launched in To Kwa Wan in the 1910s to 1920s. The reclamation works for the Subject Site was completed in 1924.	No information available.	Hong Kong Geological Survey.	No.
1945	The Subject Site is situated in an urban built area.	No information available.	Aerial Photograph from Lands Department.	No.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.

Site Inspection and Observation

- 4.3.5** A site visit was conducted in April 2016. From the site visit, observations included car repairing workshops, hardware stores, and small retail stores, etc. There were no aboveground / underground oil storage tanks, chemicals and dangerous goods observed to be stored on site during the site visit.

Discussion

- 4.3.6** A land contamination assessment has been conducted for the Subject Site. Background information including geosocial information obtained from Hong Kong Geological Survey and selected aerial photos from Land Department has been reviewed and site survey has been conducted to identify the potential land contamination in the Subject Site.
- 4.3.7** It is observed that the Subject Site is situated on reclaimed land from the 1920s. The Subject Site was then developed into an urban area and the current buildings have remained since its erection from 1957 to 1960. However, it is unable to identify the building usage and there may be a change in landuse over time.
- 4.3.8** Based on the observations during site visit, the ground floor of the Subject Site is currently comprised of several car repairing workshops, hardware stores, and small retail stores, while the upper floors are of residential use. Since car repairing workshops may release oils and fuels and lubricant from vehicles during vehicle and equipment maintenance and refueling, potential land contamination issues are present at the Subject Site.
- 4.3.9** Therefore, a Contamination Assessment Plan (CAP) and subsequently, Contamination Assessment Report (CAR), and Remediation Action Plan (RAP) will be prepared in later stages to identify the potential land contamination issues in the Subject Site.

5. Overall Conclusion

- 5.1.1** The Subject Site is proposed to be redeveloped into a 759-unit residential complex. The potential air quality, noise impact, and land contamination issues have been qualitatively addressed.
- 5.1.2** Based on the air quality impact assessment, it is concluded that there is adequate vertical buffer distance and the proposed redevelopment will not be subject to adverse vehicular emission impact.
- 5.1.3** Based on the noise impact assessment, it is concluded that with noise mitigation measures such as disposition and orientation of the building towers and vertical fins provided, the proposed redevelopment will not be subject to adverse traffic noise impact.
- 5.1.4** Based on the land contamination review on historical landuses from aerial photographs, there are potential land contamination issues associated with the car repairing workshops on the ground floor of the Subject Site. Contamination Assessment Plan and subsequent reports will be prepared in later stages.
- 5.1.5** The environmental assessment confirms the feasibility of the proposed redevelopment.

FIGURES

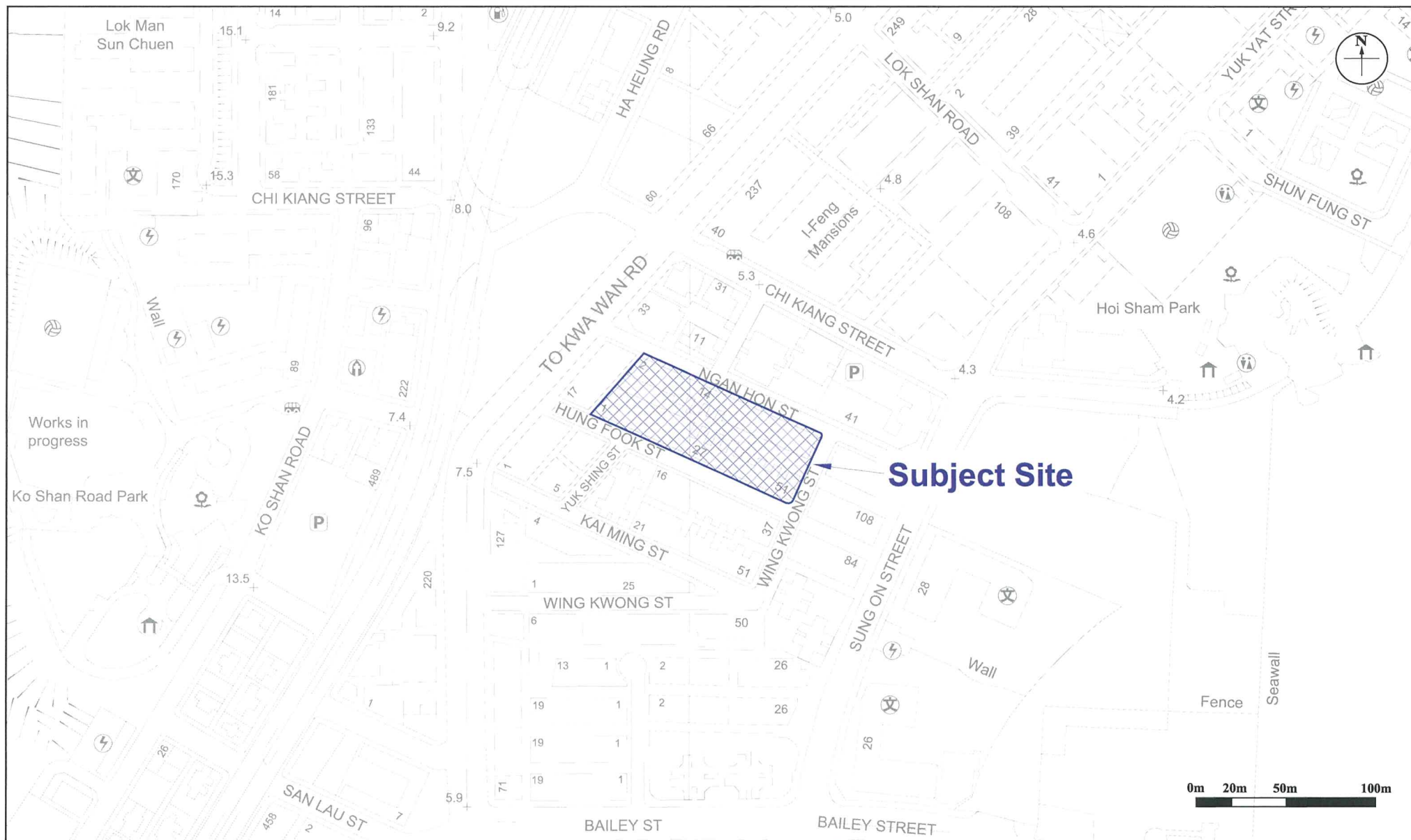


Figure: 1

Title: Subject Site and its Environ

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: MW

Checked by: DY

Rev.: 1.0

Date: May 2016



Figure: 2

Title: Locations of Fixed Noise Sources

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

Drawn by: MW

Checked by: DY

Rev.: 1.0

Date: May 2016

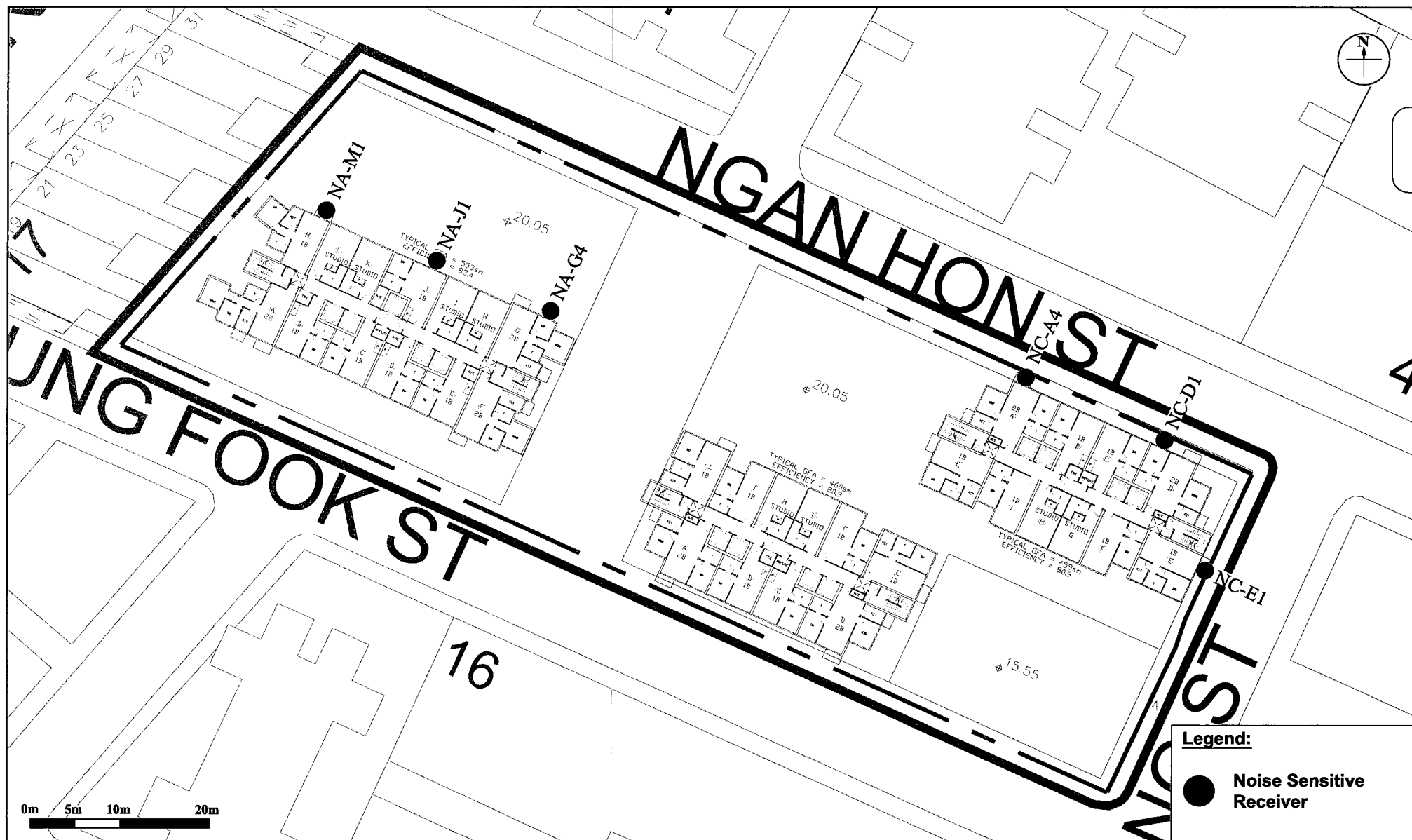


Figure: 3

Title: Locations of Noise Sensitive Receivers for Fixed Noise Impact Assessment

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City



Drawn by: MW




Checked by: DY





Rev.: 1.0

Date: May 2016

APPENDIX A1: SITE SURVEY PHOTO RECORD

ID	Noise Source	Photos
S1	Fuji Engineering (H.K.) Company Limited	
S2	Jointly Motors Centre Vehicles Maintenance	
S3	BP Car Shop Vehicles Maintenance & Repairing	

ID	Noise Source	Photos
S4	Top Gear Vehicles Maintenance & Repairing	
S5	Kowloon Auto Service & Maintenance Company	
S6	Yau Shing Auto Service & Maintenance Company Limited	

ID	Noise Source	Photos
S7	Auto Service Vehicles Maintenance & Repairing	
S8	MEGA Racing Centre	
S9	Recycling Workshop	
S10	Chun Tung Garage Vehicles Maintenance & Repairing	

ID	Noise Source	Photos
L1	The World-wide Industrial Building	
L2	Fung Yu Industrial Building	

APPENDIX A2: DETAILED CALCULATIONS OF FIXED NOISE IMPACT ASSESSMENT

Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (20 min)
NA-M1	837467.9867	819505.5848	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	35.8	-39.1	0	3	0	53.03
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	61.7	-43.8	0	3	0	48.29
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	68.1	-44.7	0	3	0	47.44
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	88.3	-46.9	0	3	0	45.18
				S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	101.5	-48.1	0	3	0	43.97
				S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	110.5	-48.9	0	3	0	43.23
				S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	131.8	-50.4	0	3	0	41.70
				S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	137.9	-50.8	0	3	0	41.31
				S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	123.3	-49.8	-10	3		32.28

Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (28 min)
NA-J1	837480.3581	819499.9254	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	31.8	-38.0	0	3	0	54.05
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	50.9	-42.1	0	3	0	49.97
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	56.7	-43.1	0	3	0	49.03
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	75.8	-45.6	0	3	0	46.50
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	88.7	-47.0	0	3	0	45.14	
			S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	97.6	-47.8	0	3	0	44.31	
			S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	118.7	-49.5	0	3	0	42.61	
			S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	124.4	-49.9	0	3	0	42.21	
			S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	109.7	-48.8	-10	3	0	33.30	
			Total														

Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (30 min)	
NA-G4	837493.1394	819494.2592	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	33.3	-38.5	0	3	0	53.65	
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	41.3	-40.3	0	3	0	51.78	
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	46.1	-41.3	0	3	0	50.82	
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	63.4	-44.0	0	3	0	48.06	
				S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	75.8	-45.6	0	3	0	46.50	
				S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	84.6	-46.5	0	3	0	45.56	
				S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	105.3	-48.4	0	3	0	43.66	
				S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	110.5	-48.9	0	3	0	43.24	
				S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	95.7	-47.6	-10	3	0	34.48	

Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (20 min)	
NC-A4	837549.5147	819485.3691	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	67.8	-44.6	0	3	0	47.47	
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	35.8	-39.1	0	3	0	53.02	
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	29.8	-37.5	0	3	0	54.62	
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	16.6	-32.4	0	3	0	59.68	
				S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	21.7	-34.7	0	3	0	57.39	
				S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	29.0	-37.2	0	3	0	54.86	
				S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	48.3	-41.7	0	3	0	50.43	
				S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	54.8	-42.8	0	3	0	49.33	
				S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	42.4	-40.6	-10	3	0	41.55	

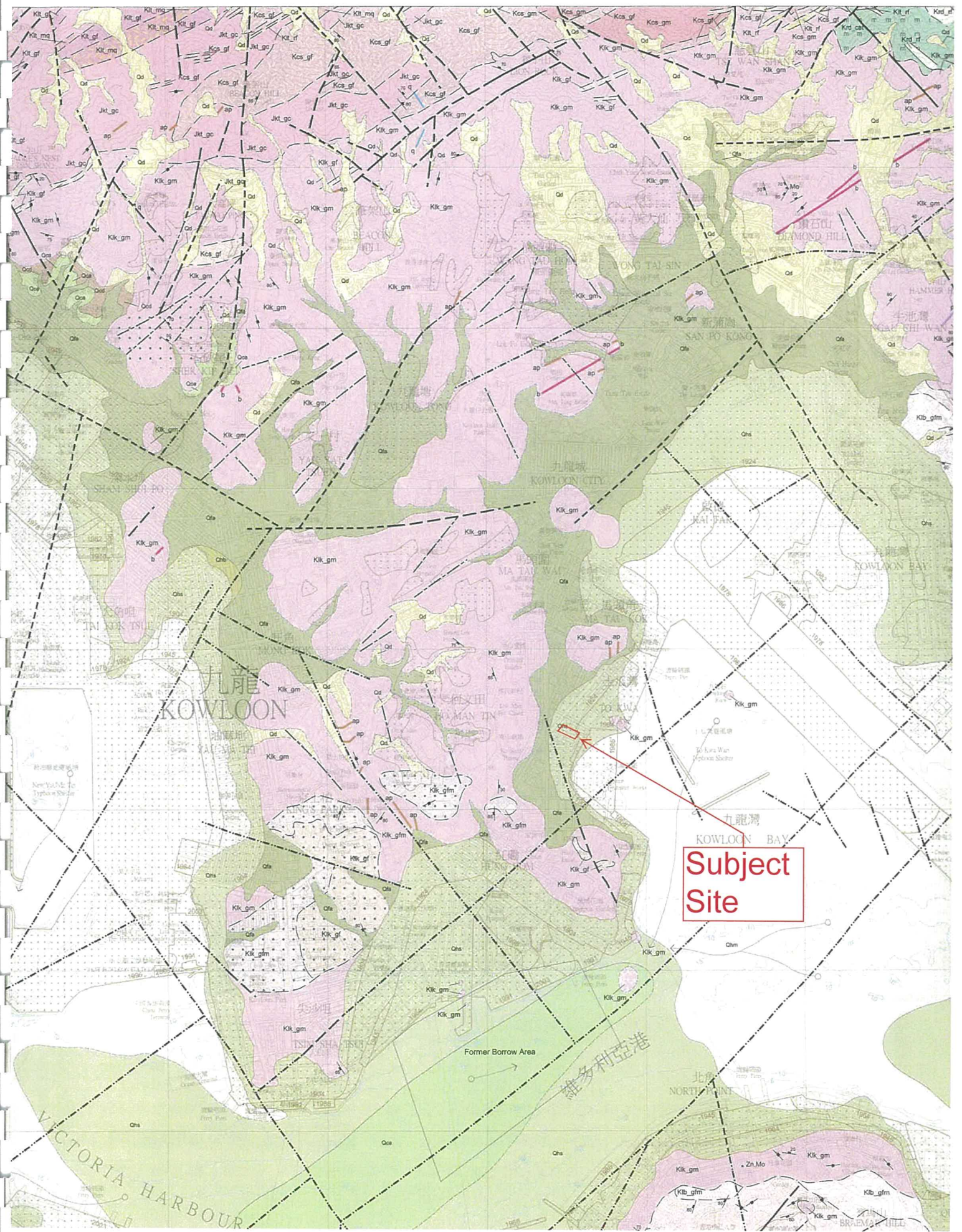
Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (20 min)
NC-D1	837561.9496	819479.6749	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	81.2	-46.2	0	3	0	45.91
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	48.1	-41.6	0	3	0	50.46
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	41.4	-40.3	0	3	0	51.76
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	21.7	-34.7	0	3	0	57.36
				S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	16.8	-32.5	0	3	0	59.61
				S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	20.0	-34.0	0	3	0	58.06
				S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	35.8	-39.1	0	3	0	53.03
				S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	41.1	-40.3	0	3	0	51.82
				S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	29.6	-37.4	-10	3		44.68
Total																65	

Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	NSR (z)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	Noise Source (z)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, dB(A), L_{eq} (20 min)
NC-E1	837566.5163	819464.9697	0	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	0	89.1	1	89.1	94.0	-47.5	-10	3	0	34.64
				S2	Jointly Motors Centre	837529.0149	819514.7237	0	89.1	1	89.1	62.3	-43.9	-10	3	0	38.21
				S3	BP Car Shop	837535.8049	819511.7967	0	89.1	1	89.1	56.0	-43.0	-10	3	0	39.14
				S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	0	89.1	1	89.1	37.1	-39.4	-10	3	0	42.71
				S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	0	89.1	1	89.1	30.0	-37.6	-10	3	0	44.55
				S7	Auto Service	837577.6806	819492.092	0	89.1	1	89.1	29.3	-37.3	0	3	0	54.75
				S8	Fung Yu Industrial Building	837597.6569	819481.8427	0	89.1	1	89.1	35.4	-39.0	0	3	0	53.12
				S9	Recycling Workshop	837598.2857	819460.4365	0	89.1	1	89.1	32.1	-38.1	0	3	0	53.97
				S10	Chun Tung Garage	837581.4646	819457.4575	0	89.1	1	89.1	16.7	-32.5	0	3	0	59.63
				Total													

APPENDIX B1: HONG KONG GEOLOGICAL SURVEY



APPENDIX B2: AERIAL PHOTOS

Historical Aerial Photo – Year 1945



Subject Site

Height of Aerial Photographs: 20,000 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 1967

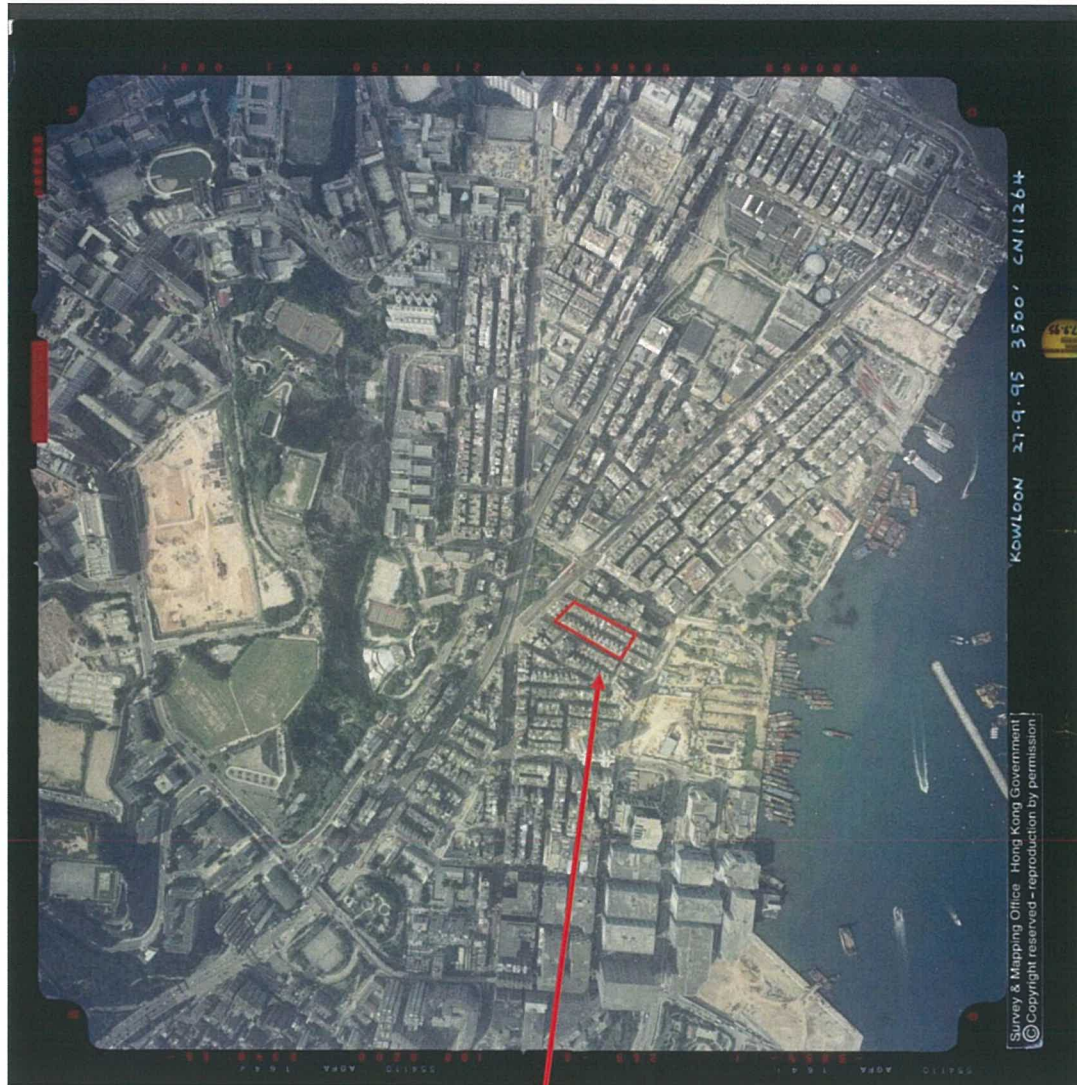


Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 1995

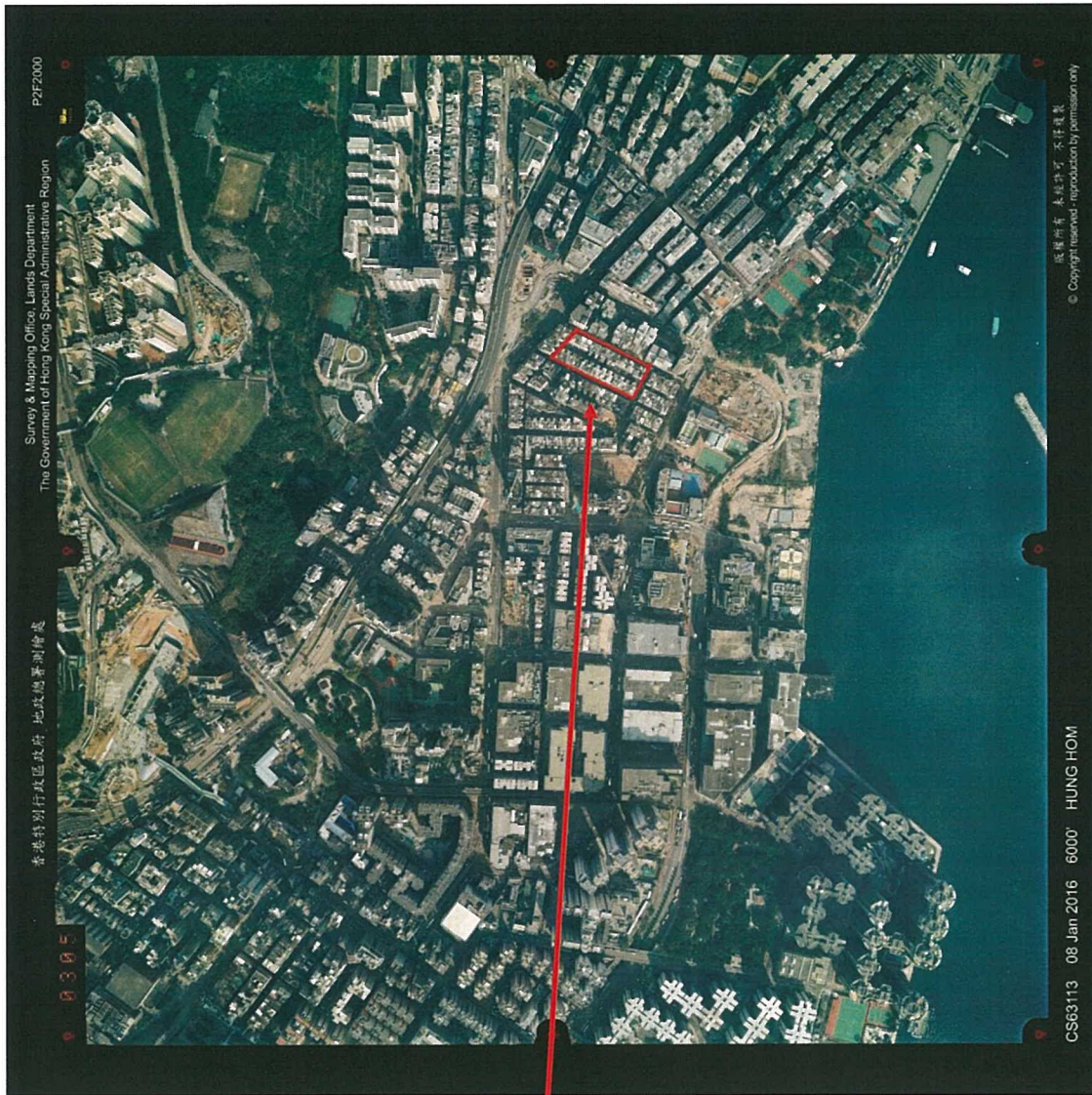


Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 2016



Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 4

**Drainage Impact Assessment / Sewerage
Impact Assessment Report**



**Term Environmental Consultancy Services
For a Project at
Hung Fook Street / Ngan Hon Street, Kowloon City District
(KC-010)**

Drainage and Sewerage Impact Assessment

Prepared for:
Urban Renewal Authority

Prepared by:
Ramboll Environ Hong Kong Limited

Date:
May 2016

Reference Number:
R5142_V1.0

Prepared by:



Monique Wong
Assistant Environmental Consultant

Approved by:



David Yeung
Managing Director

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18 Fenwick Street, Wan Chai, Hong Kong

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Fax: (852) 3465 2899

Email: hkinfo@environcorp.com

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1.2 SUBJECT SITE AND ITS ENVIRONS	2
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2. DRAINAGE IMPACT ASSESSMENT	3
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Figure 2	Existing Sewerage System in the Vicinity of the Subject Site
Figure 3	Existing Sewerage System and Catchment Areas in the Vicinity of the Subject Site

Appendices

Appendix A	Detailed Sewerage Impact Assessment Calculations
Appendix B	Drainage Record Plan obtained from Drainage Services Department

1. Introduction

1.1 Background and Objectives

- 1.1.1 The Urban Renewal Authority (URA) has proposed the Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street in Kowloon City.
- 1.1.2 Ramboll Environ Hong Kong Limited has been appointed by the Urban Renewal Authority (URA) to conduct this Drainage and Sewerage Impact Assessment for submission to the Town Planning Board for approval.

1.2 Subject Site and its Environs

- 1.2.1 The Subject Site is bounded by Hung Fook Street and Ngan Hon Street in Kowloon City. The existing buildings within the Subject Site were built between 1957 and 1960, and range from 6 storeys to 8 storeys high. The buildings are mainly residential in nature with commercial premises at the ground floor.
- 1.2.2 **Figure 1** shows the location of the Subject Site and its environs.

1.3 Proposed Redevelopment

- 1.3.1 The proposed redevelopment to be built in the Subject Site will be built on the area zoned Residential (Group A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. The adjacent public pavement area shown as "Road", will not be built upon and it will be excluded from calculation for plot ratio purposes.
- 1.3.2 In line with the "Residential (Group A)" zoning, the redevelopment intention includes both residential and commercial uses. The subject development covers a gross site area of approximately 4,951m², with an approximate domestic gross floor area of 34,215m² and non-domestic gross floor area of 6,843m².
- 1.3.3 Three 23-storey residential towers are proposed with a total of 759 units, and a common clubhouse will be shared among the residential towers.

2. Drainage Impact Assessment

2.1 Scope of Work

- 2.1.1** The aim of this study is to assess the changes to runoff from the Subject Site as result of the proposed redevelopment and the potential impacts on the existing drainage system and surrounding areas.

2.2 Discussion

- 2.2.1** Surface runoff is mainly from rainfall and it will be directed to existing public storm drains. As the existing site is currently on hard-paved ground, it is anticipated that the redevelopment project will not affect the drainage path or lead to any changes in the runoff behaviour. There is no change or abandonment of existing drains and hence no extra stormwater discharge is imposed. In addition, the landscape garden at the podium within the Subject Site will channel some rainwater below grade and soaked away via soil. It is likely that there will be a slight reduction in surface runoff.
- 2.2.2** Therefore, adverse impact on the existing drainage system is not anticipated and a detailed drainage impact assessment is considered unnecessary.

3. Sewerage Impact Assessment

3.1 Scope of Work

- 3.1.1 The aim of this study is to assess whether the capacity of the existing sewerage networking to the Subject Site is sufficient to cope with the sewage flow from the proposed redevelopment and existing development in the vicinity. Drainage Record Plans from the Drainage Services Department were obtained for the information of sewerage impact assessment.

3.2 Existing Sewerage System

- 3.2.1 According to the Drainage Record obtained from the Drainage Services Department, there are existing Ø150mm and Ø 300mm sewers running along the western side of the Subject Site (manhole reference no. FMH4024355 to FMH4024356), which are then connected to the Ø525mm sewers running along the Hung Fook Street (manhole reference no. FMH4024356 to FMH4025769).
- 3.2.2 The existing sewer network along Hung Fook Street is also connected to sewers including Yuk Shing Street, Kai Ming Street, and Wing Kwong Street. The sewage flow from the connected sewer network is included as catchment areas as shown in **Figure 3**.

3.3 Assessment Criteria and Methodology

- 3.3.1 Environmental Protection Department's (EPD's) Guidelines for Estimating Sewage Flows for Sewage Infrastructure Planning, Version 1 (GESF) is referred to estimate the quantity of the sewage generated from the proposed development and the existing development. Sewage flow parameters and global peaking factors in this document are adopted.
- 3.3.2 According to the Table T-1 of the said EPD Guideline, the unit flow factors of Domestic Flow – Private R2 is 0.27 m³/day.
- 3.3.3 According to the Table T-2 of the said EPD Guideline, the general unit flow factors is 0.20 m³/day and the flow of commercial employee is 0.08 m³/day.
- 3.3.4 The Catchment Inflow Factor (P_{CIF}) for East Kowloon – 1.10 is used for both upstream and downstream catchment areas.

3.4 Wastewater Generated by the Proposed Development

- 3.4.1 Wastewater arising from the proposed redevelopment will be primarily contributed by the residents, and commercial employees of the clubhouse and retail shops. Under the current proposal, there will be a total of 759 residential units, with non-domestic gross floor area of 6,843m².
- 3.4.2 The average household density of 2.9 according to 2011 Hong Kong Population Census (Kowloon City District) is assumed as a worse cast scenario, which would amount to about 2201 residents.
- 3.4.3 73 employees are assumed for the retail shops and 15 employees are assumed for the residential clubhouse.

3.4.4 Detailed calculation for the proposed development is given in **Table 1** below.

Table 1 Estimated Peak Flow

Calculation for Sewage Generation Rate of the Proposed Development			
Proposed Development			
1a. Total number of residential units	=	759	units
1b. Total number of residents	=	2201	residents (average household density of 2.9 in Kowloon City)
1c. Design flow	=	270	litre/employee/day -- (Private R2 in Table-1 of GESF)
1d. Sewage generation rate	=	594.3	m ³ /day
2. Total Non-Domestic Area (Including Clubhouse)	=	6843	m ²
2a. Assumed floor area per employee	=	29.4	m ² per employee – (refer to Table 8 of CIFSUS – All Economic Activities)
2b. Total number of employees	=	233	employees
2c. Design flow for commercial employee	=	80	litre/employee/day -- (refer to Table T-2 of GESF)
2d. Design flow for commercial activities	=	200	litre/employee/day -- (refer to Table T-2 of GESF – General)
2e. Sewage generation rate	=	65.2	m ³ /day
Proposed Connection to Manhole S1 (FMH4024355)			
Flow Rate	=	659.5	m ³ /day
Population	=	2434	persons
Peaking factor	=	6	Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	45.8	litre/sec

3.5 Assessment of Sewerage Impact

3.5.1 Sewage generated from the Subject Site will be discharged to the existing sewerage system along Hung Fook Street, to manhole no. FMH4024355, as shown in **Figure 2**. According to the drainage record available from DSD, the sewers on Hung Fook Street are 525mm in diameter (i.e. pipe segments from manhole no. FMH4024355 to FMH4025769).

3.5.2 It is estimated that the capacity of the aforementioned pipe segments is sufficient to cater the sewage generated from its catchment and from the Subject Site.

3.5.3 **Appendix A** shows the detailed calculation on the estimated hydraulic capacity of the existing sewer sections and the calculation of the amount of the sewage entering each segment of the said sewer network.

3.6 Discussion

3.6.1 The potential sewerage impact due to the proposed development has been quantitatively addressed as shown in **Appendix A**. The average and peak flow rates from the proposed development are respectively about 659.5 m³/day and 45.8 litre/sec.

- 3.6.2** Since the incoming invert level of Manhole S2 (FMH4024356) is missing on the DSD Drainage Record, the outgoing level of 3.15mPD is used. For the inconsistent invert levels (upward slope) on the DSD Drainage Records, the incoming and outgoing invert levels of Manholes S3 (FMH2024357) have been amended to 3.13mPD to align with the downstream sewer pipes.
- 3.6.3** After calculating the appropriate invert levels as mentioned above, the estimated sewage flow from the Subject Site has been compared with the capacity of the existing sewerage system to determine whether it has adequate spare capacity to accommodate the flow from the proposed connection.
- 3.6.4** According to **Table 4 of Appendix A**, it is found that the existing sewerage system has sufficient capacity to cater the sewage generated from the proposed development.
- 3.6.5** No upgrading or improvement work of the existing sewerage system will therefore be required.

4. Overall Conclusion

- 4.1.1** Drainage and sewerage impact assessments were conducted based on the latest development parameters for the redevelopment project at Hung Fook Street / Ngan Hon Street, Kowloon City.
- 4.1.2** The desktop review on the drainage impact shows that no adverse impact is anticipated for the existing drainage system and a detailed drainage impact assessment is therefore considered unnecessary.
- 4.1.3** Based on the sewerage impact assessment results, it is found that the capacity of the existing sewerage system serving the area would be sufficient to cater for the sewage generation from the proposed development. No upgrading or improvement works of the existing sewerage system will therefore be required.
- 4.1.4** The sewerage impact assessment confirms the feasibility of the proposed development.

FIGURES

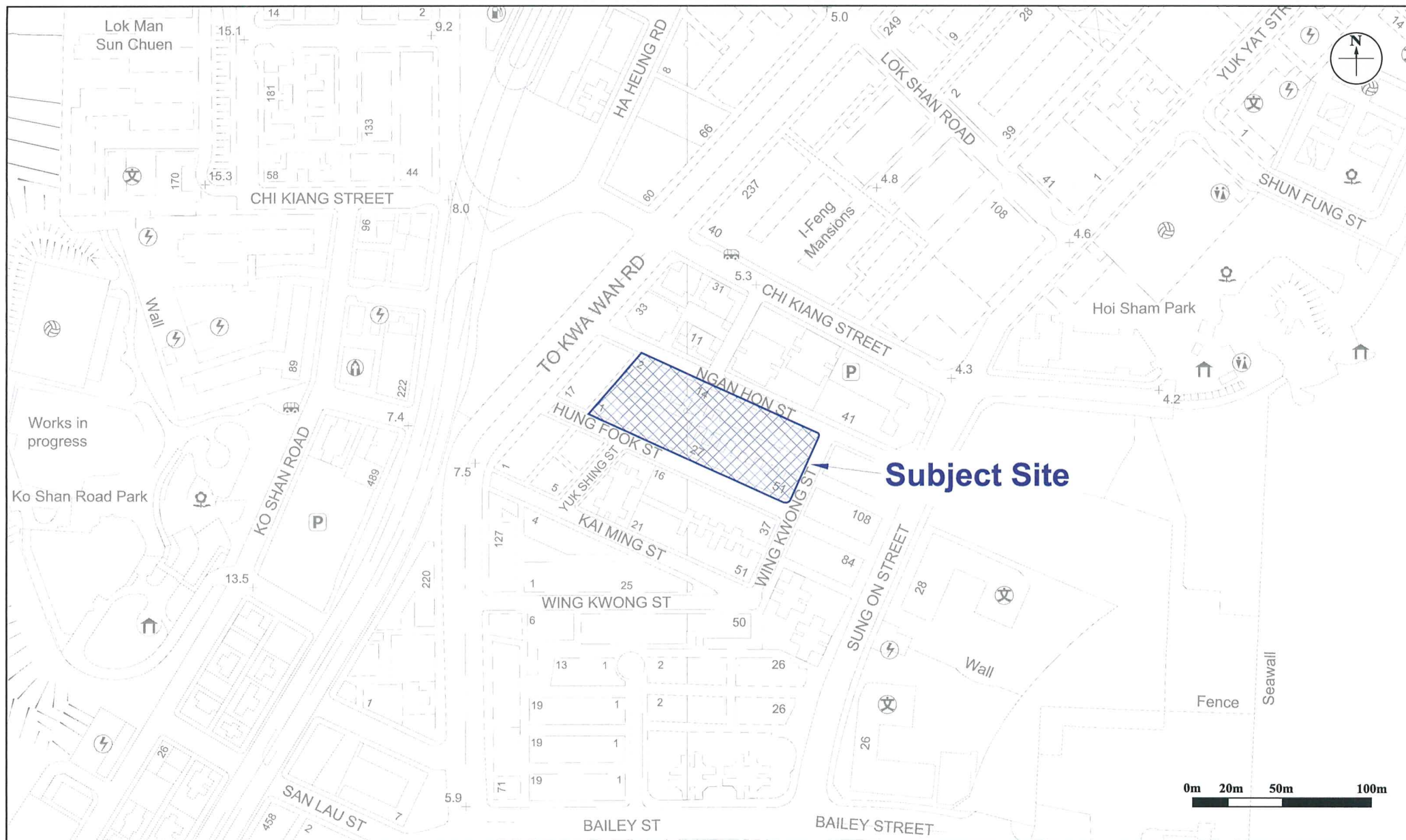


Figure: 1

Title: Subject Site and its Environ

Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: MW

Checked by: DY

Rev.: 1.0

Date: May 2016

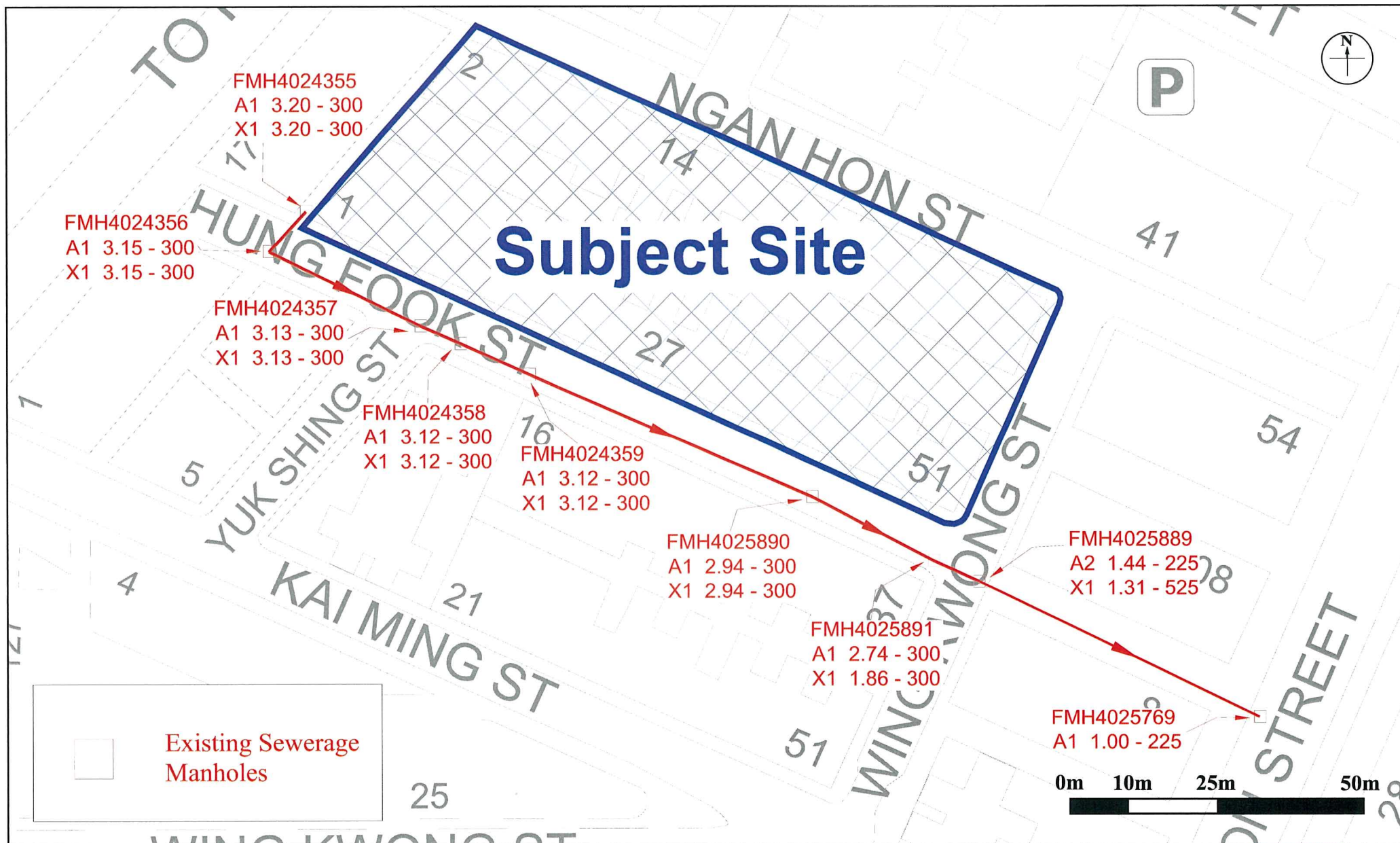


Figure: 2	RAMBOLL ENVIRON
Title: Existing Sewerage System in the Vicinity of the Subject Site	Drawn by: MW
Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City	Checked by: TC
	Rev.: 1.0
	Date: May 2016

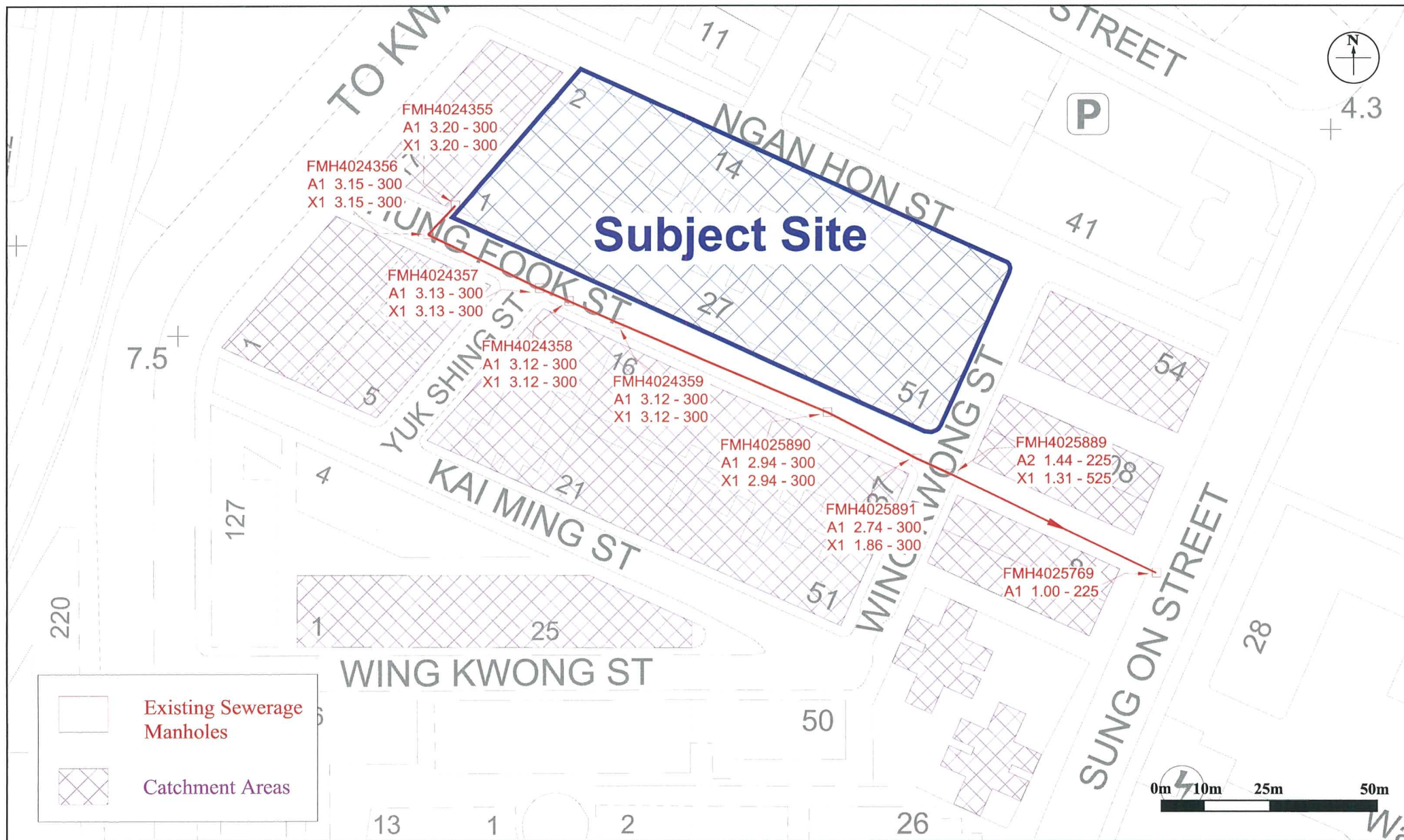


Figure: 3

Title: Existing Sewerage System and Catchment Areas in the Vicinity of the Subject Site

Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: MW

Checked by: TC

Rev.: 1.0

Date: May 2016

APPENDIX A: DETAILED SEWERAGE IMPACT ASSESSMENT CALCULATIONS

Table 1 Calculation for Sewage Generation Rate of the Proposed Development at Hung Fook Street / Ngan Hon Street, Kowloon City

<u>Proposed Development</u>		Remarks
1a. Total number of residential units	=	759 units
1b. Total number of residents	=	2201 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Sewage Generation rate	=	594.3 m ³ /day
2. Total Non-Domestic Area (Including Clubhouse)	=	6843 m ²
2a. Assumed floor area per employee	=	29.4 m ² per employee -- (refer to Table 8 of CIFSUS - All Economic Activities)
2b. Total number of employees	=	233 employees
2c. Design flow for commercial employee	=	80 litre/employee/day -- (refer to Table T-2 of GESF)
2d. Design flow for commercial activities	=	200 litre/employee/day -- (refer to Table T-2 of GESF - General)
2e. Sewage Generation rate	=	65.2 m ³ /day
Proposed Connection at Manhole S1 (FMH4024355)		
Flow Rate	=	659.5 m ³ /day
Population	=	2434 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>45.8</u> litre/sec

Table 2 Hydraulic Capacity at Existing Sewers at Hung Fook Street / Ngan Hon Street, Kowloon City

Segment	Manhole Reference	Manhole Reference	Pipe Dia. mm	Pipe Length m	Invert Level 1 mPD	Invert Level 2 mPD	g m/s ²	k _s m	s	v m ² /s	V m/s	Area m ²	Q m ³ /s	Estimated Capacity L/s
S1-S2	FMH4024355	FMH4024356	300	9.0	3.20	3.15	9.81	0.006	0.007	0.000001	0.89	0.07	0.06	63
S2-S3	FMH4024356	FMH4024357	525	28.9	3.15	3.13	9.81	0.006	0.001	0.000001	0.46	0.22	0.10	100
S3-S4	FMH4024357	FMH4024358	525	7.4	3.13	3.12	9.81	0.006	0.001	0.000001	0.59	0.22	0.13	128
S4-S5	FMH4024358	FMH4024359	525	12.6	3.12	3.12	9.81	0.006	0.001	0.000001	0.45	0.22	0.10	97
S5-S6	FMH4024359	FMH4025890	525	52.7	3.12	2.94	9.81	0.006	0.003	0.000001	0.94	0.22	0.20	204
S6-S7	FMH4025890	FMH4025891	525	23.4	2.94	2.74	9.81	0.006	0.009	0.000001	1.49	0.22	0.32	322
S7-S8	FMH4025891	FMH4025889	525	26.0	1.86	1.44	9.81	0.006	0.016	0.000001	2.05	0.22	0.44	443
S8-S9	FMH4025889	FMH4025769	525	53.1	1.31	1.00	9.81	0.006	0.006	0.000001	1.23	0.22	0.27	266

Notes (highlighted in red):

- (1) According to DSD Drainage Records, the incoming level of Manhole S2 (FMH4024356) is missing. Therefore the outgoing level of 3.15mPD is used instead.
- (2) According to DSD Drainage Records, both invert levels of Manhole S3 (FMH4024357) are listed as 3.17mPD. This would cause an upward slope from Manhole S2 and is disregarded. Therefore, they are amended to align with the downstream sewer pipes.

- Remarks:
- (1) g=gravitational acceleration; k_s=equivalent sand roughness; s=gradient; v=kinematic viscosity of water; V=mean velocity
 - (2) The value of k_s = 6mm is used for the calculation of concrete sewer (based on Table 5: Recommended roughness values in Sewerage Manual)
 - (3) The value of velocity (V) is referred to the Tables for the hydraulic design of pipes, sewers and channels (8th edition)
 - (4) Equation used:
$$V = -\sqrt{(8gDs)} \log\left(\frac{k_s}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}}\right)$$

Table 3a Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment A)

Catchment A

Residential Developments at 17-31 To Kwa Wan Road

1a. Assumed number of total residential units	=	73 units
1b. Total number of residents	=	212 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	57.2 m ³ /day

Sub-total Catchment A

Flow Rate	=	57.2 m ³ /day
Population	=	212 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u>5.3</u> litre/sec

Total Flow at Manhole S1 (FMH4024355), Proposed Development and Catchment A

Flow Rate	=	716.6 m ³ /day
Flow Rate with Catchment Inflow Factor (East Kowloon)	=	788.3 m ³ /day
Population	=	2646 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>54.7</u> litre/sec

Table 3b Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment B)

Catchment B

Residential Developments at 1-15 To Kwa Wan Road, 3 Kai Ming Street, and 2-6 Hung Fook Street

1a. Assumed number of total residential units	=	111 units
1b. Total number of residents	=	322 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	86.9 m ³ /day

Sub-total Catchment B

Flow Rate	=	86.9 m ³ /day
Population	=	322 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u>8.0</u> litre/sec

Total Flow at Manhole S2 (FMH4024356), Proposed Development and Catchment A-B

Flow Rate	=	803.5 m ³ /day
Flow Rate with Catchment Inflow Factor (East Kowloon)	=	883.9 m ³ /day
Population	=	2967 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>61.4</u> litre/sec

Table 3c Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment C)

Catchment C

Residential Development at 11-19 Kai Ming Street

1a. Assumed number of total residential units	=	168 units
1b. Total number of residents	=	487 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	131.5 m ³ /day

Sub-total Catchment C

Flow Rate	=	131.5 m ³ /day
Population	=	487 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u><u>12.2</u></u> litre/sec

Total Flow at Manhole S3 (FMH4024357), Proposed Development and Catchment A-C

Flow Rate	=	935.1 m ³ /day
Flow Rate with Catchment Inflow Factor (East Kowloon)	=	1028.6 m ³ /day
Population	=	3455 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u><u>71.4</u></u> litre/sec

Table 3d Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment D)

Catchment D

Residential Developments at 44-54 Ngan Hon Street, 72-108 Wing Kwong Street

1a. Assumed number of total residential units	=	269 units
1b. Total number of residents	=	780 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	210.6 m ³ /day

2. Wing Fai Mansion (52 Wing Kwong Street)

2a. Assumed number of total residential units	=	204 units
2b. Total number of residents	=	592 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
2c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
2d. Residential Sewage Generation rate	=	159.7 m ³ /day

3. Residential Developments at 16-46 Hung Fook Street, 21-51 Kai Ming Street

3a. Assumed number of total residential units	=	243 units
3b. Total number of residents	=	705 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
3c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
3d. Residential Sewage Generation rate	=	190.3 m ³ /day

4. Residential Developments at 1-33 Wing Kwong Street

4a. Assumed number of total residential units	=	61 units
4b. Total number of residents	=	177 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
4c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
4d. Residential Sewage Generation rate	=	47.8 m ³ /day

Sub-total Catchment D

Flow Rate	=	608.4 m ³ /day
Population	=	2253 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>42.2</u> litre/sec

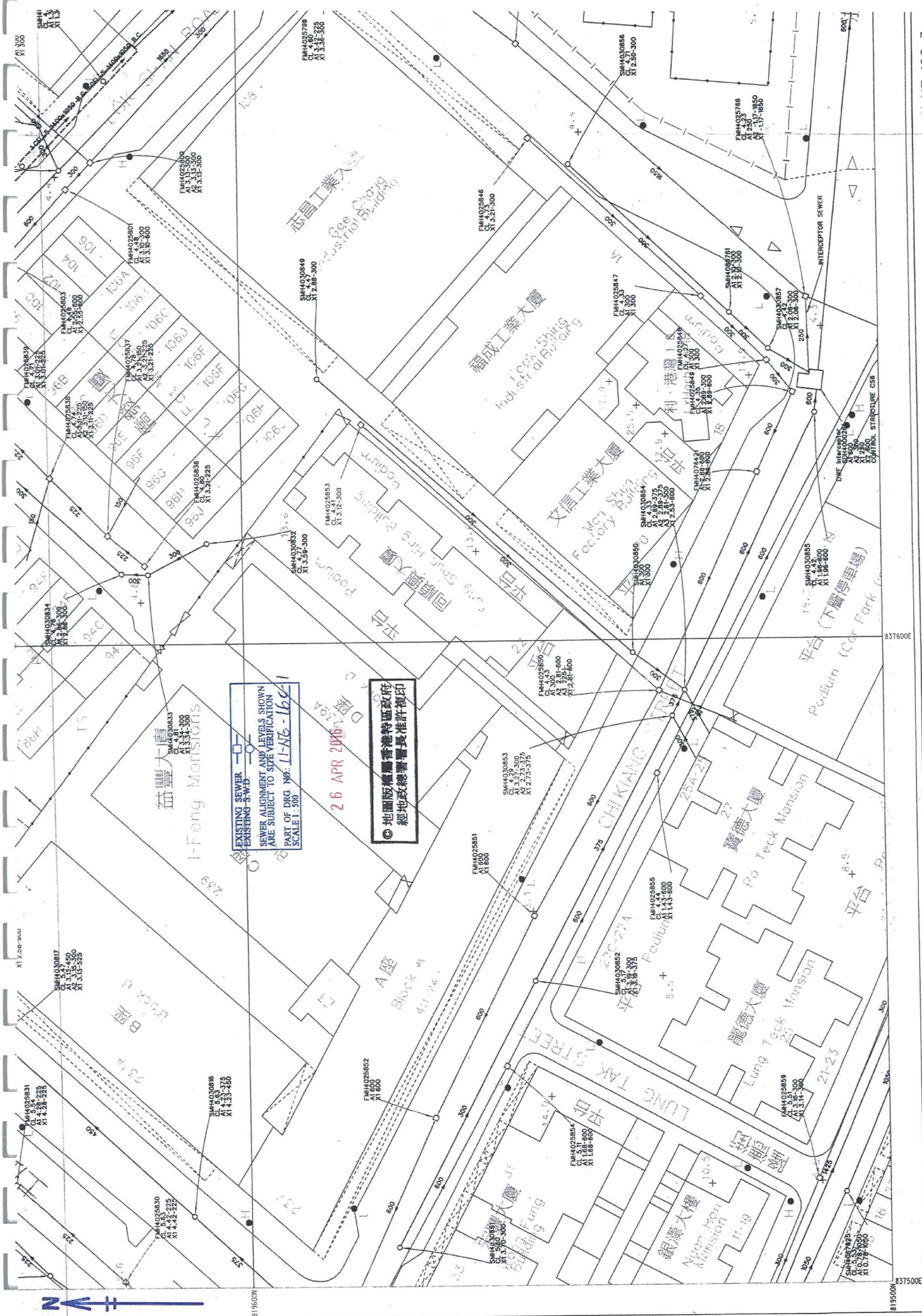
Total Flow at Manhole S8 (FMH4025889), Proposed Development and Catchment A-D

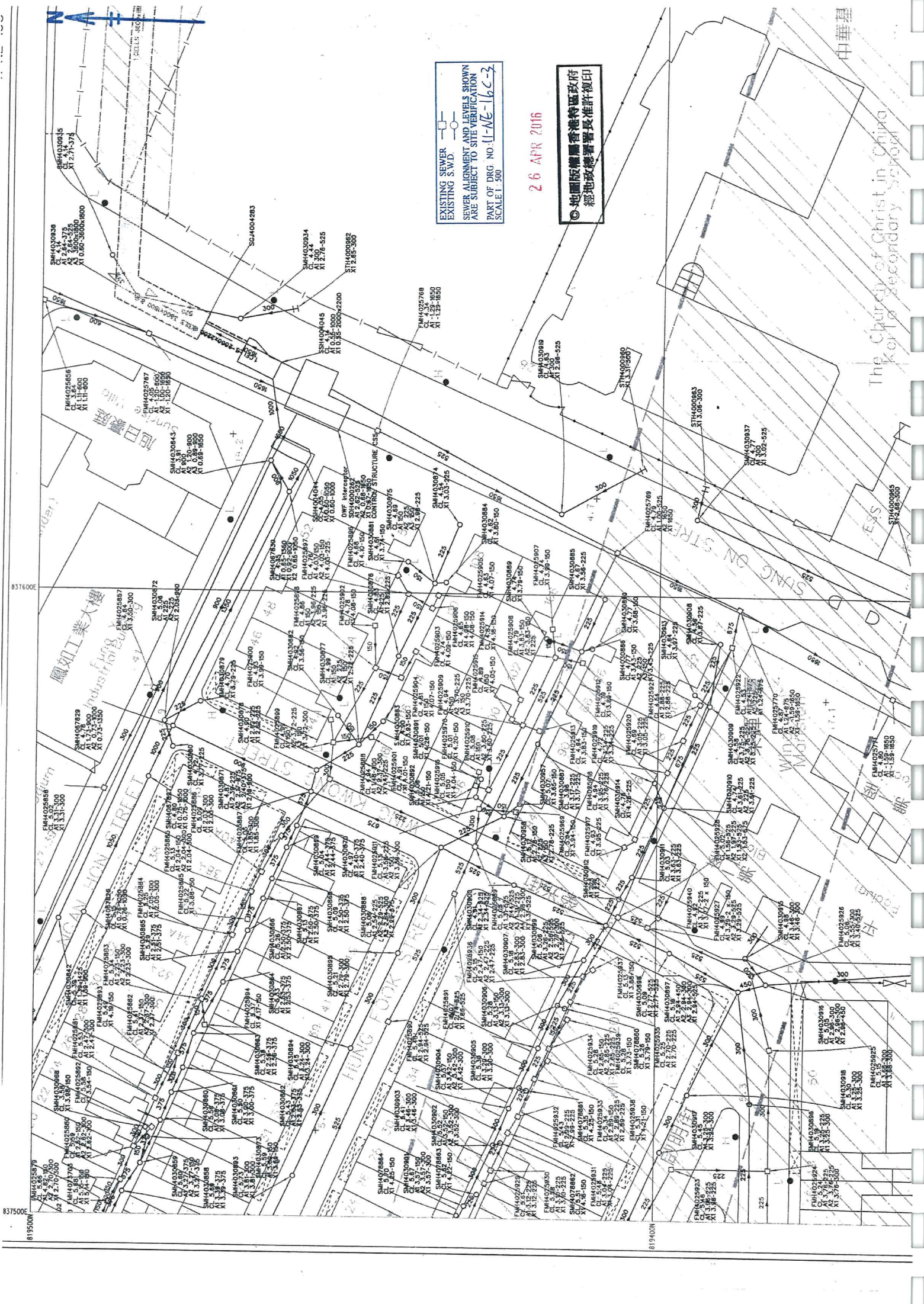
Flow Rate	=	1543.5 m ³ /day
Flow Rate with Catchment Inflow Factor (East Kowloon)	=	1697.8 m ³ /day
Population	=	5708 people
Peaking factor	=	5 Refer to Table T-5 of GESF for population 5,000-10,000 including stormwater allowance
Peak Flow	=	<u>98.3</u> litre/sec

Table 4 Hydraulic Capacity of Existing Sewers for Sewerage generated from the Proposed Development only

Segment	Pipe Dia. (mm)	Pipe Length (m)	Gradient	Estimated Capacity (L/s)	Estimated Flow including the Proposed Development only (L/s)	Contributed by the Proposed Development only (%)	Status	Estimated Flow including the Proposed Development and Catchment Areas (L/s)	Contributed by the Proposed Development and Catchment Areas (%)	Status
S1-S2	300	9.0	0.0066	63	45.8	72.8%	OK	54.7	87.1%	OK
S2-S3	525	28.9	0.0008	100	45.8	45.7%	OK	61.4	61.2%	OK
S3-S4	525	7.4	0.0014	128	45.8	35.8%	OK	71.4	55.8%	OK
S4-S5	525	12.6	0.0008	97	45.8	47.0%	OK	71.4	73.3%	OK
S5-S6	525	52.7	0.0034	204	45.8	22.5%	OK	71.4	35.1%	OK
S7-S8	525	26.0	0.0161	443	45.8	10.3%	OK	71.4	16.1%	OK
S8-S9	525	53.1	0.0058	266	45.8	17.2%	OK	98.3	36.9%	OK

APPENDIX B: DRAINAGE RECORD PLANS OBTAINED FROM DRAINAGE SERVICES DEPARTMENT





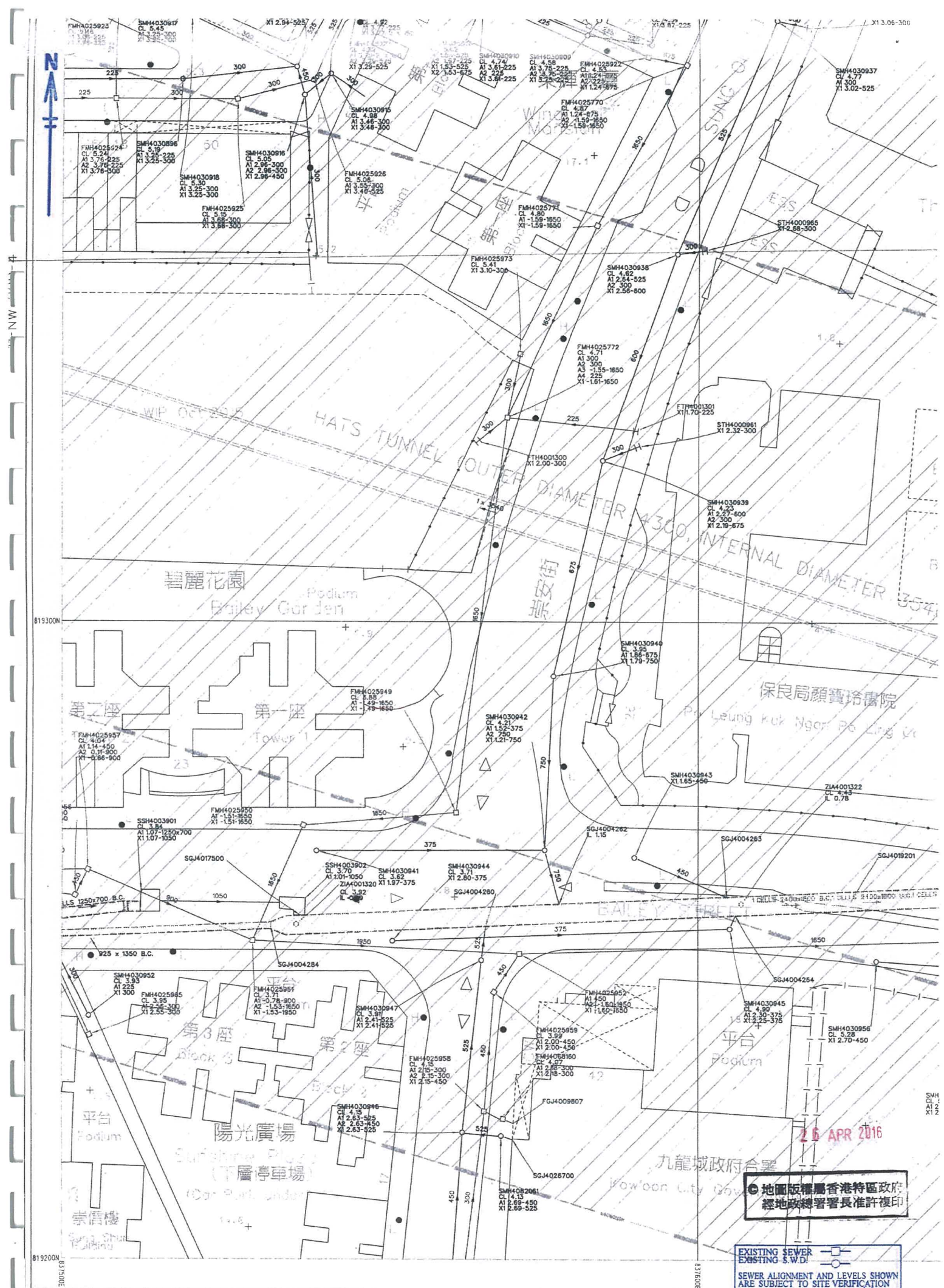
EXISTING SEWER
EXISTING S.W.D.
SEWER ALIGNMENT AND LEVELS SHOWN
ARE SUBJECT TO SITE VERIFICATION
PART OF DRG NO. 11-NE-16C-3
SCALE 1:500

26 APR 2016

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The Church of Christ in China
Kowloon Secondary School

中華基督



Legend:

- Storm Water Manhole
- STH Storm Water Terminal Manhole
- Existing Pipe (Storm/Sewer/Combined)
- Existing Pipe (Storm/Sewer/Combined)

- Tunnel Protection Zone
- Gully Sump / Gully

- △ Slope Sign Board
- △ Slope Number

Metres

EXISTING SEWER
EXISTING S.W.D.
SEWER ALIGNMENT AND LEVELS SHOWN
ARE SUBJECT TO SITE VERIFICATION
PART OF DRG NO. 11-A6-1663
SCALE 1:500

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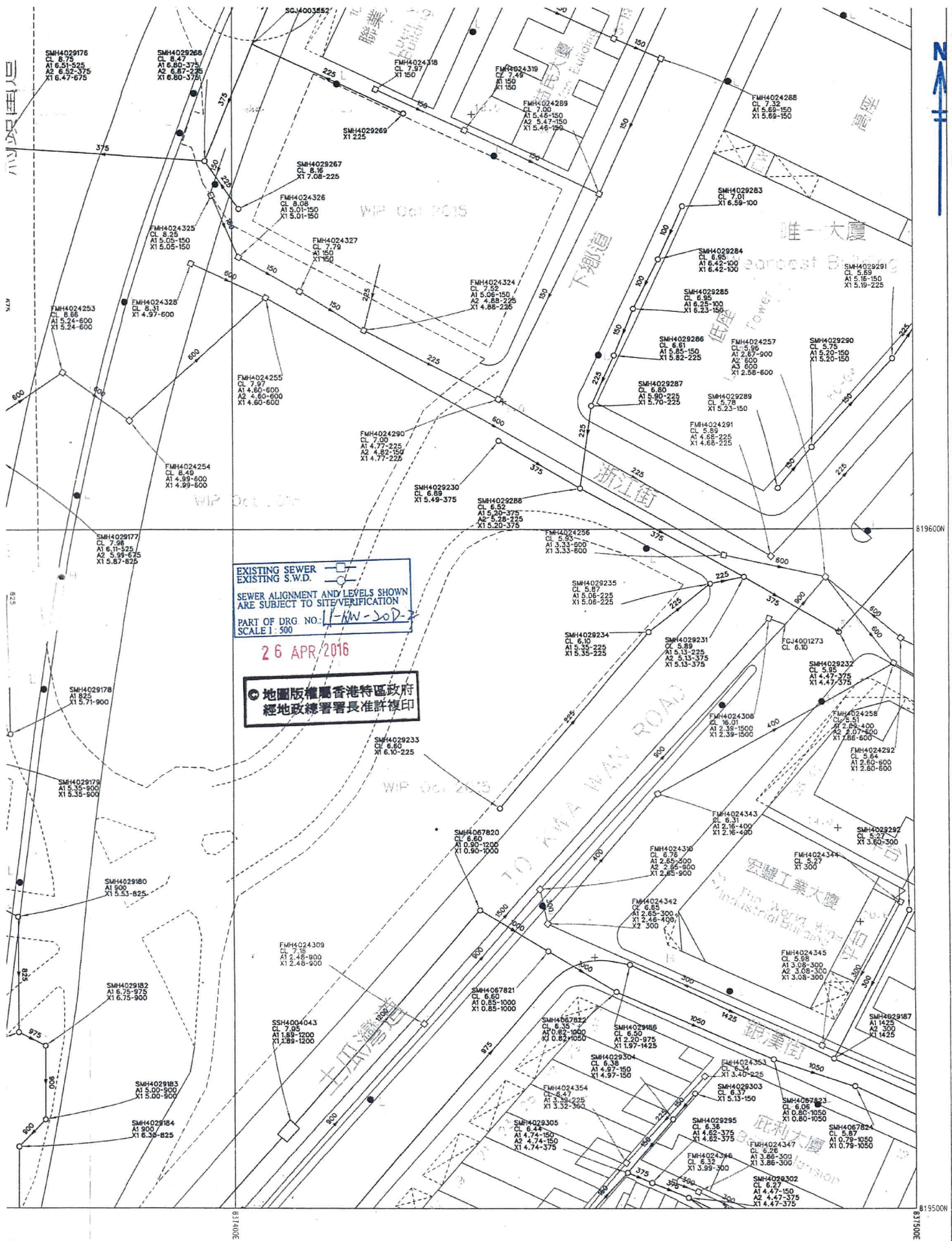
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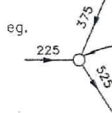
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Notes :

- All levels are given in metres principle datum.
- All dimensions shown are in millimetres unless otherwise stated.
- The information shown on the record drawings are subject to verification on site and no guarantee can be given that this is a complete record.
- Abbreviations for Channels of width smaller or equal to 1200mm:
900C = 900mm width Surface Channel
900SC = 900mm width Slipped Channel
- The Incoming Pipes are marked A1, A2, A3, ... counting clockwise from the first Outgoing Pipe X1. Outgoing Pipes are marked X1, X2, X3 ... counting clockwise from North.

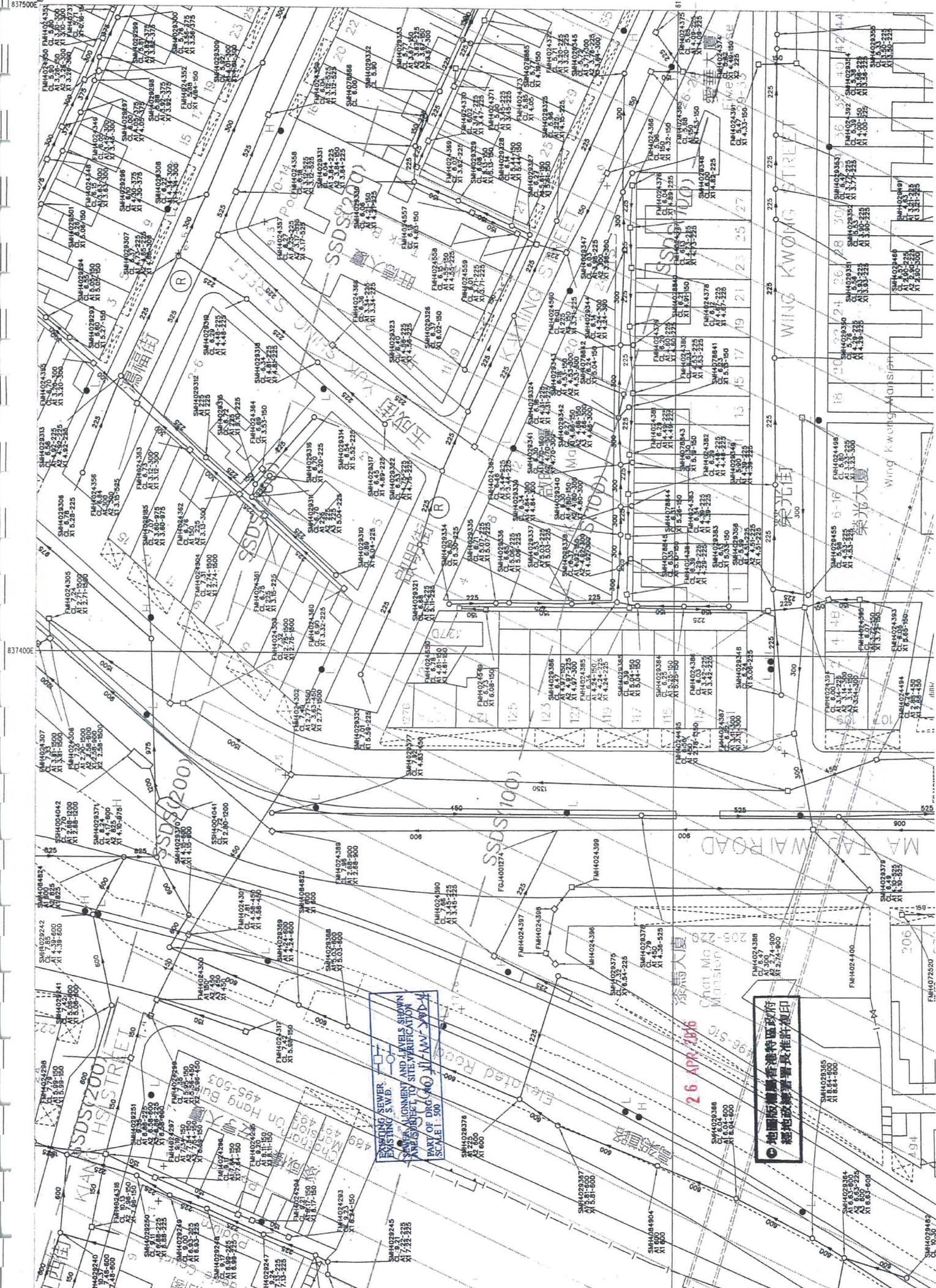


- Manhole number
Cover Level or Ground Level
225mm dia. Incoming Pipe Invert Level
375mm dia. Incoming Pipe Invert Level
525mm dia. Outgoing Pipe Invert Level

Drainage Record Sheet Number

11-NW-20D-2

Last Updating : 27-11-2015



PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 5

**Social Impact Assessment (Stage 1)
Report**

Hung Fook Street / Ngan Hon Street

Development Scheme (KC-010)



Stage 1 Social Impact Assessment

June 2016

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1. INTRODUCTION

1.1 In the new Urban Renewal Strategy (URS) issued by the Government in February 2011, the Urban Renewal Authority (URA) will carry out Social Impact Assessment (SIA) studies in the form of “a Stage 1 social impact assessment ... before the publication of any proposed redevelopment project in the Government Gazette”; and “a Stage 2 social impact assessment ... after the proposed project has been published in the Government Gazette”.

1.2 The URS also states “Early social impact assessments will be initiated and conducted by the DURF (District Urban Renewal Forum) before redevelopment is recommended as the preferred option. The URA will update these assessments by DURF before implementing any specific redevelopment project.” As the Project falls within the study area of Kowloon City DURF (“KC DURF”) and the consultants for the KC DURF have completed a DURF SIA report of the Urban Renewal Plan for Kowloon City in 2014, reference has been made to the DURF SIA report, where appropriate, in preparation of this Stage 1 SIA.

According to the URS, the main elements of the Stage 1 SIA conducted by the URA before publication of a proposed project should include:-

- the population characteristics of the proposed project area;
- the socio-economic characteristics of the area;
- the housing conditions in the area;
- the characteristics of local business activities, including small shops and street stalls;
- the degree of overcrowding in the area;
- the availability of amenities, community and welfare facilities in the area;
- the historical background of the area;
- the cultural and local characteristics of the area;
- an initial assessment of the potential social impact of the proposed project; and
- an initial assessment of the mitigation measures required.

1.3 The Stage 2 SIA to be conducted after publication of the project will use factual information collected as part of the freezing survey to be conducted upon project commencement. The URS stipulates URA should submit the reports of both the Stage 1 and Stage 2 SIAs to the Town Planning Board (TPB) when it submits a development scheme under section 25 of the Urban Renewal Authority Ordinance (URAO). It also stipulates the URA should release the reports for public information.

2. THE PROJECT AREA

- 2.1 The proposed development scheme (the Scheme) comprises buildings at Nos. 1-51 Hung Fook Street (odd nos.) and Nos. 2-42A Ngan Hon Street (even nos.), To Kwa Wan, Kowloon City (**Figure 2.1**).
- 2.2 The Scheme is bounded by Ngan Hon Street to the north, Wing Kwong Street to the east and Hung Fook Street to the south and a back lane of the adjacent buildings to the west. It covers an area of about 4,951m², including portion of surrounding public pavement where some of the affected buildings overhang, and a Government back lane. The net site area used for GFA calculation (which includes the Government lane but excludes the pavement) is about 4,562m². The existing height of the buildings ranges from 6 to 8 storeys.
- 2.3 The Scheme falls within one of the "Proposed Redevelopment Priority Areas" in the KC DURF's Urban Renewal Plan. The KC DURF's Urban Renewal Plan recommended that in the "Proposed Redevelopment Priority Area", *"redevelopment should be given priority as the urban renewal approach for the area"*. The DURF SIA, in particularly the community profile compiled for this Priority Area, will be taken into consideration in this report.
- 2.4 There are a number of URA projects in the proximity to the Project. In order to realise the community benefits under a district-based approach, on 3 June 2016, URA also self-initiated Hung Fook Street/Kai Ming Street (KC-011) and Wing Kwong Street (KC-012) under Section 26 of the URAO located to the immediate south and the southwest of the Scheme (**Figure 2.2** refers). The Bailey Street/Wing Kwong Street Development Project (KC-009) was commenced on 4 March 2016 and is located further south of the Scheme.
- 2.5 To improve traffic accessibility and enhance pedestrian connectivity in the area, it is intended to provide a through road extending from Wan On Street via URA Projects KC-009, KC-010, KC-011 and KC-012 to Ngan Hon Street. An underground car park will be provided at the Scheme accommodating car parking spaces, loading/unloading bays and coach parking spaces to serve the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street, as well as coach parking spaces. Such arrangement can maintain street fronts of the area by discouraging necessities of open up run in and out in individual developments.

- 2.6 To the immediate southeast of Development Project (KC-011) is the Kai Ming Street Demand-Led Development Project (DL-8:KC, Nos. 41-51 Kai Ming Street (odd nos.)), which was commenced in response to the dangerous building condition found at Nos. 45, 47, 49 and 51 Kai Ming Street, where Buildings Department applied for a closure order issued on 30 August 2013 and demolition orders issued on 19 December 2013.
- 2.7 Further south of the Scheme is the URA Ma Tau Wai Road/Chun Tin Street Development Project (TKW/1/002) which commenced in response to the tragic collapse of No. 45J Ma Tau Wai Road on 29 January 2010, which caused loss of lives and seriously affected the structural integrity of adjacent buildings along Ma Tau Wai Road.
- 2.8 East of TKW/1/002 is another URA project - URA Chun Tin Street/Sung Chi Street Development Scheme (KC-008(A)) commenced on 6 May 2016. **Figure 2.2** shows the location of URA projects in the vicinity.

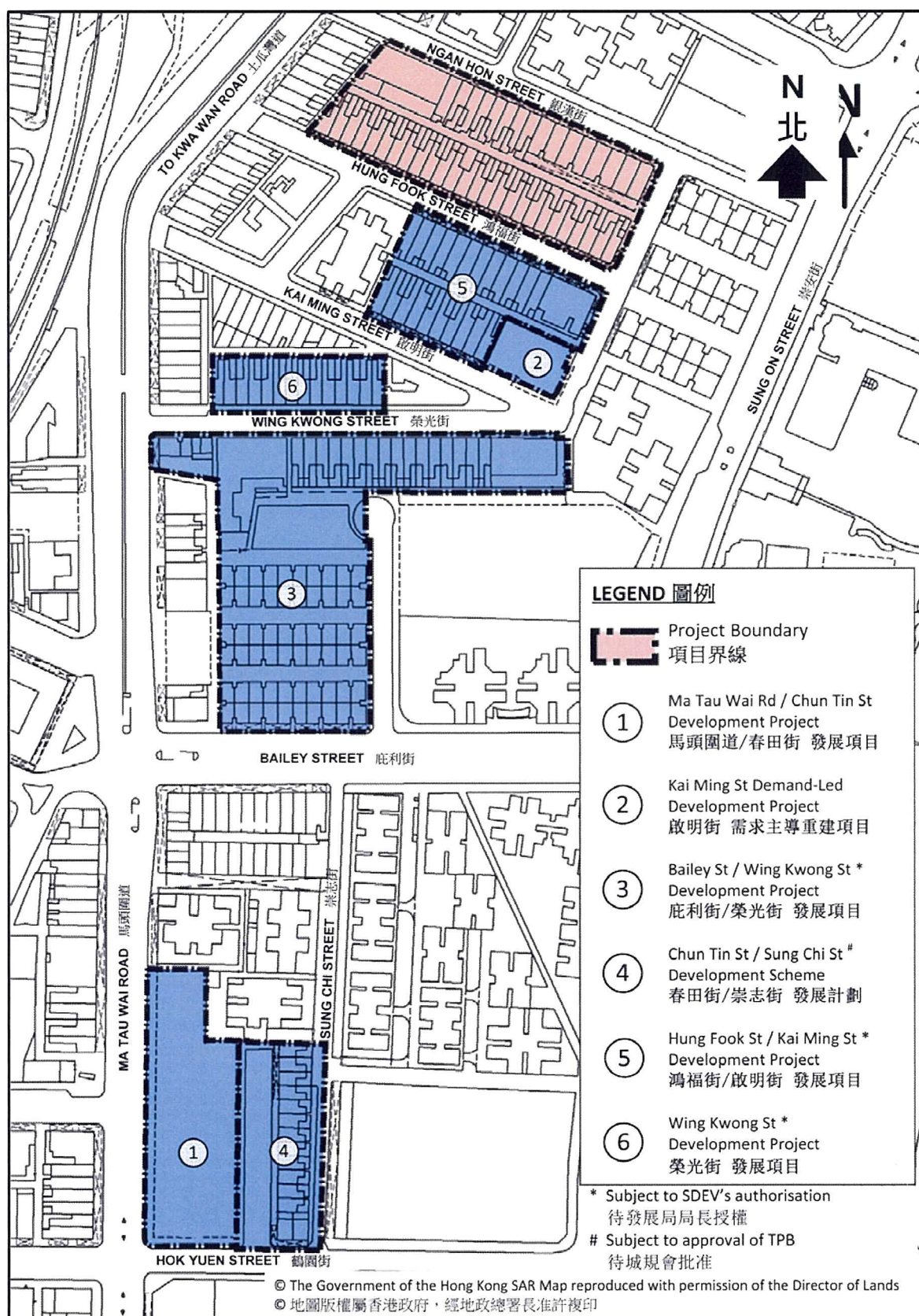


Figure 2.2 URA projects in the vicinity

3. HISTORICAL BACKGROUND

- 3.1 The origin of the name "To Kwa Wan" has two different narratives. One was named after the bay with plentiful crops of sweet potatoes ("To Kwa"). Another suggests that the Hoi Sum Island (in the middle of the sea) just opposite to To Kwa Wan was shaped like a "To Kwa".
- 3.2 Before 1864, To Kwa Wan was a bay lined by beaches and mud-flats and the only significant stream from the hinterland running towards the bay was Ma Hang Chung, which flowed closely along the existing Ma Hang Chung Road. Ma Tau Kok (means pier's headland) was the northern headland of the bay, which was probably named after the long pier of Kowloon Walled City nearby.
- 3.3 On the north-facing coast of the headland, there was a small settlement with 8 to 11 cottages bearing the same name as the headland. The quarries at Ma Tau Kok and To Kwa Wan had started to operate since mid-18th century and one of the quarries was located at the slope of a knoll named San Shan (means new quarry). San Shan Road was believed to commemorate the quarry and the settlements of stone-cutters nearby.
- 3.4 To the south of Ma Tau Kok, there was a string of villages along the coast of To Kwa Wan Bay sharing the name of To Kwa Wan Village. This agricultural and stone-cutters village was believed to be the largest village in the Hung Hom and To Kwa Wan area with over 400 people living in 1873.

Land Reclamation

- 3.5 The Project area was probably located near the coastline in the 1890s. Reclamation had been undertaken off the northern side of Ma Tau Kok and To Kwa Wan Bay since the late 1890s. The last phase of reclamation in To Kwa Wan was carried out in the 1960s and the reclaimed land was designated for residential development and industrial uses such as manufacturing, car-repairing, weaving, bleaching and dyeing, printing and electroplating.

Hoi Sham Island and Tin Hau Temple

- 3.6 Hoi Sum Island was originally a small island but was subsequently linked with the mainland by reclamation. In 1964, the Lung Mo Temple at the island was relocated

into the Tin Hau Temple at No. 49 Ha Heung Road. Hoi Sum Island was developed as Hoi Sham Park with the landmark of Fishtail Rock. The Tin Hau Temple has been graded the Grade III status by the Antiquities Advisory Board.

Pak Tai Temple

- 3.7 The temple was originally located on a hillock, at the junction of the present Tsing Chau Street and Ma Tau Wai Road. According to the inscription on the incense burner, the temple was built in 1876. However, because of urbanization, the temple was demolished and a new temple was built at the present location in the 1930s. The temple had undergone major renovation in 2005.

Green Island Cement

- 3.8 Green Island Cement Company was founded in Macau in 1887. The company has set up a cement plant in Hung Hom, near the existing Hok Yuen Street and Hung Hom Road. The plant covered a large area with both rail and pier for raw materials and products transportation. It supplied cement for both local and overseas construction projects and provided support to the city development. The plant was moved and redeveloped into industrial and commercial blocks, while part of the pier still exists near the building named "Harbour Centre".

Ko Shan Theatre

- 3.9 The Ko Shan Theatre first opened in March 1983 and was designed as a 3,000 seat semi-open-air theatre. However, it suffered from inclement weather and noise problems, and was re-opened with an indoor 1031-seat theatre in October 1996. Ko Shan Theatre underwent extension works in 2008/09 with a new wing with a 600-seat theatre, activity and dance rooms. It was opened in October 2014.

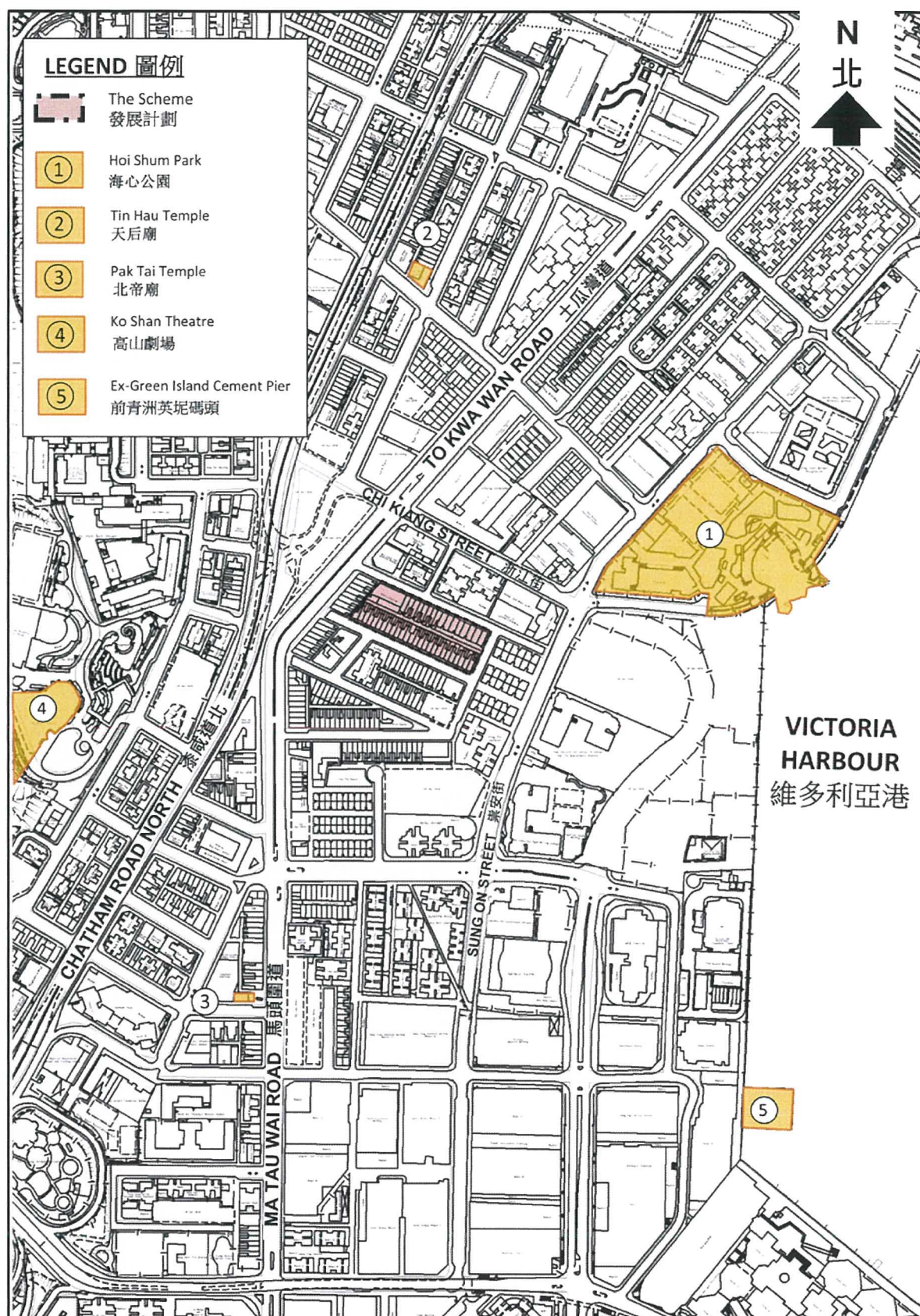


Figure 3.1 Places with Historical Background and Local Character in To Kwa Wan

4. POPULATION & SOCIO-ECONOMIC CHARACTERISTICS

- 4.1 To assess the population and socio-economic characteristics of the Scheme, a combination of sources has been used, namely the 2011 Population Census, past experience from other URA redevelopment projects, and for the accommodation assessment, approved building plans and on-site non-obtrusive inspection.
- 4.2 The Census and Statistics Department's (C&SD) website provides the 2011 Population Census and the information is down to Tertiary Planning Unit (TPU) and Constituency Area levels. Some information, which is at the Large Street Block (LSB) level, is also obtained from C&SD upon request.
- 4.3 The Scheme falls within To Kwa Wan North Constituency (G13) of the Kowloon City (KC) District (**Figure 4.1**). It lies within TPU/SB 244/01 under the LSB 244/01L (**Figure 4.2**). Apart from existing buildings covered by the Scheme which fall TPU/SB 244/01, the said LSB 244/01L also covers similar type of buildings within TPU/SBs 244/02 and 244/07.
- 4.4 The DURF SIA report has suggested that the average household size within its study area is around 2.6-2.9. However, such figure corresponds to the wide study area of KC DURF, which may be less representative for older, dilapidated buildings with sub-divided flats that are in need of redevelopment. Based on past URA experience, the average household size of those within the Scheme is estimated to be around 2.1. A tenure split of about 20% owner occupied and 80% tenanted households is assumed in the preparation of this report.
- 4.5 Given the background of the (general and non-obtrusive nature) source of the data available to carry out this Stage 1 SIA, the assessments derived should only be considered as indicative and for reference use only.

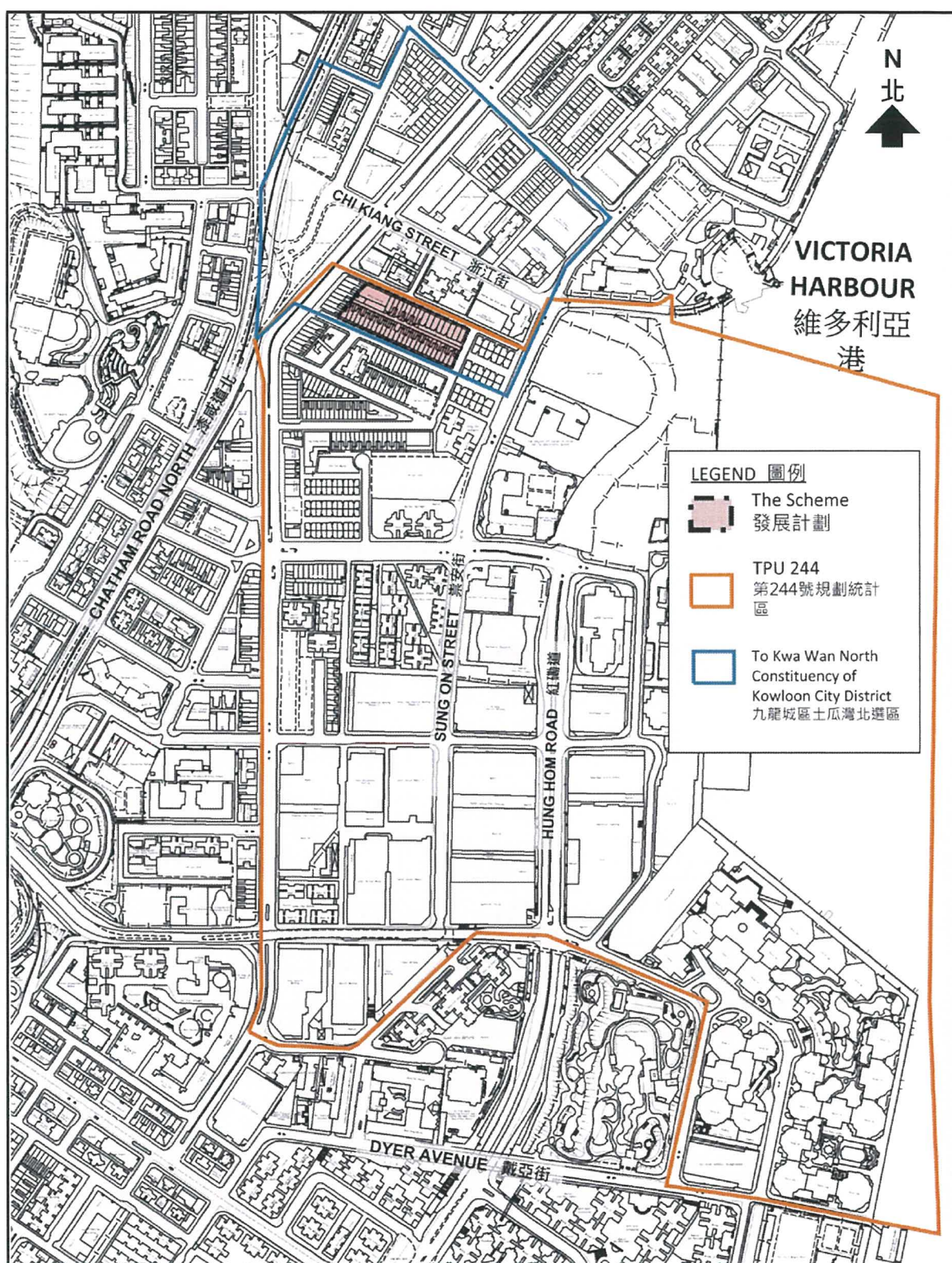


Figure 4.1 Boundary of To Kwa Wan North Constituency (G13) of the Kowloon City District and Tertiary Planning Unit (TPU) 244

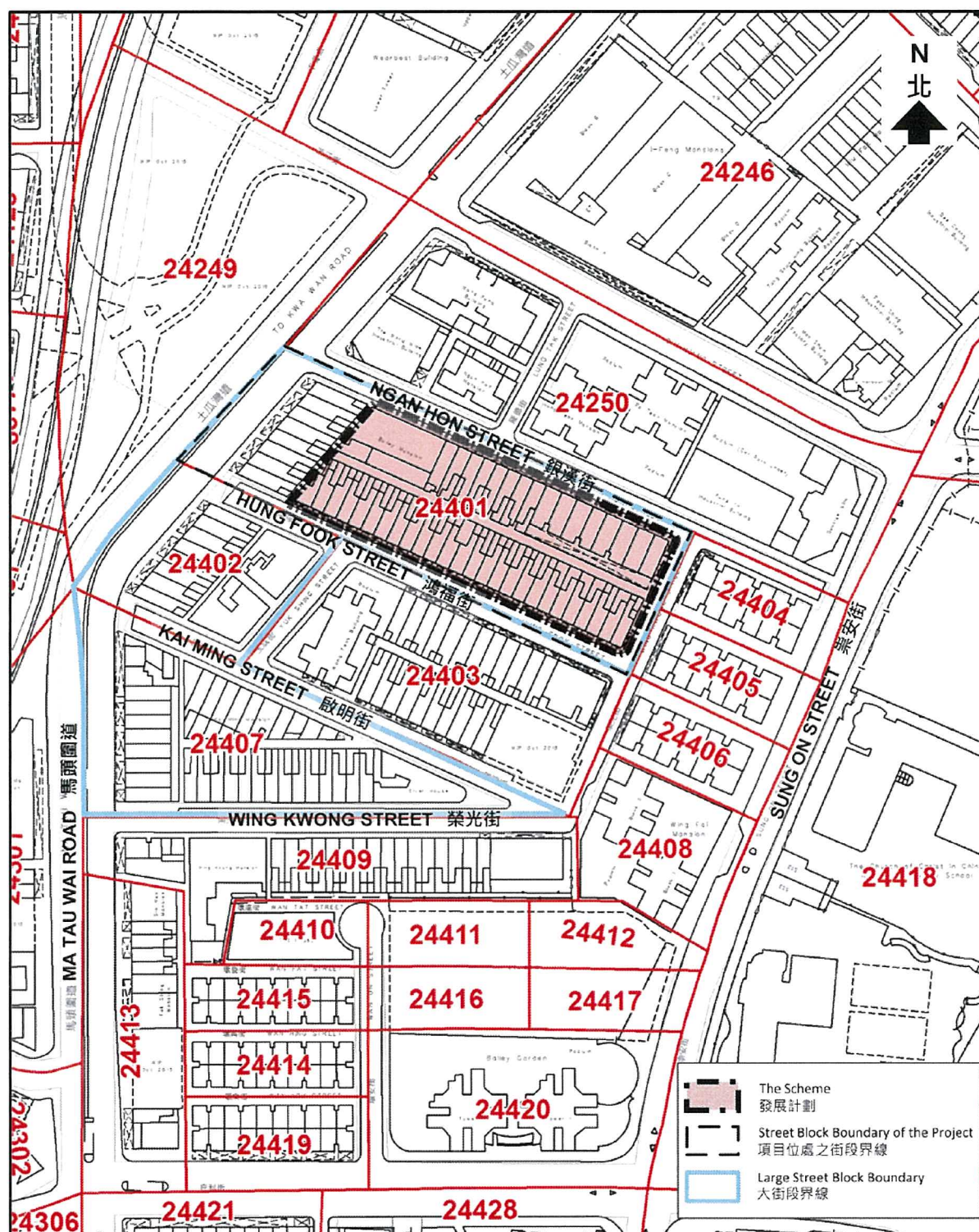


Figure 4.2 Street Block and Large Street Block Boundary of the Scheme (TPU/SB 244/01 and LSB244/01L)

Overview of Housing & Population Characteristics of Kowloon City District

- 4.6 As revealed in the 2011 Population Census, the KC District has a population of about 377,351. In term of monthly household income, the DURF SIA reported that the particular "Proposed Redevelopment Priority Areas" which covers the Scheme has a median monthly household income of \$18,068, which is much lower than the average of the whole KC District (i.e. \$23,560) and is the second lowest among the seven study areas of DURF. The DURF SIA explained that the low income level might be related to the residents' occupations and the high proportion of residents with low educational attainment.
- 4.7 The majority of the living quarters in KC District are in private housing blocks (about 81%); only about 18% of living quarters are in public rental housings/subsidized home ownership housings. The remaining living quarters (about 1%) are in other types of housing. Being built in the 1950s and 1960s, quite a number of private residential blocks in KC District appear to lack proper maintenance and management. The Building Conditions Survey (BCS) data for the entire KC District on private residential buildings with a building age of 30 years or above revealed relatively high percentages of "Varied" and "Poor" conditioned building blocks in the district.

Household Composition

- 4.8 According to the approved General Building Plans, the total number of living units within the Scheme is 354. The degree of sharing of living quarters of the KC District as revealed in the Census is about 1.0. However, as some subdivided units were observed within the Scheme, and based on past URA experience, the degree of sharing (or the "average number of domestic households per unit of quarters") is estimate to be around 2.0. Hence the total number of households may be around 708. This will be confirmed at the Freezing Survey to be reported in the Stage 2 SIA.
- 4.9 Applying the Census LSB proportion of singleton (36%), doubletons (9%) and 3-person or above households (55%), it gives a distribution of 255 singletons, 64 doubletons and 389 3-person or above households in the Scheme. The proportion of singleton households (36%) is higher than the territorial ratio (17%) while that of the doubleton households (9%) estimated from the LSB data is far below to the territorial ratio (25%). On the other hand, the proportion of 3-person households (55%) is slightly lower than the territorial ratio of 58%.

- 4.10 Based on the assumption as stated in Para. 4.4 above, the average household size of those within the Project is estimated to be around 2.1, which is lower than the average household sizes of the KC District (2.9) and the territory (2.9). This 'smaller' household size compared to district and territorial data could be a result of the presence of many sub-divided units and/or cubicle apartments in URA redevelopment projects.

Population

- 4.11 As stated in Paras. 4.4 and 4.8, the total number of households estimated to be residing within the Scheme is approximately 708. With an average household size is 2.1, the population is estimate to be around 1,486. The actual number of households, living quarters and population affected will all be verified by the results of the Stage 2 SIA.
- 4.12 The labour force participation rate (of age 15 and over) of the LSB data is 57%, which is slightly lower than the Census data of the KC District (59%) and the whole territory (60%).
- 4.13 The LSB data indicates a lower percentage of elderly residents over 65 (8.7%) than the KC District figures (16%) and for the whole territory (13%). This will be confirmed by the freezing survey, and the needs of this elderly group are expected to be manageable and extra attention would be paid to them.

Tenure of Accommodation

- 4.14 As explained in Para. 4.4 above, the assumption adopted is that about 20% of the households will be owner occupiers with 80% tenants. This assumption appears reasonable given that many redevelopment projects previously undertaken by URA contain sub-divided units, which are mainly rented out. The LSB data (which covers the Scheme and other residential buildings) shows 37% of owner occupiers, which is lower than the KC District data (57%) and the territorial data (52%).
- 4.15 The median monthly domestic household rent for all housing types in the To Kwa Wan North Constituency (G13) area is \$3,600. Median rent for the all housing types in KC District is \$2,900, and the territorial level is \$1,600. The much higher medium rent for

the Constituency area than the KC District and the territory may be due to the presence of the new and modern private residential development in the Constituency area, such as Sunrise Villa and other large scale developments such as I-Feng Mansions to the north of the Scheme. They are of much higher rent than the general old private housings and public housings.

- 4.16 In conclusion, the socio-economic characteristics that are derived from the 2011 Population Census LSB data may not totally reflect the characteristics of those residing within this Scheme as the figure is limited by the inclusion of a large number of buildings in other SBs into the LSB boundary for the study. Non-obtrusive site observation indicates a number of units within the Scheme appear to have been converted into sub-divided units and poor living conditions were observed. The Stage 2 SIA to be conducted upon project commencement will give clearer and accurate information concerning those living and working within the Scheme.

5. HOUSING & ENVIRONMENTAL CONDITIONS

Building Age

- 5.1 The Scheme involves 55 street numbers and all the buildings have no lifts. The buildings were completed between 1957 and 1960 and are over 50 years of age.

Building and Living Condition

- 5.2 The buildings within the Scheme are all under multiple ownerships except No. 19 Hung Fook Street and No. 20 Ngan Hon Street (based on records in the Land Registry as of May 2016). Based on Land Registry and Home Affairs Department's website information as of May 2016, some of the buildings in the Scheme (Nos. 1-3, 29-31 and 49 Hung Fook Street and Nos. 16, 18 and 22 Ngan Hon Street) have Owners' Corporations (OCs) or (Nos. 2-14 Ngan Hon Street) have Mutual Aid Committees (MACs). All other buildings within the project have neither OC nor MACs for their effective management. According to URA's Building Condition Survey (BCS), majority of the buildings are in "Varied" and "Less Satisfactory" conditions ("Varied" being the 2nd worst category). The structural elements and building fabric have defects. The defects are not only localized but also noticed at some internal common areas (e.g. roof and staircase). Some suspected UBWs can be found on the roof of some buildings.
- 5.3 As of May 2016, twenty eight buildings have completed rehabilitation works carried out by Government Contractors under Operation Building Bright (OBB) Scheme (Category 2) and one building has completed rehabilitation works by the OC under OBB (Category 1). OBB Category 2 involves buildings having difficulties in organizing such repair works, such as buildings without OCs. Once selected as OBB Category 2 target buildings, BD will arrange consultants and contractors engaged by the Government to carry out repair works stipulated in the repair orders on behalf of the owners. However, it is understood that these Category 2 works comprised mainly repairing defects (e.g. major cracks, spalling) in common or public areas of the buildings, such that repair works to the interior of private units are usually not included. In general, the physical condition of these Category 2 buildings after repair works, are improved but they cannot be considered to have been comprehensively rehabilitated due to the nature of Category 2 works. Based on past URA's experience in rehabilitation works, even buildings that have undergone repair works need to

undertake comprehensive building rehabilitation every 5-6 years in order to avoid deterioration. Besides, owners of two buildings have carried out rehabilitation works by themselves even though the buildings do not have any building order issued by the Government.

- 5.4 Suspected UBWs are found on the rooftop of some buildings and the rear of some buildings in the Scheme. A number of original units in the approved General Building Plans (GBPs) are suspected to be sub-divided into smaller units (sub-divided units). The living condition is considered crowded and not satisfactory, and also posing potential hazard on fire safety and hygiene problem.

Existing Uses

- 5.5 Based on non-obtrusive site observation, the ground floor units within the Scheme are used for non-domestic purpose. The upper floor units appear to be used mainly for domestic purpose, which is in conformity with the use as permitted in the approved GBPs. Detail uses within the Project will be verified in the freezing survey and to be reported in the SIA Stage 2.

Degree of Overcrowding

- 5.6 The degree of sharing of living quarters of the KC District as revealed in the Census is about 1.0. As stated in Para. 4.8, this is not expected to be representative of the situation in the Scheme and the degree of sharing of the living quarters in the Scheme is expected to be higher 2.0. The degree of sharing of about 1.0 for KC district is because the district has public housing estates and some relatively newer private residential blocks completed after the 1970s or 1980s, which tend to have only "one household per living quarter".

6. CULTURAL & LOCAL CHARACTERISTICS, AND CHARACTERISTICS OF LOCAL BUSINESS ACTIVITIES

- 6.1 Unlike other parts of Kowloon City district such as Lung Shing area, To Kwa Wan area in Kowloon City district does not have a long history of inhabitation as well as specialized local industries. The area is dominated by large industrial/commercial buildings to the east of Ma Tau Wai Road, along Hok Yuen Street East, Man Yue Street and Hung Hom Road. The majority of these buildings appear not to be engaged in manufacturing activities but mainly office, storage, workshop/showroom uses. In particular, jewellery and watch shops and workshops/showrooms are concentrated in this area.
- 6.2 A significant number of light engineering workshops and car repair trades are found throughout the To Kwa Wan area, with a higher concentration of such activities in proximity to the Scheme.
- 6.3 A relatively significant presence of Government Departments' service centres and civil servants' quarters are also found in the area to the west of Ma Tau Wai Road/Chatham Road North.
- 6.4 About 56 ground floor shop were observed within the Scheme on the days of site visit in May 2016, of which 5 are staircase shops. The shops are all facing streets with direct vehicular access (Hung Fook Street, Wing Kwong Street and Ngan Hon Street), except Nos. 1-5 Hung Fook Street where vehicular access is limited by Emergency Vehicular Access (EVA) in front of the premises. Majority of the shops are for car repair and services, retail, services, engineering works and restaurant businesses. The location and nature of the business activities of the above mentioned 56 ground floor shop premises are listed in **Table 6.1**. The exact number of non-domestic operators/businesses would need to be verified by the results of the Stage 2 SIA.

Table 6.1 Ground Floor Business Activities Facing Major Street

	Address	Current Use
1.	1 Hung Fook Street Shop A	Beauty Saloon
2.	1 Hung Fook Street Shop B	Beauty Saloon
3.	Staircase Shop between 1 and 3 Hung Fook Street	Property Agent
4.	3 Hung Fook Street Shop A	Barber Shop

	Address	Current Use
5.	3 Hung Fook Street Shop B	Retail (Noodle)
6.	5 Hung Fook Street	Retail (Grocery Store)
7.	Staircase shop between 5 and 7 Hung Fook Street	Property Agent
8.	7 Hung Fook Street	Construction Materials Company
9.	9 Hung Fook Street	Construction Materials Company
10.	Staircase shop between 9 and 11 Hung Fook Street	Retail (Grocery Store)
11.	11 Hung Fook Street	Metal Workshop
12.	13 Hung Fook Street	Beauty Saloon
13.	15 Hung Fook Street	Retail (Bean Products)
14.	17 Hung Fook Street	Engineering Company
15.	19 Hung Fook Street	Car repair and services
16.	21 Hung Fook Street	Car repair and services
17.	23 Hung Fook Street	Cannot be identified*
18.	25 Hung Fook Street	Kitchen Wares Company
19.	27 Hung Fook Street	Car repair and services
20.	29 Hung Fook Street	Car repair and services
21.	31 Hung Fook Street	Motorcycle repair and services
22.	33 Hung Fook Street	Car repair and services
23.	Staircase shop between 33 and 35 Hung Fook Street	Engineering Company
24.	35 Hung Fook Street	Advertising Production Company
25.	37 Hung Fook Street	Car repair and services
26.	39 Hung Fook Street	Car repair and services
27.	41 Hung Fook Street	Construction Materials Company
28.	43 Hung Fook Street	Engineering Company
29.	45 Hung Fook Street	Scaffolding Works Company
30.	47 Hung Fook Street	Car repair and services
31.	49 Hung Fook Street	Car repair and services
32.	51 Hung Fook Street	Cannot be identified*
33.	51 Hung Fook Street Shop B	Transportation Company
34.	2 Ngan Hon Street	Restaurant
35.	4 Ngan Hon Street	Restaurant
36.	6 Ngan Hong Street	Car Services

Service Unit	Operator	Address
<u>Support Team for the Elderly Based at District Elderly Community Centres</u>		
15. Hong Kong Sheng Kung Hui Lok Man Alice Kwok Integrated Service Centre	Hong Kong Sheng Kung Hui Welfare Council Limited	G/F, Block H, Lok Man Sun Chuen, 111 Ko Shan Road, To Kwa Wan, Kowloon
D. Rehabilitation and Medical Social Services		
<u>Integrated Community Centre for Mental Wellness</u>		
16. SRACP Vitality Place (Kowloon City) (1.1 Team)	Society of Rehabilitation and Crime Prevention, Hong Kong (The)	Unit 911B, 9/F, Tower B, Hunghom Commercial Centre, 37 Ma Tau Wai Road, Kowloon
<u>Integrated Programme in Kindergarten-cum-Child Care Centre</u>		
17. PLK Chan Lai Wai Lin Nursery (OCCS)/ Chan Lai Wai Lin Kindergarten-cum-Nursery (IP)	Po Leung Kuk	G/F, 1/F and 2/F, Honour Court, 188-194 Ma Tau Wai Road, Kowloon City, Kowloon
18. SA Lok Man Nursery School	Salvation Army (The)	1/F, Block H, Lok Chi House, Lok Man Sun Chuen, 111 Ko Shan Road, To Kwa Wan, Kowloon
E. Services for Offenders		
<u>Probation and Community Service Orders Office</u>		
19. Kowloon City Probation and Community Service Orders Office (2)	Social Welfare Department	11/F, To Kwa Wan Government Offices, 165 Ma Tau Wai Road, Kowloon City, Kowloon
F. Services for Young People		
<u>After School Care Programme</u>		
20. BGCAHK Lok Man Children and Family Integrated Activity Centre	Boys' and Girls' Clubs Association of Hong Kong (The)	G/F, Block G, Lok Man Sun Chuen, To Kwa Wan, Kowloon
<u>Children Centres with Reading/Study Rooms</u>		
21. BGCAHK Lok Man Children and Family Integrated Activity Centre	Boys' and Girls' Clubs Association of Hong Kong (The)	G/F, Block G, Lok Man Sun Chuen, To Kwa Wan, Kowloon

Service Unit	Operator	Address
<i>School Social Work Service (provided for all secondary schools and administered by offices shown below)</i>		
22. BBHK School Social Work Division	Boys' Brigade, Hong Kong (The)	G/F, Block A, Lok Man Sun Chuen, To Kwa Wan, Kowloon

Source: Social Welfare Department's website: Local District Service Profile: Welfare Service Units Managed or Funded by Social Welfare Department (Kowloon City), as of 9 October 2015.

8. INITIAL ASSESSMENT OF POTENTIAL SOCIAL IMPACT, AND MITIGATION MEASURES

Potential Social Impact

- 8.1 The Scheme falls under the “*Proposed Redevelopment Priority Areas*” in the KC DURF’s Urban Renewal Plan. There are suspected UBWs and presence of original units appear to be sub-divided resulting in crowded and unsatisfactory living environment. In this regard, the proposed redevelopment offers a chance of improving the living environment of the affected households, and continues the general renewal of the local area in line with the recommendations under the Kowloon City DURF, as mentioned in Para. 6.2.1 of the ‘Urban Renewal Plan for Kowloon City’.
- 8.2 The Scheme, if implemented, will inevitably affect the domestic and non-domestic occupants within the Project. Generally, the most vulnerable resident groups in the Scheme are the elderly, the disabled, single parent families, low-income households, and those who rely heavily on their social network (including receiving support/care from their friends/relatives who live nearby). Upon implementation of the Scheme, the existing residents will be displaced to areas where they have to rebuild their social networks, whereas the existing shops will have to move to other locations to continue their businesses, depending on individual operator’s needs.
- 8.3 During the freezing and SIA surveys, needy cases such as households with single elderly, elderly couples, family members with disability or new immigrants worrying about the impact of redevelopment on employment, living expenses and social network etc will be identified. The Social Service Team (SST) commissioned by the Urban Renewal Trust Fund (URTF) will provide assistance to those in need. This SST is independent of the URA.

Mitigation Measures and Prevailing Compensation & Rehousing Policies

- 8.4 As the affected households are having an unsatisfactory living environment due to deteriorating building conditions within their units, the proposed scheme, if implemented, should be considered acceptable by the affected residents.
- 8.5 Upon implementation, compensation based on the prevailing URA Policy would be offered to the affected owners. For affected tenanted households, rehousing or

ex-gratia payment would be offered. Shortly after the freezing and SIA surveys, the URA will carry out briefing sessions to the owners and tenants to explain the URA compensation and rehousing policies.

- 8.6 In handling problems related to different kinds of livelihood problems, the SST, apart from offering counselling, will mobilise different community resources to liaise closely with Government departments and work with the URA to resolve the residents' and operators' problems and reduce their anxiety. The SST will also provide orientation assistance for those in needs after moving home such as familiarisation with new neighbourhood, accommodation and local facilities.
- 8.7 For the vulnerable groups (including the elderly, disabled and single parent families), arrangements for assistance such as child care/foster services, domestic help services, etc. offered by the Social Welfare Department, and other social service agencies would be made. For the low-income households, arrangement could be made with the Hong Kong Housing Authority or the Hong Kong Housing Society on public rental housing allocation if they are eligible. In addition, if practicable, those vulnerable groups would be re-housed on compassionate ground as early as possible.

Prevailing Measures

- 8.8 The URA will offer an owner-occupier of domestic property the market value (valued on vacant possession basis) of his/her property plus an ex-gratia allowance, namely Home Purchase Allowance (HPA), for purchase of the property. The assessment of HPA is based on the value of a notional flat, which is defined as a seven-year-old flat in a building of comparable quality, situated in a similar locality in terms of characteristics and accessibility, and located at the middle floor with average orientation. The HPA is the difference between the value of the notional replacement flat and the market value of the property being acquired.
- 8.9 The URA may also offer 'flat-for-flat' (FFF) (subject to any changes in the relevant legislations) in a URA new development in-situ, or in the same district (as URA may select for the purpose provided that necessary approvals/authorization has been obtained at the time of FFF offer), as an additional choice to cash compensation to owner-occupiers of domestic units. The amount of cash compensation and ex-gratia payment offered to an owner-occupier will not be changed by his/her choice of using that amount, or part of it, to join the flat-for-flat arrangement or otherwise.

- 8.10 According to the new URS, the URA will offer an ex-gratia allowance to eligible elderly owners of tenanted domestic units on compassionate ground in exceptional circumstances such as elderly owners who rely on the rental income from their properties for a living.
- 8.11 Tenants who are not allocated re-housing due to various reasons or who decline re-housing, may receive ex-gratia payments. The amount of ex-gratia payment will be dependent on, amongst other things, whether the tenancies commenced before the date of the freezing survey and continued, or commenced on or after the freezing survey.
- 8.12 In case where tenants were threatened not to have their tenancies renewed, the URA will explain to the owners that they would not get more compensation by evicting the tenants. The URA has also introduced the "Domestic Tenants Compassionate Assistance Programme" to take care of those domestic tenants whose tenancies commenced before the freezing survey of the Project and who was required by their landlord to move out from the affected properties due to expiry or termination of their tenancies and before URA acquired the properties successfully.
- 8.13 For owner-occupied non-domestic properties, the market value of the affected property plus an ex-gratia allowance of 4 times the rateable value or 35% of the market value of the affected property, whichever is the higher, will be offered. Owner-occupiers may lodge a claim for business loss in lieu of the above two allowances. For owners of tenanted or vacant non-domestic properties, the market value of the affected property plus an ex-gratia allowance of 1 time the rateable value or 10% of the market value of the affected property, whichever is the higher, will be offered.
- 8.14 For non-domestic tenants of non-domestic properties, an ex-gratia allowance of 3 times the rateable value of the affected property will be offered. An additional payment of EGBA is also payable to tenants who commenced occupying the premises for business before the date of freezing survey. The amount is calculated at a rate of 0.1 times the rateable value for each year that the affected property has been in operation up to a maximum of 30 years, and subject to a maximum amount of HK\$500,000 and a minimum amount of HK\$70,000. Non-domestic tenants may lodge a claim for business loss in lieu of the above two allowances.

- 8.15 According to the new URS, if requested, the URA will help identify suitable premises in the district of the redevelopment projects to enable the affected shop operators to relocate and continue operation in the same district as far as practicable.
- 8.16 Details of the current acquisition, compensation and rehousing policies are published on the URA's website and will be communicated to affected persons when acquisition of property interests for this Project commences. Prevailing policies relating to property acquisition, rehousing and ex-gratia allowances will be reviewed by the URA from time to time.
- 8.17 The Stage 2 SIA to be conducted after the Freezing Survey will further assess the impact of the Project in detail on both domestic and non-domestic occupants and propose mitigation measures. It may also be able to highlight the psychological stress and worry for some of the affected within the Project. Special measures may have to be adopted under exceptional circumstances.

9. CONCLUSION

- 9.1 The local community and the surrounding neighbourhoods are likely to experience gains and losses due to the proposed redevelopment. Residents, business operators and their employees within the Project will be affected in different ways and to various degrees depending on their particular circumstances. Those who currently live in overcrowded or poor building condition within the Project may welcome the opportunity to improve their living environment through cash compensation or rehousing if eligible; whilst others (e.g. some business operators) may prefer to remain undisturbed and maintain the status quos. The various degrees of concerns and social impacts to the affected residents, business operators and their employees within the Project will be assessed in the Stage 2 SIA in details.
- 9.2 This Stage 1 SIA study can only provide a general profile of the Project. Based on past URA experience of similar scale and context of redevelopment projects, it can be expected that there will be some sharing of living quarters and a relatively low average household income for those within the Project. The assumptions in this report will be verified by the Stage 2 SIA to be carried out after the freezing survey. The needs of the affected households will be assessed and appropriate arrangements to minimise major adverse social impact, if any, from the Project will be proposed in the Stage 2 SIA.
- 9.3 For the non-domestic uses, a number of ground floor shops are found in the Project, whereas the upper floor non-domestic uses, if any, are to be recorded in the freezing survey upon commencement of the Project under section 23 of the URAO. The ground floor business activities are commonly found in the surrounding area. It is possible for most of them to be relocated to other areas where the respective uses are permissible in both lease and planning terms, and in compliance with the DMC of the buildings. The needs of the affected non-domestic occupants will be assessed in the Stage 2 SIA.

URBAN RENEWAL AUTHORITY

June 2016

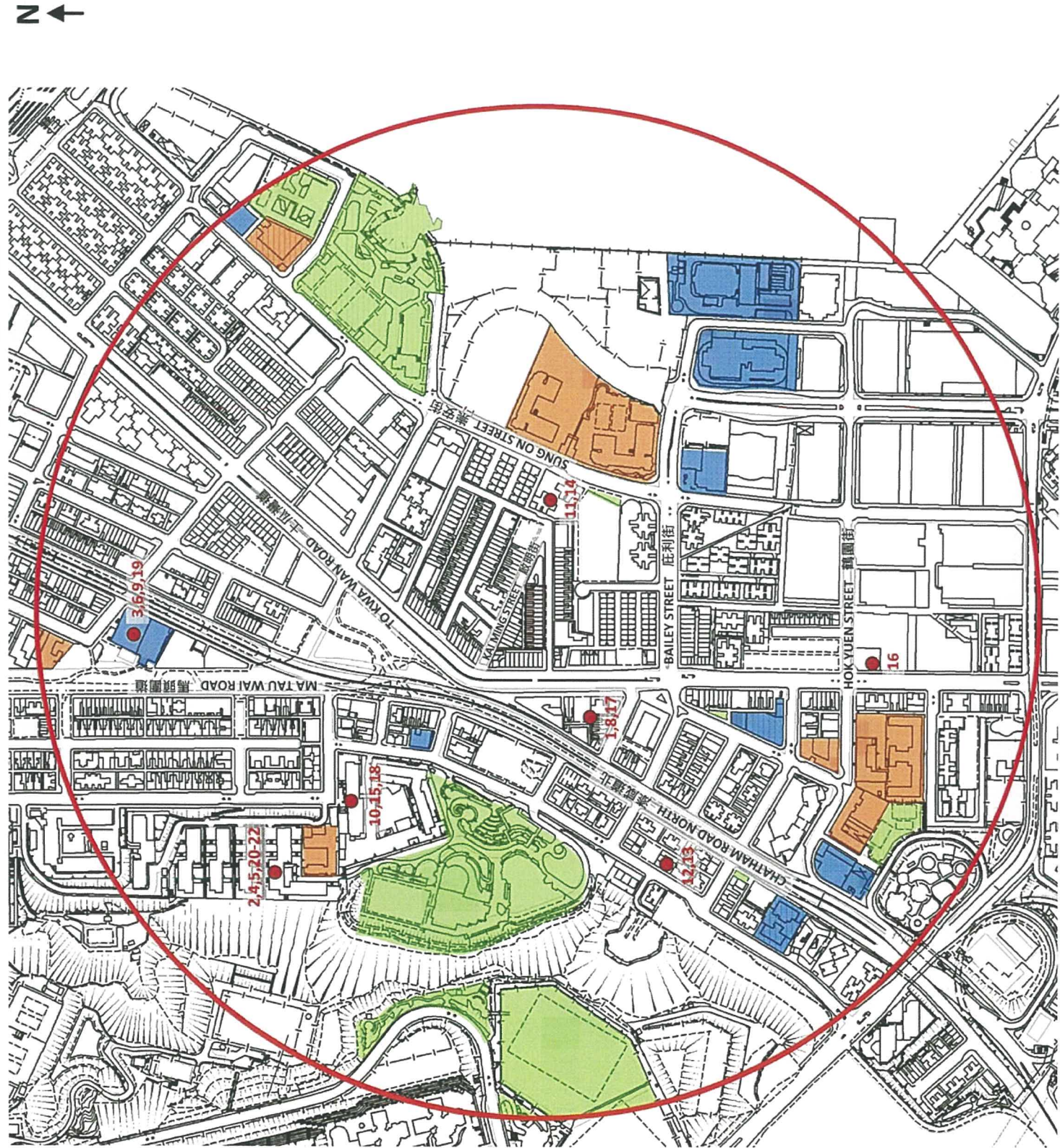
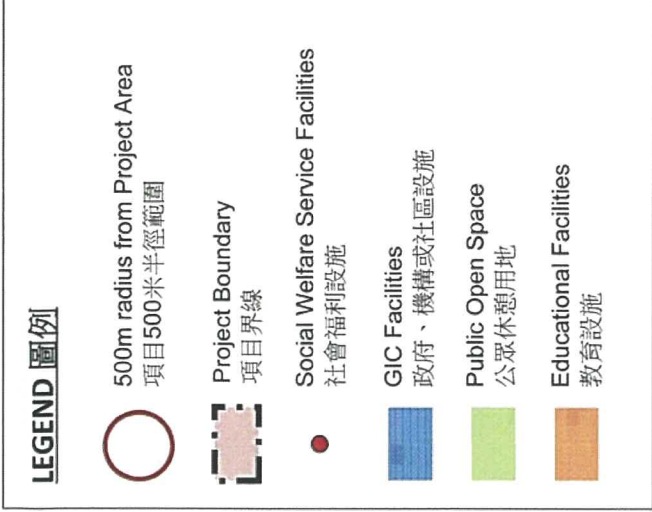


Figure 7.1 GIC and Amenity Facilities Within 500m Radius of the Project Area

圖 7.1 項目500米半徑範圍內的政府、機構或社區設施及公眾休憩用地

PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 6

Tentative Implementation Programme

**KC-010 URA Hung Fook Street / Ngan Hon Street Development Scheme
Tentative Implementation Programme**

[illegible]

PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 7

**Acquisition and Resumption of Affected
Properties**

ACQUISITION AND RESUMPTION OF AFFECTED PROPERTIES

1. Section 29(1) of the Urban Renewal Authority Ordinance (URAO) empowers the Urban Renewal Authority (URA) to apply in writing to the Secretary for Development requesting her to recommend to the Chief Executive in Council the resumption of properties affected by its redevelopment projects.
2. The URA normally makes offers to acquire affected properties by agreement before seeking resumption. Such offers are based on Government policies (approved by the Finance Committee of the Legislative Council) regarding ex-gratia allowances applicable when private property interests are resumed. In outline, the principles adopted by the URA in property acquisition are as follows:

Buildings in Multiple Ownership

A Domestic Properties

- For owner-occupiers, the market value of the property plus a Home Purchase Allowance (HPA) defined as the difference between the market value of the property and the value of a notional replacement flat based on a seven-year-old flat of a size similar to the property and in the same locality according to the guidelines announced by the Government. A unit rate of a notional replacement flat (i.e. dollar per square meter) is assessed for each project.
- For owners of tenanted and vacant flats, the market value of the property plus a Supplementary Allowance (SA) of 50% of the HPA described above for the first such flat and 25% of HPA for a second such flat.
- An owner will be eligible to receive HPA or SA for a maximum of 3 flats per project, including a maximum number of SA for two flats.
- In addition, an Incidental Costs Allowance (ICA) is offered to owners of domestic flats to contribute to costs incurred (legal and professional fees, removal etc.). The ICA rate is reviewed bi-annually.
- "Flat for flat" (FFF) in a URA new development in-situ or in the same district (as URA may select for the purpose provided that necessary approval/ authorization has been obtained at the time of FFF offer) as an additional choice to cash compensation to owner-occupiers of domestic units (subject to any changes in the relevant policies).
- An Elderly Domestic Owner-Landlords Compassionate Allowance will

be offered to eligible elderly owners of tenanted domestic properties on compassionate grounds in the circumstances that the elderly owners rely on the rental income from their sole properties to sustain their livelihood.

B Non-Domestic Properties

Non-domestic properties are primarily ground-floor retail premises and upper floor commercial premises.

- For owner-occupied premises, the market value plus an ex-gratia allowance of 4 times the rateable value or 35% of the market value of the affected property whichever is the higher. In addition, an Ex-gratia Business Allowance is payable to owner-occupier who had commenced occupying the property for business before the date of Freezing Survey. Owner-occupiers may lodge a claim for business loss in lieu of the above two allowances.
- For owners of tenanted properties, market value plus an ex-gratia allowance of 1 times the rateable value or 10% of the market value of the affected property whichever is the higher.

C Roof-top Interests

- For owner-occupiers, market value on an open roof basis (disregarding any illegal structures). If the owner meets the normal public housing eligibility criteria, he/she may be offered re-housing. Owners, who are not eligible for re-housing or who do not wish to accept it, will be offered an ex-gratia allowance of 10% of the value of the affected property.
- For owners of tenanted or vacant properties, market value on an open roof basis (disregarding any illegal structures and any rent passing) plus an ex-gratia allowance of 10% of the value.

Buildings in Single Ownership

The higher of:

- Existing Use Value (EUV) plus HPA/SA and ICA and ex-gratia allowances for non-domestic properties as appropriate; or
- Redevelopment value plus an ex-gratia allowance of 5%.

3. Prevailing acquisition policies are subject to review by the URA from time to time. Details of the current policies are published on the URA's website and will be communicated to affected persons when acquisition for a specific project commences.
4. If the URA is unable to reach agreement to acquire affected properties, resumption will be sought under section 29(1) of the URAO as described above.

URBAN RENEWAL AUTHORITY
JUNE 2016

PART 3

**SUPPLEMENTARY
INFORMATION**

Appendix 8

**Rehousing and Ex-Gratia Payment
Package for Domestic and Non-domestic
Tenants**

REHOUSING AND EX-GRATIA PAYMENT PACKAGE FOR DOMESTIC AND NON-DOMESTIC TENANTS

Domestic Tenants

1. The Urban Renewal Authority (URA) will offer an ex-gratia payment to domestic tenants residing in the project area at the time of clearance. For eligible domestic tenants, rehousing will be offered as an alternative to the ex-gratia payment.
2. Re-housing will be offered in units provided by the Hong Kong Housing Authority or the Hong Kong Housing Society subject to the normal eligibility requirements of those bodies. An ex-gratia removal allowance will be offered to eligible domestic tenants who are rehoused.
3. Domestic tenants who are not allocated rehousing units provided by the Hong Kong Housing Authority or the Hong Kong Housing Society due to various reasons may be re-housed in units provided by the URA subject to fulfillment of the prevailing eligibility criteria of the URA. An ex-gratia removal allowance will also be offered to eligible domestic tenants who are rehoused.
4. Domestic tenants who are not allocated rehousing units due to various reasons or who decline re-housing may receive ex-gratia payments based on 3.5 times the rateable value of the area occupied by the tenant if they commence occupying the premises under valid tenancies before the date of Freezing Survey of the project.
5. In case where tenants were threatened not to have their tenancies renewed, "Domestic Tenants Compassionate Assistance Programme" can be applied by those domestic tenants whose tenancies commenced before the Freezing Survey of the Project and who was required by their landlord to move out from the affected properties due to expiry or termination of their tenancies and before URA acquired the properties successfully.

Non-Domestic Tenants

6. For non-domestic tenants, an ex-gratia allowance of 3 times the rateable value of the affected premises will be offered. An additional payment of ex-gratia business allowance (EGBA) is also payable to any tenant who operates business at the affected premises. The amount is calculated at a rate of 0.1 times the rateable value of the affected premises up to a

maximum of 30 years, and subject to a maximum amount of \$500,000 and a minimum of \$70,000.

7. Details of the current policies are published on the URA's website and will be communicated to affected persons when clearance for a specific project commences. Prevailing policies relating to re-housing and ex-gratia allowances will be reviewed by the URA from time to time.

URBAN RENEWAL AUTHORITY
JUNE 2016

Hung Fook Street / Ngan Hon Street

Development Scheme (KC-010)



Stage 2 Social Impact Assessment

July 2016

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Appendix1 : Non-domestic G/F premises within the Scheme Area

1. INTRODUCTION

- 1.1 The new Urban Renewal Strategy (URS) issued by the Government in February 2011 states that the Urban Renewal Authority (URA) will carry out Social Impact Assessment (SIA) studies in the form of “a Stage 1 social impact assessment... before the publication of any proposed redevelopment project in the Government Gazette”, and “a Stage 2 social impact assessment... after the proposed project has been published in the Government Gazette”.
- 1.2 URA published in the Government Gazette the commencement of the Hung Fook Street / Ngan Hon Street Development Scheme (KC-010) (the Scheme) on 3 June 2016. On 14 June 2016 Stage 1 SIA was made available for public inspection. This Stage 2 SIA report is based on the factual data and opinions collected as part of the freezing survey for this Scheme conducted from 3 June 2016 to 5 June 2016, and from the follow-up survey visits by appointments conducted up to 3 July 2016.
- 1.3 This report covers the elements listed in paragraph 37 of the URS for the affected residents, families and businesses within the Project, including:
- (a) the population characteristics of the residents affected by the proposed project;
 - (b) the socio-economic characteristics of the affected residents;
 - (c) the rehousing needs of the affected tenants;
 - (d) the relocation needs of the affected shop operators;
 - (e) the housing preferences of the affected owners and tenants;
 - (f) the employment status of the affected owners and tenants;
 - (g) the place of work of the affected owners and tenants;
 - (h) the social networks of the affected owners and tenants;
 - (i) the educational needs of children of the affected families;
 - (j) the special needs of the elderly;
 - (k) the special needs of the disabled;
 - (l) the special needs of single-parent families, particularly those with small children;
 - (m) a detailed assessment of the potential social impact of the proposed project; and
 - (n) a detailed assessment of the mitigation measures required.
- 1.4 St. James' Settlement has been commissioned by the Urban Renewal Fund (URF) to act as the Social Service Team (SST) for this scheme. They are tasked to provide assistance and advice to residents and operators affected by the Scheme. Cases requesting assistance and those identified in the course of the SIA analysis as requiring assistance have been referred to the SST for their follow-up action.

2. BACKGROUND

- 2.1 The proposed development scheme (the Scheme) comprises buildings at Nos. 1-51 Hung Fook Street (odd nos.) and Nos. 2-42A Ngan Hon Street (even nos.), To Kwa Wan, Kowloon City (**Figure 2.1**).

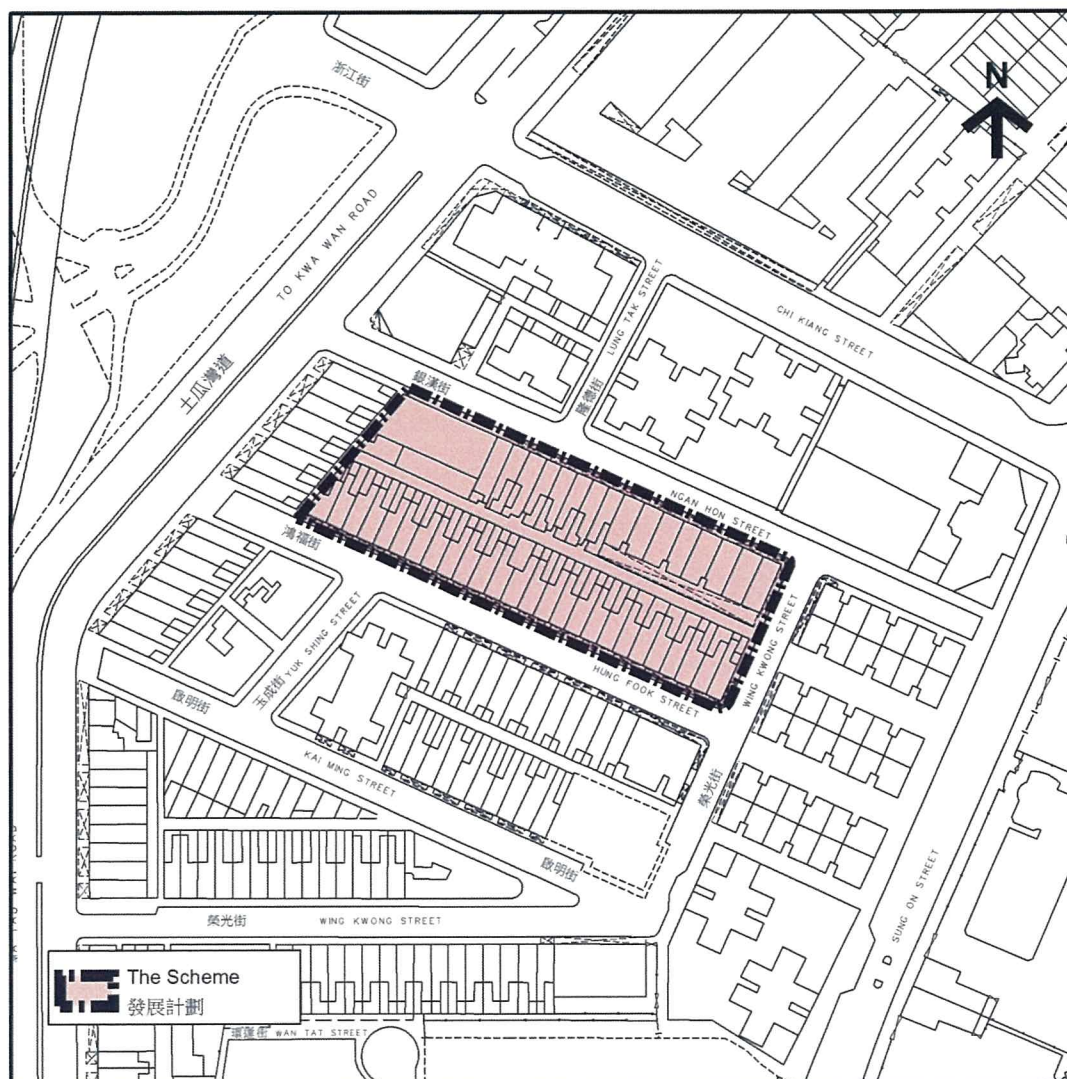


Figure 2.1 Location Plan

- 2.2 The Scheme is bounded by Ngan Hon Street to the north, Wing Kwong Street to the east and Hung Fook Street to the south and a back lane of the adjacent buildings to the west. It covers an area of about 4,951m², including portion of surrounding public pavement where some of the affected buildings overhang, and a Government back lane. The net site area used for GFA calculation (which includes the Government lane but excludes the pavement) is about 4,562m². The existing height of the buildings ranges from 6 to 8 storeys.

- 2.3 The Scheme falls within one of the “Proposed Redevelopment Priority Areas” in the Kowloon City District Urban Renewal Forum’s (KC DURF’s) Urban Renewal Plan. The KC DURF’s Urban Renewal Plan recommended that in the “Proposed Redevelopment Priority Areas”, *“redevelopment should be given priority as the urban renewal approach for the area”*. The DURF SIA, in particular the community profile compiled for this Priority Area, will be taken into consideration in this report.
- 2.4 There are a number of URA projects in the proximity to the Scheme. In order to realise the community benefits under a district-based approach, on 3 June 2016, URA also self-initiated Hung Fook Street/Kai Ming Street (KC-011) and Wing Kwong Street (KC-012) redevelopment projects under Section 26 of the URAO located to the immediate south and the southwest of the Scheme (**Figure 2.2** refers). The Bailey Street/Wing Kwong Street Development Project (KC-009) was commenced in March 2016 and is located further south of the Scheme.
- 2.5 To improve traffic accessibility and enhance pedestrian connectivity in the area, it is intended to provide a through road extending from Wan On Street via URA projects KC-009, KC-010, KC-011 and KC-012 to Ngan Hon Street. An underground car park will be provided at the Scheme accommodating car parking spaces, loading/unloading bays and coach parking spaces to serve the area bounded by Ngan Hon Street, To Kwa Wan Road, Wing Kwong Street and Sung On Street, as well as coach parking spaces. Such arrangement can maintain street fronts of the area by discouraging necessities of open up run in and out in individual developments.
- 2.6 To the immediate southeast of Development Project (KC-011) is the Kai Ming Street Demand-Led Development Project (DL-8:KC, Nos. 41-51 Kai Ming Street (odd nos.)), which was commenced in response to the dangerous building condition found at Nos. 45, 47, 49 and 51 Kai Ming Street, where Buildings Department applied for a closure order issued on 30 August 2013 and demolition orders issued on 19 December 2013.
- 2.7 Further south of the Scheme is the URA Ma Tau Wai Road/Chun Tin Street Development Project (TKW/1/002) which was commenced in response to the tragic collapse of No. 45J Ma Tau Wai Road on 29 January 2010, which caused loss of lives and seriously affected the structural integrity of adjacent buildings along Ma Tau Wai Road.

2.8 East of TKW/1/002 is another URA project - URA Chun Tin Street/Sung Chi Street Development Scheme (KC-008(A)) commenced on 6 May 2016. **Figure 2.2** shows the location of URA projects in the vicinity.

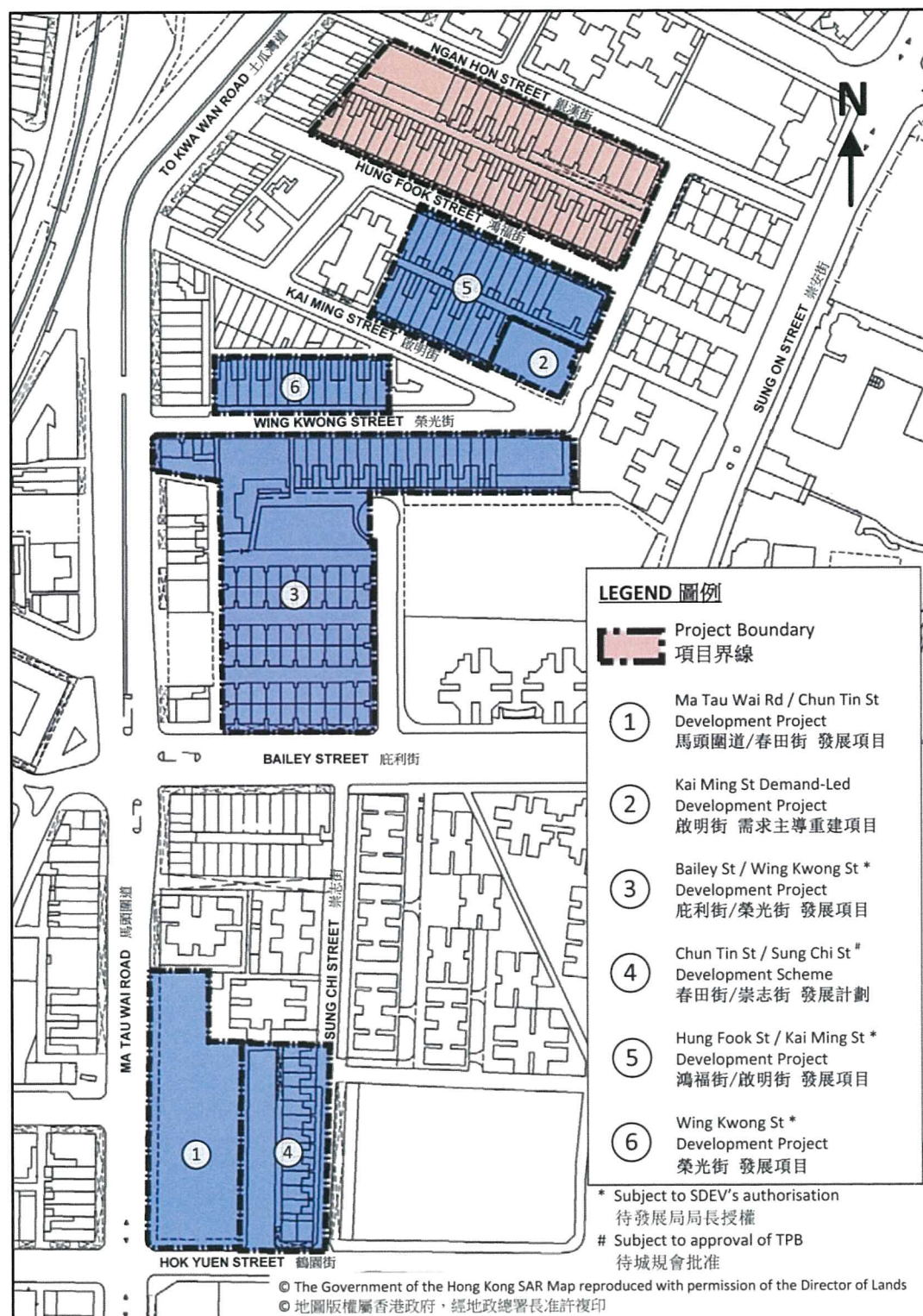


Figure 2.2 URA projects in the vicinity

Distribution of Units and Households

- 2.9 According to the approved General Building Plans (“GBPs”) and Occupation Permits (“OPs”) for the buildings within the Scheme, there are 368 units for domestic uses of which 354 units are above the ground floor while 14 units are on the ground floor. In addition, 33 units on the ground floor are designated as “Shop”. A total of 368 domestic units and 33 non-domestic units will be adopted for this SIA report. The results of the Freezing Survey (“FS”) will reflect the existing occupancy situation and uses.
- 2.10 Some 607 units (including self-contained sub-divided units, cockloft units and roof structures) being used for domestic purpose were identified in the FS, 538 (89%) were successfully surveyed, 12 units were found to be vacant and no survey was conducted for the remaining 57 units since the residents of these units could not be contacted or refused to respond (up to 3 July 2016). The number of domestic units found in the Project is higher than the total 368 original units shown in the approved GBPs and OPs for the buildings within the boundary of the Scheme, mainly because about 37% of units (135 units) have been sub-divided and also the presence of roof structures.

	Living Units
Successfully surveyed (for domestic uses)	538
Surveyed Vacant	12
No contact/ Refused to respond	57
Total	607

Table 2.1 Number of living units and households found within Scheme boundary

- 2.11 A total of 576 households were surveyed and the owners of 12 vacant units have submitted FS/SIA forms, so the following assessment will be based on 588 surveyed FS questionnaires, of which 495 (about 84%) have responded to both FS and SIA forms. Ninety-three (93) (about 16%) only answered the FS form and refused to answer the SIA questions. The following assessment related to SIA will be based on

the 495 surveyed SIA forms. Those who gave no response to particular FS/SIA questions in the surveys are categorized as “No Response” when processing/analyzing the relevant data in this report. Those who refused to do SIA questionnaire are not counted in the statistics relating to questions of the SIA questionnaire.

	Household Nos.
Total No. of surveyed households	576
Vacant Units	12
Successfully responded to both FS (successfully surveyed) and SIA questionnaire (successfully interviewed).	495
Only responded to FS (successfully surveyed) but refused to do SIA questionnaire (Refused to do the SIA).	93

Table 2.2 Results of FS and SIA surveys within the Scheme

- 2.12 Regarding non-domestic uses, FS records have shown that there are some 75 non-domestic premises, including 57 ground floor premises, 16 staircase premises and 2 cockloft premises with 6 of them were found to be mixed uses. 71 business operators were surveyed of which 68 operators answered both the FS and SIA surveys. 3 operators only responded to the FS survey, refusing to respond to the SIA questionnaire. The responses of the 71 operators form the basis of Section 11 (Business Impact) of this report.

Physical condition and living environment

- 2.13 For the domestic household, among the 495 households who have completed both FS and SIA form, nearly 54% of the interviewed households indicated that their units occasionally or frequently suffered from water seepage and about 58% indicated the same frequency of problem with concrete spalling. Around 35% of households indicated that their units occasionally or frequently suffered from problem of no flushing water supply. It indicates that over half of the households are not satisfied with the physical conditions of their units. Regarding the living environment within the units, 65%, 65% and 74% of the respondents indicated that they occasionally or frequently suffered from problems of noise nuisance, poor indoor and outdoor air quality respectively. It indicates more than two-third of the households considered their living environment to be unsatisfactory. **Figure 2.3** shows the opinions of the

interviewed households on the physical condition and the living environment within their units.

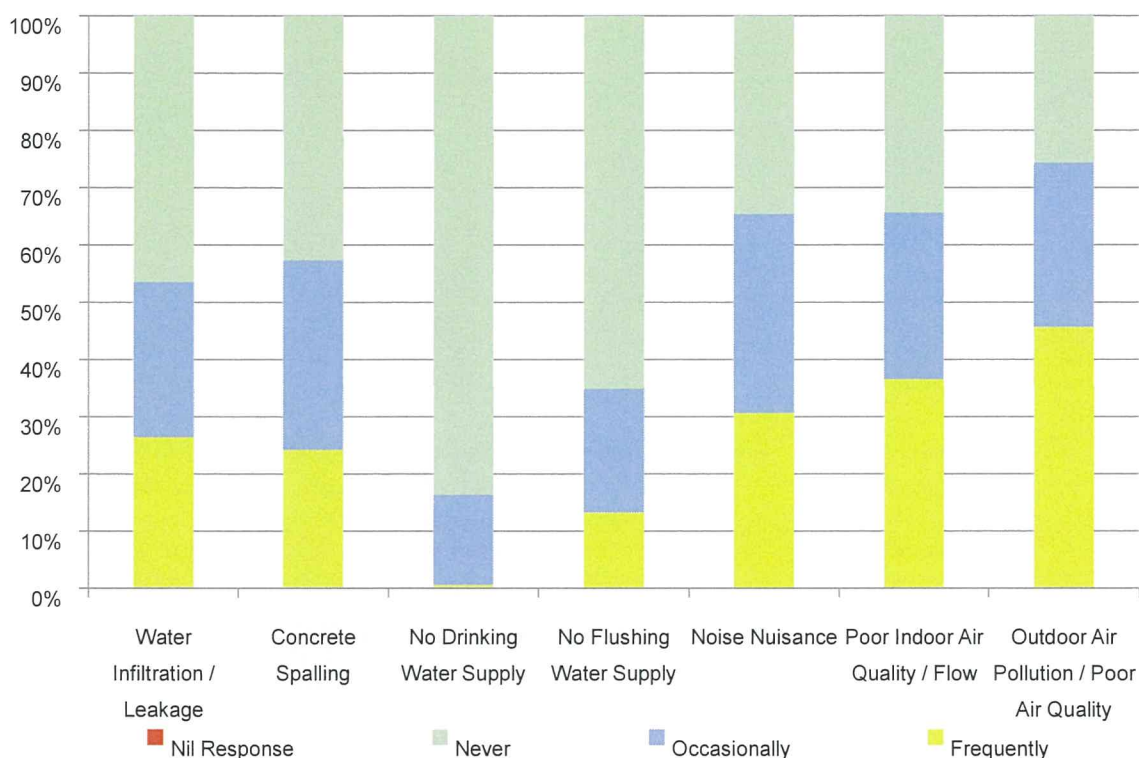


Figure 2.3 Opinions on the Physical Conditions and Living Environment of their Units

- 2.14 On the question of fire safety and hygiene concerns, about 54% of the interviewed households commented that they occasionally or frequently suffered from problem of obstruction of staircases / corridors. About 71% of households responded that they suffered of poor hygiene condition of common areas (e.g. in yards, staircases, corridors, etc). On security matters, about 53% and 23% of interviewed households expressed that they experienced occasionally or frequently loitering of strangers, and crime events, e.g. burglary, robbery, drug abuse, respectively. **Figure 2.4** shows the opinions of the interviewed households on the living environment of their units in terms of fire safety, hygiene and security issues.

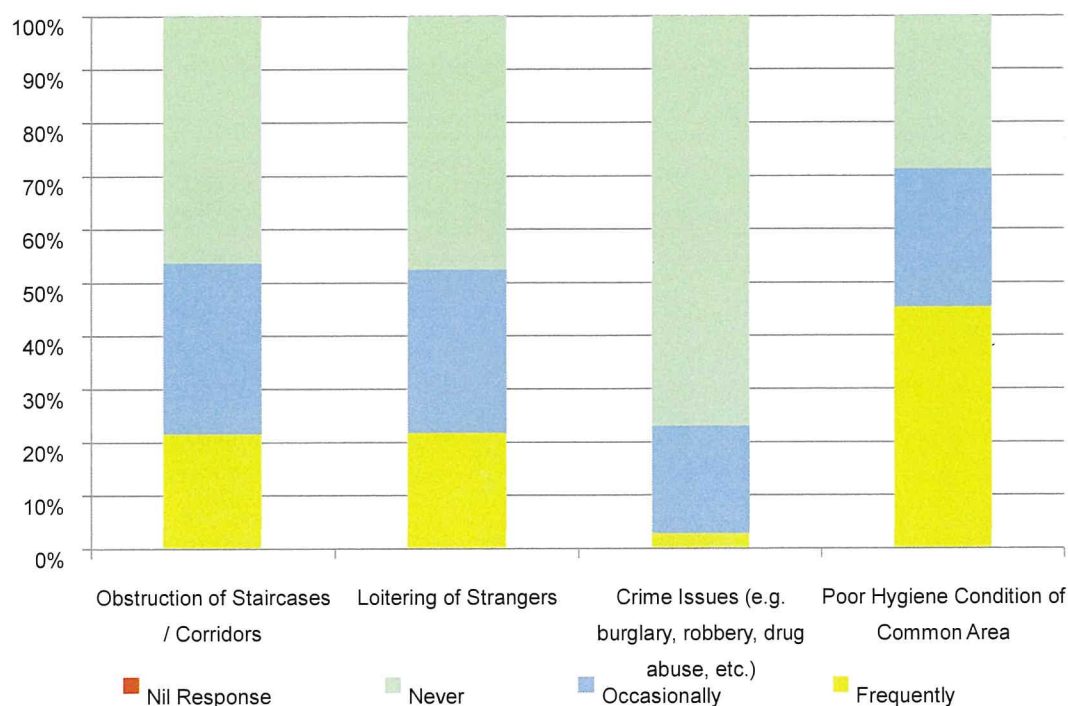


Figure 2.4 Opinions on the Living Environment of their Units

Views on redevelopment

2.15 Of the 495 households who responded to the SIA questionnaire, 395 households (about 80%) of respondents support or strongly support the development scheme. Seventy eight (78) households (about 15%) had no comment, and only 19 households (around 4%) did not support the redevelopment and 3 households (about 1%) were strongly against it.

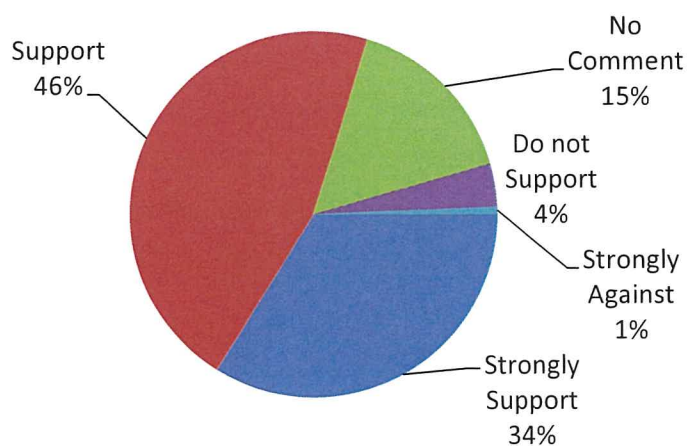


Figure 2.5 Views on Proposed Scheme

3. POPULATION & HOUSEHOLD CHARACTERISTICS

- 3.1 Except stated otherwise, the territorial average numbers used for comparison in this report is based on 2011 Population Census results published by the Census and Statistics Department in March 2012.
- 3.2 A total of 1,462 residents were recorded in the 576 successfully inhabited surveyed households within the Project, resulting in an average household size of 2.5 persons. It reflects a lower average household size in the Scheme area as compared to the territorial average of 2.9 persons per household. This lower average household size may be due to the presence of relatively large number of singletons and doubletons (334 households), which accounts for about 58% of the total number of households in the Project.
- 3.3 Based on the number of surveyed households (576) and the successfully surveyed living units (538) within the Scheme, the degree of sharing (or the “average number of domestic households per unit of quarters”) in the Scheme is 1.07 (576 households / 538 surveyed units), which is comparable to the territory-wide average of 1.0 for private permanent housing in the 2011 Census. However, this figure does not truly reflect the overcrowding situation within the Scheme. If the original number of domestic units in approved GBPs and OPs are adopted as the basis, the degree of sharing will be about 1.57 (576 households / 368 original units).
- 3.4 The degree of sharing is much higher within those sub-divided units. Of the 368 original residential units indicated in the approved GBPs and OPs, 135 units (37%) were found to be sub-divided. Three-hundred and seven (307) households were surveyed in these subdivided units. If the 307 households living in sub-divided units are considered as “sharing” of units and the original number of domestic units is adopted as the basis, the degree of sharing is at about 2.3 (307 households / 135 original units). Among these sub-divided units, only the conversion of 27 number of property interests into 61 number of property interests have been registered in the Land Registry.
- 3.5 In addition to the sub-division of the original domestic units, there are many rooftop structures being used for domestic purpose within the Project. **Table 3.1** shows the sub-division of domestic units. **Figure 3.1** shows the percentage of sub-division of approved GBP units of the Project.

- 3.6 There are 12 vacant units within the Scheme. Since the owners of these 12 units have also responded to the FS and/or SIA forms, their views will be included in the assessment where applicable.

		GBP	Living Units	Households
Non-subdivided	<i>Surveyed units</i>	193	193	193
Subdivided	<i>Surveyed units (self-contained)</i>	135	308	307*
	<i>Surveyed units (partitioned rooms)</i>	15	18	57
	<i>Surveyed Roof structures</i>	N/A	13	13
	<i>Surveyed Non-dom unit for mixed use</i>	N/A	6	6
Sub-total Surveyed Inhabited Units		343	538	576
Vacant Units		10	12	12
Total Surveyed Units		353	550	588
Unsurveyed/refused to be surveyed units		15	57	N/A
Grand Total		368	607	588

* One household occupied 2 living units.

Table 3.1 Sub-division of domestic units

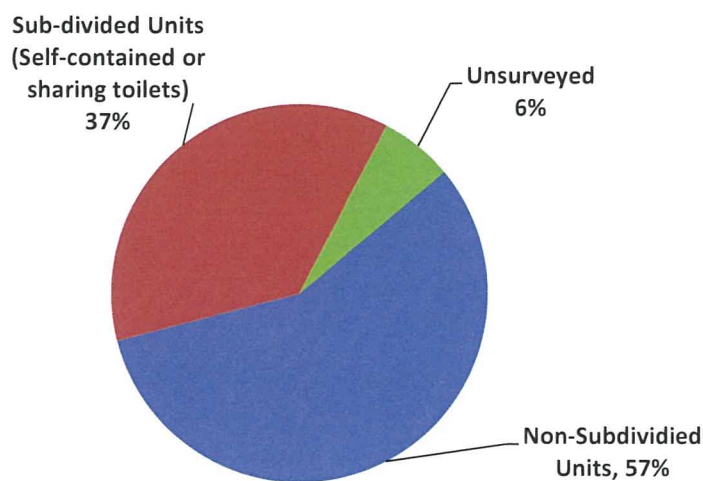


Figure 3.1 Percentage of Sub-division of Approved GBP and OPs units

- 3.7 A total of 1,462 residents were recorded in the Freezing Survey, 740 were male, 721 were female. One (1) persons did not fill in this part of the questionnaire. The distribution gives a ratio of 103 male residents to every 100 female residents.
- 3.8 Among the 1,396 residents who disclosed their age information, the economically active age group of 25 to 64 accounts for the majority of the total population (about 60.6%). The number of residents in this age group is comparable to the corresponding territory-wide level of 62.7%, while the youth age group of 15-24 (about 12.5%) is also close to the territory-wide level of 12.4%. The 0-14 age group, representing about 14.3% of the population in the Project, is higher than the corresponding territory-wide level (11.6%). The elderly group (65 and above), representing about 12.6% of the total population in the Scheme, is slightly lower than the corresponding territory-wide level of 13.3%. The findings of the survey show that the percentage share of the number of children and elderly in the total population of the Scheme is not particularly high. It is anticipated that the assistance required to support this more vulnerable group should be manageable. **Figure 3.2** showed the age structure of the Scheme.

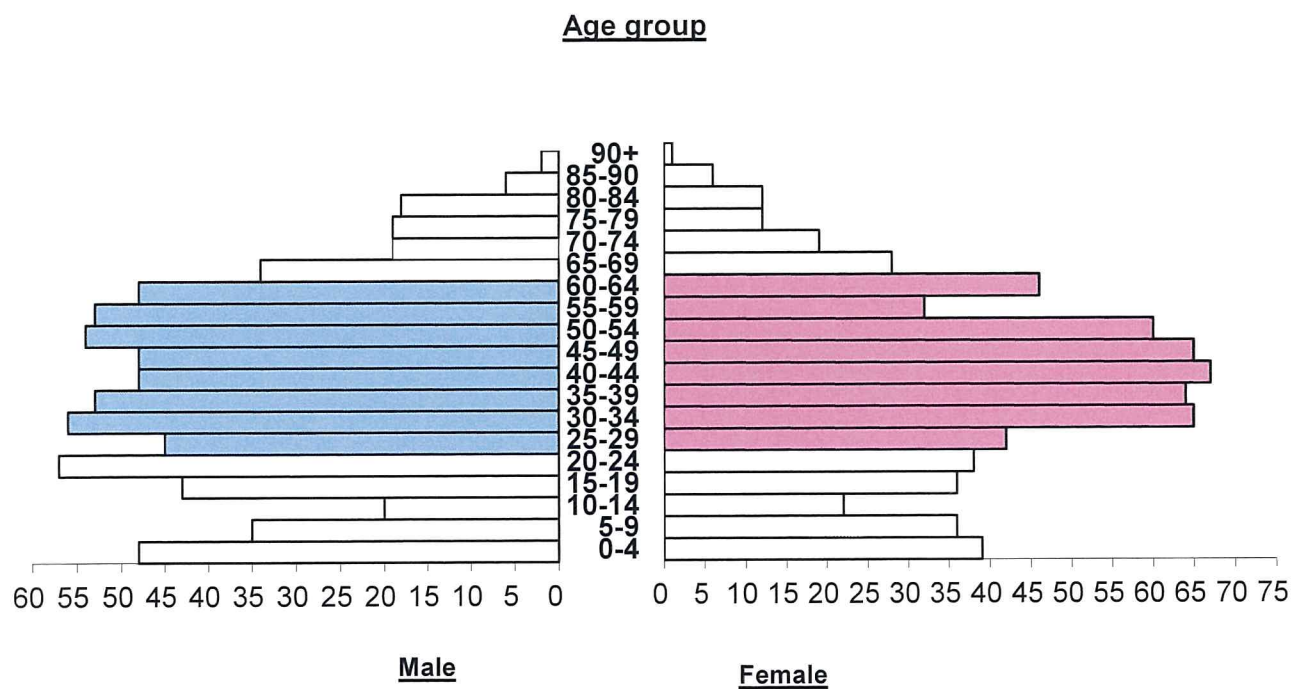


Figure 3.2 Age Structure

4. SOCIO-ECONOMIC CHARACTERISTICS

Income Level

- 4.1 Of the 495 households responded SIA questions, 411 households have responded to the this question and are recorded and analyzed. As shown in **Figure 4.1**, approximately 30% of the households have monthly income less than HK\$10,000 per month, which is higher than the territory-wide average of 24%. For households receiving less than HK\$4,000 per month, the proportion was about 10% which is slightly higher than the territory-wide average of 9% as reported in the 2011 Census. A higher proportion of interviewed households (45%) have monthly income of more than HK\$15,000 per month. Based on those who responded, the percentage of low-income household living within the Project appears to be low.

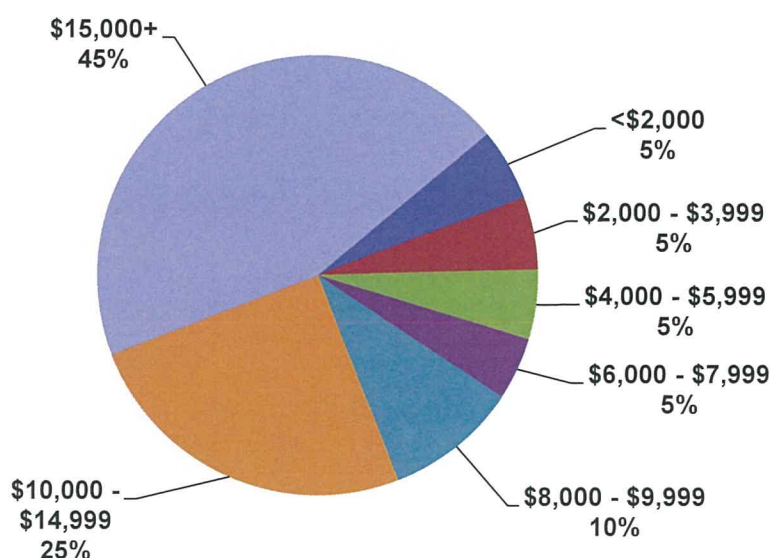


Figure 4.1 Household Income (HK\$ per month)

- 4.2 Out of the 495 households, only 49 households (i.e. around 10% of the total households) were recorded as currently receiving Comprehensive Social Security Assistance (CSSA). The percentage share of the population in the Project receiving CSSA (10%) is much higher than the territorial level of 5.3% as at end 2014¹. The URA and the SST will pay particular attention and to offer assistance to those in need of help and refer them to relevant services and practical assistance from various Government Departments and services providers.

¹ 'Statistics on Comprehensive Social Security Assistance Scheme, 2004 to 2014, Feature Article of Hong Kong Monthly Digest of Statistics', September 2015, Census and Statistic Department.

Residence

- 4.3 Approximately 88 out of 1,462 residents (about 6%) replied that they were subject to conditional stay, as shown in **Figure 4.2**. All the residents in this group could not meet the eligibility criteria for public rental housing as applied by the Hong Kong Housing Authority (HKHA) and the Hong Kong Housing Society (HKHS). Subject to the merits of individual cases, some may be considered under special circumstances by the SST and the URA and rehousing may be offered on genuine compassionate grounds. Assistance will also be provided, if requested, in finding potential suitable premises at affordable rent in the private market.

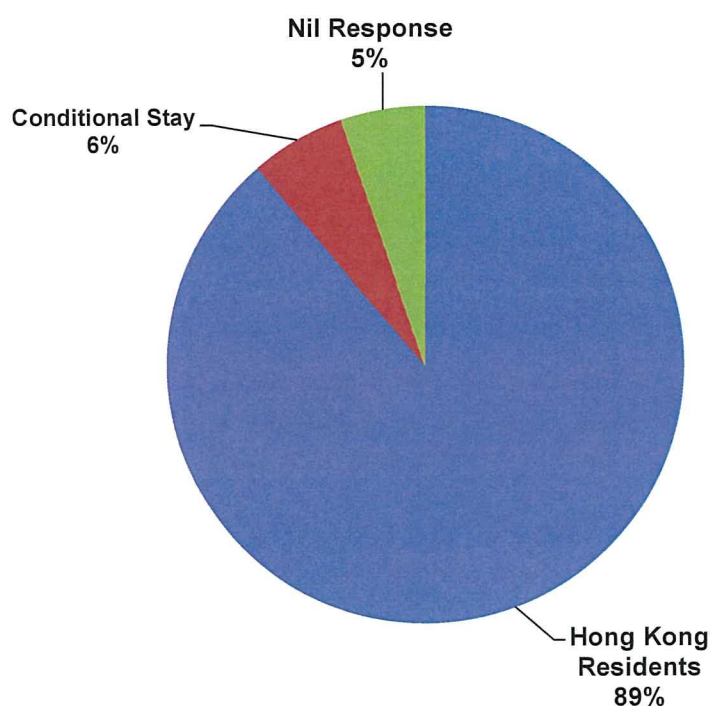


Figure 4.2 HKSAR Resident Status

- 4.4 **Figure 4.3** indicates the length of time the affected households have resided in their current abode. Out of 576 households, 308 households (about 53%) of households have lived in the Scheme for less than 5 years, of which 90 households (about 15%), less than a year. 125 households (about 22%) have lived within the Scheme for over 20 years. Of this group of households living over 20 years, 82 had elderly family members. This elderly group might find it more difficult to adjust to a new living environment. The assistance of the SST in providing orientation services and holding community gatherings will be important in helping these residents adapt to their new environment.

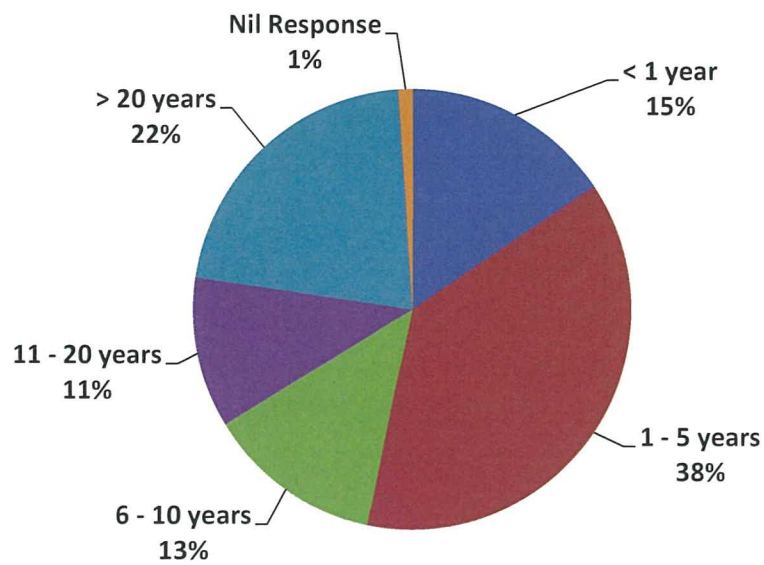


Figure 4.3 Period of Residence at Current Premises

5. HOUSING

Re-housing Needs and Location Preference

- 5.1 Owner-occupiers accounted for about 34% (199 households)² of households (which includes the owners of the 12 vacant units) in the survey. This rate of owner occupancy is lower than the territory-wide average of 52% and is not surprising given a number of units were sub-divided within the Scheme which appear to be for rent. Tenants (including principal tenants and sub-tenants) accounted for about 64% of households surveyed (588 households). Licensees / occupiers accounted for about 2% of households surveyed (13 households). For analyses purposes, 376 tenant households and 13 licensees / occupiers will be summarised as 389 tenant households in this report. The occupancy status³ of the surveyed households is shown in **Figure 5.1**. The high proportion of tenants in the Project may result in a higher demand for rehousing services should the Scheme be authorized to be implemented and subject to their eligibility for rehousing. There are households living in the rooftop structures. Since the roof of all the buildings in the Scheme are open roofs without approved domestic units according to the approved GBPs, all the rooftop structures are considered to be unauthorised structures. For rooftop structure occupiers, apart from meeting the eligibility criteria adopted by the HKHA in rehousing, there are other specific criteria⁴ that the rooftop structure occupiers have to satisfy in order to be eligible for public rental housings. For rooftop households which are not eligible for rehousing but with genuine needs, they may be considered as special cases and rehousing may be offered on genuine compassionate grounds.

² The owner-occupier status has not been confirmed yet. All responses related to owner-occupiers are based on the questionnaire surveys only.

³ The occupancy status of the surveyed households has yet to be ascertained.

⁴ The relevant specific criteria are listed in the URA website "Re-housing Eligibility of Illegal Rooftop Structure Occupiers":
<http://www.ura.org.hk/en/schemes-and-policies/redevelopment/ura-implemented-projects/ex-gratia/urao/ura-o-rehousing.aspx>

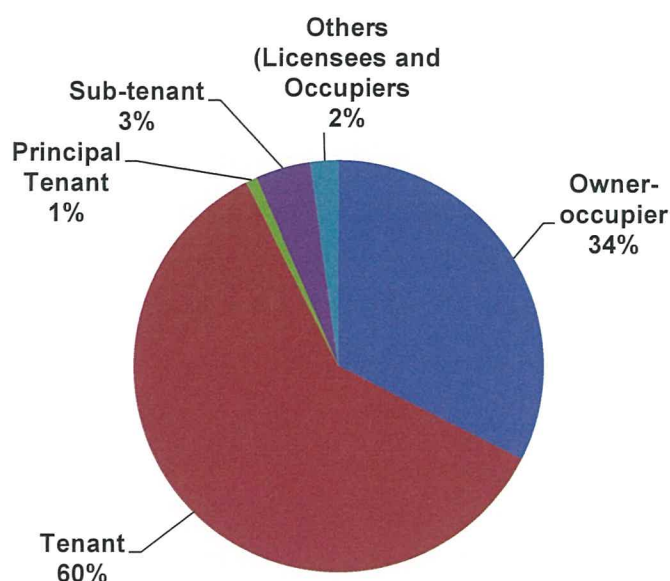


Figure 5.1 Occupancy Status of Households (588 surveyed households)

Affected Owners

- 5.2 Among the 199 surveyed owner-occupier households, 108 (about 54%) indicated their preference of finding alternative accommodations in the same district as where they are living, i.e. Kowloon City District. Ten (10) owner-occupiers responded that he would look for alternative accommodation in other districts. 60 (about 30%) owner-occupiers responded that they had yet to decide and 21 owner-occupiers did not respond to the question.
- 5.3 When looking for new accommodation, 95 owner-occupier households (about 48%) indicated that they would prefer to look for flats with similar size, 55 owner-occupiers (about 28%) preferred bigger flats, and 12 owner-occupiers (about 6%) preferred smaller flats. Twelve (12) owner-occupiers (about 6%) had not yet decided. 25 owner-occupier (about 13%) did not respond to the question.
- 5.4 Among the 199 surveyed owner-occupier households, 1 expressed intention to move to a flat older than their existing premises in terms of building age. Six (6) owner-occupiers (3%) expressed that he would like to move to a flat with comparable building age as the existing one. The majority (121 or about 61%) preferred newer flats and 29 (about 15%) preferred first-hand new flats. Fourteen (14) (about 7 %) had yet to decide, and 28 (about 14%) did not respond to the question.

- 5.5 Over 78% of the surveyed owner-occupiers wanted their alternative accommodation to be newer than the current abode. Subject to the authorization by Chief Executive in Council to implement the Scheme, URA will offer an owner-occupier of domestic property the market value, plus an ex-gratia allowance (namely home purchase allowance). It is believed that the affected owner-occupiers will be able to buy a newer flat of similar size in the same district. For those affected owner occupiers expressing their desire to move to a new flat, URA will offer 'Flat-for-Flat' (FFF) option for them to choose to buy a URA new development in-situ, or in the same district (as URA may select for the purpose provided necessary approval/authorization has been obtained at the time of FFF offer) as an additional option to cash compensation to owner-occupiers of domestic units.

Affected Tenants

- 5.6 294 tenant households (about 76%) expressed their preference for moving into public rental housing, of which 55% preferred Kowloon East, 10% preferred location in Kowloon West, 1% preferred HK Island and 28% did not respond to this question. The majority preference for Kowloon East is understandable. However, as stated in paragraphs 4.3 and 5.1 above, those residents who are subject to conditional stay and some of those who lived in rooftop structures may not be eligible for public rental housing. Rehousing may only be considered for very special circumstances and on compassionate grounds.
- 5.7 The URA has made special arrangements with the HKHA and HKHS to provide flats within their estates for rehousing eligible households. According to information from the HKHA and HKHS websites, there are 7 HKHA public rental housing estates/Tenant Purchase Schemes Estates and 3 HKHS subsidised rental housing estates in Kowloon City district. In addition, there are 22 HKHA in the neighbouring districts of Wong Tai Sin whilst 1 HKHA and 1 HKHS public rental housing estates in Yau Tsim Mong districts. The URA will liaise with HKHA and HKHS to reserve flats in available estates to cater for the potential demand and accord priority to vulnerable groups. Subject to the availability of rehousing flats, the URA will endeavour to arrange rehousing for the eligible tenants in the same or adjacent districts as far as practicable.

Affected Rooftop Residents

- 5.8 The 13 surveyed rooftop households should follow the eligibility criteria for public rental housing adopted by the HKHA for illegal rooftop structure occupiers and the specific criteria as stated in paragraph 5.1 above should they be eligible for rehousing under current prevailing policies.

6. EMPLOYMENT STATUS AND PLACE OF WORK

- 6.1 Around 47% of the population (680 residents out of a total of 1,462 residents) within the Scheme was employed. Unemployed persons accounted for about 6.4%. The employment status of the affected owners and tenants are analyzed in paragraphs 6.2 to 6.7 below.

Affected Owners

- 6.2 A total of 564 residents from the 199 owner-occupier households were recorded in the survey. About 43% of this type of residents was employed, whereas only about 4% were unemployed. **Figure 6.1** shows details of employment status of the family members of owner-occupier households.

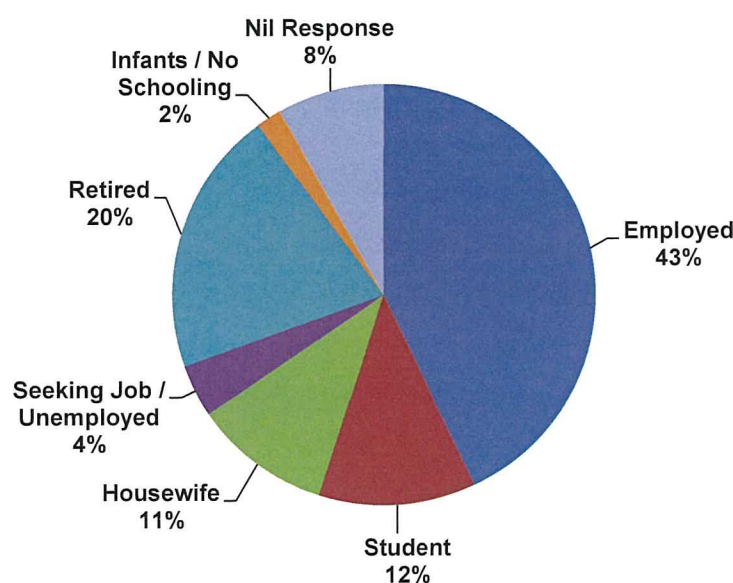


Figure 6.1 Employment Status of Affected Owners' Family Members

- 6.3 About 26% of the employed residents in the owner-occupier households were working in Kowloon City, and around 5% in Sham Shui Po. About 17% did not have a fixed working district. Other employed residents worked in different districts including HK Island, other parts of Kowloon, the New Territories and outside Hong Kong. Around 3% did not respond to this question. **Figure 6.2** shows the percentage share of different places of work of the employed persons of the owner-occupier households. As mentioned in paragraph 5.2 above, a large number of owner-occupiers (about 56%) indicated their preference of finding alternative accommodation in Kowloon City District; this generally coincides with the survey result of their work places in Kowloon City.

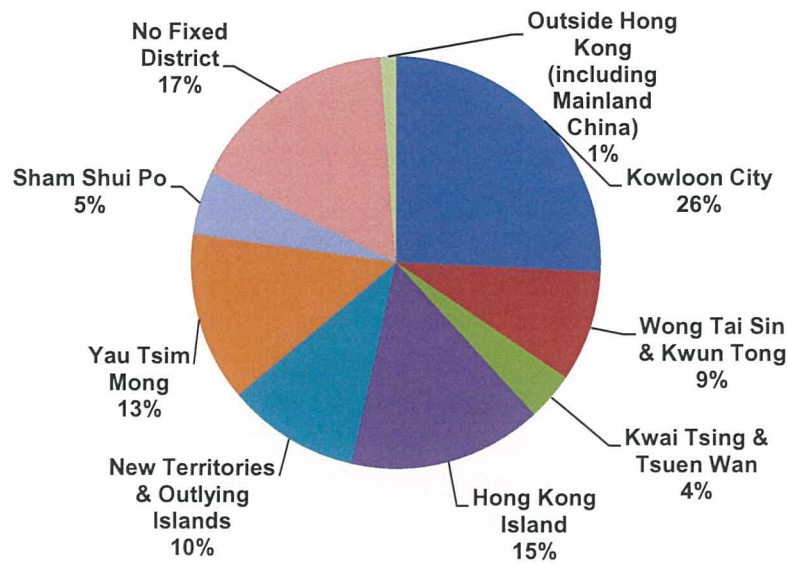


Figure 6.2 Place of Work of Affected Owners' Family Members

Affected Tenants

- 6.4 Among the 389 affected tenant households comprising 898 persons, about 49% was employment, whilst about 8% were unemployed (**Figure 6.3**).

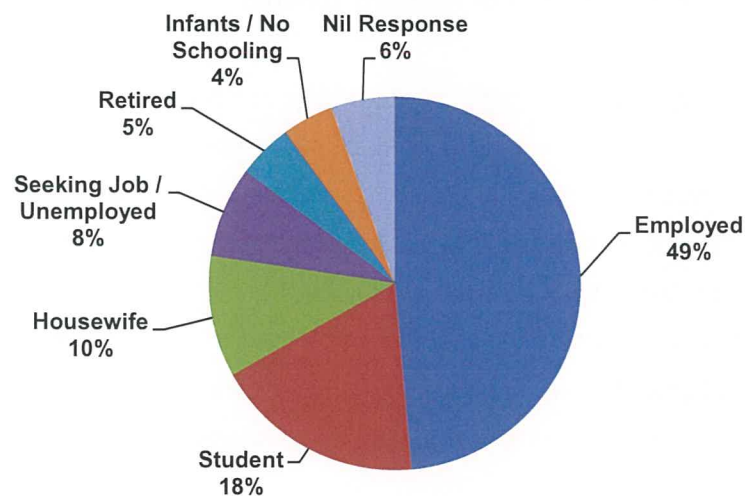


Figure 6.3 Employment Status of Affected Tenants

- 6.5 Among the 437 tenanted residents who were employees, about 30% were working in Kowloon City and 11% in Wong Tai Sin/Kwun Tong. In Kowloon West, about 13% were working in Yau Tsim Mong, 3% in Sham Shui Po and 2% in Kwai Tsing and Tsuen Wan. Around 5% are working in New Territories & outlying islands and 1% is outside Hong Kong (including mainland China). About 20% of residents had no fixed location of work, whilst 9 residents did not respond to this question. As

mentioned in paragraph 5.6 above, a large number of affected tenants, who preferred to move to public rental housings, hoped to be relocated in Kowloon East; this generally coincides with the survey result of their work places (i.e. about 41% working in Kowloon City, Wong Tai Sin and Kwun Tong districts). **Figure 6.4** shows the places of work of the tenant residents.

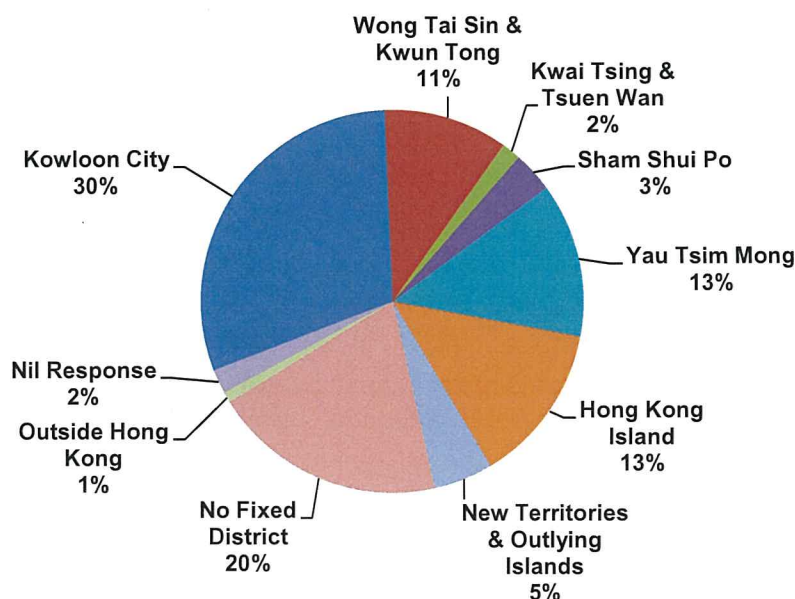


Figure 6.4 Place of Work of Affected Tenants

- 6.6 For those employee tenants who were working in Kowloon or Hong Kong Island, there would be economic concerns arising from higher transportation cost if they were to be relocated to the New Territories. The URA will endeavor to meet the locational preferences of residents for public rental housing from the HKHA and the HKHS subject to their eligibilities and the availability of flats at that time. Priority will be given to households of the most vulnerable groups (e.g. disabled persons). The SST will investigate the needy cases as identified and depending on justifications, may make recommendations for rehousing on compassionate grounds.
- 6.7 The findings of the survey show that the unemployment rate of the family members of the owner-occupier households (4%) and the tenant households (8%) which are much higher than the territory-wide figure [The territory-wide figure is 3.4% for a period of March 2016 – May 2016⁵]. There may be a financial difficulty for this group of unemployed residents in the Project. If the project is to be implemented, the eligible tenanted households will be subject to the URA's prevailing compensation policies.

⁵ Information from website of Census and Statistic Department as of 14 July, 2016.

7. ECONOMIC AND EMPLOYMENT IMPACTS

Affected Owners

- 7.1 **Figure 7.1** summarizes the impact of the redevelopment on employment condition as anticipated by the 158 interviewed domestic owner-occupier households who have answered SIA forms. The majority (107 households or 68%) considered that it would have no impact. 4 households about (2%) considered there would be a positive impact and 17 households (about 11%) considered there would be negative impact whilst 30 households (19%) chose to give no response.

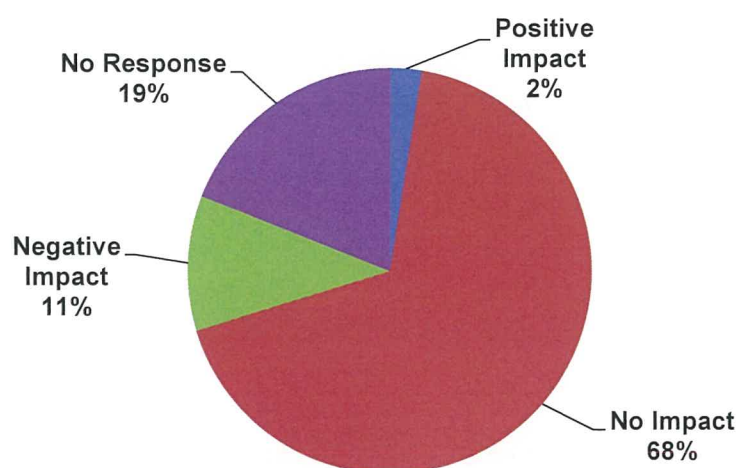


Figure 7.1 Impact on Employment to Affected Owners

- 7.2 The expected impact on family finances of the 158 interviewed owner-occupiers is summarized in **Figure 7.2**. 20 households (about 13%) considered that there would be positive impact on his financial condition, 59 households (about 37%) considered that there would not be any impact, and 40 households (about 25%) expected negative impact. The remaining 39 affected owner households (about 25%) gave no response to this question.

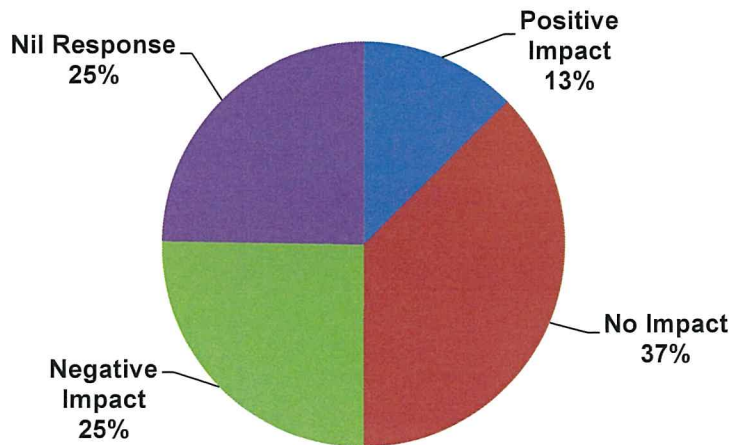


Figure 7.2 Impact on Economic Condition to Affected Owners

- 7.3 Eleven (11) domestic owner households expecting positive impact cited both more saving and better cash flow are the two major advantages. Among the 40 owner households who considered having negative impact, most of them cited more expenditure and less saving as their main concerns of bad impacts resulting from the proposed scheme.

Affected Tenants

- 7.4 The expected impact on employment condition as reflected by the 337 interviewed tenant households who have responded to this question is summarized in **Figure 7.3**. Only 11 households (about 3%) considered there would be positive impact to their employment whilst more than half of the tenant households (207 households, about 62%) considered there would not be any impact. Sixty-four (64) households (19%) expected negative impact. Another 55 households (about 16%) gave no response to this question.

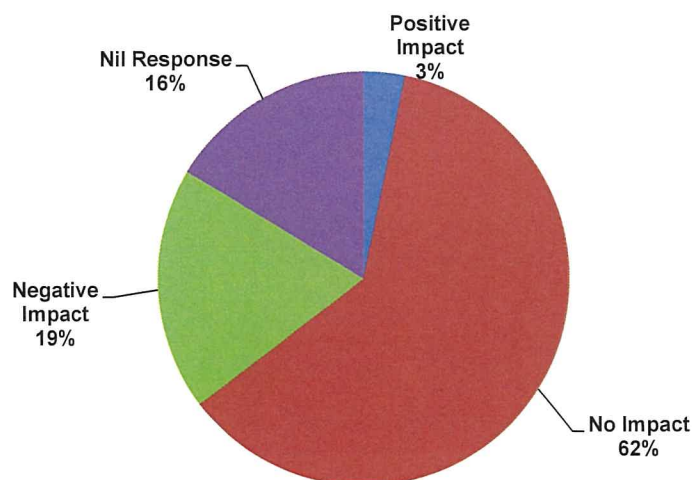


Figure 7.3 Impact on Employment to Affected Tenants

- 7.5 The expected impact on family finance as reflected by interviewed tenants is summarized in **Figure 7.4**. Thirty-eight (38) (about 11%) tenant households considered there would be positive impact to their finance condition due to the Scheme. One hundred and thirteen (113) households (about 34%) considered there would not be any impact, whilst 126 households (about 37%) expected negative financial impact and 60 households (about 18%) gave no response to this question.

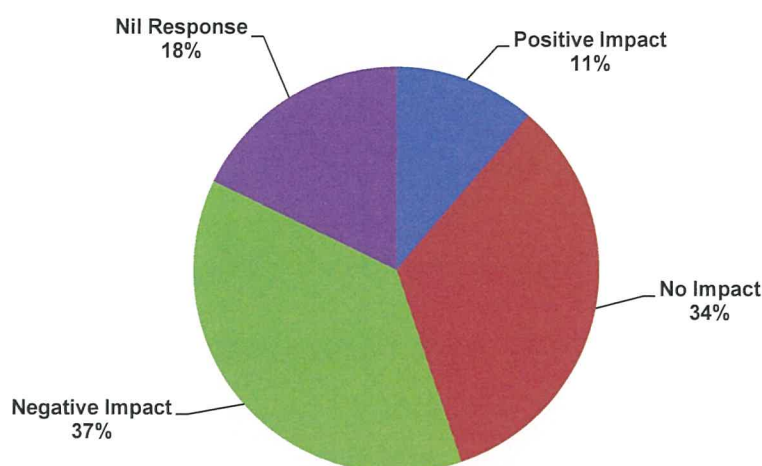


Figure 7.4 Impact on Economic Condition to Affected Tenants

- 7.6 For those 126 interviewed tenant households which considered the proposed redevelopment project would have negative financial impact, 94 households provide further information. Eighty (80) anticipated that their living expenditure would increase due to the Scheme. Eight (8) tenant households anticipated that they would have less saving and 3 tenant households anticipated that they would have less cash flow, and 3 households considered that the Scheme may lead to greater debt burden (each household can express more than one concern).
- 7.7 After the Freezing Survey, URA organised public briefing sessions to the affected owners and tenants to explain the prevailing policies on compensation and rehousing to alleviate their concerns. If the Scheme is to be implemented, the eligible tenant households will be subject to the URA's prevailing compensation policies.

8. SOCIAL NETWORK

Affected Owners

- 8.1 When asked about the likely impact of the proposed redevelopment on their social network, 49 (about 31%) of the 158 interviewed owner-occupier households answered that their current social network would be affected, whilst the majority, 97 households or about 61%, considered that their network would not be affected; 12 households (8%) gave no response to this question. The distribution pattern is shown in **Figure 8.1**.

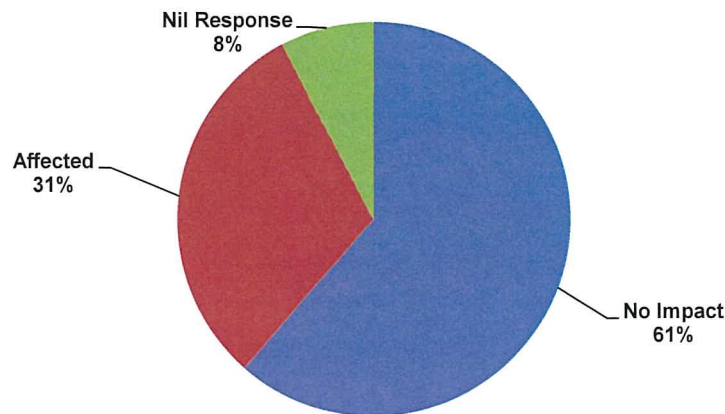


Figure 8.1 Redevelopment Effect on Social Network to Affected Owners

- 8.2 Of those respondents who were concerned about the possible adverse effects of the Project on their social network (about 31% of the owner-occupiers), the perceived impact on network related to medical support (57 households or 24%), neighbours and friends (52 households or 21%) and children/ relatives (38 households or 16%) were cited as their three major concerns (each respondent can provide more than one concern). The distribution pattern is shown in **Figure 8.2**.

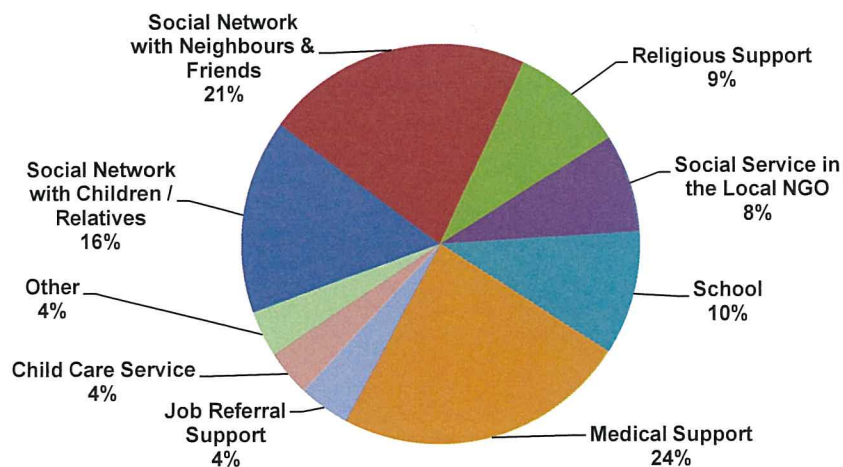


Figure 8.2 Nature of effect on Social Network to Affected Owners

Affected Tenants

- 8.3 When asked about the likely impact of the proposed redevelopment on their social network, 93 or about 27% of the 337 interviewed tenant households answered that their current social network would be affected. The two-hundred and eleven (211 or about 63%) responded that their network would not be affected. Thirty-three (33) households or 10% gave no response to this question (**Figure 8.3**).

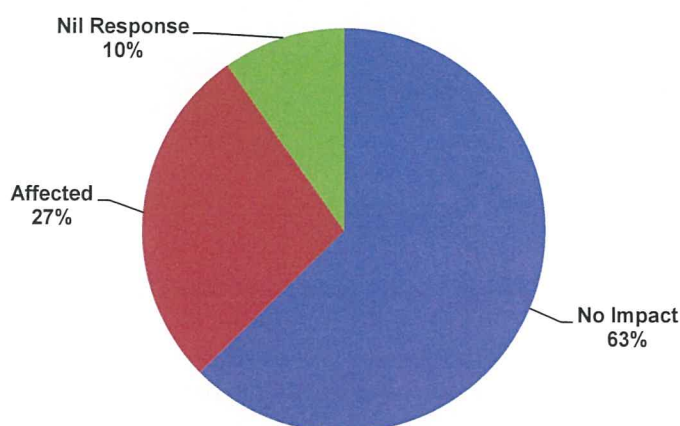


Figure 8.3 Redevelopment Effect on Social Network to Affected Tenants

- 8.4 Of those tenant households who were concerned about the possible adverse effects of the Scheme on their social network (about 27% of the tenant households), the perceived impact on network related to with neighbours and friends (25%), with children / relatives (19%), school (about 17%), and medical support (about 11%) as their major concerns (respondent could provide more than one concern). The distribution pattern is shown in **Figure 8.4**.
- 8.5 More than half of both interviewed owner-occupiers and tenants (61% and 63% respectively) who responded that they did not expect impact on their social network would arise is not surprising given that around 53% of total households (see paragraph 4.4) have only lived in the Scheme for 5 years or less.

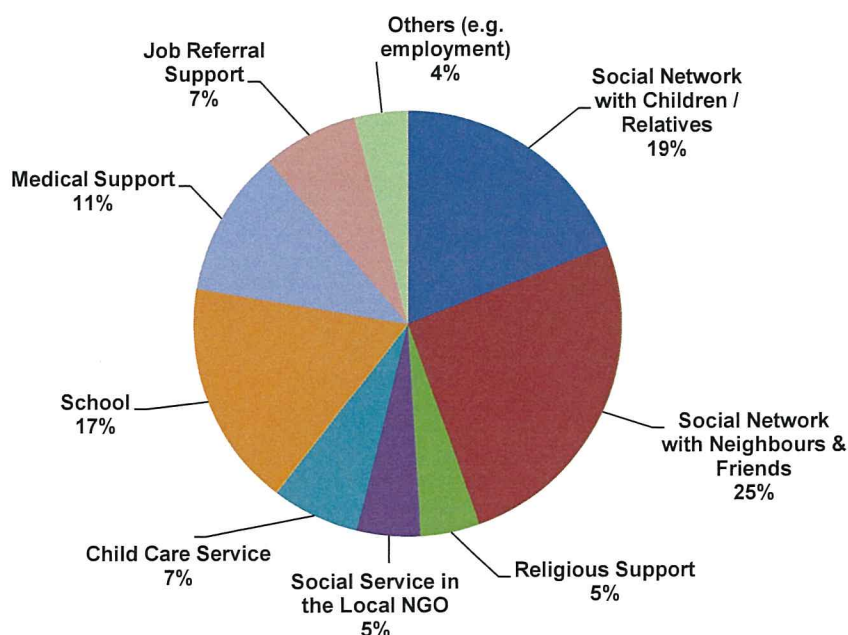


Figure 8.4 Nature of effect on Social Network to Affected Tenants

- 8.6 The SST will provide orientation sessions before and after rehousing to help the affected residents adapt to their new homes and introduce various community resources available in the area, including medical support, NGO services and community facilities. It will help them to identify suitable medical/ social service providers and religious institutions in the new residence setting. However, the social support from children/ relatives may take longer to establish/ re-establish in a new environment. If such residents prefer to live close to their relatives to retain social support, the URA will endeavour to arrange rehousing, subject to the availability of public rental flats, and their eligibility for rehousing based on their locational preference as far as practicable. The URA will also offer in-situ FFF option for those eligible domestic owner-occupiers to choose to buy the newly redeveloped flats in the Scheme so that they can move back to the same area and retain the social networks upon completion of the redevelopment.

9. EDUCATION NEEDS OF CHILDREN

9.1 The survey identified 232 students residing within the Scheme. Among these students, 118 (51%) were primary or kindergarten students, 76 (33%) were secondary students and 33 students (about 14%) were tertiary or above levels. The remaining 5 did not indicate their education level. There were 158 students (about 68%) studying in schools in Kowloon City. Five (5) students (2%) were studying in Hong Kong Island, thirty-five (35) students (15%) elsewhere in Kowloon and thirteen (13) students (6%) in the New Territories. Ten (10) students (4%) studied in mainland China/ Macau or overseas. Two (2) students had no fixed location for study whilst 9 students did not respond to this question. **Figure 9.1** shows the type of schools attended by the students residing in the Scheme.

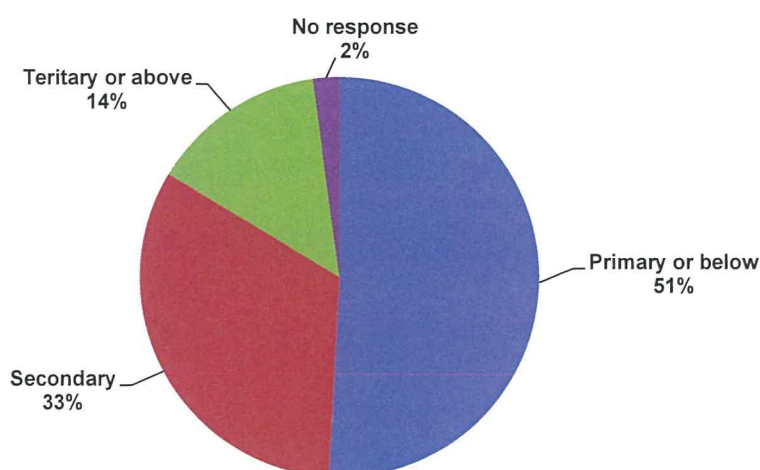


Figure 9.1 Educational Level of Student

9.2 Given the preponderance of students studying locally, it is not surprising that 97 students (about 42%) did not need to pay for transport to school. 71 students (about 31%) spent HK\$10 or less per trip travelling to school and 39 student (about 17%) incurred relatively higher travelling costs of over HK\$10 per trip. One (1) student had no fixed traveling costs whilst 24 students did not respond to this question. **Figure 9.2** shows the transport costs of the students in the interviewed households.

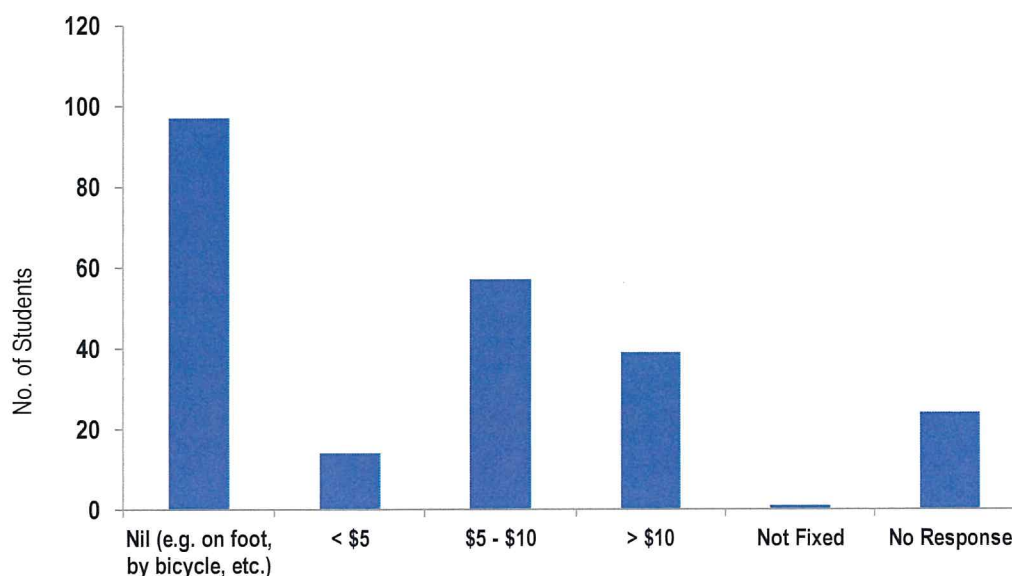


Figure 9.2 Travelling Cost to School – Single Trip

9.3 **Figure 9.3** shows the students' travelling time to school. 95 students (about 41%) spent 20 minutes or less travelling to their schools. 96 students (about 41%) students spent 20 to 60 minutes. 15 students (about 6%) spent over 60 minutes travelling to school. 3 students (about 1%) had no fixed traveling time whilst 23(about 10%) students did not respond to this question.

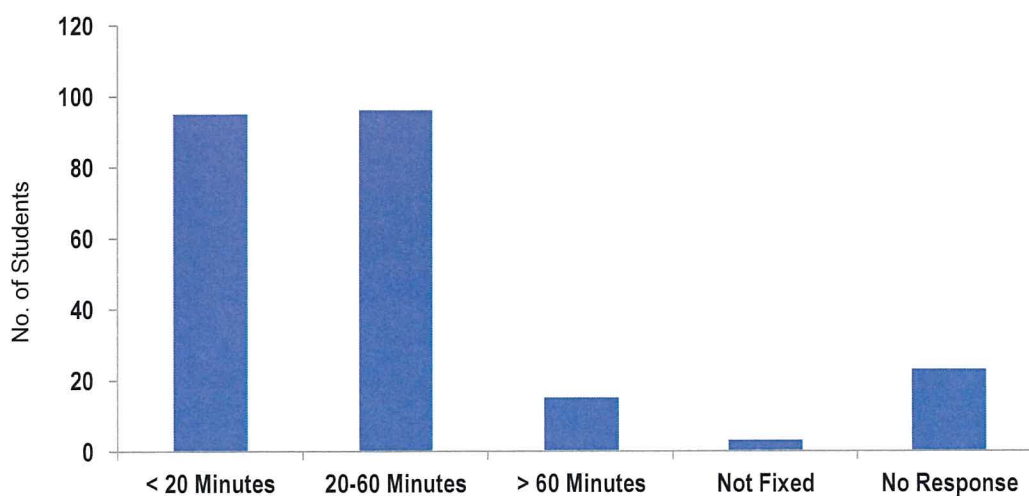


Figure 9.3 Travelling Time to School – Single Trip

9.4 The 232 students came from 167 households of which 164 students (about 71%) were from tenant households. 88 of these 164 students in tenant households were studying in primary school or kindergarten. Impact of the Scheme on this group of students may be greater as these students may need to change to another school if their families chose to move to public rental housing estates in other areas. It is understandable that parents generally wish their children to continue in their present

schools. Relocation away from this area may cause inconvenience especially for primary and kindergarten students. The URA with the assistance of the SST, will assist the affected families during the acquisition and rehousing stages to meet the educational needs of their children as much as possible. If necessary, appropriate assistance, resources and services from relevant Government departments will be sought.

10. GROUPS WITH SPECIAL NEEDS

- 10.1 An assessment has been made on the special needs of the elderly, persons with disability, single-parent families and ethnic minority identified in the survey.

Elderly Persons (65 years and above)

- 10.2 A total of 176 elderly residents in 124 households in the Scheme were recorded during the survey. Of these, 45 elderly were singleton households whilst 26 elderly residents comprised 13 doubleton households. The remaining 105 elderly residents were from 66 households. Among the 124 households with elderly residents, 43 households were tenant household and the rest were living in self-owned units.
- 10.3 The presence of elderly population within the Scheme has implications on types of re-housing and other age-related concerns such as accessibility to medical facilities. It is generally understood that elderly persons, particularly singletons, may have more difficulty adapting to their new environment once rehoused. The URA and the SST will make effort to alleviate their anxiety by providing information on the arrangement of rehousing and the new environment surrounding the estate.
- 10.4 **Figure 10.1** shows the aspirations of the elderly when questioned about the improvements they would like to see in their new home.

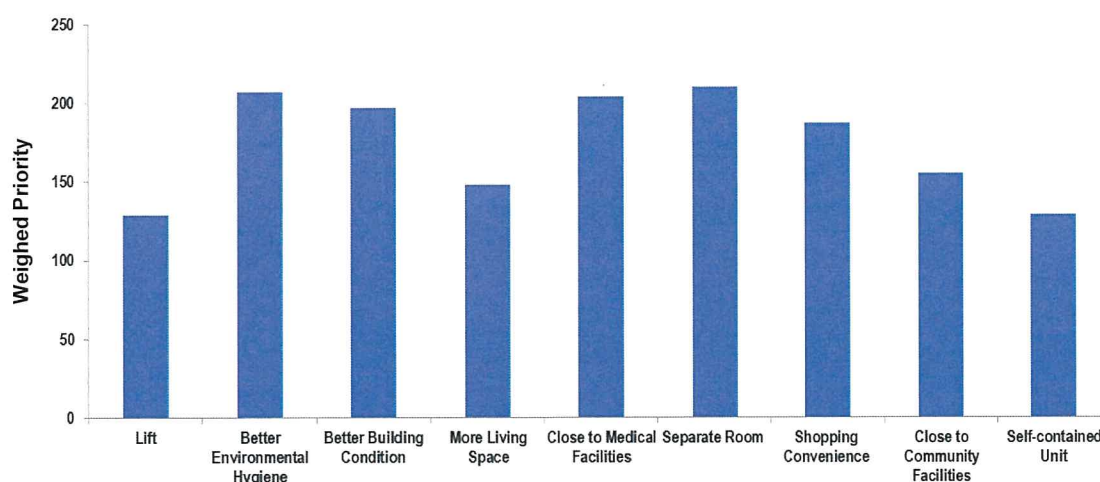


Figure 10.1 Elderly Concerns on Living Environment (based on the first three priorities chosen)

Persons with Disability

- 10.5 26 residents with disabilities (about 1.8%) were recorded in the survey. Of these, 3 residents require wheelchair access, 3 residents has mentally handicapped, 1 residents has visual impairment, 1 residents has hearing impairment, 7 residents has mental health problem. Whilst 1 residents indicated having 3 type of disabilities, 10 residents indicated suffering from other type of disability such as stroke, dementia and autism etc. 15 residents with disabilities considered medical support as their primary concern while 6 residents considered disability access/facility are important. Rehousing for disabilities may be considered on compassionate grounds if they are not eligible under the normal eligibilities.

Single-parent Families

- 10.6 29 single-parent families with 38 children were identified. These families were particularly concerned with better living environment, child care service provision, more living space and the schooling needs for their children. The concerns are showed in **Figure 10.2**.

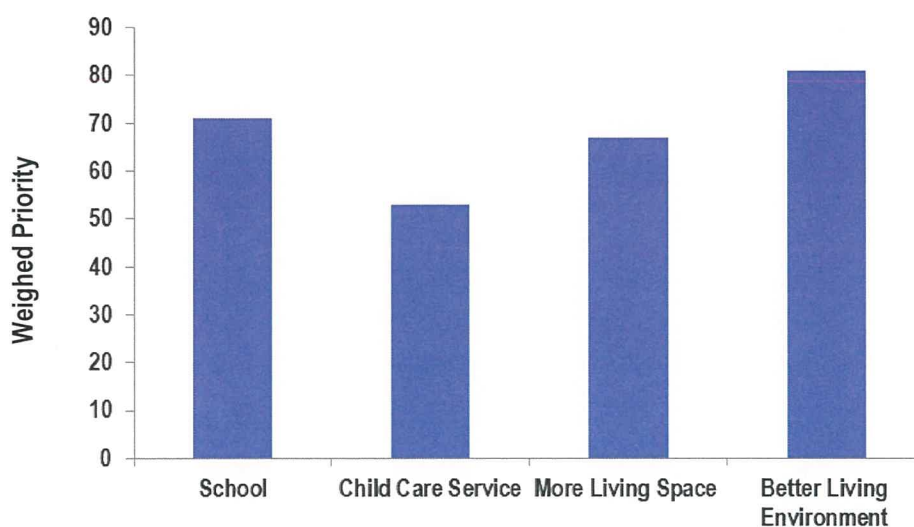


Figure 10.2 Major Needs of Single-parent Families (based on the first three priorities chosen)

Ethnic Minority Group

- 10.7 Among the 576 households, 89 residents from 32 households claimed to be non-ethnic Chinese. 9 households are from Pakistan, 6 households are from India, 3 households are from Thailand, 2 households are from Indonesia, 1 household is from Vietnam, 1 household is from Philippines, 1 household is from Sri Lanka, 1 is

from Bangladesh and 1 households are from Nepal. 2 households have family members from both Pakistan and Indonesia. 8 household did not respond to this question. Among these 89 non-ethnic Chinese residents, 27 residents are under conditional stay.

- 10.8 Among 32 households who claimed to be non-ethnic Chinese, 31 households were tenant households. The 24 households who answered the SIA questionnaire expressed strongly support or support to the proposed Scheme and 3 households have no comment. 5 households refused to do SIA questionnaire. Majority of households (63%) 15 households who responded considered there would be no impact of the Scheme on their social network. However, 8 households who responded considered that there will be impact on their impact particularly on social network with children/relatives, neighbours and friends and school. 9 households had expressed willingness to meet the SST. The SST will provide assistance to the families to mitigate the adverse impacts. The URA will endeavor to arrange rehousing to tenant households, subject to the availability of public rental flats and their eligibilities.

11. BUSINESS IMPACT

- 11.1 According to the approved GBPs and OPs of all buildings in the Scheme, there were a total of 47 ground floor premises, in which, 33 ground floor premises were for “non-domestic uses”. The remaining 14 premises were for “domestic use” in the approved GBP and OP and had been converted for non-domestic use. Furthermore, 7 approved GBP ground floor units were found to be sub-divided into 17 premises. As a result, the Freezing Survey identified a total of 75 premises as “non-domestic uses”, which comprise of 57 ground floor premises, 16 staircase structures and 2 cockloft premises. Among these 75 premises, 6 of them were found to be mixed uses.
- 11.2 Among the 75 non-domestic premises, 70 premises were successfully surveyed, 1 was confirmed vacant and there were no contact for the remaining 4. A total of 71 business operators were identified in the 70 occupied premises as 1 premises was found to be shared by 2 operators.
- 11.3 Among the 71 business operators, 68 operators answered both FS and SIA forms whilst 3 chose to answer the FS questions only. Therefore, this section’s analysis in regard to some specific questions, like the size of premises, reason for operating in current premises, length and performance of business, etc. are based upon the answers of the 68 business operators who completed both FS and SIA forms. **Table 11.1** shows the number of non-domestic premises and business operators identified in the Scheme.

	Non-domestic premises	Business operators
Successfully responded to both FS (successfully surveyed) and SIA (successfully interviewed)	67	68
Responded to FS forms only (successfully surveyed), but refused to do SIA	3	3
Confirmed vacant unit	1	N/A
Did not completed both FS nor SIA (No contact)	4	N/A
Total	75	71

Table 11.1 **Number of non-domestic premises and business operators identified in the Project**

- 11.4 Of those 71 operators who answered FS questions regarding their occupancy status, 12 were owner operators, 47 were tenant operators, 1 was licensee and 11 were occupiers, as shown in **Figure 11.1**.

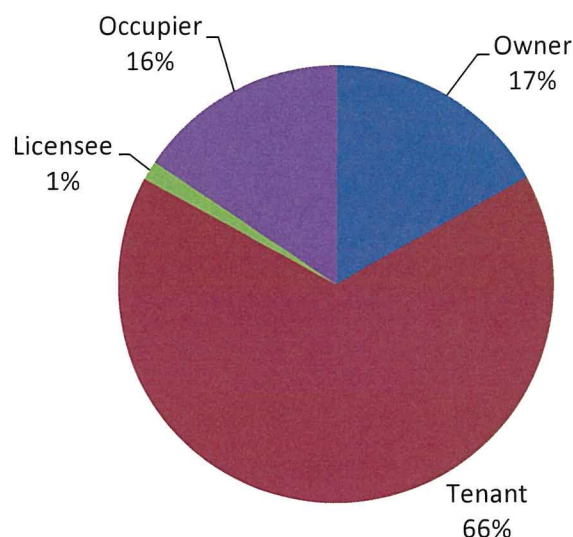


Figure 11.1 Occupancy Status of Business Operators

- 11.5 The nature of existing businesses of the 68 interviewed operators who responded to both FS and SIA is shown in **Figure 11.2**. Nine (9) premises (about 13%) were used for retail sales activities. Three (3) premises (about 4%) were used as offices. Thirteen (13) premises (about 19%) were used to provide services. Three (3) premises (about 4%) were used as eateries. Twenty-five (25) premises (about 37%) were used as workshops including recycling, metal hardware processing and vehicle repair. One (1) premises (about 2%) was used as a workshop and to provide service, another (1) premises (about 2%) was used for retail and also engineering. The rest of the shops (13, about 19%) were related to engineering, storage, showroom, etc. These business activities are typical of shops and services found in many parts of Kowloon City District.
- 11.6 For the workshop operators (including the recycling business, metal hardware processing and the car repair workshops), they will need to find alternative shop premises which can satisfy their various operational requirements and where the respective uses are permissible in both the lease and planning terms, and in compliance with the Deed of Mutual Covenant (DMC) of the buildings. If those operators cannot find suitable premises meeting above criteria for relocation, there is a possibility that they might have to close down their businesses but they will be compensated with relevant ex-gratia allowance (if eligible) according to the Authority's prevailing policy.

- 11.7 Among those 68 interviewed operators who answered both FS and SIA, 4 indicated that it was a chain store or had a branch elsewhere.

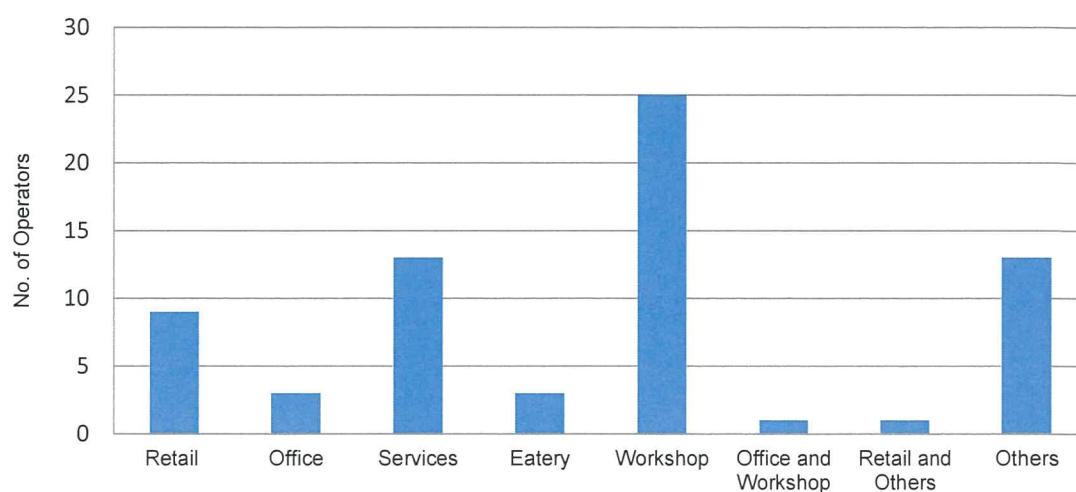


Figure 11.2 Classification of Non-domestic Uses

- 11.8 As for the size of the non-domestic premises, according to those 68 interviewed operators who responded to both FS and SIA questions, only 2 premises (about 3%) were less than 50 sq ft. Most of the premises (30 or about 44%) were between 500 to 1,000 sq ft. None of the premises are over 2,000 sq ft. The size distribution of the non-domestic premises are shown in **Figure 11.3**. [NB: The exact size of the premises can only be confirmed subject to detailed survey after the approval of the Scheme by Chief Executive in Council].

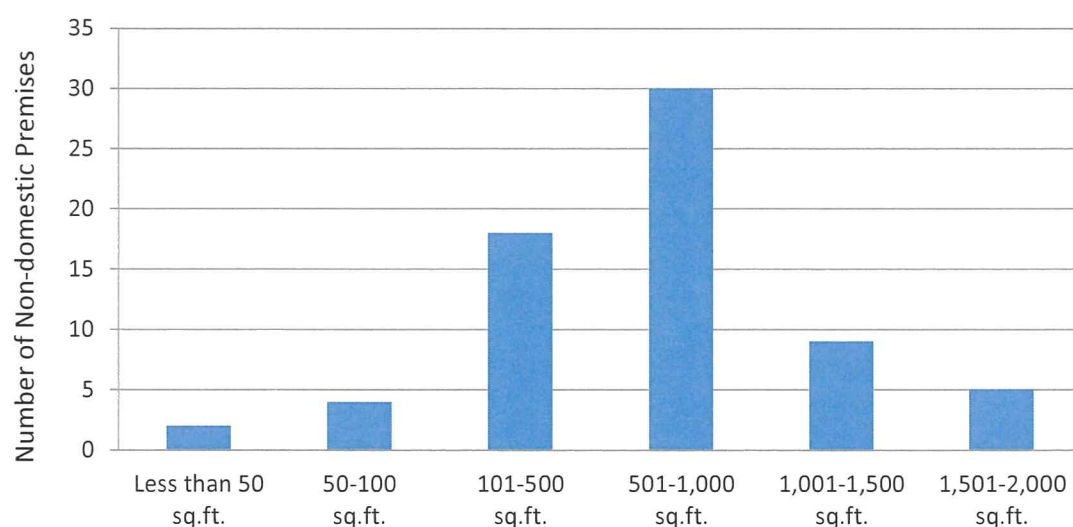


Figure 11.3 Size of Non-domestic Premises as claimed by operators

- 11.9 Among the 68 interviewed business operators, 32 operators (about 47%) either strongly supported or supported the proposed redevelopment. 25 operators (about 37%) had no comment and 11 operators (about 16%) either did not support or

strongly against the proposed development. Those who supported the Scheme mainly considered that the building condition was poor and that the buildings should be demolished for redevelopment, which could improve the local environment. Those not supporting the Scheme mainly responded that the Scheme would affect their business operations, 2 operators worried about the inadequacy of compensation, 1 operator considered the Scheme would destroy the social network and 1 operator worried about the difficulty in relocating to other ground floor shops at affordable rent. **Figure 11.4** shows the views of the business operators to the proposed redevelopment.

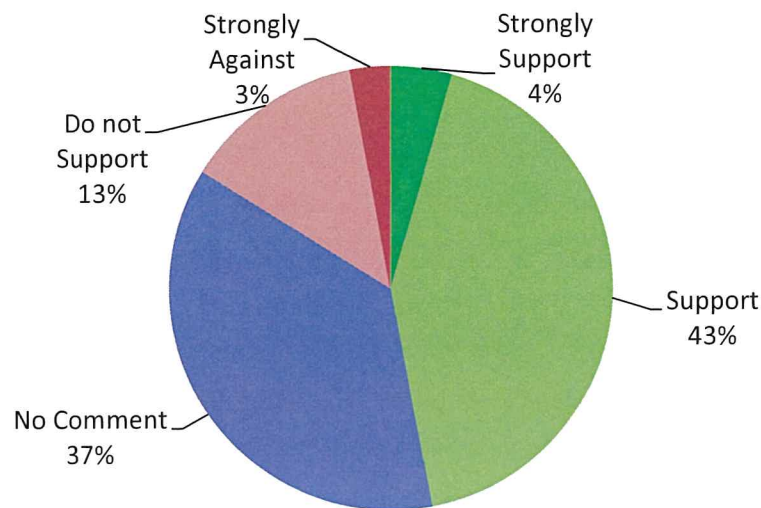


Figure 11.4 View of Business Operators to the Proposed Redevelopment

11.10 Should the Scheme proceed and be implemented, 36 operators would consider relocating their businesses, 10 operators would consider closing down, while 22 operators had not decided yet.

11.11 37 operators (about 54%) out of the 68 interviewed business operators mentioned that they had operated their business in Kowloon City district before moving to the current premises. 21 operators (about 31%) out of the 68 interviewed business operators mentioned that they had operated their businesses in other districts before moving to the current premises. It is anticipated that these operators may find it easier to relocate to other premises given their experiences operating in other districts. The remaining 47 operators (about 69%) indicated that they had only operated in Kowloon City District.

11.12 In terms of year of business operation of their existing businesses, among the 68 interviewed operators, 24 operators (about 35%) had been operating in the current premises for less than 5 years, 10 operators (about 15%) between 5 and 10 years, 18 operators (about 27%) between 11 and 20 years, 9 operators (about 13%) between 21 and 30 years and 7 operators (about 10%) has been operating in the current premises for over 30 years (**Figure 11.5**).

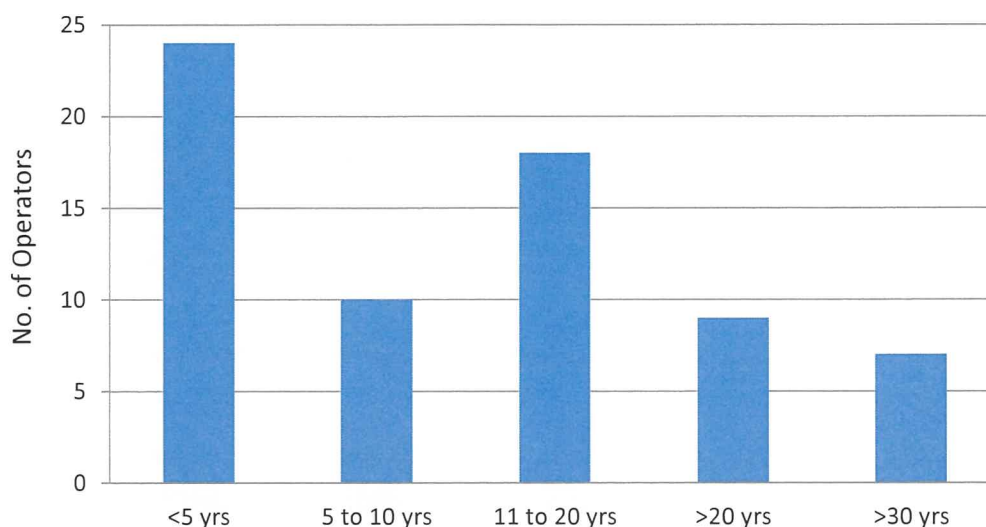


Figure 11.5 Year of Business Operation in the Existing Premises

11.13 **Figure 11.6** shows the reasons of 68 interviewed operators for operating their businesses at the current premises (operators can choose more than one reason). 25 operators chose low rent as one of their reasons for operating in the current premises. 14 operators chose wide customer base while 12 chose convenient parking and loading/unloading as their reasons. Convenient for suppliers and Long history of business in the area were selected 11 and 9 times respectively, while agglomeration effect and good neighbours had 8 selections each. 5 operators picked owning the existing property and 2 operators cited physical attributes of the premises as their reasons. 7 operators provided other reasons such as living or used to live nearby, spacious, regular customers, etc for operating the current premises.

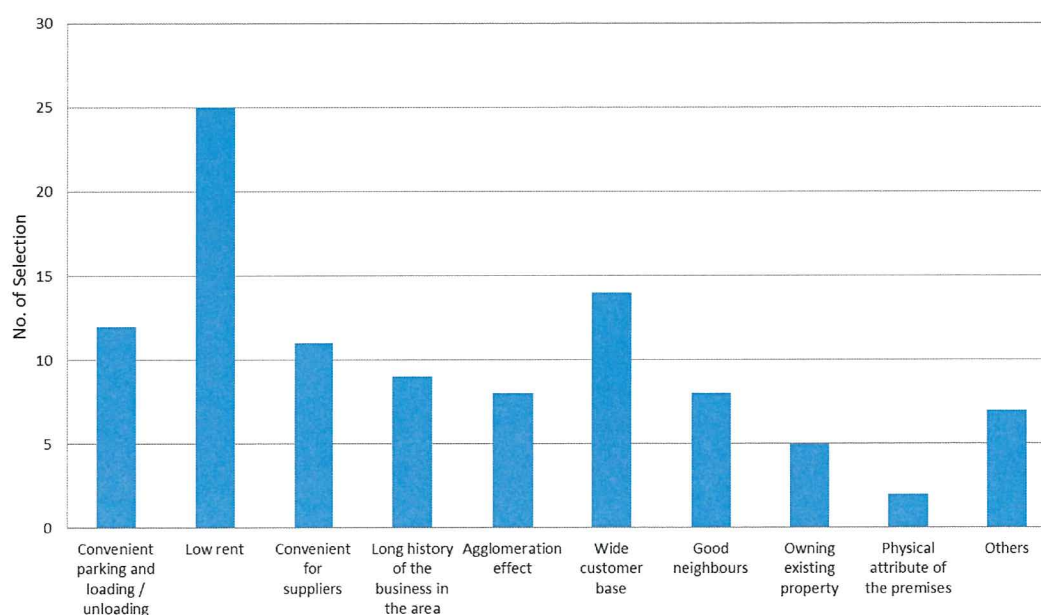


Figure 11.6 Reasons for Businesses Operating in the Current Premises

11.14 In terms of the satisfaction level of their business/ business performance of the 68 interviewed operators, 42 (about 61%) operators responded that they were either very satisfied or satisfied. 16 (about 24%) indicated it was neither satisfied nor dissatisfied on the performance, while only 8 (about 12%) indicated they were dissatisfied or very dissatisfied (**Figure 11.7**). 2 operators (about 3%) gave no response to the question. In summary, most of the operators were satisfied with their current business performance.

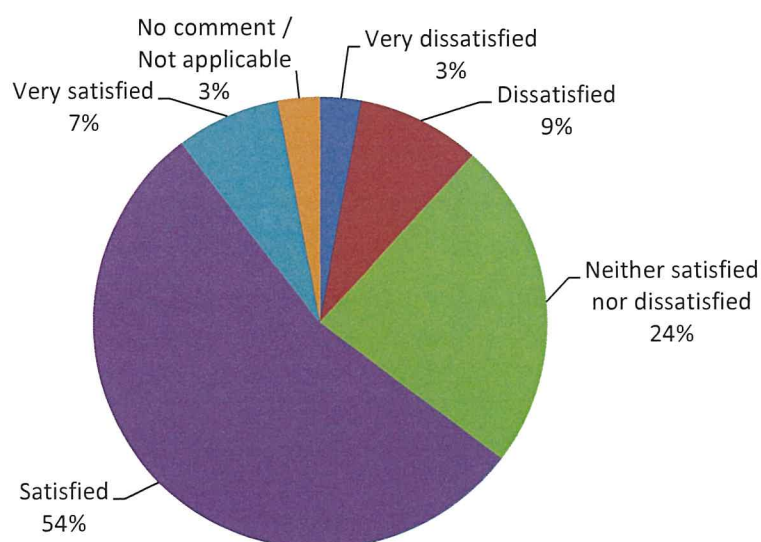


Figure 11.7 Satisfaction of Business Performance in Existing Premises

11.15 On the opinion of interpersonal relationship built from the existing business of the 68 operators, 49 (about 72%) operators were satisfied and 10 (about 15%) operators were very satisfied. It can be observed that most of the operators considered themselves as having good interpersonal network in the area (**Figure 11.8**).

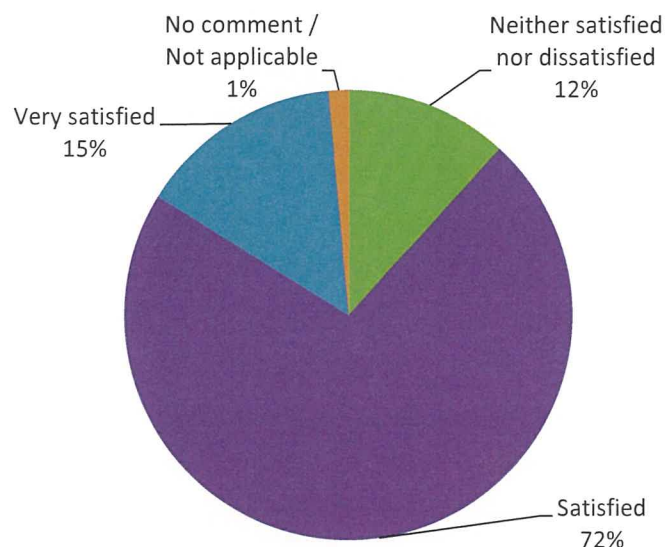


Figure 11.8 Satisfaction of Interpersonal Relation in Existing Premises

11.16 On the opinion of business network established from the existing shops, e.g. customer base, number of suppliers and etc. Among the 68 interviewed operators, 42 operators (62%) were satisfied while 7 operators (about 10%) were very satisfied. 14 operators (about 21%) responded they are neither satisfied nor dissatisfied (**Figure 11.9**).

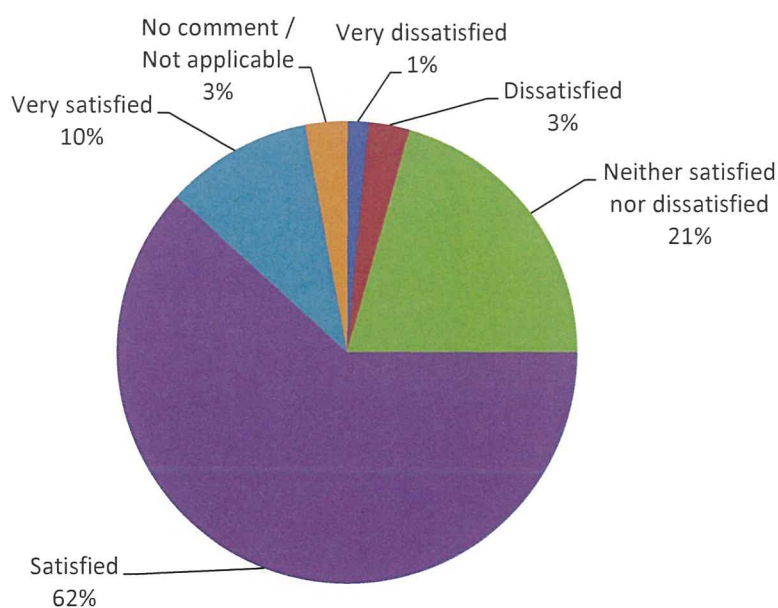


Figure 11.9 Business Network Built up by Operators in Existing Premises

11.17 On the future intention of the operators if the proposed Project is implemented, among the 68 interviewed operators, 31 (about 46%) operators responded that they would like to continue their businesses nearby. Only 5 operators (about 7%) would consider continue business anywhere suitable, preferably in Kowloon City District or Sham Shui Po district. 22 operators (about 32%) replied that they had not decided yet and 10 operators (about 15%) replied that they would close the business.

11.18 Among the 58 interviewed operators who indicated their wish to continue business or had not decided yet (i.e. excluding those 10 who replied that they would close the business), 48 operators provided their major concerns when looking for alternative premises for relocation (more than one concern can be selected). 38 operators responded that they were concerned with the possibility of high rental cost after relocation. 21 operators were concerned that relocation would result in loss of existing customer base and 10 operators were concerned about the loss of agglomeration advantage. Loss of good neighbours and lack of suitable premises with the required physical attributes each had 9 selections. 6 operators were concerned with the high acquisition cost of the new location and 3 operators were concerned that their unique business characteristics would made them difficult to find suitable accommodation elsewhere. 7 operators expressed other concerns regarding transportation, convenience, size of the new premises and the amount of compensation. **Figure 11.10** shows the major concerns on relocation.

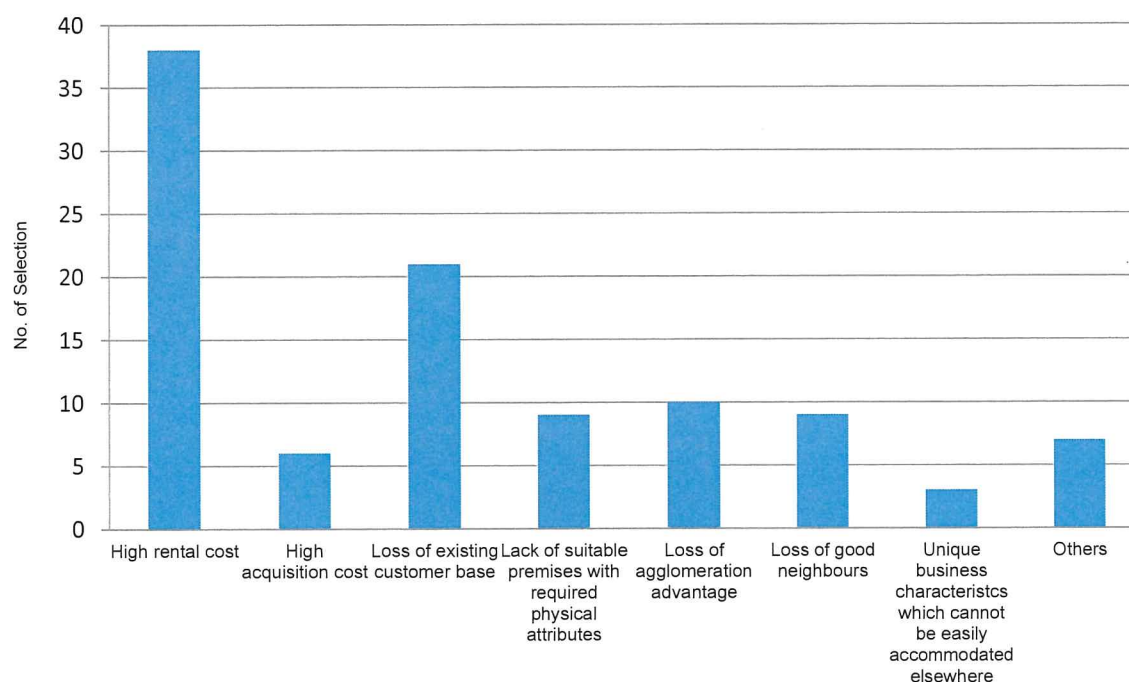


Figure 11.10 Major Concerns on Relocating to New Premises

- 11.19 Among the 58 interviewed operators (excluding those 10 who replied that they would close the business), 29 operators (50%) indicated they would like the URA to assist them in finding new premises to continue their businesses. 25 operators (about 43%) indicated that they did not need any support from the URA. 3 operators (about 5%) replied that they had not decided or depending on the situation, while the remaining 1 (about 2%) did not respond to the question.
- 11.20 Among the 68 interviewed operators, 19 operators (about 28%) expressed their willingness to meet the URA and the SST whilst 38 operators (about 56%) did not want any assistance. 11 operators (about 16%) provided no response to this question.
- 11.21 Fifty-nine (59) interviewed business operators expressed that their employees had no major concern on the Project. Eight (8) operators provided information on 10 employees who expressed their concerns of employment uncertainty arising from the Project and less convenient to go to work after relocation of the business. The remaining one (1) operator did not respond to this question. None of the employees expressed the need to meet the SST.

12. MITIGATION MEASURES REQUIRED

Social Service Team

- 12.1 In accordance with the new URS, the URF has been set up to, inter alia, fund the SST who provides assistance to residents and operators affected by URA-implemented redevelopment projects. The SST reports directly to the Board of the URF. The SST is expected to play a co-ordinating role in assisting the residents to access the services they need from relevant Government departments and/ or other service providers. For instance, the help of the HKHA and the HKHS will be sought in the rehousing process, the Education Bureau in providing school places for children affected by home removal, the Social Welfare Department and various social organisations for counseling services, the Hospital Authority and Department of Health in medical assistance, etc. As at 18 July 2016, the SST has successfully contacted 242 households. A breakdown of the Cases (Contacts) by client groups is listed in **Table 12.1**.

<u>Client Group</u>	<u>No. of Households/ Shops</u>	<u>Percentage</u>
Domestic Tenants	117	48.4%
Domestic Owners	64	26.4%
Business Operators	18	7.4 %
Unknown	43	17.8%
Total	242	100%

Table 12.1 Breakdown of SST Cases (Contacts) by Client Groups

12.2 The nature of the problems identified is summarized as follows:

<u>Problem or Enquiry Nature</u>	<u>No. of Households/ Shops</u>	<u>Percentage</u>
<i>Domestic Tenants (117 households)</i>		
a) Unclear on compensation and rehousing policies	95	81%
b) Evicted by owners / outrageous rent rise	1	1%
c) Worry about eviction, termination of tenancy or outrageous rent rise in future	1	1%
d) Worry about new accommodation will not be in the same district	2	2%
e) Others	18	15%
<i>Domestic Owners (64 households)</i>		
a) Unclear on compensation policies	53	83%
b) Family members of the owners are requested to move out after receiving compensation.	2	3%
c) Received Order from Building Department	1	2%
d) Others	8	12%
<i>Business Operators (18 shops)</i>		
a) Unclear on compensation policies	9	50%
b) Worry about the compensation is not enough to resume their businesses in the same district	1	6%
c) Others	8	44
<i>Unknown (43 households)</i>		
a) Unclear on compensation and rehousing policy	12	28%
b) Worry about new accommodation will not be in the same district	1	2%
c) Others	30	70%

Table 12.2 Nature of Problems Identified Among the Cases (Contacts)

12.3 The SST is expected to adopt a proactive approach to identify individuals at risk early through home and shop visits and to deliver prompt assistance to the residents and operators in need. For residents with no imminent needs, such a proactive approach can also enable the SST to establish a rapport with the clients and facilitate cooperation or engagement in future.

Public Briefing

- 12.4 The URA has arranged 4 public briefing sessions between 7 and 8 June 2016 to inform all the stakeholders, including owners, tenants and business operators affected, the details of the Scheme, and to obtain public views on the Scheme. The total attendance of the public briefing was about 515 persons. Questions on Freezing Survey, planning, acquisition and compensation and rehousing issues were addressed at the meeting. Those present were also informed that owners would not get more compensation by evicting tenants identified in the Freezing Survey.

Enquiries and Hotline Services

- 12.5 The URA also answers enquiries and provides hotline services to residents within the redevelopment area. Some 93 enquiries had been received between 3 June 2016 and 12 July 2016. The subject matters of the enquiries are summarised in **Table 12.3**.

<u>Subject Matters of Enquiries</u>	<u>Percentage</u>
Project information, progress, timetable and procedures of urban renewal	10%
Acquisition Compensation and rehousing policies	50%
Household Survey (e.g. registration arrangement)	23%
Others (e.g. residents briefing session information)	17%
Total	100%

Table 12.3 Nature of Enquiries

- 12.6 Based on past experience with implementation of redevelopment projects, the URA is confident that the prevailing compensation and rehousing policies and arrangements, with the services offered by the SST and the URA will be sufficient to reasonably mitigate the impact on the majority of the residents / business operators arising from the proposed redevelopment. In summary, the principal mitigation measures being pursued include:-

- (i) outreach activities by the SST to ensure that all affected persons potentially in

need are identified on top of those who were already identified in the SIA survey;

- (ii) assistance in finding public rental rehousing for eligible persons in need;
- (iii) conducting initial assessment of the elderly with low incomes or disability and other vulnerable groups for eligibility for compassionate housing;
- (iv) providing orientation assistance for those in need after moving home such as familiarisation with new neighbourhood, accommodation and local facilities; and
- (v) providing assistance to identify suitable replacement premises for affected businesses.

12.7 As revealed by the survey, 80% of the interviewed households either supported or strongly supported the Project as some of them considered that they would have more savings and less expenditure through the URA's prevailing compensation and rehousing policies. In addition, 15% of the households had neutral or no comment on the Project. Only around 5% of the interviewed households did not support or was strongly against the Project (Paragraph 2.15 refers). Those households who did not support the Project considered that the redevelopment would have negative impacts on the social network and their family finance. With regard to the view of business operators, 47% of the interviewed operators indicated support or strongly support for the Scheme. The operators supporting the Scheme considered that the building condition was poor and the building should be redeveloped to bring better local environment. About 16% of operators indicated not support or strongly against to the proposed redevelopment. They considered that the Scheme would affect their business operations and destroy the social network. The remaining 37% of operators had no opinion (Paragraph 11.9 refers).

12.8 The URA will assist displaced elderly owner-occupiers to find replacement flats within urban Kowloon. Redevelopment will inevitably affect the existing social network of some residents in the Project. The SST will follow up their cases for 6 months after their relocation to a new accommodation. In helping "the affected residents in maintaining and rebuilding social support network", the team will also conduct below activities:-

- (i) the displaced residents have contacted at least once their old acquaintance in Kowloon City after resettlement, e.g. through organizing a re-union gathering for displaced residents;

- (ii) the displaced residents have established connections in their new neighbourhood, e.g. programs on getting to know the local facilities in new community and visitation to the social service providers in the new neighbourhood.

12.9 The URA together with the SST will ensure that the requisite services and practical assistance by relevant Government Departments and/or service providers are made available to the community in need, and that social and livelihood problems relating to the Project are resolved in a timely manner.

Prevailing Acquisition, Compensation and Rehousing Policy

12.10 At the public meetings held between 7 and 8 June 2016, compensation, rehousing or ex-gratia payment based on the URA's prevailing policy for the affected owners and tenants were fully explained.

Domestic Properties

12.11 The URA will offer an owner-occupier of domestic property the market value (valued on vacant possession basis) of his property plus an ex-gratia allowance, namely Home Purchase Allowance (HPA), for purchase of the property. The assessment of HPA is based on the value of a notional replacement flat of similar size, which is defined as a seven-year-old flat in a building of comparable quality, situated in a similar locality in terms of characteristics and accessibility, and located at the middle floor with average orientation. The HPA is the difference between the value of the notional replacement flat and the market value of the property being acquired. The URA will offer an owner of tenanted or vacant domestic property the market value (valued on vacant possession basis) of his property plus a Supplementary Allowance (SA), where applicable, up to 50% of the HPA above mentioned. In addition to HPA or SA, URA will offer an incidental cost allowance to owners of domestic properties to assist payment of removal expenses and expenditure relating to the purchase of a replacement property.

12.12 According to the new URS, and as far as relevant legislation allows, the URA will offer FFF arrangement to eligible owner-occupiers of domestic properties. Under such arrangement, new flats will be made available in the new development in-situ as an additional option to cash compensation to such owner-occupiers. As this is an additional option, the amount of cash compensation offered to an owner-occupier

will not be affected by his/her choice of joining the FFF Scheme.

- 12.13 Affected eligible domestic tenants will be re-housed in units provided by the HKHA or the HKHS. Tenants who are re-housed will be offered an ex-gratia removal allowance. The allowance is in line with the HKHA's rates. The amount receivable will be according to the size of the household and the rates prevailing at the time.
- 12.14 Tenants who are not allocated re-housing due to various reasons or who decline re-housing, may receive ex-gratia payments. The amount of ex-gratia payment will be dependent on, amongst other things, whether the tenancies commenced before the date of the Freezing Survey and continued, or commenced on or after the freezing survey. Details of the ex-gratia payments for domestic tenants can be obtained from www.ura.org.hk.
- 12.15 According to the new URS, the URA will offer an ex-gratia allowance to eligible elderly owners of tenanted domestic units on compassionate ground in exceptional circumstances such as elderly owners who rely on the rental income from their properties for a living.
- 12.16 In case where tenants were threatened not to have their tenancies renewed, URA will explain to the owners that they would not get more compensation by evicting the tenants. The URA has also introduced the "Domestic Tenants Compassionate Assistance Programme" to take care of those domestic tenants whose tenancies commenced before the Freezing Survey of this Project and moved out from the properties because they have been required to move out from their properties by their landlords upon expiry or termination of their tenancies and before URA purchases the properties. In general, eligible domestic tenants who meet the criteria under this programme will be offered, after acquisition or resumption of the properties concerned, special ex-gratia payment of 3 times the ratable value of their properties subject to a minimum of HK\$70,000 (for a 1 person household) and HK\$80,000 (for a 2 persons plus household). Under very special circumstances, the URA will provide special arrangements for eligible tenants in special hardship on compassionate grounds.

Non-domestic Properties

- 12.17 For owner-occupied non-domestic premises, the market value of the affected

property (valued on vacant possession basis) plus an ex-gratia allowance of 4 times the ratable value or 35% of the market value of the affected property, whichever is the higher, will be offered. Owner-occupiers of non-domestic premises may choose to claim for business loss as an alternative to both ex-gratia allowance mentioned above and Ex-gratia Business Allowance (EGBA) mentioned in Paragraph 12.18 below. For owners of tenanted or vacant non-domestic properties, the market value (valued on vacant possession basis) of the affected property plus an ex-gratia allowance of 1 time the ratable value or 10% of the market value of the affected property, whichever is the higher, will be offered.

- 12.18 For non-domestic tenants of non-domestic premises, an ex-gratia allowance of 3 times the ratable value of the affected premises will be offered. An additional payment of EGBA is also payable to tenants and owner-occupiers who commenced occupying the premises for business before the date of freezing survey. The amount is calculated at a rate of 0.1 times the ratable value for each year that the affected premises has been in operation up to a maximum of 30 years, and subject to a maximum amount of HK\$500,000 and a minimum amount of HK\$70,000. Non-domestic tenants may lodge a claim for business loss in lieu of the above two allowances.
- 12.19 According to the new URS, if requested, the URA will help identify suitable premises in the district of the redevelopment project to enable the affected shop operators to relocate and continue operation in the same district as far as practicable.
- 12.20 Details of the current acquisition and compensation policies are published on the URA's website and will be communicated to affected persons when acquisition of property interests for this Project commences. Prevailing policies relating to property acquisition, rehousing and ex-gratia allowances will be reviewed by the URA from time to time.

URBAN RENEWAL AUTHORITY

July 2016

Non-domestic G/F premises within the Scheme Area



Unit 2, G/F, 2-10 Ngan Hon Street



Unit B, G/F, 2-10 Ngan Hon Street



Unit C, G/F, 2-10 Ngan Hon Street



G/F, 10 Ngan Hon Street



Staircase shop, G/F, 2-10 Ngan Hon Street



G/F, 12 Ngan Hon Street



G/F, 14 Ngan Hon Street



G/F, 16 Ngan Hon Street



G/F, 18 Ngan Hon Street



G/F, 20 Ngan Hon Street



G/F, 22 Ngan Hon Street



G/F, 24 Ngan Hon Street



G/F, 26 Ngan Hon Street



G/F, 28/28A Ngan Hon Street



Staircase structure, G/F,
28, 28A, 30, 30A Ngan Hon
Street



G/F, 30-30A Ngan Hon Street



G/F, 32&32A Ngan Hon Street



G/F, 34-34A Ngan Hon Street



Staircase & light well structure,
G/F, 36 Ngan Hon Street



G/F, 36-36A Ngan Hon Street



G/F, 38-38A Ngan Hon Street



G/F, 40 Ngan Hon Street



G/F, 40A Ngan Hon Street



G/F, 42A Ngan Hon Street



Shop A, G/F, 42 Ngan Hon Street



Shop A2, G/F, 42 Ngan Hon Street



Staircase structure, G/F, 40-42 Ngan Hon Street



Unit A, G/F, 1 Hung Fook Street



Unit B, G/F, 1 Hung Fook Street



Rear Shop, G/F, 1 Hung Fook Street



Staircase structure, G/F, 1-3 Hung Fook Street



Unit A, G/F, 3 Hung Fook Street



Unit B, G/F, 3 Hung Fook Street



Rear Shop, G/F, 3 Hung Fook Street



G/F & C/L, 5 Hung Fook Street



Staircase structure, G/F, 5-7 Hung Fook Street



G/F & C/L, 7 Hung Fook Street



G/F & C/L, 9 Hung Fook Street



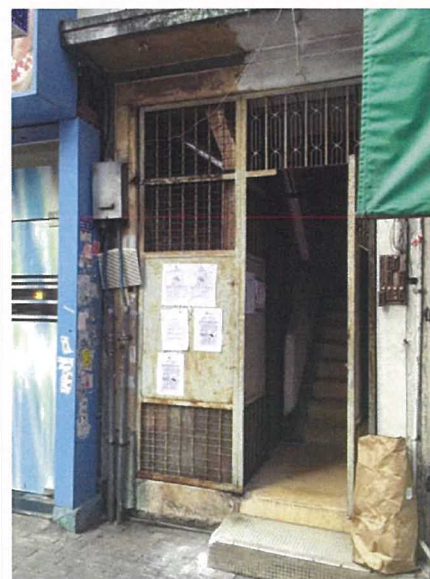
Staircase structure, G/F, 9-11 Hung Fook Street



G/F, 11 Hung Fook Street



G/F & C/L, 13 Hung Fook Street



Staircase structure, G/F, 13-15 Hung Fook Street



G/F, 15 Hung Fook Street



G/F, 17 Hung Fook Street



Staircase structure, G/F, 17-19 Hung Fook Street



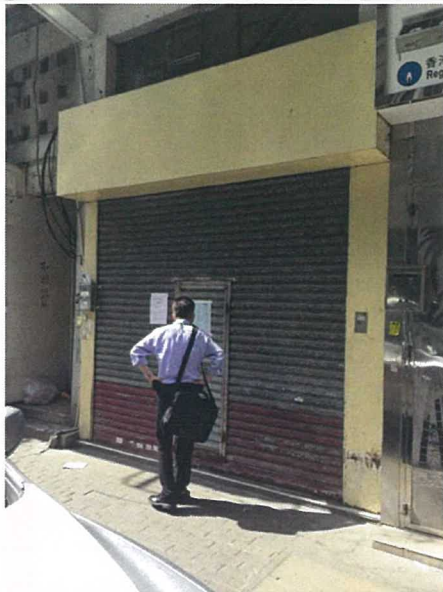
G/F, 19 Hung Fook Street



G/F & C/L, 21 Hung Fook Street



Staircase structure, G/F, 21-23 Hung Fook Street



G/F & C/L, 23 Hung Fook Street



G/F & C/L, 25 Hung Fook Street



Staircase structure, G/F, 25-27 Hung Fook Street



G/F, 27 Hung Fook Street



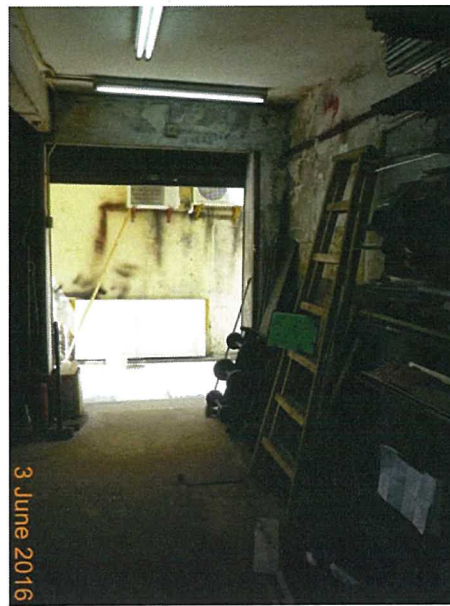
G/F & C/L, 29 Hung Fook Street



Staircase structure, G/F, 29-31 Hung Fook Street



G/F, 31 Hung Fook Street



Rear Shop, G/F, 31 Hung Fook Street



G/F, 33 Hung Fook Street



Staircase structure, G/F, 33 Hung Fook Street



G/F & C/L, 35 Hung Fook Street



G/F, 37 Hung Fook Street



Staircase structure, G/F, 37-39 Hung Fook Street



G/F, 39 Hung Fook Street



G/F, 41 Hung Fook Street



Staircase structure, G/F, 41-43 Hung Fook Street



Front shop, G/F & C/L, 43 Hung Fook Street



Rear shop, G/F, 43 Hung Fook Street



G/F, 45 Hung Fook Street



G/F & C/L, 47 Hung Fook Street



G/F, 49 Hung Fook Street



Staircase structure, G/F, 49-51 Hung Fook Street



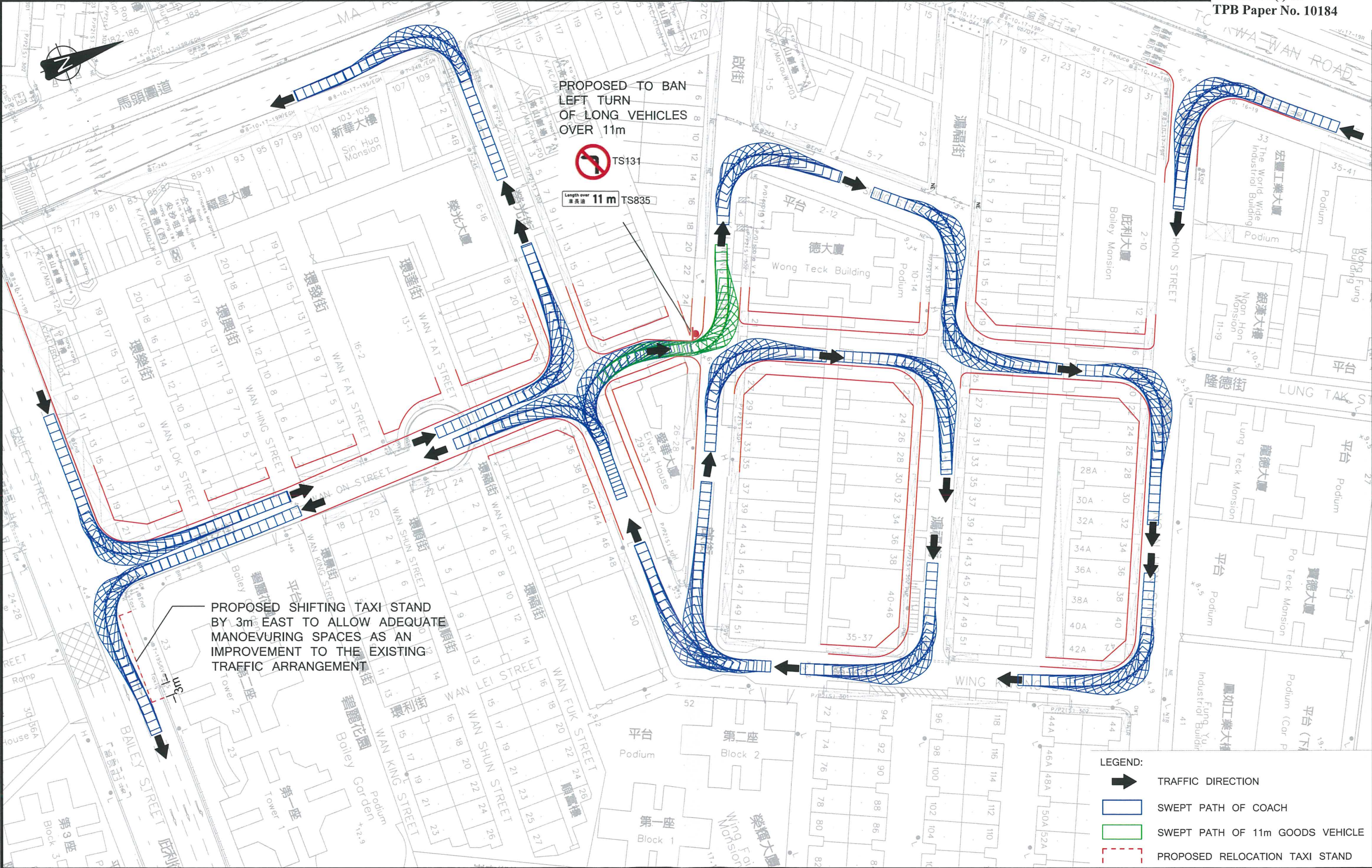
G/F, 51A Hung Fook Street



Shop B, 51 Hung Fook Street



Shop C, 51 Hung Fook Street



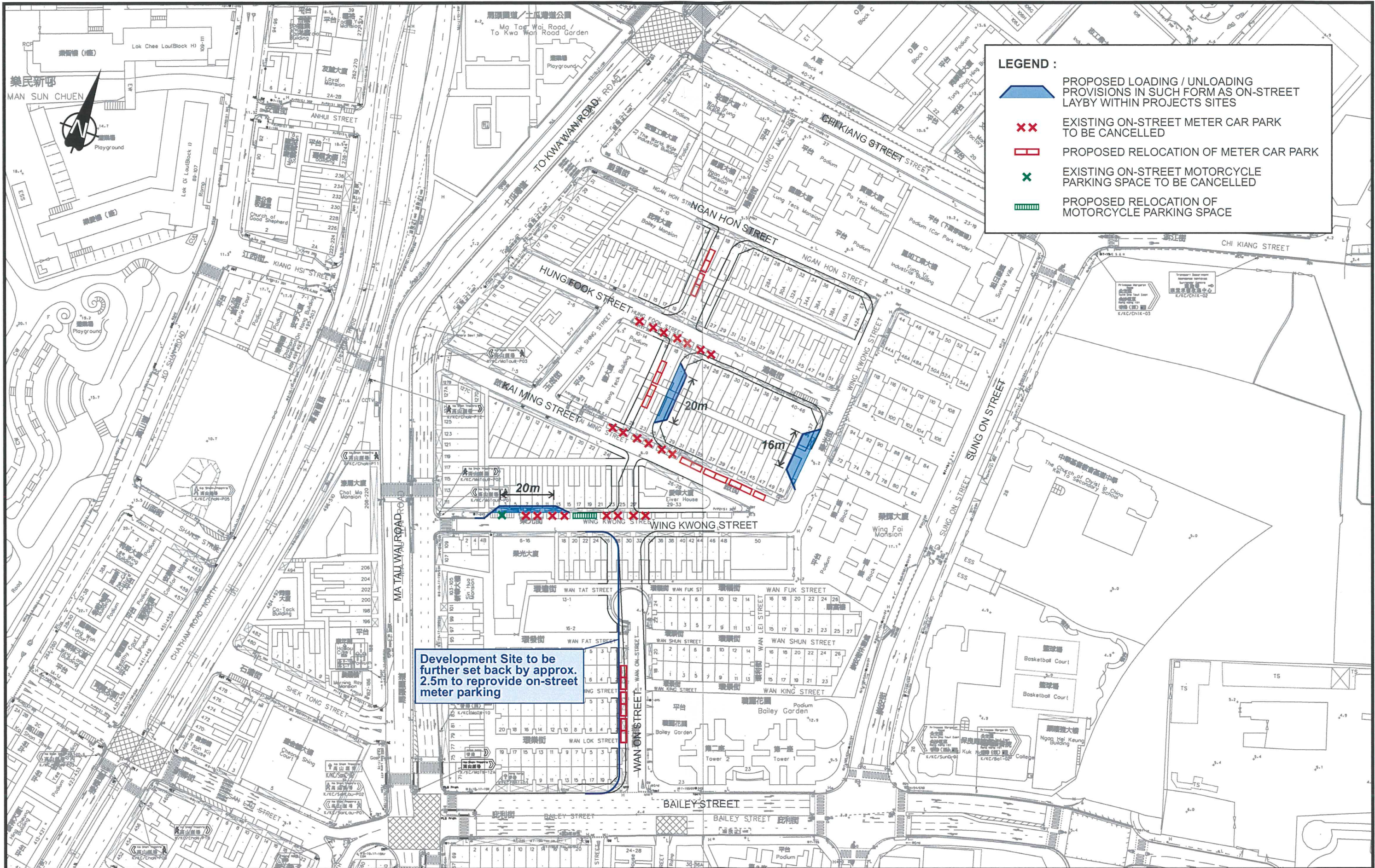
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B	REVISED BASED ON TD'S COMMENT	KSC	26JUL16
A	REVISED BASED ON TD'S COMMENT	KSC	24MAY16
Rev.	Description	Checked	Date

CHK50143110/AA1-A+B/F34-B.DWG/CKC/26JUL16

Project Title
TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1

Drawing Title											
SWEPT PATH ANALYSIS											
Designed	HCY	Checked	KSC	Scale	1:800(A3)	Date	MAY 2016	Drawing No.	3.4	Rev.	B





LEGEND :

- PROPOSED LOADING / UNLOADING PROVISIONS IN SUCH FORM AS ON-STREET LAYBY WITHIN PROJECTS SITES
- EXISTING ON-STREET METER CAR PARK TO BE CANCELLED
- PROPOSED RELOCATION OF METER CAR PARK
- EXISTING ON-STREET MOTORCYCLE PARKING SPACE TO BE CANCELLED
- PROPOSED RELOCATION OF MOTORCYCLE PARKING SPACE

Rev.	Description	Checked	Date
A	INCORPORATE WITH TD'S COMMENT	KSC	27JUL16

Project Title	TRAFFIC IMPACT ASSESSMENT FOR REDEVELOPMENT OF A+B SITE AT SECTION AREA 1
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Drawing Title	PROPOSED ON STREET PARKING AND LOADING / UNLOADING LAY-BY			
Designed	WHY	Checked	KSC	Scale
				NTS
Date	MAY 2016	Drawing No.	3.5	Rev.
				A





**Term Environmental Consultancy Services
For a Project at
Hung Fook Street / Ngan Hon Street, Kowloon City District
(KC-010)**

Drainage, Sewerage, and Water Impact Assessment

Prepared for:
Urban Renewal Authority

Prepared by:
Ramboll Environ Hong Kong Limited

Date:
August 2016


Reference Number:
R5142_V1.1

Prepared by:



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1. Introduction

1.1 Background and Objectives

- 1.1.1 The Urban Renewal Authority (URA) has proposed the Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street in Kowloon City.
- 1.1.2 Ramboll Environ Hong Kong Limited has been appointed by the Urban Renewal Authority (URA) to conduct this Drainage and Sewerage Impact Assessment for submission to the Town Planning Board for approval.

1.2 Subject Site and its Environs

- 1.2.1 The Subject Site is bounded by Hung Fook Street and Ngan Hon Street in Kowloon City. The existing buildings within the Subject Site were built between 1957 and 1960, and range from 6 storeys to 8 storeys high. The buildings are mainly residential in nature with commercial premises at the ground floor.
- 1.2.2 **Figure 1** shows the location of the Subject Site and its environs.

1.3 Proposed Redevelopment

- 1.3.1 The proposed redevelopment to be built in the Subject Site will be built on the area zoned Residential (Group A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. The adjacent public pavement area shown as "Road", will not be built upon and it will be excluded from calculation for plot ratio purposes.
- 1.3.2 In line with the "Residential (Group A)" zoning, the redevelopment intention includes both residential and commercial uses. The subject development covers a gross site area of approximately 4,951m², with an approximate domestic gross floor area of 34,215m² and non-domestic gross floor area of 6,843m².
- 1.3.3 Three 23-storey residential towers are proposed with a total of 759 units, and a common clubhouse will be shared among the residential towers.

2. Drainage Impact Assessment

2.1 Scope of Work

- 2.1.1 The aim of this study is to assess the changes to runoff from the Subject Site as result of the proposed redevelopment and the potential impacts on the existing drainage system and surrounding areas.

2.2 Existing Drainage

- 2.2.1 The overall catchment and drainage regimes in the area are shown on **Figure 2**. The Site is located within a fairly small, largely self-contained catchment, with runoff from the existing buildings collected by a storm drain running towards the east within an alleyway between the buildings along Ngan Hon Street to the north and the buildings along Hung Fook Street to the south. However, this drain also collects runoff from Nos. 17-31 To Kwa Wan Road, which are located to the west of the Site, as shown on **Figure 2**. The entire catchment is fully paved.
- 2.2.2 The local drain running through the Site area discharges to the main drainage system at manhole SMH4030871, where it discharges to a 900mm diameter drain running to the north along Wing Kwong Street to manhole SMH4067829 in Ngan Hon Street.
- 2.2.3 There are no existing flooding blackspots or known drainage problems in the vicinity of the Site.

2.3 Assessment Criteria

- 2.3.1 The assessment standard complies with the DSD SDM (2013 Edition). The Site discharges to an urban branch drainage system and a 1 in 50 year return storm has therefore been adopted for the DIA.
- 2.3.2 The catchment runoff has been calculated using the "Rational Method", as outlined in the DSD SDM:

$$Q = 0.278 C i A$$

Where	Q	=	peak runoff in m ³ /s
	C	=	runoff coefficient (dimensionless)
	i	=	rainfall intensity in mm/hr
	A	=	catchment area in km ²

- 2.3.3 The existing Site and upstream catchment are fully paved under both existing and future scenarios and an overall runoff coefficient of 1.0 has been adopted throughout.
- 2.3.4 The rainfall intensity parameter "i" is dependent on the return period, rainfall duration and the time of concentration of the catchment under consideration. As the catchments comprise small paved urban areas, assumed times of concentration have been adopted. Runoff calculations are included in **Appendix A**.

2.4 Drainage Impact Assessment

General

- 2.4.1** As both existing and future scenarios will involve full paving of the Site, the proposed development at the Site will not change the amount of runoff. Furthermore, it is intended that internal drainage systems will convey runoff from the proposed development to a Terminal Manhole discharging to the same main connection point, i.e. manhole SMH4030871.
- 2.4.2** Although the Site itself will be provided with new drainage facilities, the drain serving the small upstream catchment of Nos. 17-31 To Kwa Wan Road will be cut off and a new drainage route will be required for runoff from this area. Furthermore, it is noted that the existing drains in this area are only 150mm diameter, which is unacceptably small. It is therefore proposed to provide a new 300mm diameter drain to replace the existing drains in the alleyway at the rear of Nos. 17-31 To Kwa Wan Road, as shown on **Figure 2**. The new drain will run from manhole SMH4029293 to the existing manhole SMH4067823 in Ngan Hon Street (a distance of approximately 42m), where it will discharge to a 1050mm diameter drain running to the south. The existing intermediate manholes and building connections would be reconstructed to suit the new drainage arrangement. As shown in **Appendix A**, the runoff from the catchment only represents a small percentage (less than 16%) of the main drainage capacity and the drain is considered adequate. Furthermore, it is noted that the 1050mm diameter drain has a fairly small upstream catchment, which apparently commences at chamber SSH4004041, a short distance along To Kwa Wan Road, and is therefore likely to have adequate spare capacity.

Construction and Maintenance

- 2.4.3** The Project Proponent will be responsible for the design, construction, repair and maintenance of the proposed internal drainage facilities and connection to the main drainage system(s) to the satisfaction of relevant Government departments.

2.5 Discussion

- 2.5.1** It is concluded that the proposed development at the Site will not affect the peak runoff to the main drainage system.
- 2.5.2** The proposed development will affect drainage from a small upstream catchment and a new short length of drain will be required to convey runoff from this area to the existing main drainage system in Ngan Hon Street. This new drain will remove the existing unacceptably small drains.
- 2.5.3** It is concluded that the proposed development can be implemented without unacceptable drainage impact or increased flooding risk to nearby areas. No further drainage assessment is considered necessary.

3. Sewerage Impact Assessment

3.1 Scope of Work

- 3.1.1 The aim of this study is to assess whether the capacity of the existing sewerage networking to the Subject Site is sufficient to cope with the sewage flow from the proposed redevelopment and existing development in the vicinity. Drainage Record Plans from the Drainage Services Department were obtained for the information of sewerage impact assessment.

3.2 Existing Sewerage System

- 3.2.1 According to the Drainage Record obtained from the Drainage Services Department, there are existing Ø300mm sewers running along the western side of the Subject Site (manhole reference no. FMH4024355 to FMH4024356), which are then connected to the Ø525mm sewers running along the Hung Fook Street (manhole reference no. FMH4024356 to FMH4025769).
- 3.2.2 The existing sewer network along Hung Fook Street is also connected to sewers including Yuk Shing Street, Kai Ming Street, and Wing Kwong Street. The sewage flow from the connected sewer network is included as catchment areas as shown in **Figure 3**.

3.3 Assessment Criteria and Methodology

- 3.3.1 Environmental Protection Department's (EPD's) Guidelines for Estimating Sewage Flows for Sewage Infrastructure Planning, Version 1 (GESF) is referred to estimate the quantity of the sewage generated from the proposed development and the existing development. Sewage flow parameters and global peaking factors in this document are adopted.
- 3.3.2 According to the Table T-1 of the said EPD Guideline, the unit flow factors of Domestic Flow – Private R2 is 0.27 m³/day.
- 3.3.3 According to the Table T-2 of the said EPD Guideline, the general unit flow factors is 0.20 m³/day and the flow of commercial employee is 0.08 m³/day.
- 3.3.4 The Catchment Inflow Factor (P_{CIF}) for Central Kowloon – 1.00 is used for both upstream and downstream catchment areas.

3.4 Wastewater Generated by the Proposed Development

- 3.4.1 Wastewater arising from the proposed redevelopment will be primarily contributed by the residents, and commercial employees of the clubhouse and retail shops. Under the current proposal, there will be a total of 759 residential units, with non-domestic gross floor area of 6,843m².
- 3.4.2 The average household density of 2.9 according to 2011 Hong Kong Population Census (Kowloon City District) is assumed as a worse cast scenario, which would amount to about 2201 residents.
- 3.4.3 73 employees are assumed for the retail shops and 15 employees are assumed for the residential clubhouse.

3.4.4 Detailed calculation for the proposed development is given in **Table 1** below.

Table 1 Estimated Peak Flow

Calculation for Sewage Generation Rate of the Proposed Development		
Proposed Development		
1a. Total number of residential units	=	759 units
1b. Total number of residents	=	2201 residents (average household density of 2.9 in Kowloon City)
1c. Design flow	=	270 litre/employee/day -- (Private R2 in Table-1 of GESF)
1d. Sewage generation rate	=	594.3 m ³ /day
2. Total Non-Domestic Area (Including Clubhouse)	=	6843 m ²
2a. Assumed floor area per employee	=	25 m ² per employee – (refer to Table 2 in Chapter 5 of HKPSG)
2b. Total number of employees	=	274 employees
2c. Design flow for commercial employee	=	80 litre/employee/day -- (refer to Table T-2 of GESF)
2d. Design flow for commercial activities	=	200 litre/employee/day -- (refer to Table T-2 of GESF – General)
2e. Sewage generation rate	=	76.6 m ³ /day
Proposed Connection to Manhole S1 (FMH4024355)		
Flow Rate	=	670.9 m ³ /day
Contributing Population	=	2485 persons
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	46.6 litre/sec

3.5 Assessment of Sewerage Impact

- 3.5.1** Sewage generated from the Subject Site will be discharged to the existing sewerage system along Hung Fook Street, to manhole no. FMH4024355, as shown in **Figure 3**. According to the drainage record available from DSD, the sewers on Hung Fook Street are 300mm (i.e. pipe segments from manhole no. FMH4024355 to FMH4024356) and 525mm in diameter (i.e. pipe segments from manhole no. FMH4024356 to FMH4025769).
- 3.5.2** It is estimated that the capacity of the aforementioned pipe segments is sufficient to cater the sewage generated from its catchment and from the Subject Site.
- 3.5.3** **Appendix B** shows the detailed calculation on the estimated hydraulic capacity of the existing sewer sections and the calculation of the amount of the sewage entering each segment of the said sewer network.

3.6 Discussion

- 3.6.1** The potential sewerage impact due to the proposed development has been quantitatively addressed as shown in **Appendix B**. The average and peak flow rates from the proposed development are respectively about 670.9 m³/day and 46.6 litre/sec.

- 3.6.2** Since the incoming invert level of Manhole S2 (FMH4024356) is missing on the DSD Drainage Record, the outgoing level of 3.15mPD is used. For the inconsistent invert levels (upward slope) on the DSD Drainage Records, the incoming and outgoing invert levels of Manholes S3 (FMH2024357) have been amended to 3.13mPD to align with the downstream sewer pipes. Also, the invert levels of Manhole S4 (FMH4024358) and Manhole S5 (FMH4024359) are identical. To maintain velocity between the two manholes, the invert levels of Manhole S5 have been adjusted to align with Manhole S4 and Manhole S6.
- 3.6.3** After calculating the appropriate invert levels as mentioned above, the estimated sewage flow from the Subject Site has been compared with the capacity of the existing sewerage system to determine whether it has adequate spare capacity to accommodate the flow from the proposed connection.
- 3.6.4** According to **Table 4 of Appendix B**, it is found that the existing sewerage system has sufficient capacity to cater the sewage generated from the proposed development.
- 3.6.5** No upgrading or improvement work of the existing sewerage system will therefore be required.

4. Water Impact Assessment

4.1 Existing Freshwater Supply

- 4.1.1 The Site is within the supply zone of Ho Man Tin East Fresh Water Service Reservoir (Capacity 152,000m³) and Ho Man Tin West Fresh Water Service Reservoir (Capacity 67,434m³).
- 4.1.2 The Site is supplied by a 6" water main in Ngan Hon Street and smaller mains within an access alleyway running through the middle of the Site (see extracts from WSD Water Mains Record Plans in **Appendix D**).

4.2 Existing Saltwater Supply

- 4.2.1 The Site is within the supply zone of Tai Wan Saltwater Pumping Station (Design Capacity (121.83MLD) and Lok Fu Salt Water Service Reservoir (capacity 8,597m³).
- 4.2.2 The Site is supplied by a 100mm diameter water main in an access alleyway running through the middle of the Site (see extracts from WSD Water Mains Record Plans in **Appendix D**).

4.3 Freshwater Supply Impact

- 4.3.1 Water demands for the existing and future scenarios are included in **Appendix E**, together with comparisons with the existing supply facilities.
- 4.3.2 The freshwater demand is expected to increase from 324.9m³/day to 670.6m³/d (an increase of 345.7m³/day, or 0.35MLD) as a result of the proposed development. However, as shown in **Appendix E**, there is substantially greater spare capacity in the service reservoirs serving the Site. The expected increase in demand can therefore be accommodated by the existing main supply facilities.
- 4.3.3 The detailed connection arrangements for the Site and the local watermains will be reviewed in later stages of project implementation. Local upgrading and/or realignment will be implemented as required.

4.4 Salt Water Supply Impact

- 4.4.1 The salt water demand is expected to increase from 82.1m³/day to 174.7m³/d (an increase of 92.6m³/day, or 0.09MLD) as a result of the proposed development. However, as shown in **Appendix E**, there is substantially greater spare capacity in the salt water pumping station serving the Site. The expected increase in demand can therefore be accommodated by the existing main supply facilities.
- 4.4.2 The detailed connection arrangements for the Site and the local watermains will be reviewed in later stages of project implementation. Local upgrading and/or realignment will be implemented as required.

Construction and Maintenance

- 4.4.3** The Project Proponent will be responsible for the design, construction, repair and maintenance of the internal water supply facilities and connection to the main water system(s) to the satisfaction of relevant Government departments.

4.5 Discussion

- 4.5.1** It is concluded that the proposed development at the Site will result in increases in both the fresh and salt water demands. However, it is also concluded that the increases can be accommodated by the existing main supply facilities.
- 4.5.2** It is concluded that the local supply mains will need to be investigated in detail during later stages of implementation.
- 4.5.3** It is concluded that the proposed development can be implemented without unacceptable water supply impact.

5. Overall Conclusion

- 5.1.1** Drainage, sewerage, and water impact assessments were conducted based on the latest development parameters for the redevelopment project at Hung Fook Street / Ngan Hon Street, Kowloon City.
- 5.1.2** Based on the drainage impact assessment results, a new drainage pipe is proposed to convey runoff from the proposed development to the existing main drainage system in Ngan Hon Street. It is concluded that the proposed can c be implemented without unacceptable drainage impact or increased flooding risk to nearby areas. No further drainage assessment is considered necessary.
- 5.1.3** Based on the sewerage impact assessment results, it is found that the capacity of the existing sewerage system serving the area would be sufficient to cater for the sewage generation from the proposed development. No upgrading or improvement works of the existing sewerage system will therefore be required.
- 5.1.4** Based on the water impact assessment results, the increases in fresh and salt water demands can be accommodated by the existing main supply facilities. It is concluded that the proposed development can be implemented without unacceptable water supply impact.

FIGURES

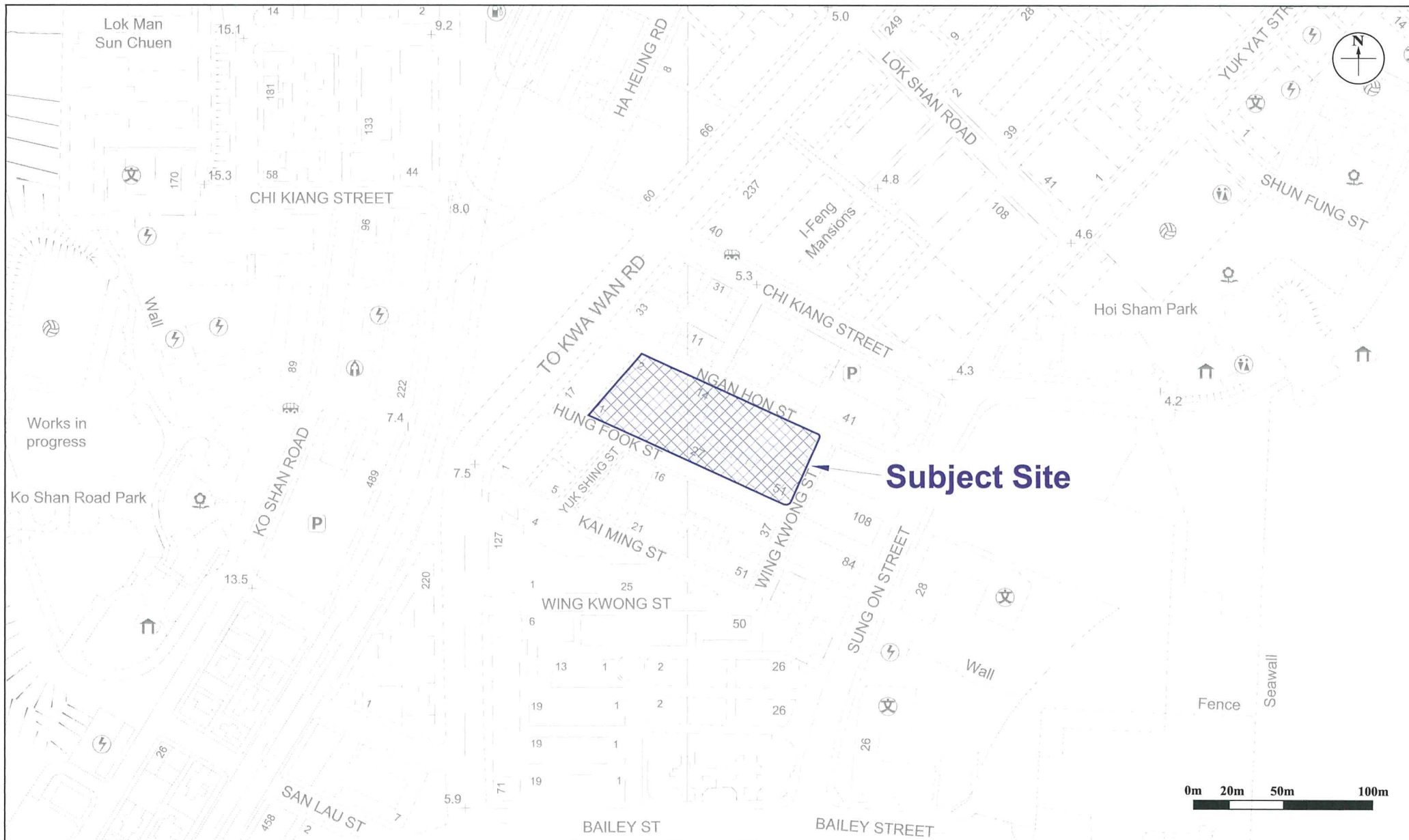


Figure: 1

Title: Subject Site and its Environ

Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: MW

Checked by: DY

Rev.: 1.0

Date: May 2016

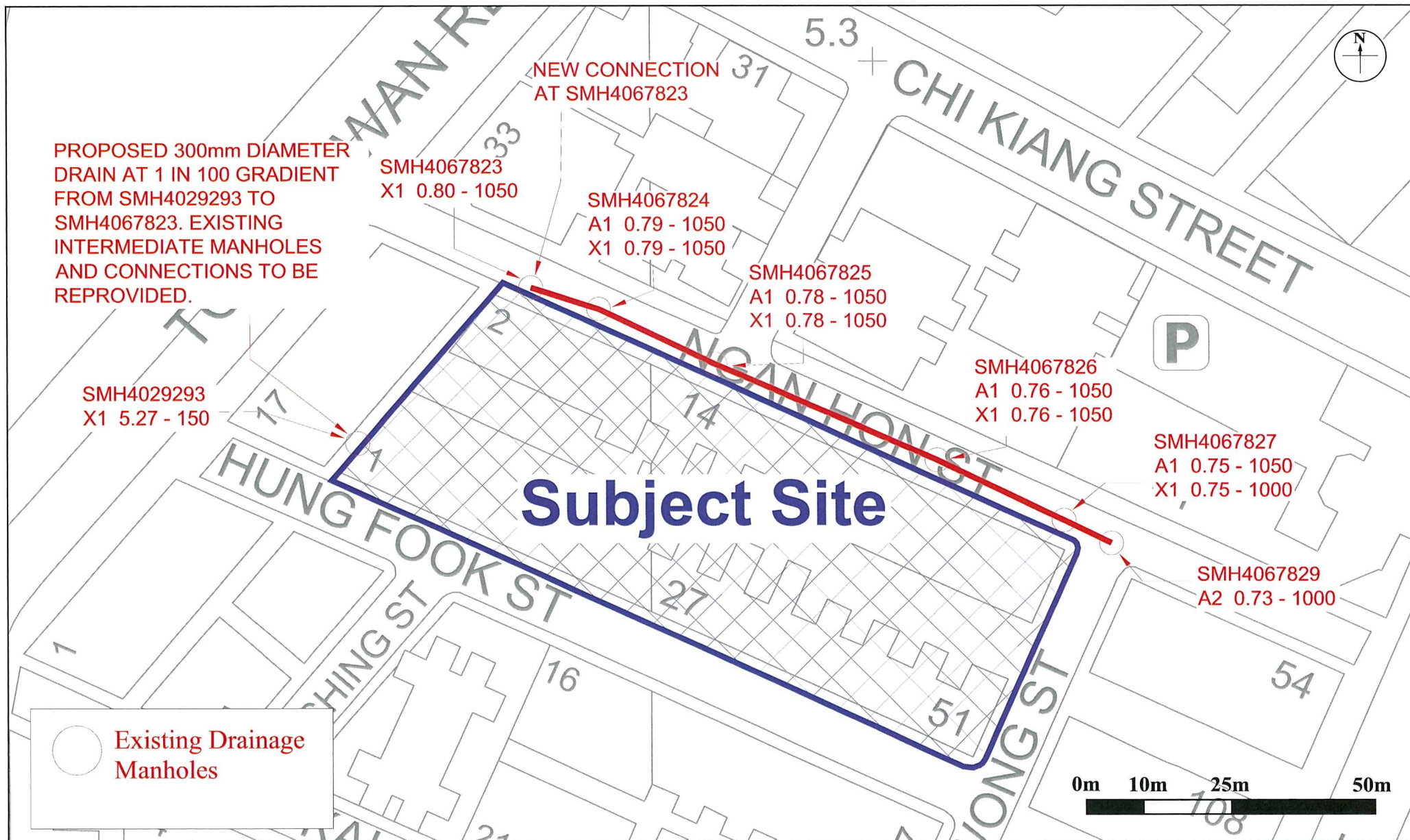


Figure: 2	RAMBOLL ENVIRON
Title: Existing Drainage System in the Vicinity of the Subject Site	Drawn by: MW
Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City	Checked by: TC
	Rev.: 1.1
	Date: Aug 2016

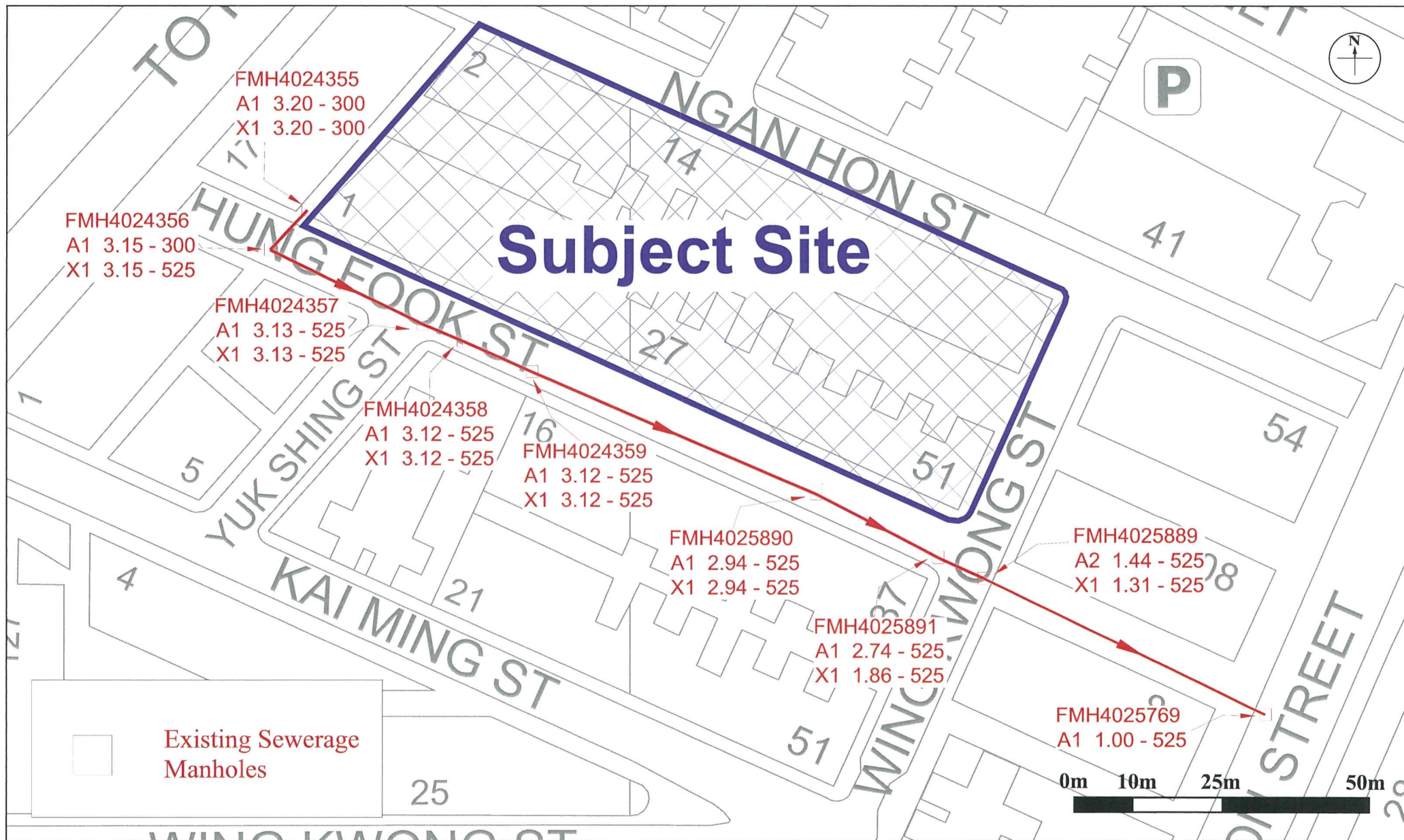


Figure: 3

Title: Existing Sewerage System in the Vicinity of the Subject Site

Project: Drainage and Sewerage Impact Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: MW

Checked by: TC

Rev.: 1.1

Date: Jul 2016

APPENDIX A: STORMWATER RUNOFF CALCULATIONS AND HYDRAULIC CALCULATIONS

1 in 50 year

C120 Proposed URA Redevelopment at Hung Fook Street/Ngan Hon Street, Kowloon City

Existing Upstream Catchment

Catchment Areas and Run-off (1 in 50 Year)

Refer:

DSD Stormwater Drainage Manual

Notes:

Runoff from Nos. 17-31 To Kwa Wan Road to be collected

Existing Catchment is fully paved; C = 1.0

Catchment is small, so Rational Method is appropriate

a, b and c from the DSD Stormw:

1 in 50 year

Intensity=

$$a/(t_c+b)^c$$

a

b

c

687

4.2

0.42

Catchment	Area (m ²)	Total t _c ¹ (min)	Intensity (mm/h)	Runoff Coefficient	Run-off (m ³ /s)
<u>Existing</u>					
Upstream Catchment (17-31 To Kwa Wan Road)	800	1.0	344		
Paved @ 100%	800			1.00	0.076
Total					0.076

¹ Assumed Time of Concentration

Check capacity of existing drains and proposed reconstructed drain from SMH4029293 to SMH4067823

Peak Discharge (1 in 50-year, m ³ /s)	0.076
--	-------

Manhole Ref.	CL (mPD)	IL (mPD)	Distance (m)	Diameter (mm)	Gradient (1 in)	Capacity (m³/s)	% of Capacity Overall (m³/s)	Remarks
<u>Existing Drains in Ngan Hon Street</u>								
SMH4067822	6.35	0.82 0.82	25	1050	1250	0.681	11.2	Drainage in Ngan Hon Street
SMH4067823	6.06	0.80 0.80						12
SMH4067824	5.87	0.79 0.79	25	1050	2500	0.480	15.8	
SMH4067825	5.53	0.78 0.78						37
SMH4067826	5.15	0.76 0.76	24	1050	2400	0.490	15.5	
SMH4067827	4.80	0.75 0.75						9
SMH4067829	4.78	0.73						
<u>Proposed New Connection in Alleyway</u>								
SMH4029293	6.60	5.27	42	300	100	0.100	75.8	Proposed New Connection. Intermittent manholes to be provided.
SMH4067823	6.06	4.85						

APPENDIX B: DETAILED SEWERAGE IMPACT ASSESSMENT CALCULATIONS

Table 1 Calculation for Sewage Generation Rate of the Proposed Development at Hung Fook Street / Ngan Hon Street, Kowloon City

<u>Proposed Development</u>		Remarks
1a. Total number of residential units	=	759 units
1b. Total number of residents	=	2201 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Sewage Generation rate	=	594.3 m ³ /day
2. Total Non-Domestic Area (Including Clubhouse)	=	6843 m ²
2a. Assumed floor area per employee	=	25 m ² per employee -- (refer to Table 2 in Chapter 5 of HKPSG)
2b. Total number of employees	=	274 employees
2c. Design flow for commercial employee	=	80 litre/employee/day -- (refer to Table T-2 of GESF)
2d. Design flow for commercial activities	=	200 litre/employee/day -- (refer to Table T-2 of GESF - General)
2e. Sewage Generation rate	=	76.6 m ³ /day
Proposed Connection at Manhole S1 (FMH4024355)		
Flow Rate	=	670.9 m ³ /day
Contributing Population	=	2485 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u><u>46.6</u></u> litre/sec

Table 2 Hydraulic Capacity at Existing Sewers at Hung Fook Street / Ngan Hon Street, Kowloon City

Segment	Manhole Reference	Manhole Reference	Pipe Dia.	Pipe Length	Invert Level 1	Invert Level 2	g	k _s	s	s	v	V	Area	Q	Estimated Capacity
			mm	m	mPD	mPD	m/s ²	m	1 in		m ² /s	m/s	m ²	m ³ /s	L/s
S1-S2	FMH4024355	FMH4024356	300	9.0	3.20	3.15	9.81	0.006	153	0.007	0.000001	0.89	0.07	0.06	63
S2-S3	FMH4024356	FMH4024357	525	28.9	3.15	3.13	9.81	0.006	1203	0.001	0.000001	0.46	0.22	0.10	100
S3-S4	FMH4024357	FMH4024358	525	7.4	3.13	3.12	9.81	0.006	739	0.001	0.000001	0.59	0.22	0.13	128
S4-S5	FMH4024358	FMH4024359	525	12.6	3.12	3.10	9.81	0.006	630	0.002	0.000001	0.64	0.22	0.14	139
S5-S6	FMH4024359	FMH4025890	525	52.7	3.10	2.94	9.81	0.006	329	0.003	0.000001	0.89	0.22	0.19	192
S6-S7	FMH4025890	FMH4025891	525	23.4	2.94	2.74	9.81	0.006	117	0.009	0.000001	1.49	0.22	0.32	322
S7-S8	FMH4025891	FMH4025889	525	8.4	1.86	1.44	9.81	0.006	20	0.050	0.000001	3.61	0.22	0.78	782
S8-S9	FMH4025889	FMH4025769	525	53.1	1.31	1.00	9.81	0.006	171	0.006	0.000001	1.23	0.22	0.27	266

Notes (highlighted in red):

- (1) According to DSD Drainage Records, the incoming level of Manhole S2 (FMH4024356) is missing. Therefore the outgoing level of 3.15mPD is used instead.
- (2) According to DSD Drainage Records, both invert levels of Manhole S3 (FMH4024357) are listed as 3.17mPD. This would cause an upward slope from Manhole S2 and is disregarded. Therefore, they are amended to align with the downstream sewer pipes.
- (3) According to DSD Drainage Records, the invert levels of Manhole S4 (FMH4024358) and Manhole S5 (FMH4024359) are identical, at 3.12mPD. This would cause flat gradient and is disregarded. Therefore, the invert levels of Manhole S5 have been adjusted to align with Manhole S4 (from upstream) and Manhole S6 (to downstream).

- Remarks: (1) g=gravitational acceleration; k_s=equivalent sand roughness; s=gradient; v=kinematic viscosity of water; V=mean velocity
- (2) The value of k_s = 6mm is used for the calculation of concrete sewer (based on Table 5: Recommended roughness values in Sewerage Manual)
 - (3) The value of velocity (V) is referred to the Tables for the hydraulic design of pipes, sewers and channels (8th edition)
 - (4) Equation used:
$$V = -\sqrt{(8gDs)} \log\left(\frac{k_s}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}}\right)$$

Table 3a Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment A)

Catchment A

Residential Developments at 17-31 To Kwa Wan Road

1a. Assumed number of total residential units	=	73 units
1b. Total number of residents	=	212 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	57.2 m ³ /day

Sub-total Catchment A

Flow Rate	=	57.2 m ³ /day
Population	=	212 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u>5.3</u> litre/sec

Total Flow at Manhole S1 (FMH4024355), Proposed Development and Catchment A

Flow Rate	=	728.1 m ³ /day
Flow Rate with Catchment Inflow Factor (Central Kowloon)	=	728.1 m ³ /day
Contributing Population	=	2697 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>50.6</u> litre/sec

Table 3b Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment B)

Catchment B

Residential Developments at 1-15 To Kwa Wan Road, 3 Kai Ming Street, and 2-6 Hung Fook Street

1a. Assumed number of total residential units	=	111 units
1b. Total number of residents	=	322 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	86.9 m ³ /day

Sub-total Catchment B

Flow Rate	=	86.9 m ³ /day
Population	=	322 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u>8.0</u> litre/sec

Total Flow at Manhole S2 (FMH4024356), Proposed Development and Catchment A-B

Flow Rate	=	815.0 m ³ /day
Flow Rate with Catchment Inflow Factor (Central Kowloon)	=	815.0 m ³ /day
Contributing Population	=	3019 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>56.6</u> litre/sec

Table 3c Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment C)

Catchment C

Residential Development at 11-19 Kai Ming Street

1a. Assumed number of total residential units	=	168 units
1b. Total number of residents	=	487 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	131.5 m ³ /day

Sub-total Catchment C

Flow Rate	=	131.5 m ³ /day
Population	=	487 people
Peaking factor	=	8 Refer to Table T-5 of GESF for population <1,000 including stormwater allowance
Peak Flow	=	<u>12.2 litre/sec</u>

Total Flow at Manhole S3 (FMH4024357), Proposed Development and Catchment A-C

Flow Rate	=	946.6 m ³ /day
Flow Rate with Catchment Inflow Factor (Central Kowloon)	=	946.6 m ³ /day
Contributing Population	=	3506 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>65.7 litre/sec</u>

Table 3d Calculation for Sewage Generation Rate of the Existing Surrounding Buildings (Catchment D)

Catchment D

Residential Developments at 44-54 Ngan Hon Street, 72-108 Wing Kwong Street

1a. Assumed number of total residential units	=	269 units
1b. Total number of residents	=	780 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
1c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
1d. Residential Sewage Generation rate	=	210.6 m ³ /day

2. Wing Fai Mansion (52 Wing Kwong Street)

2a. Assumed number of total residential units	=	204 units
2b. Total number of residents	=	592 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
2c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
2d. Residential Sewage Generation rate	=	159.7 m ³ /day

3. Residential Developments at 16-46 Hung Fook Street, 21-51 Kai Ming Street

3a. Assumed number of total residential units	=	243 units
3b. Total number of residents	=	705 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
3c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
3d. Residential Sewage Generation rate	=	190.3 m ³ /day

4. Residential Developments at 1-33 Wing Kwong Street

4a. Assumed number of total residential units	=	61 units
4b. Total number of residents	=	177 residents (refer to 2011 Population Census - avg household density of 2.9 in Kowloon City District)
4c. Design flow	=	270 litre/person/day -- (Private R2 in Table T-1 of GESF)
4d. Residential Sewage Generation rate	=	47.8 m ³ /day

Sub-total Catchment D

Flow Rate	=	608.4 m ³ /day
Population	=	2253 people
Peaking factor	=	6 Refer to Table T-5 of GESF for population 1,000-5,000 including stormwater allowance
Peak Flow	=	<u>42.2</u> litre/sec

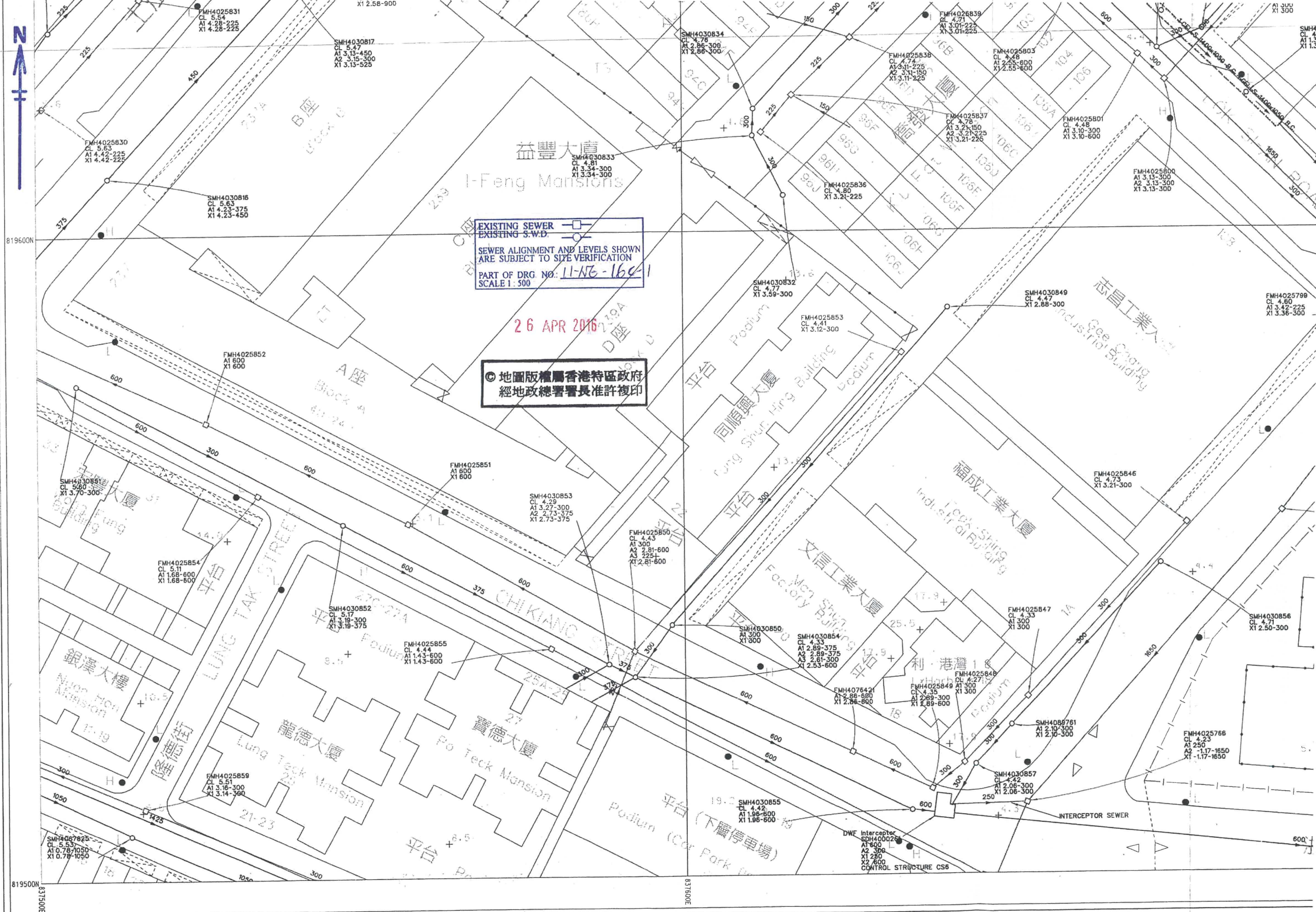
Total Flow at Manhole S8 (FMH4025889), Proposed Development and Catchment A-D

Flow Rate	=	1554.9 m ³ /day
Flow Rate with Catchment Inflow Factor (Central Kowloon)	=	1554.9 m ³ /day
Contributing Population	=	5759 people
Peaking factor	=	5 Refer to Table T-5 of GESF for population 5,000-10,000 including stormwater allowance
Peak Flow	=	<u>90.0</u> litre/sec

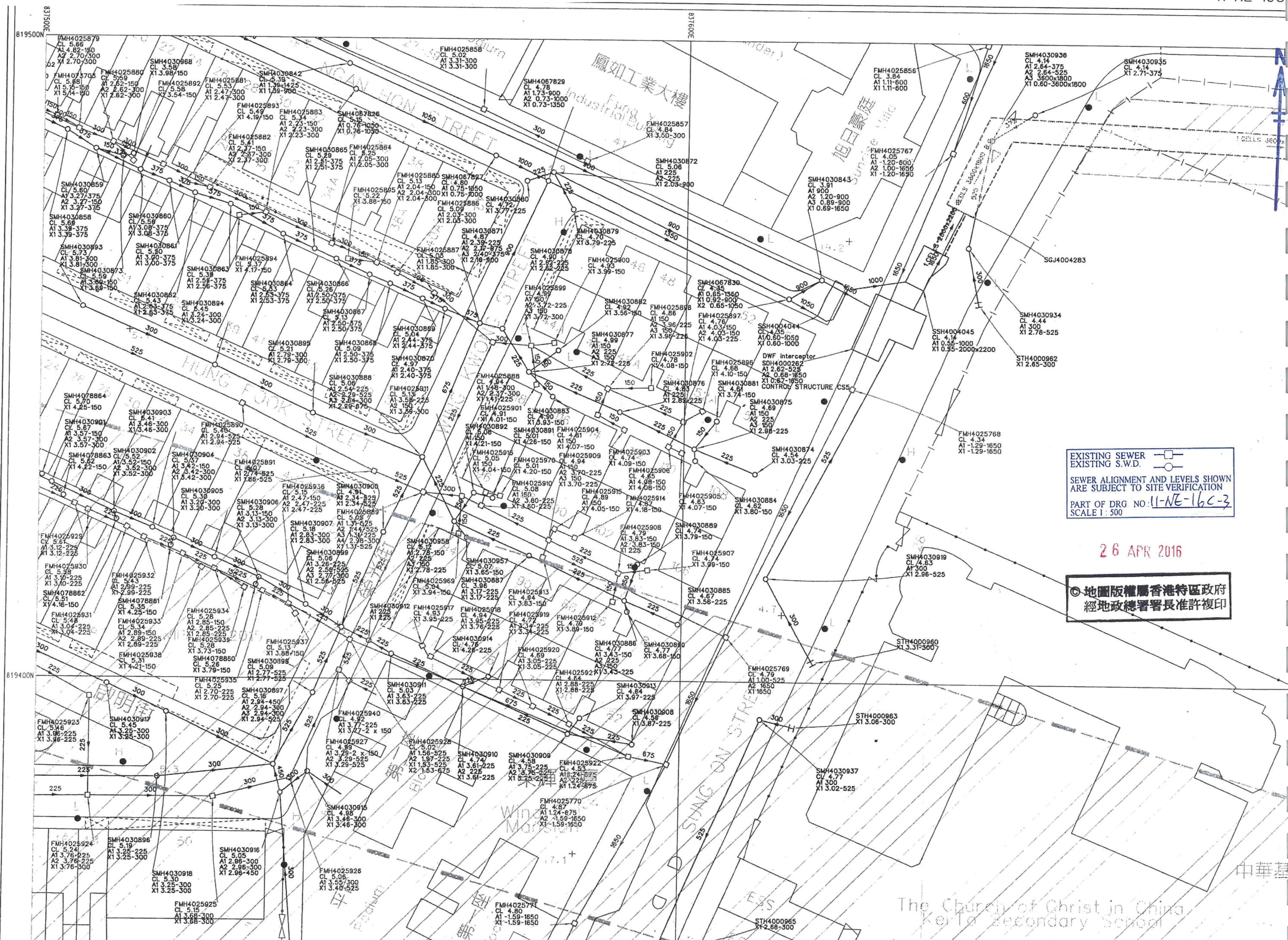
Table 4 Hydraulic Capacity of Existing Sewers for Sewerage generated from the Proposed Development only

Segment	Pipe Dia. (mm)	Pipe Length (m)	Gradient	Estimated Capacity (L/s)	Estimated Flow including the Proposed Development only (L/s)	Contributed by the Proposed Development only (%)	Status	Estimated Flow including the Proposed Development and Catchment Areas (L/s)	Contributed by the Proposed Development and Catchment Areas (%)	Status
S1-S2	300	9.0	0.0066	63	46.6	74.1%	OK	50.6	80.4%	OK
S2-S3	525	28.9	0.0008	100	46.6	46.5%	OK	56.6	56.4%	OK
S3-S4	525	7.4	0.0014	128	46.6	36.4%	OK	65.7	51.4%	OK
S4-S5	525	12.6	0.0016	139	46.6	33.6%	OK	65.7	47.4%	OK
S5-S6	525	52.7	0.0030	192	46.6	24.3%	OK	65.7	34.2%	OK
S7-S8	525	8.4	0.0502	782	46.6	6.0%	OK	65.7	8.4%	OK
S8-S9	525	53.1	0.0058	266	46.6	17.5%	OK	90.0	33.8%	OK

APPENDIX C: DRAINAGE RECORD PLANS OBTAINED FROM DRAINAGE SERVICES DEPARTMENT



Legend :

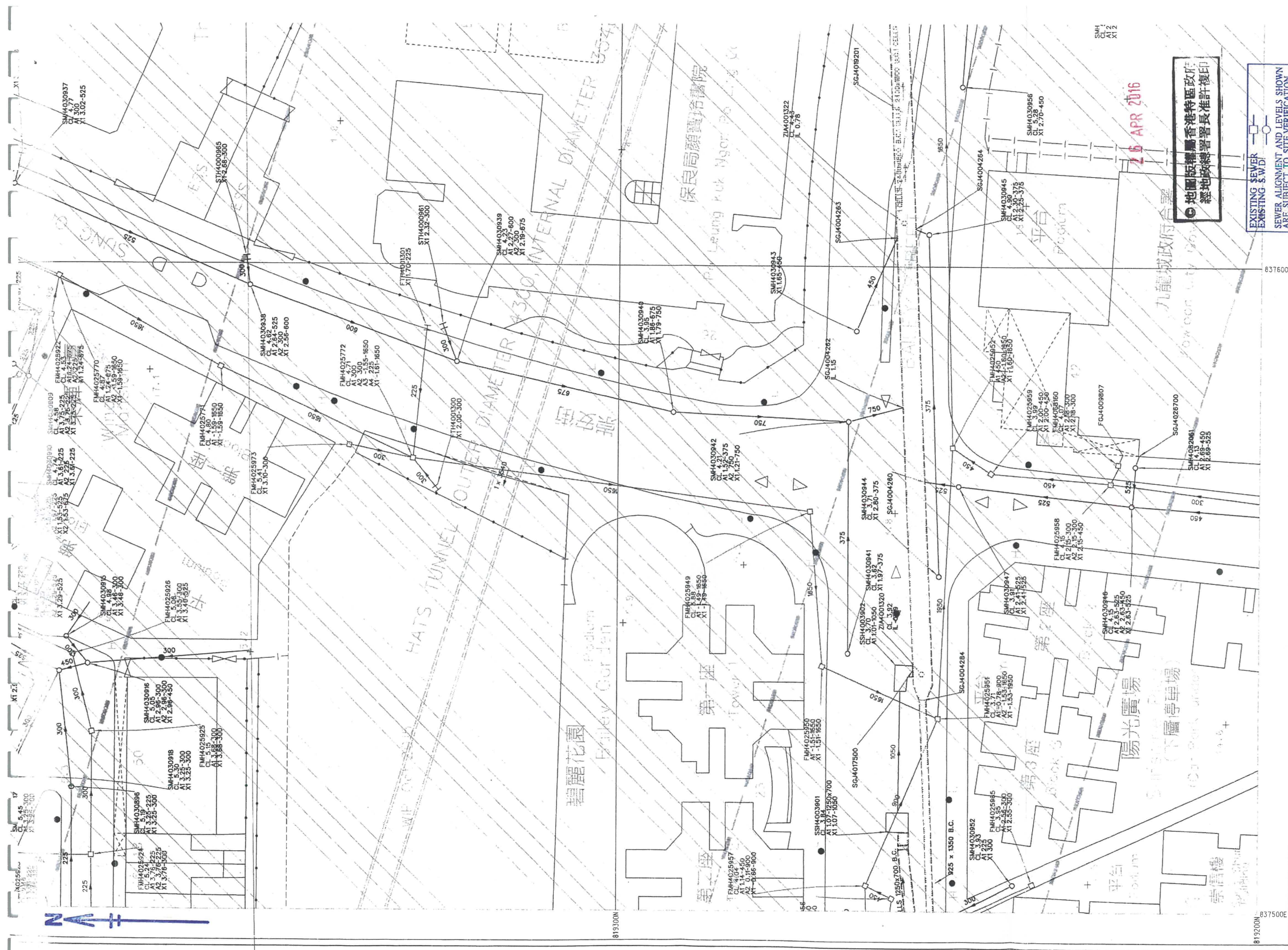


EXISTING SEWER
EXISTING S.W.D.
SEWER ALIGNMENT AND LEVELS SHOWN
ARE SUBJECT TO SITE VERIFICATION
PART OF DRG NO.: 11-NE-16C-3
SCALE 1:500

26 APR 2016

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經地政總署署長准許複印

The Church of Christ in China
Ker Lo Secondary School



Legend:

- Storm Water Manhole
- Storm Water Terminal Manhole
- Existing Pipe (Storm/Sewer/Combined)
- Existing Pipe (Storm/Sewer/Combined)

Tunnel Protection Zone

Slope Sign Board
Slope Number

Metres

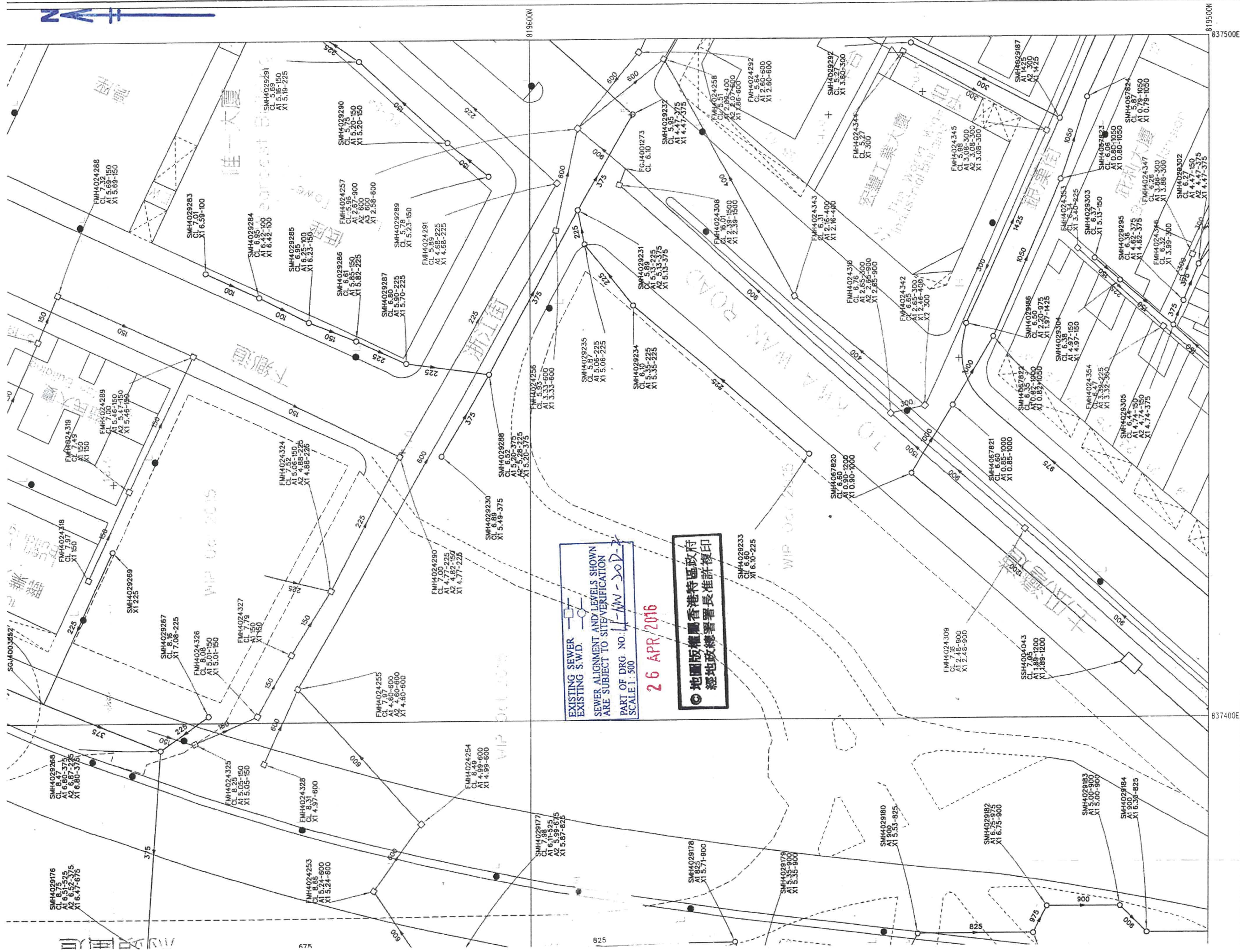
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經地政總署署長准許複印

EXISTING SEWER
EXISTING S.W.D.

SEWER ALIGNMENT AND LEVELS SHOWN
ARE SUBJECT TO SITE VERIFICATION

PART OF DRG NO. H-16-1663

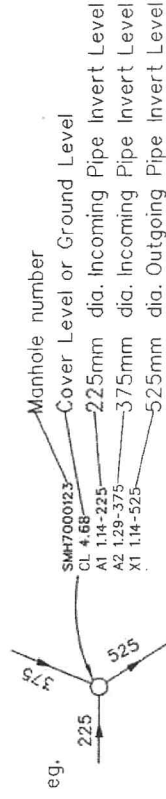
SCALE 1:500



Notes:

- All levels are given in metres principle datum.
- All dimensions shown are in millimetres unless otherwise stated.
- The information shown on the record drawings are subject to verification on site and no guarantee can be given that this is a complete record.
- Abbreviations for Channels of width smaller or equal to 1200mm:
900C = 900mm width Surface Channel

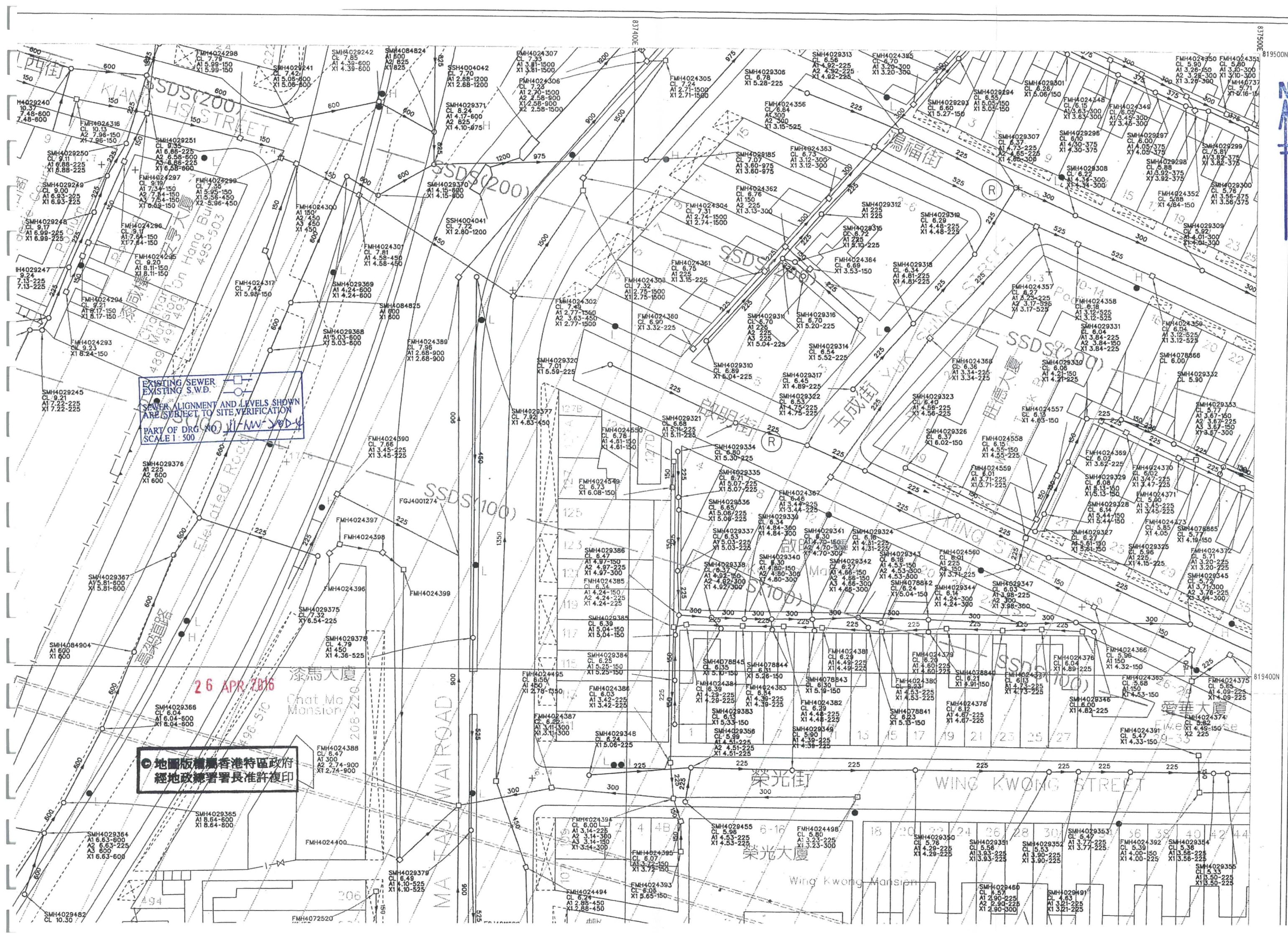
- The Incoming Pipes are marked A1, A2, A3, ... counting clockwise from the first Outgoing Pipe X1. Outgoing Pipes are marked X1, X2, X3 ... counting clockwise from North.



Drainage Record Sheet Number

11-NW-20D-2

Last Updating : 27-11-2015



EXISTING SEWER
EXISTING S.W.D.
SEWER ALIGNMENT AND LEVELS SHOWN
ARE SUBJECT TO SITE VERIFICATION
PART OF DRG. NO. J1-MW-3004
SCALE 1:500

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26 APR 2016

漆馬大廈
Chat Ma Mansion

榮光街
Wing Kwong Street

榮光大廈
Wing Kwong Mansion

WING KWONG STREET

MA TAU WAI ROAD

APPENDIX D: EXTRACTS OF WATER MAINS OBTAINED FROM WATER SERVICES DEPARTMENT

APPENDIX E: WATER DEMAND CALCULATIONS

C120 Proposed URA Redevelopment at Hung Fook Street/Ngan Hon Street, Kowloon City
Populations and Water Demands
Summary

Scenario	Daily Demand - Freshwater (m ³ /d)	Daily Demand - Saltwater (m ³ /d)
Existing	324.9	82.1
Future	670.6	174.7
Increase	345.7	92.6
	(MLD)	(MLD)
Increase	0.35	0.09

Freshwater Reservoir Capacity

Reservoir Capacity Factors are 0.8 for both FWSRs

Reservoir	Capacity (x 1,000 m ³)	Supply Capacity @ 0.8 (MLD)	Demand Trend (MLD)	Spare Capacity (MLD)	Remarks
Ho Man Tin East FWSR	152	190	144.88	45.12	>>0.35MLD Additional Demand from Development
Ho Man Tin West FWSR	67.43	84.29	32.63	51.66	>>0.35MLD Additional Demand from Development

Saltwater Pumping Capacity

Pumping Station	Design Capacity (MLD)	Actual Station Output (MLD)	Spare Capacity (MLD)	Remarks
Tai Wan SWPS	121.81	93.92	27.89	>>0.09MLD Additional Demand from Development

C120 Proposed URA Redevelopment at Hung Fook Street/Ngan Hon Street, Kowloon City
Populations and Water Demands
Existing Situation

Populations

Residential Accommodation

Address	Properties	Units/Prop	Density	Population
Bailey Mansion	1	58	2.6	151
12-42 Ngan Hon St	16	7	2.6	291
28A-42A Ngan Hon St	8	14	2.6	291
1-51 Hung Fook St	26	5	2.6	338
	51			1071 Persons

	Properties	Staff/Prop	Population
Management Staff (Service Trade), say	51	2	102 Persons

Water Demands

Refer WSD Departmental Instruction 1309

Population	Category	Population	Unit Demand Freshwater (m ³ /h/d)	Unit Demand Saltwater (m ³ /h/d)	Daily Demand Freshwater (m ³ /d)	Daily Demand Saltwater (m ³ /d)
<u>Residential Accommodation</u> Residential	R2	1,071	0.300	0.070	321.3	75.0
Staff	Service Trade	102	0.035	0.070	3.6	7.1
TOTAL					324.9	82.1

N.B. Accuracy as shown

C120 Proposed URA Redevelopment at Hung Fook Street/Ngan Hon Street, Kowloon City
Populations and Water Demands
Future Scenario

Populations

	<u>Units</u>	<u>PPF</u>	<u>Popn.</u>
<u>Residential Accommodation</u>			
Residential	759	2.9	2,201 Persons

Management Staff, say **20 Persons**

Non-Domestic
Area Unit Area
(m²) (m²)

<u>Service Trade</u>			
Employees	6,843	25	274 Persons

Water Demands

Refer WSD Departmental Instruction 1309

Population	Category	Population	Unit Demand Freshwater (m ³ /h/d)	Unit Demand Saltwater (m ³ /h/d)	Daily Demand Freshwater (m ³ /d)	Daily Demand Saltwater (m ³ /d)
<u>Residential Accommodation</u>						
Residential	R2	2,201	0.300	0.070	660.3	154.1
General staff	Service Trade	20	0.035	0.070	0.7	1.4
<u>Service Trade</u>						
Employees	Service Trade	274	0.035	0.070	9.6	19.2
TOTAL					670.6	174.7

N.B. Accuracy as shown



**Term Environmental Consultancy Services
For a Project at
Hung Fook Street / Ngan Hon Street, Kowloon City District
(KC-010)**

Environmental Assessment

Prepared for:
Urban Renewal Authority

Prepared by:
Ramboll Environ Hong Kong Limited

Date:
August 2016

Reference Number:
R5152_V1.1_20160812

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1. Introduction

1.1 Background and Objectives

- 1.1.1 The Urban Renewal Authority (URA) has proposed the Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street in Kowloon City.
- 1.1.2 Ramboll Environ Hong Kong Limited has been appointed by the Urban Renewal Authority (URA) to conduct this Environmental Assessment for submission to the Town Planning Board for approval.

1.2 Subject Site and its Environs

- 1.2.1 The Subject Site is bounded by Hung Fook Street to the South, Wing Kwong Street to the East, Ngan Hon Street to the North and a back lane of the adjacent buildings to the West. The existing buildings within the Subject Site were built between 1957 and 1960, and range from 6 storeys to 8 storeys high. The buildings are mainly residential in nature with commercial premises at the ground floor.
- 1.2.2 **Figure 1** shows the location of the Subject Site and its environs.

1.3 Proposed Redevelopment

- 1.3.1 The proposed redevelopment to be built in the Subject Site will be built on the area zoned Residential (Group A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. The adjacent public pavement area shown as "Road", will not be built upon and it will be excluded from calculation for plot ratio purposes.
- 1.3.2 In line with the "Residential (Group A)" zoning, the redevelopment intention includes both residential and commercial uses. The subject redevelopment covers a gross site area of approximately 4,951m², with an approximate domestic gross floor area of 34,215m² and non-domestic gross floor area of 6,843m².
- 1.3.3 Three 23-storey residential towers are proposed with a total of 759 units, and a common clubhouse will be shared among the residential towers.

2. Noise Impact Assessment

2.1 Scope of Work

- 2.1.1 The Subject Site is zoned as R(A) according to the Approved Hung Hom Outline Zoning Plan No. S/K9/24. Under the R(A) zone, residential development is always permitted for the Subject Site.
- 2.1.2 The aim of this study is to review the potential noise impacts (i.e. road traffic noise impact and fixed noise impact) on the noise sensitive uses of the proposed redevelopment, and to recommend mitigation measures where practicable to attenuate the impact.
- 2.1.3 There is no railway noise source identified in the surrounding so that no impact is anticipated on the proposed development.

2.2 Appraisal of Noise Environment

- 2.2.1 The Subject Site is located in an urban area and surrounded by a high density low-rise private housing environment. Two industrial buildings are located at the north and northeast to the Subject Site respectively.
- 2.2.2 The noise environment is mainly dominated by the existing roads surrounded, namely To Kwa Wan Road to the west, Ngan Hon Street to the north, Wing Kwong Street to the east and Hung Fook Street to the south, while the Chatham Road Flyover (from Ma Tau Kok to Chatham Road North) and Chatham Road North to the west are also considered as highly concerned road traffic noise sources to the proposed development. A future access road just between **Tower A and Tower B, C** in close proximity to the proposed development would also be considered for the contribution of adverse noise impact to the Subject Site.
- 2.2.3 Garages, workshops for car washing and car painting and metal workshops are mainly located along Ngan Hon Street and Wing Kwong Street which are identified as potential fixed noise sources to the Subject Site. There are also two industrial buildings located at both ends of the Ngan Hon Street, including the World Wide Industrial Building at the junction of the Ngan Hon Street and To Kwa Wan Road and the Fung Yu Industrial Building behind the Sunrise Villa. The location of two industrial buildings is shown in **Figure 2.3**.
- 2.2.4 There is no railway source identified in the vicinity (i.e. 300m) of the Subject Site. Therefore, no impact is anticipated on the proposed development.

2.3 Road Traffic Noise Impact Assessment

- 2.3.1 **Introduction**
 - 2.3.1.1 This traffic noise impact assessment is prepared to address road traffic noise impact on the noise sensitive uses of the proposed development and recommend mitigation measures where practicable to attenuate the impact.

2.3.2 Assessment Criteria

- 2.3.2.1 Noise standards are recommended in the Hong Kong Planning Standards and Guidelines (HKPSG) for planning against noise impact from sources such as road traffic, railway and aircraft. The proposed development comprises car park, plant room, retail, clubhouse and residential uses. Retail and clubhouse will be air conditioned and will not rely on opened window for ventilation. Under the HKPSG, the criterion for road traffic noise impact on domestic premises (habitable rooms) is L10(1-hour) 70dB(A). This criterion applies to uses which rely on opened windows for ventilation.

2.3.3 Assessment Methodology

- 2.3.3.1 In this assessment, the potential noise impact arising from nearby existing and future road carriageways on the development has been assessed. It involved the prediction of future noise impacts on Noise Sensitive Receivers (NSRs) arising from both AM and PM peak hourly traffic flows along existing and future road carriageways situated within or in the vicinity of the Subject Site. Calculations of predicted road traffic noise were based on the peak hour traffic flows projected within a 15-year period from the target completion date of the proposed development, year 2026. For worst-case scenario evaluation, the assessment year was chosen to be year 2041, which has the maximum forecasted traffic within 15-year period. The traffic forecast of year 2041 provided by the Project Traffic Consultant, involving traffic flows of both AM and PM peak hour and mix were used for this traffic noise impact assessment. The year 2041 traffic forecast data is attached in **Appendix 2.1**.

- 2.3.3.2 The U.K. Department of Transport's procedure "Calculation of Road Traffic Noise" was used to predict the hourly L10 noise levels generated from road traffic at selected representative NSRs. Practicable environmental mitigation measures have been recommended, where necessary. The predicted noise levels were compared with the relevant HKPSG noise standards (i.e. L10(1-hour) 70dB(A)).

2.3.4 Noise Sensitive Receivers

- 2.3.4.1 A number of Noise Sensitive Receivers (NSRs) are selected for the assessment. All assessment points were taken at 1.2 m above the floor and 1m away the façade of ventilation windows of rooms of sensitive use (living rooms and bedrooms). **Figure 2.1** shows the location of the selected NSRs of all residential floors for traffic noise impact assessment.

2.3.5 Noise Mitigation Measures of the MLP

Traffic Noise Mitigation Measures Considered

- 2.3.5.1 According to the traffic forecast, it is likely that the road traffic noise sources are mainly come from the western road carriageways, including To Kwa Wan Road and Chatham Road North, while the Chatham Road Flyover with the highest traffic flow and speed limit of 70km/hr among all other roads would contribute the most of adverse noise impact to the Subject Site. Various noise mitigation measures as listed below have been duly studied and incorporated in the MLP where possible.

a. Placement of Buildings

According to the proposed scheme shown in **Appendix 1.1**, Tower B and Tower C are located in the east of Tower A. Under consideration of the aforementioned major concerned road carriageways noise impact derived from the west, Tower A is positioned to act as an effective shielding block to cut off the direct view angle from the dominant traffic noise sources to both Tower B and Tower C.

b. Orientation of Buildings

As the dominant road noise sources mainly derive from the west while Tower A is the closest tower sat in the west of the Subject Site, the orientation of open windows in Tower A are designed to avoid any direct line of sight to the western road carriageways as far as practicable.

c. Building Setback

Building setback from the side roads is considered as one of the mitigation measures for the proposed development. For Tower A, approximately a 18m setback from Ngan Hon Street and a 8m setback from Hung Fook Street is adopted in the MLP.

d. Shielding Effect from Non-noise Sensitive Structures

Only having building setback from the nearby roads cannot effectively eliminate the road traffic noise impact for the Subject Site under the limitation of site condition (a further discussion would be in section 2.3.7) and a close proximity of the surrounding roads, therefore, podium is proposed altogether to provide further shielding for the low-level residential units which are facing those surrounding roads. All residential floors are sat beyond the non-noise sensitive floors including a podium consisted of three retail floors, a floor purposed as clubhouse and a transfer plate. The podium with top level of +20.05mPD acts as a noise shielding structure, especially for the low-level residential floors. As the lowest residential floor is +27.55mPD, with certain setback distance from the site boundary and further shielding from podium, some of the residential units in low-level floors would fall into the shadow zone so that the noise impact from the surrounding roads would be reduced.

e. Use of Vertical Fin

Full-height acoustic fins of 0.5m are proposed at specified NSRs to reduce the view angle to the traffic noise sources. The locations of acoustic fins are shown in **Figure 2.2**. Further increase of fins length on specific location is not feasible due to the nearby road carriageways. As the dominant road noise sources, including To Kwa Wan Road, Chatham Road North and Chatham Road Flyover, extend to both north and south within 300m area, the view angle cannot be effectively reduced due to the further lengthening of vertical fins. The noise reduction effect is determined using CRTN methodology taking into account the view angle correction but would have the maximum value of not more than 3 dB(A).

f. Use of Acoustic Balcony

In case the assessment determines the need to provide extra protection, balcony structure with solid parapet on 3 sides will be adopted to provide shielding in the living rooms. The ceiling of the acoustic balcony will be covered with sound absorption material to minimize

reflection of noise. The depth of the balcony is more than 1m and the solid parapet of the balcony is of at least 1.1m high. There will be no gap and opening in-between the parapet walls. The NSRs (which is put at 1m away from the opening of window) will be within the balcony feature and shielded by the parapet of the balcony. Taking into account possible noise reflection from the ceiling, a noise reduction of 2 dB is assumed as in many other applications. The locations of acoustic balcony applied in those living rooms are illustrated in **Figure 2.2**.

2.3.5.2 Various noise mitigation measures have been considered and exhausted. All practicable mitigation measures have been incorporated in the MLP for implementation in future. **Figure 2.2** shows the proposed direct noise mitigation measures.

2.3.6 Assessment Result with Proposed Noise Mitigation Measures

2.3.6.1 The predicted traffic noise impacts on the selected NSRs based on the unmitigated scenario of base scheme is summarized in **Appendix 2.2**.

2.3.6.2 Based on the results on the unmitigated scenario, noise mitigation measures discussed above were considered and adopted as described in **Figure 2.2** while the traffic noise impacts are quantified which are shown in the result tables in **Appendix 2.2**. There were 183 out of a total of 759 residential units with the predicted noise level exceeding the criteria of 70 dB(A). All exceedances were found only in Tower A regarding the assessment results. The compliance rate is about 76% with the maximum noise level of 75 dB(A) at the affected units.

2.3.7 Not Adopted Design in the Proposed Layout

2.3.7.1 The current notional design has exhaustively explored all possible noise mitigation measures to minimize the traffic noise impact to the proposed development in order to achieve a 76% of noise compliance level. However, the design of layout has been restricted owing to various site constraints as stated in the following:

- a. The Subject Site is shaped as a long and narrow rectangle and surrounded by local roads and adjacent existing buildings in a close proximity which have limited the flexibility of the placement and orientation of towers within the site boundary. Furthermore, the future access road within the site boundary separates the Subject Site into two portions – Site A and Site B, the flexibility is further eliminated for both separated buildable area;
- b. A notable building setback is limited by the site condition. As aforementioned, the road carriageways contributed most toward the Subject Site are To Kwa Wan Road, Chatham Road North and Chatham Road Flyover which located in the west of the Subject Site, building setback, especially for Tower A which is the most affected tower, is not effective to contribute certain extent on reducing road traffic noise impact from those dominant road carriageways from west;
- c. There is no window opening towards To Kwa Wan Road directly in order to prevent any direct line of sight, which affect orientation of towers (mainly Tower A); and
- d. Three towers approach to accommodate more residential units for market needs.

2.3.7.2 Apart from the above proposed layout design and at-receiver mitigation measures for minimizing the traffic noise impact, some other typical mitigation measures described in the EPD's Innovative Noise Mitigation Designs and Measures, including noise barrier and canopy, have been studied to investigate the effectiveness for further reducing the traffic noise impact to the proposed development. Two hypothetical studies are conducted with the incorporation of i) 5m(H) noise barrier erected at the podium and ii) 0.7m(W) of canopy extended from the edge of podium.

2.3.7.3 According to the assessment results of the hypothetical studies shown in **Appendix 2.3a and 2.3b**, both noise barrier and canopy cannot achieve a notable improvement for further reducing the traffic noise impact on the proposed layout. Detailed analysis is given below.

a. Noise Barrier

A 5m(H) noise barrier is considered to erect at the podium facing Ngan Hon Street, To Kwa Wan Road and Hung Fook Street in Site A for the hypothetical study. Noise barrier built along Ngan Hon Street and Hung Fook Street would not be effective since the noise sources contributed most on the traffic noise impact toward the Subject Site is derived from the western road carriageways.

Besides, the top level of the noise barrier is 25.05 mPD; while the road level of the To Kwa Wan Road in front of the Subject Site is about 7.5 mPD. Therefore, the noise barrier facing west along the podium is about 18m above the road surface of To Kwa Wan Road. The horizontal distance between To Kwa Wan Road and the noise barrier is approximately 25m while, due to the limitation of buildable area in Site A as mentioned above, Tower A is sat closely to the noise barrier. Thus, the impact from the noise barrier facing west would be insignificant.

A sensitivity test with the presence of 5m(H) noise barrier has been carried out. The results shown in **Appendix 2.3a** indicate that only three more units can be further mitigated to meet the noise criteria of 70dB(A). Therefore, it is concluded that there would not have obvious improvement on the traffic noise performance from the noise barrier.

b. Canopy

According to section 10 of the Building (Planning) Regulations on balconies and canopies over street, the maximum projection of any canopy erected over any street shall be either one-tenth of the width of the street; or 3m, whichever is the less. The width of Ngan Hon Street is about 7.2m while the width of Hung Fook Street is around 8.5m. Therefore, canopies with the depth of 0.7m and 0.8m are proposed at the podium facing Ngan Hon Street and Hung Fook Street respectively as shown in **Appendix 2.3b**.

For Building Department the canopy projection over street shall be not more than 7.5m high above ground. However, such proposed canopy would be constructed about 15m above ground projected over street, which is more than the maximum criteria of 7.5m above ground, the proposed canopy could not meet the relevant requirement.

Another sensitivity test has been conducted with the presence of canopies, at the top level of 20.05 mPD, with 0.7m and 0.8m in depth along Ngan Hon Street and Hung Fook Street respectively. The assessment results show that there would not have any significant

improvement of the traffic noise performance as only three more units can be further mitigated.

2.3.8 Indirect Noise Mitigation Measures

- 2.3.8.1 According to the predicted results and relevant hypothetical studies, all practicable direct noise mitigation measures are exhaustively explored, considered, developed and proposed for implementation of the proposed development. Following the HKPSG's recommendations, indirect mitigation measure (acoustic insulation) in terms of provision of air-conditioning system and well gasketed single frame double glazing window is proposed in accordance with the Appendix 4.4 of Chapter 9 of Hong Kong Planning Standard and Guidelines (Suitable Window Types for Noise Insulation) to those flats with noise level in excess of 70 dB(A) as a last resort.
- 2.3.8.2 The single frame double glazing window should have a sound transmission class (STC) of at least 31. Its configuration can be 6mm thick window glazing 9mm thick gap 6mm thick window glazing or other design with similar acoustic performance.
- 2.3.8.3 For balcony door, either glass pane of higher thickness (say, 8mm or 10mm) or the use of single frame double pane sliding door would be considered and adopted subject to considered of practicality and safety in detailed design stage. The noise insulation required for the upgraded door should be at least STC 31.

2.3.9 Conclusion

- 2.3.9.1 A road traffic noise impact assessment including some hypothetic studies have been carried out to evaluate the potential traffic noise impact to the proposed development.
- 2.3.9.2 Based on the assessment results and relevant studies, the package of direct noise mitigation measures including placement and orientation of buildings, shielding from non-noise sensitive structures, use of vertical fin and acoustic balcony is proposed to address the adverse traffic noise impact upon the proposed development.
- 2.3.9.3 With all practical noise mitigation measures considered, exhausted and incorporated for implementation where appropriate, the traffic noise compliance level is 76% complying the relevant HKPSG traffic noise standard for residential use of 70 dB(A). The maximum residual noise level of the proposed development is about 75 dB(A).
- 2.3.9.4 Indirect measures as a last resort in terms of air-conditioning system, single frame double glazing window (or similar acoustic performance window following the recommended acoustic performance in the HKPSG) is recommended for those units with residual noise level.
- 2.3.9.5 With implementation of the practical direct mitigation measures and indirect mitigation measures, the future resident would not be subject to adverse traffic noise impact.

2.4 Fixed Noise Impact Assessment

2.4.1 Introduction

- 2.4.1.1 The aim of this study is to review potential noise impacts arising from the nearby fixed noise sources, including industrial buildings and activities, within 300m on the proposed development. Practicable noise mitigation measures would be recommended, where necessary.

2.4.2 Fixed Noise Sources

- 2.4.2.1 Site visits were carried out on 5th May 2016 and 25th May 16 to study the environment of the proposed development in adjacent. Potential fixed noise sources to the Subject Site mainly include the garages and workshops for car repair, motor services and metal alloys along Ngan Hon Street and Wing Kwong Street. Two industrial buildings are also identified at both ends of the Ngan Hon Street namely the World Wide Industrial Building (Location ID: L1) at the junction of the Ngan Hon Street and To Kwa Wan Road and the Fung Yu Industrial Building (Location ID: L2) behind the Sunrise Villa. Location of two industrial buildings and potential fixed noise sources (Location ID: S1 to S10) is indicated in **Figure 2.3** while the site photos are shown in **Appendix 2.4**.
- 2.4.2.2 According to the site visits, no industrial activities were observed at both World Wide Industrial Building (L1) and Fung Yu Industrial Building (L2) as most are office use only while some units are already vacant. Also, no cooling towers or either chillers were observed at the roof-top of both concerned industrial buildings while only ordinary window type air conditioners were noticed from the exterior wall. Therefore, no adverse fixed noise impact from industrial activities or chiller plant are anticipated from both World Wide Industrial Building and Fung Yu Industrial Building.
- 2.4.2.3 For the potential fixed noise impact arising from the garages and the workshops for car repair, motor services (e.g. car washing and car painting) and metal alloys (S1 to S10), no night-time (23:00 – 07:00) operation were observed during the site visits. It is also confirmed with the workers of the garages and workshops that there is no night-time operation. Therefore, no adverse night-time fixed noise impact arising from the above fixed noise sources is anticipated.
- Potential noise impact from manual and tool/machine assisted operations at source during day-time (07:00 – 23:00) would be highly concerned. However, no operating noisy activity was observed during the site visits, therefore, no on-site noise measurement was conducted to obtain representative noise data. In order to evaluate the fixed noise impact arising from S2 to S7, S9 and S10 to the proposed development, the Sound Power Levels (SWLs) of those noise sources are referenced from the “Noise Impact Assessment for Proposed Comprehensive Residential Development with “Shop and Services” and “Eating Place” in “Comprehensive Development Area (2)” Zone at KIL 10578, 7628 and 7626 in Ma Tau Kok” Report (refer to as NIA-MTK Report) where the study area is near to the Subject Site in same district while the studied noise sources are of similar nature and size. According to the NIA-MTK Report as shown in **Appendix 2.5**, the relevant SWL of similar motor service workshop calculated based on the on-site noise measurement is 91.7 dB.

2.4.3 Noise Sensitive Receivers

2.4.3.1 Representative Noise Sensitive Receivers (NSRs) (NA-M1, NA-J1, NA-G2, NC-A3, NC-D1 and NC-E1) of residential towers nearest to the identified noise sources have been selected for the assessment. **Figure 2.4** shows the locations of the representative NSRs.

2.4.4 Assessment Criteria

2.4.4.1 The Subject Site is located in an urban area and surrounded by To Kwa Wan Road to the northwest, Ngan Hon Street to the northeast, Wing Kong Street to the southeast and Hung Fook Street to the southwest. In accordance with the *Technical Memorandum for the Assessment of Noise from Places other than Domestic Premises, Public Places or Construction Sites* (herein referred as TM), the type of the area where the Subject Site located is considered to be "Type (iii) Urban Area". Moreover, Ma Tau Wai Road (from Chi Kiang Street to Tin Kwong Road) and Chatham Road Flyover (from Ma Tau Kok to Chatham Road North) are in close proximity to the Subject Site, which consist of Annual Average Daily Traffic (AADT) of 36,770 and 77,910 respectively according to "The Annual Traffic Census 2014" published by Transport Department, are considered as influencing factors that can affect the proposed development. As a result, the area sensitive rating (ASR) "B" or "C" is adopted as the assessment criteria depended on whether the selected NSR at such a location that noise generated by the IF is noticeable at the NSR according to the MLP. According to the observation during the site survey, there are different kinds of noise sources identified (such as road traffic noise from the surrounding roads with heavy traffic flows, fixed noise sources, etc.) and the proposed redevelopment project is in an urban area. Therefore, it is anticipated that the prevailing background noise levels should be higher than ANLs. Therefore, ANLs have been adopted as the noise criteria. **Table 2.1** below shows the details of the fixed noise impact assessment criteria for this study.

Table 2.1 Industrial Noise Standard

Standards	NSRs	ASRs	Criteria	Acceptable Noise Level (ANL)*	Noise Criteria in Current Assessment Leq (30mins), dB(A)
NCO	NA-M1, NA-J1, NA-G2, NC-A3, NC-D1	C	Leq (30 mins) (07:00 – 23:00)	70 dB(A)	70 dB(A)
			Leq (30 mins) (23:00 – 07:00)	60 dB(A)	60 dB(A)
	NC-E1	B	Leq (30 mins) (07:00 – 23:00)	65 dB(A)	65 dB(A)
			Leq (30 mins) (23:00 – 07:00)	55 dB(A)	55 dB(A)

Notes:

* Acceptable Noise Level for Area Sensitivity Ratings "B" or "C" stipulated in the TM.

2.4.4.2 Chiller and cooling tower of the central air-conditioning system for commercial and retail podium within the Subject Site are proposed in an enclosed indoor environment. Therefore, any potential fixed noise generated from these equipment would not directly affect the proposed redevelopment project or the surrounding NSRs. The inlet/outlet of the system would be equipped with acoustic louvre to eliminate any potential fixed noise impact from the system to the proposed redevelopment project or the surrounding NSRs.

2.4.5 Assessment Methodology

2.4.5.1 Standard acoustical principles were adopted for prediction of cumulative industrial noise impact. In accordance with the "Technical Memorandum On Noise From Construction Work Other Than Percussive Piling", a – 10dB shielding correction was adopted where the line of sight from the representative NSR would be completely blocked by buildings or barriers. A façade correction of + 3dB was assumed.

2.4.5.2 The fixed noise sources are mainly from the activities of garages, workshops for car repair, car washing and car painting and workshops for metal alloys. Since the activities should be free of the characteristics of tonality, and impulsiveness, no corrections for tonality and impulsiveness were assumed. Moreover, as mentioned in **S.2.4.2.3**, there is no night-time operation from the identified fixed noise sources. Therefore, no correction of intermittency was applied for the calculations.

2.4.6 Assessment Result

2.4.6.1 A summary of predicted fixed noise levels during day time at selected NSRs is tabulated below in **Table 2.2**. Detailed calculations are shown in **Appendix 2.6**.

Table 2.2 Predicted Industrial Noise Level

Representative Noise Sensitive Receiver	Noise Criteria in Current Assessment, Leq (30mins), dB(A)	Predicted Noise Level (Day-time), Leq (30mins), dB(A)	Compliance
NA-M1	70	59	Yes
NA-J1	70	60	Yes
NA-G2	70	61	Yes
NC-A3	70	67	Yes
NC-D1	70	68	Yes
NC-E1	65	62	Yes

2.4.6.2 The assessment results indicate that the maximum predicted noise level for those selected NSRs with ASR of "C" is Leq(30 min) 68 dB(A) for day-time, which complies with the 70 dB(A) day-time criterion; while the predicted noise level for the selected NSR with ASR of "B" is Leq(30 min) 62 dB(A) for day-time, which also complies the criteria of 65 dB(A).

2.4.7 **Conclusion**

- 2.4.7.1 The potential noise impact from fixed noise sources has been assessed based on the proposed development scheme. According to the assessment results, the proposed development will not be subject to adverse noise impact from fixed noise sources.

3. Air Quality Impact Assessment

3.1 Scope of Work

- 3.1.1 The aim of this study is to assess the potential air quality impact due to dust emissions during construction phase and the traffic emission from the surrounding roads within 500m from the Subject Site upon the sensitive receivers of the Proposed Development on the Subject Site. Air pollutants including Nitrogen Dioxide (NO₂), Respirable Suspended Particulate (RSP), as well as Fine Suspended Particulate (FSP) are taken into account in this assessment.
- 3.1.2 The predicted air pollutants concentrations at sensitive receivers will then be compared with the relevant Air Quality Objectives (AQO). Appropriate mitigation measures, if required, will be recommended if there is predicted exceedance in AQOs.

3.2 Assessment Criteria

- 3.2.1 The assessment criteria have been based on the new Hong Kong Air Quality Objectives (AQOs) enacted on 1/1/2014. AQOs are tabulated below for information. They are listed in Table 3.1 below.

Table 3.1 Hong Kong Air Quality Objectives

Pollutants	Average Time	Standard [i] (µg/m ³)	No. of exceedances allowed
Sulphur Dioxide (SO ₂)	10-min	500	3
	24-Hour	125	3
Respirable Suspended Particulates (PM ₁₀) [ii]	24-Hour	100	9
	Annual	50	NA
Fine Suspended Particulates (PM _{2.5}) [iii]	24-Hour	75	9
	Annual	35	NA
Nitrogen Dioxide (NO ₂)	1-Hour	200	18
	Annual	40	NA
Ozone (O ₃)	8-Hour	160	9
Carbon Monoxide (CO)	1-Hour	30,000	0
	8-Hour	10,000	0
Lead (Pb)	Annual	0.5	NA

Notes:

- [i] All measurements of the concentration of gaseous air pollutants, i.e. sulphur dioxide, nitrogen dioxide, ozone and carbon monoxide, are to be adjusted to a reference temperature of 293 Kelvin and a reference pressure of 101.325 kilopascal.
- [ii] Respirable suspended particulates means suspended particles in air with a nominal aerodynamic diameter of 10 µm or less.
- [iii] Fine suspended particulates means suspended particles in air with a nominal aerodynamic diameter of 2.5 µm or less.

- 3.2.2 In addition, EPD recommends a maximum level of hourly TSP of 500µgm⁻³ at the boundary of any construction site.

3.3 Construction Phase

Identification of Sources

- 3.3.1 Fugitive dust will be the only potential major source of air quality impact during the construction phase. Unacceptable impacts from the criteria pollutants - NO_x, SO₂, and CO, etc. are unlikely as significant emissions are not anticipated, as number of diesel or petroleum fuelled machinery operating in the site shall be limited as compared to the traffic volume in nearby roads.

Air Sensitive Receivers

- 3.3.2 There are a number of industrial buildings, residential buildings, schools, church, park and playgrounds located within 500m to the Sites, which residential buildings, schools and playgrounds are considered as ASRs. The ASRs are identified as Residential Buildings along both eastern and western sides of Chatham Road North, Ma Tau Wai Road, To Kwa Wan Road and Kowloon City Road; S.K.H. St. Timothy's Primary School, Po Leung Kuk Ngan Po Ling College, S.K.H. Holy Carpenter Primary School, PLK Madam Chan Wai Chow Memorial School, ELCHK Hung Hom Lutheran Primary School, Po Leung Kuk Lam Man Chan English Primary School, S.K.H. Good Shephard Primary School, Oblate Fathers Primary School, Holy Angels Canossian School and St. Timothy's School; the Church of Christ in China Hung Hom Kei To Church; Hoi Sham Park and Ko Shan Road Park; To Kwa Wan Playground, To Kwa Wan Complex Playground and East Ho Man Tin Service Reservoir Playground.

Mitigation Measures for Dust Emission

- 3.3.3 Fugitive dust emission arising from the construction can be effectively suppressed by incorporating proper mitigation measures into work procedures through contractual clauses (e.g. regular and sufficient watering, enclosure of potential dust sources, etc.), good site management, and close enforcement of the resident engineers. The Contractor(s) shall be required to follow the requirements of the Air Pollution Control (Construction Dust) Regulation which requires notification before carrying out construction works and to adopt dust reduction measures while carrying out construction activities. A monitoring programme can be instigated to monitor the construction process in order to enforce dust controls and modify methods of works to reduce the dust emission down to acceptable levels.
- 3.3.4 Also, emission control during construction phase shall be carried out in accordance to the requirements of the Air Pollution Control (Non-road Mobile Machinery) (Emission) Regulation. The emissions of non-road mobile machinery (NRMMS) include mobile machines and vehicles powered by internal combustion engines used primarily off-road. All NRMMS operating on-site which are subject to emission control of the Air Pollution Control (Non-road Mobile Machinery) (Emission) Regulation shall be approved/exempted (as the case may be) and affixed with the requisite approval/exemption labels.
- 3.3.5 The recommended dust mitigation measures for protection of nearby ASRs are described below:

Good Site Management

Good site management is important to help for reducing potential air quality impact down to an acceptable level. As a general guidance, the Contractor shall maintain high standard of housekeeping to prevent emission of fugitive dust emission. Loading, unloading, handling and storage of fuel, demolished debris and wastes or by-products should be carried out in a manner so as to minimise the release of visible dust emission.

Appropriate working methods should be devised and arranged to minimize dust emissions and to ensure any installed control system and/or measures are operated and/or implemented in accordance with their design merits. No free falling of debris should be allowed, which should be let down by hoist or enclosed tunnel to the ground.

A high standard of housekeeping shall be maintained. Any piles of materials accumulated on or around the work areas shall be cleaned up regularly. Cleaning, repair and maintenance of all plant facilities within the work areas shall be carried out in a manner without generating fugitive dust emissions. The materials shall be handled properly to prevent fugitive dust emission before cleaning.

Frequent mist spraying should be applied on dusty areas. The frequency of spraying will depend upon local conditions such as rainfall, temperature, wind speed and humidity. The amount of mist spraying should be just enough to dampen the materials without over-watering, which could result in surface water runoff.

Dust Emissions from Site Traffic

Dust emission from construction traffic comes predominantly from the travelling of waste removal lorries. Areas within the site where there are regular vehicle movements should have a hard surface. Speed controls at an upper limit of 10km/hr should be imposed and their movements should be confined to designated roadways within the site. All dusty vehicle loads should have side and tail boards covered by tarpaulin extending at least 300mm over the edges of the side and tail boards. Wheel-wash troughs and hoses should be provided at exit points of the site.

Due to the small site area, the number of diesel-driven construction plant and equipment operating simultaneously on-site will be limited and therefore, the potential gaseous emissions from these plant and equipment are expected to be minimal and unlikely to cause any adverse air quality impacts.

“Recommended Pollution Control Clauses for Construction Contracts” is available on EPD website. It contains the recommended air pollution control measures to be implemented by the Contractor(s) during the construction stage of the Project.

With the adoption of good practices, it is expected that emission of construction fugitive dust can be kept at an acceptable level.

3.4 Operation Phase

Air Sensitive Receivers

- 3.4.1 Representative Air Sensitive Receivers (ASRs) (16 ASRs) as shown in **Figure 3.1** were selected at the representative location from the proposed conversion. Assessment heights for the ASRs scattered around the proposed conversion are taken from 1/F to 23/F of residential uses including fresh air intake at 5.05 mPD, 11.05 mPD, 15.55 mPD and 20.05 mPD of which the base elevation of the proposed conversion is taken as 5.05 mPD.

Identification of Sources

Chimney Emissions

- 3.4.2 On-site visit has been conducted in May 2016 to identify any active chimneys located within 500m from the Subject Site. Based on the survey, no active chimneys were found in the assessment area.

Vehicular Emissions from Open Roads

- 3.4.3 Air pollutant concentration at the Subject Site due to the emission from the nearby road networks is assessed. The emission rate of each road within 500m from the Subject Site is calculated with latest EMFAC-HK issued by EPD. Based on the emission rate of each road, the air pollutant concentration at the Subject Site will be modelled with CALINE 4.

Methodology

- 3.4.4 The aim of conducting EMFAC Model is to calculate project-specific vehicle emission factor of criteria air pollutants (e.g. NO₂ and RSP) arising from vehicular tailpipe emission on the road carriageways within 500m study area of the Subject Site.

EMFAC-HK Model

- 3.4.5 The EMFAC-HK Model version 3.1.1 dated 17 Feb 2016 (the EMFAC-HK Model), which was the latest available version, was adopted.

Guideline and Document

- 3.4.6 Several guidelines and documents published by the EPD, which are available from the following EPD EMFAC-HK website (the EPD website), are referred for EMFAC-HK Model input:
- http://www.epd.gov.hk/epd/english/environmentinhk/air/guide_ref/emfac-hk.html (the EPD website)
 - Guideline on Modelling Vehicle Emissions (Revised on 4.1.2016) (the EPD Guideline)
 - 2013 Licensed Vehicle by Age and Technology Group Fractions (the EPD Document)

Traffic Data

- 3.4.7 Project specific hourly traffic data for 2041 having the maximum traffic data of the concerned roadway network within 15 years from the assumed completion year of the developments is

adopted in combination with emission factor based on EMFAC-HK Model (with calendar year set as 2026) to estimate the highest vehicular emission rate for each roadway. The traffic data predicted by the project traffic consultant, CKM Asia Limited, includes hourly traffic flow with composition of 16 vehicle classes according to “Guideline on Modelling Vehicle Emissions” released by EPD for road carriageways within 500m radius from the boundary of the proposed developments; hourly VKT and number of trips travelled; and speed fraction for road carriageways respectively with speed limit of 50kph and 70kph for 24 hours. A listing of the EMFAC-HK vehicle classes is provided in **Table 3.2** below. Traffic forecast is included in **Appendix 3.1**.

Table 3.2 EMFAC-HK Vehicle Classes

Vehicle Class Description	Fuel Type	Gross Vehicle Weight (tonnes)	Symbol 1 (in csv output file)	Symbol 2 (in bcd output file & traffic data)
Private Cars (PC)	ALL	ALL	PC	PC
Taxi	ALL	ALL	Taxi	Taxi
Light Goods Vehicles (<=2.5t)	ALL	<=2.5t	LGV<=2.5t	LGV3
Light Goods Vehicles (2.5-3.5t)	ALL	>2.5-3.5t	LGV2.5-3.5t	LGV4
Light Goods Vehicles (3.5-5.5t)	ALL	>3.5-5.5t	LGV>3.5t	LGV6
Medium & Heavy Goods Vehicles (5.5-15t)	ALL	>5.5-15t	HGV<=15t	HGV7
Medium & Heavy Goods Vehicles (>=15t)	ALL	>15t	HGV>15t	HGV8
Public Light Buses	ALL	ALL	PLB	PLB
Private Light Buses (<=3.5t)	ALL	<=3.5t	PrLB<=3.5t	PV4
Private Light Buses (>3.5t)	ALL	>3.5t	PrLB>3.5t	PV5
Non-franchised Buses (<6.4t)	ALL	<=6.36t	NFB<=6.4t	NFB6
Non-franchised Buses (6.4-15t)	ALL	>6.36-15t	NFB6.4-15t	NFB7
Non-franchised Buses (>15t)	ALL	>15t	NFB>15t	NFB8
Single Deck Franchised Buses	ALL	ALL	FBSD	FBSD
Double Deck Franchised Buses	ALL	ALL	FBDD	FBDD
Motor Cycles	ALL	ALL	MC	MC

3.4.8 The estimated 16-class distribution as defined in EMFAC-HK was derived by sectoring the relevant classes in the Transport Department’s Annual Traffic Census record or vehicle distribution obtained from manual traffic count surveys, in proportion to the recorded distribution in EPD document: “2013 Vehicle Licensed Number by Age and Technology Group Fractions” from the website of EPD.

3.4.9 All concerned roadways shall be characterized into different road groups in accordance with speed limits. It is assumed that speed fraction is applicable for all roads under the same

concerned road groups (i.e. with the same speed limit). AM and PM hourly speed fractions for each vehicle class are prepared for each road group.

The roadway network within the 500m study area consists of 196 distinct roadway links. In this study, all concerned roads are characterized into 3 road groups for EMFAC modelling, i.e. Local Distributor (LD), District Distributor (DD) and Primary Distributor (PD) (all with 50 and 70 kilometer per hour, kph).

- 3.4.10 Endorsement from Transport Department on the traffic data forecast (including traffic flow & vehicle mix in 24 hours, speed limit) and the assumption that speed fraction is applicable for all roads under same road group will be obtained by the Project Traffic Consultant. Relevant correspondence of the endorsement will be provided separately.

EMFAC-HK Input

Geographical Area

- 3.4.11 "Hong Kong" is selected as the Geographical Area.

Calendar Year

- 3.4.12 2026 is chosen as the Calendar Year in EMFAC-HK Model to represent the worst case scenario emissions (because the vehicle fleet will become cleaner over time as the fleet incorporates newer vehicles adhering to more stringent emission standards).

Season or Month

- 3.4.13 Per the EPD Guideline, "Annual" is selected in this study to evaluate the highest vehicle emission within the Model Year.

Mode and Output

- 3.4.14 EMFAC-HK Model is run in Emfac mode for calculating area fleet average emissions.

Temperature and Humidity

- 3.4.15 The information from the WRF data in the PATH model of the grid (42, 33) where the proposed development located is used in this assessment. The hourly average values which are arithmetic mean of the same hourly interval over the entire year were selected to obtain the annual hourly average of temperature and relative humidity to generate the representative emission factor from EMFAC-HK.

- 3.4.16 Referring to the said grid, the hourly average temperature ranges from 23°C to 27°C; hourly average relative humidity (RH) ranges from 60% to 80% (see **Table 1 of Appendix 3.2**). For output configuration, temperature is set from 20°C to 30°C with increment of 5°C. RH is set from 60% to 80% with increment of 10%.

Speeds

- 3.4.17 The speed fraction data provided by project traffic consultant is from 0kph to 112kph with an interval of 8kph as one sub-class. To match with the speed fraction data for subsequent calculation, speed is configured from 4kph and in increment of 8kph (i.e. mid-value of each speed fraction sub-class).

Exhaust / Evaporation Technology Fractions

- 3.4.18 Vehicle classes are grouped with different exhaust and evaporation technology group indexes and technology fractions. Each technology group represents a distinct emission control technologies. Default exhaust and evaporation technology fractions are adopted in this assessment.

Population and Accrual Rate

- 3.4.19 Default vehicle populations forecast and accrual rate in EMFAC-HK Model is adopted.

Trips and VKT

- 3.4.20 Default trips and VKT for HK total is adopted.
- 3.4.21 Detailed impact rates will be generated with respect to each combination of temperature, RH and speed for running exhaust emission, and combination of temperature and duration for cold start emission.

Calculation of Emission Factors by EMFAC-HK Model Output

Running Exhaust Emission Rate

- 3.4.22 To represent the worst case scenario, maximum running exhaust emission rate (g/km) among all combinations of temperature (20°C to 30°C) and relative humidity (60% to 80%) with respect to each combination of speed and vehicle class will be adopted for NO_x, RSP and FSP.
- 3.4.23 For each road group (i.e. roads with same speed limit), hourly running exhaust emission rate (NO_x/RSP/FSP) for each vehicle class is determined by:

Hourly running exhaust emission rate (NO_x/RSP/FSP) for each vehicle class (g/veh-km) = \sum [running exhaust emission rate for a particular speed x speed fraction of particular speed]

For each road, hourly composite running exhaust emission rate (NO_x/RSP/FSP) is determined by:

Hourly composite running exhaust emission rate (NO_x/RSP/FSP) (g/veh-km) = \sum [hourly running exhaust emission rate for each vehicle class (determined for the corresponding road group) x % composition of corresponding vehicle class]

Starting Emission Rate

3.4.24 For cold start emission which is applicable to non-trunk road only, maximum starting emission (g/trip) among different durations (from 5min to 720min) is adopted. It is notable that only 8 out of 16 vehicle classes would have starting emission.

3.4.25 Reference is made to "Agreement No. CE 45/2008 (CE) Liantang / Heung Yuen Wai Boundary Control Point and Associated Works" (EIA-190/2010) (the Liantang EIAR). In the EIA Report, correlation is established between number of trips and VMT/VKT. The estimated VMT/VKT for rural and local roads with possible cold start emission amounts to 13% of total VMT/VKT.

3.4.26 In this assessment, the assumption in Liantang EIAR is followed. EMFAC Model is used to generate HK total number of trips and VKT travelled in Year 2026 (the assessment year of the traffic forecast) for each vehicle class. Trip per VKT for rural and local road for each class is determined by:

Trip/VKT (1/veh-km) for each vehicle class = HK total number of trips for each vehicle class ÷ (HK total VKT travelled for each vehicle class × 13%).

3.4.27 Based on the hourly VKT travelled data from project traffic consultant, hourly total cold start emission (gram) for each vehicle class along each road is determined by:

Hourly total cold start emission (g) for each vehicle class = starting emission rate for each vehicle class × Trip/VKT for each vehicle class × VKT travelled for each vehicle class along each road

3.4.28 The hourly total cold start emission rate for each road is the sum of hourly total cold start emission (gram) for each vehicle class along the same road. The hourly composite cold start emission rate is calculated by dividing the value using the hourly traffic flow:

Hourly total cold start emission (g) = ∑ [hourly total cold start emission for each vehicle class]

Hourly composite cold start emission rate (g/veh-km) = hourly total cold start emission ÷ hourly total VKT travelled

Meteorological Data

3.4.29 The level of emission depends upon the way in which the nature and location of the emission sources interacts with a number of key hourly meteorological elements such as wind speed and direction, temperature, rainfall, Pasquill stability classes, and cloud cover. The information from the WRF data used in the PATH model of the grid (42, 33) where the proposed development located is used in the assessment. The stability class is also converted from the WRF Data by PCRAMMET.

3.5 Assessment Results

3.5.1 Air quality impact due to vehicular emission from the surrounding road networks has been quantitatively assessed separately for NO₂, RSP and FSP.

- 3.5.2 Predicted results of NO₂, RSP and FSP at all predetermined ASRs are summarized in **Appendix 3.3** and contour plots are shown from **Appendix 3.4a** to **Appendix 3.4f**.

NO₂

- 3.5.3 The predicted results of the 19th highest 1-hour average and annual average NO₂ concentration due to vehicular emission for all selected ASRs at all levels are summarized in **Appendix 3.3**. The results indicated that NO₂ concentration at the residential portion of the proposed development, i.e. from 27.55 mPD to 100mPD, can comply with the relevant AQOs. For the retails, i.e. from G/F to 2/F, the assessment results show that the predicted annual NO₂ concentration on G/F and 1/F of the western portion of the proposed development and on G/F of the eastern portion of the proposed development would exceed the relevant AQO. The predicted hourly NO₂ concentration on all retail floors can comply with the relevant AQO.

RSP

- 3.5.4 The predicted results of the 10th highest 24-hours average and annual average RSP concentration due to vehicular emission for all selected ASRs at all levels are summarized in **Appendix 3.3**. The results indicated that RSP concentration at the proposed development can comply with the relevant AQOs.

FSP

- 3.5.5 The predicted results of the 10th highest 24-hours average and annual average FSP concentration due to vehicular emission for all selected ASRs at all levels are summarized in **Appendix 3.3**. The results indicated that FSP concentration at the proposed development can comply with the relevant AQOs.

3.6 Summary

- 3.6.1 The concentrations of NO₂ (hourly average), RSP (daily average and annual average) and FSP (daily average and annual average) at all ASRs of the proposed development are expected to be within the relevant AQOs.
- 3.6.2 However, concentrations of NO₂ (annual average) at ASRs (A01-A03) of 5.05 mPD and 11.05 mPD and at ASRs (A04-A08 and B01) of 5.05 mPD exceeded the relevant AQOs. The ASRs with exceedance of the annual NO₂ concentration are located at the podium where is used as retail. The section of the proposed development shows that location of the retails starting from 5.05 mPD to 20.5 mPD. It is common that central air conditioning system would be provided for the retail, such that openable window would not be required for this air sensitive use for ventilation purpose. With careful location of the fresh air intake at the location where air pollutant concentrations comply with the AQOs, for example at the podium roof with 20.5 mPD, the employees of the proposed development will not be subject to unacceptable air quality impact.

4. Waste Management

4.1 Construction Phase Impact

- 4.1.1 The volume of construction waste is expected to be insignificant as the materials on site will be reused and recycled as far as engineering practicable.
- 4.1.2 The waste produced, if any, will largely consist of excavated and demolished construction materials during site formation stage. Other waste may include chemical waste and general refuse. The volume of waste to be generated will be quantified based on the proposed construction programme during the detailed design stage.

4.2 Operation Phase Impact

- 4.2.1 The volume of construction waste is expected to be insignificant as the materials on site will be reused and recycled as far as engineering practicable.
- 4.2.2 The proposed residential use of the Project would generate a small amount of municipal waste during its operation. Also, general refuse from daily operation from non-domestic area (including clubhouse) are mainly generated from office activities and pantries such as waste paper, food scraps, etc. Thus, the waste impact generated by the proposed development is minimal.
- 4.2.3 During the operational phase of the proposed development, waste generated will be properly controlled through the implementation of refuse collection points and measures to minimise the generation of waste and to reuse or recycle, if applicable. Therefore, no adverse waste impact is anticipated.

5. Land Contamination Review

5.1 Scope of Work

- 5.1.1 The aim of this study is to assess the potential land contamination impact at the Subject Site due to previous land uses and/or the existing operations.

5.2 Assessment Criteria

- 5.2.1 The following guidelines published by EPD have been followed:

- Guidance Manual for Use of Risk-Based Remediation Goals for Contaminated Land Management (RBRGs), dated December 2007;
- Guidance Note for Contaminated Land Assessment and Remediation, dated 15 August 2007; and
- Practice Guide for Investigation and Remediation of Contaminated Land (EPD's Practice Guide), dated August 2011.

- 5.2.2 As the RBRGs and the Practice Guide are the latest guidelines promulgated for use in August 2007 and August 2011 respectively, the RBRGs criteria and the requirements stated in the Practice Guide will be adopted in this Land Contamination Review.

5.3 Review of Historical and Current Available Information

Landuses

- 5.3.1 According to the Hong Kong Geological Survey Map (Series: HGM20) – Sheet No. 11 (1:20,000 scale) on the solid and superficial geology in To Kwa Wan district, the Subject Site was reclaimed in 1924. The Subject Site is comprised mainly of marine sand. The Geological Survey Map for the Subject Site is captured and presented in **Appendix 5.1**.
- 5.3.2 The Subject Site is currently located in an area consisting of low to mid-rise residential buildings. According to the aerial photograph taken in 1945 by Lands Department, the Subject Site is situated in an urban built area with buildings.
- 5.3.3 In the aerial photograph taken in 1967 by lands Department, buildings were already visible within the Subject Site, and such buildings have remained unchanged till present day.
- 5.3.4 **Table 5.3.1** below presents the landuse summary of the Subject Site. The aerial photos of Year 1945, 1967, 1995, and 2016 are attached in **Appendix 5.2**.

Table 5.3.1 Landuse Summary of the Subject Site

Period / Year	Landuse / Description	Owner or occupier	Source(s) of Information	Off-site Property Affected?
Before 1945	Reclamation work was launched in To Kwa Wan in the 1910s to 1920s. The reclamation works for the Subject Site was completed in 1924.	No information available.	Hong Kong Geological Survey.	No.
1945	The Subject Site is situated in an urban built area.	No information available.	Aerial Photograph from Lands Department.	No.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.	No information available.	Aerial Photograph from Lands Department.	No.

Site Inspection and Observation

- 5.3.5 Site visits conducted in April and May 2016. From the site visit, observations included car repairing workshops, hardware stores, and small retail stores, etc. There were no aboveground / underground oil storage tanks, chemicals and dangerous goods observed to be stored on site during the site visit. Photo records and location of where the photos were taken are presented in **Figure 5**.

5.3.6 Apart from the information reviewed, the following HKSAR Government Departments have been enquired on the latest update on the availability of landuse status and records of land contamination and/or spillage for the Subject Site. The summary of correspondence is presented in **Table 5.3.2** below. Copy of the letters replied from various Government Departments are included in **Appendix 5.3** for reference.

Table 5.3.2 Enquiries and Responses on Land Contamination Related Records

Consultant's Letter Ref.	Department	Response Letter Ref.	Response Date	Summary
URAKC010EI00_0_0002L.15	Buildings Department	(2) in BD 10/4022/16	1 st Aug 2016	It is advised that records for completed private buildings may be inspected at the Building Information Centre of the BD or via the "Building Records Access and Viewing On-line" system upon application and prescribed fee.
URAKC010EI00_0_0003L.15	Geotechnical Engineering Office, Civil Engineering and Development Department	(email)	1 st Aug 2016	It is recommended to obtain geotechnical information from the Geotechnical Information Unit (GIU). The Works Bureau Technical Circular No. 20/2001 was attached for reference.
URAKC010EI00_0_0004L.15	Environmental Protection Department	-	-	No formal reply received upon the preparation of this EA Report.
URAKC010EI00_0_0005L.15	Fire Services Department	(184) in FSD GR 6-5/4 R Pt. 12	3 rd Aug 2016	It is advised that that the enquiry is under review and only Dangerous Good Licence Record from the year of 1990 to present moment and Incident Record from the past three years of fire and special services incidents will be provided.
URAKC010EI00_0_0006L.15	Explosive Ordinance Disposal Bureau, Hong Kong Police	(149) in CP OPS EOD 6-20/1 II	3 rd Aug 2016	It is advised that no records of their attending incidents were found at the mentioned locations.
URAKC010EI00_0_0007L.15	Lands Department	-	-	No formal reply received upon the preparation of this EA Report.
URAKC010EI00_0_0008L.15	Planning Department	() in K-R/URA/15	3 rd Aug 2016	It is advised that no information regarding the past uses/activities of the site was found. It is recommended to consult Lands Department or Buildings Department.

Discussion

- 5.3.7 A land contamination assessment has been conducted for the Subject Site. Background information including geosocial information obtained from Hong Kong Geological Survey and selected aerial photos from Land Department has been reviewed and site survey has been conducted to identify the potential land contamination in the Subject Site.
- 5.3.8 It is observed that the Subject Site is situated on reclaimed land from the 1920s. The Subject Site was then developed into an urban area and the current buildings have remained since its erection from 1957 to 1960. However, it is unable to identify the building usage and there may be a change in land use over time.
- 5.3.9 Based on the observations during site visit, the ground floor of the Subject Site is currently comprised of several car repairing workshops, hardware stores, and small retail stores, while the upper floors are of residential use. Since car repairing workshops may release oils and fuels and lubricant from vehicles during vehicle and equipment maintenance and refueling, potential land contamination issues are present at the Subject Site.
- 5.3.10 Further site appraisal and soil sampling may be required to determine whether the site is contaminated, and if so, the extent of the potential contamination. Nevertheless, the consequent Contamination Assessment Plan (CAP) will be prepared to cover the whole site to be developed.
- 5.3.11 A Contamination Assessment Plan (CAP) and subsequently, Contamination Assessment Report (CAR), and Remediation Action Plan (RAP) will be prepared in later stages to identify the potential land contamination issues in the Subject Site.

6. Overall Conclusion

- 6.1.1 The Subject Site is proposed to be redeveloped into a 759-unit residential complex. The potential air quality, noise impact, and land contamination issues have been qualitatively addressed.
- 6.1.2 Based on the noise impact assessment, it is concluded that with noise mitigation measures such as disposition and orientation of the building towers and vertical fins provided, the proposed redevelopment will not be subject to adverse traffic noise impact.
- 6.1.3 Based on the air quality impact assessment, it is concluded that the concentrations of NO₂ (hourly average), RSP (daily average and annual average) and FSP (daily average and annual average) at all ASRs of the proposed development are expected to be within the relevant AQOs. Therefore, no adverse air quality impact is anticipated.
- 6.1.4 The waste management implications associated with construction phase and operation phase have been addressed. Provided that the recommendations are implemented, no waste related impacts are expected to arise from the proposed development.
- 6.1.5 Based on the land contamination review on historical landuses from aerial photographs, there are potential land contamination issues associated with the car repairing workshops on the ground floor of the Subject Site. Contamination Assessment Plan and subsequent reports will be prepared in later stages.
- 6.1.6 The environmental assessment confirms the feasibility of the proposed redevelopment.

FIGURES

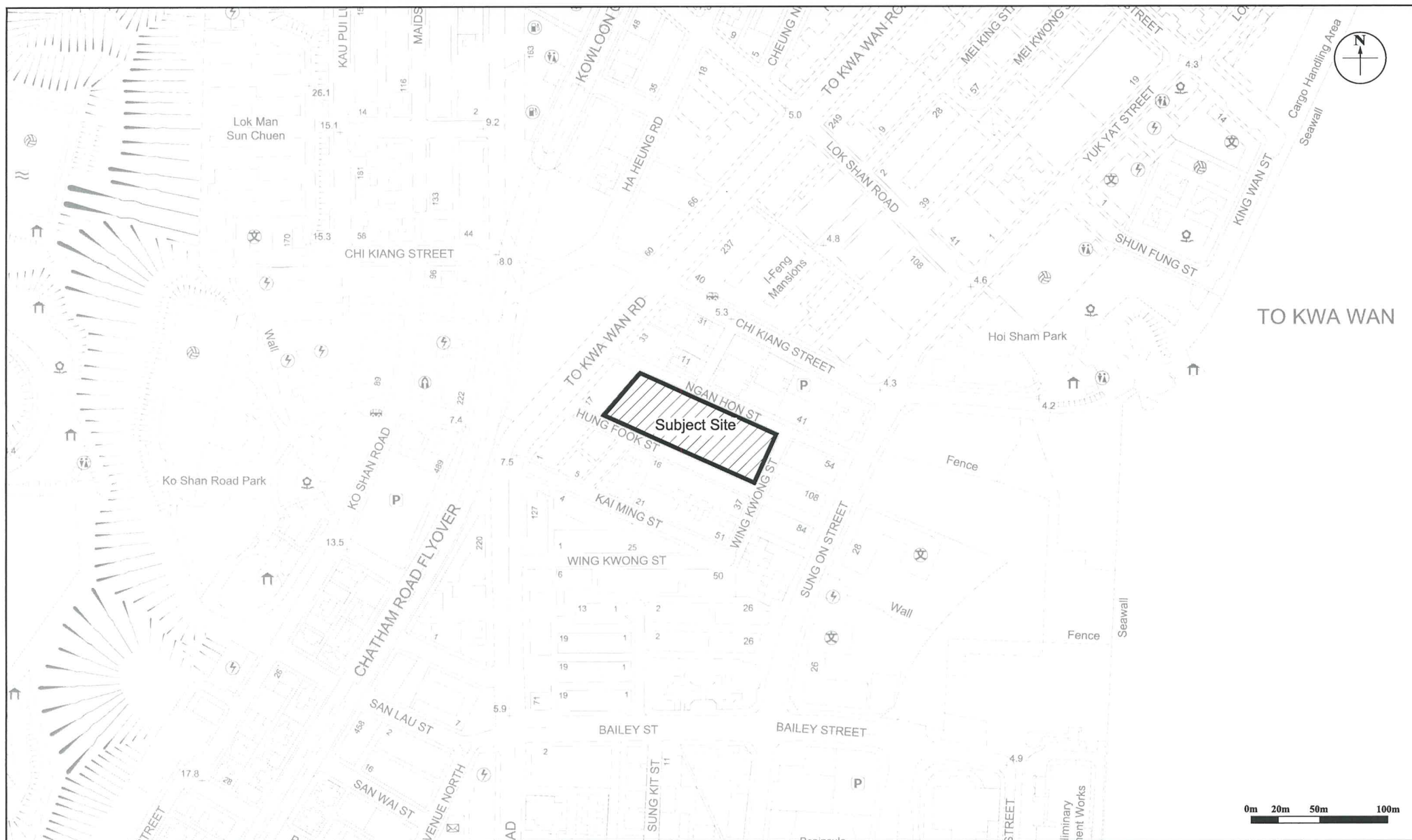


Figure: 1

Title: Subject Site and its Environ

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

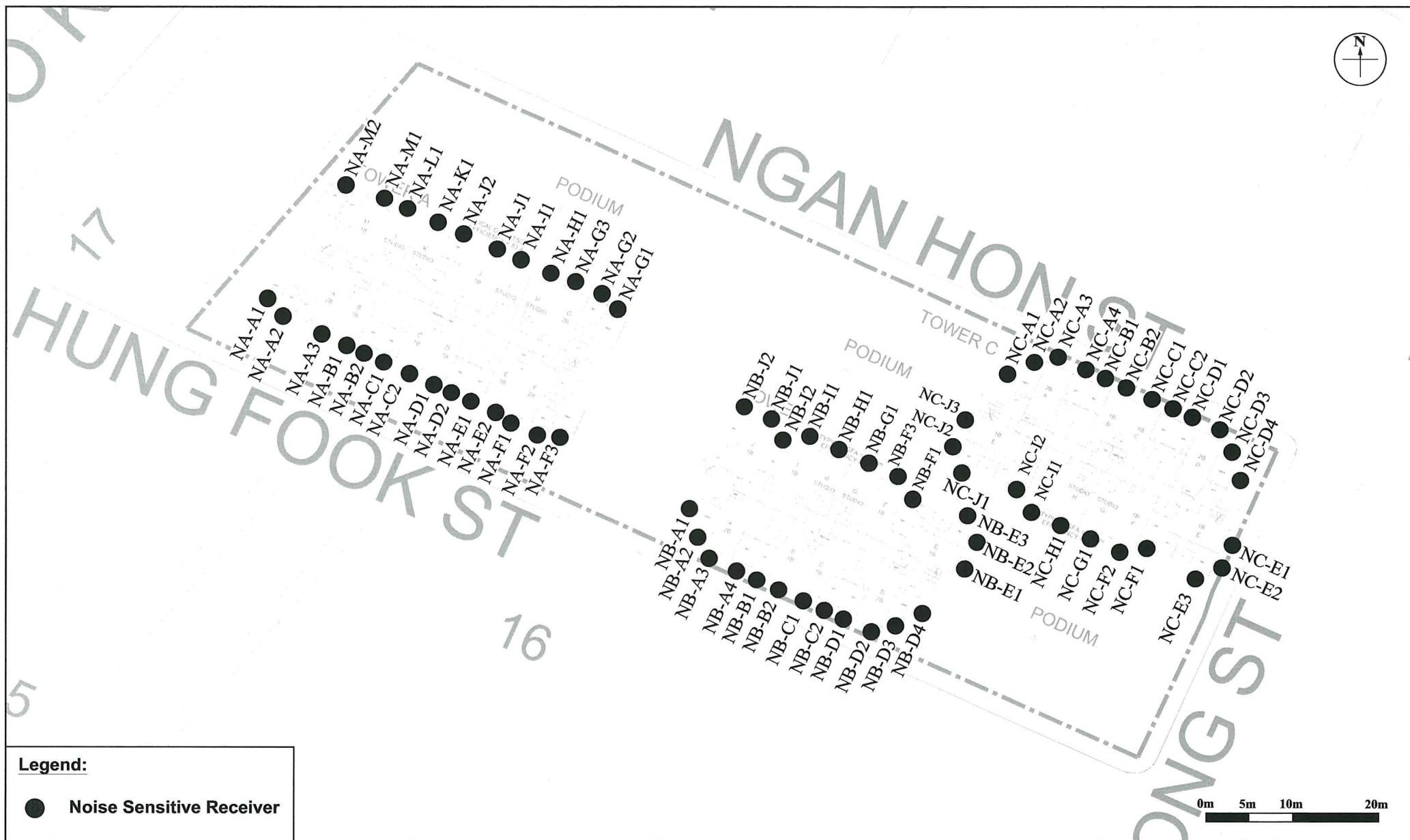
RAMBOLL ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.1

Date: Aug 2016



Legend:

● Noise Sensitive Receiver

Figure: 2.1

Title: Location of Representative Noise Sensitive Receivers for Road Traffic Noise Impact Assessment

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.0

Date: Jul 2016

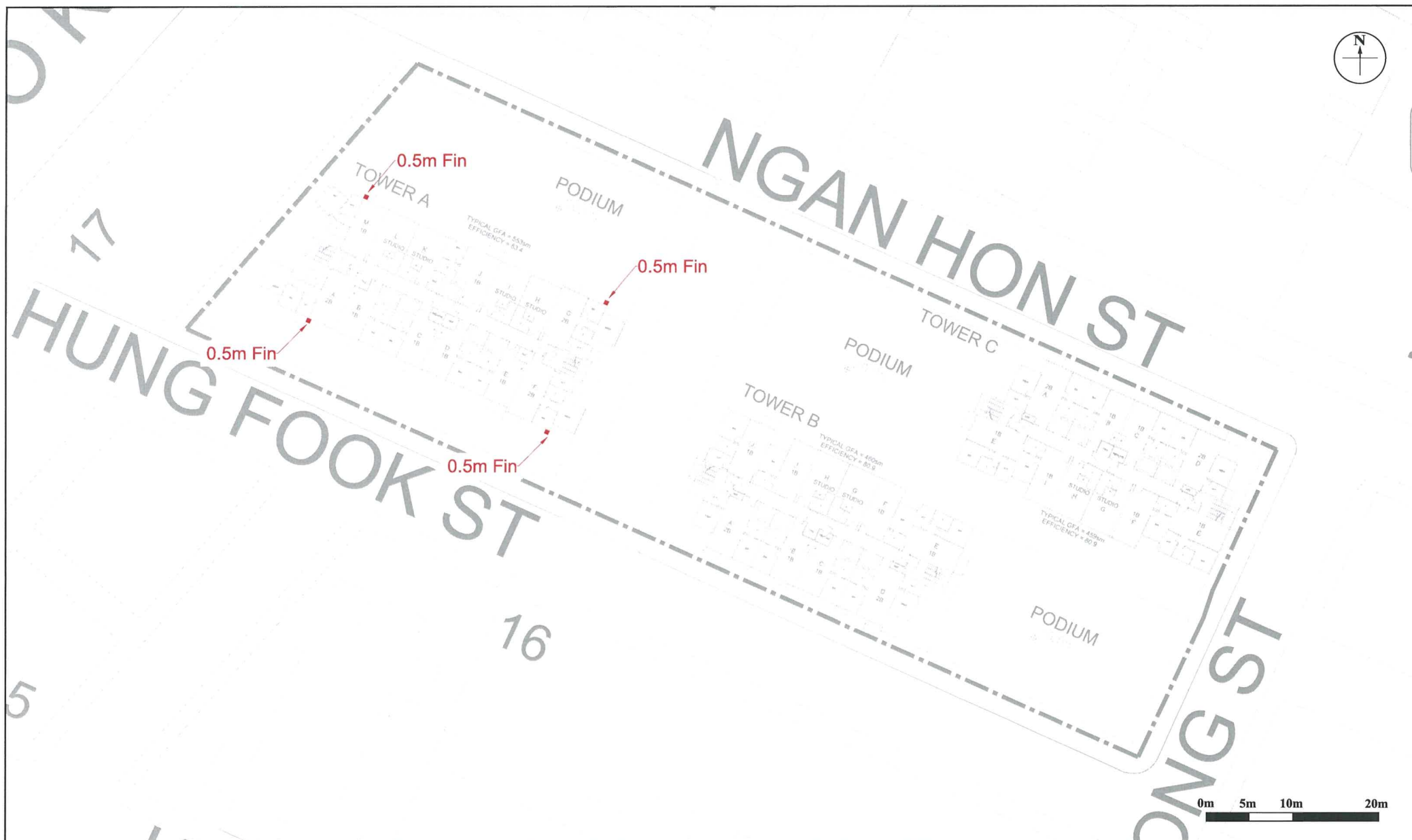


Figure: 2.2

Title: Recommended Road Traffic Noise Mitigation Measures

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.0

Date: Jul 2016

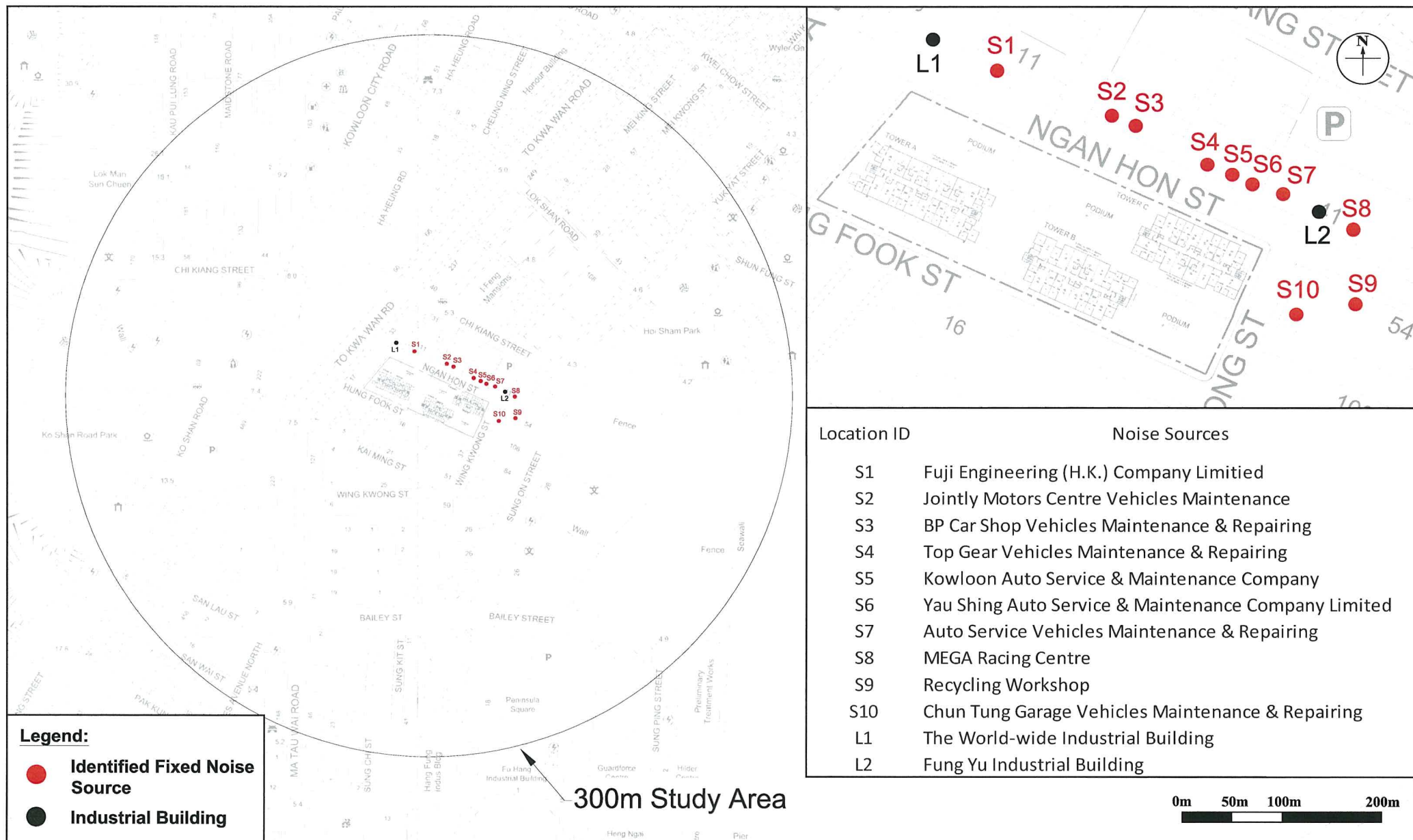


Figure: 2.3

Title: Location of Fixed Noise Sources Identified Within 300m of Subject Site

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.2

Date: Jul 2016

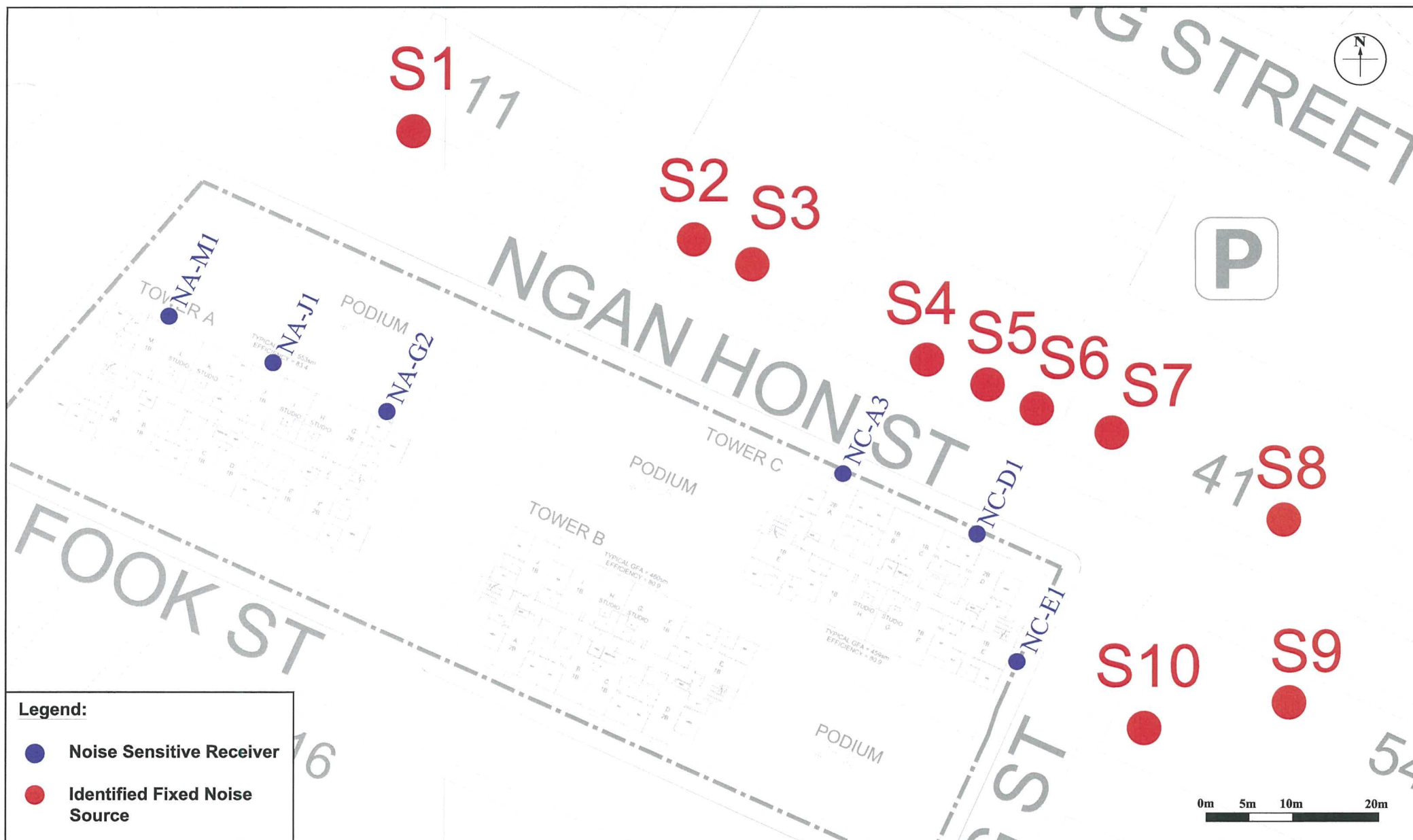


Figure: 2.4

Title: Location of Representative Noise Sensitive Receivers for Fixed Noise Impact Assessment

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

Drawn by: JC

Checked by: TC

Rev.: 1.1

Date: Jul 2016

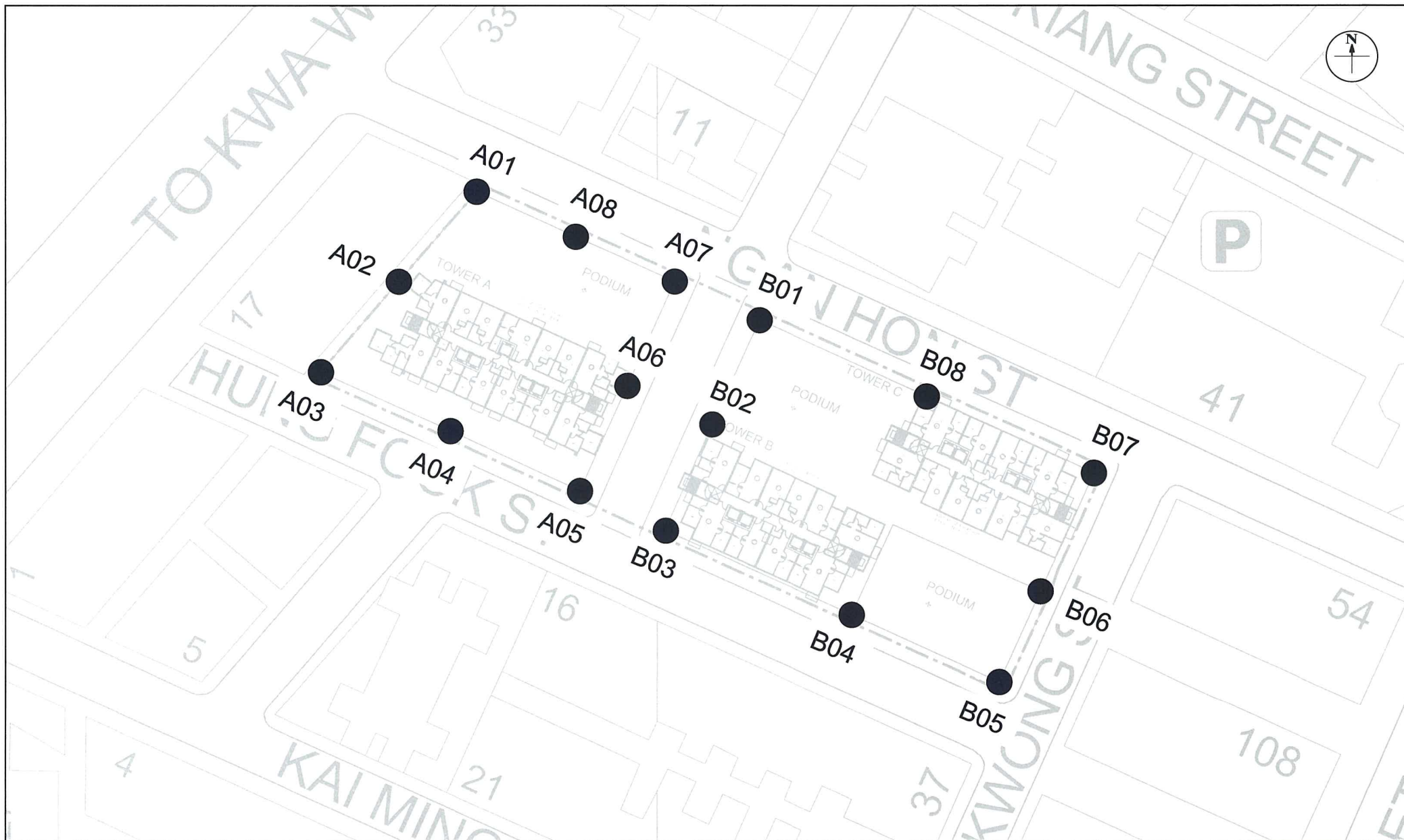


Figure: 3.1

Title: Location of Representative ASRs

Project: Term Environmental Consultancy Services For a Project at Hung Fook Street/ Ngan Hon Street, Kowloon City District (KC-010)

RAMBOLL ENVIRON

Drawn by: RC

Checked by: TC

Rev.: 1.0

Date: Jul 2016

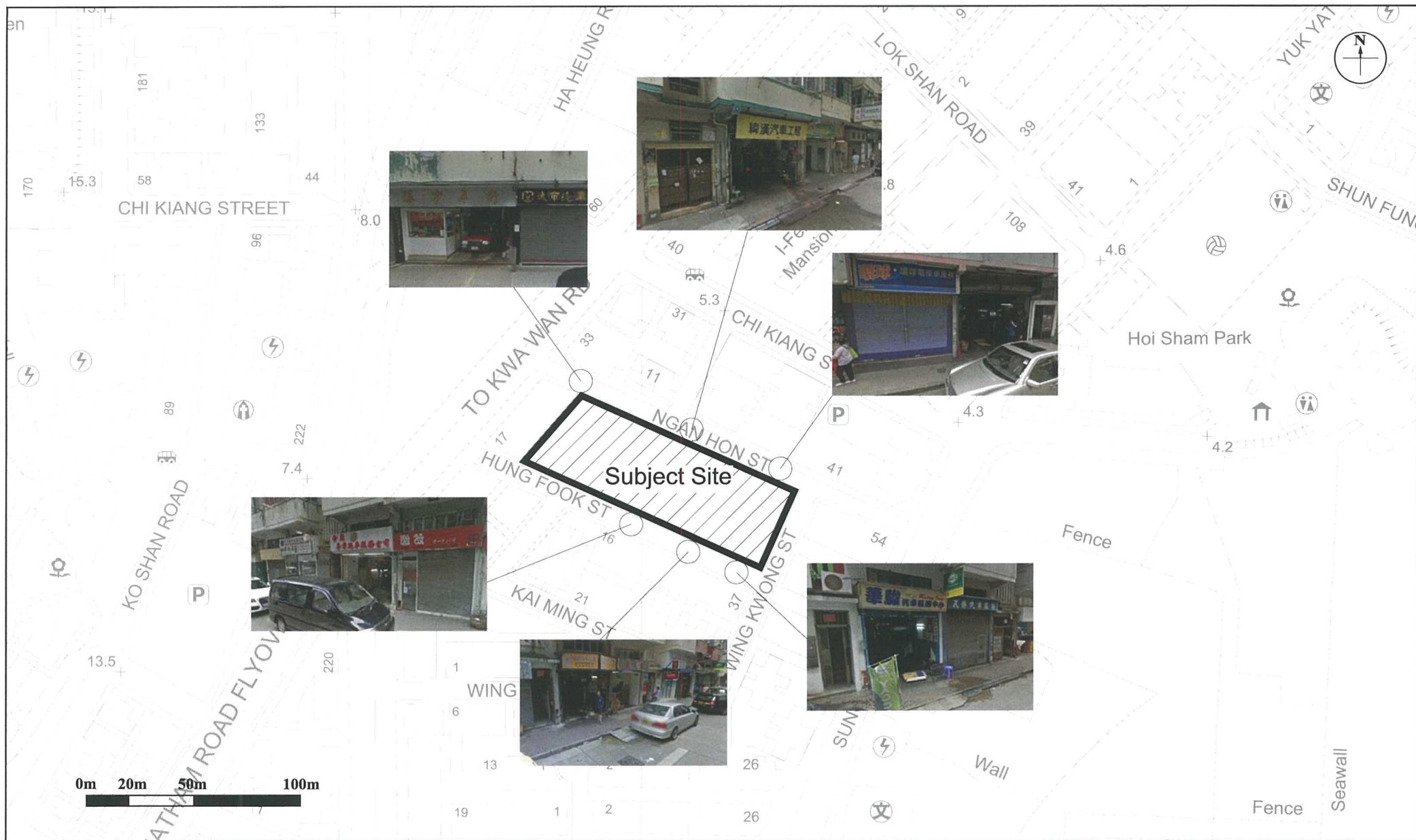


Figure: 5

Title: Photo Records of Potential Land Contamination Issue at the Subject Site

Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City

RAMBOLL ENVIRON

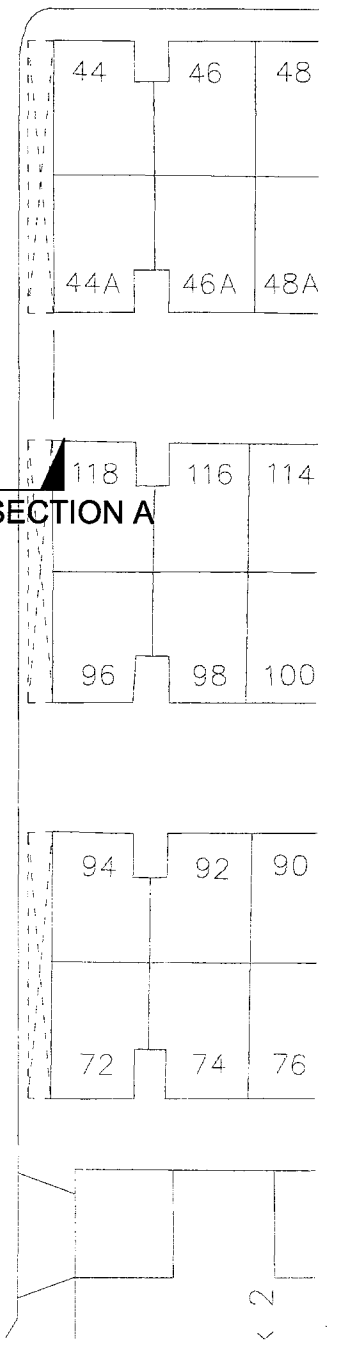
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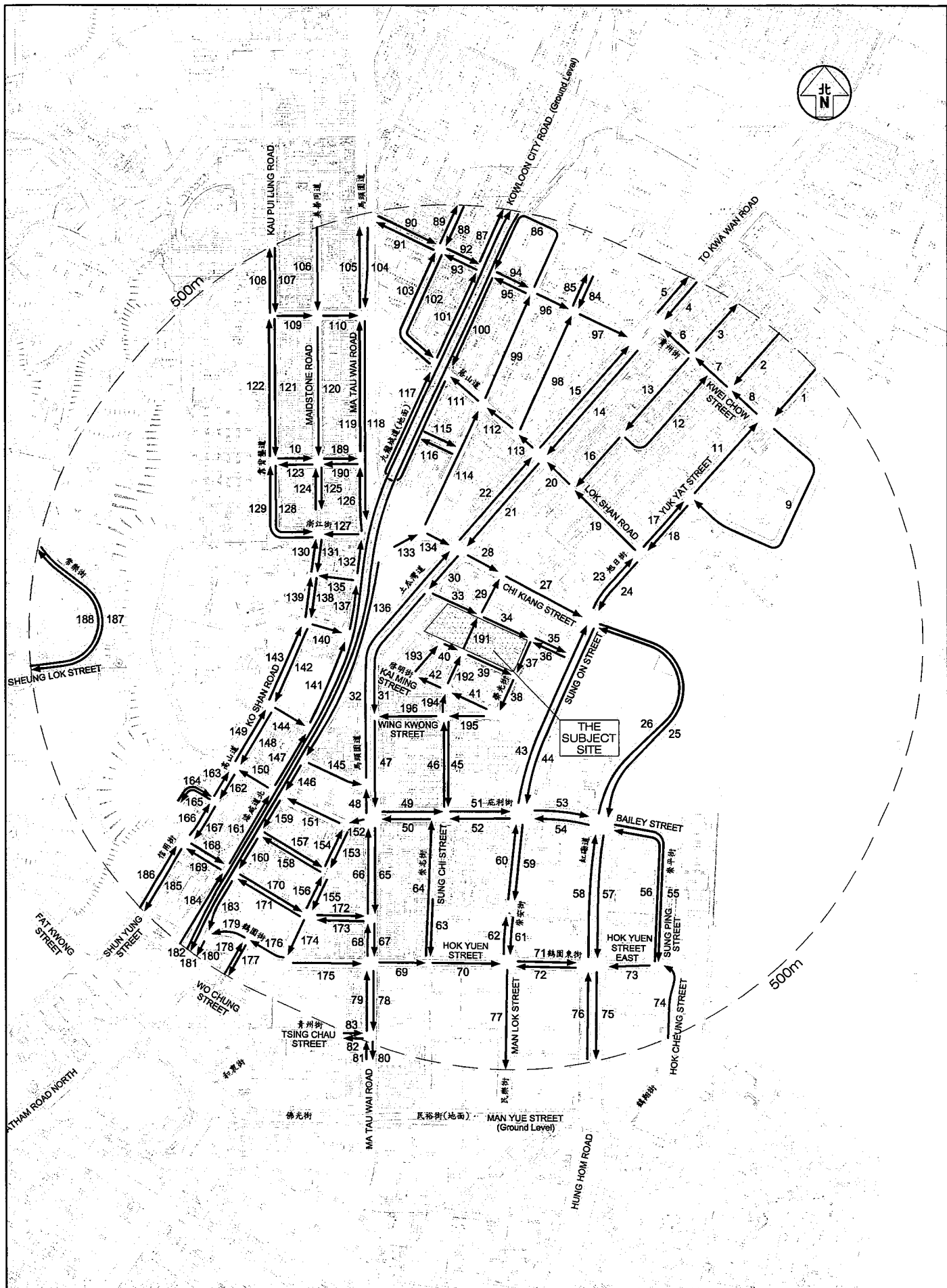
Rev.: 1.1

Date: Aug 2016

APPENDIX 1.1: MASTER LAYOUT PLAN OF THE PROPOSED DEVELOPMENT



APPENDIX 2.1: TRAFFIC FORECAST FOR YEAR 2041



Project Title
PROVISION OF TRAFFIC DATA FOR A STUDY FOR A DEVELOPMENT SCHEME AT KOWLOON CITY (KC-010)

Job No. J6518	Figure No. EIA1	Scale in A3 1 : 4,500
Designed by L K K	Drawn by W S W	Checked by K C
		Revision A
		Date 10 MAY 2016

Figure Title
THE CONCERNED ROAD SECTIONS

CKM Asia Limited
Traffic and Transportation Planning Consultants
21st Floor, Methodist House, 36 Hennessy Road, Wan Chai, Hong Kong
Tel : (852) 2520 5990 Fax : (852) 2528 6343 Email : mail@ckmasia.com.hk

T:\JOB\J6500-J6449\J6518\Fig EIA1 RevA.dwg

Estimated Year 2041 Traffic Data (2041 AM Peak Hour)

No.	Road	From	To	2041 AM Peak Hour Traffic Flows			
				Veh/hr	Vehicle Type for Noise Assessment (%)		
					1 PV	2 HV	Total
1	Long Yuet Street	San Ma Tau Street	Kwei Chow Street	250	74%	26%	100%
2	Wai King Street	San Ma Tau Street	Kwei Chow Street	300	65%	35%	100%
3	Mei King Street	Kwei Chow Street	San Ma Tau Street	50	74%	26%	100%
4	To Kwa Wan Road	San Ma Tau Street	Kwei Chow Street	1200	67%	33%	100%
5	To Kwa Wan Road	Kwei Chow Street	San Ma Tau Street	900	56%	44%	100%
6	Kwei Chow Street	Mei King Street	To Kwa Wan Road	300	60%	40%	100%
7	Kwei Chow Street	Wai King Street	Mei King Street	550	66%	34%	100%
8	Kwei Chow Street	Long Yuet Street	Wai King Street	250	65%	35%	100%
9	King Wan Street	Kwei Chow Street	Shun Fung Street	350	74%	26%	100%
10	Kiang Su Street	Kau Pui Lung Road	Maidstone Road	50	92%	8%	100%
11	Yuk Yat Street	Shun Fung Street	Kwei Chow Street	200	61%	39%	100%
12	Mei Kwong Street	Mei King Street	Kwei Chow Street	100	77%	23%	100%
13	Mei King Street	Kwei Chow Street	Mei Kwong Street	250	80%	20%	100%
14	To Kwa Wan Road	Kwei Chow Street	Lok Shan road	1250	63%	37%	100%
15	To Kwa Wan Road	Lok Shan road	Kwei Chow Street	450	51%	49%	100%
16	Mei King Street	Mei Kwong Street	Lok Shan road	200	89%	11%	100%
17	Yuk Yat Street	Lok Shan road	Shun Fung Street	200	61%	39%	100%
18	Yuk Yat Street	Shun Fung Street	Lok Shan road	350	74%	26%	100%
19	Lok Shan road	Yuk Yat Street	Mei King Street	300	64%	36%	100%
20	Lok Shan road	Mei King Street	To Kwa Wan Road	450	78%	22%	100%
21	To Kwa Wan Road	Lok Shan road	Chi Kiang Street	1150	61%	39%	100%
22	To Kwa Wan Road	Chi Kiang Street	Lok Shan road	500	49%	51%	100%
23	Yuk Yat Street	Chi Kiang Street	Lok Shan road	450	61%	39%	100%
24	Yuk Yat Street	Lok Shan road	Chi Kiang Street	300	86%	14%	100%
25	Chi Kiang Street	Yuk Yat Street	Bailey Street	150	59%	41%	100%
26	Chi Kiang Street	Bailey Street	Yuk Yat Street	100	61%	39%	100%
27	Chi Kiang Street	Lung Tak Street	Yuk Yat Street	450	60%	40%	100%
28	Chi Kiang Street	To Kwa Wan Road	Lung Tak Street	350	55%	45%	100%
29	Lung Tak Street	Ngan Hon Street	Chi Kiang Street	100	86%	14%	100%
30	To Kwa Wan Road	Chi Kiang Street	Ngan Hon Street	1500	64%	36%	100%
31	To Kwa Wan Road	Ngan Hon Street	Wing Kwong Street	1200	60%	40%	100%
32	To Kwa Wan Road	Shek Tong Street	Chi Kiang Street	450	40%	60%	100%
33	Ngan Hon Street	To Kwa Wan Road	Lung Tak Street	300	80%	20%	100%
34	Ngan Hon Street	Lung Tak Street	Wing Kwong Street	300	81%	19%	100%
35	Ngan Hon Street	Wing Kwong Street	Sung On Street	50	81%	19%	100%
36	Ngan Hon Street	Sung On Street	Wing Kwong Street	50	83%	17%	100%
37	Wing Kwong Street	Ngan Hon Street	Hung Fook Street	300	82%	18%	100%
38	Wing Kwong Street	Hung Fook Street	Kai Ming Street	350	82%	18%	100%
39	Hung Fook Street	Access Road	Wing Kwong Street	50	83%	17%	100%
40	Hung Fook Street	Yuk Shing Street	Access Road	100	81%	19%	100%
41	Kai Ming Street	Wing Kwong Street	Access Road	100	82%	18%	100%
42	Kai Ming Street	Access Road	Yuk Shing Street	150	79%	21%	100%
43	Sung On Street	Bailey Street	Chi Kiang Street	250	57%	43%	100%
44	Sung On Street	Chi Kiang Street	Bailey Street	450	66%	34%	100%
45	Wan On Street	Wing Kwong Street	Bailey Street	250	82%	18%	100%
46	Wan On Street	Bailey Street	Wing Kwong Street	250	82%	18%	100%
47	Ma Tau Wai Road	Wing Kwong Street	Bailey Street	1450	64%	36%	100%
48	Ma Tau Wai Road	Gillies Avenue North	Shek Tong Street	400	34%	66%	100%
49	Bailey Street	Ma Tau Wai Road	Wan On Street	650	76%	24%	100%
50	Bailey Street	Sung Chi Street	Ma Tau Wai Road	550	72%	28%	100%
51	Bailey Street	Wan On Street	Sung On Street	650	75%	25%	100%
52	Bailey Street	Sung On Street	Sung Chi Street	450	70%	30%	100%

53	Bailey Street	Sung On Street	Hung Hom Road	800	74%	26%	100%
54	Bailey Street	Hung Hom Road	Sung On Street	500	67%	33%	100%
55	Sung Ping Street	Bailey Street	Hok Yeun Street East	150	77%	23%	100%
56	Sung Ping Street	Hok Yeun Street East	Bailey Street	100	89%	11%	100%
57	Hung Hom Road	Bailey Street	Hok Yeun Street East	900	74%	26%	100%
58	Hung Hom Road	Hok Yeun Street East	Bailey Street	500	63%	37%	100%
59	Sung On Street	Bailey Street	Access Road	200	65%	35%	100%
60	Sung On Street	Access Road	Bailey Street	200	60%	40%	100%
61	Sung On Street	Access Road	Hok Yeun Street East	50	84%	16%	100%
62	Sung On Street	Hok Yeun Street East	Access Road	150	77%	23%	100%
63	Sung Chi Street	Access Road	Hok Yeun Street	50	75%	25%	100%
64	Sung Chi Street	Hok Yeun Street	Bailey Street	100	85%	15%	100%
65	Ma Tau Wai Road	Bailey Street	Pak Kung Street	750	56%	44%	100%
66	Ma Tau Wai Road	Pak Kung Street	Bailey Street	550	49%	51%	100%
67	Ma Tau Wai Road	Pak Kung Street	Hok Yeun Street	750	57%	43%	100%
68	Ma Tau Wai Road	Hok Yeun Street	Pak Kung Street	950	60%	40%	100%
69	Hok Yeun Street	Ma Tau Wai Road	Sung Chi Street	550	69%	31%	100%
70	Hok Yeun Street	Sung Chi Street	Sung On Street	500	68%	32%	100%
71	Hok Yeun Street East	Sung On Street	Hung Hom Road	250	75%	25%	100%
72	Hok Yeun Street East	Hung Hom Road	Sung On Street	200	67%	33%	100%
73	Hok Yeun Street East	Sung Ping Street	Hung Hom Road	200	76%	24%	100%
74	Hok Cheung Street	Hok Yeun Street East	Man Yue Street	200	84%	16%	100%
75	Hung Hom Road	Hok Yeun Street East	Man Yue Street	1050	76%	24%	100%
76	Hung Hom Road	Man Yue Street	Hok Yeun Street East	450	66%	34%	100%
77	Man Lok Street	Hok Yeun Street East	Man Yue Street	400	60%	40%	100%
78	Ma Tau Wai Road	Hok Yeun Street	Tsing Chau Street	550	53%	47%	100%
79	Ma Tau Wai Road	Tsing Chau Street	Hok Yeun Street	900	60%	40%	100%
80	Ma Tau Wai Road	Tsing Chau Street	Man Yue Street	550	52%	48%	100%
81	Ma Tau Wai Road	Man Yue Street	Tsing Chau Street	900	60%	40%	100%
82	Tsing Chau Street	Ma Tau Wai Road	Wo Chung Street	100	89%	11%	100%
83	Tsing Chau Street	Wo Chung Street	Ma Tau Wai Road	100	80%	20%	100%
84	Cheung Ning Street	Cul-de-sac	Sheung Heung Street	50	50%	50%	100%
85	Cheung Ning Street	Sheung Heung Street	Cul-de-sac	50	80%	20%	100%
86	Ha Heung Road	Sheung Heung Street	Kowloon City Road	150	83%	17%	100%
87	Kowloon City Road	Sheung Heung Street	Ma Hang Chung Road	750	77%	23%	100%
88	Pau Chung Street	Ma Hang Chung Road	Sheung Heung Street	50	77%	23%	100%
89	Pau Chung Street	Sheung Heung Street	Ma Hang Chung Road	350	79%	21%	100%
90	Sheung Heung Street	Ma Tau Wai Road	Pau Chung Street	600	62%	38%	100%
91	Sheung Heung Street	Pau Chung Street	Ma Tau Wai Road	100	70%	30%	100%
92	Sheung Heung Street	Pau Chung Street	Kowloon City Road	400	58%	42%	100%
93	Sheung Heung Street	Kowloon City Road	Pau Chung Street	150	70%	30%	100%
94	Sheung Heung Street	Kowloon City Road	Ha Heung Road	300	57%	43%	100%
95	Sheung Heung Street	Ha Heung Road	Kowloon City Road	600	85%	15%	100%
96	Sheung Heung Street	Ha Heung Road	Cheung Ning Street	350	50%	50%	100%
97	Sheung Heung Street	Cheung Ning Street	To Kwa Wan Road	400	54%	46%	100%
98	Cheung Ning Street	Lok Shan road	Sheung Heung Street	100	79%	21%	100%
99	Ha Heung Road	Lok Shan road	Sheung Heung Street	700	77%	23%	100%
100	Kowloon City Road	Sheung Heung Street	Lok Shan road	50	79%	21%	100%
101	Kowloon City Road	Lok Shan road	Sheung Heung Street	200	68%	32%	100%
102	Pau Chung Street	Sheung Heung Street	Kowloon City Road	100	79%	21%	100%
103	Pau Chung Street	Kowloon City Road	Sheung Heung Street	250	84%	16%	100%
104	Ma Tau Wai Road	Sheung Heung Street	Lok Shan road	1400	59%	41%	100%
105	Ma Tau Wai Road	Lok Shan road	Sheung Heung Street	900	59%	41%	100%
106	Maidstone Road	Maidstone Lane	Lok Shan road	250	83%	17%	100%
107	Kau Pui Lung Road	Maidstone Lane	Lok Shan road	500	75%	25%	100%
108	Kau Pui Lung Road	Lok Shan road	Maidstone Lane	350	74%	26%	100%
109	Lok Shan road	Kau Pui Lung Road	Maidstone Road	250	76%	24%	100%
110	Lok Shan road	Maidstone Road	Ma Tau Wai Road	250	81%	19%	100%

111	Lok Shan road	Ha Heung Road	Kowloon City Road	250	76%	24%	100%
112	Lok Shan road	Cheung Ning Street	Ha Heung Road	600	74%	26%	100%
113	Lok Shan road	To Kwa Wan Road	Cheung Ning Street	700	76%	24%	100%
114	Ha Heung Road	Chi Kiang Street	Lok Shan road	350	78%	22%	100%
115	Sze Chuen Street	Kowloon City Road	Ha Heung Road	100	77%	23%	100%
116	Sze Chuen Street	Ha Heung Road	Kowloon City Road	100	78%	22%	100%
117	Kowloon City Road	Ha Heung Road	Lok Shan road	100	62%	38%	100%
118	Ma Tau Wai Road	Lok Shan road	Chi Kiang Street	1600	62%	38%	100%
119	Ma Tau Wai Road	Kiang Su Street	Lok Shan road	900	59%	41%	100%
120	Maidstone Road	Lok Shan road	Kiang Su Street	150	68%	32%	100%
121	Kau Pui Lung Road	Lok Shan road	Kiang Su Street	300	77%	23%	100%
122	Kau Pui Lung Road	Kiang Su Street	Lok Shan road	500	74%	26%	100%
123	Kiang Su Street	Maidstone Road	Kau Pui Lung Road	150	71%	29%	100%
124	Maidstone Road	Chi Kiang Street	Kiang Su Street	50	88%	13%	100%
125	Maidstone Road	Kiang Su Street	Chi Kiang Street	50	85%	15%	100%
126	Ma Tau Wai Road	Chi Kiang Street	Kiang Su Street	900	59%	41%	100%
127	Chi Kiang Street	Ma Tau Wai Road	Ko Shan Road	100	85%	15%	100%
128	Kau Pui Lung Road	Kiang Su Street	Ko Shan Road	300	73%	27%	100%
129	Kau Pui Lung Road	Ko Shan Road	Kiang Su Street	400	73%	27%	100%
130	Ko Shan Road	Chi Kiang Street	Anhui Street	400	72%	28%	100%
131	Ko Shan Road	Anhui Street	Chi Kiang Street	300	73%	27%	100%
132	Ma Tau Wai Road	Anhui Street	Chi Kiang Street	1550	65%	35%	100%
133	Chi Kiang Street	Chatham Road North	Ha Heung Road	1100	71%	29%	100%
134	Chi Kiang Street	Ha Heung Road	To Kwa Wan Road	750	68%	32%	100%
135	Anhui Street	Chatham Road North	Ko Shan Road	200	81%	19%	100%
136	Chatham Road North	Chi Kiang Street	Shek Tong Street	1100	62%	38%	100%
137	Chatham Road North	Kiang Hsi Street	Anhui Street	1750	69%	31%	100%
138	Ko Shan Road	Anhui Street	Kiang Hsi Street	300	75%	25%	100%
139	Ko Shan Road	Kiang Hsi Street	Anhui Street	250	70%	30%	100%
140	Kiang Hsi Street	Ko Shan Road	Chatham Road North	250	78%	22%	100%
141	Chatham Road North	Shan Si Street	Kiang Hsi Street	1500	68%	32%	100%
142	Ko Shan Road	Kiang Hsi Street	Shan Si Street	300	76%	24%	100%
143	Ko Shan Road	Shan Si Street	Kiang Hsi Street	450	75%	25%	100%
144	Shan Si Street	Ko Shan Road	Chatham Road North	250	83%	17%	100%
145	Shek Tong Street	Chatham Road North	Ma Tau Wai Road	100	84%	16%	100%
146	Chatham Road North	Shek Tong Street	San Lau Street	1100	62%	38%	100%
147	Chatham Road North	San Lau Street	Shan Si Street	1300	65%	35%	100%
148	Ko Shan Road	Shan Si Street	San Lau Street	300	76%	24%	100%
149	Ko Shan Road	San Lau Street	Shan Si Street	650	78%	22%	100%
150	San Lau Street	Chatham Road North	Ko Shan Road	300	73%	27%	100%
151	San Lau Street	Gillies Avenue North	Chatham Road North	750	75%	25%	100%
152	Gillies Avenue North	Ma Tau Wai Road	San Lau Street	800	72%	28%	100%
153	Gillies Avenue North	San Lau Street	San Wai Street	300	70%	30%	100%
154	Gillies Avenue North	San Wai Street	San Lau Street	200	73%	27%	100%
155	Gillies Avenue North	San Wai Street	Pak Kung Street	350	69%	31%	100%
156	Gillies Avenue North	Pak Kung Street	San Wai Street	200	73%	28%	100%
157	San Wai Street	Chatham Road North	Gillies Avenue North	150	77%	23%	100%
158	San Wai Street	Gillies Avenue North	Chatham Road North	50	85%	15%	100%
159	Chatham Road North	San Lau Street	San Wai Street	1100	62%	38%	100%
160	Chatham Road North	San Wai Street	Pak Kung Street	1000	62%	38%	100%
161	Chatham Road North	Pak Kung Street	San Lau Street	850	58%	42%	100%
162	Ko Shan Road	San Lau Street	Wai Yin Path	450	78%	22%	100%
163	Ko Shan Road	Wai Yin Path	San Lau Street	550	81%	19%	100%
164	Wai Yin Path	Cul-de-sac	Ko Shan Road	50	79%	21%	100%
165	Wai Yin Path	Ko Shan Road	Cul-de-sac	100	91%	9%	100%
166	Ko Shan Road	Pak Kung Street	Wai Yin Path	600	82%	18%	100%
167	Ko Shan Road	Wai Yin Path	Pak Kung Street	400	77%	23%	100%
168	Pak Kung Street	Shun Yung Street	Chatham Road North	250	85%	15%	100%

169	Pak Kung Street	Chatham Road North	Shun Yung Street	300	83%	17%	100%
170	Pak Kung Street	Chatham Road North	Gillies Avenue North	150	77%	23%	100%
171	Pak Kung Street	Gillies Avenue North	Chatham Road North	50	56%	44%	100%
172	Pak Kung Street	Gillies Avenue North	Ma Tau Wai Road	50	77%	23%	100%
173	Pak Kung Street	Ma Tau Wai Road	Gillies Avenue North	450	74%	26%	100%
174	Gillies Avenue North	Pak Kung Street	Hok Yeun Street	650	72%	28%	100%
175	Hok Yeun Street	Gillies Avenue North	Ma Tau Wai Road	450	71%	29%	100%
176	Hok Yeun Street	Gillies Avenue North	Hok Yuen Lane	250	72%	28%	100%
177	Hok Yuen Lane	Hok Yeun Street	Cul-de-sac	50	94%	6%	100%
178	Hok Yuen Lane	Cul-de-sac	Hok Yeun Street	50	100%	0%	100%
179	Hok Yeun Street	Hok Yuen Lane	Chatham Road North	250	72%	28%	100%
180	Chatham Road North	Hok Yeun Street	Wo Chung Street	1100	61%	39%	100%
181	Chatham Road Flyover	Ha Heung Road	Wo Chung Street	2300	77%	23%	100%
182	Chatham Road Flyover	Wo Chung Street	Ha Heung Road	3150	80%	20%	100%
183	Chatham Road North	Pak Kung Street	Hok Yeun Street	900	59%	41%	100%
184	Chatham Road North	Wo Chung Street	Pak Kung Street	900	59%	41%	100%
185	Shun Yung Street	Pak Kung Street	Fat Kwong Street	650	79%	21%	100%
186	Shun Yung Street	Fat Kwong Street	Pak Kung Street	750	83%	17%	100%
187	Sheung Lok Street	Sheung Wo Street	Fat Kwong Street	200	72%	28%	100%
188	Sheung Lok Street	Fat Kwong Street	Sheung Wo Street	200	76%	24%	100%
189	Kiang Su Street	Maidstone Road	Ma Tau Wai Road	50	88%	13%	100%
190	Kiang Su Street	Ma Tau Wai Road	Maidstone Road	50	81%	19%	100%
191	Access Road	Hung Fook Street	Ngan Hon Street	100	84%	16%	100%
192	Access Road	Kai Ming Street	Hung Fook Street	100	84%	16%	100%
193	Yuk Shing Street	Kai Ming Street	Hung Fook Street	100	79%	21%	100%
194	Access Road	Wing Kwong Street	Kai Ming Street	150	84%	16%	100%
195	Wing Kwong Street	Kai Ming Street	Wan On Street	300	82%	18%	100%
196	Wing Kwong Street	Wan On Street	Ma Tau Wai Road	250	82%	18%	100%

Estimated Year 2041 Traffic Data (2041 PM Peak Hour)

No.	Road	From	To	2041 PM Peak Hour Traffic Flows			
				Veh/hr	Vehicle Type for Noise Assessment (%)		
					1 PV	2 HV	Total
1	Long Yuet Street	San Ma Tau Street	Kwei Chow Street	250	79%	21%	100%
2	Wai King Street	San Ma Tau Street	Kwei Chow Street	250	64%	36%	100%
3	Mei King Street	Kwei Chow Street	San Ma Tau Street	100	77%	23%	100%
4	To Kwa Wan Road	San Ma Tau Street	Kwei Chow Street	1150	70%	30%	100%
5	To Kwa Wan Road	Kwei Chow Street	San Ma Tau Street	1000	59%	41%	100%
6	Kwei Chow Street	Mei King Street	To Kwa Wan Road	300	61%	39%	100%
7	Kwei Chow Street	Wai King Street	Mei King Street	450	64%	36%	100%
8	Kwei Chow Street	Long Yuet Street	Wai King Street	150	64%	36%	100%
9	King Wan Street	Kwei Chow Street	Shun Fung Street	350	81%	19%	100%
10	Kiang Su Street	Kau Pui Lung Road	Maidstone Road	50	88%	13%	100%
11	Yuk Yat Street	Shun Fung Street	Kwei Chow Street	150	64%	36%	100%
12	Mei Kwong Street	Mei King Street	Kwei Chow Street	50	77%	23%	100%
13	Mei King Street	Kwei Chow Street	Mei Kwong Street	150	69%	31%	100%
14	To Kwa Wan Road	Kwei Chow Street	Lok Shan road	1250	67%	33%	100%
15	To Kwa Wan Road	Lok Shan road	Kwei Chow Street	450	52%	48%	100%
16	Mei King Street	Mei Kwong Street	Lok Shan road	100	84%	16%	100%
17	Yuk Yat Street	Lok Shan road	Shun Fung Street	150	64%	36%	100%
18	Yuk Yat Street	Shun Fung Street	Lok Shan road	350	81%	19%	100%
19	Lok Shan road	Yuk Yat Street	Mei King Street	300	63%	37%	100%
20	Lok Shan road	Mei King Street	To Kwa Wan Road	400	76%	24%	100%
21	To Kwa Wan Road	Lok Shan road	Chi Kiang Street	1150	66%	34%	100%
22	To Kwa Wan Road	Chi Kiang Street	Lok Shan road	600	51%	49%	100%
23	Yuk Yat Street	Chi Kiang Street	Lok Shan road	400	64%	36%	100%
24	Yuk Yat Street	Lok Shan road	Chi Kiang Street	300	86%	14%	100%
25	Chi Kiang Street	Yuk Yat Street	Bailey Street	150	43%	57%	100%
26	Chi Kiang Street	Bailey Street	Yuk Yat Street	100	43%	57%	100%
27	Chi Kiang Street	Lung Tak Street	Yuk Yat Street	450	58%	42%	100%
28	Chi Kiang Street	To Kwa Wan Road	Lung Tak Street	350	55%	45%	100%
29	Lung Tak Street	Ngan Hon Street	Chi Kiang Street	100	86%	14%	100%
30	To Kwa Wan Road	Chi Kiang Street	Ngan Hon Street	1450	70%	30%	100%
31	To Kwa Wan Road	Ngan Hon Street	Wing Kwong Street	1150	67%	33%	100%
32	To Kwa Wan Road	Shek Tong Street	Chi Kiang Street	550	56%	44%	100%
33	Ngan Hon Street	To Kwa Wan Road	Lung Tak Street	300	80%	20%	100%
34	Ngan Hon Street	Lung Tak Street	Wing Kwong Street	300	81%	19%	100%
35	Ngan Hon Street	Wing Kwong Street	Sung On Street	50	81%	19%	100%
36	Ngan Hon Street	Sung On Street	Wing Kwong Street	50	91%	9%	100%
37	Wing Kwong Street	Ngan Hon Street	Hung Fook Street	300	83%	17%	100%
38	Wing Kwong Street	Hung Fook Street	Kai Ming Street	350	83%	17%	100%
39	Hung Fook Street	Access Road	Wing Kwong Street	50	84%	16%	100%
40	Hung Fook Street	Yuk Shing Street	Access Road	50	83%	17%	100%
41	Kai Ming Street	Wing Kwong Street	Access Road	100	82%	18%	100%
42	Kai Ming Street	Access Road	Yuk Shing Street	150	83%	17%	100%
43	Sung On Street	Bailey Street	Chi Kiang Street	300	70%	30%	100%
44	Sung On Street	Chi Kiang Street	Bailey Street	450	71%	29%	100%
45	Wan On Street	Wing Kwong Street	Bailey Street	200	84%	16%	100%
46	Wan On Street	Bailey Street	Wing Kwong Street	250	83%	17%	100%
47	Ma Tau Wai Road	Wing Kwong Street	Bailey Street	1350	70%	30%	100%
48	Ma Tau Wai Road	Gillies Avenue North	Shek Tong Street	500	53%	47%	100%
49	Bailey Street	Ma Tau Wai Road	Wan On Street	700	74%	26%	100%
50	Bailey Street	Sung Chi Street	Ma Tau Wai Road	600	75%	25%	100%
51	Bailey Street	Wan On Street	Sung On Street	650	73%	27%	100%
52	Bailey Street	Sung On Street	Sung Chi Street	500	74%	26%	100%

53	Bailey Street	Sung On Street	Hung Hom Road	800	75%	25%	100%
54	Bailey Street	Hung Hom Road	Sung On Street	500	70%	30%	100%
55	Sung Ping Street	Bailey Street	Hok Yeun Street East	150	85%	15%	100%
56	Sung Ping Street	Hok Yeun Street East	Bailey Street	100	90%	10%	100%
57	Hung Hom Road	Bailey Street	Hok Yeun Street East	850	74%	26%	100%
58	Hung Hom Road	Hok Yeun Street East	Bailey Street	550	68%	32%	100%
59	Sung On Street	Bailey Street	Access Road	150	60%	40%	100%
60	Sung On Street	Access Road	Bailey Street	200	83%	17%	100%
61	Sung On Street	Access Road	Hok Yeun Street East	100	88%	12%	100%
62	Sung On Street	Hok Yeun Street East	Access Road	50	81%	19%	100%
63	Sung Chi Street	Access Road	Hok Yeun Street	50	75%	25%	100%
64	Sung Chi Street	Hok Yeun Street	Bailey Street	100	84%	16%	100%
65	Ma Tau Wai Road	Bailey Street	Pak Kung Street	650	63%	37%	100%
66	Ma Tau Wai Road	Pak Kung Street	Bailey Street	600	58%	42%	100%
67	Ma Tau Wai Road	Pak Kung Street	Hok Yeun Street	650	64%	36%	100%
68	Ma Tau Wai Road	Hok Yeun Street	Pak Kung Street	1150	69%	31%	100%
69	Hok Yeun Street	Ma Tau Wai Road	Sung Chi Street	500	83%	17%	100%
70	Hok Yeun Street	Sung Chi Street	Sung On Street	450	83%	17%	100%
71	Hok Yeun Street East	Sung On Street	Hung Hom Road	300	86%	14%	100%
72	Hok Yeun Street East	Hung Hom Road	Sung On Street	250	84%	16%	100%
73	Hok Yeun Street East	Sung Ping Street	Hung Hom Road	200	87%	13%	100%
74	Hok Cheung Street	Hok Yeun Street East	Man Yue Street	200	92%	8%	100%
75	Hung Hom Road	Hok Yeun Street East	Man Yue Street	1000	79%	21%	100%
76	Hung Hom Road	Man Yue Street	Hok Yeun Street East	450	68%	32%	100%
77	Man Lok Street	Hok Yeun Street East	Man Yue Street	450	83%	17%	100%
78	Ma Tau Wai Road	Hok Yeun Street	Tsing Chau Street	450	55%	45%	100%
79	Ma Tau Wai Road	Tsing Chau Street	Hok Yeun Street	1100	69%	31%	100%
80	Ma Tau Wai Road	Tsing Chau Street	Man Yue Street	400	51%	49%	100%
81	Ma Tau Wai Road	Man Yue Street	Tsing Chau Street	1050	68%	32%	100%
82	Tsing Chau Street	Ma Tau Wai Road	Wo Chung Street	100	84%	16%	100%
83	Tsing Chau Street	Wo Chung Street	Ma Tau Wai Road	100	90%	10%	100%
84	Cheung Ning Street	Cul-de-sac	Sheung Heung Street	50	86%	14%	100%
85	Cheung Ning Street	Sheung Heung Street	Cul-de-sac	50	33%	67%	100%
86	Ha Heung Road	Sheung Heung Street	Kowloon City Road	100	88%	12%	100%
87	Kowloon City Road	Sheung Heung Street	Ma Hang Chung Road	750	74%	26%	100%
88	Pau Chung Street	Ma Hang Chung Road	Sheung Heung Street	100	83%	17%	100%
89	Pau Chung Street	Sheung Heung Street	Ma Hang Chung Road	250	79%	21%	100%
90	Sheung Heung Street	Ma Tau Wai Road	Pau Chung Street	500	57%	43%	100%
91	Sheung Heung Street	Pau Chung Street	Ma Tau Wai Road	150	82%	18%	100%
92	Sheung Heung Street	Pau Chung Street	Kowloon City Road	400	55%	45%	100%
93	Sheung Heung Street	Kowloon City Road	Pau Chung Street	150	82%	18%	100%
94	Sheung Heung Street	Kowloon City Road	Ha Heung Road	350	59%	41%	100%
95	Sheung Heung Street	Ha Heung Road	Kowloon City Road	600	82%	18%	100%
96	Sheung Heung Street	Ha Heung Road	Cheung Ning Street	450	58%	42%	100%
97	Sheung Heung Street	Cheung Ning Street	To Kwa Wan Road	500	63%	37%	100%
98	Cheung Ning Street	Lok Shan road	Sheung Heung Street	100	78%	22%	100%
99	Ha Heung Road	Lok Shan road	Sheung Heung Street	700	75%	25%	100%
100	Kowloon City Road	Sheung Heung Street	Lok Shan road	50	73%	27%	100%
101	Kowloon City Road	Lok Shan road	Sheung Heung Street	250	75%	25%	100%
102	Pau Chung Street	Sheung Heung Street	Kowloon City Road	150	84%	16%	100%
103	Pau Chung Street	Kowloon City Road	Sheung Heung Street	200	80%	20%	100%
104	Ma Tau Wai Road	Sheung Heung Street	Lok Shan road	1050	63%	37%	100%
105	Ma Tau Wai Road	Lok Shan road	Sheung Heung Street	850	55%	45%	100%
106	Maidstone Road	Maidstone Lane	Lok Shan road	150	76%	24%	100%
107	Kau Pui Lung Road	Maidstone Lane	Lok Shan road	500	79%	21%	100%
108	Kau Pui Lung Road	Lok Shan road	Maidstone Lane	350	69%	31%	100%
109	Lok Shan road	Kau Pui Lung Road	Maidstone Road	250	75%	25%	100%
110	Lok Shan road	Maidstone Road	Ma Tau Wai Road	300	85%	15%	100%

111	Lok Shan road	Ha Heung Road	Kowloon City Road	250	70%	30%	100%
112	Lok Shan road	Cheung Ning Street	Ha Heung Road	550	70%	30%	100%
113	Lok Shan road	To Kwa Wan Road	Cheung Ning Street	650	72%	28%	100%
114	Ha Heung Road	Chi Kiang Street	Lok Shan road	350	76%	24%	100%
115	Sze Chuen Street	Kowloon City Road	Ha Heung Road	100	73%	27%	100%
116	Sze Chuen Street	Ha Heung Road	Kowloon City Road	100	74%	26%	100%
117	Kowloon City Road	Ha Heung Road	Lok Shan road	100	59%	41%	100%
118	Ma Tau Wai Road	Lok Shan road	Chi Kiang Street	1250	67%	33%	100%
119	Ma Tau Wai Road	Kiang Su Street	Lok Shan road	850	54%	46%	100%
120	Maidstone Road	Lok Shan road	Kiang Su Street	150	69%	31%	100%
121	Kau Pui Lung Road	Lok Shan road	Kiang Su Street	300	81%	19%	100%
122	Kau Pui Lung Road	Kiang Su Street	Lok Shan road	500	70%	30%	100%
123	Kiang Su Street	Maidstone Road	Kau Pui Lung Road	150	70%	30%	100%
124	Maidstone Road	Chi Kiang Street	Kiang Su Street	50	86%	14%	100%
125	Maidstone Road	Kiang Su Street	Chi Kiang Street	50	78%	22%	100%
126	Ma Tau Wai Road	Chi Kiang Street	Kiang Su Street	850	54%	46%	100%
127	Chi Kiang Street	Ma Tau Wai Road	Ko Shan Road	100	85%	15%	100%
128	Kau Pui Lung Road	Kiang Su Street	Ko Shan Road	350	77%	23%	100%
129	Kau Pui Lung Road	Ko Shan Road	Kiang Su Street	400	69%	31%	100%
130	Ko Shan Road	Chi Kiang Street	Anhui Street	400	68%	32%	100%
131	Ko Shan Road	Anhui Street	Chi Kiang Street	350	77%	23%	100%
132	Ma Tau Wai Road	Anhui Street	Chi Kiang Street	1550	56%	44%	100%
133	Chi Kiang Street	Chatham Road North	Ha Heung Road	1000	66%	34%	100%
134	Chi Kiang Street	Ha Heung Road	To Kwa Wan Road	650	61%	39%	100%
135	Anhui Street	Chatham Road North	Ko Shan Road	150	66%	34%	100%
136	Chatham Road North	Chi Kiang Street	Shek Tong Street	950	72%	28%	100%
137	Chatham Road North	Kiang Hsi Street	Anhui Street	1700	64%	36%	100%
138	Ko Shan Road	Anhui Street	Kiang Hsi Street	350	80%	20%	100%
139	Ko Shan Road	Kiang Hsi Street	Anhui Street	250	72%	28%	100%
140	Kiang Hsi Street	Ko Shan Road	Chatham Road North	250	73%	27%	100%
141	Chatham Road North	Shan Si Street	Kiang Hsi Street	1500	62%	38%	100%
142	Ko Shan Road	Kiang Hsi Street	Shan Si Street	350	90%	10%	100%
143	Ko Shan Road	Shan Si Street	Kiang Hsi Street	450	78%	22%	100%
144	Shan Si Street	Ko Shan Road	Chatham Road North	200	78%	22%	100%
145	Shek Tong Street	Chatham Road North	Ma Tau Wai Road	100	78%	22%	100%
146	Chatham Road North	Shek Tong Street	San Lau Street	900	72%	28%	100%
147	Chatham Road North	San Lau Street	Shan Si Street	1300	60%	40%	100%
148	Ko Shan Road	Shan Si Street	San Lau Street	350	90%	10%	100%
149	Ko Shan Road	San Lau Street	Shan Si Street	650	78%	22%	100%
150	San Lau Street	Chatham Road North	Ko Shan Road	300	75%	25%	100%
151	San Lau Street	Gillies Avenue North	Chatham Road North	800	77%	23%	100%
152	Gillies Avenue North	Ma Tau Wai Road	San Lau Street	750	73%	27%	100%
153	Gillies Avenue North	San Lau Street	San Wai Street	300	81%	19%	100%
154	Gillies Avenue North	San Wai Street	San Lau Street	350	87%	13%	100%
155	Gillies Avenue North	San Wai Street	Pak Kung Street	350	83%	17%	100%
156	Gillies Avenue North	Pak Kung Street	San Wai Street	300	89%	11%	100%
157	San Wai Street	Chatham Road North	Gillies Avenue North	100	82%	18%	100%
158	San Wai Street	Gillies Avenue North	Chatham Road North	100	71%	29%	100%
159	Chatham Road North	San Lau Street	San Wai Street	900	72%	28%	100%
160	Chatham Road North	San Wai Street	Pak Kung Street	850	71%	29%	100%
161	Chatham Road North	Pak Kung Street	San Lau Street	800	50%	50%	100%
162	Ko Shan Road	San Lau Street	Wai Yin Path	500	88%	12%	100%
163	Ko Shan Road	Wai Yin Path	San Lau Street	550	82%	18%	100%
164	Wai Yin Path	Cul-de-sac	Ko Shan Road	50	83%	17%	100%
165	Wai Yin Path	Ko Shan Road	Cul-de-sac	50	74%	26%	100%
166	Ko Shan Road	Pak Kung Street	Wai Yin Path	550	82%	18%	100%
167	Ko Shan Road	Wai Yin Path	Pak Kung Street	500	88%	12%	100%
168	Pak Kung Street	Shun Yung Street	Chatham Road North	150	84%	16%	100%

169	Pak Kung Street	Chatham Road North	Shun Yung Street	350	75%	25%	100%
170	Pak Kung Street	Chatham Road North	Gillies Avenue North	200	77%	23%	100%
171	Pak Kung Street	Gillies Avenue North	Chatham Road North	100	82%	18%	100%
172	Pak Kung Street	Gillies Avenue North	Ma Tau Wai Road	50	81%	19%	100%
173	Pak Kung Street	Ma Tau Wai Road	Gillies Avenue North	600	81%	19%	100%
174	Gillies Avenue North	Pak Kung Street	Hok Yeun Street	700	79%	21%	100%
175	Hok Yeun Street	Gillies Avenue North	Ma Tau Wai Road	400	79%	21%	100%
176	Hok Yeun Street	Gillies Avenue North	Hok Yuen Lane	300	79%	21%	100%
177	Hok Yuen Lane	Hok Yeun Street	Cul-de-sac	50	100%	0%	100%
178	Hok Yuen Lane	Cul-de-sac	Hok Yeun Street	100	91%	9%	100%
179	Hok Yeun Street	Hok Yuen Lane	Chatham Road North	350	80%	20%	100%
180	Chatham Road North	Hok Yeun Street	Wo Chung Street	1100	74%	26%	100%
181	Chatham Road Flyover	Ha Heung Road	Wo Chung Street	2050	80%	20%	100%
182	Chatham Road Flyover	Wo Chung Street	Ha Heung Road	3350	76%	24%	100%
183	Chatham Road North	Pak Kung Street	Hok Yeun Street	750	71%	29%	100%
184	Chatham Road North	Wo Chung Street	Pak Kung Street	1000	53%	47%	100%
185	Shun Yung Street	Pak Kung Street	Fat Kwong Street	600	82%	18%	100%
186	Shun Yung Street	Fat Kwong Street	Pak Kung Street	450	80%	20%	100%
187	Sheung Lok Street	Sheung Wo Street	Fat Kwong Street	150	78%	22%	100%
188	Sheung Lok Street	Fat Kwong Street	Sheung Wo Street	150	74%	26%	100%
189	Kiang Su Street	Maidstone Road	Ma Tau Wai Road	50	89%	11%	100%
190	Kiang Su Street	Ma Tau Wai Road	Maidstone Road	50	83%	17%	100%
191	Access Road	Hung Fook Street	Ngan Hon Street	100	84%	16%	100%
192	Access Road	Kai Ming Street	Hung Fook Street	100	83%	17%	100%
193	Yuk Shing Street	Kai Ming Street	Hung Fook Street	100	81%	19%	100%
194	Access Road	Wing Kwong Street	Kai Ming Street	150	84%	16%	100%
195	Wing Kwong Street	Kai Ming Street	Wan On Street	300	83%	17%	100%
196	Wing Kwong Street	Wan On Street	Ma Tau Wai Road	250	83%	17%	100%

APPENDIX 2.2: RESULTS OF ROAD TRAFFIC NOISE IMPACT ASSESSMENT

Predicted Road Traffic Noise at Selected Sensitive Receivers Unmitigated Scenario (AM)

[illegible]

Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2
1/F	27.55	66	66	66	66	66	66	65	65	65	66	63	63	63	63	63	53	61	62	62	62	58	64	66
2/F	30.70	66	66	66	66	66	65	65	65	65	66	63	63	63	63	64	54	63	63	63	64	60	65	66
3/F	33.85	66	66	66	66	66	65	65	65	65	66	63	63	63	63	64	55	63	63	64	64	60	66	67
4/F	37.00	66	66	66	66	66	65	65	65	65	66	63	63	63	63	64	55	64	64	64	65	60	66	67
5/F	40.15	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	56	64	64	65	65	61	66	67
6/F	43.30	66	66	66	66	66	65	65	66	66	66	64	63	63	63	65	57	64	65	65	66	61	66	67
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	65	66	61	67	67
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	58	65	66	66	67	61	67	68
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68
11/F	59.05	66	67	67	67	67	66	66	66	66	66	64	64	63	63	66	59	66	67	67	67	61	68	68
12/F	62.20	67	67	67	67	67	67	66	66	66	67	64	64	63	63	66	59	67	67	67	68	61	68	68
13/F	65.35	67	67	68	67	67	67	67	67	67	67	64	64	63	63	66	60	67	67	68	68	62	68	69
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	64	63	67	60	67	68	68	68	62	69	69
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	64	63	67	61	68	68	68	69	63	69	69
16/F	74.80	68	69	69	68	68	68	68	67	67	68	65	64	64	63	67	61	68	68	68	69	63	69	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	69	69	63	69	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	69	69	63	69	69
19/F	84.25	69	69	70	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	69	69	63	69	69
20/F	87.40	69	69	70	69	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	69	63	69	69
21/F	90.55	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	69	69	63	69	69
22/F	93.70	69	70	70	70	69	69	69	69	69	69	66	64	64	63	68	61	68	68	69	69	63	69	69
23/F	96.85	70	70	70	70	70	69	69	69	69	69	66	65	64	64	68	62	68	68	69	69	63	69	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		0				0		0		0		0		0		0		0		0		0		
No. of Premises		23				23		23		23		23		23		23		23		23		23		

Floor	mPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-J3		
1/F	27.55	67	69	70	70	70	70	70	70	70	70	69	69	69	69	68	63	64	63	63	62	60	63	61	62		
2/F	30.70	67	69	70	70	70	69	69	70	70	70	69	69	69	69	67	63	64	63	63	62	59	64	62	63		
3/F	33.85	68	69	69	69	69	69	69	69	69	69	69	69	69	69	67	64	64	63	63	62	59	64	63	64		
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	63	62	59	65	63	64		
5/F	40.15	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	63	62	59	65	64	65		
6/F	43.30	67	68	69	68	68	68	68	68	68	68	68	68	68	68	67	64	64	63	63	62	59	65	64	65		
7/F	46.45	67	68	68	68	68	68	68	68	68	68	68	68	68	68	66	64	64	63	63	62	59	65	65	66		
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	63	62	59	65	65	66		
9/F	52.75	67	67	68	68	68	68	68	68	68	68	67	67	68	68	66	64	64	63	63	62	59	65	65	66		
10/F	55.90	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	64	63	63	59	66	66	66		
11/F	59.05	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	64	63	63	59	66	66	66		
12/F	62.20	67	67	68	68	68	68	68	68	67	68	67	67	67	67	66	64	64	64	63	63	59	66	67	67		
13/F	65.35	67	68	68	68	68	68	67	67	67	68	67	67	67	67	66	64	64	64	64	64	60	67	67	67		
14/F	68.50	68	68	68	68	68	68	67	68	68	67	68	66	67	67	66	64	64	64	64	64	60	67	67	67		
15/F	71.65	68	68	69	68	68	68	68	68	68	68	66	67	67	67	66	64	65	64	64	64	60	68	68	68		
16/F	74.80	68	69	69	69	68	68	68	68	68	68	66	67	67	67	66	64	65	65	64	64	60	68	68	68		
17/F	77.95	69	69	69	69	69	68	68	68	68	68	66	67	67	67	66	64	65	65	64	65	60	68	68	68		
18/F	81.10	69	69	69	69	69	69	68	68	68	68	66	67	67	67	66	64	65	65	65	65	60	68	68	69		
19/F	84.25	69	69	70	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69		
20/F	87.40	69	69	70	69	69	69	69	69	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69		
21/F	90.55	69	69	70	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69		
22/F	93.70	69	69	70	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	69		
23/F	96.85	69	69	70	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	69		
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
No. of Exceedance		0				0		0				0				0				0		0		0			
No. of Premises		23				23		23				23				23				23		23		23			

Max. Noise Level =	75
Total no. of Exceedance =	190
Total no. of Premises =	759
%Compliance =	75%

Predicted Road Traffic Noise at Selected Sensitive Receivers
Unmitigated Scenario (PM)

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	27.55	58	61	65	65	65	65	65	65	64	65	65	65	66	66	67	66	65	64	63	61	59	56	55	55	54
2/F	30.70	60	62	65	65	65	65	65	65	65	65	65	66	66	66	67	67	66	65	64	62	61	58	57	57	57
3/F	33.85	62	63	65	66	66	66	66	66	65	66	66	66	66	66	67	67	66	66	65	63	62	60	60	60	61
4/F	37.00	65	65	66	66	66	66	66	66	66	66	66	66	66	66	68	68	67	66	66	65	64	63	64	64	65
5/F	40.15	69	68	67	67	66	66	66	66	66	66	66	66	66	66	68	68	68	67	67	67	67	67	68	68	70
6/F	43.30	72	71	68	68	67	67	67	66	66	66	66	66	66	66	68	68	68	68	68	69	69	69	70	71	72
7/F	46.45	73	72	70	69	68	68	67	67	67	67	66	66	66	66	69	69	69	69	70	70	70	71	71	72	73
8/F	49.60	74	73	71	70	69	69	68	68	68	67	67	67	67	67	69	70	70	70	71	71	71	71	72	72	73
9/F	52.75	74	73	71	71	70	70	69	69	68	68	68	67	67	67	70	70	70	71	71	71	71	71	72	72	73
10/F	55.90	74	74	72	71	71	70	69	69	69	68	68	68	68	68	70	71	71	71	71	71	71	72	72	72	73
11/F	59.05	74	74	72	72	71	71	70	70	69	69	69	68	68	68	71	71	71	71	71	71	71	72	72	72	73
12/F	62.20	74	74	72	72	72	71	71	70	70	69	69	69	69	69	71	71	71	71	71	71	71	72	72	72	74
13/F	65.35	74	74	73	72	72	71	71	71	71	70	70	69	69	69	71	71	71	71	71	71	71	72	72	72	74
14/F	68.50	75	74	73	72	72	72	71	71	71	71	70	70	69	69	71	71	71	71	71	71	72	72	72	73	74
15/F	71.65	75	74	73	72	72	72	71	71	71	71	71	70	70	70	71	71	71	71	71	71	72	72	72	73	74
16/F	74.80	75	74	73	72	72	72	71	71	71	71	71	70	70	70	71	71	71	71	71	71	72	72	72	73	74
17/F	77.95	75	74	73	72	72	72	72	71	71	71	71	71	70	70	71	71	71	71	71	72	72	72	72	73	74
18/F	81.10	75	74	73	72	72	72	72	71	71	71	71	71	70	70	71	71	71	71	71	72	72	72	72	73	74
19/F	84.25	75	74	73	72	72	72	72	71	71	71	71	71	70	70	71	71	71	71	71	72	72	72	72	73	74
20/F	87.40	75	74	73	72	72	72	72	71	71	71	71	71	71	70	71	71	71	71	71	72	72	72	72	73	74
21/F	90.55	75	74	73	72	72	72	72	71	71	71	71	71	71	70	71	71	71	71	71	72	72	72	72	73	74
22/F	93.70	75	74	73	72	72	72	72	71	71	71	71	71	71	70	71	71	71	71	71	72	72	72	72	73	74
23/F	96.85	74	74	73	72	72	72	72	71	71	71	71	71	71	70	71	71	71	71	71	72	72	72	72	73	74
Exceedance		18	18	16	15	14	13	12	11	11	10	9	8	4	0	13	14	14	15	15	16	16	17	17	18	18
No. of exceedance		18			15		13		11		10		8			14			15	15	16		17	17		18
No. of Premises		23			23		23		23		23		23			23			23	23	23		23	23		23

Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-J2	NB-K1	NB-K2	NB-L1	NB-L2
1/F	27.55	65	65	66	65	65	65	65	65	65	65	65	63	63	63	63	53	61	61	61	62	58	64	66		
2/F	30.70	66	65	66	65	65	65	65	65	65	65	65	63	63	63	62	63	54	62	62	63	60	65	66		
3/F	33.85	66	65	66	65	65	65	65	65	65	65	65	63	63	63	62	63	55	63	63	64	60	65	66		
4/F	37.00	65	65	66	65	65	65	65	65	65	65	65	63	63	63	62	64	56	63	63	64	65	61	66	66	
5/F	40.15	65	65	66	65	65	65	65	65	65	65	65	63	63	63	62	64	57	64	64	65	65	61	66	67	
6/F	43.30	65	65	66	65	65	65	65	65	65	66	66	63	63	63	62	64	57	64	64	65	65	61	66	67	
7/F	46.45	65	65	66	65	65	65	65	65	65	66	64	63	63	63	62	64	58	64	65	65	65	61	66	67	
8/F	49.60	65	66	66	66	66	66	66	65	66	66	64	63	63	63	62	65	58	65	65	65	66	61	67	67	
9/F	52.75	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67		
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	59	66	66	66	67	61	67	68		
11/F	59.05	66	66	67	66	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68		
12/F	62.20	67	67	67	67	67	66	66	66	66	66	64	64	63	63	66	59	66	67	67	67	61	68	68		
13/F	65.35	67	67	68	67	67	67	67	67	66	66	64	64	63	63	66	60	67	67	67	67	62	68	68		
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	66	60	67	67	68	68	62	68	69		
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	62	68	69		
16/F	74.80	68	69	69	68	68	68	68	67	67	67	65	64	63	63	67	61	68	68	68	68	63	68	69		
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	63	63	67	61	68	68	68	68	63	69	69		
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69		
19/F	84.25	69	69	70	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69		
20/F	87.40	69	69	70	69	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69		
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69		
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69		
23/F	96.85	69	70	70	70	70	69	69	69	69	69	66	64	64	63	68	62	68	68	68	68	63	68	69		
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
No. of exceedance		0				0		0			0						0		0	0	0	0	0	0		
No. of Premises		23				23		23		23		23		23			23		23	23	23	23		23		

Floor	mPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-J3				
1/F	27.55	67	69	70	70	70	70	70	70	70	70	69	69	69	69	67	63	63	63	62	62	59	63	60	61				
2/F	30.70	67	69	69	69	69	69	69	69	69	70	69	69	69	69	67	63	63	63	62	62	59	63	61	62				
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	63	64	63	62	62	59	64	62	63				
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	62	62	59	64	63	64				
5/F	40.15	67	68	69	68	68	68	68	68	68	69	68	68	68	68	67	64	64	63	62	62	59	64	63	64				
6/F	43.30	67	68	68	68	68	68	68	68	68	69	68	68	68	68	66	64	64	63	62	62	59	65	64	65				
7/F	46.45	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	62	62	59	65	64	65				
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	63	62	62	59	65	65	66				
9/F	52.75	67	67	68	68	68	68	68	68	68	68	67	67	67	67	66	64	64	63	63	62	59	65	65	66				
10/F	55.90	67	67	68	68	68	67	67	68	67	68	67	67	67	67	66	64	64	63	63	63	59	65	66	66				
11/F	59.05	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	66				
12/F	62.20	67	67	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	63	63	59	66	66	67				
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	64	59	67	67	67				
14/F	68.50	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	67				
15/F	71.65	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	68				
16/F	74.80	68	68	69	68	68	68	68	68	67	68	66	66	67	67	66	64	65	64	64	64	60	68	68	68				
17/F	77.95	69	69	69	69	68	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68				
18/F	81.10	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68				
19/F	84.25	69	69	69	69	69	69	68	68	68	68	66	66	67	67	66	64	65	65	65	65	60	68	68	68				
20/F	87.40	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68				
21/F	90.55	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68				
22/F	93.70	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68				
23/F	96.85	69	69	69	69	69	69	68	68	68	69	66	66	67	67	66	64	65	65	65	65	61	68	68	68				
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
No. of exceedance		0				0				0				0				0				0				0			
No. of Premises		23				23				23				23				23				23				23			

Predicted Road Traffic Noise at Selected Sensitive Receivers Mitigated Scenario (AM)

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	27.55	58	61	63	64	65	65	63	63	65	65	63	63	66	65	67	66	63	64	63	59	60	56	55	53	55
2/F	30.70	60	62	63	64	66	66	64	64	66	66	64	64	66	65	67	67	64	65	64	60	61	58	57	55	58
3/F	33.85	62	63	64	64	66	66	64	64	66	66	64	64	67	65	68	67	65	66	65	61	62	60	60	58	61
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	65	67	65	68	68	65	66	66	63	64	63	64	62	65
5/F	40.15	69	68	65	65	67	67	65	64	66	66	64	65	67	65	68	68	66	67	67	65	67	67	68	66	70
6/F	43.30	72	71	66	66	68	67	65	65	67	67	65	65	67	65	68	69	67	69	69	67	69	69	70	69	72
7/F	46.45	73	72	68	67	68	68	66	65	67	67	65	65	67	65	69	69	68	69	70	68	70	71	71	70	73
8/F	49.60	74	73	69	68	69	69	66	66	68	68	65	65	67	65	69	70	68	70	70	69	71	71	72	70	73
9/F	52.75	74	73	69	69	70	69	67	67	68	68	66	66	67	66	69	70	69	71	71	69	71	71	72	70	73
10/F	55.90	74	73	70	69	71	70	68	67	69	69	66	66	68	66	69	71	69	71	71	69	71	72	72	70	73
11/F	59.05	74	74	70	70	71	71	68	68	70	69	67	67	68	66	70	71	69	71	71	69	71	72	72	70	74
12/F	62.20	74	74	70	70	72	71	69	68	70	70	67	67	69	67	70	71	69	71	71	69	71	72	72	71	74
13/F	65.35	75	74	71	70	72	71	69	69	70	70	68	68	69	67	70	71	69	71	71	69	72	72	72	71	74
14/F	68.50	75	74	71	70	72	72	69	69	71	71	68	68	70	68	70	71	69	71	71	69	72	72	72	71	74
15/F	71.65	75	74	71	70	72	72	69	69	71	71	69	68	70	68	70	71	69	71	71	69	72	72	72	71	74
16/F	74.80	75	74	71	70	72	72	69	69	71	71	69	69	70	69	70	71	69	71	71	69	72	72	72	71	74
17/F	77.95	75	74	71	70	72	72	70	69	71	71	69	69	70	69	70	71	69	71	71	70	72	72	72	71	74
18/F	81.10	75	74	71	70	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
19/F	84.25	75	74	71	70	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
20/F	87.40	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
21/F	90.55	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
22/F	93.70	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
23/F	96.85	75	74	71	71	72	72	70	70	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
Exceedance		18	18	11	4	14	13	0	0	10	10	0	0	6	0	0	14	0	15	15	0	16	17	17	12	18
No. of exceedance		18			14		13		10		10			6			14		15	15	16		17	17		18
No. of Premises		23			23		23		23		23			23			23		23	23	23		23	23		23

Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2
1/F	27.55	66	65	66	66	66	65	65	65	65	66	63	63	63	63	63	53	61	61	62	62	58	64	66
2/F	30.70	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	54	62	62	63	63	60	65	66
3/F	33.85	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	55	63	63	63	64	60	65	66
4/F	37.00	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	55	63	64	64	65	60	66	67
5/F	40.15	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	56	64	64	65	65	61	66	67
6/F	43.30	66	66	66	66	66	65	65	65	65	66	64	63	63	63	65	57	64	65	65	66	61	66	67
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	65	66	61	66	67
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	58	65	66	66	67	61	67	68
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	65	59	66	66	67	67	61	67	68
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68
12/F	62.20	67	67	67	67	67	67	66	66	66	67	64	64	63	63	66	59	66	67	67	67	61	68	68
13/F	65.35	67	67	68	67	67	67	67	67	66	67	64	64	63	63	66	60	67	67	68	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	69
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	63	69	69
16/F	74.80	68	68	69	68	68	68	68	67	67	67	65	64	64	63	67	61	68	68	68	68	63	69	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
19/F	84.25	69	69	69	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
20/F	87.40	69	69	70	69	69	69	69	69	68	69	65	64	64	63	68	61	68	68	68	68	63	69	69
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
23/F	96.85	69	70	70	70	70	69	69	69	69	69	66	64	64	64	68	62	68	68	68	68	63	69	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		0				0		0		0		0		0		0		0		0		0		0
No. of Premises		23				23		23		23		23		23		23		23		23		23		23

[illegible]

Max. Noise Level =	75
Total no. of Exceedance =	183
Total no. of Premises =	759
%Compliance =	76%

Applied with Acoustic Balcony (-2dB(A))

Predicted Road Traffic Noise at Selected Sensitive Receivers Mitigated Scenario (PM)

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	27.55	58	61	63	63	65	65	63	63	64	65	63	63	65	65	67	66	63	64	63	58	59	56	55	53	54
2/F	30.70	60	62	63	63	65	65	63	63	65	65	63	64	66	65	67	67	64	65	64	60	60	58	57	55	57
3/F	33.85	62	63	63	64	66	66	64	64	65	66	64	64	66	65	67	67	64	66	65	61	62	60	60	58	61
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	64	66	65	68	68	65	66	66	62	64	63	64	62	65
5/F	40.15	69	68	65	65	66	66	64	64	66	66	64	64	66	65	68	68	66	67	67	65	67	67	68	66	70
6/F	43.30	72	71	66	66	67	67	64	64	66	66	64	64	66	65	68	68	66	68	68	67	69	69	70	69	72
7/F	46.45	73	72	68	67	68	68	65	65	67	67	64	64	66	65	68	69	67	69	70	68	70	70	71	70	73
8/F	49.60	74	73	69	68	69	69	66	66	67	67	65	65	67	65	69	70	68	70	70	68	71	71	71	70	73
9/F	52.75	74	73	69	69	70	69	67	66	68	68	66	65	67	65	69	70	68	71	71	69	71	71	72	70	73
10/F	55.90	74	74	70	69	71	70	68	67	69	69	66	66	68	66	69	70	69	71	71	69	71	71	72	70	73
11/F	59.05	74	74	70	70	71	71	68	68	69	69	67	67	68	66	69	71	69	71	71	69	71	72	72	70	73
12/F	62.20	74	74	70	70	71	71	69	68	70	70	67	67	69	67	69	71	69	71	71	69	71	72	72	70	74
13/F	65.35	74	74	71	70	72	71	69	69	70	70	68	68	69	67	69	71	69	71	71	69	71	72	72	70	74
14/F	68.50	75	74	71	70	72	72	69	69	71	71	68	68	70	68	69	71	69	71	71	69	71	72	72	71	74
15/F	71.65	75	74	71	70	72	72	69	69	71	71	68	68	70	68	69	71	69	71	71	69	72	72	72	71	74
16/F	74.80	75	74	71	70	72	72	69	69	71	71	69	68	70	69	69	71	69	71	71	69	72	72	72	71	74
17/F	77.95	75	74	71	70	72	72	69	69	71	71	69	69	70	69	69	71	69	71	71	69	72	72	72	71	74
18/F	81.10	75	74	71	70	72	72	69	69	71	71	69	69	70	69	69	71	69	71	71	69	72	72	72	71	74
19/F	84.25	75	74	71	70	72	72	70	69	71	71	69	69	70	69	69	71	69	71	71	69	72	72	72	71	74
20/F	87.40	75	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	69	72	72	72	71	74
21/F	90.55	75	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	69	72	72	72	71	74
22/F	93.70	74	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	69	72	72	72	71	74
23/F	96.85	74	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	69	72	72	72	71	74
Exceedance		18	18	11	0	14		13	0	0	10	10	0	4	0	0	13	0	15	15	0	16	16	17	10	18
No. of exceedance		18			14			13		10			4				13		15	15	16		16	17		18
No. of Premises		23			23			23		23		23	23				23		23	23	23		23	23		23

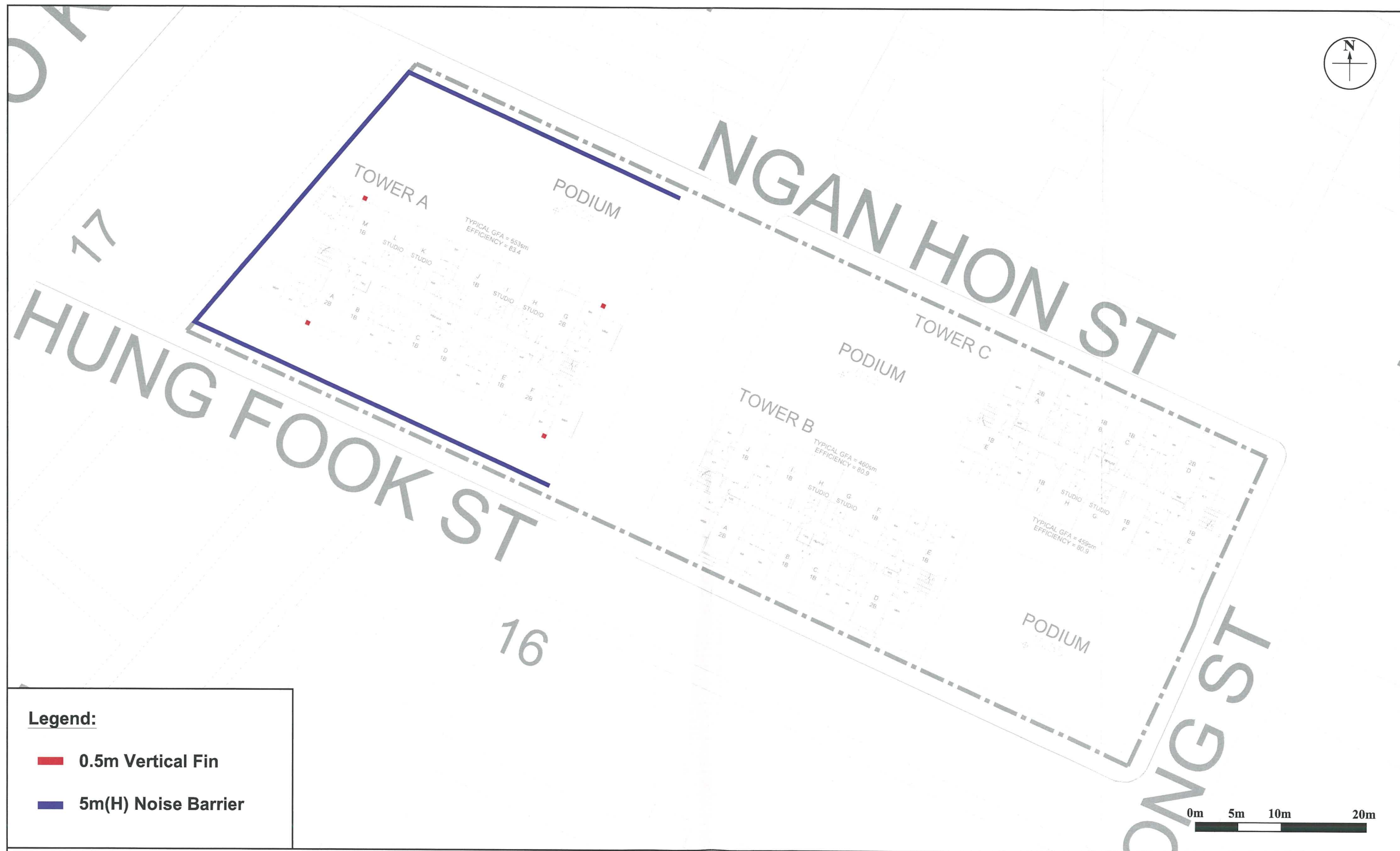
Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2	
1/F	27.55	65	65	66	65	65	65	65	65	65	65	63	62	63	63	63	53	61	61	61	62	58	64	66	
2/F	30.70	66	65	66	65	65	65	65	65	65	65	63	63	63	62	63	54	62	62	62	63	60	65	66	
3/F	33.85	66	65	66	65	65	65	65	65	65	65	63	63	63	62	63	55	63	63	63	64	60	65	66	
4/F	37.00	65	65	66	65	65	65	65	65	65	65	63	63	63	62	64	55	63	63	64	64	60	66	66	
5/F	40.15	65	65	66	65	65	65	65	65	65	65	63	63	63	62	64	56	63	64	64	65	61	66	66	
6/F	43.30	65	65	66	65	65	65	65	65	65	66	63	63	63	62	64	57	64	64	65	65	61	66	66	
7/F	46.45	65	65	66	65	65	65	65	65	65	66	64	63	63	62	64	58	64	65	65	65	61	66	67	
8/F	49.60	65	65	66	66	66	66	66	65	66	66	64	63	63	62	65	58	65	65	65	66	61	66	67	
9/F	52.75	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67	
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	59	65	66	66	67	61	67	68	
11/F	59.05	66	66	67	66	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	67	68	
12/F	62.20	67	67	67	67	67	66	66	66	66	66	64	64	63	63	66	59	66	67	67	67	61	68	68	
13/F	65.35	67	67	68	67	67	67	67	66	66	66	67	64	64	63	63	66	60	67	67	67	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	66	60	67	67	68	68	62	68	68	
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	67	68	68	68	62	68	68	
16/F	74.80	68	69	69	68	68	68	68	67	67	67	65	64	63	63	67	61	68	68	68	68	63	68	68	
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	63	63	67	61	68	68	68	68	63	68	68	
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	67	61	68	68	68	68	63	68	68	
19/F	84.25	69	69	70	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	68	
20/F	87.40	69	69	70	69	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	68	
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	68	
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	68	
23/F	96.85	69	70	70	70	70	69	69	69	69	69	66	64	64	63	68	62	68	68	68	68	63	68	68	
Exceedance		0	0	0	0		0	0		0	0		0	0	0	0		0	0		0	0		0	0
No. of exceedance			0				0		0		0		0		0			0		0				0	
No. of Premises			23				23		23		23		23		23		23		23		23			23	

[illegible]

Max. Noise Level =	75
Total no. of Exceedance =	179
Total no. of Premises =	759
%Compliance =	76%

Applied with Acoustic Balcony (-2dB(A))

APPENDIX 2.3: SENSITIVE TEST OF THE NOISE PERFORMANCE FOR THE NOISE BARRIER OR CANOPY AT PODIUM



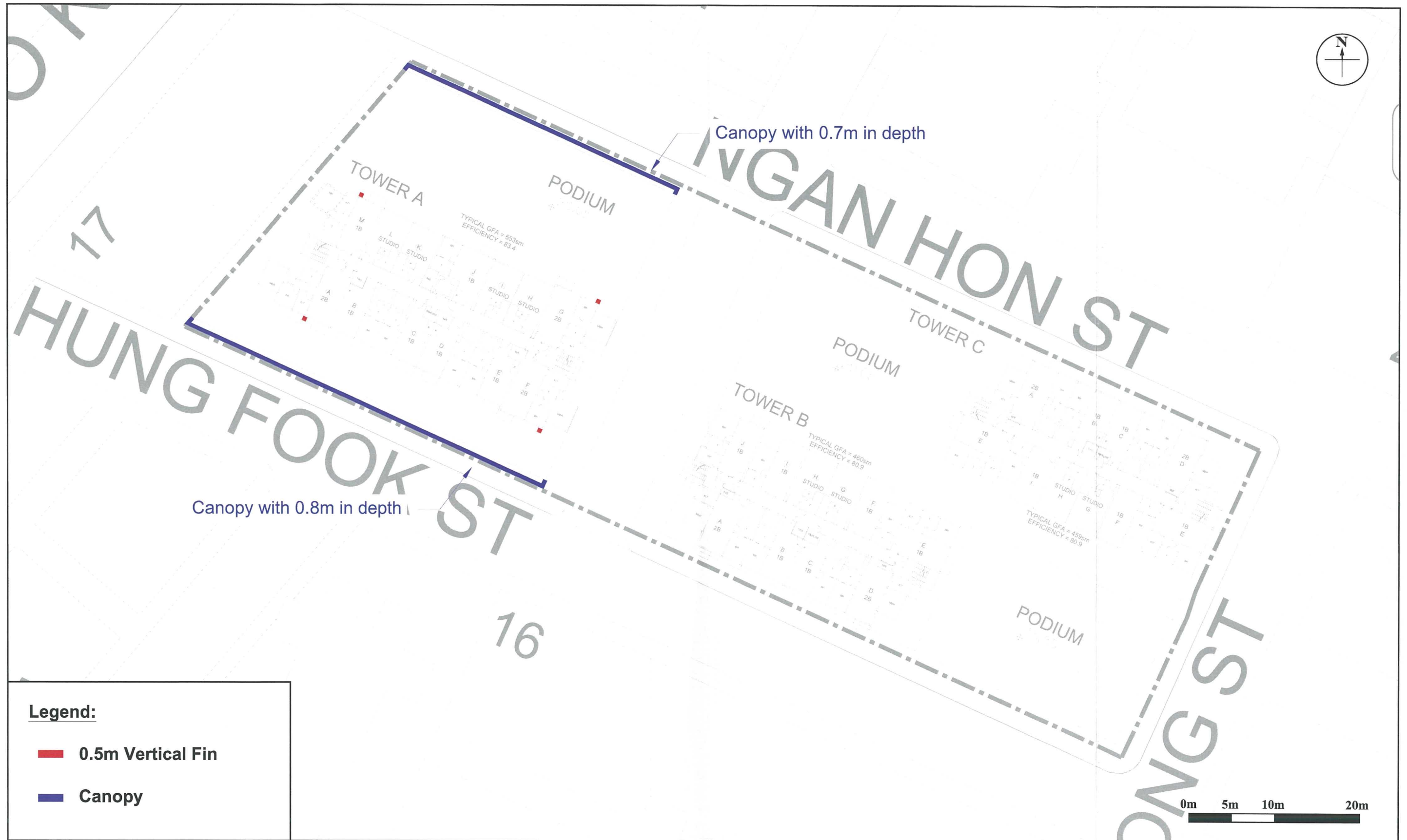
Appendix: 2.3a		RAMBOLL ENVIRON	
Title: Location of Noise Barrier at Podium for the Sensitivity Test (Scenario with Noise Barrier)		Drawn by: JC	
		Checked by: TC	
Project: Environmental Assessment for URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City		Rev.: 1.0	
		Date: Jul 2016	

Predicted Road Traffic Noise at Selected Sensitive Receivers
Mitigated Scenario (AM)

Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2	
1/F	27.55	55	59	61	61	63	62	60	60	61	61	59	60	63	63	66	65	61	62	61	55	55	54	54	52	53	
2/F	30.70	58	61	62	63	65	65	62	62	64	64	62	62	65	64	67	66	63	64	62	57	58	57	56	54	57	
3/F	33.85	61	62	63	64	65	65	63	63	65	65	63	63	66	65	67	67	64	65	64	60	61	59	59	57	60	
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	64	66	65	67	67	64	65	65	62	64	63	63	62	65	
5/F	40.15	69	68	65	65	67	66	64	64	66	66	64	64	66	65	67	68	65	67	67	65	67	67	68	66	70	
6/F	43.30	72	71	66	66	67	67	65	65	66	66	64	65	66	65	68	68	66	68	68	66	69	69	70	69	72	
7/F	46.45	73	72	68	67	68	68	65	65	67	67	65	65	67	65	69	69	67	69	70	68	70	70	71	70	73	
8/F	49.60	74	73	69	68	69	69	66	66	68	67	65	65	67	65	69	70	68	70	70	68	71	71	71	70	73	
9/F	52.75	74	73	69	69	70	69	67	67	68	68	66	66	67	66	69	70	68	70	71	69	71	71	72	70	73	
10/F	55.90	74	73	70	69	71	70	68	67	69	69	66	66	68	66	69	70	68	71	71	69	71	71	72	70	73	
11/F	59.05	74	74	70	70	71	71	68	68	69	69	67	67	68	66	69	71	69	71	71	69	71	71	72	70	73	
12/F	62.20	74	74	70	70	72	71	69	68	70	70	67	67	69	67	69	71	69	71	71	69	71	71	72	70	74	
13/F	65.35	75	74	71	70	72	71	69	69	70	70	68	68	69	67	69	71	69	71	71	69	71	72	72	70	74	
14/F	68.50	75	74	71	70	72	72	69	69	71	71	68	68	70	68	69	71	69	71	71	69	71	72	72	71	74	
15/F	71.65	75	74	71	70	72	72	69	69	71	71	69	68	70	68	69	71	69	71	71	69	72	72	72	71	74	
16/F	74.80	75	74	71	70	72	72	69	69	71	71	69	69	70	69	69	71	69	71	71	69	72	72	72	71	74	
17/F	77.95	75	74	71	70	72	72	70	69	71	71	69	69	70	69	69	71	69	71	71	69	72	72	72	71	74	
18/F	81.10	75	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	69	72	72	72	71	74	
19/F	84.25	75	74	71	70	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	70	72	72	72	71	74	
20/F	87.40	75	74	71	71	72	72	70	69	71	71	69	69	71	69	69	71	69	71	71	70	72	72	72	71	74	
21/F	90.55	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74	
22/F	93.70	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74	
23/F	96.85	75	74	71	71	72	72	70	70	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74	
Exceedance		18	18	11	4	14	13	0	0	10	10	0	0	6	0	0	13	0	14	15	0	16	16	17	10	18	
No. of exceedance		18				14				13				10				10				16				18	
No. of Premises		23				23				23				23				23				23				23	

Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2	
1/F	27.55	65	65	66	66	66	65	65	65	65	66	63	63	63	63	61	53	58	58	59	58	62	65		
2/F	30.70	65	66	66	66	65	65	65	65	65	66	63	63	63	63	62	54	60	60	61	62	60	64	66	
3/F	33.85	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	55	62	62	63	64	60	65	66	
4/F	37.00	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	55	63	63	63	64	60	65	66	
5/F	40.15	66	66	66	66	65	65	65	65	65	66	63	63	63	63	64	56	63	64	64	65	61	65	66	
6/F	43.30	66	66	66	66	66	65	65	65	65	66	64	63	63	63	64	57	64	64	64	65	61	66	66	
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	64	58	64	64	65	65	61	66	67	
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	65	66	61	67	67	
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	65	58	65	65	66	66	61	67	67	
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	65	59	66	66	66	67	61	67	68	
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	61	68	68	
12/F	62.20	67	67	67	67	67	67	66	66	66	67	64	64	63	63	66	59	66	67	67	67	61	68	68	
13/F	65.35	67	67	68	67	67	67	67	67	66	67	64	64	63	63	66	60	67	67	67	68	62	68	68	
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	68	
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	62	68	69	
16/F	74.80	68	68	69	68	68	68	68	67	67	67	65	64	64	63	67	61	68	68	68	68	62	69	69	
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69	
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	68	69	
19/F	84.25	69	69	69	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69	
20/F	87.40	69	69	70	69	69	69	69	69	68	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	68	69	
23/F	96.85	69	70	70	70	70	69	69	69	69	69	66	64	64	64	68	62	68	68	68	68	63	69	69	
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
No. of exceedance		0				0				0				0				0				0			
No. of Premises		23				23				23				23				23				23			

Floor	mPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-J3				
1/F	27.55	67	69	70	70	70	70	70	70	70	70	69	69	69	69	68	63	64	63	63	62	59	61	56	60				
2/F	30.70	67	69	70	69	69	69	69	69	69	70	69	69	69	69	67	63	64	63	63	62	59	62	60	61				
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	69	68	67	64	64	63	63	62	59	63	62	63				
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	63	62	59	64	62	63				
5/F	40.15	67	68	69	68	68	68	68	68	69	69	68	68	68	68	67	64	64	63	63	62	59	64	63	64				
6/F	43.30	67	68	68	68	68	68	68	68	68	69	68	68	68	68	67	64	64	63	63	62	59	64	64	65				
7/F	46.45	67	67	68	68	68	68	68	68	68	69	67	68	68	68	66	64	64	63	63	62	59	65	64	65				
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	63	62	59	65	65	66				
9/F	52.75	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	63	63	62	59	65	65	66				
10/F	55.90	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	64	63	63	59	66	66	66				
11/F	59.05	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	66				
12/F	62.20	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	67				
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	64	59	67	67	67				
14/F	68.50	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	67				
15/F	71.65	68	68	68	68	68	68	68	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	68				
16/F	74.80	68	68	69	69	68	68	68	68	68	68	66	67	67	67	66	64	65	64	64	64	60	68	68	68				
17/F	77.95	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	64	60	68	68	68				
18/F	81.10	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68				
19/F	84.25	69	69	69	69	69	69	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68				
20/F	87.40	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69				
21/F	90.55	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69				
22/F	93.70	69	69	69	69	69	69	69	69	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	68				
23/F	96.85	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	69				
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
No. of exceedance		0				0				0				0				0				0				0			
No. of Premises		23				23				23				23				23				23				23			



Predicted Road Traffic Noise at Selected Sensitive Receivers
Mitigated Scenario (AM)




Floor	mPD	NA-A1	NA-A2	NA-A3	NA-B1	NA-B2	NA-C1	NA-C2	NA-D1	NA-D2	NA-E1	NA-E2	NA-F1	NA-F2	NA-F3	NA-G1	NA-G2	NA-G3	NA-H1	NA-I1	NA-J1	NA-J2	NA-K1	NA-L1	NA-M1	NA-M2
1/F	27.55	58	61	63	63	65	65	63	63	65	65	63	63	65	65	67	66	63	64	63	58	59	56	55	52	54
2/F	30.70	60	62	63	64	66	66	64	63	65	65	63	64	66	65	67	67	64	65	64	60	60	58	57	55	57
3/F	33.85	62	63	63	64	66	66	64	64	66	66	64	64	66	65	68	67	65	66	65	61	62	60	60	58	61
4/F	37.00	65	65	64	64	66	66	64	64	66	66	64	64	66	65	68	68	65	66	66	63	64	63	63	62	65
5/F	40.15	69	68	65	65	67	66	64	64	66	66	64	65	67	65	68	68	66	67	67	65	67	67	68	66	70
6/F	43.30	72	71	66	66	68	67	65	65	67	67	64	65	67	65	68	69	67	68	68	67	69	69	70	69	72
7/F	46.45	73	72	68	67	68	68	66	65	67	67	65	65	67	65	69	69	67	69	70	68	70	70	71	70	73
8/F	49.60	74	73	69	68	69	69	66	66	68	68	65	65	67	65	69	70	68	70	70	68	71	71	71	70	73
9/F	52.75	74	73	69	69	70	69	67	67	68	68	66	66	67	66	69	70	68	70	71	69	71	71	72	70	73
10/F	55.90	74	73	70	69	71	70	68	67	69	69	66	66	68	66	69	70	69	71	71	69	71	71	72	70	73
11/F	59.05	74	74	70	70	71	71	68	68	70	69	67	67	68	66	69	71	69	71	71	69	71	72	72	70	74
12/F	62.20	74	74	70	70	72	71	69	68	70	70	67	67	69	67	70	71	69	71	71	69	71	72	72	70	74
13/F	65.35	75	74	71	70	72	71	69	69	70	70	68	68	69	67	70	71	69	71	71	69	71	72	72	71	74
14/F	68.50	75	74	71	70	72	72	69	69	71	71	68	68	70	68	70	71	69	71	71	69	72	72	72	71	74
15/F	71.65	75	74	71	70	72	72	69	69	71	71	69	68	70	68	70	71	69	71	71	69	72	72	72	71	74
16/F	74.80	75	74	71	70	72	72	69	69	71	71	69	69	70	69	70	71	69	71	71	69	72	72	72	71	74
17/F	77.95	75	74	71	70	72	72	70	69	71	71	69	69	70	69	70	71	69	71	71	69	72	72	72	71	74
18/F	81.10	75	74	71	70	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
19/F	84.25	75	74	71	70	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
20/F	87.40	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
21/F	90.55	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
22/F	93.70	75	74	71	71	72	72	70	69	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
23/F	96.85	75	74	71	71	72	72	70	70	71	71	69	69	71	69	70	71	69	71	71	70	72	72	72	71	74
Exceedance		18	18	11	4	14	13	0	0	10	10	0	0	6	0	0	13	0	14	15	0	16	16	17	11	18
No. of exceedance		18			14		13		10		10			6			13		14	15		16		17		18
No. of Premises		23			23		23		23		23			23			23		23	23		23		23		23




Floor	mPD	NB-A1	NB-A2	NB-A3	NB-A4	NB-B1	NB-B2	NB-C1	NB-C2	NB-D1	NB-D2	NB-D3	NB-D4	NB-E1	NB-E2	NB-E3	NB-F1	NB-F2	NB-G1	NB-H1	NB-I1	NB-I2	NB-J1	NB-J2
1/F	27.55	66	65	66	66	66	65	65	65	65	66	63	63	63	63	63	53	61	61	62	62	58	64	66
2/F	30.70	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	54	62	62	63	63	60	65	66
3/F	33.85	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	64	55	63	63	63	64	60	65
4/F	37.00	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	64	55	63	63	64	65	60	66
5/F	40.15	66	66	66	66	65	65	65	65	65	66	63	63	63	63	63	64	56	64	64	65	65	60	67
6/F	43.30	66	66	66	66	66	65	65	65	65	66	64	63	63	63	63	64	57	64	64	65	65	60	67
7/F	46.45	66	66	66	66	66	66	66	66	66	66	64	63	63	63	63	65	58	65	65	65	66	60	67
8/F	49.60	66	66	66	66	66	66	66	66	66	66	64	63	63	63	63	65	58	65	65	66	66	61	67
9/F	52.75	66	66	67	66	66	66	66	66	66	66	64	63	63	63	63	65	58	65	66	66	66	61	67
10/F	55.90	66	66	67	66	66	66	66	66	66	66	64	64	63	63	63	65	59	66	66	67	67	61	68
11/F	59.05	66	67	67	67	66	66	66	66	66	66	64	64	63	63	66	59	66	66	67	67	67	61	68
12/F	62.20	67	67	67	67	67	67	66	66	66	67	64	64	63	63	66	59	66	67	67	67	67	61	68
13/F	65.35	67	67	68	67	67	67	67	67	67	67	64	64	63	63	66	60	67	67	68	68	62	68	68
14/F	68.50	68	68	68	68	67	67	67	67	67	67	64	64	63	63	67	60	67	68	68	68	62	68	69
15/F	71.65	68	68	68	68	68	68	67	67	67	67	64	64	63	63	67	61	68	68	68	68	62	69	69
16/F	74.80	68	68	69	68	68	68	68	67	67	67	65	64	64	63	67	61	68	68	68	68	62	69	69
17/F	77.95	69	69	69	69	69	68	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
18/F	81.10	69	69	69	69	69	69	68	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
19/F	84.25	69	69	69	69	69	69	69	68	68	68	65	64	64	63	68	61	68	68	68	68	63	69	69
20/F	87.40	69	69	70	69	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
21/F	90.55	69	69	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
22/F	93.70	69	70	70	70	69	69	69	69	69	69	65	64	64	63	68	61	68	68	68	68	63	69	69
23/F	96.85	69	70	70	70	70	69	69	69	69	69	66	64	64	64	68	62	68	68	68	68	63	69	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		0				0		0			0					0		0		0		0		0
No. of Premises		23				23		23			23					23		23		23		23		23




Floor	mPD	NC-A1	NC-A2	NC-A3	NC-A4	NC-B1	NC-B2	NC-C1	NC-C2	NC-D1	NC-D2	NC-D3	NC-D4	NC-E1	NC-E2	NC-E3	NC-F1	NC-F2	NC-G1	NC-H1	NC-I1	NC-I2	NC-J1	NC-J2	NC-J3
1/F	27.55	67	69	70	70	70	70	70	70	70	70	69	69	69	69	68	63	64	63	63	62	59	63	61	61
2/F	30.70	67	69	70	69	69	69	69	69	69	70	69	69	69	69	67	63	64	63	63	62	59	63	62	62
3/F	33.85	67	68	69	69	69	69	69	69	69	69	68	68	69	68	67	64	64	63	63	62	59	64	63	63
4/F	37.00	67	68	69	69	69	69	69	69	69	69	68	68	68	68	67	64	64	63	63	62	59	64	63	64
5/F	40.15	67	68	69	68	68	68	68	68	69	69	68	68	68	68	67	64	64	63	63	62	59	65	64	64
6/F	43.30	67	68	68	68	68	68	68	68	68	69	68	68	68	68	67	64	64	63	63	62	59	65	64	65
7/F	46.45	67	67	68	68	68	68	68	68	68	69	67	68	68	68	66	64	64	63	63	62	59	65	64	65
8/F	49.60	67	67	68	68	68	68	68	68	68	68	67	68	68	68	66	64	64	63	63	62	59	65	65	66
9/F	52.75	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	63	63	62	59	65	65	66
10/F	55.90	67	67	68	68	68	68	68	68	68	68	67	67	68	67	66	64	64	64	63	63	59	66	66	66
11/F	59.05	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	66
12/F	62.20	67	67	68	68	68	67	67	67	67	68	67	67	67	67	66	64	64	64	63	63	59	66	66	67
13/F	65.35	67	68	68	68	68	67	67	67	67	68	66	67	67	67	66	64	64	64	64	64	59	67	67	67
14/F	68.50	68	68	68	68	68	68	67	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	67	67
15/F	71.65	68	68	68	68	68	68	68	67	67	68	66	67	67	67	66	64	64	64	64	64	60	67	68	68
16/F	74.80	68	68	69	69	68	68	68	68	68	68	66	67	67	67	66	64	65	64	64	64	60	68	68	68
17/F	77.95	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	64	60	68	68	68
18/F	81.10	69	69	69	69	69	68	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68
19/F	84.25	69	69	69	69	69	69	68	68	68	68	66	66	67	67	66	64	65	65	64	65	60	68	68	68
20/F	87.40	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
21/F	90.55	69	69	69	69	69	69	69	68	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
22/F	93.70	69	69	69	69	69	69	69	69	68	69	66	66	67	67	66	64	65	65	65	65	60	68	68	69
23/F	96.85	69	69	69	69	69	69	69	69	69	69	66	66	67	67	66	64	65	65	65	65	61	68	68	69
Exceedance		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
No. of exceedance		0				0		0		0		0		0		0		0		0		0		0	
No. of Premises		23				23		23		23		23		23		23		23		23		23		23	

APPENDIX 2.4: PHOTO RECORDS OF SITE SURVEYS

Location ID	Identified Noise Source
S1	<p data-bbox="300 344 767 376">Fuji Engineering (H.K.) Company Limited</p> 
S2	<p data-bbox="300 866 804 898">Jointly Motors Centre Vehicles Maintenance</p> 
S3	<p data-bbox="300 1388 847 1420">BP Car Shop Vehicles Maintenance & Repairing</p> 

Location ID	Identified Noise Source
S4	<p>Top Gear Vehicles Maintenance & Repairing</p> 
S5	<p>Kowloon Auto Service & Maintenance Company</p> 
S6	<p>Yau Shing Auto Service & Maintenance Company Limited</p> 

Location ID	Identified Noise Source
S7	<p data-bbox="296 286 847 315">Auto Service Vehicles Maintenance & Repairing</p>  <p>The photograph shows the entrance of an auto service center. A white sedan is parked in the foreground on the right. Inside the open bay, another white car is being worked on by a person. The sign above the entrance reads '祥利汽車服務' (Cheong Lee Auto Service) and 'AUTO SERVICE BODYWORK CENTRE'.</p>
S8	<p data-bbox="296 835 544 864">MEGA Racing Centre</p>  <p>The photograph shows the storefront of the MEGA Racing Centre. A white van is parked on the left, and a person on a scooter is on the right. The sign above the entrance reads 'MEGA RACING CENTRE' and '加路汽車有限公司' (Gale Auto Co., Ltd.).</p>
S9	<p data-bbox="296 1373 539 1402">Recycling Workshop</p>  <p>The photograph shows the interior of a recycling workshop. The space is cluttered with various items, including boxes, bags, and other debris, indicating a recycling or waste processing area.</p>

Location ID	Identified Noise Source
S10	<p>Chun Tung Garage Vehicles Maintenance & Repairing</p> 
L1	<p>The World-wide Industrial Building</p> 
L2	<p>Fung Yu Industrial Building</p> 

APPENDIX 2.5: EXTRACTED PAGES FROM THE NIA-MTK REPORT

1.0 DESCRIPTION

Allied Environmental Consultants Limited was commissioned to conduct the noise impact assessment for the Proposed Comprehensive Residential Development at KIL 10578, 7628 and 7626 in Ma Tau Kok. A noise measurement was conducted to obtain the noise level produced by the motor services workshops under operation, which are identified as the fixed noise sources in the vicinity, for the fixed plant noise assessment.

2.0 SITE SURVEY AND NOISE MEASUREMENT

2.1 DATE AND TIME

Several site surveys for daytime and nighttime were conducted on 21 September 2015, 26 February, 27 February, 1 March, 2 March and 17 March 2016. Noise measurement was conducted on 17 March 2016 to obtain noise data for evaluating the industrial noise impact. Photographs taken on site are given in *Appendix J*.

2.2 SURVEY PERSONNEL

The following personnel took part in the measurements.

Mr. Ronan Chan	Consultant (AEC)
Ms. Cathy Man	Senior Consultant (AEC)

2.3 WEATHER CONDITIONS

The weather conditions on site were checked to ensure the measurements were made only during "dry" weather conditions without the presence of fog and rain. The wind speed had been checked to ensure that the wind speed did not exceed 5m/s and 10m/s in any direction for steady and gusty wind respectively.

2.4 SITE OBSERVATIONS

The area consists primarily of domestic premises and industrial/ office buildings. There are a recycling workshop, a number of motor services workshops and restaurants on ground floor. *Figure 6* shows the location of the identified noise sources. The inventory of fixed noise sources is given in *Table 2-1*.

The restaurants are not considered as industrial noise sources. There are several cooling towers and chillers identified at the roof of the industrial buildings in the vicinity. These noise sources are excluded from calculation. It can be referred to Section 7.1 of the Noise Impact Assessment Report for details.

The recycling workshop (S16) is located at Mok Cheong Street opposite to the Subject Site. As observed and confirmed by the operators of the recycling workshop, only

trading of materials and materials handling are carried out in the workshop. There are no major operations (i.e. repair and maintenance works with tools/ machines) carried out. Hence it is not considered as an industrial noise source.

A number of motor services workshops are the potential noise sources that might impose impact on the proposed Development. As observed during the site inspections and confirmed by the operators, typical operations at the workshops include manual operations as well as tools/machine assisted operations (i.e. Hand-held Pneumatic Tools, Vehicle lifting machine, hammering, grinding), which are short in duration (in less than 5 minutes).

Based on our several site inspections conducted in Feb 2016 and March 2016, it is observed that most works at motor services workshops were carried out in the afternoon. It is also confirmed by the operators of the motor services workshops that afternoon, around 14:00 to 15:00 hours, are the busiest operation time. It is also observed during the night time site visits and confirmed by the operators that there is no nighttime operation at the motor services workshops.

Inventory of identified fixed noise sources is provided below.

Table 2-1 Inventory of fixed noise sources

Source ID	Source Type	Quantity	Location	Shortest Horizontal Distance to Site Boundary (m)	Remark
S1	Cooling Tower	6	Freder Centre	3	U.B.W. and excluded from assessment
S2	Cooling Tower	1	Wacker Factory	22	U.B.W. and excluded from assessment
S3	Chillers	2	DHL House	40	Abandoned
S4	Motor services workshop	1	20 Fung Yi Street	42	No night time operation
S5	Motor services workshop	1	18 Fung Yi Street (禧記)	50	
S6	Motor services workshop	1	23 Lun Cheung Street	34	
S7	Motor services workshop	1	26 Ying Yeung Street (遠東- 新興)	36	
S8	Motor services workshop	1	24 Ying Yueng Street (名城)	47	

S9	Motor services workshop	1	23 Ying Yueng Street	42
S10	Motor services workshop	1	19 Ying Yueng Street(恒進)	53
S11	Motor services workshop	1	15 Ying Yueng Street (合益)	66
S12	Motor services workshop	1	28 Pang Ching Street (龍華)	16
S13	Motor services workshop	1	24 Hung Wan Street	72
S14	Motor services workshop	1	25-27 Hung Wan Street	39
S15	Motor services workshop	1	27 Shim Luen Street	59
S16	Recycling Workshop	1	Mok Cheong Street (銘興五金)	12
S17	Motor services workshop	1	Mok Cheong Street (新張記)	50
S18	Motor services workshop	1	Mok Cheong Street (洗車達人)	29

Note:
U.B.W.: Unauthorized Building Works

2.5 NOISE MEASUREMENT

Noise measurements have been conducted on 17 March 2016 in both non traffic peak time (around 12:00 to 13:00 hours) and busiest time of operation, with all of the operation activities, i.e. Hand-held Pneumatic Tools, Vehicle lifting machine, hammering, grinding, captured during noise survey.

Noise survey was carried out at site boundary location as well as workshops simultaneously, with noise data recorded at 10s intervals. Workshop Noise Measurement points were located at 2m distance from the motor services workshops whenever possible. Noise measurements at each motor services workshop were carried out at the shop front and at the side near the activities. The location of noise monitoring points (NMP) is shown in *Annex A and Table 2-2*. The sound level meters set up positions are indicated in *Annex E*.

The workshops are of similar size and nature, i.e. car repairing activities including manual and tools/machine assisted operations. In this connection, the total sound power level of the activities at each workshop is similar. All the activities observed at each workshop were closer to the ground rather than the ceiling so that no measurement at the top of each shop front was conducted. As conservative approach, the highest sound pressure level recorded amongst the surveyed workshops, during the busiest operation hours were adopted for evaluation of sound power level emission with reference to ISO 3746.

Table 2-2 Location of Noise Measurement Point

NMP ID	Location
NMP 1	Site Boundary
NMP 2	20 Fung Yi Street
NMP 3	23 Lun Cheung Street
NMP 4	26 Ying Yeung Street (遠東-新興)
NMP 5	28 Pang Ching Street (龍華)
NMP 6	24 Hung Wan Street
NMP 7	Mok Cheong Street (銘興五金)
NMP 8	27 Shim Luen Street

As observed during survey and previous visits, the noise from manual operation at source is marginally noticeable while the noise from tools/machine assisted operations is noticeable at source. The noise climate is dominated by on-road traffic noise, merely heavy vehicle, i.e. buses and lorries during noise survey and visits.

2.6 INSTRUMENTATION

All noise measurements were conducted using the equipment listed in *Table 2-3* below. A copy of the calibration certificates for the sound level meter and calibrator are attached in *Annex B*.

Table 2-3 List of Instrumentation

Type & Model	Quantity	Standard Complied
Sound Level Meter B&K 2250	1	IEC 61672-1:2002 Class 1
Sound Level Meter Solo -Slm 01dB	1	IEC 61672-1:2002 Class 1
Sound Level Calibrator RION NC-73	1	IEC 60942

2.7 MEASUREMENT RESULT

A summary table of recorded noise levels ("Overall" and "Background" in $L_{Aeq5min}$ and $L_{Aeq30min}$) together with site observations is shown in *Annex C*. Detailed recorded data is given in *Annex D*. The "Background" is the recorded overall noise level with event duration edited out.

With reference to *Annex C*, at the Site Boundary NMP, the recorded noise level differences between "Overall" and "Background" are within 1.0dB (in terms of both $L_{Aeq5min}$ and $L_{Aeq30min}$) for all the events (i.e. manual operations and tools/machine assisted operations). No tonal, impulsiveness/ intermittency were observed at Project Site Boundary and the site boundary of the adjacent buildings along Mok Cheong Street, namely Freder Centre and Wacker Industrial Building during operation.

3.0 CALCULATION OF SOUND POWER LEVEL OF MOTOR SERVICES WORKSHOPS

This section evaluates the Sound Power Level emanated from motor services workshops. The sound power level (SWL) of the motor services workshops is calculated in accordance with ISO3746:2010.

Step 1: Measurement of Sound Pressure Level
(Details refer to Section 2.5)

Step 2: Calculation of mean time-averaged sound pressure levels-
Background noise correction is applied to recorded sound pressure level during operation activities to obtain mean time-averaged sound pressure level for the workshops. (Refer to Plate 1 below)

Step 3: Calculation of sound energy levels
Parallelepiped measurement surface offset from shopfront of motor services workshop, according to measurement distance, is used to determine the total area of measurement surface.

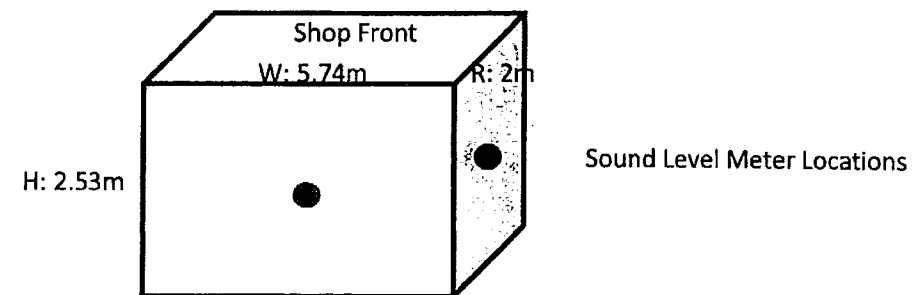
The calculation of SWL is determined by the following equation:

$$SWL = SPL + 10\log(S)dB$$

As confirmed by site survey and observations, the motor services workshops are of similar size and nature, i.e. car repairing activities including manual and tool/ machine assisted operation. In this connection, the total sound power level of the activities at each workshop is similar. For evaluation of Sound Power Level, the highest Sound Pressure Level (SPL) in $L_{Aeq(30mins)}$ obtained from the noise survey during the busiest workshop operation hour is adopted for the calculation, which is considered representative to a worst case scenario. This highest sound pressure level is recorded at NMP 6 during grinding and is adopted for evaluation. Detailed measurement data is provided in *Annex D*.

Machine Data			
Dimension of Shop Front (Width)	W	5.74	m
Dimension of Shop Front (Height)	H	2.53	m
Measurement Distance	r	2	m
Total Measurement Surface	S	31.06	m ²
Calculation of Sound Power Level			
Sound Pressure Level ($L_{A,eq30min}$)	SPL	76.8	dB(A)
Measurement Surface	S	31.06	m ²
10log(S)		14.9	dB
Sound Power Level	SWL	91.7	dB

Plate 1: Diagram of the measurement surface and location of sound level meter



4.0 CONCLUSION

Noise measurement for obtaining SPL of motor service workshop was carried out. The calculated SWL was determined to be 91.7 dB.

APPENDIX 2.6: DETAILED CALCULATIONS OF FIXED NOISE IMPACT ASSESSMENT

Appendix 2.6 - Detailed Calculations of Fixed Noise Impact Assessment for the URA Redevelopment Project KC-010 at Hung Fook Street / Ngan Hon Street, Kowloon City (Day-time)

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NA-M1	837467.966	819505.7116	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	35.7	-39.1	0	3	0	56
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	61.7	-43.8	0	3	0	51
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	68.1	-44.7	0	3	0	50
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	88.4	-46.9	0	3	0	48
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	95.6	-47.6	0	3	0	47
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	101.6	-48.1	0	3	0	47
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	110.6	-48.9	0	3	0	46
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	131.9	-50.4	0	3	0	44
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	138.0	-50.8	-10	3	0	34
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	123.3	-49.8	-10	3	0	35
Total														59	

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NA-J1	837480.0495	819500.215	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	31.7	-38.0	0	3	0	57
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	51.1	-42.2	0	3	0	53
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	56.9	-43.1	0	3	0	52
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	76.1	-45.6	0	3	0	49
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	83.2	-46.4	0	3	0	48
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	89.1	-47.0	0	3	0	48
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	98.0	-47.8	0	3	0	47
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	119.0	-49.5	0	3	0	45
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	124.7	-49.9	-10	3	0	35
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	110.1	-48.8	-10	3	0	36
														Total	60

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NA-G2	837493.2951	819494.5204	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	33.0	-38.4	0	3	0	56
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	41.0	-40.3	0	3	0	54
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	45.9	-41.2	0	3	0	53
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	63.2	-44.0	0	3	0	51
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	70.0	-44.9	0	3	0	50
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	75.7	-45.6	0	3	0	49
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	84.4	-46.5	0	3	0	48
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	105.1	-48.4	0	3	0	46
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	110.4	-48.9	-10	3	0	36
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	95.6	-47.6	-10	3	0	37
														Total	61

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NC-A3	837546.4121	819487.2658	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	64.2	-44.2	0	3	0	51
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	32.5	-38.2	0	3	0	56
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	26.7	-36.5	0	3	0	58
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	16.5	-32.4	0	3	0	62
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	19.8	-33.9	0	3	0	61
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	23.8	-35.5	0	3	0	59
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	31.6	-38.0	0	3	0	57
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	51.5	-42.2	0	3	0	52
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	58.4	-43.3	0	3	0	51
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	46.0	-41.3	-10	3	0	43
Total														67	

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NC-D1	837562.02	819480.1666	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	80.9	-46.2	0	3	0	49
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	47.8	-41.6	0	3	0	53
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	41.1	-40.3	0	3	0	54
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	21.3	-34.6	0	3	0	60
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	17.6	-32.9	0	3	0	62
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	16.3	-32.2	0	3	0	62
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	19.7	-33.9	0	3	0	61
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	35.7	-39.0	0	3	0	56
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	41.3	-40.3	0	3	0	54
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	29.9	-37.5	0	3	0	57
Total														68	

Noise Sensitive Receiver (NSR)	NSR (x)	NSR (y)	Noise Source (NS)	Description	Noise Source (x)	Noise Source (y)	SWL, dB(A)	No of Units	Overall SWL, dB(A)	Horizontal Distance from NSR to NS (m)	Distance Correction, dB(A)	Barrier Correction, dB(A)	Façade Correction, dB(A)	Tonality/Impulsiveness/Intermittency Correction, dB(A)	Corrected Noise Level, Leq (30 min), dB(A)
NC-E1	837566.6733	819465.2197	S1	Fuji Engineering (H.K.) Company Limited	837496.3198	819527.4272	91.7	1	91.7	93.9	-47.5	-10	3	0	37
			S2	Jointly Motors Centre	837529.0149	819514.7237	91.7	1	91.7	62.2	-43.9	-10	3	0	41
			S3	BP Car Shop	837535.8049	819511.7967	91.7	1	91.7	55.9	-42.9	-10	3	0	42
			S4	Top Gear Vehicles Maintenance & Repairing	837556.175	819500.6207	91.7	1	91.7	36.9	-39.3	-10	3	0	45
			S5	Kowloon Auto Service & Maintenance Company	837563.2314	819497.6936	91.7	1	91.7	32.7	-38.3	-10	3	0	46
			S6	Yau Shing Auto Service & Maintenance Company Limited	837568.9563	819494.8996	91.7	1	91.7	29.8	-37.5	0	3	0	57
			S7	Auto Service	837577.6806	819492.092	91.7	1	91.7	29.0	-37.3	0	3	0	57
			S8	MEGA Racing Centre	837597.6569	819481.8427	91.7	1	91.7	35.2	-38.9	0	3	0	56
			S9	Recycling Workshop	837598.2857	819460.4365	91.7	1	91.7	32.0	-38.1	0	3	0	57
			S10	Chun Tung Garage	837581.4646	819457.4575	91.7	1	91.7	16.7	-32.5	0	3	0	62
Total														62	

APPENDIX 3.1:
TRAFFIC FORECAST

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L001	Long Yuet Street (SB)	San Ma Tau Street	Kwei Chow Street	0:00	1:00	100	2.7%	28.8%	58.8%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	1.4%	1.4%	0.0%	0.0%	0.0%	
				1:00	2:00	50	2.4%	31.0%	64.2%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.6%	28.2%	66.6%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	3.4%	31.0%	62.2%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.8%	26.9%	65.5%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	3.2%	25.8%	58.2%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	3.2%	3.2%	0.0%	0.0%	0.0%
				6:00	7:00	100	1.7%	22.4%	57.1%	0.0%	5.2%	0.0%	0.0%	0.0%	1.7%	1.7%	1.7%	3.4%	3.4%	1.7%	0.0%	0.0%	0.0%
				7:00	8:00	150	1.3%	28.9%	47.0%	0.7%	5.4%	1.3%	0.0%	0.0%	0.0%	2.7%	2.0%	4.7%	4.0%	2.0%	0.0%	0.0%	0.0%
				8:00	9:00	250	2.6%	38.9%	32.3%	0.4%	8.3%	4.4%	0.9%	0.9%	0.0%	1.7%	1.7%	3.5%	3.1%	1.3%	0.0%	0.0%	0.0%
				9:00	10:00	250	1.9%	31.3%	35.1%	0.5%	8.4%	5.1%	0.9%	0.9%	0.0%	2.8%	2.3%	4.7%	4.2%	1.9%	0.0%	0.0%	0.0%
				10:00	11:00	200	3.3%	32.5%	32.6%	0.6%	15.5%	5.0%	1.1%	1.1%	1.1%	1.1%	1.1%	2.2%	2.2%	0.6%	0.0%	0.0%	0.0%
				11:00	12:00	200	2.2%	31.1%	27.9%	1.1%	19.6%	2.2%	0.6%	0.6%	1.7%	3.4%	1.7%	3.4%	3.4%	1.1%	0.0%	0.0%	0.0%
				12:00	13:00	200	2.7%	39.3%	32.8%	0.5%	12.4%	1.6%	0.5%	0.5%	1.1%	3.8%	1.1%	1.6%	1.6%	0.5%	0.0%	0.0%	0.0%
				13:00	14:00	200	1.7%	27.7%	23.0%	0.6%	16.3%	6.2%	1.7%	1.1%	1.1%	1.1%	1.1%	3.9%	6.7%	6.7%	2.2%	0.0%	0.0%
				14:00	15:00	200	1.7%	26.7%	32.5%	0.6%	15.1%	4.7%	1.2%	1.2%	1.2%	1.7%	2.3%	4.7%	4.7%	1.7%	0.0%	0.0%	0.0%
				15:00	16:00	250	2.5%	29.9%	32.2%	0.5%	14.2%	5.4%	1.5%	1.0%	1.0%	2.5%	2.0%	3.4%	2.9%	1.0%	0.0%	0.0%	0.0%
				16:00	17:00	200	3.9%	41.4%	24.2%	0.6%	13.5%	1.7%	0.6%	0.0%	2.8%	0.6%	2.8%	3.4%	3.9%	0.6%	0.0%	0.0%	0.0%
				17:00	18:00	250	4.4%	50.1%	24.4%	0.4%	8.4%	2.2%	0.9%	0.4%	2.2%	1.3%	1.3%	1.8%	1.8%	0.4%	0.0%	0.0%	0.0%
				18:00	19:00	250	4.5%	51.4%	25.7%	0.5%	6.4%	1.5%	0.5%	0.5%	1.0%	1.0%	2.0%	2.0%	2.5%	0.5%	0.0%	0.0%	0.0%
				19:00	20:00	200	2.8%	49.7%	37.3%	0.0%	2.3%	0.6%	0.0%	0.0%	1.1%	1.1%	1.1%	1.7%	1.7%	0.6%	0.0%	0.0%	0.0%
				20:00	21:00	150	2.9%	42.4%	47.5%	0.0%	2.2%	0.0%	0.0%	0.0%	2.2%	0.7%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	2.4%	37.4%	53.7%	0.0%	3.3%	0.8%	0.0%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	3.5%	30.4%	57.3%	0.0%	1.7%	0.0%	0.0%	0.0%	2.6%	0.9%	0.9%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%
				23:00	0:00	100	3.2%	29.8%	56.2%	0.0%	2.1%	0.0%	0.0%	0.0%	3.2%	1.1%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%
L002	Wai King Street (SB)	San Ma Tau Street	Kwei Chow Street	0:00	1:00	100	1.0%	12.2%	70.6%	0.0%	2.0%	0.0%	0.0%	0.0%	9.2%	1.0%	1.0%	2.0%	1.0%	0.0%	0.0%	0.0%	
				1:00	2:00	100	1.9%	13.5%	80.8%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.0%	12.2%	83.8%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.8%	13.9%	77.7%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	3.1%	12.5%	81.3%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.5%	10.0%	72.5%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	2.5%	2.5%	2.5%	2.5%	0.0%	0.0%	0.0%	
				6:00	7:00	100	1.2%	8.4%	64.0%	0.0%	4.8%	0.0%	0.0%	0.0%	8.4%	2.4%	2.4%	3.6%	3.6%	1.2%	0.0%	0.0%	
				7:00	8:00	200	1.6%	17.2%	46.7%	0.5%	5.4%	1.1%	0.5%	0.5%	13.4%	2.2%	2.2%	3.8%	3.8%	1.1%	0.0%	0.0%	
				8:00	9:00	300	3.1%	26.3%	36.1%	0.4%	9.0%	3.9%	1.2%	1.2%	7.8%	2.0%	2.0%	3.1%	3.1%	0.8%	0.0%	0.0%	
				9:00	10:00	250	2.4%	20.8%	38.1%	0.4%	9.0%	4.5%	1.2%	1.2%	6.9%	3.3%	2.4%	4.1%	4.5%	1.2%	0.0%	0.0%	
				10:00	11:00	250	1.8%	14.3%	41.9%	0.4%	17.9%	4.5%	1.8%	1.8%	6.7%	1.3%	1.8%	2.7%	2.7%	0.4%	0.0%	0.0%	
				11:00	12:00	250	0.9%	13.6%	34.7%	0.9%	21.9%	1.8%	0.4%	0.4%	11.4%	3.9%	2.2%	3.5%	3.5%	0.9%	0.0%	0.0%	
				12:00	13:00	250	1.4%	18.4%	44.6%	0.5%	15.2%	1.4%	0.5%	0.5%	8.8%	4.1%	0.9%	1.4%	1.8%	0.5%	0.0%	0.0%	
				13:00	14:00	250	0.9%	12.1%	29.0%	0.4%	18.3%	5.4%	1.8%	2.2%	8.5%	1.3%	4.5%	6.7%	7.1%	1.8%	0.0%	0.0%	
				14:00	15:00	250	0.9%	11.2%	40.1%	0.4%	17.0%	4.0%	1.3%	1.3%	9.0%	1.8%	2.7%	4.5%	4.5%	1.3%	0.0%	0.0%	
				15:00	16:00	300	1.2%	13.4%	41.4%	0.4%	16.6%	4.3%	1.6%	1.6%	7.1%	3.2%	2.0%	3.2%	3.2%	0.8%	0.0%	0.0%	
				16:00	17:00	250	0.5%	12.9%	40.7%	0.5%	18.0%	1.4%	0.5%	0.5%	8.8%	0.5%	3.7%	5.1%	5.5%	1.4%	0.0%	0.0%	
				17:00	18:00	250	0.8%	17.3%	45.7%	0.4%	12.5%	2.4%	1.2%	1.2%	8.1%	2.0%	2.0%	2.8%	2.8%	0.8%	0.0%	0.0%	
				18:00	19:00	250	0.9%	18.5%	49.5%	0.5%	10.2%	1.4%	0.9%	0.9%	3.7%	1.9%	2.8%	3.7%	4.2%	0.9%	0.0%	0.0%	
				19:00	20:00	200	1.5%	24.2%	53.2%	0.0%	3.0%	0.5%	0.0%	0.5%	10.1%	1.5%	1.0%	2.0%	2.0%	0.5%	0.0%	0.0%	
				20:00	21:00	200	1.7%	18.4%	60.3%	0.0%	2.3%	0.0%	0.0%	0.0%	14.9%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	
				21:00	22:00	200	1.2%	15.4%	65.5%	0.0%	3.1%	0.6%	0.0%	0.0%	13.6%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	200	1.3%	12.1%	66.4%	0.0%	1.9%	0.0%	0.0%	0.0%	14.6%	0.6%	0.6%	1.3%	0.6%	0.6%	0.0%	0.0%	
				23:00	0:00	150	1.6%	11.6%	65.0%	0.0%	2.3%	0.0%	0.0%	0.0%	14.7%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L003	Mei King Street (NB)	Kwei Chow Street	San Ma Tau Street	0:00	1:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	33.3%	44.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	11.1%	11.1%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	47.2%	26.3%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	5.3%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	58.2%	16.1%	0.0%	9.7%	3.2%	0.0%	0.0%	0.0%	3.2%	3.2%	3.2%	3.2%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	49.9%	17.9%	0.0%	10.7%	3.6%	0.0%	0.0%	0.0%	7.1%	3.6%	3.6%	3.6%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	45.2%	19.4%	0.0%	16.1%	6.5%	0.0%	0.0%	0.0%	3.2%	3.2%	3.2%	3.2%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	40.5%	15.6%	0.0%	18.8%	3.1%	0.0%	0.0%	0.0%	6.3%	3.1%	6.3%	6.3%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	51.6%	21.2%	0.0%	12.1%	3.0%	0.0%	0.0%	0.0%	6.1%	0.0%	3.0%	3.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	37.4%	12.5%	0.0%	15.6%	6.3%	0.0%	0.0%	0.0%	3.1%	6.3%	9.4%	9.4%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	36.6%	20.0%	0.0%	16.7%	6.7%	0.0%	0.0%	0.0%	3.3%	3.3%	6.7%	6.7%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	41.5%	19.4%	0.0%	13.9%	5.6%	0.0%	0.0%	0.0%	5.6%	2.8%	5.6%	5.6%	0.0%	0.0%	0.0%
				16:00	17:00	50	2.3%	46.5%	16.3%	0.0%	11.6%	2.3%	0.0%	0.0%	0.0%	0.0%	4.7%	7.0%	9.3%	0.0%	0.0%	0.0%
				17:00	18:00	100	1.9%	57.9%	17.3%	0.0%	7.7%	3.8%	0.0%	0.0%	0.0%	1.9%	1.9%	3.8%	3.8%	0.0%	0.0%	0.0%
				18:00	19:00	50	2.1%	58.1%	16.7%	0.0%	6.3%	2.1%	0.0%	0.0%	0.0%	2.1%	2.1%	4.2%	6.3%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	65.7%	21.9%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	3.1%	0.0%	3.1%	3.1%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	63.7%	31.8%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	57.9%	36.8%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	53.3%	46.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L004	To Kwa Wan Road (SB)	San Ma Tau Street	Kwei Chow Street	23:00	0:00	50	0.0%	53.8%	46.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				0:00	1:00	400	4.4%	25.1%	55.1%	0.0%	2.2%	0.0%	0.0%	0.0%	1.4%	0.6%	0.6%	1.7%	0.8%	0.6%	0.0%	7.5%
				1:00	2:00	200	4.7%	28.0%	64.2%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	3.9%	25.8%	68.1%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	5.3%	28.0%	62.2%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	4.3%	25.6%	65.7%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	3.5%	22.4%	59.4%	0.0%	4.9%	0.0%	0.0%	0.7%	0.0%	1.4%	1.4%	2.8%	2.1%	1.4%	0.0%	0.0%
				6:00	7:00	350	3.0%	18.8%	50.9%	0.3%	4.3%	0.7%	0.0%	0.3%	1.3%	1.3%	1.6%	3.3%	3.0%	1.3%	0.0%	9.9%
				7:00	8:00	850	1.6%	26.0%	35.3%	0.5%	5.6%	0.5%	0.2%	0.2%	3.5%	1.5%	1.4%	3.0%	2.7%	0.9%	0.0%	17.1%
				8:00	9:00	1,200	3.2%	37.4%	26.0%	0.7%	9.5%	1.6%	0.9%	0.9%	1.9%	1.1%	1.1%	2.3%	2.1%	0.7%	0.0%	10.6%
				9:00	10:00	1,100	2.4%	30.6%	28.3%	0.7%	9.7%	1.8%	0.9%	0.9%	1.8%	2.0%	1.5%	3.1%	3.0%	0.9%	0.0%	12.4%
				10:00	11:00	950	3.7%	26.8%	29.4%	1.0%	14.5%	3.6%	1.7%	1.8%	0.9%	0.7%	0.9%	1.9%	1.9%	0.6%	0.0%	10.6%
				11:00	12:00	950	2.4%	26.1%	25.5%	1.3%	18.3%	1.4%	0.7%	0.7%	1.5%	2.3%	1.3%	2.8%	2.9%	0.9%	0.0%	11.9%
				12:00	13:00	950	3.1%	33.9%	31.2%	0.9%	11.9%	1.2%	0.7%	0.7%	1.1%	2.5%	0.6%	1.2%	1.3%	0.4%	0.0%	9.3%
				13:00	14:00	950	2.3%	22.7%	20.8%	1.1%	14.7%	4.4%	2.1%	2.2%	1.1%	0.7%	2.5%	5.1%	5.4%	1.7%	0.0%	13.2%
				14:00	15:00	900	1.9%	22.0%	29.6%	1.0%	14.2%	3.5%	1.6%	1.7%	1.2%	1.1%	1.7%	3.6%	3.6%	1.2%	0.0%	12.1%
				15:00	16:00	1,050	2.8%	25.1%	29.7%	1.0%	13.4%	3.8%	1.8%	1.8%	1.0%	1.8%	1.3%	2.6%	2.5%	0.9%	0.0%	10.5%
				16:00	17:00	900	4.3%	31.3%	26.8%	0.8%	10.0%	2.1%	0.8%	0.9%	0.0%	0.3%	1.9%	3.9%	4.5%	1.3%	0.0%	11.1%
				17:00	18:00	1,150	5.2%	38.2%	27.3%	0.5%	6.4%	3.3%	1.5%	1.5%	0.0%	1.0%	1.0%	2.1%	2.1%	0.8%	0.0%	9.1%
				18:00	19:00	1,000	5.3%	39.8%	29.2%	0.3%	5.1%	2.2%	1.0%	1.0%	0.0%	0.8%	1.3%	2.7%	3.0%	0.9%	0.0%	7.4%
				19:00	20:00	900	3.5%	42.3%	35.1%	0.1%	2.3%	0.6%	0.2%	0.2%	1.2%	0.8%	0.6%	1.2%	1.4%	0.5%	0.0%	10.0%
				20:00	21:00	750	3.6%	34.8%	42.9%	0.1%	2.0%	0.3%	0.3%	0.3%	2.0%	0.4%	0.3%	0.6%	0.4%	0.3%	0.0%	11.7%
				21:00	22:00	700	2.6%	30.1%	47.3%	0.2%	2.6%	0.5%	0.3%	0.3%	1.8%	0.0%	0.2%	0.3%	0.0%	0.2%	0.0%	13.6%
				22:00	23:00	650	3.4%	23.2%	47.8%	0.2%	1.4%	0.0%	0.0%	0.0%	2.0%	0.6%	0.5%	0.9%	0.5%	0.5%	0.0%	19.0%
				23:00	0:00	550	3.7%	22.4%	47.4%	0.2%	1.9%	0.0%	0.0%	0.0%	1.9%	0.8%	0.6%	1.2%	0.6%	0.6%	0.0%	18.7%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L005	To Kwa Wan Road (NB)	Kwei Chow Street	San Ma Tau Street	0:00	1:00	350	3.0%	18.8%	56.2%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	3.6%	1.0%	1.0%	2.6%	1.3%	0.7%	0.0%	10.2%
				1:00	2:00	150	3.4%	23.0%	70.9%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	2.9%	20.3%	74.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.0%	23.0%	70.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.3%	20.7%	71.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.6%	17.1%	62.3%	0.0%	3.4%	0.0%	0.0%	0.9%	0.0%	1.7%	2.6%	4.3%	3.4%	1.7%	0.0%	0.0%	0.0%
				6:00	7:00	300	1.9%	13.3%	48.9%	0.0%	3.0%	1.1%	0.0%	0.4%	3.7%	2.2%	2.2%	4.8%	4.4%	1.5%	0.0%	12.6%	
				7:00	8:00	700	1.7%	17.5%	34.0%	0.2%	2.4%	2.0%	0.5%	0.5%	2.0%	2.0%	2.0%	4.1%	4.3%	1.4%	0.0%	25.4%	
				8:00	9:00	900	3.4%	26.5%	26.4%	0.1%	4.3%	7.1%	1.8%	1.8%	1.1%	1.7%	1.7%	3.3%	3.3%	1.1%	0.0%	16.4%	
				9:00	10:00	900	2.5%	20.4%	27.3%	0.1%	4.2%	7.6%	1.8%	1.9%	1.0%	2.7%	2.0%	4.3%	4.6%	1.5%	0.0%	18.1%	
				10:00	11:00	850	2.5%	19.4%	29.1%	0.2%	10.0%	7.7%	2.4%	2.5%	2.4%	1.2%	1.5%	3.0%	3.0%	1.0%	0.0%	14.1%	
				11:00	12:00	800	1.7%	19.0%	25.3%	0.4%	12.6%	3.1%	1.0%	0.9%	4.3%	3.6%	2.0%	4.3%	4.7%	1.3%	0.0%	15.8%	
				12:00	13:00	750	2.2%	25.8%	32.3%	0.3%	8.6%	2.8%	1.1%	0.9%	3.4%	4.2%	0.9%	1.9%	2.2%	0.5%	0.0%	12.9%	
				13:00	14:00	900	1.4%	15.0%	18.8%	0.2%	9.4%	8.6%	2.7%	2.9%	2.8%	0.9%	3.7%	7.3%	8.0%	2.3%	0.0%	16.0%	
				14:00	15:00	850	1.2%	15.0%	27.8%	0.2%	9.3%	7.0%	2.1%	2.2%	3.2%	1.7%	2.6%	5.2%	5.6%	1.6%	0.0%	15.3%	
				15:00	16:00	950	1.8%	17.6%	28.8%	0.2%	9.0%	7.7%	2.5%	2.5%	2.5%	2.7%	2.0%	3.9%	4.0%	1.2%	0.0%	13.6%	
				16:00	17:00	900	1.9%	22.6%	25.3%	0.2%	10.1%	2.1%	0.8%	0.9%	4.7%	0.6%	3.2%	6.3%	7.1%	1.8%	0.0%	12.4%	
				17:00	18:00	1,000	2.4%	29.3%	27.5%	0.2%	6.8%	3.6%	1.7%	1.7%	4.2%	1.8%	1.8%	3.5%	3.5%	1.1%	0.0%	10.9%	
				18:00	19:00	900	2.4%	31.3%	30.1%	0.1%	5.4%	2.4%	1.2%	1.2%	2.0%	1.5%	2.5%	4.5%	5.1%	1.3%	0.0%	9.0%	
				19:00	20:00	750	2.5%	32.4%	36.5%	0.0%	1.7%	1.3%	0.3%	0.4%	3.6%	1.3%	1.0%	2.1%	2.2%	0.6%	0.0%	14.1%	
				20:00	21:00	600	2.5%	26.1%	44.3%	0.0%	1.3%	0.5%	0.3%	0.3%	5.7%	0.5%	0.5%	0.8%	0.7%	0.3%	0.0%	16.2%	
				21:00	22:00	600	1.8%	22.1%	47.8%	0.0%	1.8%	1.1%	0.4%	0.4%	5.3%	0.0%	0.2%	0.4%	0.0%	0.2%	0.0%	18.5%	
				22:00	23:00	600	2.3%	16.3%	46.3%	0.0%	1.1%	0.0%	0.0%	0.0%	5.3%	0.9%	0.7%	1.4%	0.7%	0.5%	0.0%	24.5%	
				23:00	0:00	500	2.4%	15.7%	45.8%	0.0%	1.3%	0.0%	0.0%	0.0%	5.2%	1.1%	0.9%	1.7%	0.9%	0.7%	0.0%	24.3%	
L006	Kwei Chow Street (WB)	Mei King Street	To Kwa Wan Road	0:00	1:00	100	2.2%	14.0%	63.2%	0.0%	2.2%	0.0%	0.0%	0.0%	9.7%	1.1%	1.1%	3.2%	2.2%	1.1%	0.0%	0.0%	
				1:00	2:00	50	2.1%	17.0%	76.6%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.3%	13.6%	81.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	3.2%	16.1%	77.5%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	3.4%	13.8%	79.4%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.5%	12.5%	62.5%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	2.5%	2.5%	5.0%	5.0%	2.5%	0.0%	0.0%	
				6:00	7:00	100	1.2%	9.8%	56.2%	0.0%	3.7%	1.2%	0.0%	0.0%	8.5%	2.4%	2.4%	6.1%	6.1%	2.4%	0.0%	0.0%	
				7:00	8:00	200	1.0%	15.2%	44.1%	0.0%	5.2%	1.0%	0.5%	0.0%	13.1%	2.6%	2.6%	6.3%	5.8%	2.6%	0.0%	0.0%	
				8:00	9:00	300	2.7%	23.3%	34.0%	0.0%	9.3%	4.7%	1.2%	0.8%	7.8%	2.3%	2.3%	5.0%	4.7%	1.9%	0.0%	0.0%	
				9:00	10:00	300	2.0%	17.9%	35.3%	0.0%	9.1%	5.2%	1.2%	0.8%	6.7%	3.6%	2.8%	6.3%	6.3%	2.8%	0.0%	0.0%	
				10:00	11:00	250	2.3%	15.8%	36.7%	0.0%	15.4%	6.3%	1.8%	0.9%	6.8%	1.8%	1.8%	4.5%	4.1%	1.8%	0.0%	0.0%	
				11:00	12:00	250	1.3%	14.9%	30.0%	0.0%	18.4%	2.2%	0.4%	0.4%	11.4%	4.4%	2.6%	5.7%	6.1%	2.2%	0.0%	0.0%	
				12:00	13:00	250	1.9%	20.4%	39.4%	0.0%	12.8%	2.4%	0.5%	0.5%	9.0%	5.2%	1.4%	2.8%	2.8%	0.9%	0.0%	0.0%	
				13:00	14:00	250	1.2%	12.4%	23.3%	0.0%	14.1%	7.1%	1.7%	1.2%	7.9%	1.2%	5.0%	10.4%	10.8%	3.7%	0.0%	0.0%	
				14:00	15:00	250	1.3%	11.8%	33.7%	0.0%	13.6%	5.7%	1.3%	0.9%	8.8%	2.2%	3.1%	7.5%	7.5%	2.6%	0.0%	0.0%	
				15:00	16:00	300	1.6%	14.5%	35.3%	0.0%	13.7%	6.3%	1.6%	1.2%	7.0%	3.5%	2.3%	5.5%	5.5%	2.0%	0.0%	0.0%	
				16:00	17:00	250	2.2%	17.6%	30.8%	0.0%	11.5%	2.6%	0.4%	0.4%	8.4%	0.9%	4.0%	8.8%	9.3%	3.1%	0.0%	0.0%	
				17:00	18:00	300	2.7%	23.5%	34.9%	0.0%	8.2%	4.3%	1.2%	0.8%	7.8%	2.4%	2.4%	5.1%	4.7%	2.0%	0.0%	0.0%	
				18:00	19:00	250	2.7%	25.0%	37.5%	0.0%	6.7%	2.7%	0.9%	0.4%	3.6%	1.8%	3.1%	6.7%	6.7%	2.2%	0.0%	0.0%	
				19:00	20:00	200	2.6%	27.0%	46.4%	0.0%	2.6%	1.0%	0.0%	0.0%	10.2%	1.5%	1.5%	3.1%	3.1%	1.0%	0.0%	0.0%	
				20:00	21:00	200	2.4%	21.0%	54.4%	0.0%	1.8%	0.6%	0.0%	0.0%	15.6%	0.6%	0.6%	1.2%	1.2%	0.6%	0.0%	0.0%	
				21:00	22:00	200	2.0%	18.5%	60.2%	0.0%	2.6%	0.7%	0.0%	0.0%	14.6%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%	
				22:00	23:00	150	2.0%	14.1%	60.6%	0.0%	1.3%	0.0%	0.0%	0.0%	15.4%	1.3%	0.7%	2.0%	1.3%	1.3%	0.0%	0.0%	
				23:00	0:00	150	2.4%	13.7%	59.0%	0.0%	1.6%	0.0%	0.0%	0.0%	15.3%	1.6%	0.8%	2.4%	1.6%	1.6%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L007	Kwei Chow Street (WB)	Wai King Street	Mei King Street	0:00	1:00	200	1.9%	20.5%	62.1%	0.0%	2.6%	0.0%	0.0%	0.0%	5.8%	1.3%	1.3%	2.6%	1.3%	0.6%	0.0%	0.0%	
				1:00	2:00	100	2.4%	22.9%	71.1%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	2.6%	20.5%	74.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.5%	22.8%	68.4%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	1.9%	21.2%	71.2%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.5%	16.9%	63.2%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	1.5%	3.1%	4.6%	3.1%	1.5%	0.0%	0.0%	0.0%
				6:00	7:00	150	1.5%	14.7%	54.5%	0.0%	4.4%	0.7%	0.0%	0.0%	5.9%	2.2%	2.9%	5.9%	5.1%	2.2%	0.0%	0.0%	0.0%
				7:00	8:00	400	1.1%	22.5%	42.0%	0.3%	5.6%	1.4%	0.3%	0.3%	8.9%	2.8%	2.5%	5.3%	5.3%	1.7%	0.0%	0.0%	0.0%
				8:00	9:00	550	2.3%	32.6%	30.8%	0.4%	9.2%	4.8%	0.8%	0.8%	4.8%	2.1%	2.1%	4.0%	4.0%	1.3%	0.0%	0.0%	0.0%
				9:00	10:00	500	1.6%	25.8%	32.8%	0.4%	9.1%	5.3%	0.8%	0.8%	4.5%	3.5%	2.6%	5.3%	5.7%	1.8%	0.0%	0.0%	0.0%
				10:00	11:00	450	2.0%	22.1%	33.0%	0.7%	16.9%	7.5%	1.2%	1.2%	4.0%	1.5%	1.7%	3.5%	3.5%	1.2%	0.0%	0.0%	0.0%
				11:00	12:00	450	1.2%	20.9%	27.4%	0.7%	20.7%	2.7%	0.5%	0.5%	7.1%	4.2%	2.5%	4.9%	5.2%	1.5%	0.0%	0.0%	0.0%
				12:00	13:00	400	1.5%	28.4%	35.0%	0.5%	14.2%	2.6%	0.5%	0.5%	5.4%	4.9%	1.0%	2.1%	2.6%	0.8%	0.0%	0.0%	0.0%
				13:00	14:00	450	1.2%	17.7%	21.7%	0.7%	16.0%	8.3%	1.4%	1.4%	5.0%	1.2%	4.5%	8.7%	9.4%	2.8%	0.0%	0.0%	0.0%
				14:00	15:00	400	1.0%	17.3%	31.0%	0.8%	15.8%	6.8%	1.0%	1.0%	5.5%	2.0%	3.0%	6.3%	6.5%	2.0%	0.0%	0.0%	0.0%
				15:00	16:00	500	1.3%	20.1%	32.4%	0.6%	15.2%	7.4%	1.3%	1.3%	4.3%	3.2%	2.4%	4.5%	4.5%	1.5%	0.0%	0.0%	0.0%
				16:00	17:00	400	1.6%	24.4%	27.9%	0.5%	14.6%	3.3%	0.5%	0.5%	5.1%	0.5%	3.8%	7.0%	8.1%	2.2%	0.0%	0.0%	0.0%
				17:00	18:00	450	2.1%	31.8%	30.1%	0.5%	10.0%	5.4%	0.9%	0.9%	4.7%	2.1%	2.1%	4.0%	4.0%	1.4%	0.0%	0.0%	0.0%
				18:00	19:00	400	2.1%	33.5%	32.4%	0.3%	8.0%	3.5%	0.5%	0.5%	2.1%	1.9%	2.9%	5.1%	5.6%	1.6%	0.0%	0.0%	0.0%
				19:00	20:00	400	2.0%	37.3%	41.7%	0.0%	2.8%	1.1%	0.0%	0.3%	6.2%	1.4%	1.1%	2.5%	2.8%	0.8%	0.0%	0.0%	0.0%
				20:00	21:00	300	2.1%	30.5%	50.7%	0.0%	2.4%	0.3%	0.0%	0.3%	10.3%	0.7%	0.7%	1.0%	0.7%	0.3%	0.0%	0.0%	0.0%
				21:00	22:00	300	1.5%	26.5%	56.4%	0.0%	3.4%	1.1%	0.0%	0.4%	9.5%	0.0%	0.4%	0.4%	0.0%	0.4%	0.0%	0.0%	0.0%
				22:00	23:00	250	2.0%	21.3%	59.1%	0.0%	2.0%	0.0%	0.0%	0.0%	10.4%	1.2%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%	0.0%
				23:00	0:00	250	2.5%	20.6%	57.6%	0.0%	2.5%	0.0%	0.0%	0.0%	10.3%	1.5%	1.0%	2.0%	1.0%	1.0%	0.0%	0.0%	0.0%
L008	Kwei Chow Street (WB)	Long Yuet Street	Wai King Street	0:00	1:00	100	1.8%	21.8%	62.0%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	3.6%	1.8%	1.8%	0.0%	0.0%	
				1:00	2:00	50	3.3%	23.3%	70.1%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	3.4%	20.7%	72.5%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.8%	23.8%	66.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	22.2%	72.2%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	16.0%	60.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	4.0%	4.0%	4.0%	4.0%	4.0%	0.0%	0.0%	
				6:00	7:00	50	2.0%	14.3%	55.3%	0.0%	6.1%	2.0%	0.0%	0.0%	0.0%	2.0%	4.1%	6.1%	6.1%	2.0%	0.0%	0.0%	
				7:00	8:00	150	0.8%	23.5%	45.3%	0.8%	6.8%	2.3%	0.0%	0.0%	0.0%	3.0%	3.0%	6.1%	6.1%	2.3%	0.0%	0.0%	
				8:00	9:00	250	2.0%	31.5%	31.2%	0.5%	10.9%	6.4%	1.0%	1.0%	0.0%	2.5%	2.5%	4.5%	4.5%	1.5%	0.0%	0.0%	0.0%
				9:00	10:00	200	1.5%	24.6%	32.3%	0.5%	10.8%	7.2%	1.0%	1.0%	0.0%	4.1%	3.1%	5.6%	6.2%	2.1%	0.0%	0.0%	0.0%
				10:00	11:00	150	2.0%	21.6%	31.5%	0.7%	18.9%	9.5%	1.4%	1.4%	0.0%	1.4%	2.0%	4.1%	4.1%	1.4%	0.0%	0.0%	0.0%
				11:00	12:00	150	1.4%	21.5%	27.6%	0.7%	23.6%	3.5%	0.7%	0.7%	0.0%	4.9%	2.8%	5.6%	5.6%	1.4%	0.0%	0.0%	0.0%
				12:00	13:00	150	1.4%	28.3%	35.7%	0.7%	15.9%	3.6%	0.7%	0.7%	0.0%	5.8%	1.4%	2.2%	2.9%	0.7%	0.0%	0.0%	0.0%
				13:00	14:00	200	1.3%	16.9%	20.5%	0.6%	17.5%	10.6%	1.9%	1.9%	0.0%	1.3%	5.0%	9.4%	10.0%	3.1%	0.0%	0.0%	0.0%
				14:00	15:00	150	0.7%	17.1%	30.8%	0.7%	17.8%	8.9%	1.4%	1.4%	0.0%	2.1%	3.4%	6.8%	6.8%	2.1%	0.0%	0.0%	0.0%
				15:00	16:00	200	1.2%	19.5%	31.2%	0.6%	17.2%	9.5%	1.8%	1.8%	0.0%	3.6%	2.4%	4.7%	4.7%	1.8%	0.0%	0.0%	0.0%
				16:00	17:00	150	1.7%	25.2%	27.0%	0.0%	16.0%	4.2%	0.8%	0.8%	0.0%	0.8%	4.2%	7.6%	9.2%	2.5%	0.0%	0.0%	0.0%
				17:00	18:00	150	2.2%	32.7%	29.2%	0.0%	10.9%	7.3%	1.5%	1.5%	0.0%	2.2%	2.2%	4.4%	4.4%	1.5%	0.0%	0.0%	0.0%
				18:00	19:00	150	2.4%	34.0%	30.6%	0.0%	8.1%	4.8%	0.8%	0.8%	0.0%	1.6%	3.2%	5.6%	6.5%	1.6%	0.0%	0.0%	0.0%
				19:00	20:00	150	1.6%	39.3%	43.6%	0.0%	3.3%	1.6%	0.0%	0.0%	0.0%	1.6%	1.6%	3.3%	3.3%	0.8%	0.0%	0.0%	0.0%
				20:00	21:00	100	2.1%	33.3%	55.5%	0.0%	3.1%	1.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	1.2%	29.4%	62.3%	0.0%	4.7%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	2.5%	23.5%	64.2%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	2.5%	1.2%	1.2%	0.0%	0.0%	0.0%
				23:00	0:00	100	2.9%	22.1%	63.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	2.9%	1.5%	1.5%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L009	King Wan Street (SB)	Kwei Chow Street	Shun Fung Street	0:00	1:00	100	4.7%	47.0%	37.6%	0.0%	3.5%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	2.4%	1.2%	0.0%	0.0%	0.0%		
				1:00	2:00	50	6.3%	47.8%	41.7%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	4.7%	46.5%	46.5%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	6.1%	48.4%	39.4%	0.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	7.1%	46.4%	42.9%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	5.6%	38.7%	38.9%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	2.8%	2.8%	2.8%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	4.3%	36.5%	36.2%	0.0%	5.8%	1.4%	0.0%	0.0%	1.4%	2.9%	2.9%	4.3%	4.3%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	200	1.1%	41.6%	32.6%	0.5%	6.3%	1.6%	0.5%	0.0%	0.0%	3.2%	3.2%	4.7%	4.7%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	350	2.2%	51.5%	20.4%	0.3%	8.8%	4.7%	0.9%	0.6%	0.0%	2.2%	2.2%	3.1%	3.1%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	300	1.7%	43.1%	22.6%	0.3%	9.1%	5.6%	1.0%	0.7%	0.0%	3.8%	3.1%	4.5%	4.5%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	250	4.5%	44.2%	18.0%	0.4%	17.6%	5.7%	1.2%	0.8%	0.8%	1.2%	1.2%	2.0%	2.0%	0.4%	0.0%	0.0%	0.0%	
				11:00	12:00	250	2.9%	43.0%	15.4%	0.8%	22.1%	2.1%	0.4%	0.4%	1.3%	3.3%	1.7%	2.9%	3.3%	0.4%	0.0%	0.0%	0.0%	
				12:00	13:00	250	3.2%	53.8%	18.5%	0.4%	13.7%	2.0%	0.4%	0.4%	0.8%	3.2%	0.8%	1.2%	1.6%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	250	2.5%	37.9%	12.9%	0.4%	17.9%	7.1%	1.7%	1.3%	0.8%	0.8%	3.8%	5.8%	6.3%	0.8%	0.0%	0.0%	0.0%	
				14:00	15:00	250	2.3%	37.6%	18.9%	0.5%	18.0%	5.9%	1.4%	0.9%	0.9%	1.8%	2.7%	4.1%	4.5%	0.5%	0.0%	0.0%	0.0%	
				15:00	16:00	300	3.3%	41.4%	18.5%	0.4%	16.3%	5.9%	1.5%	1.1%	0.7%	2.6%	1.9%	3.0%	3.0%	0.4%	0.0%	0.0%	0.0%	
				16:00	17:00	250	5.7%	55.9%	10.2%	0.4%	15.2%	1.6%	0.4%	0.4%	2.0%	0.4%	1.2%	2.5%	2.9%	1.2%	0.0%	0.0%	0.0%	
				17:00	18:00	350	6.9%	64.0%	10.0%	0.3%	9.1%	2.5%	0.6%	0.6%	1.6%	0.6%	0.6%	1.3%	1.3%	0.6%	0.0%	0.0%	0.0%	
				18:00	19:00	300	7.0%	66.7%	10.6%	0.4%	7.0%	1.8%	0.4%	0.4%	0.7%	0.4%	0.7%	1.4%	1.8%	0.7%	0.0%	0.0%	0.0%	
				19:00	20:00	250	3.7%	67.0%	20.7%	0.0%	2.5%	0.8%	0.0%	0.0%	0.8%	0.8%	0.8%	1.2%	1.7%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	200	4.5%	60.5%	28.1%	0.0%	2.2%	0.6%	0.0%	0.0%	1.7%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	150	3.3%	56.7%	33.3%	0.0%	4.0%	0.7%	0.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	150	5.3%	48.8%	37.4%	0.0%	2.3%	0.0%	0.0%	0.0%	2.3%	0.8%	0.8%	1.5%	0.8%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	150	5.6%	46.8%	37.4%	0.0%	2.8%	0.0%	0.0%	0.0%	2.8%	0.9%	0.9%	1.9%	0.9%	0.0%	0.0%	0.0%	0.0%	
L010	Kiang Su Street (EB)	Kau Pui Lung Road	Maidstone Road	0:00	1:00	50	0.0%	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				1:00	2:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	50	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	50	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				6:00	7:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				7:00	8:00	50	14.3%	42.8%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				8:00	9:00	50	15.4%	53.8%	23.1%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				9:00	10:00	50	10.0%	50.0%	30.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				10:00	11:00	50	14.3%	42.8%	28.6%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				11:00	12:00	50	12.5%	37.5%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				12:00	13:00	50	12.5%	50.0%	25.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				13:00	14:00	50	12.5%	37.5%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				14:00	15:00	50	14.3%	42.8%	28.6%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				15:00	16:00	50	11.1%	33.4%	33.3%	0.0%	22.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				16:00	17:00	50	14.3%	42.8%	28.6%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				17:00	18:00	50	12.5%	50.0%	25.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				18:00	19:00	50	12.5%	50.0%	25.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				19:00	20:00	50	11.1%	55.6%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				20:00	21:00	50	14.3%	42.8%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				21:00	22:00	50	14.3%	42.8%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				22:00	23:00	50	16.7%	33.3%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				23:00	0:00	50	20.0%	40.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L011	Yuk Yat Street (NB)	Shun Fung Street	Kwei Chow Street	0:00	1:00	50	2.0%	24.5%	57.3%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.1%	2.0%	0.0%	0.0%
				1:00	2:00	50	3.8%	26.9%	65.5%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	4.0%	24.0%	68.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.6%	27.8%	61.0%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	25.0%	68.7%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	18.2%	54.8%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	4.5%	4.5%	4.5%	4.5%	4.5%	0.0%	0.0%
				6:00	7:00	50	2.2%	17.8%	48.9%	0.0%	6.7%	2.2%	0.0%	0.0%	0.0%	2.2%	4.4%	6.7%	6.7%	2.2%	0.0%	0.0%
				7:00	8:00	150	0.8%	25.4%	38.9%	0.0%	7.9%	2.4%	0.0%	0.0%	0.0%	3.2%	3.2%	7.9%	7.1%	3.2%	0.0%	0.0%
				8:00	9:00	200	1.0%	33.6%	26.4%	0.0%	12.2%	8.1%	1.0%	0.0%	0.0%	2.5%	2.5%	5.6%	5.1%	2.0%	0.0%	0.0%
				9:00	10:00	200	0.5%	26.2%	27.4%	0.0%	12.0%	8.9%	1.0%	0.0%	0.0%	4.2%	3.1%	7.3%	6.8%	2.6%	0.0%	0.0%
				10:00	11:00	150	2.1%	23.8%	26.5%	0.7%	18.9%	12.6%	1.4%	0.7%	0.0%	1.4%	2.1%	4.2%	4.2%	1.4%	0.0%	0.0%
				11:00	12:00	150	1.4%	23.4%	22.7%	0.7%	23.4%	5.0%	0.7%	0.0%	0.0%	5.0%	2.8%	6.4%	6.4%	2.1%	0.0%	0.0%
				12:00	13:00	150	1.5%	31.5%	30.1%	0.8%	16.5%	4.5%	0.8%	0.0%	0.0%	6.0%	1.5%	3.0%	3.0%	0.8%	0.0%	0.0%
				13:00	14:00	200	1.2%	18.1%	16.8%	0.6%	16.8%	13.0%	1.9%	1.2%	0.0%	1.2%	5.0%	9.9%	10.6%	3.7%	0.0%	0.0%
				14:00	15:00	150	0.7%	18.3%	25.5%	0.7%	17.6%	11.3%	1.4%	0.7%	0.0%	2.1%	3.5%	7.7%	7.7%	2.8%	0.0%	0.0%
				15:00	16:00	200	1.2%	21.3%	26.3%	0.6%	17.1%	12.2%	1.8%	0.6%	0.0%	3.7%	2.4%	5.5%	5.5%	1.8%	0.0%	0.0%
				16:00	17:00	150	2.6%	28.7%	21.7%	0.9%	13.0%	6.1%	0.9%	0.9%	0.0%	0.9%	4.3%	7.8%	9.6%	2.6%	0.0%	0.0%
				17:00	18:00	150	3.6%	36.3%	23.4%	0.7%	8.8%	9.5%	1.5%	1.5%	0.0%	2.2%	2.2%	4.4%	4.4%	1.5%	0.0%	0.0%
				18:00	19:00	150	3.3%	38.0%	24.8%	0.8%	6.6%	5.8%	0.8%	0.8%	0.0%	1.7%	3.3%	5.8%	6.6%	1.7%	0.0%	0.0%
				19:00	20:00	150	1.7%	44.1%	37.1%	0.0%	3.4%	2.6%	0.0%	0.0%	0.0%	1.7%	1.7%	3.4%	3.4%	0.9%	0.0%	0.0%
				20:00	21:00	100	2.3%	38.6%	49.1%	0.0%	3.4%	1.1%	0.0%	0.0%	0.0%	1.1%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%
				21:00	22:00	100	1.3%	35.1%	55.8%	0.0%	3.9%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	2.7%	27.4%	58.9%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.7%	1.4%	1.4%	0.0%	0.0%
				23:00	0:00	100	3.3%	26.7%	56.6%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.3%	1.7%	1.7%	0.0%	0.0%
L012	Mei Kwong Street (NB)	Mei King Street	Kwei Chow Street	0:00	1:00	50	0.0%	63.6%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	0.0%	0.0%
				1:00	2:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	75.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	16.7%	0.0%	0.0%
				6:00	7:00	50	0.0%	50.0%	30.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	10.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	58.0%	19.4%	0.0%	6.5%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	0.0%	9.7%	0.0%	0.0%
				8:00	9:00	100	1.8%	64.8%	10.5%	0.0%	10.5%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	5.3%	0.0%	0.0%
				9:00	10:00	50	2.0%	57.3%	12.2%	0.0%	12.2%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	8.2%	0.0%	0.0%
				10:00	11:00	50	2.4%	48.8%	12.2%	0.0%	19.5%	9.8%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	4.9%	0.0%	0.0%
				11:00	12:00	50	0.0%	51.4%	10.8%	0.0%	27.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	0.0%	5.4%	0.0%	0.0%
				12:00	13:00	50	2.5%	62.5%	12.5%	0.0%	17.5%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%
				13:00	14:00	50	0.0%	43.6%	7.7%	0.0%	20.5%	10.3%	0.0%	0.0%	0.0%	0.0%	0.0%	5.1%	0.0%	12.8%	0.0%	0.0%
				14:00	15:00	50	0.0%	44.5%	13.9%	0.0%	22.2%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	0.0%	8.3%	0.0%	0.0%
				15:00	16:00	50	2.3%	47.7%	13.6%	0.0%	18.2%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	6.8%	0.0%	0.0%
				16:00	17:00	50	2.6%	53.8%	10.3%	0.0%	15.4%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	5.1%	0.0%	7.7%	0.0%	0.0%
				17:00	18:00	50	2.1%	64.5%	10.4%	0.0%	10.4%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	4.2%	0.0%	0.0%
				18:00	19:00	50	2.3%	67.4%	11.6%	0.0%	7.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	4.7%	0.0%	0.0%
				19:00	20:00	50	2.5%	75.0%	15.0%	0.0%	2.5%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%
				20:00	21:00	50	3.4%	69.1%	20.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	0.0%	0.0%
				21:00	22:00	50	0.0%	69.6%	26.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	60.0%	30.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	58.8%	29.4%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.9%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L013	Mei King Street (SB)	Kwei Chow Street	Mei Kwong Street	0:00	1:00	100	1.8%	32.7%	60.1%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	2.9%	32.4%	61.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	30.0%	66.7%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	31.8%	63.7%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	30.0%	65.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	26.1%	60.9%	0.0%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	2.2%	23.9%	56.4%	0.0%	6.5%	2.2%	0.0%	0.0%	0.0%	2.2%	2.2%	2.2%	2.2%	0.0%	0.0%	0.0%
				7:00	8:00	150	0.7%	38.4%	45.1%	0.7%	6.8%	1.4%	0.0%	0.0%	0.0%	1.4%	1.4%	2.7%	1.4%	0.0%	0.0%	0.0%
				8:00	9:00	250	1.7%	48.8%	29.4%	0.4%	9.8%	5.1%	0.4%	0.0%	0.0%	0.9%	0.9%	1.7%	0.9%	0.0%	0.0%	0.0%
				9:00	10:00	250	1.4%	41.7%	33.0%	0.5%	10.5%	6.2%	0.5%	0.0%	0.0%	1.4%	1.0%	2.4%	1.4%	0.0%	0.0%	0.0%
				10:00	11:00	150	1.3%	32.6%	30.7%	0.7%	21.3%	9.3%	0.7%	0.0%	0.0%	0.7%	0.7%	1.3%	0.7%	0.0%	0.0%	0.0%
				11:00	12:00	150	0.7%	32.8%	27.3%	1.4%	28.0%	3.5%	0.0%	0.0%	0.0%	2.1%	0.7%	2.1%	1.4%	0.0%	0.0%	0.0%
				12:00	13:00	150	1.4%	40.7%	32.0%	0.7%	17.7%	3.4%	0.0%	0.0%	0.0%	2.0%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%
				13:00	14:00	150	0.7%	29.8%	23.2%	0.7%	23.2%	12.3%	0.7%	0.0%	0.0%	0.7%	2.2%	4.3%	2.2%	0.0%	0.0%	0.0%
				14:00	15:00	150	0.7%	27.9%	31.7%	0.7%	22.1%	9.6%	0.7%	0.0%	0.0%	0.7%	1.5%	2.9%	1.5%	0.0%	0.0%	0.0%
				15:00	16:00	200	1.2%	31.2%	31.1%	0.6%	20.1%	9.8%	0.6%	0.0%	0.0%	1.2%	1.2%	1.8%	1.2%	0.0%	0.0%	0.0%
				16:00	17:00	100	1.0%	34.8%	24.5%	1.0%	25.5%	6.1%	0.0%	0.0%	0.0%	0.0%	2.0%	3.1%	2.0%	0.0%	0.0%	0.0%
				17:00	18:00	150	1.6%	42.4%	25.2%	0.8%	16.3%	8.9%	0.8%	0.0%	0.0%	0.8%	0.8%	1.6%	0.8%	0.0%	0.0%	0.0%
				18:00	19:00	150	1.9%	45.4%	27.4%	0.9%	13.2%	5.7%	0.9%	0.0%	0.0%	0.9%	0.9%	1.9%	0.9%	0.0%	0.0%	0.0%
				19:00	20:00	150	1.5%	53.4%	37.2%	0.0%	3.6%	1.5%	0.0%	0.0%	0.0%	0.7%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%
				20:00	21:00	150	1.9%	45.8%	47.7%	0.0%	2.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	1.0%	40.2%	53.7%	0.0%	4.1%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	1.2%	34.5%	60.7%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%
L014	To Kwa Wan Road (SB)	Kwei Chow Street	Lok Shan road	23:00	0:00	100	1.5%	33.8%	60.3%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%
				0:00	1:00	450	3.2%	23.5%	53.4%	0.0%	2.2%	0.0%	0.0%	0.0%	6.6%	0.5%	0.5%	1.5%	0.7%	0.5%	0.0%	7.4%
				1:00	2:00	250	3.8%	27.0%	64.0%	0.0%	3.3%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	3.1%	24.7%	68.0%	0.0%	2.1%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	4.3%	27.3%	64.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	4.0%	24.6%	66.6%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	200	3.3%	22.2%	60.7%	0.0%	4.6%	0.0%	0.0%	0.7%	0.0%	1.3%	1.3%	2.6%	2.0%	1.3%	0.0%	0.0%
				6:00	7:00	350	2.3%	17.4%	49.0%	0.3%	3.8%	0.6%	0.0%	0.3%	7.3%	1.2%	1.5%	2.9%	2.6%	1.2%	0.0%	9.6%
				7:00	8:00	950	1.3%	23.3%	32.9%	0.4%	5.0%	0.4%	0.2%	0.2%	12.8%	1.2%	1.1%	2.5%	2.5%	1.0%	0.0%	15.2%
				8:00	9:00	1,250	2.6%	35.1%	25.3%	0.6%	8.9%	1.8%	0.8%	0.8%	7.4%	1.0%	1.0%	2.0%	2.0%	0.8%	0.0%	9.9%
				9:00	10:00	1,150	2.0%	28.7%	27.7%	0.5%	9.0%	2.0%	0.9%	0.9%	7.0%	1.7%	1.3%	2.7%	2.9%	1.1%	0.0%	11.6%
				10:00	11:00	1,050	3.0%	25.5%	28.8%	0.9%	13.7%	3.7%	1.5%	1.6%	4.9%	0.7%	0.9%	1.8%	1.9%	0.6%	0.0%	10.5%
				11:00	12:00	1,050	1.9%	24.1%	24.3%	1.1%	16.9%	1.4%	0.6%	0.5%	8.5%	2.0%	1.2%	2.5%	2.7%	0.8%	0.0%	11.5%
				12:00	13:00	1,050	2.4%	31.7%	30.4%	0.8%	11.2%	1.3%	0.6%	0.5%	6.4%	2.3%	0.5%	1.1%	1.3%	0.4%	0.0%	9.1%
				13:00	14:00	1,050	1.8%	21.2%	20.3%	1.0%	13.9%	4.5%	1.8%	2.0%	6.2%	0.6%	2.3%	4.7%	5.2%	1.6%	0.0%	12.9%
				14:00	15:00	1,000	1.5%	20.5%	28.5%	0.9%	13.3%	3.5%	1.4%	1.4%	6.7%	1.0%	1.6%	3.3%	3.4%	1.1%	0.0%	11.9%
				15:00	16:00	1,150	2.3%	23.7%	29.2%	0.9%	12.7%	3.8%	1.6%	1.6%	5.2%	1.7%	1.2%	2.4%	2.4%	0.8%	0.0%	10.5%
				16:00	17:00	1,050	3.4%	29.5%	26.2%	0.7%	9.7%	2.0%	0.7%	0.7%	4.1%	0.4%	2.0%	2.4%	4.3%	0.9%	0.0%	11.7%
				17:00	18:00	1,250	4.1%	36.3%	27.1%	0.5%	6.3%	3.2%	1.3%	1.3%	3.5%	1.0%	1.0%	2.0%	2.0%	0.6%	0.0%	9.8%
				18:00	19:00	1,100	4.3%	38.8%	29.6%	0.3%	5.0%	2.1%	0.8%	0.9%	1.7%	0.8%	1.5%	2.6%	2.9%	0.6%	0.0%	8.1%
				19:00	20:00	1,000	2.7%	39.5%	33.9%	0.1%	2.1%	0.5%	0.2%	0.2%	6.9%	0.7%	0.5%	1.2%	1.3%	0.4%	0.0%	9.8%
				20:00	21:00	850	2.8%	31.2%	40.2%	0.1%	1.8%	0.2%	0.2%	0.2%	10.6%	0.4%	0.2%	0.5%	0.4%	0.2%	0.0%	11.0%
				21:00	22:00	800	2.0%	27.0%	44.1%	0.1%	2.4%	0.5%	0.3%	0.3%	10.0%	0.0%	0.1%	0.3%	0.0%	0.1%	0.0%	12.8%
				22:00	23:00	750	2.7%	20.7%	44.6%	0.1%	1.3%	0.0%	0.0%	0.0%	10.4%	0.5%	0.4%	0.8%	0.4%	0.4%	0.0%	17.7%
				23:00	0:00	650	2.8%	19.9%	44.2%	0.2%	1.6%	0.0%	0.0%	0.0%	10.5%	0.7%	0.5%	1.0%	0.5%	0.5%	0.0%	17.6%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L015	To Kwa Wan Road (NB)	Lok Shan road	Kwei Chow Street	0:00	1:00	150	2.1%	12.8%	58.2%	0.0%	0.7%	0.0%	0.0%	0.0%	0.7%	0.7%	1.4%	0.7%	0.7%	0.0%	21.3%	
				1:00	2:00	100	3.1%	15.6%	79.7%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.2%	14.5%	80.7%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.5%	15.9%	77.3%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.5%	15.0%	80.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.1%	12.8%	74.6%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	2.1%	2.1%	2.1%	0.0%	0.0%
				6:00	7:00	150	1.6%	9.0%	52.7%	0.0%	1.6%	0.8%	0.0%	0.0%	0.8%	0.8%	0.8%	2.5%	1.6%	0.8%	0.0%	27.0%
				7:00	8:00	350	1.8%	9.8%	34.3%	0.0%	0.6%	1.5%	0.3%	0.3%	0.0%	0.9%	0.9%	1.8%	1.5%	1.2%	0.0%	45.1%
				8:00	9:00	450	4.2%	17.0%	30.1%	0.0%	1.2%	6.2%	1.2%	1.2%	0.0%	0.7%	0.7%	1.7%	1.2%	1.0%	0.0%	33.6%
				9:00	10:00	450	3.0%	12.9%	30.4%	0.0%	1.2%	6.4%	1.2%	1.2%	0.0%	1.2%	1.0%	2.2%	1.7%	1.2%	0.0%	36.4%
				10:00	11:00	400	2.5%	13.1%	30.8%	0.3%	6.8%	6.8%	2.2%	2.2%	0.8%	0.5%	0.5%	1.6%	1.4%	0.8%	0.0%	29.7%
				11:00	12:00	350	1.7%	13.3%	27.5%	0.3%	9.0%	2.6%	0.9%	0.9%	1.4%	1.7%	0.9%	2.3%	2.0%	0.9%	0.0%	34.6%
				12:00	13:00	350	2.1%	17.9%	35.2%	0.3%	6.1%	2.4%	0.9%	0.9%	1.2%	2.1%	0.6%	0.9%	0.9%	0.3%	0.0%	28.2%
				13:00	14:00	400	1.3%	10.5%	20.7%	0.3%	6.6%	7.6%	2.6%	2.6%	1.0%	0.5%	1.8%	3.9%	3.7%	1.8%	0.0%	35.1%
				14:00	15:00	400	1.1%	10.3%	29.7%	0.3%	6.4%	6.1%	1.9%	1.9%	1.1%	0.8%	1.1%	2.8%	2.5%	1.1%	0.0%	32.9%
				15:00	16:00	450	1.7%	12.2%	31.0%	0.2%	6.1%	6.8%	2.4%	2.2%	1.0%	1.2%	1.0%	2.2%	1.7%	1.0%	0.0%	29.3%
				16:00	17:00	400	1.3%	16.9%	26.3%	0.3%	9.4%	2.1%	1.1%	1.1%	2.1%	0.3%	1.6%	3.8%	3.8%	1.1%	0.0%	28.8%
				17:00	18:00	450	1.6%	21.8%	29.0%	0.2%	6.4%	3.4%	2.1%	1.8%	1.8%	0.9%	0.9%	2.1%	1.8%	0.7%	0.0%	25.5%
				18:00	19:00	400	1.6%	24.1%	32.4%	0.3%	5.5%	2.5%	1.4%	1.4%	0.8%	0.8%	1.4%	2.7%	2.7%	0.8%	0.0%	21.6%
				19:00	20:00	350	2.4%	21.8%	38.6%	0.0%	1.2%	1.2%	0.3%	0.3%	1.2%	0.6%	0.6%	1.2%	0.9%	0.3%	0.0%	29.4%
				20:00	21:00	300	2.4%	16.6%	43.8%	0.0%	1.0%	0.3%	0.3%	0.3%	1.7%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	32.1%
				21:00	22:00	300	1.4%	13.6%	45.5%	0.0%	1.1%	0.7%	0.4%	0.4%	1.4%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	35.1%
				22:00	23:00	350	2.0%	9.2%	41.2%	0.0%	0.7%	0.0%	0.0%	0.0%	1.6%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	43.4%
				L016	Mei King Street (SB)	Mei Kwong Street	Lok Shan road	23:00	0:00	250	2.0%	8.9%	41.1%	0.0%	0.8%	0.0%	0.0%	0.0%	1.6%	0.4%	0.4%	0.8%
0:00	1:00	50	2.1%					23.4%	72.4%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	50	0.0%					25.0%	75.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	50	0.0%					22.2%	77.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	50	0.0%					26.3%	73.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	50	0.0%					23.5%	76.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	50	0.0%					22.2%	77.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	50	0.0%					19.4%	72.2%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	0.0%	2.8%	0.0%	0.0%
7:00	8:00	150	0.8%					31.7%	59.5%	0.0%	2.4%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	2.4%	0.0%	0.0%
8:00	9:00	200	1.6%					44.7%	42.9%	0.0%	4.3%	2.2%	1.1%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	1.6%	0.0%	0.0%
9:00	10:00	200	1.2%					37.3%	48.3%	0.0%	4.8%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	2.4%	0.0%	0.0%
10:00	11:00	100	1.0%					32.3%	49.0%	0.0%	9.4%	4.2%	2.1%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%
11:00	12:00	100	1.2%					33.7%	45.3%	0.0%	12.8%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	2.3%	0.0%	0.0%
12:00	13:00	100	1.0%					38.8%	49.1%	0.0%	7.1%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%
13:00	14:00	100	1.2%					31.7%	39.0%	0.0%	11.0%	4.9%	2.4%	0.0%	0.0%	0.0%	0.0%	4.9%	0.0%	4.9%	0.0%	0.0%
14:00	15:00	100	1.1%					27.3%	50.0%	0.0%	9.1%	3.4%	2.3%	0.0%	0.0%	0.0%	0.0%	3.4%	0.0%	3.4%	0.0%	0.0%
15:00	16:00	150	1.0%					30.8%	50.0%	0.0%	8.7%	3.8%	1.9%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	1.9%	0.0%	0.0%
16:00	17:00	50	2.3%					34.9%	41.9%	0.0%	11.6%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	4.7%	0.0%	2.3%	0.0%	0.0%
17:00	18:00	100	1.8%					41.0%	41.1%	0.0%	7.1%	3.6%	1.8%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	1.8%	0.0%	0.0%
18:00	19:00	100	2.0%					41.2%	42.9%	0.0%	5.9%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	2.0%	0.0%	0.0%
19:00	20:00	150	1.0%					44.7%	50.3%	0.0%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%
20:00	21:00	100	1.2%					36.5%	61.1%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	100	1.3%					30.4%	67.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	100	1.4%					24.7%	71.1%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
23:00	0:00	100	1.7%	23.7%	71.2%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.7%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L017	Yuk Yat Street (NB)	Lok Shan road	Shun Fung Street	0:00	1:00	50	2.0%	24.5%	57.3%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.1%	2.0%	2.0%	0.0%	0.0%
				1:00	2:00	50	3.8%	26.9%	65.5%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	4.0%	24.0%	68.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.6%	27.8%	61.0%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	25.0%	68.7%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	18.2%	54.8%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	4.5%	4.5%	4.5%	4.5%	4.5%	4.5%	0.0%	0.0%
				6:00	7:00	50	2.2%	17.8%	48.9%	0.0%	6.7%	2.2%	0.0%	0.0%	0.0%	2.2%	4.4%	6.7%	6.7%	2.2%	0.0%	0.0%	
				7:00	8:00	150	0.8%	25.4%	38.9%	0.0%	7.9%	2.4%	0.0%	0.0%	0.0%	3.2%	3.2%	7.9%	7.1%	3.2%	0.0%	0.0%	
				8:00	9:00	200	1.0%	33.6%	26.4%	0.0%	12.2%	8.1%	1.0%	0.0%	0.0%	2.5%	2.5%	5.6%	5.1%	2.0%	0.0%	0.0%	
				9:00	10:00	200	0.5%	26.2%	27.4%	0.0%	12.0%	8.9%	1.0%	0.0%	0.0%	4.2%	3.1%	7.3%	6.8%	2.6%	0.0%	0.0%	
				10:00	11:00	150	2.1%	23.8%	26.5%	0.7%	18.9%	12.6%	1.4%	0.7%	0.0%	1.4%	2.1%	4.2%	4.2%	1.4%	0.0%	0.0%	
				11:00	12:00	150	1.4%	23.4%	22.7%	0.7%	23.4%	5.0%	0.7%	0.0%	0.0%	5.0%	2.8%	6.4%	6.4%	2.1%	0.0%	0.0%	
				12:00	13:00	150	1.5%	31.5%	30.1%	0.8%	16.5%	4.5%	0.8%	0.0%	0.0%	6.0%	1.5%	3.0%	3.0%	0.8%	0.0%	0.0%	
				13:00	14:00	200	1.2%	18.1%	16.8%	0.6%	16.8%	13.0%	1.9%	1.2%	0.0%	1.2%	5.0%	9.9%	10.6%	3.7%	0.0%	0.0%	
				14:00	15:00	150	0.7%	18.3%	25.5%	0.7%	17.6%	11.3%	1.4%	0.7%	0.0%	2.1%	3.5%	7.7%	7.7%	2.8%	0.0%	0.0%	
				15:00	16:00	200	1.2%	21.3%	26.3%	0.6%	17.1%	12.2%	1.8%	0.6%	0.0%	3.7%	2.4%	5.5%	5.5%	1.8%	0.0%	0.0%	
				16:00	17:00	150	2.6%	28.7%	21.7%	0.9%	13.0%	6.1%	0.9%	0.9%	0.0%	0.9%	4.3%	7.8%	9.6%	2.6%	0.0%	0.0%	
				17:00	18:00	150	3.6%	36.3%	23.4%	0.7%	8.8%	9.5%	1.5%	1.5%	0.0%	2.2%	2.2%	4.4%	4.4%	1.5%	0.0%	0.0%	
				18:00	19:00	150	3.3%	38.0%	24.8%	0.8%	6.6%	5.8%	0.8%	0.8%	0.0%	1.7%	3.3%	5.8%	6.6%	1.7%	0.0%	0.0%	
				19:00	20:00	150	1.7%	44.1%	37.1%	0.0%	3.4%	2.6%	0.0%	0.0%	0.0%	1.7%	1.7%	3.4%	3.4%	0.9%	0.0%	0.0%	
				20:00	21:00	100	2.3%	38.6%	49.1%	0.0%	3.4%	1.1%	0.0%	0.0%	0.0%	1.1%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	
				21:00	22:00	100	1.3%	35.1%	55.8%	0.0%	3.9%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	100	2.7%	27.4%	58.9%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.7%	1.4%	1.4%	0.0%	0.0%	
				23:00	0:00	100	3.3%	26.7%	56.6%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.3%	1.7%	1.7%	0.0%	0.0%	
L018	Yuk Yat Street (SB)	Shun Fung Street	Lok Shan road	0:00	1:00	100	4.7%	47.0%	37.6%	0.0%	3.5%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	2.4%	1.2%	0.0%	0.0%		
				1:00	2:00	50	6.3%	47.8%	41.7%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	4.7%	46.5%	46.5%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	50	6.1%	48.4%	39.4%	0.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	7.1%	46.4%	42.9%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	50	5.6%	38.7%	38.9%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	2.8%	2.8%	2.8%	2.8%	0.0%	0.0%		
				6:00	7:00	100	4.3%	36.5%	36.2%	0.0%	5.8%	1.4%	0.0%	0.0%	1.4%	2.9%	2.9%	4.3%	4.3%	0.0%	0.0%		
				7:00	8:00	200	1.1%	41.6%	32.6%	0.5%	6.3%	1.6%	0.5%	0.0%	0.0%	3.2%	3.2%	4.7%	4.7%	0.0%	0.0%		
				8:00	9:00	350	2.2%	51.5%	20.4%	0.3%	8.8%	4.7%	0.9%	0.6%	0.0%	2.2%	2.2%	3.1%	3.1%	0.0%	0.0%		
				9:00	10:00	300	1.7%	43.1%	22.6%	0.3%	9.1%	5.6%	1.0%	0.7%	0.0%	3.8%	3.1%	4.5%	4.5%	0.0%	0.0%		
				10:00	11:00	250	4.5%	44.2%	18.0%	0.4%	17.6%	5.7%	1.2%	0.8%	0.8%	1.2%	1.2%	2.0%	2.0%	0.4%	0.0%		
				11:00	12:00	250	2.9%	43.0%	15.4%	0.8%	22.1%	2.1%	0.4%	0.4%	1.3%	3.3%	1.7%	2.9%	3.3%	0.4%	0.0%		
				12:00	13:00	250	3.2%	53.8%	18.5%	0.4%	13.7%	2.0%	0.4%	0.4%	0.8%	3.2%	0.8%	1.2%	1.6%	0.0%	0.0%		
				13:00	14:00	250	2.5%	37.9%	12.9%	0.4%	17.9%	7.1%	1.7%	1.3%	0.8%	0.8%	3.8%	5.8%	6.3%	0.8%	0.0%		
				14:00	15:00	250	2.3%	37.6%	18.9%	0.5%	18.0%	5.9%	1.4%	0.9%	0.9%	1.8%	2.7%	4.1%	4.5%	0.5%	0.0%		
				15:00	16:00	300	3.3%	41.4%	18.5%	0.4%	16.3%	5.9%	1.5%	1.1%	0.7%	2.6%	1.9%	3.0%	3.0%	0.4%	0.0%		
				16:00	17:00	250	5.7%	55.9%	10.2%	0.4%	15.2%	1.6%	0.4%	0.4%	2.0%	0.4%	1.2%	2.5%	2.9%	1.2%	0.0%		
				17:00	18:00	350	6.9%	64.0%	10.0%	0.3%	9.1%	2.5%	0.6%	0.6%	1.6%	0.6%	0.6%	1.3%	1.3%	0.6%	0.0%		
				18:00	19:00	300	7.0%	66.7%	10.6%	0.4%	7.0%	1.8%	0.4%	0.4%	0.7%	0.4%	0.7%	1.4%	1.8%	0.7%	0.0%		
				19:00	20:00	250	3.7%	67.0%	20.7%	0.0%	2.5%	0.8%	0.0%	0.0%	0.8%	0.8%	1.2%	1.7%	0.0%	0.0%	0.0%		
				20:00	21:00	200	4.5%	60.5%	28.1%	0.0%	2.2%	0.6%	0.0%	0.0%	1.7%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%		
				21:00	22:00	150	3.3%	56.7%	33.3%	0.0%	4.0%	0.7%	0.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				22:00	23:00	150	5.3%	48.8%	37.4%	0.0%	2.3%	0.0%	0.0%	0.0%	2.3%	0.8%	0.8%	1.5%	0.8%	0.0%	0.0%		
				23:00	0:00	150	5.6%	46.8%	37.4%	0.0%	2.8%	0.0%	0.0%	0.0%	2.8%	0.9%	0.9%	1.9%	0.9%	0.0%	0.0%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L019	Lok Shan road (WB)	Yuk Yat Street	Mei King Street	0:00	1:00	100	10.6%	41.1%	31.8%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	4.5%	3.0%	1.5%	0.0%	0.0%
				1:00	2:00	50	11.4%	45.8%	37.1%	0.0%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	9.7%	41.9%	41.9%	0.0%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	12.5%	45.9%	33.3%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	14.3%	42.8%	38.1%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	10.0%	30.0%	30.0%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	3.3%	6.7%	6.7%	3.3%	0.0%	0.0%	0.0%
				6:00	7:00	100	6.6%	27.9%	26.2%	0.0%	8.2%	1.6%	0.0%	0.0%	0.0%	0.0%	3.3%	3.3%	9.8%	8.2%	4.9%	0.0%	0.0%	
				7:00	8:00	150	5.4%	37.8%	18.9%	0.7%	8.1%	2.0%	0.0%	0.0%	0.0%	0.0%	3.4%	3.4%	8.1%	8.1%	4.1%	0.0%	0.0%	
				8:00	9:00	300	8.5%	44.1%	11.2%	0.4%	11.5%	6.2%	0.8%	0.4%	0.0%	0.0%	2.3%	2.3%	5.0%	5.0%	2.3%	0.0%	0.0%	
				9:00	10:00	250	6.7%	36.7%	12.2%	0.4%	11.8%	7.1%	0.8%	0.4%	0.0%	0.0%	3.8%	2.9%	6.7%	7.1%	3.4%	0.0%	0.0%	
				10:00	11:00	250	7.5%	31.8%	12.3%	0.4%	21.6%	9.7%	0.9%	0.4%	0.0%	0.0%	1.8%	2.2%	4.4%	4.8%	2.2%	0.0%	0.0%	
				11:00	12:00	250	4.8%	30.2%	10.5%	0.9%	26.2%	3.5%	0.4%	0.4%	0.0%	0.0%	4.8%	2.6%	6.1%	7.0%	2.6%	0.0%	0.0%	
				12:00	13:00	250	6.6%	41.9%	13.7%	0.5%	18.4%	3.3%	0.5%	0.5%	0.0%	0.0%	5.7%	1.4%	2.8%	3.3%	1.4%	0.0%	0.0%	
				13:00	14:00	300	3.9%	23.5%	7.8%	0.4%	19.1%	10.1%	1.2%	0.8%	0.0%	0.0%	1.2%	5.1%	10.5%	11.7%	4.7%	0.0%	0.0%	
				14:00	15:00	250	4.1%	25.5%	12.3%	0.5%	20.5%	9.1%	0.9%	0.5%	0.0%	0.0%	2.3%	3.7%	8.2%	8.7%	3.7%	0.0%	0.0%	
				15:00	16:00	300	5.8%	29.2%	12.5%	0.4%	19.5%	9.7%	1.2%	0.4%	0.0%	0.0%	3.9%	2.7%	5.8%	6.2%	2.7%	0.0%	0.0%	
				16:00	17:00	300	6.2%	33.9%	10.1%	0.4%	17.1%	3.9%	0.4%	0.4%	0.0%	0.0%	0.8%	4.3%	8.9%	10.1%	3.5%	0.0%	0.0%	
				17:00	18:00	300	8.4%	44.2%	11.1%	0.3%	11.7%	6.4%	0.7%	0.3%	0.0%	0.0%	2.3%	2.3%	5.0%	5.0%	2.3%	0.0%	0.0%	
				18:00	19:00	300	8.2%	45.0%	11.6%	0.4%	9.0%	4.1%	0.4%	0.4%	0.0%	0.0%	1.9%	3.0%	6.3%	7.1%	2.6%	0.0%	0.0%	
				19:00	20:00	200	7.9%	57.2%	16.9%	0.0%	3.7%	1.6%	0.0%	0.0%	0.0%	0.0%	2.1%	1.6%	3.7%	3.7%	1.6%	0.0%	0.0%	
				20:00	21:00	150	10.0%	55.4%	24.6%	0.0%	3.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.8%	0.8%	1.5%	1.5%	0.8%	0.0%	0.0%	
				21:00	22:00	150	8.3%	52.8%	29.6%	0.0%	5.6%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%	
				22:00	23:00	100	11.1%	43.6%	32.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	1.0%	3.0%	2.0%	2.0%	0.0%	0.0%	
L020	Lok Shan road (WB)	Mei King Street	To Kwa Wan Road	23:00	0:00	100	12.2%	40.3%	31.7%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	1.2%	3.7%	2.4%	2.4%	0.0%	0.0%	
				0:00	1:00	150	5.5%	30.5%	55.4%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.8%	2.3%	0.8%	1.6%	0.0%	0.0%	
				1:00	2:00	100	5.5%	31.5%	60.3%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	4.5%	29.9%	64.1%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	6.0%	32.0%	58.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	4.7%	30.2%	62.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	3.6%	25.5%	54.7%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	3.6%	1.8%	3.6%	0.0%	0.0%	
				6:00	7:00	150	3.8%	23.1%	53.0%	0.0%	3.8%	1.0%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	4.8%	3.8%	2.9%	0.0%	0.0%	
				7:00	8:00	300	2.9%	35.5%	40.4%	0.4%	4.3%	1.1%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	5.0%	3.9%	2.9%	0.0%	0.0%	
				8:00	9:00	450	5.4%	45.8%	26.8%	0.2%	6.5%	3.4%	0.5%	0.5%	0.0%	0.0%	1.4%	1.4%	3.4%	2.7%	2.0%	0.0%	0.0%	
				9:00	10:00	450	4.2%	38.4%	29.9%	0.2%	6.7%	4.0%	0.5%	0.5%	0.0%	0.0%	2.2%	1.7%	4.7%	4.0%	3.0%	0.0%	0.0%	
				10:00	11:00	350	5.0%	33.8%	30.5%	0.3%	13.5%	6.0%	0.6%	0.6%	0.0%	0.0%	0.9%	1.3%	3.1%	2.5%	1.9%	0.0%	0.0%	
				11:00	12:00	350	3.3%	33.2%	26.7%	0.7%	17.6%	2.3%	0.3%	0.3%	0.0%	0.0%	2.9%	1.6%	4.6%	3.9%	2.6%	0.0%	0.0%	
				12:00	13:00	350	4.1%	41.9%	31.6%	0.3%	11.1%	1.9%	0.3%	0.3%	0.0%	0.0%	3.2%	0.6%	1.9%	1.9%	0.9%	0.0%	0.0%	
				13:00	14:00	350	2.8%	28.6%	21.1%	0.3%	13.9%	6.9%	0.9%	0.9%	0.0%	0.0%	0.9%	3.5%	8.2%	7.3%	4.7%	0.0%	0.0%	
				14:00	15:00	300	2.7%	27.7%	30.6%	0.3%	13.3%	5.7%	0.7%	0.7%	0.0%	0.0%	1.7%	2.3%	6.0%	5.0%	3.3%	0.0%	0.0%	
				15:00	16:00	400	3.7%	31.2%	30.7%	0.3%	12.7%	5.9%	0.8%	0.8%	0.0%	0.0%	2.3%	1.7%	4.2%	3.4%	2.3%	0.0%	0.0%	
				16:00	17:00	300	4.4%	36.3%	24.8%	0.3%	12.6%	2.7%	0.3%	0.3%	0.0%	0.0%	0.3%	2.7%	6.1%	6.1%	3.1%	0.0%	0.0%	
				17:00	18:00	400	5.3%	44.3%	25.8%	0.3%	8.1%	4.2%	0.6%	0.6%	0.0%	0.0%	1.4%	1.4%	3.3%	2.8%	1.9%	0.0%	0.0%	
				18:00	19:00	350	5.2%	45.2%	26.8%	0.3%	6.1%	2.7%	0.3%	0.3%	0.0%	0.0%	1.2%	1.8%	4.0%	4.0%	2.1%	0.0%	0.0%	
				19:00	20:00	350	4.5%	51.2%	35.0%	0.0%	1.9%	1.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.6%	1.9%	1.9%	1.0%	0.0%	0.0%	
				20:00	21:00	250	5.0%	44.6%	45.5%	0.0%	1.7%	0.4%	0.0%	0.0%	0.0%	0.0%	0.4%	0.4%	0.8%	0.4%	0.8%	0.0%	0.0%	
				21:00	22:00	250	3.8%	39.8%	51.7%	0.0%	2.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	
22:00	23:00	200	5.2%	32.5%	55.8%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.5%	1.5%	0.5%	1.5%	0.0%	0.0%					
23:00	0:00	200	5.1%	31.2%	55.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.6%	1.9%	0.6%	1.9%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L021	To Kwa Wan Road (SB)	Lok Shan road	Chi Kiang Street	0:00	1:00	400	3.2%	21.0%	55.3%	0.0%	2.1%	0.0%	0.0%	0.0%	6.6%	0.5%	0.5%	1.3%	0.8%	0.5%	0.0%	8.2%
				1:00	2:00	200	3.7%	24.1%	67.0%	0.0%	3.1%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	3.4%	21.9%	70.3%	0.0%	2.2%	0.0%	0.0%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	4.7%	24.4%	66.2%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	3.5%	21.9%	70.2%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.9%	19.4%	63.4%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.9%	2.2%	1.4%	0.0%	0.0%
				6:00	7:00	350	2.2%	15.3%	49.9%	0.3%	4.1%	0.6%	0.0%	0.0%	7.2%	1.3%	1.6%	2.8%	2.8%	1.3%	0.0%	10.6%
				7:00	8:00	850	1.2%	20.7%	33.3%	0.5%	5.2%	0.5%	0.2%	0.2%	11.8%	1.2%	1.1%	2.5%	2.4%	1.0%	0.0%	18.2%
				8:00	9:00	1,150	2.6%	32.0%	26.2%	0.6%	9.4%	2.0%	0.7%	0.7%	6.9%	1.0%	1.0%	2.1%	2.0%	0.8%	0.0%	12.0%
				9:00	10:00	1,050	1.9%	25.9%	28.2%	0.6%	9.5%	2.2%	0.8%	0.8%	6.5%	1.6%	1.3%	2.8%	2.8%	1.2%	0.0%	13.9%
				10:00	11:00	950	3.0%	22.8%	30.1%	1.0%	14.1%	3.9%	1.3%	1.3%	4.7%	0.7%	0.9%	1.8%	1.8%	0.6%	0.0%	12.0%
				11:00	12:00	1,000	1.9%	21.5%	25.0%	1.2%	17.3%	1.5%	0.5%	0.4%	8.2%	2.2%	1.3%	2.5%	2.7%	0.8%	0.0%	13.0%
				12:00	13:00	950	2.4%	28.6%	31.8%	0.9%	11.6%	1.3%	0.5%	0.4%	6.3%	2.5%	0.5%	1.1%	1.3%	0.4%	0.0%	10.4%
				13:00	14:00	950	1.8%	19.1%	20.8%	1.1%	14.2%	4.7%	1.5%	1.6%	5.9%	0.6%	2.4%	4.8%	5.2%	1.7%	0.0%	14.6%
				14:00	15:00	950	1.5%	18.2%	29.0%	1.0%	13.5%	3.7%	1.2%	1.2%	6.5%	1.1%	1.6%	3.4%	3.5%	1.2%	0.0%	13.4%
				15:00	16:00	1,100	2.2%	21.2%	30.2%	0.9%	13.0%	4.0%	1.3%	1.3%	5.1%	1.8%	1.2%	2.5%	2.5%	0.9%	0.0%	11.9%
				16:00	17:00	950	3.3%	26.3%	27.6%	0.7%	9.9%	2.0%	0.5%	0.5%	4.5%	0.4%	2.1%	3.9%	4.5%	1.2%	0.0%	12.6%
				17:00	18:00	1,150	4.1%	32.9%	28.8%	0.5%	6.5%	3.3%	1.1%	1.1%	3.9%	1.1%	1.1%	2.1%	2.1%	0.7%	0.0%	10.7%
				18:00	19:00	1,000	4.3%	35.4%	31.5%	0.3%	5.3%	2.2%	0.7%	0.7%	1.8%	0.9%	1.6%	2.7%	3.0%	0.8%	0.0%	8.8%
				19:00	20:00	900	2.8%	36.0%	35.7%	0.1%	2.2%	0.6%	0.1%	0.2%	6.7%	0.8%	0.6%	1.2%	1.3%	0.4%	0.0%	11.3%
				20:00	21:00	800	2.8%	28.1%	41.7%	0.1%	1.8%	0.3%	0.1%	0.1%	10.5%	0.4%	0.3%	0.5%	0.4%	0.3%	0.0%	12.6%
				21:00	22:00	700	2.0%	24.1%	45.9%	0.1%	2.4%	0.6%	0.1%	0.1%	9.6%	0.0%	0.1%	0.3%	0.0%	0.1%	0.0%	14.6%
				22:00	23:00	700	2.6%	18.2%	45.4%	0.1%	1.3%	0.0%	0.0%	0.0%	10.1%	0.6%	0.4%	0.7%	0.4%	0.4%	0.0%	19.8%
23:00	0:00	600	2.8%	17.5%	45.1%	0.2%	1.8%	0.0%	0.0%	0.0%	9.9%	0.7%	0.5%	0.9%	0.5%	0.5%	0.0%	19.6%				
L022	To Kwa Wan Road (NB)	Chi Kiang Street	Lok Shan road	0:00	1:00	200	1.7%	15.7%	48.7%	0.0%	1.2%	0.0%	0.0%	0.0%	12.8%	0.6%	0.6%	1.2%	0.6%	0.6%	0.0%	16.3%
				1:00	2:00	100	2.8%	22.2%	72.2%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	2.9%	20.6%	75.0%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.1%	22.4%	71.5%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.3%	20.5%	74.9%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	1.9%	17.0%	67.8%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	1.9%	1.9%	1.9%	0.0%	0.0%
				6:00	7:00	200	1.3%	11.2%	42.6%	0.0%	2.0%	1.3%	0.0%	0.0%	13.2%	0.7%	1.3%	2.0%	2.0%	1.3%	0.0%	21.1%
				7:00	8:00	450	1.2%	12.7%	27.0%	0.2%	0.7%	1.5%	0.2%	0.2%	14.4%	0.7%	0.7%	1.5%	1.5%	1.0%	0.0%	36.5%
				8:00	9:00	500	3.0%	21.9%	23.7%	0.2%	1.6%	6.3%	1.2%	1.2%	9.3%	0.6%	0.6%	1.4%	1.2%	0.8%	0.0%	27.0%
				9:00	10:00	500	2.3%	16.9%	24.7%	0.2%	1.7%	6.9%	1.3%	1.3%	8.4%	1.0%	0.8%	1.9%	1.7%	1.0%	0.0%	29.9%
				10:00	11:00	450	1.8%	16.5%	25.9%	0.4%	7.6%	7.1%	2.2%	2.2%	8.7%	0.4%	0.7%	1.3%	1.3%	0.7%	0.0%	23.2%
				11:00	12:00	500	1.1%	15.6%	21.6%	0.7%	9.5%	2.6%	0.9%	0.7%	15.2%	1.5%	0.9%	2.0%	1.8%	0.9%	0.0%	25.0%
				12:00	13:00	450	1.4%	21.4%	28.3%	0.5%	6.6%	2.6%	0.9%	0.7%	12.0%	1.9%	0.5%	0.9%	0.9%	0.5%	0.0%	20.9%
				13:00	14:00	500	1.1%	13.1%	17.1%	0.4%	7.4%	8.0%	2.5%	2.5%	10.5%	0.4%	1.7%	3.4%	3.4%	1.7%	0.0%	26.8%
				14:00	15:00	500	0.9%	12.8%	24.3%	0.4%	7.1%	6.4%	2.0%	2.0%	11.7%	0.7%	1.1%	2.4%	2.2%	1.1%	0.0%	24.9%
				15:00	16:00	550	1.4%	15.2%	25.6%	0.4%	7.1%	7.3%	2.4%	2.2%	9.5%	1.2%	0.8%	1.8%	1.6%	0.8%	0.0%	22.7%
				16:00	17:00	500	1.0%	19.7%	22.5%	0.4%	9.9%	2.3%	1.0%	0.8%	12.0%	0.2%	1.7%	3.1%	3.3%	1.0%	0.0%	21.1%
				17:00	18:00	600	1.2%	25.3%	24.1%	0.4%	6.7%	3.7%	1.9%	1.8%	10.7%	0.9%	0.9%	1.8%	1.6%	0.7%	0.0%	18.3%
				18:00	19:00	500	1.3%	29.5%	28.4%	0.2%	5.9%	2.6%	1.3%	1.3%	5.3%	0.9%	1.3%	2.4%	2.4%	0.9%	0.0%	16.3%
				19:00	20:00	450	1.7%	26.2%	31.1%	0.0%	1.2%	1.2%	0.2%	0.2%	12.6%	0.5%	0.5%	1.0%	1.0%	0.5%	0.0%	22.1%
				20:00	21:00	400	1.6%	19.4%	33.8%	0.0%	1.0%	0.5%	0.3%	0.3%	18.3%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	23.3%
				21:00	22:00	400	1.1%	16.3%	36.8%	0.0%	1.1%	0.8%	0.3%	0.3%	16.6%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	26.4%
				22:00	23:00	400	1.3%	11.7%	34.4%	0.0%	0.5%	0.0%	0.0%	0.0%	16.3%	0.3%	0.3%	0.5%	0.3%	0.3%	0.0%	34.1%
23:00	0:00	350	1.6%	11.1%	34.5%	0.0%	0.7%	0.0%	0.0%	0.0%	16.4%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	33.8%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L023	Yuk Yat Street (NB)	Chi Kiang Street	Lok Shan road	0:00	1:00	150	2.5%	26.3%	56.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	4.2%	2.5%	1.7%	0.0%	
				1:00	2:00	100	3.2%	28.6%	63.4%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.4%	25.9%	67.3%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.5%	27.3%	61.4%	0.0%	6.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.6%	25.6%	64.1%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	0.0%	0.0%	
				5:00	6:00	100	1.8%	20.0%	50.9%	0.0%	5.5%	0.0%	0.0%	0.0%	0.0%	1.8%	3.6%	7.3%	5.5%	3.6%	0.0%	0.0%	
				6:00	7:00	150	1.9%	17.8%	46.7%	0.0%	5.6%	1.9%	0.0%	0.0%	0.0%	3.7%	3.7%	8.4%	7.5%	2.8%	0.0%	0.0%	
				7:00	8:00	300	1.1%	24.4%	37.8%	0.4%	7.4%	2.2%	0.4%	0.0%	0.0%	4.1%	3.7%	8.1%	7.8%	2.6%	0.0%	0.0%	
				8:00	9:00	450	1.9%	32.9%	25.9%	0.5%	11.4%	6.8%	1.0%	0.5%	0.0%	2.9%	2.9%	5.8%	5.6%	1.9%	0.0%	0.0%	
				9:00	10:00	450	1.5%	25.6%	26.8%	0.5%	10.9%	7.2%	1.0%	0.5%	0.0%	4.7%	3.7%	7.4%	7.7%	2.5%	0.0%	0.0%	
				10:00	11:00	350	2.6%	24.3%	25.8%	0.6%	18.5%	11.0%	1.4%	1.2%	0.0%	1.7%	2.0%	4.6%	4.6%	1.7%	0.0%	0.0%	
				11:00	12:00	350	1.7%	23.4%	21.9%	0.9%	23.3%	4.4%	0.6%	0.3%	0.0%	5.2%	2.9%	6.4%	7.0%	2.0%	0.0%	0.0%	
				12:00	13:00	350	2.2%	32.1%	28.8%	0.6%	16.3%	4.1%	0.6%	0.3%	0.0%	6.3%	1.6%	2.8%	3.4%	0.9%	0.0%	0.0%	
				13:00	14:00	400	1.3%	18.0%	16.0%	0.5%	16.8%	11.9%	1.6%	1.3%	0.0%	1.3%	5.2%	10.9%	11.6%	3.6%	0.0%	0.0%	
				14:00	15:00	350	1.2%	18.8%	24.3%	0.6%	17.4%	10.1%	1.2%	0.9%	0.0%	2.6%	3.8%	8.1%	8.4%	2.6%	0.0%	0.0%	
				15:00	16:00	400	1.8%	21.8%	25.1%	0.5%	16.5%	11.0%	1.5%	1.0%	0.0%	4.0%	2.8%	6.0%	6.0%	2.0%	0.0%	0.0%	
				16:00	17:00	350	3.3%	29.3%	20.5%	0.3%	14.5%	5.0%	0.6%	0.6%	0.0%	0.6%	4.2%	8.6%	9.8%	2.7%	0.0%	0.0%	
				17:00	18:00	400	4.0%	37.3%	22.1%	0.3%	9.8%	8.5%	1.0%	1.0%	0.0%	2.3%	2.3%	4.8%	4.8%	1.8%	0.0%	0.0%	
				18:00	19:00	400	3.9%	38.8%	23.2%	0.3%	7.6%	5.3%	0.6%	0.6%	0.0%	2.0%	3.1%	5.9%	6.7%	2.0%	0.0%	0.0%	
				19:00	20:00	300	2.8%	43.9%	35.2%	0.0%	3.5%	1.8%	0.0%	0.4%	0.0%	2.1%	1.8%	3.5%	3.9%	1.1%	0.0%	0.0%	
				20:00	21:00	250	3.3%	39.4%	47.1%	0.0%	3.3%	0.9%	0.0%	0.0%	0.0%	0.9%	0.9%	1.9%	1.4%	0.9%	0.0%	0.0%	
				21:00	22:00	200	2.2%	35.7%	54.1%	0.0%	4.3%	2.2%	0.0%	0.0%	0.0%	0.0%	0.5%	0.5%	0.0%	0.5%	0.0%	0.0%	
				22:00	23:00	200	3.4%	28.7%	57.1%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	1.7%	1.1%	2.9%	1.7%	1.1%	0.0%	0.0%
				L024	Yuk Yat Street (SB)	Lok Shan road	Chi Kiang Street	23:00	0:00	150	3.5%	27.1%	55.4%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	1.4%	3.5%
0:00	1:00	100	3.8%					55.1%	37.2%	0.0%	2.6%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	50	4.3%					53.1%	38.3%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	2.4%					53.7%	41.5%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	3.1%					53.1%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	3.7%					51.9%	40.7%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	3.3%					50.0%	40.0%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	100	3.6%					48.1%	39.3%	0.0%	5.4%	1.8%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
7:00	8:00	200	0.0%					58.6%	34.6%	0.0%	4.3%	1.9%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
8:00	9:00	300	0.0%					66.3%	19.9%	0.0%	5.7%	5.7%	1.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
9:00	10:00	250	0.0%					59.9%	23.7%	0.0%	6.4%	7.2%	2.0%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
10:00	11:00	250	3.2%					53.7%	17.7%	0.0%	16.8%	5.0%	1.8%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
11:00	12:00	250	2.0%					54.7%	16.2%	0.0%	22.1%	2.0%	1.0%	0.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
12:00	13:00	250	2.2%					63.1%	17.8%	0.0%	13.0%	1.7%	0.9%	0.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
13:00	14:00	200	2.1%					52.1%	14.2%	0.0%	19.5%	6.8%	2.6%	1.6%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
14:00	15:00	200	1.6%					50.0%	20.1%	0.0%	18.5%	5.4%	2.2%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
15:00	16:00	250	2.6%					52.4%	19.0%	0.0%	16.4%	5.2%	2.2%	1.3%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
16:00	17:00	250	5.6%					62.7%	9.8%	0.0%	18.6%	0.0%	0.5%	0.5%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
17:00	18:00	300	6.2%					70.4%	9.3%	0.0%	11.0%	0.0%	0.7%	0.7%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
18:00	19:00	300	6.3%					73.7%	9.8%	0.0%	8.6%	0.0%	0.4%	0.4%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
19:00	20:00	250	2.6%					74.8%	18.7%	0.0%	2.1%	0.9%	0.0%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
20:00	21:00	200	2.9%					67.2%	25.3%	0.0%	2.3%	0.6%	0.0%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	150	2.0%					62.4%	29.5%	0.0%	3.4%	0.7%	0.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	150	3.2%					56.5%	35.5%	0.0%	2.4%	0.0%	0.0%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	100	4.0%	55.0%	35.0%	0.0%	3.0%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L025	Chi Kiang Street (SB)	Yuk Yat Street	Bailey Street	0:00	1:00	50	0.0%	60.0%	0.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	5.0%	10.0%	5.0%	10.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	87.5%	0.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	83.3%	0.0%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	36.3%	0.0%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	9.1%	9.1%	9.1%	18.2%	0.0%	0.0%
				6:00	7:00	50	0.0%	33.2%	0.0%	0.0%	4.8%	4.8%	0.0%	0.0%	0.0%	4.8%	9.5%	14.3%	14.3%	14.3%	0.0%	0.0%	
				7:00	8:00	100	0.0%	52.4%	0.0%	0.0%	1.7%	3.4%	1.7%	1.7%	0.0%	5.1%	5.1%	8.5%	10.2%	10.2%	0.0%	0.0%	
				8:00	9:00	150	0.0%	59.0%	0.0%	0.0%	2.8%	10.2%	2.8%	2.8%	0.0%	2.8%	2.8%	5.6%	5.6%	5.6%	0.0%	0.0%	
				9:00	10:00	150	0.0%	47.3%	0.0%	0.0%	2.9%	11.8%	2.9%	2.9%	0.0%	4.9%	3.9%	7.8%	7.8%	7.8%	0.0%	0.0%	
				10:00	11:00	100	0.0%	37.4%	0.0%	0.0%	16.5%	14.1%	2.4%	2.4%	0.0%	2.4%	3.5%	7.1%	7.1%	7.1%	0.0%	0.0%	
				11:00	12:00	100	0.0%	34.8%	0.0%	0.0%	20.2%	4.5%	1.1%	1.1%	0.0%	7.9%	4.5%	9.0%	9.0%	7.9%	0.0%	0.0%	
				12:00	13:00	100	0.0%	51.9%	0.0%	0.0%	14.3%	5.2%	1.3%	1.3%	0.0%	10.4%	2.6%	3.9%	5.2%	3.9%	0.0%	0.0%	
				13:00	14:00	150	0.0%	23.5%	0.0%	0.0%	12.3%	12.3%	1.8%	1.8%	0.0%	1.8%	7.0%	13.2%	14.0%	12.3%	0.0%	0.0%	
				14:00	15:00	100	0.0%	27.9%	0.0%	0.0%	14.4%	12.2%	2.2%	2.2%	0.0%	3.3%	5.6%	11.1%	11.1%	10.0%	0.0%	0.0%	
				15:00	16:00	100	0.0%	34.0%	0.0%	0.0%	15.0%	13.0%	2.0%	2.0%	0.0%	6.0%	4.0%	8.0%	8.0%	8.0%	0.0%	0.0%	
				16:00	17:00	150	0.0%	28.7%	0.0%	0.0%	19.0%	3.8%	0.0%	0.0%	0.0%	1.0%	7.6%	13.3%	15.2%	11.4%	0.0%	0.0%	
				17:00	18:00	150	0.0%	42.9%	0.0%	0.0%	15.0%	7.5%	0.0%	0.0%	0.0%	4.7%	8.4%	8.4%	8.4%	8.4%	0.0%	0.0%	
				18:00	19:00	100	0.0%	43.4%	0.0%	0.0%	11.1%	5.1%	0.0%	0.0%	0.0%	4.0%	6.1%	10.1%	11.1%	9.1%	0.0%	0.0%	
				19:00	20:00	100	0.0%	71.5%	0.0%	0.0%	3.0%	3.0%	0.0%	0.0%	0.0%	3.0%	3.0%	6.0%	6.0%	4.5%	0.0%	0.0%	
				20:00	21:00	50	0.0%	80.0%	0.0%	0.0%	2.5%	2.5%	0.0%	0.0%	0.0%	2.5%	2.5%	2.5%	2.5%	5.0%	0.0%	0.0%	
				21:00	22:00	50	0.0%	83.4%	0.0%	0.0%	6.7%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	0.0%	3.3%	0.0%	0.0%	
				22:00	23:00	50	0.0%	70.4%	0.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	3.7%	3.7%	7.4%	3.7%	7.4%	0.0%	0.0%	
				23:00	0:00	50	0.0%	65.4%	0.0%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	4.3%	4.3%	8.7%	4.3%	8.7%	0.0%	0.0%	
L026	Chi Kiang Street (NB)	Bailey Street	Yuk Yat Street	0:00	1:00	50	0.0%	63.6%	0.0%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	9.1%	9.1%	0.0%	0.0%
				1:00	2:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	49.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	16.7%	16.7%	16.7%	0.0%	0.0%
				6:00	7:00	50	0.0%	30.7%	0.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	7.7%	7.7%	15.4%	15.4%	15.4%	0.0%	0.0%	
				7:00	8:00	50	0.0%	53.3%	0.0%	0.0%	0.0%	3.3%	0.0%	0.0%	0.0%	6.7%	6.7%	10.0%	10.0%	10.0%	0.0%	0.0%	
				8:00	9:00	100	0.0%	60.5%	0.0%	0.0%	1.8%	10.7%	1.8%	1.8%	0.0%	3.6%	3.6%	5.4%	5.4%	5.4%	0.0%	0.0%	
				9:00	10:00	100	0.0%	50.1%	0.0%	0.0%	1.9%	11.5%	1.9%	1.9%	0.0%	5.8%	3.8%	7.7%	7.7%	7.7%	0.0%	0.0%	
				10:00	11:00	100	0.0%	37.8%	0.0%	0.0%	17.0%	13.2%	1.9%	1.9%	0.0%	1.9%	3.8%	7.5%	7.5%	7.5%	0.0%	0.0%	
				11:00	12:00	100	0.0%	34.5%	0.0%	0.0%	20.0%	5.5%	0.0%	0.0%	0.0%	7.3%	3.6%	9.1%	10.9%	9.1%	0.0%	0.0%	
				12:00	13:00	50	0.0%	52.3%	0.0%	0.0%	15.2%	4.3%	0.0%	0.0%	0.0%	10.9%	2.2%	4.3%	6.5%	4.3%	0.0%	0.0%	
				13:00	14:00	100	0.0%	22.2%	0.0%	0.0%	12.5%	12.5%	1.4%	1.4%	0.0%	1.4%	6.9%	13.9%	15.3%	12.5%	0.0%	0.0%	
				14:00	15:00	100	0.0%	26.2%	0.0%	0.0%	14.0%	12.3%	1.8%	1.8%	0.0%	3.5%	5.3%	12.3%	12.3%	10.5%	0.0%	0.0%	
				15:00	16:00	100	0.0%	31.8%	0.0%	0.0%	14.3%	12.7%	1.6%	1.6%	0.0%	6.3%	4.8%	9.5%	9.5%	7.9%	0.0%	0.0%	
				16:00	17:00	100	0.0%	28.5%	0.0%	0.0%	18.2%	3.9%	0.0%	0.0%	0.0%	1.3%	6.5%	14.3%	15.6%	11.7%	0.0%	0.0%	
				17:00	18:00	100	0.0%	42.8%	0.0%	0.0%	14.3%	7.8%	0.0%	0.0%	0.0%	3.9%	3.9%	9.1%	9.1%	9.1%	0.0%	0.0%	
				18:00	19:00	100	0.0%	43.0%	0.0%	0.0%	11.1%	4.2%	0.0%	0.0%	0.0%	2.8%	5.6%	11.1%	12.5%	9.7%	0.0%	0.0%	
				19:00	20:00	50	0.0%	72.5%	0.0%	0.0%	2.5%	2.5%	0.0%	0.0%	0.0%	2.5%	2.5%	5.0%	7.5%	5.0%	0.0%	0.0%	
				20:00	21:00	50	0.0%	80.0%	0.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	4.0%	0.0%	4.0%	4.0%	4.0%	0.0%	0.0%	
				21:00	22:00	50	0.0%	83.2%	0.0%	0.0%	5.6%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.6%	0.0%	0.0%	
				22:00	23:00	50	0.0%	63.0%	0.0%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	5.3%	5.3%	5.3%	5.3%	10.5%	0.0%	0.0%	
				23:00	0:00	50	0.0%	56.0%	0.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	6.3%	6.3%	6.3%	6.3%	12.5%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L027	Chi Kiang Street (EB)	Lung Tak Street	Yuk Yat Street	0:00	1:00	150	2.6%	26.5%	50.2%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	2.6%	1.7%	0.9%	0.0%	0.0%
				1:00	2:00	100	3.4%	30.5%	61.0%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.6%	28.6%	64.2%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.9%	29.3%	58.5%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.8%	27.8%	63.8%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.1%	22.9%	52.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	2.1%	2.1%	4.2%	4.2%	2.1%	0.0%	0.0%
				6:00	7:00	150	1.9%	18.4%	43.9%	0.0%	6.8%	1.9%	0.0%	0.0%	9.7%	1.9%	2.9%	5.8%	4.9%	1.9%	0.0%	0.0%
				7:00	8:00	300	1.5%	26.4%	31.8%	0.4%	7.7%	1.9%	0.4%	0.4%	13.0%	2.3%	2.3%	5.0%	5.0%	1.9%	0.0%	0.0%
				8:00	9:00	450	2.5%	35.6%	21.6%	0.2%	11.7%	6.7%	1.7%	1.7%	6.7%	1.7%	1.7%	3.5%	3.5%	1.2%	0.0%	0.0%
				9:00	10:00	400	1.9%	28.5%	23.3%	0.3%	11.7%	7.4%	1.9%	1.9%	6.1%	2.9%	2.4%	4.8%	5.0%	1.9%	0.0%	0.0%
				10:00	11:00	400	2.2%	23.6%	22.3%	0.3%	21.2%	10.0%	2.5%	2.5%	5.6%	1.1%	1.4%	3.1%	3.1%	1.1%	0.0%	0.0%
				11:00	12:00	400	1.4%	22.5%	18.9%	0.6%	25.9%	3.9%	1.1%	0.8%	9.7%	3.3%	1.9%	4.2%	4.4%	1.4%	0.0%	0.0%
				12:00	13:00	350	1.8%	31.7%	25.0%	0.3%	18.4%	3.6%	1.2%	0.9%	7.8%	3.9%	0.9%	1.8%	2.1%	0.6%	0.0%	0.0%
				13:00	14:00	400	1.3%	18.4%	14.5%	0.3%	19.7%	11.2%	2.9%	2.9%	6.5%	1.0%	3.6%	7.3%	8.1%	2.3%	0.0%	0.0%
				14:00	15:00	400	1.1%	18.8%	21.4%	0.3%	19.9%	9.1%	2.3%	2.3%	7.7%	1.7%	2.6%	5.4%	5.7%	1.7%	0.0%	0.0%
				15:00	16:00	450	1.5%	21.9%	22.1%	0.2%	19.2%	10.1%	2.7%	2.7%	5.9%	2.7%	2.0%	3.9%	3.9%	1.2%	0.0%	0.0%
				16:00	17:00	400	1.9%	27.0%	19.4%	0.3%	18.8%	4.4%	0.8%	1.1%	7.2%	0.6%	3.0%	6.4%	7.2%	1.9%	0.0%	0.0%
				17:00	18:00	450	2.6%	34.7%	20.9%	0.2%	12.7%	7.3%	1.9%	1.9%	6.4%	1.6%	1.6%	3.5%	3.5%	1.2%	0.0%	0.0%
				18:00	19:00	400	2.8%	37.5%	23.2%	0.3%	10.5%	5.0%	1.4%	1.4%	3.0%	1.4%	2.2%	4.7%	5.2%	1.4%	0.0%	0.0%
				19:00	20:00	300	2.4%	43.6%	30.8%	0.0%	3.8%	1.7%	0.3%	0.3%	9.2%	1.4%	1.0%	2.4%	2.4%	0.7%	0.0%	0.0%
				20:00	21:00	250	2.6%	36.2%	38.2%	0.0%	3.4%	0.9%	0.4%	0.4%	14.9%	0.4%	0.4%	0.9%	0.9%	0.4%	0.0%	0.0%
				21:00	22:00	250	1.9%	32.1%	43.4%	0.0%	4.8%	1.4%	0.5%	0.5%	14.4%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%
				22:00	23:00	200	2.6%	26.2%	46.8%	0.0%	2.6%	0.0%	0.0%	0.0%	16.2%	1.0%	1.0%	1.6%	1.0%	1.0%	0.0%	0.0%
				23:00	0:00	200	3.2%	25.3%	45.4%	0.0%	3.2%	0.0%	0.0%	0.0%	15.8%	1.3%	1.3%	1.9%	1.3%	1.3%	0.0%	0.0%
L028	Chi Kiang Street (EB)	To Kwa Wan Road	Lung Tak Street	0:00	1:00	100	2.2%	23.7%	50.4%	0.0%	4.3%	0.0%	0.0%	0.0%	11.8%	1.1%	1.1%	3.2%	1.1%	1.1%	0.0%	0.0%
				1:00	2:00	50	4.3%	27.7%	61.6%	0.0%	6.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.4%	26.2%	66.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	3.1%	28.1%	59.4%	0.0%	9.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.6%	25.0%	64.3%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.6%	21.1%	52.7%	0.0%	7.9%	0.0%	0.0%	0.0%	0.0%	2.6%	2.6%	5.3%	2.6%	2.6%	0.0%	0.0%
				6:00	7:00	100	2.4%	16.7%	42.7%	0.0%	7.1%	1.2%	0.0%	0.0%	11.9%	2.4%	2.4%	6.0%	4.8%	2.4%	0.0%	0.0%
				7:00	8:00	250	1.4%	23.2%	31.1%	0.5%	8.2%	1.9%	0.5%	0.5%	16.4%	2.4%	2.4%	4.8%	4.8%	1.9%	0.0%	0.0%
				8:00	9:00	350	2.5%	31.6%	21.3%	0.3%	13.1%	6.7%	1.9%	1.9%	8.6%	1.9%	1.9%	3.5%	3.5%	1.3%	0.0%	0.0%
				9:00	10:00	300	2.0%	25.5%	22.7%	0.3%	13.2%	7.5%	2.0%	2.0%	7.8%	3.1%	2.4%	4.7%	5.1%	1.7%	0.0%	0.0%
				10:00	11:00	300	2.1%	21.0%	22.0%	0.3%	23.1%	9.6%	2.7%	2.7%	6.9%	1.4%	1.4%	2.7%	3.1%	1.0%	0.0%	0.0%
				11:00	12:00	300	1.3%	19.5%	18.1%	0.7%	27.9%	3.7%	1.0%	1.0%	11.7%	3.4%	2.0%	4.0%	4.4%	1.3%	0.0%	0.0%
				12:00	13:00	300	1.9%	27.9%	24.5%	0.4%	20.1%	3.3%	1.1%	1.1%	9.7%	4.1%	1.1%	1.9%	2.2%	0.7%	0.0%	0.0%
				13:00	14:00	350	1.3%	16.1%	14.2%	0.3%	21.4%	10.7%	3.2%	3.2%	7.9%	0.9%	3.8%	6.9%	7.6%	2.5%	0.0%	0.0%
				14:00	15:00	300	1.0%	16.2%	21.0%	0.3%	21.9%	9.0%	2.4%	2.4%	9.3%	1.7%	2.4%	5.2%	5.5%	1.7%	0.0%	0.0%
				15:00	16:00	350	1.5%	19.1%	22.0%	0.3%	20.9%	9.7%	3.0%	2.7%	7.3%	2.7%	1.8%	3.9%	3.9%	1.2%	0.0%	0.0%
				16:00	17:00	300	2.0%	23.9%	19.5%	0.3%	20.8%	4.4%	1.0%	1.0%	8.7%	0.7%	3.0%	6.0%	7.0%	1.7%	0.0%	0.0%
				17:00	18:00	350	2.6%	31.1%	21.0%	0.3%	14.1%	7.2%	2.3%	2.0%	7.8%	1.7%	1.7%	3.5%	3.5%	1.2%	0.0%	0.0%
				18:00	19:00	300	2.8%	34.6%	23.9%	0.3%	11.8%	4.8%	1.7%	1.4%	3.8%	1.4%	2.4%	4.5%	5.2%	1.4%	0.0%	0.0%
				19:00	20:00	250	2.6%	39.4%	31.2%	0.0%	4.3%	1.7%	0.4%	0.4%	11.7%	1.3%	1.3%	2.2%	2.6%	0.9%	0.0%	0.0%
				20:00	21:00	200	2.6%	32.3%	38.3%	0.0%	3.7%	0.5%	0.5%	0.5%	18.5%	0.5%	0.5%	1.1%	0.5%	0.5%	0.0%	0.0%
				21:00	22:00	200	1.8%	28.6%	42.7%	0.0%	5.4%	1.8%	0.6%	0.6%	17.9%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	200	2.6%	23.2%	46.0%	0.0%	3.2%	0.0%	0.0%	0.0%	20.0%	1.3%	0.6%	1.9%	0.6%	0.6%	0.0%	0.0%
				23:00	0:00	150	3.1%	22.0%	44.9%	0.0%	3.9%	0.0%	0.0%	0.0%	19.7%	1.6%	0.8%	2.4%	0.8%	0.8%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L029	Lung Tak Street (NB)	Ngan Hon Street	Chi Kiang Street	0:00	1:00	50	0.0%	76.2%	23.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	75.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	72.7%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	75.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	71.4%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	71.4%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	64.3%	28.6%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	75.5%	18.4%	0.0%	4.1%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	0.0%	77.0%	9.0%	0.0%	6.0%	7.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	0.0%	71.7%	11.1%	0.0%	7.4%	8.6%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	0.0%	63.1%	10.8%	0.0%	12.3%	12.3%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	0.0%	67.8%	10.2%	0.0%	16.9%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	0.0%	74.0%	11.6%	0.0%	10.1%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	0.0%	59.3%	8.5%	0.0%	13.6%	16.9%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	0.0%	58.3%	12.7%	0.0%	14.5%	12.7%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	0.0%	62.4%	11.6%	0.0%	11.6%	13.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	0.0%	73.7%	10.5%	0.0%	10.5%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	0.0%	77.2%	8.4%	0.0%	6.0%	7.2%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	0.0%	80.8%	9.6%	0.0%	4.1%	4.1%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	0.0%	86.1%	11.1%	0.0%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	82.0%	16.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	76.8%	18.6%	0.0%	2.3%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	72.8%	24.2%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	0.0%	70.4%	25.9%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
L030	To Kwa Wan Road (SB)	Chi Kiang Street	Ngan Hon Street	0:00	1:00	500	4.0%	24.2%	53.7%	0.0%	2.1%	0.0%	0.0%	0.0%	6.8%	0.4%	0.4%	0.8%	0.4%	0.2%	0.0%	7.0%	
				1:00	2:00	250	4.5%	27.2%	63.4%	0.0%	3.3%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	250	3.5%	25.1%	67.4%	0.0%	2.2%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	200	4.9%	27.6%	63.2%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	150	4.1%	25.3%	67.2%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	200	3.5%	23.4%	62.4%	0.0%	4.7%	0.0%	0.0%	0.6%	0.0%	0.6%	1.2%	1.8%	1.2%	0.6%	0.0%	0.0%	
				6:00	7:00	400	2.8%	18.3%	50.0%	0.3%	4.1%	0.8%	0.0%	0.3%	7.7%	0.8%	1.0%	1.8%	1.8%	0.8%	0.0%	9.5%	
				7:00	8:00	1,100	1.2%	23.3%	35.0%	0.6%	4.3%	1.3%	0.2%	0.2%	10.8%	1.1%	1.0%	2.2%	2.2%	0.8%	0.0%	15.8%	
				8:00	9:00	1,500	2.5%	34.4%	26.5%	0.7%	7.5%	4.7%	0.7%	0.7%	6.1%	0.9%	0.9%	1.8%	1.8%	0.7%	0.0%	10.1%	
				9:00	10:00	1,400	1.9%	28.2%	29.0%	0.7%	7.6%	5.2%	0.7%	0.7%	5.7%	1.5%	1.2%	2.4%	2.6%	0.9%	0.0%	11.7%	
				10:00	11:00	1,200	3.5%	25.8%	28.6%	1.2%	13.8%	5.8%	1.2%	1.2%	4.8%	0.5%	0.6%	1.2%	1.2%	0.4%	0.0%	10.2%	
				11:00	12:00	1,200	2.3%	25.0%	24.7%	1.5%	17.2%	2.3%	0.4%	0.4%	8.8%	1.4%	0.8%	1.6%	1.8%	0.5%	0.0%	11.3%	
				12:00	13:00	1,200	2.9%	32.5%	30.4%	1.1%	11.3%	2.0%	0.4%	0.4%	6.5%	1.5%	0.3%	0.7%	0.8%	0.3%	0.0%	8.9%	
				13:00	14:00	1,200	2.2%	22.4%	20.8%	1.3%	14.3%	7.3%	1.4%	1.5%	6.4%	0.4%	1.6%	3.1%	3.4%	1.0%	0.0%	12.9%	
				14:00	15:00	1,150	1.9%	21.3%	28.7%	1.2%	13.6%	5.6%	1.1%	1.1%	7.0%	0.7%	1.1%	2.1%	2.2%	0.7%	0.0%	11.7%	
				15:00	16:00	1,350	2.7%	24.4%	29.4%	1.1%	12.9%	6.0%	1.2%	1.2%	5.4%	1.1%	0.8%	1.5%	1.6%	0.5%	0.0%	10.2%	
				16:00	17:00	1,150	4.6%	32.0%	25.5%	1.0%	12.4%	2.0%	0.5%	0.4%	6.0%	0.2%	0.8%	1.5%	1.7%	0.4%	0.0%	11.0%	
				17:00	18:00	1,450	5.6%	38.5%	25.8%	0.7%	7.8%	3.2%	0.9%	0.9%	5.0%	0.4%	0.4%	0.8%	0.8%	0.2%	0.0%	9.0%	
				18:00	19:00	1,200	5.9%	41.8%	28.7%	0.5%	6.4%	2.2%	0.6%	0.6%	2.3%	0.3%	0.6%	1.0%	1.2%	0.3%	0.0%	7.6%	
				19:00	20:00	1,150	3.3%	40.2%	33.9%	0.1%	2.2%	0.9%	0.1%	0.2%	7.0%	0.4%	0.3%	0.8%	0.8%	0.3%	0.0%	9.5%	
				20:00	21:00	1,000	3.3%	31.8%	39.9%	0.1%	1.8%	0.3%	0.1%	0.2%	10.8%	0.2%	0.2%	0.3%	0.2%	0.1%	0.0%	10.7%	
				21:00	22:00	900	2.4%	27.4%	43.9%	0.1%	2.4%	0.8%	0.1%	0.2%	10.0%	0.0%	0.1%	0.1%	0.0%	0.1%	0.0%	12.4%	
				22:00	23:00	900	3.1%	21.3%	44.8%	0.1%	1.3%	0.0%	0.0%	0.0%	10.7%	0.3%	0.2%	0.5%	0.2%	0.2%	0.0%	17.3%	
23:00	0:00	700	3.3%	20.5%	44.6%	0.1%	1.7%	0.0%	0.0%	0.0%	10.7%	0.4%	0.3%	0.6%	0.3%	0.3%	0.0%	17.2%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L031	To Kwa Wan Road (SB)	Ngan Hon Street	Wing Kwong Street	0:00	1:00	400	3.0%	19.7%	56.4%	0.0%	1.8%	0.0%	0.0%	0.0%	8.1%	0.5%	0.3%	1.0%	0.5%	0.3%	0.0%	8.4%
				1:00	2:00	200	3.5%	23.0%	69.0%	0.0%	2.5%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	2.7%	20.9%	72.2%	0.0%	2.1%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	3.8%	23.5%	68.9%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	3.4%	21.0%	72.3%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.9%	19.4%	68.4%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.7%	0.7%	2.2%	1.4%	0.7%	0.0%	0.0%
				6:00	7:00	350	2.1%	14.8%	52.2%	0.0%	3.3%	0.9%	0.0%	0.0%	9.0%	0.9%	0.9%	2.1%	2.1%	0.6%	0.0%	11.1%
				7:00	8:00	950	0.8%	19.5%	35.2%	0.2%	3.5%	1.3%	0.2%	0.2%	12.6%	1.2%	1.1%	2.4%	2.4%	0.8%	0.0%	18.6%
				8:00	9:00	1,200	1.8%	30.5%	28.1%	0.3%	6.3%	4.9%	0.8%	0.7%	7.5%	1.0%	1.0%	2.0%	2.0%	0.7%	0.0%	12.4%
				9:00	10:00	1,150	1.3%	24.5%	30.0%	0.3%	6.3%	5.5%	0.8%	0.7%	6.9%	1.7%	1.3%	2.7%	2.8%	0.9%	0.0%	14.3%
				10:00	11:00	1,000	2.8%	22.1%	32.0%	0.4%	11.5%	5.9%	1.2%	1.1%	6.0%	0.5%	0.6%	1.4%	1.4%	0.4%	0.0%	12.7%
				11:00	12:00	1,000	1.9%	21.2%	27.0%	0.5%	14.3%	2.3%	0.5%	0.4%	10.9%	1.6%	0.9%	1.9%	2.1%	0.5%	0.0%	14.0%
				12:00	13:00	950	2.3%	27.9%	33.8%	0.4%	9.4%	2.0%	0.5%	0.4%	8.1%	1.8%	0.4%	0.8%	1.0%	0.2%	0.0%	11.0%
				13:00	14:00	950	1.7%	18.9%	22.5%	0.4%	11.7%	7.2%	1.5%	1.5%	7.8%	0.5%	1.9%	3.6%	3.9%	1.1%	0.0%	15.8%
				14:00	15:00	950	1.5%	17.8%	30.9%	0.4%	11.0%	5.6%	1.2%	1.1%	8.4%	0.9%	1.2%	2.5%	2.6%	0.7%	0.0%	14.2%
				15:00	16:00	1,100	2.1%	20.6%	32.1%	0.4%	10.6%	6.1%	1.3%	1.2%	6.6%	1.3%	0.9%	1.8%	1.9%	0.6%	0.0%	12.5%
				16:00	17:00	900	4.0%	26.5%	29.3%	0.3%	10.1%	1.9%	0.6%	0.4%	7.5%	0.2%	1.0%	1.9%	2.1%	0.4%	0.0%	13.8%
				17:00	18:00	1,150	4.9%	32.3%	30.0%	0.3%	6.5%	3.1%	1.0%	0.9%	6.3%	0.5%	0.5%	1.0%	1.0%	0.3%	0.0%	11.4%
				18:00	19:00	950	5.1%	35.8%	33.5%	0.2%	5.3%	2.0%	0.6%	0.6%	3.0%	0.4%	0.7%	1.3%	1.5%	0.3%	0.0%	9.7%
				19:00	20:00	950	2.7%	34.1%	36.9%	0.0%	1.8%	0.9%	0.1%	0.2%	8.6%	0.5%	0.4%	0.9%	1.0%	0.2%	0.0%	11.7%
				20:00	21:00	850	2.5%	26.3%	42.4%	0.0%	1.4%	0.4%	0.1%	0.1%	13.0%	0.2%	0.1%	0.4%	0.2%	0.1%	0.0%	12.8%
				21:00	22:00	750	1.9%	22.4%	46.1%	0.0%	1.9%	0.7%	0.1%	0.1%	11.9%	0.0%	0.0%	0.1%	0.0%	0.1%	0.0%	14.7%
				22:00	23:00	750	2.4%	16.9%	45.6%	0.0%	1.1%	0.0%	0.0%	0.0%	12.3%	0.4%	0.3%	0.5%	0.3%	0.3%	0.0%	19.9%
				23:00	0:00	650	2.5%	16.3%	45.6%	0.0%	1.3%	0.0%	0.0%	0.0%	12.4%	0.5%	0.3%	0.7%	0.3%	0.3%	0.0%	19.8%
L032	To Kwa Wan Road (NB)	Shek Tong Street	Chi Kiang Street	0:00	1:00	150	4.8%	20.4%	34.7%	0.0%	2.0%	0.0%	0.0%	0.0%	17.0%	0.7%	0.7%	2.0%	1.4%	0.7%	0.0%	15.6%
				1:00	2:00	100	7.1%	32.1%	57.2%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	6.0%	30.0%	62.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.9%	31.6%	55.2%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	6.1%	30.3%	60.6%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	4.5%	25.0%	50.1%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	2.3%	2.3%	4.5%	4.5%	2.3%	0.0%	0.0%
				6:00	7:00	150	3.0%	14.4%	30.4%	0.0%	3.0%	0.8%	0.0%	0.0%	17.4%	1.5%	1.5%	3.8%	3.8%	1.5%	0.0%	18.9%
				7:00	8:00	400	1.3%	11.2%	18.7%	0.3%	2.4%	1.1%	0.3%	0.3%	17.4%	1.9%	1.9%	4.0%	4.0%	1.6%	0.0%	33.6%
				8:00	9:00	450	3.2%	19.7%	16.7%	0.2%	4.8%	4.3%	1.1%	1.1%	11.5%	1.8%	1.8%	3.6%	3.6%	1.4%	0.0%	25.2%
				9:00	10:00	450	2.3%	15.1%	17.1%	0.2%	4.6%	4.6%	1.1%	1.1%	10.0%	3.0%	2.3%	4.6%	4.8%	1.8%	0.0%	27.4%
				10:00	11:00	450	3.7%	20.5%	17.3%	0.5%	10.9%	5.2%	1.2%	1.2%	11.1%	1.0%	1.2%	2.2%	2.2%	0.7%	0.0%	21.1%
				11:00	12:00	450	2.3%	18.1%	13.5%	0.7%	12.6%	1.8%	0.5%	0.5%	18.3%	2.5%	1.4%	2.7%	3.2%	0.9%	0.0%	21.0%
				12:00	13:00	400	3.0%	26.0%	18.5%	0.5%	9.1%	1.8%	0.5%	0.5%	15.0%	3.0%	0.8%	1.3%	1.5%	0.5%	0.0%	18.0%
				13:00	14:00	450	2.0%	15.8%	11.0%	0.5%	9.9%	5.6%	1.4%	1.4%	12.8%	0.7%	2.9%	5.2%	5.9%	2.0%	0.0%	22.9%
				14:00	15:00	450	1.9%	15.7%	16.1%	0.5%	9.9%	4.6%	1.2%	1.0%	14.7%	1.2%	1.9%	3.9%	4.1%	1.4%	0.0%	21.9%
				15:00	16:00	500	2.8%	18.8%	17.2%	0.4%	10.0%	5.2%	1.3%	1.3%	12.0%	2.2%	1.5%	2.8%	3.1%	1.1%	0.0%	20.3%
				16:00	17:00	500	4.0%	28.7%	13.8%	0.4%	10.5%	1.8%	0.4%	0.2%	14.9%	0.2%	1.8%	2.6%	3.1%	0.9%	0.0%	16.7%
				17:00	18:00	550	5.1%	35.8%	14.7%	0.4%	7.0%	2.8%	0.7%	0.6%	13.2%	0.9%	0.9%	1.5%	1.5%	0.6%	0.0%	14.3%
				18:00	19:00	450	5.7%	41.2%	17.2%	0.2%	6.1%	2.0%	0.5%	0.5%	6.6%	0.9%	1.4%	2.0%	2.3%	0.7%	0.0%	12.7%
				19:00	20:00	400	3.6%	32.0%	20.5%	0.0%	1.8%	0.8%	0.3%	0.3%	15.8%	1.0%	0.8%	1.6%	1.6%	0.5%	0.0%	19.4%
				20:00	21:00	350	3.2%	24.4%	23.2%	0.0%	1.5%	0.3%	0.3%	0.3%	23.8%	0.3%	0.3%	0.6%	0.6%	0.3%	0.0%	20.9%
				21:00	22:00	350	2.6%	21.2%	25.7%	0.0%	2.0%	0.7%	0.3%	0.3%	22.1%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	24.8%
				22:00	23:00	350	3.1%	15.2%	24.2%	0.0%	0.9%	0.0%	0.0%	0.0%	22.0%	0.6%	0.3%	0.9%	0.6%	0.3%	0.0%	31.9%
				23:00	0:00	300	3.1%	14.9%	24.1%	0.0%	1.1%	0.0%	0.0%	0.0%	21.8%	0.8%	0.4%	1.1%	0.8%	0.4%	0.0%	31.5%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L033	Ngan Hon Street (EB)	To Kwa Wan Road	Lung Tak Street	0:00	1:00	100	14.3%	54.3%	27.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	14.3%	52.3%	28.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	10.8%	51.4%	32.4%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	13.8%	51.7%	27.6%	0.0%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	13.0%	52.3%	30.4%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	11.5%	50.0%	30.8%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	11.3%	45.3%	28.3%	1.9%	9.4%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%
				7:00	8:00	150	8.9%	55.5%	19.9%	2.7%	8.2%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	0.0%	0.0%
				8:00	9:00	300	12.4%	57.3%	10.7%	2.4%	10.3%	4.5%	0.7%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				9:00	10:00	250	10.5%	53.0%	13.0%	2.5%	11.8%	5.9%	0.8%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%
				10:00	11:00	250	9.5%	45.0%	11.3%	3.9%	21.6%	6.5%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%
				11:00	12:00	250	6.4%	45.2%	10.0%	5.5%	28.3%	2.7%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%
				12:00	13:00	250	7.9%	55.9%	11.8%	3.5%	17.5%	2.2%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%
				13:00	14:00	250	6.3%	42.3%	8.8%	4.9%	24.4%	8.8%	1.5%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%
				14:00	15:00	200	5.7%	41.9%	13.0%	4.7%	24.4%	6.7%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%
				15:00	16:00	250	7.8%	44.6%	12.3%	4.1%	21.4%	7.0%	1.2%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%
				16:00	17:00	250	7.2%	55.2%	9.4%	3.6%	21.1%	2.7%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	300	8.4%	62.5%	9.1%	2.4%	12.5%	3.7%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	300	8.6%	66.9%	9.7%	1.6%	10.1%	2.3%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	9.3%	71.7%	13.9%	0.5%	3.2%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				20:00	21:00	200	10.8%	65.7%	19.1%	0.6%	3.2%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	8.4%	62.5%	22.9%	0.8%	4.6%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	12.8%	56.0%	26.6%	0.9%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%
23:00	0:00	100	13.3%	53.4%	26.7%	1.1%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%			
L034	Ngan Hon Street (EB)	Lung Tak Street	Wing Kwong Street	0:00	1:00	100	8.8%	55.9%	32.4%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	7.3%	53.7%	34.1%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	8.1%	51.4%	37.8%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	10.3%	51.8%	31.0%	0.0%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	8.3%	50.0%	37.5%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	7.4%	48.2%	37.0%	0.0%	7.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	5.9%	47.0%	35.3%	2.0%	7.8%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	150	4.8%	56.8%	24.0%	4.1%	6.8%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				8:00	9:00	300	7.1%	60.4%	13.1%	3.9%	8.8%	4.9%	0.7%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				9:00	10:00	250	6.0%	54.8%	15.7%	4.3%	10.2%	6.4%	0.9%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				10:00	11:00	250	6.3%	46.6%	14.0%	5.9%	18.1%	6.8%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				11:00	12:00	250	4.3%	46.9%	12.4%	8.1%	23.9%	2.9%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				12:00	13:00	250	5.0%	57.0%	14.4%	5.4%	14.9%	2.3%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	200	4.1%	43.7%	11.2%	7.6%	20.8%	9.1%	1.5%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%
				14:00	15:00	200	3.8%	43.0%	15.6%	7.0%	20.4%	7.5%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				15:00	16:00	250	5.1%	45.4%	14.9%	6.4%	17.9%	7.7%	1.3%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%
				16:00	17:00	250	5.7%	56.4%	11.5%	4.8%	17.7%	2.9%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	300	6.4%	63.8%	11.0%	3.2%	10.3%	3.9%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	250	6.6%	68.0%	11.9%	2.0%	8.2%	2.5%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	5.7%	73.2%	16.7%	0.5%	2.9%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	200	6.5%	66.6%	22.9%	0.7%	2.6%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	5.4%	62.3%	26.9%	0.8%	3.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	8.3%	55.9%	32.1%	0.9%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	100	9.2%	54.1%	32.2%	1.1%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L035	Ngan Hon Street (EB)	Wing Kwong Street	Sung On Street	0:00	1:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	94.1%	0.0%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	80.8%	0.0%	0.0%	4.8%	4.8%	4.8%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	76.4%	0.0%	0.0%	5.9%	5.9%	5.9%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	69.0%	0.0%	0.0%	10.3%	6.9%	6.9%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	73.2%	0.0%	0.0%	15.4%	3.8%	3.8%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	83.4%	0.0%	0.0%	6.7%	3.3%	3.3%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	58.8%	0.0%	0.0%	10.3%	10.3%	10.3%	10.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	64.0%	0.0%	0.0%	12.0%	8.0%	8.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	63.6%	0.0%	0.0%	9.1%	9.1%	9.1%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	79.5%	0.0%	0.0%	10.3%	3.4%	3.4%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	81.2%	0.0%	0.0%	4.7%	4.7%	4.7%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	88.8%	0.0%	0.0%	2.8%	2.8%	2.8%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				L036	Ngan Hon Street (WB)	Sung On Street	Wing Kwong Street	23:00	0:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
7:00	8:00	50	0.0%					95.0%	0.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
8:00	9:00	50	0.0%					83.2%	0.0%	0.0%	4.2%	4.2%	4.2%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
9:00	10:00	50	0.0%					78.8%	0.0%	0.0%	5.3%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
10:00	11:00	50	0.0%					74.8%	0.0%	0.0%	6.3%	6.3%	6.3%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
11:00	12:00	50	0.0%					79.5%	0.0%	0.0%	10.3%	3.4%	3.4%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
12:00	13:00	50	0.0%					85.4%	0.0%	0.0%	5.9%	2.9%	2.9%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
13:00	14:00	50	0.0%					71.6%	0.0%	0.0%	7.1%	7.1%	7.1%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
14:00	15:00	50	0.0%					69.2%	0.0%	0.0%	7.7%	7.7%	7.7%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
15:00	16:00	50	0.0%					75.6%	0.0%	0.0%	6.1%	6.1%	6.1%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
16:00	17:00	50	0.0%					93.2%	0.0%	0.0%	3.4%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
17:00	18:00	50	0.0%					91.2%	0.0%	0.0%	2.2%	2.2%	2.2%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
18:00	19:00	50	0.0%					90.4%	0.0%	0.0%	2.4%	2.4%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
19:00	20:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
20:00	21:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	50	0.0%					100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L037	Wing Kwong Street (SB)	Ngan Hon Street	Hung Fook Street	0:00	1:00	100	10.0%	55.7%	31.4%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	9.3%	53.4%	32.6%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	8.1%	51.4%	37.8%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	10.3%	51.8%	31.0%	0.0%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	8.0%	52.0%	36.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	7.4%	48.2%	37.0%	0.0%	7.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	7.8%	47.0%	35.3%	2.0%	5.9%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	150	5.4%	57.1%	23.5%	4.0%	6.7%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				8:00	9:00	300	7.7%	61.0%	12.9%	3.8%	8.0%	4.9%	0.7%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				9:00	10:00	250	6.7%	55.6%	15.5%	4.2%	9.2%	6.3%	0.8%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				10:00	11:00	250	6.8%	47.7%	14.1%	5.9%	16.8%	6.8%	0.9%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				11:00	12:00	250	4.9%	49.0%	12.6%	7.8%	21.8%	2.9%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				12:00	13:00	250	5.4%	58.7%	14.5%	5.0%	13.6%	2.3%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	200	4.6%	45.5%	11.3%	7.2%	19.1%	9.3%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%
				14:00	15:00	200	3.8%	44.9%	15.8%	7.1%	18.6%	7.7%	1.1%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%
				15:00	16:00	250	5.6%	47.4%	15.1%	6.0%	16.4%	7.8%	0.9%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%
				16:00	17:00	250	6.3%	59.0%	11.6%	4.3%	15.9%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	300	6.8%	65.4%	11.0%	2.8%	9.3%	3.9%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	250	6.9%	68.9%	11.7%	2.0%	7.3%	2.4%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	6.1%	73.8%	16.4%	0.5%	2.3%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	200	7.0%	67.0%	22.3%	0.6%	2.5%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	5.3%	62.8%	26.5%	0.8%	3.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	9.0%	55.9%	31.5%	0.9%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L038	Wing Kwong Street (SB)	Hung Fook Street	Kai Ming Street	23:00	0:00	100	9.0%	55.0%	31.5%	1.1%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				0:00	1:00	100	7.4%	55.5%	34.6%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	6.3%	54.1%	35.4%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	7.0%	51.2%	39.5%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	8.8%	52.9%	32.4%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	6.9%	51.8%	37.9%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	6.3%	49.9%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	100	5.1%	47.4%	35.6%	3.4%	6.8%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	200	4.0%	58.1%	25.3%	4.0%	6.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
				8:00	9:00	350	5.7%	62.3%	13.7%	3.9%	7.8%	5.1%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.3%	0.0%	0.0%	
				9:00	10:00	300	4.7%	57.6%	16.7%	4.0%	8.7%	6.5%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%	
				10:00	11:00	300	5.2%	48.6%	14.7%	6.0%	16.3%	7.6%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	250	3.8%	49.3%	13.4%	8.4%	21.4%	2.9%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	300	4.3%	58.8%	15.3%	5.5%	12.9%	2.4%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	250	3.6%	45.8%	11.7%	7.7%	18.5%	9.9%	1.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%	
				14:00	15:00	250	3.3%	44.9%	16.5%	7.1%	17.9%	8.0%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%	
				15:00	16:00	300	4.1%	47.7%	15.8%	6.4%	15.8%	7.9%	1.1%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.0%	
				16:00	17:00	250	5.2%	58.8%	12.0%	4.7%	15.9%	3.0%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	350	5.7%	65.5%	11.4%	3.2%	9.2%	4.1%	0.6%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	300	5.8%	69.2%	12.3%	2.2%	7.2%	2.5%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	250	4.9%	73.7%	17.0%	0.8%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	200	5.5%	67.4%	23.2%	1.1%	2.2%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	200	4.5%	62.4%	27.3%	1.3%	3.2%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	150	7.0%	56.3%	32.8%	1.6%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					
23:00	0:00	150	6.9%	55.4%	32.7%	2.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L039	Hung Fook Street (EB)	Access Road	Wing Kwong Street	0:00	1:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	60.0%	28.0%	4.0%	4.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	2.1%	65.9%	14.9%	4.3%	6.4%	6.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	2.6%	59.0%	17.9%	5.1%	7.7%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	3.3%	50.0%	16.7%	6.7%	13.3%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	51.9%	17.2%	10.3%	17.2%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	3.1%	59.3%	18.8%	6.3%	9.4%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	48.2%	14.8%	7.4%	14.8%	14.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	48.0%	20.0%	8.0%	12.0%	12.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	3.0%	48.5%	18.2%	6.1%	12.1%	12.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	4.2%	58.2%	16.7%	4.2%	12.5%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	3.1%	65.6%	15.6%	3.1%	6.3%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	3.6%	67.7%	17.9%	3.6%	3.6%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	3.2%	74.2%	19.4%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	4.5%	68.2%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	58.3%	41.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L040	Hung Fook Street (EB)	Yuk Shing Street	Access Road	0:00	1:00	50	0.0%	63.6%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	57.1%	28.6%	0.0%	3.6%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	7.1%	0.0%	0.0%
				8:00	9:00	100	0.0%	65.5%	15.4%	0.0%	3.8%	7.7%	1.9%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%
				9:00	10:00	50	0.0%	57.8%	17.8%	0.0%	4.4%	8.9%	2.2%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	6.7%	0.0%	0.0%
				10:00	11:00	50	0.0%	51.4%	16.2%	0.0%	13.5%	10.8%	2.7%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	0.0%	0.0%
				11:00	12:00	50	0.0%	58.1%	16.1%	0.0%	19.4%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	0.0%	0.0%
				12:00	13:00	50	0.0%	66.7%	19.4%	0.0%	11.1%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	48.5%	12.1%	0.0%	15.2%	12.1%	3.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.1%	0.0%	0.0%
				14:00	15:00	50	0.0%	46.9%	18.8%	0.0%	15.6%	9.4%	3.1%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%
				15:00	16:00	50	0.0%	51.2%	17.9%	0.0%	12.8%	10.3%	2.6%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	0.0%
				16:00	17:00	50	0.0%	63.6%	15.2%	0.0%	18.2%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	69.6%	13.0%	0.0%	10.9%	4.3%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	73.3%	14.6%	0.0%	7.3%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	76.4%	18.4%	0.0%	2.6%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	70.4%	25.9%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	65.3%	30.4%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	61.1%	38.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L041	Kai Ming Street (WB)	Wing Kwong Street	Access Road	0:00	1:00	50	5.0%	55.0%	35.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	9.1%	54.5%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	37.5%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	2.5%	60.0%	25.0%	5.0%	5.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	3.9%	63.6%	14.3%	3.9%	7.8%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	3.1%	57.8%	17.2%	4.7%	9.4%	7.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	3.3%	50.0%	15.0%	5.0%	16.7%	8.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	1.8%	50.9%	14.0%	7.0%	22.8%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	3.3%	60.6%	14.8%	4.9%	13.1%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	1.9%	46.2%	11.1%	7.4%	20.4%	11.1%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	2.0%	45.1%	17.6%	5.9%	19.6%	7.8%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	3.1%	47.7%	15.4%	6.2%	16.9%	9.2%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	3.4%	60.5%	12.1%	3.4%	17.2%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	3.8%	67.1%	11.4%	2.5%	10.1%	3.8%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	4.3%	70.0%	11.4%	1.4%	8.6%	2.9%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	3.3%	75.0%	16.7%	0.0%	3.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	4.7%	69.7%	23.3%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	2.8%	66.6%	27.8%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	3.3%	60.1%	33.3%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	4.2%	58.3%	33.3%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L042	Kai Ming Street (WB)	Access Road	Yuk Shing Street	0:00	1:00	50	0.0%	59.3%	37.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	52.9%	41.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	54.5%	36.4%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	41.7%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	50.0%	40.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	
				7:00	8:00	100	0.0%	57.9%	26.3%	0.0%	3.5%	3.5%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	7.0%	0.0%	
				8:00	9:00	150	0.0%	64.2%	15.1%	0.0%	4.7%	7.5%	2.8%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	
				9:00	10:00	100	0.0%	56.6%	17.8%	0.0%	5.6%	8.9%	3.3%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	5.6%	0.0%	
				10:00	11:00	100	0.0%	51.1%	17.9%	0.0%	17.9%	8.3%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	
				11:00	12:00	100	0.0%	53.2%	15.6%	0.0%	23.4%	3.9%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	
				12:00	13:00	100	0.0%	63.5%	17.6%	0.0%	14.1%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	
				13:00	14:00	100	0.0%	45.5%	12.7%	0.0%	19.0%	11.4%	3.8%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	5.1%	0.0%	
				14:00	15:00	100	0.0%	45.3%	18.7%	0.0%	18.7%	9.3%	2.7%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	4.0%	0.0%	
				15:00	16:00	100	0.0%	50.0%	17.8%	0.0%	16.7%	8.9%	3.3%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	0.0%	
				16:00	17:00	100	0.0%	61.0%	14.9%	0.0%	21.8%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	150	0.0%	69.0%	13.8%	0.0%	12.9%	3.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	150	0.0%	72.5%	14.7%	0.0%	9.8%	2.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	0.0%	76.4%	18.8%	0.0%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	
				20:00	21:00	100	0.0%	70.5%	26.2%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	100	0.0%	64.1%	30.2%	0.0%	3.8%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	0.0%	59.0%	36.4%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	
				23:00	0:00	50	0.0%	55.5%	38.9%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L043	Sung On Street (NB)	Bailey Street	Chi Kiang Street	0:00	1:00	100	4.0%	24.0%	57.4%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	1.3%	1.3%	5.3%	2.7%	1.3%	0.0%	0.0%
				1:00	2:00	50	4.9%	26.8%	65.9%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.7%	24.3%	70.3%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	3.7%	25.9%	66.7%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.8%	23.1%	65.5%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.9%	17.6%	53.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	2.9%	2.9%	8.8%	5.9%	2.9%	0.0%	0.0%
				6:00	7:00	100	2.8%	15.5%	46.5%	0.0%	2.8%	1.4%	0.0%	0.0%	0.0%	4.2%	4.2%	9.9%	8.5%	4.2%	0.0%	0.0%
				7:00	8:00	200	1.2%	17.5%	39.3%	0.6%	4.8%	1.2%	0.0%	0.0%	0.0%	4.8%	4.8%	11.4%	10.2%	4.2%	0.0%	0.0%
				8:00	9:00	250	2.1%	25.6%	29.2%	0.4%	8.1%	4.7%	0.9%	0.9%	0.0%	3.8%	3.8%	9.0%	8.1%	3.4%	0.0%	0.0%
				9:00	10:00	250	1.7%	18.9%	28.7%	0.4%	7.6%	5.0%	0.8%	0.8%	0.0%	5.9%	4.6%	10.9%	10.5%	4.2%	0.0%	0.0%
				10:00	11:00	250	3.5%	24.3%	29.1%	0.5%	12.9%	8.4%	1.0%	0.5%	0.0%	2.5%	3.0%	6.4%	5.9%	2.0%	0.0%	0.0%
				11:00	12:00	250	1.9%	22.8%	24.4%	0.5%	15.5%	3.4%	0.5%	0.0%	0.0%	6.8%	3.9%	8.7%	8.7%	2.9%	0.0%	0.0%
				12:00	13:00	200	3.1%	31.1%	31.6%	0.5%	10.9%	3.1%	0.5%	0.0%	0.0%	7.8%	1.6%	4.1%	4.1%	1.6%	0.0%	0.0%
				13:00	14:00	250	1.7%	17.4%	17.3%	0.4%	11.0%	8.9%	0.8%	0.8%	0.0%	1.7%	6.8%	14.3%	14.3%	4.6%	0.0%	0.0%
				14:00	15:00	250	1.4%	18.0%	26.6%	0.5%	11.4%	7.6%	0.9%	0.5%	0.0%	3.3%	4.7%	10.9%	10.4%	3.8%	0.0%	0.0%
				15:00	16:00	250	2.5%	21.3%	28.2%	0.4%	11.3%	8.4%	0.8%	0.4%	0.0%	5.0%	3.8%	7.9%	7.5%	2.5%	0.0%	0.0%
				16:00	17:00	250	4.1%	32.0%	22.4%	0.0%	9.1%	4.1%	0.0%	0.0%	0.0%	0.9%	5.0%	9.6%	10.5%	2.3%	0.0%	0.0%
				17:00	18:00	300	5.4%	40.5%	23.8%	0.0%	6.1%	6.5%	0.4%	0.0%	0.0%	2.7%	2.7%	5.4%	5.0%	1.5%	0.0%	0.0%
				18:00	19:00	250	5.4%	40.7%	24.5%	0.0%	4.6%	4.1%	0.4%	0.0%	0.0%	2.1%	3.3%	6.6%	6.6%	1.7%	0.0%	0.0%
				19:00	20:00	200	3.4%	41.1%	37.6%	0.0%	2.2%	1.1%	0.0%	0.0%	0.0%	2.2%	1.7%	4.5%	4.5%	1.7%	0.0%	0.0%
				20:00	21:00	150	3.7%	36.6%	50.2%	0.0%	2.2%	0.7%	0.0%	0.0%	0.0%	1.5%	0.7%	2.2%	1.5%	0.7%	0.0%	0.0%
				21:00	22:00	150	2.6%	33.0%	58.3%	0.0%	2.6%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%
				22:00	23:00	150	3.5%	25.7%	58.3%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	3.5%	1.8%	1.8%	0.0%	0.0%
L044	Sung On Street (SB)	Chi Kiang Street	Bailey Street	23:00	0:00	100	4.3%	23.4%	57.5%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	2.1%	2.1%	4.3%	2.1%	2.1%	0.0%	0.0%
				0:00	1:00	150	3.4%	33.9%	45.1%	0.0%	2.5%	0.0%	0.0%	0.0%	10.2%	0.8%	0.8%	1.7%	0.8%	0.8%	0.0%	0.0%
				1:00	2:00	100	3.2%	38.7%	53.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.6%	35.7%	57.1%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.8%	38.1%	52.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.8%	36.1%	58.3%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.1%	29.8%	49.0%	0.0%	6.4%	0.0%	0.0%	0.0%	0.0%	2.1%	2.1%	4.3%	2.1%	2.1%	0.0%	0.0%
				6:00	7:00	100	2.0%	25.5%	42.0%	0.0%	5.1%	1.0%	0.0%	0.0%	11.2%	2.0%	2.0%	4.1%	4.1%	1.0%	0.0%	0.0%
				7:00	8:00	300	1.5%	30.0%	31.7%	0.4%	4.9%	1.5%	0.4%	0.4%	12.7%	2.6%	2.6%	4.9%	4.9%	1.5%	0.0%	0.0%
				8:00	9:00	450	2.5%	41.4%	22.1%	0.2%	7.7%	5.0%	1.2%	1.2%	6.7%	2.0%	2.0%	3.5%	3.5%	1.0%	0.0%	0.0%
				9:00	10:00	400	1.9%	33.7%	24.3%	0.3%	7.8%	5.7%	1.3%	1.3%	6.2%	3.5%	2.7%	4.9%	5.1%	1.3%	0.0%	0.0%
				10:00	11:00	350	3.0%	33.7%	22.1%	0.3%	16.9%	5.7%	1.8%	1.8%	6.6%	1.2%	1.2%	2.4%	2.4%	0.9%	0.0%	0.0%
				11:00	12:00	350	1.8%	31.2%	18.4%	0.6%	20.5%	2.1%	0.9%	0.6%	11.3%	3.0%	1.8%	3.3%	3.6%	0.9%	0.0%	0.0%
				12:00	13:00	350	2.4%	41.5%	22.8%	0.3%	13.7%	1.8%	0.9%	0.6%	8.5%	3.3%	0.9%	1.5%	1.5%	0.3%	0.0%	0.0%
				13:00	14:00	350	1.8%	27.6%	15.1%	0.3%	16.6%	6.8%	2.4%	2.4%	8.0%	0.9%	3.6%	5.9%	6.5%	2.1%	0.0%	0.0%
				14:00	15:00	350	1.6%	27.3%	22.0%	0.3%	16.6%	5.4%	1.9%	1.6%	9.2%	1.6%	2.2%	4.5%	4.5%	1.3%	0.0%	0.0%
				15:00	16:00	400	2.2%	31.1%	22.2%	0.3%	15.7%	5.9%	2.2%	1.9%	7.0%	2.4%	1.6%	3.2%	3.2%	1.1%	0.0%	0.0%
				16:00	17:00	350	3.3%	41.1%	16.6%	0.3%	16.3%	1.8%	0.9%	0.6%	8.9%	0.3%	1.8%	3.3%	3.6%	1.2%	0.0%	0.0%
				17:00	18:00	450	4.0%	49.9%	16.8%	0.2%	10.4%	2.6%	1.4%	1.2%	7.6%	0.9%	0.9%	1.7%	1.7%	0.7%	0.0%	0.0%
				18:00	19:00	400	4.1%	53.9%	18.5%	0.3%	8.6%	1.7%	0.8%	0.8%	3.6%	0.8%	1.4%	2.2%	2.5%	0.8%	0.0%	0.0%
				19:00	20:00	350	2.9%	52.3%	26.0%	0.0%	2.5%	1.0%	0.3%	0.3%	9.2%	1.0%	1.0%	1.6%	1.6%	0.3%	0.0%	0.0%
				20:00	21:00	300	2.8%	43.4%	32.4%	0.0%	2.4%	0.4%	0.4%	0.4%	15.4%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%
				21:00	22:00	250	2.3%	39.6%	37.4%	0.0%	3.2%	0.9%	0.5%	0.5%	15.1%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	200	3.0%	33.0%	41.2%	0.0%	2.0%	0.0%	0.0%	0.0%	17.3%	1.0%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%
				23:00	0:00	200	3.7%	31.5%	40.8%	0.0%	2.5%	0.0%	0.0%	0.0%	17.3%	1.2%	0.6%	1.2%	0.6%	0.6%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L045	Wan On Street (SB)	Wing Kwong Street	Bailey Street	0:00	1:00	50	4.1%	53.1%	38.8%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%
				1:00	2:00	50	3.4%	51.8%	41.4%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	3.8%	50.1%	42.3%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.0%	50.0%	40.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	6.3%	49.9%	43.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	5.0%	45.0%	40.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	0.0%
				6:00	7:00	50	2.9%	47.1%	41.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	0.0%	0.0%
				7:00	8:00	150	1.8%	52.1%	31.9%	1.8%	3.5%	1.8%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%
				8:00	9:00	250	3.0%	60.1%	18.9%	2.0%	5.0%	4.5%	2.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%
				9:00	10:00	200	2.4%	53.3%	22.4%	2.4%	5.3%	5.3%	2.4%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	4.7%	0.0%	0.0%
				10:00	11:00	150	3.5%	49.4%	18.3%	3.5%	13.4%	7.0%	2.1%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				11:00	12:00	150	2.3%	51.5%	16.9%	4.6%	17.7%	3.1%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	0.0%
				12:00	13:00	150	2.8%	61.3%	18.3%	2.8%	10.6%	2.1%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				13:00	14:00	150	2.3%	45.7%	14.0%	3.9%	14.7%	9.3%	3.1%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	4.7%	0.0%	0.0%
				14:00	15:00	150	2.5%	45.0%	19.7%	4.1%	13.9%	7.4%	2.5%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	0.0%	0.0%
				15:00	16:00	200	2.6%	48.4%	19.2%	3.3%	12.6%	7.3%	2.6%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%
				16:00	17:00	150	4.0%	63.8%	11.3%	2.4%	15.3%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	200	4.7%	69.1%	10.5%	1.7%	8.7%	4.1%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	200	4.6%	72.3%	11.2%	1.3%	6.6%	2.6%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	150	3.5%	72.9%	20.1%	0.0%	2.1%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				20:00	21:00	150	3.8%	66.0%	27.4%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%
				21:00	22:00	100	3.3%	61.2%	32.2%	0.0%	2.2%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	4.0%	56.1%	37.3%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
L046	Wan On Street (NB)	Bailey Street	Wing Kwong Street	23:00	0:00	100	5.0%	53.3%	38.3%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.7%	0.0%	0.0%
				0:00	1:00	100	0.0%	56.6%	41.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	54.6%	42.4%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	51.8%	44.8%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	54.6%	40.9%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	45.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	52.3%	42.9%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	46.4%	41.5%	2.4%	4.9%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	0.0%
				7:00	8:00	150	0.0%	58.9%	27.7%	3.4%	4.2%	2.5%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%
				8:00	9:00	250	0.0%	66.0%	16.1%	3.2%	5.0%	6.0%	1.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				9:00	10:00	200	0.0%	59.5%	19.1%	3.3%	5.5%	7.7%	1.6%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	0.0%	0.0%
				10:00	11:00	200	0.0%	51.0%	18.6%	5.6%	12.4%	8.7%	1.9%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				11:00	12:00	150	0.0%	53.0%	17.0%	7.5%	17.0%	3.4%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				12:00	13:00	200	0.0%	61.6%	18.9%	4.9%	9.8%	3.0%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				13:00	14:00	150	0.0%	47.5%	14.5%	6.9%	13.8%	11.0%	2.8%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%
				14:00	15:00	150	0.0%	45.8%	20.7%	6.4%	13.6%	8.6%	2.1%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				15:00	16:00	200	0.0%	49.0%	19.5%	5.7%	12.1%	9.2%	2.3%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%
				16:00	17:00	150	0.0%	61.2%	16.3%	4.8%	13.6%	3.4%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	250	0.0%	67.6%	15.4%	3.0%	8.0%	4.5%	1.0%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	200	0.0%	71.5%	16.5%	1.7%	6.3%	2.8%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	200	0.0%	75.1%	20.7%	0.6%	1.8%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				20:00	21:00	150	0.0%	68.4%	28.3%	0.8%	1.7%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	0.0%	62.4%	32.7%	1.0%	2.9%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	0.0%	57.6%	40.0%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	100	0.0%	56.7%	40.3%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L047	Ma Tau Wai Road (SB)	Wing Kwong Street	Bailey Street	0:00	1:00	450	3.8%	25.2%	52.5%	0.0%	2.0%	0.0%	0.0%	0.0%	7.2%	0.2%	0.2%	0.9%	0.4%	0.2%	0.0%	7.4%
				1:00	2:00	250	3.9%	28.7%	62.7%	0.0%	3.0%	0.0%	0.0%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	250	3.7%	26.2%	65.9%	0.0%	2.3%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	200	5.2%	29.2%	61.7%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	4.4%	26.3%	65.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	200	3.8%	24.5%	62.3%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.6%	0.6%	1.9%	1.3%	0.6%	0.0%	0.0%
				6:00	7:00	400	2.5%	19.1%	49.1%	0.5%	3.8%	0.8%	0.0%	0.0%	8.2%	0.8%	0.8%	1.9%	1.6%	0.8%	0.0%	10.1%
				7:00	8:00	1,050	1.2%	24.7%	33.4%	0.8%	4.1%	1.3%	0.2%	0.2%	11.1%	1.0%	0.9%	2.0%	2.0%	0.8%	0.0%	16.3%
				8:00	9:00	1,450	2.3%	36.5%	25.4%	1.0%	7.0%	4.5%	0.6%	0.6%	6.3%	0.8%	0.8%	1.6%	1.6%	0.6%	0.0%	10.4%
				9:00	10:00	1,350	1.7%	30.0%	27.8%	1.0%	7.2%	5.1%	0.7%	0.7%	5.9%	1.3%	1.0%	2.2%	2.3%	0.9%	0.0%	12.2%
				10:00	11:00	1,150	3.4%	27.0%	27.9%	1.5%	12.9%	5.7%	1.1%	1.1%	5.1%	0.4%	0.5%	1.1%	1.1%	0.4%	0.0%	10.8%
				11:00	12:00	1,150	2.2%	26.0%	24.0%	2.0%	16.0%	2.2%	0.4%	0.4%	9.2%	1.3%	0.7%	1.5%	1.7%	0.5%	0.0%	11.9%
				12:00	13:00	1,150	2.7%	33.6%	29.5%	1.4%	10.5%	2.0%	0.4%	0.4%	6.8%	1.4%	0.4%	0.6%	0.7%	0.3%	0.0%	9.3%
				13:00	14:00	1,100	2.1%	23.4%	20.3%	1.7%	13.4%	7.0%	1.3%	1.3%	6.8%	0.4%	1.6%	2.9%	3.2%	1.0%	0.0%	13.6%
				14:00	15:00	1,100	1.8%	22.1%	28.2%	1.6%	12.7%	5.5%	1.0%	0.9%	7.3%	0.7%	1.0%	2.0%	2.1%	0.7%	0.0%	12.4%
				15:00	16:00	1,300	2.6%	25.4%	28.8%	1.5%	12.1%	5.9%	1.1%	1.0%	5.7%	1.0%	0.7%	1.4%	1.5%	0.5%	0.0%	10.8%
				16:00	17:00	1,100	4.5%	32.4%	25.1%	1.2%	11.2%	1.9%	0.5%	0.4%	6.4%	0.2%	0.9%	1.4%	1.7%	0.4%	0.0%	11.8%
				17:00	18:00	1,350	5.4%	39.0%	25.4%	0.8%	7.1%	3.0%	0.8%	0.8%	5.3%	0.5%	0.5%	0.8%	0.8%	0.2%	0.0%	9.6%
				18:00	19:00	1,150	5.7%	42.7%	28.1%	0.5%	5.9%	2.0%	0.5%	0.5%	2.5%	0.4%	0.6%	1.0%	1.2%	0.3%	0.0%	8.1%
				19:00	20:00	1,150	3.1%	41.3%	32.6%	0.2%	2.0%	0.8%	0.1%	0.2%	7.2%	0.5%	0.4%	0.7%	0.7%	0.3%	0.0%	9.9%
				20:00	21:00	950	3.0%	32.8%	38.5%	0.2%	1.6%	0.3%	0.1%	0.1%	11.3%	0.2%	0.1%	0.3%	0.2%	0.1%	0.0%	11.2%
				21:00	22:00	900	2.2%	28.3%	42.6%	0.2%	2.2%	0.7%	0.1%	0.1%	10.5%	0.0%	0.0%	0.1%	0.0%	0.1%	0.0%	12.9%
				22:00	23:00	850	3.0%	21.9%	43.3%	0.2%	1.2%	0.0%	0.0%	0.0%	11.1%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	18.0%
L048	Ma Tau Wai Road (NB)	Gillies Avenue North	Shek Tong Street	23:00	0:00	700	3.1%	21.2%	43.0%	0.3%	1.5%	0.0%	0.0%	0.0%	11.2%	0.3%	0.3%	0.6%	0.3%	0.3%	0.0%	17.9%
				0:00	1:00	150	4.1%	18.7%	31.8%	0.0%	1.6%	0.0%	0.0%	0.0%	20.3%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	18.7%
				1:00	2:00	50	7.1%	33.3%	57.2%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	5.3%	31.6%	60.5%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.1%	32.1%	57.2%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	7.7%	30.8%	57.7%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	5.9%	23.5%	47.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	2.9%	2.9%	5.9%	2.9%	2.9%	0.0%	0.0%
				6:00	7:00	150	2.7%	13.3%	26.3%	0.0%	2.7%	0.9%	0.0%	0.0%	20.4%	1.8%	1.8%	3.5%	2.7%	1.8%	0.0%	22.1%
				7:00	8:00	350	1.2%	9.6%	14.6%	0.3%	1.9%	0.9%	0.3%	0.3%	20.2%	1.6%	1.6%	3.4%	3.4%	1.6%	0.0%	39.1%
				8:00	9:00	400	2.8%	17.5%	13.9%	0.3%	4.2%	3.6%	0.8%	0.8%	14.1%	1.7%	1.7%	3.3%	3.3%	1.4%	0.0%	30.6%
				9:00	10:00	400	2.0%	13.2%	14.0%	0.3%	3.9%	3.9%	0.8%	0.8%	12.3%	2.5%	2.0%	4.2%	4.5%	2.0%	0.0%	33.6%
				10:00	11:00	350	3.6%	19.1%	15.8%	0.3%	9.3%	4.2%	1.2%	0.9%	13.4%	0.9%	1.2%	2.1%	2.1%	0.9%	0.0%	25.0%
				11:00	12:00	400	2.2%	16.4%	12.1%	0.5%	10.5%	1.3%	0.3%	0.3%	21.6%	2.4%	1.3%	2.4%	2.7%	1.1%	0.0%	24.9%
				12:00	13:00	350	2.8%	24.0%	16.8%	0.3%	7.6%	1.5%	0.3%	0.3%	18.0%	2.8%	0.6%	1.2%	1.5%	0.6%	0.0%	21.7%
				13:00	14:00	400	1.9%	14.3%	10.0%	0.3%	8.6%	4.3%	1.1%	1.1%	15.4%	0.8%	2.7%	4.6%	5.1%	2.2%	0.0%	27.6%
				14:00	15:00	350	1.7%	14.5%	14.5%	0.3%	8.4%	3.5%	0.9%	0.9%	17.7%	1.2%	1.7%	3.5%	3.5%	1.4%	0.0%	26.3%
				15:00	16:00	400	2.7%	17.5%	15.9%	0.3%	8.5%	4.2%	1.1%	1.1%	14.6%	2.1%	1.3%	2.7%	2.7%	1.1%	0.0%	24.2%
				16:00	17:00	400	3.6%	26.4%	13.5%	0.3%	8.8%	1.3%	0.3%	0.3%	17.7%	0.3%	1.6%	2.3%	2.9%	1.0%	0.0%	19.7%
				17:00	18:00	500	4.8%	33.6%	14.4%	0.2%	5.9%	2.0%	0.7%	0.4%	15.8%	0.9%	0.9%	1.3%	1.3%	0.7%	0.0%	17.1%
				18:00	19:00	400	5.5%	39.3%	17.0%	0.3%	5.2%	1.4%	0.5%	0.3%	8.0%	0.8%	1.4%	1.9%	2.2%	0.8%	0.0%	15.4%
				19:00	20:00	350	3.1%	29.6%	18.5%	0.0%	1.5%	0.6%	0.0%	0.0%	18.8%	0.9%	0.6%	1.2%	1.5%	0.6%	0.0%	23.1%
				20:00	21:00	300	3.1%	21.8%	20.1%	0.0%	1.0%	0.3%	0.0%	0.0%	27.7%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	24.1%
				21:00	22:00	300	2.3%	18.8%	22.6%	0.0%	1.5%	0.4%	0.0%	0.0%	25.6%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	28.4%
				22:00	23:00	300	2.8%	13.3%	20.6%	0.0%	0.7%	0.0%	0.0%	0.0%	24.8%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	35.9%
				23:00	0:00	250	3.0%	12.9%	20.7%	0.0%	0.9%	0.0%	0.0%	0.0%	24.6%	0.4%	0.4%	0.9%	0.4%	0.4%	0.0%	35.4%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L049	Bailey Street (EB)	Ma Tau Wai Road	Wan On Street	0:00	1:00	250	2.8%	27.1%	62.1%	0.0%	2.8%	0.0%	0.0%	0.0%	0.9%	0.5%	0.5%	1.9%	0.9%	0.5%	0.0%	0.0%
				1:00	2:00	150	2.4%	27.6%	66.7%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	2.6%	25.0%	69.8%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.5%	27.1%	64.7%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	2.6%	25.0%	68.5%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.2%	22.0%	62.6%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	3.3%	2.2%	1.1%	0.0%	0.0%
				6:00	7:00	200	1.7%	20.2%	57.8%	0.6%	5.1%	1.1%	0.0%	0.0%	1.1%	1.7%	1.7%	3.9%	3.4%	1.7%	0.0%	0.0%
				7:00	8:00	450	1.0%	27.4%	51.8%	0.5%	5.2%	1.7%	0.2%	0.2%	1.4%	1.4%	1.4%	3.3%	3.1%	1.2%	0.0%	0.2%
				8:00	9:00	650	1.6%	37.7%	36.2%	0.6%	8.6%	5.6%	0.6%	0.5%	0.8%	1.1%	1.1%	2.4%	2.2%	0.8%	0.0%	0.2%
				9:00	10:00	600	1.2%	30.6%	39.4%	0.7%	8.7%	6.3%	0.7%	0.5%	0.7%	1.9%	1.5%	3.2%	3.2%	1.2%	0.0%	0.2%
				10:00	11:00	550	2.4%	29.1%	33.7%	1.3%	16.8%	7.5%	0.7%	0.7%	0.7%	0.9%	1.1%	2.2%	2.2%	0.7%	0.0%	0.0%
				11:00	12:00	550	1.7%	28.8%	29.4%	1.7%	21.6%	3.0%	0.4%	0.2%	1.3%	2.7%	1.5%	3.2%	3.4%	1.1%	0.0%	0.0%
				12:00	13:00	550	2.1%	36.6%	35.4%	1.1%	13.9%	2.6%	0.4%	0.2%	0.9%	2.8%	0.6%	1.3%	1.5%	0.6%	0.0%	0.0%
				13:00	14:00	550	1.5%	25.2%	24.1%	1.3%	17.5%	9.2%	0.9%	0.9%	0.9%	0.8%	3.0%	6.0%	6.4%	2.1%	0.0%	0.2%
				14:00	15:00	550	1.4%	24.1%	33.5%	1.4%	16.7%	7.2%	0.8%	0.6%	1.0%	1.4%	1.9%	4.1%	4.3%	1.6%	0.0%	0.0%
				15:00	16:00	650	1.8%	27.1%	33.8%	1.2%	15.6%	7.7%	0.8%	0.7%	0.8%	2.0%	1.5%	3.0%	3.0%	1.0%	0.0%	0.0%
				16:00	17:00	600	3.2%	35.9%	24.4%	1.3%	15.6%	2.9%	0.2%	0.2%	1.1%	0.4%	2.5%	5.0%	5.7%	1.6%	0.0%	0.0%
				17:00	18:00	700	3.9%	44.6%	25.3%	0.9%	10.1%	4.7%	0.4%	0.4%	0.9%	1.3%	1.3%	2.6%	2.6%	1.0%	0.0%	0.0%
				18:00	19:00	650	3.9%	46.3%	26.6%	0.5%	7.8%	2.9%	0.3%	0.3%	0.3%	1.1%	1.8%	3.3%	3.8%	1.1%	0.0%	0.0%
				19:00	20:00	550	2.4%	46.4%	40.6%	0.2%	2.8%	1.2%	0.0%	0.2%	1.0%	0.8%	0.6%	1.6%	1.6%	0.6%	0.0%	0.0%
				20:00	21:00	450	2.5%	39.5%	51.0%	0.2%	2.5%	0.5%	0.0%	0.0%	1.7%	0.5%	0.2%	0.7%	0.5%	0.2%	0.0%	0.0%
				21:00	22:00	400	1.9%	34.3%	56.9%	0.3%	3.3%	1.1%	0.0%	0.0%	1.6%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%
				22:00	23:00	350	2.7%	28.2%	61.3%	0.3%	1.8%	0.0%	0.0%	0.0%	1.8%	0.6%	0.6%	1.2%	0.6%	0.6%	0.0%	0.3%
				23:00	0:00	300	2.6%	27.2%	60.7%	0.4%	2.6%	0.0%	0.0%	0.0%	2.2%	0.7%	0.7%	1.5%	0.7%	0.7%	0.0%	0.0%
L050	Bailey Street (WB)	Sung Chi Street	Ma Tau Wai Road	0:00	1:00	200	4.1%	33.5%	47.0%	0.0%	2.4%	0.0%	0.0%	0.0%	6.5%	0.6%	0.6%	1.2%	0.6%	0.0%	0.0%	3.5%
				1:00	2:00	100	4.5%	37.1%	55.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.7%	34.6%	59.2%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.9%	37.7%	52.5%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.8%	34.0%	58.4%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.2%	32.3%	54.9%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	1.6%	1.6%	0.0%	0.0%	0.0%
				6:00	7:00	150	3.0%	26.3%	46.4%	0.0%	5.3%	0.8%	0.0%	0.0%	7.5%	0.8%	0.8%	2.3%	1.5%	0.8%	0.0%	4.5%
				7:00	8:00	400	1.6%	29.6%	38.4%	0.5%	4.1%	1.1%	0.3%	0.3%	9.9%	1.1%	1.1%	2.5%	1.9%	0.8%	0.0%	6.8%
				8:00	9:00	550	3.0%	41.5%	27.4%	0.6%	6.7%	3.5%	1.1%	1.1%	5.2%	0.9%	0.9%	1.9%	1.5%	0.6%	0.0%	4.1%
				9:00	10:00	500	2.3%	34.7%	30.5%	0.6%	7.0%	4.1%	1.2%	1.2%	4.9%	1.6%	1.2%	2.7%	2.3%	0.8%	0.0%	4.9%
				10:00	11:00	450	3.7%	35.2%	25.0%	0.9%	16.1%	3.9%	1.4%	1.4%	4.6%	0.5%	0.5%	1.1%	1.1%	0.2%	0.0%	4.4%
				11:00	12:00	450	2.3%	34.0%	21.3%	1.2%	19.9%	1.4%	0.7%	0.5%	8.3%	1.4%	0.7%	1.6%	1.6%	0.5%	0.0%	4.6%
				12:00	13:00	450	2.9%	42.6%	25.3%	0.9%	12.6%	1.3%	0.7%	0.4%	6.1%	1.6%	0.4%	0.7%	0.7%	0.2%	0.0%	3.6%
				13:00	14:00	450	2.2%	31.3%	18.4%	1.0%	16.9%	4.8%	1.9%	1.9%	6.3%	0.5%	1.7%	3.4%	3.1%	1.0%	0.0%	5.6%
				14:00	15:00	450	2.0%	30.1%	25.7%	1.0%	16.2%	3.7%	1.5%	1.2%	7.0%	0.7%	1.0%	2.2%	2.0%	0.7%	0.0%	5.0%
				15:00	16:00	500	2.7%	33.5%	25.7%	0.8%	15.1%	4.0%	1.7%	1.5%	5.2%	1.0%	0.8%	1.7%	1.5%	0.4%	0.0%	4.4%
				16:00	17:00	450	4.1%	45.6%	16.4%	0.7%	16.4%	0.9%	0.5%	0.5%	6.2%	0.2%	0.7%	1.4%	1.6%	0.2%	0.0%	4.6%
				17:00	18:00	600	4.8%	54.1%	16.4%	0.5%	10.2%	1.4%	0.9%	0.7%	5.0%	0.4%	0.4%	0.7%	0.7%	0.2%	0.0%	3.6%
				18:00	19:00	500	5.0%	58.5%	18.0%	0.4%	8.3%	1.0%	0.6%	0.4%	2.3%	0.2%	0.4%	0.8%	1.0%	0.2%	0.0%	2.9%
				19:00	20:00	450	3.2%	52.6%	28.1%	0.0%	2.3%	0.5%	0.2%	0.2%	6.4%	0.5%	0.5%	0.7%	0.7%	0.2%	0.0%	3.9%
				20:00	21:00	400	3.4%	43.2%	34.4%	0.0%	2.0%	0.3%	0.3%	0.3%	10.4%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	4.5%
				21:00	22:00	350	2.6%	38.7%	39.3%	0.0%	2.9%	0.6%	0.3%	0.3%	9.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.4%
				22:00	23:00	300	3.5%	31.6%	42.2%	0.0%	1.7%	0.0%	0.0%	0.0%	11.1%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	8.0%
				23:00	0:00	250	3.4%	30.5%	42.1%	0.0%	2.1%	0.0%	0.0%	0.0%	11.2%	0.4%	0.4%	0.9%	0.4%	0.4%	0.0%	8.2%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L051	Bailey Street (EB)	Wan On Street	Sung On Street	0:00	1:00	250	2.9%	25.5%	63.8%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	1.0%	0.5%	0.5%	1.9%	1.0%	0.5%	0.0%	0.0%	
				1:00	2:00	150	2.5%	26.4%	67.8%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.6%	23.7%	71.1%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.6%	25.3%	66.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	2.7%	23.0%	70.2%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.2%	21.1%	63.5%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	3.3%	2.2%	1.1%	0.0%	0.0%	0.0%
				6:00	7:00	200	1.7%	19.4%	59.1%	0.0%	5.1%	1.1%	0.0%	0.0%	1.1%	1.7%	1.7%	4.0%	3.4%	1.7%	0.0%	0.0%	0.0%	
				7:00	8:00	450	0.9%	26.6%	52.2%	0.5%	5.6%	1.6%	0.2%	0.2%	1.4%	1.6%	1.6%	3.2%	3.0%	1.2%	0.0%	0.2%	0.2%	
				8:00	9:00	650	1.6%	36.8%	36.9%	0.6%	9.0%	5.3%	0.6%	0.5%	0.8%	1.2%	1.2%	2.3%	2.2%	0.8%	0.0%	0.2%	0.2%	
				9:00	10:00	600	1.2%	29.8%	39.5%	0.7%	9.2%	6.0%	0.7%	0.5%	0.7%	2.2%	1.7%	3.2%	3.2%	1.2%	0.0%	0.2%	0.2%	
				10:00	11:00	550	2.5%	27.8%	34.4%	0.8%	17.2%	7.4%	0.9%	0.8%	0.8%	0.9%	1.1%	2.3%	2.3%	0.8%	0.0%	0.0%	0.0%	
				11:00	12:00	550	1.8%	27.5%	30.3%	1.0%	22.0%	2.9%	0.4%	0.2%	1.4%	2.9%	1.6%	3.3%	3.5%	1.2%	0.0%	0.0%	0.2%	
				12:00	13:00	550	2.1%	34.9%	36.5%	0.8%	14.1%	2.5%	0.4%	0.2%	1.0%	3.1%	0.8%	1.4%	1.6%	0.6%	0.0%	0.0%	0.0%	
				13:00	14:00	550	1.6%	23.9%	24.7%	0.8%	17.7%	8.9%	1.2%	1.0%	1.0%	0.8%	3.3%	6.2%	6.6%	2.1%	0.0%	0.2%	0.2%	
				14:00	15:00	500	1.4%	22.8%	34.8%	0.8%	16.8%	7.0%	1.0%	0.6%	1.0%	1.4%	2.2%	4.2%	4.4%	1.6%	0.0%	0.0%	0.0%	
				15:00	16:00	600	1.9%	25.8%	34.9%	0.7%	15.8%	7.5%	1.0%	0.7%	0.8%	2.2%	1.5%	3.1%	3.1%	1.0%	0.0%	0.0%	0.0%	
				16:00	17:00	550	3.5%	33.9%	25.3%	0.4%	15.5%	2.9%	0.4%	0.2%	1.2%	0.4%	2.7%	5.5%	6.3%	1.8%	0.0%	0.0%	0.0%	
				17:00	18:00	650	4.3%	42.2%	26.4%	0.3%	10.1%	4.7%	0.8%	0.5%	1.0%	1.4%	1.4%	2.9%	2.9%	1.1%	0.0%	0.0%	0.0%	
				18:00	19:00	600	4.3%	43.3%	27.7%	0.2%	7.9%	3.0%	0.5%	0.4%	0.4%	1.3%	2.0%	3.6%	4.1%	1.3%	0.0%	0.0%	0.0%	
				19:00	20:00	500	2.4%	44.5%	42.0%	0.0%	2.9%	1.2%	0.2%	0.2%	1.0%	1.0%	0.8%	1.6%	1.6%	0.6%	0.0%	0.0%	0.0%	
				20:00	21:00	400	2.6%	37.5%	52.6%	0.0%	2.3%	0.5%	0.3%	0.0%	1.8%	0.5%	0.3%	0.8%	0.5%	0.3%	0.0%	0.0%	0.0%	
				21:00	22:00	400	2.0%	32.5%	58.4%	0.0%	3.4%	1.1%	0.3%	0.0%	1.7%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	0.0%	
				22:00	23:00	350	2.8%	26.8%	62.9%	0.0%	1.8%	0.0%	0.0%	0.0%	1.8%	0.6%	0.6%	1.2%	0.6%	0.6%	0.0%	0.3%	0.3%	
				L052	Bailey Street (WB)	Sung On Street	Sung Chi Street	23:00	0:00	300	2.6%	25.7%	62.1%	0.0%	2.6%	0.0%	0.0%	0.0%	2.3%	0.8%	0.8%	1.5%	0.8%	0.8%
0:00	1:00	150	2.9%					30.9%	49.7%	0.0%	2.2%	0.0%	0.0%	0.0%	7.9%	0.7%	0.0%	0.7%	0.7%	0.0%	0.0%	4.3%		
1:00	2:00	100	2.7%					35.1%	58.1%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	100	2.9%					32.4%	61.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	4.1%					34.7%	57.1%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	2.3%					32.6%	62.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	2.0%					30.0%	58.0%	0.0%	6.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	150	1.8%					24.3%	47.8%	0.0%	4.5%	0.9%	0.0%	0.0%	9.0%	0.9%	0.9%	1.8%	1.8%	0.9%	0.0%	5.4%	5.4%	
7:00	8:00	350	0.6%					26.0%	40.0%	0.3%	3.9%	1.0%	0.3%	0.3%	11.7%	1.3%	1.0%	2.6%	1.9%	1.0%	0.0%	8.1%	8.1%	
8:00	9:00	450	1.6%					38.4%	29.8%	0.2%	6.9%	3.5%	0.9%	0.9%	6.5%	0.9%	0.9%	2.1%	1.6%	0.7%	0.0%	5.1%	5.1%	
9:00	10:00	400	1.3%					31.6%	32.6%	0.3%	7.1%	4.0%	1.0%	1.0%	6.1%	1.5%	1.3%	2.8%	2.3%	1.0%	0.0%	6.1%	6.1%	
10:00	11:00	400	2.5%					33.4%	26.6%	0.6%	15.9%	3.7%	1.4%	1.1%	5.7%	0.6%	0.6%	1.1%	1.1%	0.3%	0.0%	5.4%	5.4%	
11:00	12:00	400	1.7%					31.6%	22.4%	0.8%	19.3%	1.4%	0.6%	0.3%	10.1%	1.4%	0.8%	1.7%	1.7%	0.6%	0.0%	5.6%	5.6%	
12:00	13:00	400	1.9%					40.2%	27.1%	0.6%	12.4%	1.1%	0.6%	0.3%	7.5%	1.7%	0.3%	0.8%	0.8%	0.3%	0.0%	4.4%	4.4%	
13:00	14:00	350	1.5%					29.1%	19.5%	0.6%	16.6%	4.4%	1.8%	1.5%	7.7%	0.6%	1.8%	3.6%	3.3%	1.2%	0.0%	6.8%	6.8%	
14:00	15:00	350	1.5%					27.5%	26.9%	0.6%	15.6%	3.6%	1.2%	1.2%	8.4%	0.9%	1.2%	2.4%	2.1%	0.9%	0.0%	6.0%	6.0%	
15:00	16:00	400	2.0%					31.4%	27.0%	0.5%	14.8%	3.8%	1.5%	1.3%	6.4%	1.3%	0.8%	1.8%	1.5%	0.5%	0.0%	5.4%	5.4%	
16:00	17:00	400	3.4%					43.8%	17.1%	0.6%	16.0%	0.8%	0.6%	0.3%	7.6%	0.3%	0.8%	1.4%	1.4%	0.3%	0.0%	5.6%	5.6%	
17:00	18:00	500	4.0%					52.4%	17.3%	0.4%	10.0%	1.3%	0.9%	0.7%	6.2%	0.4%	0.4%	0.7%	0.7%	0.2%	0.0%	4.4%	4.4%	
18:00	19:00	400	4.2%					57.0%	19.2%	0.3%	8.1%	0.8%	0.5%	0.5%	2.9%	0.3%	0.5%	0.8%	1.0%	0.3%	0.0%	3.6%	3.6%	
19:00	20:00	400	2.2%					49.4%	29.8%	0.0%	2.2%	0.6%	0.0%	0.3%	7.9%	0.6%	0.3%	0.8%	0.8%	0.3%	0.0%	4.8%	4.8%	
20:00	21:00	300	2.4%					40.3%	36.1%	0.0%	2.0%	0.3%	0.0%	0.0%	12.6%	0.3%	0.0%	0.3%	0.3%	0.0%	0.0%	5.4%	5.4%	
21:00	22:00	300	1.9%					35.6%	41.0%	0.0%	2.7%	0.4%	0.0%	0.0%	11.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.5%	6.5%	
22:00	23:00	250	2.4%					28.6%	42.9%	0.0%	1.6%	0.0%	0.0%	0.0%	13.1%	0.4%	0.4%	0.4%	0.4%	0.4%	0.0%	9.4%	9.4%	
23:00	0:00	200	2.5%	27.6%	42.8%	0.0%	2.0%	0.0%	0.0%	0.0%	13.1%	0.5%	0.5%	0.5%	0.5%	0.5%	0.0%	9.5%	9.5%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L053	Bailey Street (EB)	Sung On Street	Hung Hom Road	0:00	1:00	300	2.7%	30.0%	55.6%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	5.4%	0.4%	0.4%	1.6%	0.8%	0.4%	0.0%	0.0%	
				1:00	2:00	150	2.8%	31.7%	62.0%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.3%	28.8%	65.9%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.1%	32.0%	59.7%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	2.3%	29.1%	63.9%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	1.9%	26.2%	59.2%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	1.0%	2.9%	1.9%	1.0%	0.0%	0.0%	0.0%
				6:00	7:00	250	1.9%	23.1%	52.8%	0.5%	5.3%	1.0%	0.0%	0.0%	6.3%	1.4%	1.4%	2.9%	2.4%	1.0%	0.0%	0.0%	0.0%	
				7:00	8:00	550	0.9%	28.5%	46.4%	0.6%	5.1%	1.3%	0.4%	0.4%	7.1%	1.3%	1.3%	2.8%	2.6%	0.9%	0.0%	0.4%	0.0%	
				8:00	9:00	800	1.9%	39.2%	32.6%	0.6%	8.2%	4.5%	1.3%	1.1%	3.8%	1.0%	1.0%	2.0%	1.9%	0.6%	0.0%	0.3%	0.0%	
				9:00	10:00	750	1.5%	32.4%	35.9%	0.5%	8.4%	5.2%	1.4%	1.2%	3.6%	1.8%	1.4%	2.7%	2.7%	1.0%	0.0%	0.3%	0.0%	
				10:00	11:00	700	2.5%	31.9%	30.0%	0.8%	17.1%	5.8%	1.4%	1.4%	3.8%	0.6%	0.8%	1.7%	1.5%	0.5%	0.0%	0.2%	0.0%	
				11:00	12:00	650	1.6%	31.2%	25.8%	1.1%	21.4%	2.2%	0.6%	0.5%	6.9%	2.0%	1.1%	2.5%	2.3%	0.6%	0.0%	0.2%	0.0%	
				12:00	13:00	700	2.0%	39.4%	30.9%	0.8%	13.6%	2.0%	0.6%	0.5%	4.9%	2.1%	0.5%	1.1%	1.1%	0.3%	0.0%	0.2%	0.0%	
				13:00	14:00	650	1.6%	28.0%	21.8%	1.0%	17.8%	7.2%	1.8%	1.8%	5.1%	0.6%	2.4%	4.7%	4.7%	1.3%	0.0%	0.2%	0.0%	
				14:00	15:00	650	1.3%	26.9%	30.5%	0.8%	17.0%	5.6%	1.3%	1.3%	5.4%	1.0%	1.5%	3.3%	3.1%	0.8%	0.0%	0.2%	0.0%	
				15:00	16:00	750	1.9%	30.1%	30.3%	0.8%	15.8%	6.0%	1.5%	1.4%	4.2%	1.7%	1.1%	2.4%	2.1%	0.6%	0.0%	0.1%	0.0%	
				16:00	17:00	650	3.0%	41.5%	20.8%	0.5%	16.5%	2.0%	0.5%	0.3%	5.7%	0.3%	1.7%	3.3%	3.3%	0.6%	0.0%	0.0%	0.0%	
				17:00	18:00	800	3.6%	49.8%	21.0%	0.4%	10.4%	3.1%	0.8%	0.6%	4.8%	0.9%	0.9%	1.8%	1.5%	0.4%	0.0%	0.0%	0.0%	
				18:00	19:00	700	3.7%	53.2%	22.8%	0.3%	8.3%	2.0%	0.4%	0.4%	2.2%	0.7%	1.1%	2.3%	2.2%	0.4%	0.0%	0.0%	0.0%	
				19:00	20:00	650	2.2%	49.5%	35.0%	0.2%	2.7%	0.8%	0.2%	0.2%	5.4%	0.6%	0.5%	1.1%	1.1%	0.3%	0.0%	0.2%	0.0%	
				20:00	21:00	550	2.4%	40.8%	42.8%	0.2%	2.2%	0.4%	0.2%	0.2%	8.8%	0.4%	0.2%	0.6%	0.4%	0.2%	0.0%	0.2%	0.0%	
				21:00	22:00	500	1.8%	36.1%	48.7%	0.2%	3.1%	0.9%	0.2%	0.2%	8.4%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	0.2%	0.0%	
				22:00	23:00	450	2.4%	30.1%	53.0%	0.2%	1.9%	0.0%	0.0%	0.0%	9.5%	0.5%	0.5%	1.0%	0.5%	0.2%	0.0%	0.2%	0.0%	
				23:00	0:00	350	2.7%	29.0%	52.7%	0.3%	2.4%	0.0%	0.0%	0.0%	9.3%	0.6%	0.6%	1.2%	0.6%	0.3%	0.0%	0.3%	0.0%	
L054	Bailey Street (WB)	Hung Hom Road	Sung On Street	0:00	1:00	150	2.1%	28.7%	49.6%	0.0%	2.1%	0.0%	0.0%	0.0%	8.4%	0.7%	0.7%	2.1%	0.7%	0.7%	0.0%	4.2%		
				1:00	2:00	100	2.7%	34.2%	60.4%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	1.5%	31.3%	64.2%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.0%	34.7%	59.2%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.3%	32.6%	62.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	1.9%	27.8%	55.3%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	3.7%	1.9%	1.9%	0.0%	0.0%	0.0%	
				6:00	7:00	150	1.7%	21.5%	45.3%	0.0%	4.1%	0.8%	0.0%	0.0%	9.1%	1.7%	1.7%	4.1%	3.3%	1.7%	0.0%	5.0%	0.0%	
				7:00	8:00	350	0.3%	25.8%	36.4%	0.3%	3.6%	0.6%	0.3%	0.3%	12.2%	1.8%	1.8%	3.6%	3.6%	1.5%	0.0%	7.9%	0.0%	
				8:00	9:00	500	0.9%	38.8%	27.5%	0.2%	6.2%	2.6%	1.1%	0.9%	6.8%	1.5%	1.5%	2.9%	2.9%	1.1%	0.0%	5.1%	0.0%	
				9:00	10:00	450	0.7%	31.5%	29.8%	0.2%	6.1%	3.1%	1.2%	0.9%	6.4%	2.6%	2.1%	3.8%	4.0%	1.7%	0.0%	5.9%	0.0%	
				10:00	11:00	400	1.9%	31.8%	26.9%	0.3%	14.2%	3.3%	1.4%	1.1%	6.1%	1.1%	1.1%	2.2%	2.5%	0.8%	0.0%	5.3%	0.0%	
				11:00	12:00	400	1.3%	29.3%	22.0%	0.5%	16.9%	1.3%	0.5%	0.3%	10.2%	2.7%	1.6%	3.2%	3.5%	1.1%	0.0%	5.6%	0.0%	
				12:00	13:00	400	1.6%	38.3%	27.3%	0.3%	11.2%	1.1%	0.5%	0.3%	7.7%	3.0%	0.8%	1.4%	1.6%	0.5%	0.0%	4.4%	0.0%	
				13:00	14:00	400	1.1%	26.0%	18.6%	0.3%	14.0%	4.1%	1.6%	1.4%	7.4%	0.8%	3.3%	6.0%	6.6%	2.5%	0.0%	6.3%	0.0%	
				14:00	15:00	350	1.1%	25.1%	26.5%	0.3%	13.4%	3.1%	1.1%	1.1%	8.3%	1.4%	2.0%	4.3%	4.6%	1.7%	0.0%	6.0%	0.0%	
				15:00	16:00	450	1.5%	29.2%	27.2%	0.2%	12.8%	3.5%	1.5%	1.2%	6.4%	2.2%	1.5%	3.2%	3.2%	1.2%	0.0%	5.2%	0.0%	
				16:00	17:00	400	3.0%	37.5%	18.6%	0.3%	13.5%	1.1%	0.3%	0.3%	7.3%	0.3%	2.2%	4.0%	4.9%	1.3%	0.0%	5.4%	0.0%	
				17:00	18:00	500	3.5%	46.7%	19.5%	0.2%	8.8%	1.8%	0.7%	0.7%	6.2%	1.1%	1.1%	2.2%	2.2%	0.9%	0.0%	4.4%	0.0%	
				18:00	19:00	400	3.6%	49.6%	21.1%	0.3%	7.1%	1.3%	0.5%	0.5%	2.8%	1.0%	1.5%	2.8%	3.3%	1.0%	0.0%	3.6%	0.0%	
				19:00	20:00	400	1.9%	46.6%	30.4%	0.0%	2.2%	0.6%	0.0%	0.3%	8.0%	0.8%	0.8%	1.4%	1.7%	0.6%	0.0%	4.7%	0.0%	
				20:00	21:00	300	1.7%	38.6%	37.1%	0.0%	1.7%	0.3%	0.0%	0.0%	13.3%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	5.4%	0.0%	
				21:00	22:00	300	1.5%	34.0%	41.9%	0.0%	2.7%	0.4%	0.0%	0.0%	12.6%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	6.5%	0.0%	
				22:00	23:00	250	2.0%	26.9%	43.4%	0.0%	1.6%	0.0%	0.0%	0.0%	13.7%	0.8%	0.4%	1.2%	0.4%	0.4%	0.0%	9.2%	0.0%	
				23:00	0:00	250	2.0%	26.0%	43.0%	0.0%	2.0%	0.0%	0.0%	0.0%	13.7%	1.0%	0.5%	1.5%	0.5%	0.5%	0.0%	9.3%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L055	Sung Ping Street (SB)	Bailey Street	Hok Yeun Street East	0:00	1:00	50	3.0%	57.6%	36.4%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	4.8%	52.3%	38.1%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	5.6%	50.0%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	8.3%	50.0%	41.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	4.0%	48.0%	40.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	100	1.5%	58.5%	20.0%	0.0%	4.6%	3.1%	0.0%	0.0%	0.0%	1.5%	1.5%	3.1%	3.1%	3.1%	0.0%	0.0%	0.0%
				8:00	9:00	150	2.4%	63.6%	11.4%	0.0%	6.5%	8.9%	0.8%	0.0%	0.0%	0.8%	0.8%	1.6%	1.6%	1.6%	0.0%	0.0%	0.0%
				9:00	10:00	150	1.9%	54.5%	13.0%	0.0%	7.4%	11.1%	0.9%	0.0%	0.0%	1.9%	0.9%	2.8%	2.8%	2.8%	0.0%	0.0%	0.0%
				10:00	11:00	150	3.9%	49.4%	16.5%	0.0%	13.6%	10.7%	1.9%	1.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	2.2%	52.1%	16.3%	0.0%	19.6%	4.3%	1.1%	0.0%	0.0%	1.1%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%
				12:00	13:00	150	2.9%	61.8%	17.6%	0.0%	10.8%	3.9%	1.0%	0.0%	0.0%	1.0%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	2.1%	45.3%	12.6%	0.0%	14.7%	13.7%	2.1%	2.1%	0.0%	0.0%	1.1%	2.1%	2.1%	2.1%	0.0%	0.0%	0.0%
				14:00	15:00	100	2.3%	45.1%	18.4%	0.0%	14.9%	11.5%	2.3%	1.1%	0.0%	0.0%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%
				15:00	16:00	150	2.7%	47.2%	17.9%	0.0%	13.4%	11.6%	1.8%	0.9%	0.0%	0.9%	0.9%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%
				16:00	17:00	150	4.7%	58.9%	17.8%	0.0%	13.1%	3.7%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	150	4.7%	64.2%	16.2%	0.0%	7.4%	4.7%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	150	4.6%	67.0%	17.6%	0.0%	6.1%	3.1%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	150	2.9%	73.1%	19.2%	0.0%	1.9%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	4.0%	68.1%	25.3%	0.0%	1.3%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	3.1%	61.5%	30.8%	0.0%	3.1%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.8%	57.8%	36.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L056	Sung Ping Street (NB)	Hok Yeun Street East	Bailey Street	23:00	0:00	50	4.8%	54.7%	38.1%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				0:00	1:00	50	3.7%	55.6%	37.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	5.9%	52.9%	35.3%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	53.8%	46.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	54.5%	36.4%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	40.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	5.3%	47.3%	42.1%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	2.0%	77.6%	12.2%	0.0%	8.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	100	3.0%	79.8%	6.1%	0.0%	10.1%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	2.6%	76.9%	7.7%	0.0%	11.5%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	2.9%	57.1%	20.0%	0.0%	18.6%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	100	1.5%	55.9%	17.6%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	2.6%	64.5%	18.4%	0.0%	14.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	1.7%	56.0%	16.9%	0.0%	23.7%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	100	1.7%	52.6%	22.0%	0.0%	22.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	2.7%	56.7%	20.3%	0.0%	18.9%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	100	2.9%	54.3%	27.1%	0.0%	14.3%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	100	3.2%	61.3%	25.8%	0.0%	8.6%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	3.5%	61.6%	26.7%	0.0%	7.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	2.5%	76.0%	19.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	100	3.4%	69.0%	25.9%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	2.0%	63.3%	30.6%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	50	2.4%	58.6%	36.6%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					
23:00	0:00	50	3.0%	57.6%	36.4%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L057	Hung Hom Road (SB)	Bailey Street	Hok Yeun Street East	0:00	1:00	300	2.5%	28.2%	58.4%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	5.1%	0.4%	0.4%	1.4%	0.7%	0.4%	0.0%	0.0%	
				1:00	2:00	200	2.6%	29.5%	64.1%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.1%	27.1%	68.0%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	2.9%	29.5%	62.8%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.2%	26.3%	66.2%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.7%	23.9%	60.9%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	2.7%	1.8%	0.9%	0.0%	0.0%	0.0%
				6:00	7:00	250	1.8%	21.3%	55.6%	0.4%	4.9%	0.9%	0.0%	0.0%	5.8%	1.3%	1.3%	3.1%	2.7%	0.9%	0.0%	0.0%	0.0%	
				7:00	8:00	600	0.9%	25.4%	50.1%	0.5%	4.9%	1.4%	0.3%	0.3%	6.5%	1.4%	1.4%	2.7%	2.9%	1.0%	0.0%	0.3%	0.0%	
				8:00	9:00	900	1.8%	35.8%	35.8%	0.6%	8.1%	4.6%	1.3%	1.2%	3.5%	1.1%	1.1%	2.1%	2.1%	0.7%	0.0%	0.2%	0.0%	
				9:00	10:00	800	1.4%	29.2%	39.1%	0.5%	8.2%	5.2%	1.4%	1.3%	3.3%	1.8%	1.4%	2.9%	3.0%	1.0%	0.0%	0.3%	0.0%	
				10:00	11:00	700	2.4%	30.2%	31.9%	0.9%	17.2%	5.4%	1.4%	1.3%	3.6%	0.7%	0.9%	1.7%	1.7%	0.6%	0.0%	0.1%	0.0%	
				11:00	12:00	700	1.6%	29.5%	27.3%	1.2%	21.8%	2.0%	0.6%	0.4%	6.4%	2.2%	1.2%	2.5%	2.5%	0.7%	0.0%	0.1%	0.0%	
				12:00	13:00	700	1.9%	37.4%	33.0%	0.7%	14.0%	1.9%	0.6%	0.4%	4.6%	2.3%	0.6%	1.0%	1.2%	0.3%	0.0%	0.1%	0.0%	
				13:00	14:00	700	1.5%	26.2%	23.3%	1.1%	18.2%	6.8%	1.8%	1.7%	4.8%	0.6%	2.6%	4.8%	5.0%	1.4%	0.0%	0.2%	0.0%	
				14:00	15:00	700	1.2%	25.2%	32.3%	0.9%	17.2%	5.2%	1.4%	1.2%	5.1%	1.1%	1.7%	3.2%	3.2%	0.9%	0.0%	0.2%	0.0%	
				15:00	16:00	800	1.8%	28.4%	32.3%	0.9%	16.0%	5.6%	1.6%	1.4%	3.9%	1.7%	1.2%	2.3%	2.2%	0.6%	0.0%	0.1%	0.0%	
				16:00	17:00	700	3.0%	41.0%	20.7%	0.7%	17.3%	1.6%	0.4%	0.3%	5.4%	0.3%	1.8%	3.4%	3.4%	0.7%	0.0%	0.0%	0.0%	
				17:00	18:00	850	3.7%	49.3%	21.2%	0.5%	11.0%	2.6%	0.7%	0.6%	4.5%	1.0%	1.0%	1.8%	1.6%	0.5%	0.0%	0.0%	0.0%	
				18:00	19:00	750	3.8%	52.4%	22.8%	0.3%	8.8%	1.8%	0.4%	0.4%	2.1%	0.8%	1.4%	2.3%	2.2%	0.5%	0.0%	0.0%	0.0%	
				19:00	20:00	700	2.2%	47.5%	37.4%	0.1%	2.7%	0.7%	0.1%	0.1%	5.1%	0.7%	0.6%	1.2%	1.2%	0.3%	0.0%	0.1%	0.0%	
				20:00	21:00	550	2.4%	38.7%	45.4%	0.2%	2.2%	0.4%	0.2%	0.2%	8.3%	0.4%	0.2%	0.6%	0.4%	0.2%	0.0%	0.2%	0.0%	
				21:00	22:00	500	1.6%	34.0%	51.3%	0.2%	3.3%	0.8%	0.2%	0.2%	7.8%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	0.2%	0.0%	
				22:00	23:00	450	2.5%	28.2%	55.4%	0.2%	1.8%	0.0%	0.0%	0.0%	8.8%	0.5%	0.5%	0.9%	0.5%	0.5%	0.0%	0.2%	0.0%	
23:00	0:00	400	2.8%	27.1%	54.9%	0.3%	2.5%	0.0%	0.0%	0.0%	8.6%	0.6%	0.6%	1.1%	0.6%	0.6%	0.0%	0.3%	0.0%					
L058	Hung Hom Road (NB)	Hok Yeun Street East	Bailey Street	0:00	1:00	200	2.0%	27.6%	50.6%	0.0%	2.6%	0.0%	0.0%	0.0%	7.9%	0.7%	0.7%	2.0%	1.3%	0.7%	0.0%	3.9%		
				1:00	2:00	100	2.6%	32.5%	61.0%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	100	1.4%	30.0%	65.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	100	1.9%	32.7%	59.6%	0.0%	5.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	2.1%	29.8%	63.8%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	100	1.7%	25.9%	55.3%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.4%	3.4%	1.7%	0.0%	0.0%		
				6:00	7:00	150	1.6%	20.2%	45.4%	0.0%	4.7%	0.8%	0.0%	0.0%	8.5%	1.6%	2.3%	4.7%	3.9%	1.6%	0.0%	4.7%		
				7:00	8:00	350	0.0%	22.5%	38.0%	0.3%	3.5%	1.4%	0.3%	0.3%	11.6%	2.0%	2.0%	4.3%	4.6%	1.7%	0.0%	7.5%		
				8:00	9:00	500	0.2%	33.8%	29.1%	0.2%	6.1%	4.9%	1.1%	1.1%	6.5%	1.7%	1.7%	3.6%	3.6%	1.5%	0.0%	4.9%		
				9:00	10:00	500	0.2%	26.8%	30.9%	0.2%	6.0%	5.3%	1.1%	1.1%	6.0%	2.9%	2.2%	4.7%	5.1%	2.0%	0.0%	5.5%		
				10:00	11:00	400	1.8%	28.9%	26.3%	0.5%	15.4%	5.3%	1.3%	1.5%	5.6%	1.0%	1.3%	2.5%	2.8%	1.0%	0.0%	4.8%		
				11:00	12:00	450	1.2%	27.1%	21.7%	0.7%	18.5%	2.0%	0.5%	0.5%	9.4%	3.0%	1.7%	3.4%	3.9%	1.2%	0.0%	5.2%		
				12:00	13:00	400	1.5%	36.1%	27.5%	0.5%	12.5%	1.8%	0.5%	0.5%	7.1%	3.3%	0.8%	1.5%	1.8%	0.5%	0.0%	4.1%		
				13:00	14:00	450	1.0%	23.6%	17.8%	0.5%	14.9%	6.1%	1.5%	1.7%	6.6%	1.0%	3.4%	6.6%	7.3%	2.4%	0.0%	5.6%		
				14:00	15:00	400	1.0%	23.1%	25.5%	0.5%	14.4%	4.9%	1.3%	1.3%	7.5%	1.5%	2.3%	4.6%	4.9%	1.8%	0.0%	5.4%		
				15:00	16:00	450	1.3%	26.9%	26.3%	0.4%	14.1%	5.4%	1.3%	1.3%	5.8%	2.5%	1.8%	3.3%	3.6%	1.3%	0.0%	4.7%		
				16:00	17:00	450	3.1%	37.1%	17.3%	0.5%	15.7%	1.4%	0.5%	0.5%	6.5%	2.2%	4.1%	4.6%	1.2%	0.0%	4.8%	4.8%		
				17:00	18:00	550	3.8%	46.2%	18.2%	0.4%	10.4%	2.2%	0.8%	0.8%	5.6%	1.2%	1.2%	2.2%	2.2%	0.8%	0.0%	4.0%		
				18:00	19:00	450	3.9%	49.9%	20.0%	0.2%	8.4%	1.4%	0.5%	0.5%	2.6%	0.9%	1.6%	2.8%	3.2%	0.9%	0.0%	3.2%		
				19:00	20:00	400	1.8%	45.2%	31.1%	0.0%	2.4%	0.8%	0.3%	0.3%	7.6%	1.1%	0.8%	1.8%	1.8%	0.5%	0.0%	4.5%		
				20:00	21:00	350	1.6%	37.3%	38.3%	0.0%	1.9%	0.3%	0.3%	0.3%	12.7%	0.3%	0.3%	0.6%	0.6%	0.3%	0.0%	5.2%		
				21:00	22:00	300	1.4%	32.9%	42.5%	0.0%	2.9%	0.7%	0.4%	0.4%	11.9%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	6.1%		
				22:00	23:00	300	1.9%	26.1%	44.4%	0.0%	1.5%	0.0%	0.0%	0.0%	13.0%	0.8%	0.8%	1.1%	0.8%	0.8%	0.0%	8.8%		
23:00	0:00	250	1.9%	25.2%	44.0%	0.0%	1.9%	0.0%	0.0%	0.0%	13.1%	0.9%	0.9%	1.4%	0.9%	0.9%	0.0%	8.9%						

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L059	Sung On Street (SB)	Bailey Street	Access Road	0:00	1:00	50	0.0%	51.4%	27.0%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	2.7%	5.4%	2.7%	2.7%	0.0%	0.0%
				1:00	2:00	50	0.0%	61.1%	33.3%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	56.2%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	61.5%	30.8%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	54.5%	36.4%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	38.7%	22.2%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	5.6%	5.6%	11.1%	5.6%	5.6%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	35.2%	20.6%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	5.9%	5.9%	11.8%	11.8%	2.9%	0.0%	0.0%	0.0%
				7:00	8:00	150	0.0%	44.2%	19.8%	0.0%	6.0%	1.7%	0.0%	0.0%	0.0%	0.0%	3.4%	3.4%	8.6%	8.6%	4.3%	0.0%	0.0%	0.0%
				8:00	9:00	200	0.0%	53.4%	12.2%	0.0%	8.6%	6.1%	0.5%	0.5%	0.0%	0.0%	2.5%	2.5%	5.6%	5.6%	2.5%	0.0%	0.0%	0.0%
				9:00	10:00	200	0.0%	43.0%	13.0%	0.0%	8.7%	7.1%	0.5%	0.5%	0.0%	0.0%	4.3%	3.3%	7.6%	8.2%	3.8%	0.0%	0.0%	0.0%
				10:00	11:00	150	0.8%	40.1%	10.2%	0.0%	19.7%	6.3%	2.4%	1.6%	0.0%	0.0%	2.4%	3.1%	5.5%	5.5%	2.4%	0.0%	0.0%	0.0%
				11:00	12:00	150	0.7%	36.5%	8.1%	0.0%	23.0%	2.2%	0.7%	0.7%	0.0%	0.0%	6.7%	3.7%	7.4%	8.1%	2.2%	0.0%	0.0%	0.0%
				12:00	13:00	150	0.8%	50.9%	11.3%	0.0%	16.1%	2.4%	0.8%	0.8%	0.0%	0.0%	7.3%	1.6%	3.2%	4.0%	0.8%	0.0%	0.0%	0.0%
				13:00	14:00	200	0.6%	28.1%	5.8%	0.0%	16.9%	6.5%	2.6%	1.9%	0.0%	0.0%	1.9%	6.5%	11.7%	13.0%	4.5%	0.0%	0.0%	0.0%
				14:00	15:00	150	0.8%	30.9%	10.1%	0.0%	18.6%	5.4%	2.3%	1.6%	0.0%	0.0%	3.1%	4.7%	9.3%	10.1%	3.1%	0.0%	0.0%	0.0%
				15:00	16:00	150	0.7%	35.5%	10.1%	0.0%	17.4%	6.0%	2.7%	2.0%	0.0%	0.0%	5.4%	3.4%	6.7%	7.4%	2.7%	0.0%	0.0%	0.0%
				16:00	17:00	150	1.7%	39.9%	3.5%	0.0%	18.3%	0.9%	1.7%	0.9%	0.0%	0.0%	0.9%	7.0%	10.4%	12.2%	2.6%	0.0%	0.0%	0.0%
				17:00	18:00	150	2.3%	53.4%	3.8%	0.0%	13.0%	0.8%	3.1%	2.3%	0.0%	0.0%	3.8%	3.8%	6.1%	6.1%	1.5%	0.0%	0.0%	0.0%
				18:00	19:00	150	2.5%	53.6%	4.1%	0.0%	9.9%	0.8%	1.7%	1.7%	0.0%	0.0%	3.3%	5.0%	7.4%	8.3%	1.7%	0.0%	0.0%	0.0%
				19:00	20:00	150	0.9%	68.1%	13.3%	0.0%	3.5%	0.9%	0.0%	0.0%	0.0%	0.0%	2.7%	1.8%	3.5%	4.4%	0.9%	0.0%	0.0%	0.0%
				20:00	21:00	100	1.3%	67.3%	19.7%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	1.3%	2.6%	1.3%	1.3%	0.0%	0.0%	0.0%
				21:00	22:00	100	1.6%	65.7%	24.6%	0.0%	4.9%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	1.9%	55.3%	27.8%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	3.7%	1.9%	1.9%	0.0%	0.0%	0.0%
				L060	Sung On Street (NB)	Access Road	Bailey Street	23:00	0:00	50	2.2%	53.5%	26.7%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	2.2%	4.4%	2.2%
0:00	1:00	50	5.0%					50.0%	25.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	2.5%	2.5%	5.0%	2.5%	2.5%	0.0%	0.0%	0.0%	
1:00	2:00	50	4.8%					57.1%	33.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	5.6%					55.5%	33.3%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	7.1%					57.2%	28.6%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	7.7%					53.8%	30.8%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	5.6%					38.6%	27.8%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.6%	5.6%	5.6%	5.6%	0.0%	0.0%	
6:00	7:00	50	3.1%					40.6%	25.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	3.1%	9.4%	6.3%	3.1%	0.0%	0.0%	
7:00	8:00	150	0.9%					33.3%	25.0%	0.0%	7.4%	1.9%	0.0%	0.0%	0.0%	0.0%	4.6%	4.6%	9.3%	9.3%	3.7%	0.0%	0.0%	
8:00	9:00	200	2.2%					41.7%	15.7%	0.0%	11.2%	5.6%	1.1%	1.1%	0.0%	0.0%	3.4%	3.4%	6.2%	6.2%	2.2%	0.0%	0.0%	
9:00	10:00	200	1.8%					32.6%	16.4%	0.0%	11.1%	6.4%	1.2%	1.2%	0.0%	0.0%	5.3%	4.1%	8.2%	8.8%	2.9%	0.0%	0.0%	
10:00	11:00	150	3.1%					44.1%	11.8%	0.0%	19.7%	6.3%	1.6%	0.8%	0.0%	0.0%	1.6%	1.6%	3.9%	3.9%	1.6%	0.0%	0.0%	
11:00	12:00	150	2.3%					41.9%	9.3%	0.0%	24.0%	2.3%	0.8%	0.0%	0.0%	0.0%	4.7%	2.3%	5.4%	5.4%	1.6%	0.0%	0.0%	
12:00	13:00	150	2.4%					54.2%	11.8%	0.0%	15.7%	2.4%	0.8%	0.0%	0.0%	0.0%	5.5%	1.6%	2.4%	2.4%	0.8%	0.0%	0.0%	
13:00	14:00	150	1.4%					33.5%	7.1%	0.0%	18.4%	7.1%	2.1%	1.4%	0.0%	0.0%	1.4%	5.0%	9.9%	9.2%	3.5%	0.0%	0.0%	
14:00	15:00	150	1.7%					36.3%	11.6%	0.0%	19.8%	5.8%	1.7%	0.8%	0.0%	0.0%	2.5%	3.3%	7.4%	6.6%	2.5%	0.0%	0.0%	
15:00	16:00	150	2.1%					40.5%	11.2%	0.0%	18.2%	6.3%	2.1%	0.7%	0.0%	0.0%	3.5%	2.8%	5.6%	4.9%	2.1%	0.0%	0.0%	
16:00	17:00	150	4.3%					66.4%	2.6%	0.0%	15.5%	1.7%	0.9%	0.0%	0.0%	0.0%	0.0%	1.7%	4.3%	1.7%	0.9%	0.0%	0.0%	
17:00	18:00	200	4.5%					76.3%	2.6%	0.0%	9.1%	1.9%	1.3%	0.0%	0.0%	0.0%	0.6%	0.6%	1.9%	0.6%	0.6%	0.0%	0.0%	
18:00	19:00	150	4.3%					78.5%	2.9%	0.0%	7.2%	1.4%	0.7%	0.0%	0.0%	0.0%	0.7%	0.7%	2.2%	0.7%	0.7%	0.0%	0.0%	
19:00	20:00	150	3.3%					70.1%	13.3%	0.0%	3.3%	0.8%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	2.5%	2.5%	0.8%	0.0%	0.0%	
20:00	21:00	100	3.6%					67.5%	19.3%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%	
21:00	22:00	100	3.0%					66.8%	24.2%	0.0%	4.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	100	5.0%					54.9%	26.7%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.3%	1.7%	1.7%	0.0%	0.0%	
23:00	0:00	50	4.0%	52.0%	28.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.0%	2.0%	2.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L061	Sung On Street (SB)	Access Road	Hok Yeun Street East	0:00	1:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	95.2%	0.0%	0.0%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	83.8%	0.0%	0.0%	2.0%	12.2%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	79.4%	0.0%	0.0%	2.6%	15.4%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	2.7%	70.3%	0.0%	0.0%	10.8%	13.5%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	3.0%	75.7%	0.0%	0.0%	15.2%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	2.6%	84.6%	0.0%	0.0%	7.7%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	2.9%	64.8%	0.0%	0.0%	11.8%	17.6%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	3.3%	70.1%	0.0%	0.0%	10.0%	13.3%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	2.6%	69.1%	0.0%	0.0%	10.3%	15.4%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	5.0%	80.0%	0.0%	0.0%	12.5%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	5.1%	83.0%	0.0%	0.0%	6.8%	3.4%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	5.7%	84.8%	0.0%	0.0%	5.7%	1.9%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	2.4%	92.8%	0.0%	0.0%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	3.7%	96.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	4.5%	95.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	5.9%	94.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	7.7%	92.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L062	Sung On Street (NB)	Access Road	Hok Yeun Street East	0:00	1:00	50	4.3%	47.9%	47.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	7.1%	42.9%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	41.7%	58.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	44.4%	55.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	5.0%	35.0%	45.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	5.0%	0.0%	0.0%	0.0%	
				7:00	8:00	100	1.2%	39.5%	38.3%	0.0%	2.5%	2.5%	0.0%	0.0%	0.0%	2.5%	2.5%	4.9%	4.9%	1.2%	0.0%	0.0%	
				8:00	9:00	150	1.5%	50.1%	25.0%	0.0%	3.8%	6.8%	1.5%	1.5%	0.0%	1.5%	1.5%	3.0%	3.0%	0.8%	0.0%	0.0%	
				9:00	10:00	150	0.9%	46.3%	21.3%	0.0%	4.6%	8.3%	1.9%	1.9%	0.0%	2.8%	1.9%	4.6%	4.6%	0.9%	0.0%	0.0%	
				10:00	11:00	100	2.9%	43.8%	21.7%	0.0%	8.7%	13.0%	1.4%	1.4%	0.0%	1.4%	1.4%	1.4%	2.9%	0.0%	0.0%	0.0%	
				11:00	12:00	100	1.7%	46.6%	21.7%	0.0%	11.7%	6.7%	0.0%	0.0%	0.0%	3.3%	1.7%	3.3%	3.3%	0.0%	0.0%	0.0%	
				12:00	13:00	100	3.0%	55.1%	23.9%	0.0%	7.5%	4.5%	0.0%	0.0%	0.0%	3.0%	0.0%	1.5%	1.5%	0.0%	0.0%	0.0%	
				13:00	14:00	100	1.5%	36.7%	14.7%	0.0%	8.8%	16.2%	1.5%	2.9%	0.0%	1.5%	2.9%	5.9%	5.9%	1.5%	0.0%	0.0%	
				14:00	15:00	100	1.6%	36.4%	22.2%	0.0%	9.5%	12.7%	1.6%	1.6%	0.0%	1.6%	1.6%	4.8%	4.8%	1.6%	0.0%	0.0%	
				15:00	16:00	100	2.6%	40.2%	22.1%	0.0%	7.8%	14.3%	1.3%	1.3%	0.0%	2.6%	1.3%	2.6%	2.6%	1.3%	0.0%	0.0%	
				16:00	17:00	50	9.1%	69.7%	0.0%	0.0%	12.1%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	50	8.3%	72.9%	0.0%	0.0%	6.3%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	50	9.8%	78.0%	0.0%	0.0%	4.9%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	3.9%	86.1%	0.0%	0.0%	2.0%	2.0%	0.0%	0.0%	0.0%	2.0%	2.0%	2.0%	2.0%	0.0%	0.0%	0.0%	
				20:00	21:00	50	5.4%	81.1%	10.8%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	3.3%	76.8%	13.3%	0.0%	3.3%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	4.2%	75.0%	20.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	50	4.2%	58.3%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L063	Sung Chi Street (SB)	Access Road	Hok Yeun Street	0:00	1:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	0																
				2:00	3:00	0																
				3:00	4:00	0																
				4:00	5:00	0																
				5:00	6:00	0																
				6:00	7:00	50	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	50.0%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	62.5%	12.5%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	57.1%	14.3%	0.0%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	40.0%	20.0%	0.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	33.3%	16.7%	0.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	50.0%	16.7%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	40.0%	20.0%	0.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	40.0%	20.0%	0.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	40.0%	20.0%	0.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	33.3%	33.3%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	50.0%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	50.0%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	75.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L064	Sung Chi Street (NB)	Hok Yeun Street	Bailey Street	0:00	1:00	50	21.4%	57.2%	21.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	22.2%	55.6%	22.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	25.0%	50.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	33.3%	50.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	20.0%	60.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	16.7%	50.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	18.2%	45.4%	27.3%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	15.2%	60.5%	15.2%	3.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	19.7%	57.9%	7.0%	2.8%	7.0%	2.8%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	17.5%	54.4%	8.8%	3.5%	8.8%	3.5%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	16.0%	46.0%	10.0%	6.0%	16.0%	4.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	11.1%	48.9%	8.9%	6.7%	22.2%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	12.2%	57.2%	10.2%	4.1%	14.3%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	11.9%	45.4%	7.1%	7.1%	19.0%	7.1%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	9.8%	43.9%	12.2%	7.3%	19.5%	4.9%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	13.5%	46.1%	11.5%	5.8%	15.4%	5.8%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	11.1%	53.4%	11.1%	4.4%	17.8%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	13.1%	60.7%	9.8%	3.3%	9.8%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	13.2%	64.2%	11.3%	1.9%	7.5%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	14.6%	70.8%	12.5%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	16.7%	63.8%	16.7%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	13.8%	62.1%	20.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	19.2%	53.9%	23.1%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	22.7%	50.1%	22.7%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L065	Ma Tau Wai Road (SB)	Bailey Street	Pak Kung Street	0:00	1:00	250	3.2%	20.8%	48.7%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	9.0%	0.5%	0.5%	0.9%	0.5%	0.5%	0.0%	13.6%
				1:00	2:00	150	4.0%	26.7%	66.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.2%	24.7%	69.9%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.4%	26.5%	64.7%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.3%	24.6%	68.8%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.8%	22.2%	63.8%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	1.4%	1.4%	1.4%	0.0%	0.0%
				6:00	7:00	200	2.2%	15.3%	45.6%	0.5%	3.3%	0.5%	0.0%	0.0%	9.8%	0.5%	1.1%	1.6%	1.6%	0.5%	0.0%	17.5%	
				7:00	8:00	600	0.9%	19.5%	28.4%	0.5%	3.2%	1.3%	0.2%	0.0%	11.2%	1.1%	1.1%	2.3%	2.2%	0.7%	0.0%	27.4%	
				8:00	9:00	750	1.8%	31.0%	23.2%	0.7%	6.0%	4.7%	0.4%	0.3%	6.8%	1.0%	1.0%	2.0%	1.8%	0.6%	0.0%	18.7%	
				9:00	10:00	700	1.3%	25.1%	24.7%	0.6%	5.9%	5.3%	0.4%	0.3%	6.2%	1.6%	1.3%	2.7%	2.5%	0.7%	0.0%	21.4%	
				10:00	11:00	550	2.7%	22.6%	27.0%	0.9%	11.1%	5.5%	0.7%	0.5%	6.2%	0.4%	0.5%	1.1%	0.9%	0.4%	0.0%	19.5%	
				11:00	12:00	600	1.6%	21.2%	22.4%	1.3%	13.5%	2.2%	0.2%	0.2%	11.0%	1.3%	0.7%	1.6%	1.4%	0.4%	0.0%	21.0%	
				12:00	13:00	550	2.2%	28.3%	28.4%	0.9%	9.1%	1.9%	0.2%	0.2%	8.4%	1.5%	0.4%	0.7%	0.7%	0.2%	0.0%	16.9%	
				13:00	14:00	550	1.6%	19.0%	18.8%	1.1%	11.2%	6.6%	0.7%	0.7%	8.0%	0.4%	1.5%	2.9%	2.7%	0.9%	0.0%	23.9%	
				14:00	15:00	550	1.3%	18.1%	26.3%	0.9%	10.5%	5.1%	0.6%	0.6%	8.7%	0.6%	0.9%	2.1%	1.9%	0.6%	0.0%	21.8%	
				15:00	16:00	650	2.0%	21.1%	27.3%	1.0%	10.3%	5.7%	0.7%	0.7%	6.9%	1.0%	0.7%	1.5%	1.3%	0.5%	0.0%	19.3%	
				16:00	17:00	500	3.7%	26.6%	25.3%	0.6%	9.8%	1.6%	0.2%	0.2%	8.8%	0.0%	0.4%	1.0%	0.4%	0.2%	0.0%	21.2%	
				17:00	18:00	650	4.5%	32.5%	26.2%	0.5%	6.3%	2.5%	0.5%	0.5%	7.5%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	17.7%	
				18:00	19:00	500	4.8%	36.6%	29.8%	0.4%	5.4%	1.8%	0.4%	0.4%	3.6%	0.2%	0.2%	0.6%	0.2%	0.2%	0.0%	15.4%	
				19:00	20:00	550	2.4%	34.4%	31.3%	0.2%	1.7%	0.8%	0.0%	0.0%	8.8%	0.4%	0.4%	0.8%	0.8%	0.2%	0.0%	17.8%	
				20:00	21:00	500	2.4%	26.4%	35.7%	0.2%	1.3%	0.2%	0.0%	0.0%	13.3%	0.2%	0.2%	0.2%	0.2%	0.2%	0.0%	19.5%	
				21:00	22:00	450	1.6%	22.5%	38.7%	0.2%	1.9%	0.7%	0.0%	0.0%	12.0%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	22.2%	
				22:00	23:00	450	2.0%	16.5%	37.5%	0.2%	0.9%	0.0%	0.0%	0.0%	12.2%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	29.4%	
				L066	Ma Tau Wai Road (NB)	Pak Kung Street	Bailey Street	23:00	0:00	400	2.2%	16.0%	37.2%	0.3%	1.1%	0.0%	0.0%	0.0%	12.0%	0.3%	0.3%	0.6%	0.3%
0:00	1:00	200	2.8%					18.1%	49.5%	0.0%	1.7%	0.0%	0.0%	0.0%	11.9%	0.6%	0.6%	1.7%	0.6%	0.6%	0.0%	11.9%	
1:00	2:00	100	3.8%					24.4%	69.2%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	100	2.7%					21.9%	72.7%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	100	3.8%					24.5%	67.9%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	4.2%					22.9%	70.8%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	100	3.4%					18.6%	62.7%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.4%	1.7%	1.7%	0.0%	0.0%	
6:00	7:00	200	1.9%					12.8%	43.6%	0.6%	3.2%	1.3%	0.0%	0.0%	12.2%	1.3%	1.3%	3.2%	2.6%	1.3%	0.0%	14.7%	
7:00	8:00	450	0.7%					12.7%	29.5%	0.5%	2.9%	1.0%	0.2%	0.2%	13.2%	1.7%	1.7%	3.6%	3.1%	1.0%	0.0%	28.0%	
8:00	9:00	550	1.6%					21.7%	26.0%	0.8%	5.6%	4.0%	0.6%	0.6%	8.6%	1.6%	1.6%	3.2%	2.8%	0.8%	0.0%	20.5%	
9:00	10:00	500	1.2%					16.6%	26.8%	0.8%	5.3%	4.3%	0.6%	0.6%	7.5%	2.6%	2.0%	4.1%	3.9%	1.0%	0.0%	22.7%	
10:00	11:00	500	2.4%					19.1%	26.0%	1.5%	11.5%	7.2%	0.7%	0.7%	8.0%	0.9%	0.9%	2.0%	1.7%	0.7%	0.0%	16.7%	
11:00	12:00	500	1.5%					17.6%	21.2%	1.9%	13.9%	2.5%	0.2%	0.2%	13.9%	2.1%	1.3%	2.5%	2.5%	0.8%	0.0%	17.9%	
12:00	13:00	450	2.0%					24.4%	28.0%	1.3%	9.6%	2.5%	0.2%	0.2%	11.0%	2.5%	0.7%	1.1%	1.3%	0.4%	0.0%	14.8%	
13:00	14:00	500	1.5%					15.4%	17.2%	1.5%	11.0%	8.1%	0.8%	0.8%	9.8%	0.6%	2.5%	4.8%	4.8%	1.7%	0.0%	19.5%	
14:00	15:00	500	1.1%					15.0%	24.4%	1.5%	10.6%	6.5%	0.7%	0.7%	10.8%	1.1%	1.5%	3.5%	3.3%	1.1%	0.0%	18.2%	
15:00	16:00	550	1.7%					17.8%	26.0%	1.4%	10.6%	7.1%	0.8%	0.8%	8.7%	1.7%	1.2%	2.5%	2.3%	0.8%	0.0%	16.6%	
16:00	17:00	500	2.9%					26.0%	21.1%	1.4%	11.1%	3.5%	0.2%	0.2%	11.5%	0.2%	1.2%	2.5%	2.9%	1.0%	0.0%	14.3%	
17:00	18:00	600	3.7%					32.5%	22.2%	1.0%	7.3%	5.6%	0.3%	0.3%	10.0%	0.7%	0.7%	1.4%	1.4%	0.7%	0.0%	12.2%	
18:00	19:00	500	4.1%					36.9%	25.7%	0.6%	6.2%	3.9%	0.2%	0.2%	5.0%	0.6%	1.0%	1.9%	2.1%	0.8%	0.0%	10.8%	
19:00	20:00	450	2.3%					30.6%	31.3%	0.2%	1.9%	1.2%	0.0%	0.0%	11.8%	0.7%	0.7%	1.4%	1.4%	0.5%	0.0%	16.0%	
20:00	21:00	400	2.1%					23.3%	35.5%	0.3%	1.3%	0.5%	0.0%	0.0%	17.8%	0.3%	0.3%	0.5%	0.3%	0.3%	0.0%	17.5%	
21:00	22:00	350	1.4%					20.1%	38.6%	0.3%	2.0%	0.9%	0.0%	0.0%	16.3%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	20.1%	
22:00	23:00	400	1.9%					14.5%	37.0%	0.3%	1.1%	0.0%	0.0%	0.0%	16.4%	0.6%	0.3%	0.8%	0.3%	0.3%	0.0%	26.5%	
23:00	0:00	300	2.1%	14.1%	36.8%	0.3%	1.4%	0.0%	0.0%	0.0%	16.2%	0.7%	0.3%	1.0%	0.3%	0.3%	0.0%	26.5%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L067	Ma Tau Wai Road (SB)	Pak Kung Street	Hok Yuen Street	0:00	1:00	250	3.1%	21.1%	49.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.4%	0.4%	0.9%	0.4%	0.4%	0.0%	13.5%
				1:00	2:00	150	3.9%	27.5%	65.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.2%	24.5%	70.2%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.3%	27.1%	64.3%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.3%	24.6%	68.8%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.7%	21.9%	64.3%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	1.4%	1.4%	1.4%	0.0%	0.0%
				6:00	7:00	200	2.2%	15.6%	45.8%	0.5%	3.2%	0.5%	0.0%	0.0%	9.7%	0.5%	1.1%	1.6%	1.6%	0.5%	0.0%	17.2%
				7:00	8:00	600	0.9%	19.9%	28.4%	0.5%	3.2%	1.3%	0.2%	0.0%	11.1%	1.1%	1.1%	2.3%	2.1%	0.7%	0.0%	27.2%
				8:00	9:00	750	1.9%	31.6%	23.0%	0.8%	5.9%	4.7%	0.4%	0.3%	6.7%	1.0%	1.0%	1.9%	1.8%	0.6%	0.0%	18.4%
				9:00	10:00	700	1.5%	25.4%	24.6%	0.7%	5.8%	5.3%	0.4%	0.3%	6.1%	1.6%	1.3%	2.6%	2.5%	0.7%	0.0%	21.2%
				10:00	11:00	600	2.7%	23.0%	26.8%	1.1%	11.0%	5.6%	0.7%	0.5%	6.1%	0.4%	0.5%	1.1%	0.9%	0.4%	0.0%	19.2%
				11:00	12:00	600	1.8%	21.6%	22.4%	1.4%	13.5%	2.1%	0.2%	0.2%	10.8%	1.2%	0.7%	1.6%	1.4%	0.4%	0.0%	20.7%
				12:00	13:00	550	2.2%	28.8%	28.4%	0.9%	9.0%	1.8%	0.2%	0.2%	8.3%	1.5%	0.4%	0.7%	0.7%	0.2%	0.0%	16.7%
				13:00	14:00	600	1.6%	19.3%	18.8%	1.3%	11.2%	6.7%	0.7%	0.7%	7.9%	0.4%	1.4%	2.9%	2.7%	0.9%	0.0%	23.5%
				14:00	15:00	550	1.3%	18.4%	26.2%	1.1%	10.6%	5.2%	0.6%	0.6%	8.5%	0.6%	0.9%	2.0%	1.9%	0.6%	0.0%	21.5%
				15:00	16:00	650	2.1%	21.5%	27.4%	1.1%	10.2%	5.7%	0.6%	0.6%	6.8%	1.0%	0.6%	1.5%	1.3%	0.5%	0.0%	19.1%
				16:00	17:00	500	3.6%	27.2%	25.4%	0.6%	9.8%	1.6%	0.2%	0.2%	8.6%	0.0%	0.4%	1.0%	0.4%	0.2%	0.0%	20.8%
				17:00	18:00	650	4.4%	33.1%	26.1%	0.5%	6.3%	2.6%	0.5%	0.5%	7.3%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	17.4%
				18:00	19:00	550	4.7%	37.2%	29.8%	0.4%	5.3%	1.8%	0.4%	0.4%	3.5%	0.2%	0.2%	0.6%	0.2%	0.2%	0.0%	15.1%
				19:00	20:00	550	2.4%	35.1%	31.2%	0.2%	1.7%	0.7%	0.0%	0.0%	8.7%	0.4%	0.4%	0.7%	0.7%	0.2%	0.0%	17.6%
				20:00	21:00	500	2.3%	26.9%	35.7%	0.2%	1.3%	0.2%	0.0%	0.0%	13.1%	0.2%	0.2%	0.2%	0.2%	0.2%	0.0%	19.3%
				21:00	22:00	450	1.6%	22.9%	38.7%	0.2%	1.8%	0.7%	0.0%	0.0%	11.9%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	22.0%
				22:00	23:00	450	2.2%	17.0%	37.4%	0.2%	0.9%	0.0%	0.0%	0.0%	12.1%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	29.0%
				23:00	0:00	400	2.2%	16.3%	37.3%	0.3%	1.1%	0.0%	0.0%	0.0%	11.9%	0.3%	0.3%	0.6%	0.3%	0.3%	0.0%	29.1%
L068	Ma Tau Wai Road (NB)	Hok Yuen Street	Pak Kung Street	0:00	1:00	400	3.7%	21.2%	54.1%	0.0%	1.4%	0.0%	0.0%	0.0%	10.5%	0.6%	0.6%	1.1%	0.6%	0.3%	0.0%	5.9%
				1:00	2:00	200	4.6%	25.3%	67.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	3.7%	22.8%	71.6%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	5.1%	25.4%	66.1%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	4.7%	22.6%	69.9%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	4.0%	20.8%	64.8%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	2.4%	1.6%	0.8%	0.0%	0.0%
				6:00	7:00	300	2.7%	15.7%	50.2%	0.3%	2.7%	0.7%	0.0%	0.0%	11.6%	1.0%	1.4%	2.4%	2.4%	0.7%	0.0%	8.2%
				7:00	8:00	750	1.4%	18.9%	33.5%	0.4%	3.2%	1.0%	0.3%	0.3%	16.2%	1.4%	1.3%	2.7%	2.7%	1.0%	0.0%	15.7%
				8:00	9:00	950	3.1%	29.8%	27.2%	0.6%	5.9%	3.5%	1.0%	0.9%	9.7%	1.2%	1.2%	2.2%	2.2%	0.9%	0.0%	10.6%
				9:00	10:00	900	2.3%	24.0%	29.4%	0.6%	5.9%	4.0%	1.0%	0.9%	9.0%	1.9%	1.5%	3.0%	3.2%	1.1%	0.0%	12.2%
				10:00	11:00	850	3.5%	23.8%	30.8%	1.2%	10.3%	5.4%	1.3%	1.2%	7.9%	0.7%	0.8%	1.7%	1.7%	0.5%	0.0%	9.2%
				11:00	12:00	900	2.3%	22.6%	25.7%	1.5%	12.5%	2.0%	0.5%	0.4%	13.9%	2.0%	1.2%	2.2%	2.5%	0.6%	0.0%	10.1%
				12:00	13:00	850	2.9%	29.6%	31.9%	1.1%	8.3%	1.8%	0.5%	0.4%	10.4%	2.3%	0.5%	1.0%	1.1%	0.2%	0.0%	8.0%
				13:00	14:00	850	2.1%	20.3%	21.6%	1.3%	10.4%	6.4%	1.6%	1.6%	10.1%	0.6%	2.3%	4.3%	4.7%	1.2%	0.0%	11.5%
				14:00	15:00	850	1.8%	19.1%	30.0%	1.2%	9.9%	5.0%	1.2%	1.1%	11.0%	1.0%	1.5%	2.9%	3.0%	0.9%	0.0%	10.4%
				15:00	16:00	950	2.7%	22.1%	30.9%	1.2%	9.5%	5.5%	1.4%	1.3%	8.5%	1.6%	1.2%	2.1%	2.2%	0.6%	0.0%	9.2%
				16:00	17:00	950	4.0%	29.9%	27.6%	1.1%	8.5%	2.3%	0.4%	0.4%	9.6%	0.2%	1.5%	2.6%	3.1%	0.4%	0.0%	8.4%
				17:00	18:00	1,150	4.8%	36.1%	28.0%	0.8%	5.4%	3.6%	0.8%	0.8%	8.1%	0.8%	0.8%	1.4%	1.4%	0.3%	0.0%	6.9%
				18:00	19:00	1,000	5.0%	39.6%	31.2%	0.5%	4.5%	2.4%	0.5%	0.6%	3.8%	0.7%	1.1%	1.9%	2.1%	0.3%	0.0%	5.8%
				19:00	20:00	850	3.2%	36.1%	35.1%	0.1%	1.6%	0.8%	0.1%	0.2%	10.9%	0.6%	0.5%	1.1%	1.1%	0.2%	0.0%	8.4%
				20:00	21:00	750	3.2%	27.7%	40.2%	0.1%	1.2%	0.3%	0.1%	0.1%	16.5%	0.3%	0.3%	0.4%	0.3%	0.1%	0.0%	9.2%
				21:00	22:00	700	2.3%	24.0%	44.1%	0.2%	1.7%	0.6%	0.2%	0.2%	15.4%	0.0%	0.2%	0.2%	0.0%	0.2%	0.0%	10.7%
				22:00	23:00	700	3.1%	18.4%	44.6%	0.2%	0.9%	0.0%	0.0%	0.0%	16.1%	0.5%	0.3%	0.6%	0.3%	0.3%	0.0%	14.7%
				23:00	0:00	550	3.2%	17.6%	44.4%	0.2%	1.1%	0.0%	0.0%	0.0%	16.1%	0.6%	0.4%	0.8%	0.4%	0.4%	0.0%	14.8%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L069	Hok Yeun Street (EB)	Ma Tau Wai Road	Sung Chi Street	0:00	1:00	200	4.2%	29.5%	59.1%	0.0%	1.8%	0.0%	0.0%	0.0%	1.2%	0.6%	0.6%	1.8%	0.6%	0.6%	0.0%	0.0%
				1:00	2:00	100	4.2%	30.2%	62.5%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.4%	27.3%	67.0%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.6%	30.8%	61.5%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.5%	28.1%	66.6%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.9%	24.3%	60.0%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.9%	1.4%	1.4%	0.0%	0.0%
				6:00	7:00	150	3.0%	22.4%	56.7%	0.0%	3.7%	1.5%	0.0%	0.0%	1.5%	1.5%	1.5%	3.7%	3.0%	1.5%	0.0%	0.0%
				7:00	8:00	350	2.0%	32.4%	35.4%	0.6%	4.4%	2.0%	0.3%	0.3%	2.3%	3.2%	2.9%	6.1%	5.8%	2.3%	0.0%	0.0%
				8:00	9:00	550	3.5%	42.1%	23.2%	0.5%	6.8%	6.8%	0.9%	0.9%	1.1%	2.2%	2.2%	4.2%	4.0%	1.6%	0.0%	0.0%
				9:00	10:00	550	2.5%	33.9%	25.1%	0.6%	6.9%	7.6%	1.0%	1.0%	1.0%	3.7%	2.9%	5.7%	5.7%	2.4%	0.0%	0.0%
				10:00	11:00	450	3.5%	31.3%	31.7%	0.9%	13.2%	8.9%	1.2%	1.4%	0.9%	0.9%	1.2%	2.1%	2.1%	0.7%	0.0%	0.0%
				11:00	12:00	400	2.5%	31.9%	28.5%	1.3%	17.4%	3.8%	0.5%	0.5%	2.0%	2.8%	1.5%	3.0%	3.3%	1.0%	0.0%	0.0%
				12:00	13:00	450	2.9%	39.6%	33.5%	1.0%	10.8%	3.1%	0.5%	0.5%	1.4%	2.9%	0.7%	1.2%	1.4%	0.5%	0.0%	0.0%
				13:00	14:00	450	2.2%	27.2%	22.6%	1.0%	13.6%	11.2%	1.5%	1.7%	1.5%	0.7%	3.2%	5.6%	5.8%	2.2%	0.0%	0.0%
				14:00	15:00	400	1.8%	26.3%	32.1%	1.0%	13.1%	8.8%	1.3%	1.3%	1.5%	1.3%	2.0%	4.0%	4.0%	1.5%	0.0%	0.0%
				15:00	16:00	500	2.7%	29.3%	31.9%	0.8%	12.2%	9.3%	1.3%	1.3%	1.1%	2.1%	1.5%	2.7%	2.7%	1.1%	0.0%	0.0%
				16:00	17:00	400	3.7%	39.8%	35.3%	0.8%	12.8%	3.5%	0.5%	0.5%	1.6%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	500	4.4%	45.2%	33.6%	0.6%	7.6%	5.0%	0.8%	0.8%	1.2%	0.2%	0.2%	0.2%	0.2%	0.0%	0.0%	0.0%
				18:00	19:00	450	4.6%	47.1%	36.1%	0.5%	6.2%	3.2%	0.5%	0.5%	0.5%	0.2%	0.2%	0.2%	0.2%	0.0%	0.0%	0.0%
				19:00	20:00	450	3.2%	49.2%	37.3%	0.0%	2.0%	1.2%	0.2%	0.2%	1.5%	1.0%	0.7%	1.5%	1.5%	0.5%	0.0%	0.0%
				20:00	21:00	350	3.5%	41.8%	47.3%	0.0%	1.9%	0.6%	0.3%	0.3%	2.5%	0.3%	0.3%	0.6%	0.3%	0.3%	0.0%	0.0%
				21:00	22:00	300	2.5%	36.8%	53.1%	0.0%	2.5%	1.4%	0.4%	0.4%	2.5%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	300	3.9%	30.7%	57.9%	0.0%	1.6%	0.0%	0.0%	0.0%	2.7%	0.8%	0.4%	1.2%	0.4%	0.4%	0.0%	0.0%
				23:00	0:00	250	3.8%	29.7%	57.8%	0.0%	1.9%	0.0%	0.0%	0.0%	2.9%	1.0%	0.5%	1.4%	0.5%	0.5%	0.0%	0.0%
L070	Hok Yeun Street (EB)	Sung Chi Street	Sung On Street	0:00	1:00	150	3.3%	28.0%	60.6%	0.0%	2.0%	0.0%	0.0%	0.0%	1.3%	0.7%	0.7%	2.0%	0.7%	0.7%	0.0%	0.0%
				1:00	2:00	100	3.5%	29.1%	65.1%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	2.5%	26.3%	68.7%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.4%	29.3%	63.9%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.8%	26.9%	67.4%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.1%	23.1%	60.1%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	3.1%	1.5%	1.5%	0.0%	0.0%
				6:00	7:00	150	2.4%	21.6%	56.8%	0.0%	4.0%	1.6%	0.0%	0.0%	1.6%	1.6%	1.6%	4.0%	3.2%	1.6%	0.0%	0.0%
				7:00	8:00	350	1.6%	31.0%	35.8%	0.3%	4.8%	2.3%	0.3%	0.3%	2.6%	3.2%	2.9%	6.5%	6.1%	2.3%	0.0%	0.0%
				8:00	9:00	500	3.1%	40.9%	23.9%	0.4%	7.2%	6.7%	1.0%	0.8%	1.2%	2.2%	2.2%	4.5%	4.3%	1.6%	0.0%	0.0%
				9:00	10:00	500	2.4%	32.7%	25.7%	0.4%	7.2%	7.6%	1.1%	0.9%	1.1%	3.7%	2.8%	6.1%	6.1%	2.2%	0.0%	0.0%
				10:00	11:00	400	3.1%	30.3%	32.8%	0.8%	13.8%	8.9%	1.3%	1.0%	1.0%	1.0%	1.0%	2.1%	2.1%	0.8%	0.0%	0.0%
				11:00	12:00	400	2.2%	30.6%	29.3%	0.8%	18.2%	3.6%	0.6%	0.3%	2.2%	2.8%	1.7%	3.3%	3.3%	1.1%	0.0%	0.0%
				12:00	13:00	400	2.4%	38.6%	34.6%	0.5%	11.5%	2.9%	0.5%	0.3%	1.6%	2.9%	0.8%	1.3%	1.6%	0.5%	0.0%	0.0%
				13:00	14:00	400	1.9%	26.1%	23.4%	0.8%	14.2%	10.8%	1.6%	1.3%	1.6%	0.8%	3.2%	5.9%	6.2%	2.2%	0.0%	0.0%
				14:00	15:00	400	1.7%	25.1%	33.2%	0.8%	13.6%	8.6%	1.1%	1.1%	1.7%	1.4%	1.9%	4.2%	4.2%	1.4%	0.0%	0.0%
				15:00	16:00	450	2.3%	28.2%	32.9%	0.7%	12.8%	9.1%	1.4%	1.2%	1.2%	2.1%	1.4%	3.0%	2.8%	0.9%	0.0%	0.0%
				16:00	17:00	350	3.3%	38.4%	36.8%	0.6%	13.4%	3.3%	0.3%	0.3%	1.8%	0.0%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%
				17:00	18:00	450	3.8%	44.1%	35.2%	0.4%	8.1%	4.9%	0.7%	0.7%	1.3%	0.2%	0.2%	0.2%	0.2%	0.0%	0.0%	0.0%
				18:00	19:00	400	3.8%	46.0%	37.6%	0.3%	6.3%	3.3%	0.5%	0.5%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%
				19:00	20:00	400	2.8%	47.5%	38.8%	0.0%	2.2%	1.4%	0.0%	0.3%	1.7%	0.8%	0.8%	1.4%	1.7%	0.6%	0.0%	0.0%
				20:00	21:00	300	3.1%	40.6%	49.2%	0.0%	1.7%	0.7%	0.0%	0.0%	2.8%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	0.0%
				21:00	22:00	300	2.3%	35.5%	55.2%	0.0%	2.7%	1.2%	0.0%	0.0%	2.7%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	3.4%	29.4%	59.1%	0.0%	1.7%	0.0%	0.0%	0.0%	3.0%	0.9%	0.4%	1.3%	0.4%	0.4%	0.0%	0.0%
				23:00	0:00	200	3.6%	28.1%	59.0%	0.0%	2.1%	0.0%	0.0%	0.0%	3.1%	1.0%	0.5%	1.6%	0.5%	0.5%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L071	Hok Yeun Street East (EB)	Sung On Street	Hung Hom Road	0:00	1:00	100	2.3%	24.4%	68.6%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	1.2%	0.0%	0.0%
				1:00	2:00	50	2.0%	24.0%	72.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.1%	21.3%	74.5%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	2.9%	23.5%	70.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.1%	21.9%	71.9%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.8%	19.4%	69.4%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	0.0%	2.8%	0.0%	0.0%
				6:00	7:00	100	1.5%	19.4%	67.1%	0.0%	3.0%	1.5%	0.0%	0.0%	0.0%	1.5%	1.5%	1.5%	1.5%	1.5%	0.0%	0.0%	
				7:00	8:00	150	1.4%	32.9%	44.4%	0.7%	5.0%	1.4%	0.7%	0.0%	0.0%	2.1%	2.1%	3.6%	3.6%	2.1%	0.0%	0.0%	
				8:00	9:00	250	2.3%	42.5%	29.5%	0.5%	8.2%	5.5%	1.4%	0.9%	0.0%	1.4%	1.4%	2.7%	2.3%	1.4%	0.0%	0.0%	
				9:00	10:00	250	2.0%	34.6%	31.9%	0.5%	8.3%	6.4%	1.5%	1.0%	0.0%	2.5%	2.0%	3.9%	3.4%	2.0%	0.0%	0.0%	
				10:00	11:00	200	2.6%	28.6%	40.9%	0.5%	13.3%	6.1%	2.0%	1.5%	0.0%	0.5%	0.5%	1.5%	1.0%	1.0%	0.0%	0.0%	
				11:00	12:00	200	1.7%	30.3%	37.6%	1.1%	18.0%	2.8%	0.6%	0.6%	0.0%	1.7%	0.6%	2.2%	1.7%	1.1%	0.0%	0.0%	
				12:00	13:00	200	2.1%	35.9%	43.4%	0.5%	10.9%	2.1%	0.5%	0.5%	0.0%	1.6%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%	
				13:00	14:00	200	1.7%	26.6%	31.4%	0.6%	14.7%	8.5%	2.3%	2.3%	0.0%	0.6%	1.7%	4.0%	2.8%	2.8%	0.0%	0.0%	
				14:00	15:00	200	1.7%	24.6%	42.3%	0.6%	13.4%	6.1%	1.7%	1.7%	0.0%	0.6%	1.1%	2.8%	1.7%	1.7%	0.0%	0.0%	
				15:00	16:00	250	1.8%	27.2%	41.6%	0.5%	12.4%	6.5%	1.8%	1.8%	0.0%	0.9%	0.9%	1.8%	1.4%	1.4%	0.0%	0.0%	
				16:00	17:00	200	3.1%	34.0%	45.7%	0.5%	11.0%	2.1%	0.5%	0.5%	0.0%	0.0%	0.0%	1.0%	0.0%	1.6%	0.0%	0.0%	
				17:00	18:00	300	3.6%	38.9%	43.6%	0.4%	6.7%	3.2%	1.2%	1.2%	0.0%	0.0%	0.0%	0.4%	0.0%	0.8%	0.0%	0.0%	
				18:00	19:00	250	3.5%	39.9%	45.6%	0.4%	5.3%	2.2%	0.9%	0.9%	0.0%	0.0%	0.0%	0.4%	0.0%	0.9%	0.0%	0.0%	
				19:00	20:00	200	2.6%	44.0%	47.3%	0.0%	2.1%	1.0%	0.0%	0.0%	0.0%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%	
				20:00	21:00	200	2.6%	35.9%	57.8%	0.0%	1.9%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%	
				21:00	22:00	150	2.1%	31.2%	63.9%	0.0%	2.1%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	150	2.3%	25.6%	68.9%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.0%	0.0%	
L072	Hok Yeun Street East (WB)	Hung Hom Road	Sung On Street	23:00	0:00	150	2.9%	24.8%	68.4%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%	
				0:00	1:00	100	2.9%	29.0%	61.1%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	1.4%	1.4%	1.4%	0.0%	0.0%	
				1:00	2:00	50	2.5%	30.0%	65.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.7%	27.0%	67.6%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	3.7%	29.6%	63.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	4.3%	26.1%	69.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	3.3%	23.3%	60.2%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	3.3%	3.3%	0.0%	0.0%	
				6:00	7:00	100	1.8%	21.8%	58.4%	0.0%	3.6%	1.8%	0.0%	0.0%	0.0%	1.8%	1.8%	3.6%	3.6%	1.8%	0.0%	0.0%	
				7:00	8:00	150	0.7%	22.6%	46.9%	0.0%	2.2%	1.5%	0.7%	0.7%	0.0%	3.6%	3.6%	7.3%	7.3%	2.9%	0.0%	0.0%	
				8:00	9:00	200	1.5%	31.8%	34.0%	0.0%	4.0%	4.5%	2.5%	2.5%	0.0%	3.0%	3.0%	5.6%	5.6%	2.0%	0.0%	0.0%	
				9:00	10:00	200	1.0%	24.4%	34.6%	0.0%	4.1%	4.7%	2.6%	2.6%	0.0%	4.7%	3.6%	7.3%	7.8%	2.6%	0.0%	0.0%	
				10:00	11:00	200	2.4%	31.8%	33.2%	0.6%	11.2%	6.5%	2.9%	2.4%	0.0%	1.2%	1.2%	2.4%	2.4%	1.8%	0.0%	0.0%	
				11:00	12:00	200	1.9%	32.7%	31.2%	0.6%	14.6%	2.5%	1.3%	0.6%	0.0%	3.2%	1.9%	3.8%	3.8%	1.9%	0.0%	0.0%	
				12:00	13:00	200	1.8%	40.1%	35.8%	0.6%	9.1%	2.4%	1.2%	0.6%	0.0%	3.6%	0.6%	1.8%	1.8%	0.6%	0.0%	0.0%	
				13:00	14:00	200	1.2%	26.5%	23.5%	0.6%	11.2%	7.6%	3.5%	2.9%	0.0%	1.2%	3.5%	7.1%	7.1%	4.1%	0.0%	0.0%	
				14:00	15:00	200	1.2%	25.9%	33.9%	0.6%	10.5%	6.2%	2.5%	2.5%	0.0%	1.9%	2.5%	4.9%	4.9%	2.5%	0.0%	0.0%	
				15:00	16:00	200	1.6%	29.0%	33.7%	0.5%	9.8%	6.7%	3.1%	2.6%	0.0%	2.6%	1.6%	3.6%	3.1%	2.1%	0.0%	0.0%	
				16:00	17:00	200	3.0%	48.0%	27.9%	0.6%	12.7%	3.0%	0.6%	0.6%	0.0%	0.0%	0.0%	1.2%	0.0%	2.4%	0.0%	0.0%	
				17:00	18:00	250	3.6%	54.0%	26.5%	0.4%	7.6%	4.0%	1.3%	0.9%	0.0%	0.0%	0.0%	0.4%	0.0%	1.3%	0.0%	0.0%	
				18:00	19:00	200	3.5%	55.9%	28.1%	0.5%	6.0%	2.5%	1.0%	0.5%	0.0%	0.0%	0.0%	0.5%	0.0%	1.5%	0.0%	0.0%	
				19:00	20:00	200	2.4%	48.6%	39.4%	0.0%	1.8%	1.2%	0.0%	0.6%	0.0%	1.2%	0.6%	1.8%	1.8%	0.6%	0.0%	0.0%	
				20:00	21:00	150	2.3%	42.2%	49.9%	0.0%	1.6%	0.8%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.8%	0.8%	0.0%	0.0%	
				21:00	22:00	150	1.8%	37.5%	58.0%	0.0%	1.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	150	2.9%	30.5%	60.6%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%					
23:00	0:00	100	2.4%	29.4%	61.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L073	Hok Yuen Street East (WB)	Sung Ping Street	Hung Hom Road	0:00	1:00	100	2.8%	20.8%	75.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	2.2%	20.0%	75.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.4%	16.7%	78.5%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	3.3%	20.0%	73.4%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.7%	18.5%	77.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	3.3%	16.7%	76.7%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	1.7%	15.3%	71.1%	0.0%	3.4%	1.7%	0.0%	0.0%	0.0%	1.7%	1.7%	1.7%	1.7%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	150	0.0%	8.0%	75.9%	0.0%	4.0%	1.3%	0.7%	0.7%	0.0%	2.0%	2.0%	2.7%	2.7%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	200	0.0%	12.7%	63.5%	0.0%	7.4%	5.8%	1.6%	1.6%	0.0%	1.6%	1.6%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	200	0.0%	9.5%	64.2%	0.0%	6.9%	6.3%	1.6%	1.6%	0.0%	2.6%	2.1%	2.6%	2.6%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	200	2.5%	25.2%	45.3%	0.6%	11.7%	6.7%	2.5%	2.5%	0.0%	0.6%	0.6%	0.6%	1.2%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	150	2.1%	27.3%	44.0%	0.7%	16.1%	2.8%	0.7%	0.7%	0.0%	2.1%	0.7%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	200	1.9%	31.6%	49.0%	0.6%	9.5%	2.5%	0.6%	0.6%	0.0%	1.9%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	150	1.4%	23.9%	36.7%	0.7%	13.4%	9.2%	2.8%	3.5%	0.0%	0.7%	2.1%	2.8%	2.8%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	150	1.4%	21.6%	47.9%	0.7%	11.5%	6.8%	2.0%	2.0%	0.0%	0.7%	1.4%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	200	1.7%	23.6%	47.3%	0.6%	10.7%	7.3%	2.2%	2.2%	0.0%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	150	5.1%	54.9%	24.8%	0.7%	10.2%	2.9%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	200	5.7%	59.0%	22.3%	0.5%	5.7%	3.6%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	200	5.7%	61.0%	23.6%	0.6%	4.6%	2.3%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	200	2.5%	38.0%	53.3%	0.0%	1.9%	1.3%	0.0%	0.6%	0.0%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	150	2.3%	31.0%	64.3%	0.0%	1.6%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	1.7%	26.4%	69.4%	0.0%	1.7%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	2.7%	21.6%	74.8%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	100	2.2%	21.3%	75.4%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L074	Hok Cheung Street (NB)	Hok Yuen Street East	Man Yue Street	0:00	1:00	100	3.2%	30.2%	65.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	2.6%	28.9%	65.9%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.9%	25.7%	71.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	3.7%	29.6%	63.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	4.3%	26.1%	69.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	3.8%	26.9%	65.5%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	2.2%	26.7%	68.9%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	150	0.9%	25.0%	63.0%	0.0%	3.4%	0.9%	0.0%	0.0%	0.0%	1.7%	1.7%	1.7%	1.7%	0.0%	0.0%	0.0%	
				8:00	9:00	200	1.2%	36.1%	46.5%	0.0%	6.0%	3.6%	1.2%	0.6%	0.0%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%	0.0%	
				9:00	10:00	200	0.7%	29.4%	50.8%	0.0%	5.9%	3.9%	1.3%	0.7%	0.0%	2.0%	1.3%	2.0%	2.0%	0.0%	0.0%	0.0%	
				10:00	11:00	150	2.9%	37.4%	40.4%	0.0%	9.4%	4.3%	1.4%	1.4%	0.0%	0.7%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	
				11:00	12:00	150	2.4%	39.6%	37.9%	0.0%	12.9%	1.6%	0.8%	0.8%	0.0%	1.6%	0.8%	0.8%	0.8%	0.0%	0.0%	0.0%	
				12:00	13:00	150	2.1%	45.2%	40.8%	0.0%	7.0%	1.4%	0.7%	0.7%	0.0%	1.4%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	
				13:00	14:00	150	2.6%	36.7%	33.3%	0.0%	11.1%	6.0%	2.6%	1.7%	0.0%	0.9%	1.7%	1.7%	1.7%	0.0%	0.0%	0.0%	
				14:00	15:00	150	1.7%	33.1%	43.7%	0.0%	9.9%	5.0%	1.7%	1.7%	0.0%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%	0.0%	
				15:00	16:00	200	2.6%	35.8%	41.7%	0.0%	8.6%	4.6%	2.0%	1.3%	0.0%	1.3%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	
				16:00	17:00	150	5.3%	58.3%	26.5%	0.0%	6.8%	1.5%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	200	5.4%	62.2%	24.2%	0.0%	3.8%	2.2%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	200	5.4%	64.1%	25.1%	0.0%	3.0%	1.2%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	150	2.7%	51.6%	42.3%	0.0%	1.3%	0.7%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	
				20:00	21:00	150	2.5%	44.1%	52.6%	0.0%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	150	1.8%	37.6%	57.9%	0.0%	1.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	100	3.1%	32.0%	63.9%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	100	3.8%	30.8%	64.1%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L075	Hung Hom Road (SB)	Hok Yuen Street East	Man Yue Street	0:00	1:00	350	2.6%	25.8%	62.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	4.1%	0.3%	0.3%	0.9%	0.6%	0.3%	0.0%	0.0%
				1:00	2:00	200	2.5%	26.8%	67.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	200	2.2%	24.0%	71.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	3.0%	27.1%	66.1%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	150	2.5%	24.4%	70.6%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.2%	22.3%	66.3%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.7%	1.4%	1.4%	0.7%	0.0%	0.0%
				6:00	7:00	300	1.8%	20.5%	61.3%	0.4%	4.4%	0.7%	0.0%	0.0%	4.8%	0.7%	1.1%	1.8%	1.8%	0.7%	0.0%	0.0%	
				7:00	8:00	700	0.9%	25.8%	52.5%	0.4%	5.2%	1.3%	0.3%	0.3%	5.5%	1.2%	1.2%	2.2%	2.2%	0.7%	0.0%	0.3%	
				8:00	9:00	1,050	1.8%	36.2%	37.9%	0.5%	8.4%	4.3%	1.2%	1.1%	2.9%	0.9%	0.9%	1.6%	1.6%	0.5%	0.0%	0.2%	
				9:00	10:00	950	1.4%	29.7%	41.7%	0.4%	8.7%	4.9%	1.3%	1.2%	2.8%	1.5%	1.2%	2.1%	2.2%	0.7%	0.0%	0.2%	
				10:00	11:00	850	2.3%	29.7%	36.4%	0.7%	15.4%	5.5%	1.5%	1.5%	3.1%	0.5%	0.6%	1.1%	1.1%	0.5%	0.0%	0.1%	
				11:00	12:00	800	1.7%	29.7%	32.1%	1.0%	20.0%	2.2%	0.6%	0.5%	5.6%	1.5%	0.9%	1.7%	1.8%	0.6%	0.0%	0.1%	
				12:00	13:00	850	1.8%	36.7%	38.0%	0.6%	12.5%	1.8%	0.6%	0.5%	3.9%	1.6%	0.4%	0.6%	0.7%	0.2%	0.0%	0.1%	
				13:00	14:00	800	1.6%	27.2%	27.5%	0.9%	16.9%	7.2%	2.0%	1.9%	4.3%	0.5%	1.9%	3.2%	3.6%	1.2%	0.0%	0.1%	
				14:00	15:00	800	1.3%	25.1%	37.3%	0.8%	15.6%	5.4%	1.5%	1.3%	4.4%	0.8%	1.2%	2.1%	2.3%	0.8%	0.0%	0.1%	
				15:00	16:00	950	1.8%	28.0%	36.8%	0.8%	14.4%	5.8%	1.7%	1.4%	3.3%	1.2%	0.9%	1.6%	1.6%	0.6%	0.0%	0.1%	
				16:00	17:00	750	3.1%	40.6%	27.7%	0.7%	14.0%	2.0%	0.5%	0.5%	4.8%	0.1%	1.1%	1.9%	2.1%	0.9%	0.0%	0.0%	
				17:00	18:00	1,000	3.6%	48.0%	27.4%	0.4%	8.6%	3.0%	0.9%	0.8%	4.0%	0.5%	0.5%	0.9%	0.9%	0.5%	0.0%	0.0%	
				18:00	19:00	850	3.7%	50.4%	29.5%	0.2%	6.9%	2.0%	0.6%	0.6%	1.8%	0.5%	0.7%	1.2%	1.3%	0.6%	0.0%	0.0%	
				19:00	20:00	800	2.1%	45.1%	41.9%	0.1%	2.4%	0.8%	0.1%	0.3%	4.3%	0.5%	0.4%	0.8%	0.8%	0.3%	0.0%	0.1%	
				20:00	21:00	700	2.1%	36.7%	50.1%	0.2%	2.0%	0.3%	0.2%	0.2%	6.8%	0.2%	0.2%	0.3%	0.3%	0.2%	0.0%	0.2%	
				21:00	22:00	600	1.7%	31.8%	55.6%	0.2%	2.7%	0.7%	0.2%	0.2%	6.3%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	0.2%	
				22:00	23:00	550	2.4%	26.3%	60.1%	0.2%	1.6%	0.0%	0.0%	0.0%	7.1%	0.4%	0.4%	0.5%	0.4%	0.4%	0.0%	0.2%	
				23:00	0:00	450	2.5%	25.5%	59.9%	0.2%	2.0%	0.0%	0.0%	0.0%	7.0%	0.5%	0.5%	0.7%	0.5%	0.5%	0.0%	0.2%	
L076	Hung Hom Road (NB)	Man Yue Street	Hok Yuen Street East	0:00	1:00	150	1.5%	27.6%	50.2%	0.0%	2.2%	0.0%	0.0%	0.0%	9.0%	0.7%	0.7%	2.2%	0.7%	0.7%	0.0%	4.5%	
				1:00	2:00	100	1.5%	33.3%	62.2%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	1.6%	31.1%	65.7%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.2%	33.3%	60.1%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.5%	30.0%	65.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.0%	26.0%	56.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.0%	2.0%	2.0%	0.0%	0.0%	
				6:00	7:00	150	0.9%	20.5%	45.4%	0.0%	3.6%	0.9%	0.0%	0.0%	9.8%	1.8%	1.8%	4.5%	3.6%	1.8%	0.0%	5.4%	
				7:00	8:00	350	0.6%	25.5%	34.3%	0.3%	2.8%	0.9%	0.3%	0.3%	12.6%	1.9%	1.9%	4.4%	4.1%	1.9%	0.0%	8.2%	
				8:00	9:00	450	1.1%	38.4%	26.3%	0.2%	5.0%	3.2%	1.1%	1.1%	7.1%	1.6%	1.6%	3.4%	3.2%	1.4%	0.0%	5.3%	
				9:00	10:00	450	1.0%	30.8%	28.2%	0.2%	5.1%	3.6%	1.2%	1.2%	6.6%	2.7%	2.2%	4.6%	4.6%	1.9%	0.0%	6.1%	
				10:00	11:00	350	1.8%	30.2%	27.1%	0.3%	13.7%	4.2%	1.2%	1.2%	6.5%	1.2%	1.2%	2.4%	2.4%	0.9%	0.0%	5.7%	
				11:00	12:00	350	1.2%	27.7%	22.2%	0.6%	16.1%	1.4%	0.6%	0.3%	11.0%	2.9%	1.7%	3.5%	3.5%	1.2%	0.0%	6.1%	
				12:00	13:00	350	1.5%	36.4%	27.6%	0.3%	10.9%	1.5%	0.6%	0.3%	8.2%	3.2%	0.9%	1.5%	1.8%	0.6%	0.0%	4.7%	
				13:00	14:00	350	0.9%	24.6%	18.4%	0.3%	13.4%	5.0%	1.5%	1.5%	7.9%	0.9%	3.5%	6.4%	6.7%	2.3%	0.0%	6.7%	
				14:00	15:00	350	0.9%	23.9%	26.3%	0.3%	13.0%	3.9%	1.2%	1.2%	8.8%	1.5%	2.1%	4.5%	4.5%	1.5%	0.0%	6.4%	
				15:00	16:00	400	1.3%	28.0%	27.0%	0.3%	12.4%	4.2%	1.3%	1.3%	6.9%	2.4%	1.6%	3.4%	3.2%	1.1%	0.0%	5.6%	
				16:00	17:00	350	2.1%	35.5%	19.8%	0.3%	14.7%	1.5%	0.3%	0.3%	8.1%	0.3%	2.4%	3.6%	4.2%	0.9%	0.0%	6.0%	
				17:00	18:00	450	2.7%	44.6%	20.8%	0.2%	9.7%	2.2%	0.5%	0.5%	6.9%	1.2%	1.2%	2.0%	2.0%	0.5%	0.0%	5.0%	
				18:00	19:00	350	2.9%	48.0%	22.8%	0.3%	7.8%	1.4%	0.3%	0.3%	3.2%	1.2%	1.7%	2.6%	2.9%	0.6%	0.0%	4.0%	
				19:00	20:00	350	1.5%	45.4%	30.6%	0.0%	2.1%	0.6%	0.0%	0.3%	8.7%	0.9%	0.9%	1.5%	1.8%	0.6%	0.0%	5.1%	
				20:00	21:00	300	1.5%	36.9%	37.1%	0.0%	1.8%	0.4%	0.0%	0.0%	14.2%	0.4%	0.4%	0.7%	0.4%	0.4%	0.0%	5.8%	
				21:00	22:00	250	1.2%	32.8%	42.2%	0.0%	2.5%	0.4%	0.0%	0.0%	13.5%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	7.0%	
				22:00	23:00	250	1.7%	25.8%	43.3%	0.0%	1.3%	0.0%	0.0%	0.0%	14.6%	0.9%	0.4%	1.3%	0.4%	0.4%	0.0%	9.9%	
				23:00	0:00	200	1.6%	24.9%	42.8%	0.0%	1.6%	0.0%	0.0%	0.0%	14.8%	1.1%	0.5%	1.6%	0.5%	0.5%	0.0%	10.1%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L077	Man Lok Street (SB)	Hok Yeun Street East	Man Yue Street	0:00	1:00	150	4.1%	31.4%	54.5%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	1.7%	0.8%	0.8%	2.5%	1.7%	0.8%	0.0%	0.0%
				1:00	2:00	100	4.4%	32.4%	60.3%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.2%	30.6%	64.6%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.3%	32.6%	58.8%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	4.9%	29.3%	63.4%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.8%	25.0%	54.1%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	3.8%	3.8%	1.9%	0.0%	0.0%	0.0%
				6:00	7:00	150	2.9%	23.1%	49.0%	0.0%	3.8%	1.0%	0.0%	0.0%	1.9%	2.9%	2.9%	5.8%	4.8%	1.9%	0.0%	0.0%	0.0%
				7:00	8:00	300	1.5%	20.8%	38.3%	0.4%	3.5%	1.5%	0.4%	0.4%	3.1%	4.6%	4.2%	9.3%	8.9%	3.1%	0.0%	0.0%	0.0%
				8:00	9:00	400	2.9%	29.3%	27.7%	0.3%	5.6%	5.6%	1.9%	1.9%	1.6%	3.5%	3.5%	7.2%	6.6%	2.4%	0.0%	0.0%	0.0%
				9:00	10:00	400	2.1%	22.4%	27.9%	0.3%	5.3%	5.9%	1.9%	1.9%	1.3%	5.6%	4.3%	9.1%	8.8%	3.2%	0.0%	0.0%	0.0%
				10:00	11:00	350	3.4%	32.2%	28.1%	0.3%	12.5%	7.2%	2.5%	2.2%	1.3%	1.3%	1.6%	3.4%	3.1%	0.9%	0.0%	0.0%	0.0%
				11:00	12:00	350	2.3%	32.0%	24.5%	0.6%	16.1%	2.9%	1.0%	0.6%	2.6%	4.2%	2.3%	4.8%	4.8%	1.3%	0.0%	0.0%	0.0%
				12:00	13:00	350	2.9%	40.5%	29.6%	0.3%	10.5%	2.5%	1.0%	0.6%	1.9%	4.5%	1.0%	1.9%	2.2%	0.6%	0.0%	0.0%	0.0%
				13:00	14:00	350	2.1%	25.8%	18.8%	0.3%	12.2%	8.1%	2.7%	2.7%	1.8%	1.2%	4.5%	8.4%	8.7%	2.7%	0.0%	0.0%	0.0%
				14:00	15:00	350	1.9%	25.7%	27.7%	0.3%	12.2%	6.8%	2.3%	2.3%	1.9%	1.9%	2.9%	6.1%	6.1%	1.9%	0.0%	0.0%	0.0%
				15:00	16:00	400	2.7%	29.0%	27.9%	0.3%	11.5%	7.1%	2.5%	2.2%	1.4%	3.3%	2.2%	4.4%	4.1%	1.4%	0.0%	0.0%	0.0%
				16:00	17:00	350	4.1%	49.9%	23.2%	0.3%	13.2%	2.5%	0.9%	0.6%	1.9%	0.3%	0.9%	0.9%	1.3%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	450	4.7%	55.8%	21.9%	0.2%	7.7%	3.7%	1.4%	1.2%	1.4%	0.5%	0.5%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	400	4.7%	58.8%	23.5%	0.3%	6.1%	2.4%	0.8%	0.8%	0.5%	0.3%	0.5%	0.5%	0.8%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	350	3.3%	50.2%	33.3%	0.0%	2.0%	1.0%	0.3%	0.3%	2.0%	1.3%	1.0%	2.3%	2.3%	0.7%	0.0%	0.0%	0.0%
				20:00	21:00	250	3.4%	43.8%	43.0%	0.0%	1.7%	0.4%	0.4%	0.4%	3.4%	0.9%	0.4%	0.9%	0.9%	0.4%	0.0%	0.0%	0.0%
				21:00	22:00	250	2.9%	39.3%	49.5%	0.0%	2.4%	1.0%	0.5%	0.5%	3.4%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	200	3.7%	32.3%	53.3%	0.0%	1.6%	0.0%	0.0%	0.0%	3.7%	1.1%	1.1%	1.6%	1.1%	0.5%	0.0%	0.0%	0.0%
				23:00	0:00	200	4.5%	31.0%	52.3%	0.0%	1.9%	0.0%	0.0%	0.0%	3.9%	1.3%	1.3%	1.9%	1.3%	0.6%	0.0%	0.0%	0.0%
L078	Ma Tau Wai Road (SB)	Hok Yeun Street	Tsing Chau Street	0:00	1:00	150	2.0%	19.5%	47.7%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	8.7%	0.0%	0.0%	0.7%	0.0%	0.0%	20.1%	
				1:00	2:00	100	3.1%	26.2%	67.6%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.3%	23.3%	71.7%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.4%	26.7%	64.5%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.6%	23.1%	71.7%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.3%	22.7%	68.2%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	150	1.6%	14.0%	42.4%	0.0%	2.3%	0.8%	0.0%	0.0%	9.3%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%	26.4%	
				7:00	8:00	450	0.5%	17.0%	26.6%	0.5%	2.8%	0.9%	0.2%	0.2%	12.8%	0.5%	0.5%	0.9%	0.9%	0.2%	0.0%	35.5%	
				8:00	9:00	550	1.3%	28.5%	22.8%	0.8%	5.3%	3.4%	0.8%	0.8%	8.2%	0.4%	0.4%	0.8%	0.8%	0.2%	0.0%	25.5%	
				9:00	10:00	500	1.0%	23.0%	24.4%	0.8%	5.3%	3.8%	0.8%	0.8%	7.5%	0.6%	0.4%	1.0%	1.0%	0.2%	0.0%	29.4%	
				10:00	11:00	400	2.0%	20.1%	24.9%	1.3%	9.1%	4.8%	1.0%	0.8%	6.1%	0.3%	0.3%	0.8%	0.5%	0.0%	0.0%	28.0%	
				11:00	12:00	400	1.3%	18.9%	20.7%	1.5%	11.1%	1.8%	0.3%	0.3%	10.9%	0.8%	0.3%	1.0%	0.8%	0.0%	0.0%	30.3%	
				12:00	13:00	400	1.6%	25.7%	26.7%	1.1%	7.7%	1.6%	0.3%	0.3%	8.2%	0.8%	0.3%	0.5%	0.3%	0.0%	0.0%	24.9%	
				13:00	14:00	400	1.3%	16.8%	17.3%	1.3%	9.2%	5.6%	1.0%	1.0%	7.9%	0.3%	0.8%	1.8%	1.3%	0.3%	0.0%	34.1%	
				14:00	15:00	400	1.0%	15.9%	24.3%	1.3%	8.6%	4.4%	0.8%	0.8%	8.4%	0.3%	0.5%	1.3%	0.8%	0.3%	0.0%	31.3%	
				15:00	16:00	450	1.6%	18.9%	25.4%	1.2%	8.5%	4.8%	0.9%	0.9%	6.7%	0.5%	0.5%	0.9%	0.7%	0.2%	0.0%	28.3%	
				16:00	17:00	350	2.9%	22.6%	21.8%	0.9%	7.4%	1.8%	0.3%	0.0%	6.5%	0.0%	0.6%	1.5%	0.6%	0.0%	0.0%	33.1%	
				17:00	18:00	450	3.7%	28.8%	23.0%	0.7%	4.9%	2.9%	0.5%	0.2%	5.6%	0.2%	0.2%	0.7%	0.2%	0.0%	0.0%	28.4%	
				18:00	19:00	350	3.9%	32.5%	26.7%	0.6%	4.2%	2.1%	0.3%	0.3%	2.7%	0.3%	0.3%	0.9%	0.3%	0.0%	0.0%	24.9%	
				19:00	20:00	400	1.8%	30.9%	29.0%	0.0%	1.3%	0.8%	0.0%	0.0%	8.7%	0.3%	0.3%	0.5%	0.3%	0.0%	0.0%	26.1%	
				20:00	21:00	350	1.8%	23.1%	32.6%	0.0%	1.2%	0.3%	0.0%	0.0%	12.8%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	27.9%	
				21:00	22:00	350	1.3%	19.4%	34.2%	0.0%	1.6%	0.6%	0.0%	0.0%	11.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	31.3%	
				22:00	23:00	350	1.5%	13.9%	32.3%	0.0%	0.6%	0.0%	0.0%	0.0%	11.3%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	40.1%	
				23:00	0:00	300	1.8%	13.2%	32.2%	0.0%	1.1%	0.0%	0.0%	0.0%	11.4%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	39.9%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L079	Ma Tau Wai Road (NB)	Tsing Chau Street	Hok Yeun Street	0:00	1:00	350	3.6%	20.7%	54.1%	0.0%	1.5%	0.0%	0.0%	0.0%	11.1%	0.3%	0.3%	1.2%	0.6%	0.3%	0.0%	6.3%	
				1:00	2:00	200	4.3%	25.2%	68.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	200	3.9%	22.4%	71.7%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	5.4%	25.2%	66.7%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	4.0%	22.2%	70.8%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	3.4%	20.5%	65.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	2.6%	1.7%	0.9%	0.0%	0.0%	0.0%
				6:00	7:00	300	2.5%	15.5%	49.9%	0.4%	2.9%	0.7%	0.0%	0.0%	12.2%	1.1%	1.1%	2.2%	2.2%	0.7%	0.0%	8.6%	0.0%
				7:00	8:00	700	1.5%	18.2%	33.6%	0.4%	3.1%	0.9%	0.3%	0.3%	17.0%	1.3%	1.3%	2.5%	2.5%	0.7%	0.0%	16.4%	0.0%
				8:00	9:00	900	3.1%	29.0%	27.4%	0.7%	5.8%	3.4%	0.9%	0.9%	10.3%	1.1%	1.1%	2.2%	2.2%	0.6%	0.0%	11.3%	0.0%
				9:00	10:00	850	2.3%	23.4%	29.3%	0.6%	5.8%	3.9%	1.0%	1.0%	9.6%	1.9%	1.5%	2.9%	3.0%	0.8%	0.0%	13.0%	0.0%
				10:00	11:00	800	3.6%	23.6%	30.9%	1.1%	10.1%	5.2%	1.3%	1.3%	8.4%	0.6%	0.8%	1.4%	1.5%	0.4%	0.0%	9.8%	0.0%
				11:00	12:00	850	2.4%	22.2%	25.7%	1.5%	12.3%	2.0%	0.5%	0.4%	14.8%	1.9%	1.0%	2.0%	2.1%	0.5%	0.0%	10.7%	0.0%
				12:00	13:00	800	2.9%	29.1%	32.1%	1.0%	8.1%	1.8%	0.5%	0.4%	11.0%	2.0%	0.5%	0.9%	1.0%	0.3%	0.0%	8.4%	0.0%
				13:00	14:00	800	2.2%	20.2%	22.0%	1.3%	10.3%	6.3%	1.5%	1.5%	10.8%	0.5%	2.2%	3.8%	4.2%	1.0%	0.0%	12.2%	0.0%
				14:00	15:00	800	1.8%	18.9%	30.3%	1.2%	9.7%	4.8%	1.2%	1.2%	11.7%	0.9%	1.4%	2.6%	2.7%	0.6%	0.0%	11.0%	0.0%
				15:00	16:00	900	2.7%	22.0%	31.3%	1.1%	9.3%	5.3%	1.4%	1.2%	9.1%	1.5%	1.0%	1.9%	1.9%	0.5%	0.0%	9.8%	0.0%
				16:00	17:00	850	4.0%	29.8%	27.8%	1.1%	8.4%	2.2%	0.5%	0.5%	10.2%	0.2%	1.3%	2.1%	2.5%	0.5%	0.0%	8.9%	0.0%
				17:00	18:00	1,100	4.9%	35.9%	28.2%	0.8%	5.3%	3.5%	0.8%	0.8%	8.6%	0.7%	0.7%	1.1%	1.1%	0.3%	0.0%	7.3%	0.0%
				18:00	19:00	900	5.2%	39.7%	31.5%	0.6%	4.3%	2.3%	0.6%	0.6%	4.1%	0.6%	0.9%	1.4%	1.7%	0.3%	0.0%	6.2%	0.0%
				19:00	20:00	800	3.3%	35.4%	35.1%	0.1%	1.5%	0.8%	0.1%	0.1%	11.5%	0.6%	0.5%	0.9%	1.0%	0.3%	0.0%	8.8%	0.0%
				20:00	21:00	700	3.2%	26.9%	40.0%	0.1%	1.2%	0.3%	0.1%	0.1%	17.3%	0.3%	0.1%	0.4%	0.3%	0.1%	0.0%	9.6%	0.0%
				21:00	22:00	650	2.2%	23.4%	44.1%	0.2%	1.6%	0.6%	0.2%	0.2%	16.1%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	11.2%	0.0%
				22:00	23:00	650	3.0%	17.8%	44.1%	0.2%	1.0%	0.0%	0.0%	0.0%	16.8%	0.3%	0.3%	0.6%	0.3%	0.2%	0.0%	15.4%	0.0%
				23:00	0:00	550	3.2%	17.2%	43.8%	0.2%	1.2%	0.0%	0.0%	0.0%	16.8%	0.4%	0.4%	0.8%	0.4%	0.2%	0.0%	15.4%	0.0%
L080	Ma Tau Wai Road (SB)	Tsing Chau Street	Man Yue Street	0:00	1:00	150	2.2%	18.7%	46.0%	0.0%	1.4%	0.0%	0.0%	0.0%	9.4%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	21.6%	
				1:00	2:00	100	3.4%	25.9%	69.0%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	1.9%	24.1%	72.1%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.6%	26.3%	68.5%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.9%	22.9%	71.3%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.5%	22.5%	67.5%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	150	1.6%	13.1%	41.1%	0.0%	2.5%	0.8%	0.0%	0.0%	9.8%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%	27.9%	0.0%
				7:00	8:00	450	0.9%	16.5%	25.9%	0.5%	2.8%	0.9%	0.2%	0.2%	13.0%	0.5%	0.5%	0.9%	0.9%	0.2%	0.0%	36.1%	0.0%
				8:00	9:00	550	1.9%	27.6%	22.4%	0.8%	5.4%	3.5%	0.8%	0.8%	8.3%	0.4%	0.4%	0.8%	0.8%	0.2%	0.0%	25.9%	0.0%
				9:00	10:00	500	1.4%	22.3%	24.0%	0.8%	5.3%	3.9%	0.8%	0.8%	7.6%	0.6%	0.4%	1.0%	1.0%	0.2%	0.0%	29.9%	0.0%
				10:00	11:00	400	1.9%	19.1%	24.0%	1.1%	8.7%	4.6%	1.1%	0.8%	6.6%	0.3%	0.3%	0.5%	0.5%	0.0%	0.0%	30.5%	0.0%
				11:00	12:00	400	1.3%	18.0%	19.8%	1.3%	10.7%	1.6%	0.3%	0.3%	11.5%	0.8%	0.3%	0.8%	0.8%	0.0%	0.0%	32.5%	0.0%
				12:00	13:00	400	1.7%	24.4%	25.9%	1.1%	7.4%	1.7%	0.3%	0.3%	8.8%	0.9%	0.3%	0.3%	0.3%	0.0%	0.0%	26.6%	0.0%
				13:00	14:00	400	1.1%	15.9%	16.5%	1.1%	8.6%	5.4%	1.1%	1.1%	8.4%	0.3%	0.8%	1.6%	1.4%	0.3%	0.0%	36.4%	0.0%
				14:00	15:00	400	1.1%	15.3%	23.1%	1.1%	8.3%	4.2%	0.8%	0.8%	8.9%	0.3%	0.6%	1.1%	0.8%	0.3%	0.0%	33.3%	0.0%
				15:00	16:00	450	1.5%	18.1%	24.5%	1.0%	8.2%	4.7%	1.0%	1.0%	7.2%	0.5%	0.5%	0.7%	0.7%	0.2%	0.0%	30.2%	0.0%
				16:00	17:00	300	2.3%	21.0%	20.3%	0.7%	6.3%	1.7%	0.3%	0.0%	7.3%	0.0%	0.7%	1.0%	0.7%	0.0%	0.0%	37.7%	0.0%
				17:00	18:00	400	2.8%	26.8%	21.7%	0.6%	4.2%	2.5%	0.6%	0.3%	6.5%	0.3%	0.3%	0.6%	0.3%	0.0%	0.0%	32.5%	0.0%
				18:00	19:00	300	3.2%	30.5%	25.6%	0.4%	3.5%	1.8%	0.4%	0.4%	3.2%	0.4%	0.4%	0.7%	0.4%	0.0%	0.0%	29.1%	0.0%
				19:00	20:00	400	2.0%	29.5%	28.0%	0.0%	1.4%	0.6%	0.0%	0.0%	9.3%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	28.0%	0.0%
				20:00	21:00	350	1.6%	22.2%	31.4%	0.0%	0.9%	0.3%	0.0%	0.0%	13.6%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	29.7%	0.0%
				21:00	22:00	350	1.3%	18.3%	32.9%	0.0%	1.3%	0.7%	0.0%	0.0%	12.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	33.2%	0.0%
				22:00	23:00	350	1.6%	13.1%	30.5%	0.0%	0.6%	0.0%	0.0%	0.0%	11.8%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	42.1%	0.0%
				23:00	0:00	300	1.5%	12.4%	30.9%	0.0%	0.8%	0.0%	0.0%	0.0%	12.0%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	42.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L081	Ma Tau Wai Road (NB)	Man Yue Street	Tsing Chau Street	0:00	1:00	350	4.2%	20.5%	53.6%	0.0%	1.5%	0.0%	0.0%	0.0%	11.2%	0.3%	0.3%	1.2%	0.6%	0.3%	0.0%	6.3%
				1:00	2:00	200	5.0%	24.8%	67.7%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	4.0%	22.7%	71.3%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	5.6%	25.0%	66.6%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	5.1%	22.4%	69.5%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	4.3%	20.7%	64.6%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	2.6%	1.7%	0.9%	0.0%	0.0%
				6:00	7:00	300	2.9%	15.3%	49.4%	0.4%	2.9%	0.7%	0.0%	0.0%	12.4%	1.1%	1.1%	2.2%	2.2%	0.7%	0.0%	8.7%
				7:00	8:00	700	1.8%	18.8%	33.1%	0.4%	2.9%	0.9%	0.3%	0.1%	17.0%	1.3%	1.3%	2.5%	2.5%	0.7%	0.0%	16.4%
				8:00	9:00	900	3.9%	29.8%	26.7%	0.7%	5.5%	3.2%	0.9%	0.8%	10.3%	1.1%	1.1%	2.1%	2.1%	0.6%	0.0%	11.2%
				9:00	10:00	850	3.0%	24.0%	29.1%	0.6%	5.6%	3.5%	1.0%	0.8%	9.5%	1.9%	1.4%	2.9%	3.0%	0.8%	0.0%	12.9%
				10:00	11:00	800	4.1%	23.5%	30.6%	1.1%	10.2%	5.2%	1.1%	1.1%	8.5%	0.6%	0.8%	1.4%	1.5%	0.4%	0.0%	9.9%
				11:00	12:00	850	2.6%	22.1%	25.4%	1.5%	12.4%	2.0%	0.5%	0.4%	14.9%	1.9%	1.0%	2.0%	2.1%	0.5%	0.0%	10.7%
				12:00	13:00	800	3.3%	29.0%	31.6%	1.0%	8.1%	1.8%	0.5%	0.4%	11.1%	2.0%	0.5%	0.9%	1.0%	0.3%	0.0%	8.5%
				13:00	14:00	800	2.4%	19.9%	21.8%	1.3%	10.4%	6.3%	1.4%	1.4%	10.9%	0.5%	2.2%	3.9%	4.2%	1.0%	0.0%	12.4%
				14:00	15:00	800	2.1%	18.8%	30.1%	1.2%	9.8%	4.8%	1.0%	1.0%	11.8%	0.9%	1.4%	2.6%	2.7%	0.7%	0.0%	11.1%
				15:00	16:00	900	3.2%	21.9%	30.8%	1.1%	9.4%	5.3%	1.2%	1.2%	9.2%	1.5%	1.0%	1.9%	1.9%	0.5%	0.0%	9.9%
				16:00	17:00	850	4.3%	28.9%	27.5%	1.1%	8.7%	2.4%	0.4%	0.5%	10.4%	0.2%	1.3%	2.2%	2.5%	0.5%	0.0%	9.1%
				17:00	18:00	1,050	5.2%	35.0%	27.9%	0.8%	5.6%	3.7%	0.8%	0.8%	8.8%	0.7%	0.7%	1.1%	1.1%	0.3%	0.0%	7.5%
				18:00	19:00	900	5.5%	38.6%	31.4%	0.6%	4.6%	2.5%	0.6%	0.6%	4.2%	0.6%	0.9%	1.5%	1.7%	0.3%	0.0%	6.4%
				19:00	20:00	800	3.7%	35.3%	34.6%	0.1%	1.5%	0.8%	0.1%	0.1%	11.6%	0.6%	0.5%	0.9%	1.0%	0.3%	0.0%	8.9%
				20:00	21:00	700	3.5%	26.9%	39.5%	0.1%	1.2%	0.3%	0.1%	0.1%	17.4%	0.3%	0.1%	0.4%	0.3%	0.1%	0.0%	9.7%
				21:00	22:00	650	2.6%	23.3%	43.5%	0.2%	1.6%	0.6%	0.2%	0.2%	16.3%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	11.3%
				22:00	23:00	650	3.4%	17.8%	43.4%	0.2%	1.0%	0.0%	0.0%	0.0%	17.0%	0.3%	0.3%	0.6%	0.3%	0.2%	0.0%	15.5%
				L082	Tsing Chau Street (WB)	Ma Tau Wai Road	Wo Chung Street	23:00	0:00	550	3.6%	17.1%	43.3%	0.2%	1.2%	0.0%	0.0%	0.0%	16.9%	0.4%	0.4%	0.8%
0:00	1:00	50	15.0%					55.0%	30.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	50	16.7%					58.3%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	50	18.2%					54.5%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	50	25.0%					50.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	50	14.3%					57.1%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	50	14.3%					57.1%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	50	14.3%					50.0%	28.6%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	50	8.9%					64.5%	20.0%	2.2%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	100	11.2%					66.5%	11.2%	1.1%	5.6%	2.2%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	100	9.9%					62.0%	14.1%	1.4%	7.0%	2.8%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	100	12.9%					48.4%	12.9%	1.6%	12.9%	8.1%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	100	9.3%					53.6%	13.0%	1.9%	18.5%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	100	9.8%					60.7%	13.1%	1.6%	11.5%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	100	9.6%					48.2%	9.6%	1.9%	15.4%	11.5%	1.9%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	50	8.0%					48.0%	14.0%	2.0%	16.0%	8.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	100	10.9%					48.3%	14.1%	1.6%	12.5%	9.4%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	100	14.8%					53.7%	11.1%	0.0%	14.8%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	100	16.0%					58.7%	9.3%	0.0%	8.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	100	16.7%					62.1%	10.6%	0.0%	6.1%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	100	11.1%					71.4%	14.3%	0.0%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	50	13.0%					65.2%	19.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	50	10.5%					63.2%	23.7%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	50	15.2%					54.5%	27.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	18.5%	51.9%	25.9%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L083	Tsing Chau Street (EB)	Wo Chung Street	Ma Tau Wai Road	0:00	1:00	50	9.5%	52.4%	33.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	9.1%	54.5%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	10.0%	50.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	12.5%	50.0%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	12.5%	50.0%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	12.5%	50.0%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	2.8%	55.5%	27.8%	2.8%	8.3%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	5.6%	59.2%	15.5%	1.4%	11.3%	4.2%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	5.0%	53.3%	18.3%	1.7%	13.3%	5.0%	1.7%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	8.2%	49.2%	14.8%	1.6%	16.4%	6.6%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	7.3%	52.8%	14.5%	1.8%	21.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	6.7%	61.6%	15.0%	1.7%	13.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	5.9%	48.9%	11.8%	2.0%	19.6%	7.8%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	6.0%	46.0%	18.0%	2.0%	18.0%	6.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	7.9%	49.2%	15.9%	1.6%	15.9%	6.3%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	11.3%	64.5%	11.3%	0.0%	9.7%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	12.4%	67.4%	10.1%	0.0%	5.6%	3.4%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	12.5%	69.9%	10.0%	0.0%	3.8%	2.5%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	8.1%	72.6%	16.1%	0.0%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	8.9%	66.7%	22.2%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	7.9%	63.2%	26.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	12.1%	54.6%	30.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	11.5%	53.9%	30.8%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L084	Cheung Ning Street (SB)	Cul-de-sac	Sheung Heung Street	0:00	1:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	0.0%	50.0%	0.0%	0.0%	25.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	50	0.0%	50.0%	0.0%	0.0%	20.0%	30.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	50	0.0%	44.5%	0.0%	0.0%	22.2%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	50	0.0%	60.0%	0.0%	0.0%	13.3%	26.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	50	0.0%	66.7%	0.0%	0.0%	25.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	50	0.0%	76.9%	0.0%	0.0%	15.4%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	50	0.0%	53.8%	0.0%	0.0%	15.4%	30.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	50	0.0%	58.3%	0.0%	0.0%	16.7%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	50	0.0%	60.0%	0.0%	0.0%	13.3%	26.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	50	0.0%	84.2%	0.0%	0.0%	5.3%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	50	0.0%	85.7%	0.0%	0.0%	3.6%	10.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	50	0.0%	88.0%	0.0%	0.0%	4.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	50	0.0%	92.9%	0.0%	0.0%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L085	Cheung Ning Street (NB)	Sheung Heung Street	Cul-de-sac	0:00	1:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	85.8%	0.0%	0.0%	7.1%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	80.0%	0.0%	0.0%	6.7%	13.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	75.0%	0.0%	0.0%	8.3%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	49.9%	0.0%	0.0%	18.8%	31.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	57.1%	0.0%	0.0%	28.6%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	71.4%	0.0%	0.0%	14.3%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	43.7%	0.0%	0.0%	18.8%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	46.1%	0.0%	0.0%	23.1%	30.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	47.1%	0.0%	0.0%	17.6%	35.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	28.6%	0.0%	0.0%	42.8%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	33.3%	0.0%	0.0%	22.2%	44.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	50.0%	0.0%	0.0%	16.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	92.3%	0.0%	0.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
L086	Ha Heung Road (WB)	Sheung Heung Street	Kowloon City Road	0:00	1:00	50	17.4%	65.3%	13.0%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	16.7%	66.6%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	18.2%	63.6%	18.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	22.2%	66.7%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	14.3%	71.4%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	14.3%	71.4%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	13.3%	60.0%	20.0%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	10.2%	67.5%	10.2%	0.0%	6.1%	2.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	150	12.6%	66.0%	4.9%	0.0%	7.8%	2.9%	2.9%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	11.0%	62.0%	6.1%	0.0%	9.8%	3.7%	3.7%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	12.5%	54.1%	6.9%	0.0%	13.9%	4.2%	4.2%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	100	9.7%	59.6%	6.5%	0.0%	19.4%	1.6%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	9.9%	67.6%	7.0%	0.0%	11.3%	1.4%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	7.9%	52.5%	4.8%	0.0%	15.9%	6.3%	6.3%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	100	8.8%	52.5%	7.0%	0.0%	15.8%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	10.5%	53.8%	6.6%	0.0%	13.2%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	100	12.5%	67.0%	6.3%	0.0%	9.4%	1.6%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	100	12.9%	69.7%	5.4%	0.0%	5.4%	2.2%	2.2%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	13.4%	73.2%	6.1%	0.0%	3.7%	1.2%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	11.1%	80.6%	6.9%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	100	13.5%	75.0%	9.6%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	11.9%	73.8%	11.9%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	17.1%	65.7%	14.3%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	50	17.2%	65.6%	13.8%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L087	Kowloon City Road (NB)	Sheung Heung Street	Ma Hang Chung Road	0:00	1:00	250	5.4%	34.2%	46.2%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	7.7%	0.5%	0.5%	1.4%	0.9%	0.9%	0.0%	0.0%
				1:00	2:00	150	5.8%	37.5%	53.4%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	5.6%	35.2%	57.3%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	7.4%	37.0%	51.9%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	5.6%	35.2%	56.4%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	4.7%	30.2%	51.1%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	2.3%	2.3%	2.3%	0.0%	0.0%
				6:00	7:00	200	4.0%	26.7%	45.6%	0.0%	4.0%	1.1%	0.0%	0.0%	8.5%	1.1%	1.1%	3.4%	2.8%	1.7%	0.0%	0.0%	
				7:00	8:00	500	3.1%	37.9%	32.7%	0.2%	4.2%	1.1%	0.2%	0.2%	11.4%	1.1%	1.1%	2.9%	2.4%	1.5%	0.0%	0.0%	
				8:00	9:00	750	5.7%	49.4%	21.7%	0.3%	6.2%	3.3%	0.7%	0.7%	5.7%	0.8%	0.8%	1.9%	1.7%	1.1%	0.0%	0.0%	
				9:00	10:00	650	4.6%	42.6%	25.0%	0.3%	6.7%	4.0%	0.8%	0.8%	5.7%	1.4%	1.1%	2.9%	2.5%	1.6%	0.0%	0.0%	
				10:00	11:00	600	5.1%	36.5%	25.0%	0.5%	13.5%	5.9%	1.1%	1.2%	5.3%	0.7%	0.7%	1.8%	1.6%	1.1%	0.0%	0.0%	
				11:00	12:00	600	3.4%	35.8%	21.4%	0.5%	16.9%	2.3%	0.5%	0.4%	9.5%	1.8%	1.1%	2.5%	2.5%	1.4%	0.0%	0.0%	
				12:00	13:00	600	4.0%	44.6%	25.6%	0.4%	10.7%	1.9%	0.5%	0.4%	6.8%	1.9%	0.5%	1.1%	1.1%	0.5%	0.0%	0.0%	
				13:00	14:00	550	3.1%	31.5%	18.0%	0.6%	14.1%	7.3%	1.5%	1.5%	7.0%	0.6%	2.2%	5.0%	4.8%	2.8%	0.0%	0.0%	
				14:00	15:00	550	2.9%	30.7%	25.7%	0.6%	13.6%	5.7%	1.1%	1.1%	7.7%	1.0%	1.3%	3.4%	3.3%	1.9%	0.0%	0.0%	
				15:00	16:00	650	4.0%	34.5%	25.5%	0.5%	12.7%	6.1%	1.3%	1.3%	5.8%	1.4%	1.0%	2.4%	2.2%	1.3%	0.0%	0.0%	
				16:00	17:00	600	4.5%	40.1%	20.9%	0.3%	12.6%	2.6%	0.5%	0.5%	6.8%	0.3%	1.6%	3.7%	3.7%	1.9%	0.0%	0.0%	
				17:00	18:00	750	5.4%	47.7%	21.0%	0.3%	7.9%	4.1%	0.8%	0.8%	5.7%	0.8%	0.8%	1.9%	1.7%	1.1%	0.0%	0.0%	
				18:00	19:00	650	5.6%	50.9%	22.7%	0.2%	6.4%	2.7%	0.5%	0.6%	2.5%	0.6%	1.1%	2.5%	2.4%	1.3%	0.0%	0.0%	
				19:00	20:00	600	4.5%	53.8%	27.7%	0.0%	1.9%	0.9%	0.2%	0.2%	7.1%	0.5%	0.5%	1.2%	1.0%	0.5%	0.0%	0.0%	
				20:00	21:00	500	4.8%	45.1%	34.3%	0.0%	1.7%	0.4%	0.2%	0.2%	11.7%	0.2%	0.2%	0.4%	0.4%	0.4%	0.0%	0.0%	
				21:00	22:00	400	3.8%	40.4%	39.8%	0.0%	2.5%	0.8%	0.3%	0.3%	11.5%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	
				22:00	23:00	400	5.3%	33.8%	43.4%	0.0%	1.4%	0.0%	0.0%	0.0%	13.0%	0.6%	0.3%	0.8%	0.6%	0.8%	0.0%	0.0%	
				L088	Pau Chung Street (SB)	Ma Hang Chung Road	Sheung Heung Street	23:00	0:00	300	5.8%	32.4%	43.4%	0.0%	1.7%	0.0%	0.0%	0.0%	13.0%	0.7%	0.3%	1.0%	0.7%
0:00	1:00	50	13.3%					40.0%	46.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	50	12.5%					37.5%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	12.5%					37.5%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	16.7%					33.3%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	16.7%					33.3%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	16.7%					33.3%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	50	9.1%					27.3%	45.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	9.1%	0.0%	0.0%	0.0%	
7:00	8:00	50	5.9%					41.1%	35.3%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.9%	5.9%	0.0%	0.0%	0.0%	
8:00	9:00	50	9.7%					48.3%	19.4%	0.0%	9.7%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	3.2%	0.0%	0.0%	0.0%	
9:00	10:00	50	7.7%					42.4%	23.1%	0.0%	11.5%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	3.8%	0.0%	0.0%	0.0%	
10:00	11:00	50	10.8%					40.6%	27.0%	0.0%	10.8%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	2.7%	0.0%	0.0%	0.0%	
11:00	12:00	50	7.9%					39.4%	21.1%	0.0%	15.8%	2.6%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	5.3%	5.3%	0.0%	0.0%	
12:00	13:00	50	7.5%					47.5%	25.0%	0.0%	10.0%	2.5%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	2.5%	0.0%	0.0%	0.0%	
13:00	14:00	50	5.3%					34.2%	18.4%	0.0%	13.2%	7.9%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	7.9%	7.9%	2.6%	0.0%	0.0%
14:00	15:00	50	5.7%					34.3%	25.7%	0.0%	11.4%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	5.7%	5.7%	2.9%	0.0%	0.0%
15:00	16:00	50	6.7%					35.7%	24.4%	0.0%	11.1%	6.7%	0.0%	0.0%	0.0%	0.0%	2.2%	2.2%	4.4%	4.4%	2.2%	0.0%	0.0%
16:00	17:00	100	8.9%					42.9%	21.4%	0.0%	7.1%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	3.6%	5.4%	7.1%	1.8%	0.0%	0.0%
17:00	18:00	100	11.1%					51.3%	20.8%	0.0%	4.2%	2.8%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.8%	2.8%	1.4%	0.0%	0.0%
18:00	19:00	100	10.6%					51.7%	21.2%	0.0%	3.0%	1.5%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	3.0%	4.5%	1.5%	0.0%	0.0%
19:00	20:00	50	9.8%					56.2%	26.8%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	50	10.3%					51.8%	37.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	50	7.7%					46.2%	42.3%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	50	13.0%					39.1%	47.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	11.8%	41.2%	47.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L089	Pau Chung Street (NB)	Sheung Heung Street	Ma Hang Chung Road	0:00	1:00	100	3.3%	29.7%	62.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	100	3.6%	29.1%	63.7%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.9%	27.5%	66.6%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.3%	28.9%	60.5%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.0%	27.3%	66.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.7%	24.3%	64.9%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	2.7%	23.3%	60.2%	0.0%	5.5%	1.4%	0.0%	0.0%	0.0%	1.4%	1.4%	2.7%	1.4%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	250	2.3%	35.2%	46.6%	0.5%	6.0%	1.4%	0.0%	0.0%	0.0%	1.9%	1.4%	2.8%	1.9%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	350	4.0%	45.0%	30.4%	0.6%	9.2%	4.6%	0.6%	0.3%	0.0%	1.1%	1.1%	2.0%	1.1%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	350	3.2%	38.0%	34.3%	0.6%	9.6%	5.4%	0.6%	0.3%	0.0%	1.9%	1.6%	2.9%	1.6%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	250	3.5%	32.2%	33.7%	0.9%	17.0%	7.0%	0.9%	0.4%	0.0%	0.9%	0.9%	1.7%	0.9%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	250	2.3%	32.6%	30.4%	1.4%	22.1%	2.8%	0.5%	0.0%	0.0%	2.3%	1.4%	2.8%	1.4%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	250	3.1%	39.9%	35.1%	0.9%	13.6%	2.2%	0.4%	0.0%	0.0%	2.6%	0.4%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	250	2.4%	29.6%	25.6%	0.9%	18.5%	9.0%	1.4%	0.9%	0.0%	0.9%	2.8%	5.2%	2.8%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	250	1.9%	27.6%	35.3%	1.0%	17.1%	7.1%	1.0%	0.5%	0.0%	1.4%	1.9%	3.3%	1.9%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	300	2.8%	30.6%	34.4%	0.8%	15.9%	7.1%	1.2%	0.4%	0.0%	2.0%	1.2%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	200	3.6%	38.3%	30.3%	0.6%	15.2%	3.0%	0.6%	0.0%	0.0%	0.6%	1.8%	3.6%	2.4%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	250	4.2%	45.0%	30.2%	0.5%	9.4%	4.7%	0.9%	0.5%	0.0%	0.9%	0.9%	1.9%	0.9%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	200	4.2%	46.7%	31.7%	0.5%	7.4%	3.2%	0.5%	0.5%	0.0%	0.5%	1.1%	2.1%	1.6%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	3.6%	49.7%	39.4%	0.0%	2.7%	0.9%	0.0%	0.0%	0.0%	0.9%	0.5%	1.4%	0.9%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	200	3.4%	42.5%	50.0%	0.0%	2.3%	0.6%	0.0%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	200	2.5%	36.9%	56.1%	0.0%	3.2%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	3.5%	31.2%	61.1%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	150	4.3%	29.6%	60.8%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%
L090	Sheung Heung Street (EB)	Ma Tau Wai Road	Pau Chung Street	0:00	1:00	200	1.9%	24.4%	44.4%	0.0%	3.1%	0.0%	0.0%	0.0%	22.5%	0.6%	0.6%	1.3%	0.6%	0.6%	0.0%	0.0%	
				1:00	2:00	100	2.6%	29.9%	57.1%	0.0%	5.2%	0.0%	0.0%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	1.4%	28.2%	60.6%	0.0%	4.2%	0.0%	0.0%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.0%	32.7%	59.2%	0.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.3%	30.2%	62.8%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	1.9%	25.9%	55.4%	0.0%	7.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	3.7%	1.9%	1.9%	0.0%	0.0%	
				6:00	7:00	150	1.5%	18.5%	40.8%	0.0%	5.9%	1.5%	0.0%	0.0%	23.7%	0.7%	0.7%	3.0%	2.2%	1.5%	0.0%	0.0%	
				7:00	8:00	400	1.0%	26.2%	29.9%	0.3%	6.2%	1.5%	0.0%	0.0%	27.2%	1.0%	0.8%	2.6%	1.8%	1.5%	0.0%	0.0%	
				8:00	9:00	600	2.0%	38.1%	22.1%	0.4%	10.5%	5.4%	0.4%	0.2%	15.0%	0.7%	0.7%	2.0%	1.4%	1.1%	0.0%	0.0%	
				9:00	10:00	500	1.6%	31.9%	24.7%	0.4%	11.1%	6.4%	0.4%	0.2%	14.5%	1.2%	1.0%	2.8%	2.2%	1.6%	0.0%	0.0%	
				10:00	11:00	450	1.7%	26.0%	23.3%	0.7%	19.2%	8.4%	0.5%	0.2%	15.1%	0.5%	0.5%	1.7%	1.2%	1.0%	0.0%	0.0%	
				11:00	12:00	450	0.9%	22.9%	18.3%	0.7%	22.0%	2.9%	0.2%	0.0%	25.0%	1.3%	0.7%	2.2%	1.8%	1.1%	0.0%	0.0%	
				12:00	13:00	450	1.4%	31.5%	23.9%	0.5%	15.3%	2.9%	0.2%	0.0%	19.6%	1.7%	0.5%	1.0%	1.0%	0.5%	0.0%	0.0%	
				13:00	14:00	450	0.9%	21.2%	16.0%	0.7%	19.1%	9.7%	0.7%	0.5%	18.9%	0.5%	1.7%	4.2%	3.5%	2.4%	0.0%	0.0%	
				14:00	15:00	450	0.7%	20.4%	22.5%	0.7%	18.2%	7.5%	0.5%	0.2%	20.6%	0.7%	1.0%	2.9%	2.4%	1.7%	0.0%	0.0%	
				15:00	16:00	500	1.3%	24.0%	23.5%	0.6%	17.7%	8.5%	0.6%	0.2%	16.5%	1.1%	0.9%	2.1%	1.7%	1.3%	0.0%	0.0%	
				16:00	17:00	400	1.3%	27.3%	18.9%	0.5%	16.3%	3.4%	0.3%	0.0%	22.6%	0.3%	1.3%	3.1%	2.9%	1.8%	0.0%	0.0%	
				17:00	18:00	500	1.8%	34.7%	20.2%	0.4%	10.9%	5.8%	0.4%	0.2%	20.0%	0.7%	0.7%	1.8%	1.3%	1.1%	0.0%	0.0%	
				18:00	19:00	400	2.0%	40.9%	24.3%	0.3%	9.6%	4.2%	0.3%	0.3%	10.2%	0.6%	1.1%	2.5%	2.3%	1.4%	0.0%	0.0%	
				19:00	20:00	400	1.5%	41.0%	27.9%	0.0%	3.0%	1.3%	0.0%	0.0%	21.8%	0.5%	0.5%	1.0%	1.0%	0.5%	0.0%	0.0%	
				20:00	21:00	400	1.4%	30.5%	31.1%	0.0%	2.3%	0.6%	0.0%	0.0%	32.3%	0.3%	0.3%	0.6%	0.3%	0.3%	0.0%	0.0%	
				21:00	22:00	350	1.0%	27.5%	35.6%	0.0%	3.2%	1.0%	0.0%	0.0%	31.1%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	
				22:00	23:00	300	1.4%	22.2%	37.6%	0.0%	2.1%	0.0%	0.0%	0.0%	34.4%	0.3%	0.3%	0.7%	0.3%	0.7%	0.0%	0.0%	
				23:00	0:00	250	1.7%	21.3%	37.4%	0.0%	2.6%	0.0%	0.0%	0.0%	34.0%	0.4%	0.4%	0.9%	0.4%	0.9%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L091	Sheung Heung Street (WB)	Pau Chung Street	Ma Tau Wai Road	0:00	1:00	50	4.0%	52.0%	36.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	6.3%	49.9%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	7.7%	53.8%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	8.3%	50.1%	33.3%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	45.4%	36.4%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	4.3%	39.5%	30.4%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	4.3%	4.3%	4.3%	4.3%	4.3%	0.0%	0.0%
				7:00	8:00	50	2.3%	49.8%	20.5%	2.3%	9.1%	2.3%	0.0%	0.0%	0.0%	2.3%	2.3%	4.5%	2.3%	2.3%	0.0%	0.0%
				8:00	9:00	100	2.4%	55.1%	12.2%	1.2%	12.2%	7.3%	1.2%	1.2%	0.0%	1.2%	1.2%	2.4%	1.2%	1.2%	0.0%	0.0%
				9:00	10:00	100	1.4%	47.9%	14.1%	1.4%	12.7%	8.5%	1.4%	1.4%	0.0%	2.8%	1.4%	4.2%	1.4%	1.4%	0.0%	0.0%
				10:00	11:00	100	3.8%	47.1%	15.0%	1.3%	15.0%	7.5%	1.3%	1.3%	0.0%	1.3%	1.3%	2.5%	1.3%	1.3%	0.0%	0.0%
				11:00	12:00	100	2.6%	46.7%	13.0%	2.6%	19.5%	2.6%	0.0%	0.0%	0.0%	3.9%	1.3%	3.9%	2.6%	1.3%	0.0%	0.0%
				12:00	13:00	100	2.4%	57.4%	15.9%	1.2%	12.2%	2.4%	0.0%	0.0%	0.0%	3.7%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%
				13:00	14:00	100	2.5%	39.7%	11.1%	1.2%	14.8%	8.6%	1.2%	1.2%	0.0%	1.2%	3.7%	7.4%	3.7%	3.7%	0.0%	0.0%
				14:00	15:00	100	1.4%	39.7%	16.4%	1.4%	15.1%	8.2%	1.4%	1.4%	0.0%	1.4%	2.7%	5.5%	2.7%	2.7%	0.0%	0.0%
				15:00	16:00	100	2.3%	44.3%	15.9%	1.1%	13.6%	8.0%	1.1%	1.1%	0.0%	2.3%	2.3%	3.4%	2.3%	2.3%	0.0%	0.0%
				16:00	17:00	100	3.0%	56.0%	13.0%	1.0%	8.0%	2.0%	0.0%	0.0%	0.0%	1.0%	3.0%	6.0%	4.0%	3.0%	0.0%	0.0%
				17:00	18:00	150	3.8%	64.7%	13.1%	0.8%	4.6%	3.1%	0.8%	0.0%	0.0%	1.5%	1.5%	3.1%	1.5%	1.5%	0.0%	0.0%
				18:00	19:00	150	3.4%	66.2%	13.6%	0.8%	3.4%	1.7%	0.8%	0.0%	0.0%	0.8%	1.7%	3.4%	2.5%	1.7%	0.0%	0.0%
				19:00	20:00	100	2.5%	69.7%	17.5%	0.0%	2.5%	1.3%	0.0%	0.0%	0.0%	1.3%	1.3%	1.3%	1.3%	1.3%	0.0%	0.0%
				20:00	21:00	100	3.6%	67.8%	25.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	2.1%	62.4%	29.2%	0.0%	4.2%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	5.0%	55.0%	35.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%
L092	Sheung Heung Street (EB)	Pau Chung Street	Kowloon City Road	23:00	0:00	50	6.1%	54.6%	33.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%
				0:00	1:00	150	2.3%	20.6%	42.6%	0.0%	2.3%	0.0%	0.0%	0.0%	27.5%	0.8%	0.8%	1.5%	0.8%	0.8%	0.0%	0.0%
				1:00	2:00	100	3.4%	27.6%	58.7%	0.0%	3.4%	0.0%	0.0%	0.0%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.6%	23.6%	61.9%	0.0%	3.6%	0.0%	0.0%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.3%	28.9%	60.5%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.0%	27.3%	66.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.4%	22.0%	58.7%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	2.4%	2.4%	2.4%	2.4%	2.4%	0.0%	0.0%
				6:00	7:00	150	1.8%	15.5%	39.2%	0.0%	4.5%	0.9%	0.0%	0.0%	29.1%	0.9%	1.8%	2.7%	2.7%	0.9%	0.0%	0.0%
				7:00	8:00	300	1.4%	21.0%	27.8%	0.3%	4.4%	1.0%	0.3%	0.3%	36.1%	1.4%	1.0%	2.0%	2.0%	1.0%	0.0%	0.0%
				8:00	9:00	400	3.1%	32.7%	22.1%	0.5%	8.2%	4.1%	0.8%	0.8%	21.3%	1.0%	1.0%	1.8%	1.8%	0.8%	0.0%	0.0%
				9:00	10:00	400	2.3%	27.3%	24.8%	0.6%	8.5%	4.8%	0.9%	0.9%	20.5%	1.7%	1.4%	2.6%	2.6%	1.1%	0.0%	0.0%
				10:00	11:00	350	2.5%	23.0%	23.9%	0.9%	15.4%	6.6%	1.3%	1.3%	19.8%	0.6%	0.9%	1.6%	1.6%	0.6%	0.0%	0.0%
				11:00	12:00	400	1.4%	19.5%	18.1%	0.8%	17.3%	2.3%	0.3%	0.3%	31.8%	2.0%	1.1%	2.0%	2.3%	0.8%	0.0%	0.0%
				12:00	13:00	350	2.1%	27.4%	24.2%	0.6%	12.3%	2.1%	0.3%	0.3%	25.2%	2.5%	0.6%	0.9%	1.2%	0.3%	0.0%	0.0%
				13:00	14:00	350	1.5%	18.4%	16.0%	0.9%	15.1%	7.6%	1.2%	1.5%	24.3%	0.6%	2.4%	4.2%	4.5%	1.8%	0.0%	0.0%
				14:00	15:00	350	1.2%	17.4%	22.4%	0.9%	14.3%	5.9%	0.9%	0.9%	26.5%	0.9%	1.6%	2.8%	3.1%	1.2%	0.0%	0.0%
				15:00	16:00	400	1.9%	20.8%	24.0%	0.8%	14.2%	6.7%	1.1%	1.1%	21.4%	1.7%	1.1%	2.2%	2.2%	0.8%	0.0%	0.0%
				16:00	17:00	350	2.1%	24.2%	19.6%	0.6%	13.2%	2.8%	0.3%	0.3%	26.5%	0.3%	1.8%	3.4%	3.7%	1.2%	0.0%	0.0%
				17:00	18:00	400	2.8%	30.9%	21.0%	0.5%	8.8%	4.7%	0.8%	0.8%	23.3%	1.0%	1.0%	1.8%	1.8%	0.8%	0.0%	0.0%
				18:00	19:00	300	3.3%	36.9%	25.4%	0.3%	8.0%	3.3%	0.7%	0.7%	12.0%	1.0%	1.7%	2.7%	3.0%	1.0%	0.0%	0.0%
				19:00	20:00	350	2.6%	34.6%	27.7%	0.0%	2.3%	1.0%	0.0%	0.3%	27.7%	0.6%	0.6%	1.0%	1.3%	0.3%	0.0%	0.0%
				20:00	21:00	300	2.1%	25.0%	29.5%	0.0%	1.7%	0.3%	0.0%	0.0%	39.9%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%
				21:00	22:00	300	1.6%	22.7%	34.3%	0.0%	2.4%	0.8%	0.0%	0.0%	38.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	2.1%	17.8%	35.3%	0.0%	1.2%	0.0%	0.0%	0.0%	41.2%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%
				23:00	0:00	200	2.6%	16.8%	34.7%	0.0%	2.0%	0.0%	0.0%	0.0%	40.9%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L093	Sheung Heung Street (WB)	Kowloon City Road	Pau Chung Street	0:00	1:00	50	5.9%	38.2%	50.1%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	5.0%	40.0%	50.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	5.6%	38.9%	55.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.1%	35.7%	50.1%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	9.1%	36.4%	54.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	7.1%	35.7%	50.1%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	3.6%	32.1%	46.3%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	3.6%	3.6%	3.6%	3.6%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	100	3.6%	40.9%	30.4%	0.0%	8.9%	1.8%	0.0%	0.0%	0.0%	3.6%	3.6%	3.6%	3.6%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	150	5.9%	46.5%	17.8%	0.0%	11.9%	6.9%	2.0%	1.0%	0.0%	2.0%	2.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	4.5%	39.3%	20.2%	0.0%	12.4%	7.9%	2.2%	1.1%	0.0%	3.4%	2.2%	3.4%	3.4%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	6.2%	38.0%	22.7%	0.0%	15.5%	7.2%	2.1%	1.0%	0.0%	1.0%	2.1%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	4.3%	38.7%	20.4%	0.0%	19.4%	3.2%	1.1%	0.0%	0.0%	4.3%	2.2%	3.2%	3.2%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	5.2%	47.4%	23.7%	0.0%	12.4%	2.1%	1.0%	0.0%	0.0%	5.2%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	3.2%	33.2%	17.2%	0.0%	16.1%	8.6%	2.2%	2.2%	0.0%	1.1%	5.4%	5.4%	5.4%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	3.4%	33.6%	24.1%	0.0%	16.1%	6.9%	2.3%	1.1%	0.0%	2.3%	3.4%	3.4%	3.4%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	150	4.6%	36.1%	23.1%	0.0%	13.9%	7.4%	1.9%	0.9%	0.0%	3.7%	2.8%	2.8%	2.8%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	150	6.3%	47.2%	21.4%	0.0%	8.9%	1.8%	0.0%	0.0%	0.0%	0.9%	4.5%	4.5%	4.5%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	150	6.8%	54.6%	21.1%	0.0%	5.4%	2.7%	0.7%	0.7%	0.0%	2.0%	2.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	150	6.7%	54.8%	21.5%	0.0%	4.4%	1.5%	0.7%	0.7%	0.0%	1.5%	3.0%	2.2%	3.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	5.4%	60.0%	26.9%	0.0%	2.2%	1.1%	0.0%	0.0%	0.0%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	5.8%	53.7%	36.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	5.0%	48.3%	41.7%	0.0%	3.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	7.3%	40.0%	45.5%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	6.7%	40.0%	44.5%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	2.2%	2.2%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%
L094	Sheung Heung Street (EB)	Kowloon City Road	Ha Heung Road	0:00	1:00	150	1.0%	25.7%	37.1%	0.0%	1.9%	0.0%	0.0%	0.0%	34.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	2.2%	34.8%	52.1%	0.0%	2.2%	0.0%	0.0%	0.0%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	33.3%	54.8%	0.0%	2.4%	0.0%	0.0%	0.0%	9.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	39.3%	57.1%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	36.0%	60.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	37.0%	59.3%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	100	1.1%	19.3%	34.1%	0.0%	3.4%	1.1%	0.0%	0.0%	36.6%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	0.0%	
				7:00	8:00	250	0.4%	24.7%	21.6%	0.0%	3.1%	0.9%	0.4%	0.4%	46.9%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				8:00	9:00	300	1.0%	38.9%	17.5%	0.0%	5.7%	3.0%	2.4%	2.4%	27.9%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%	
				9:00	10:00	300	0.8%	33.9%	20.2%	0.0%	6.2%	3.5%	2.7%	2.7%	28.0%	0.8%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				10:00	11:00	300	0.8%	29.1%	20.6%	0.0%	11.1%	4.7%	3.6%	3.6%	24.9%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				11:00	12:00	300	0.4%	25.3%	15.7%	0.0%	12.1%	1.4%	1.4%	1.1%	39.7%	1.1%	0.4%	0.7%	0.7%	0.0%	0.0%	0.0%	
				12:00	13:00	300	0.7%	34.1%	20.1%	0.0%	8.2%	1.5%	1.5%	1.1%	30.5%	1.1%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				13:00	14:00	300	0.4%	24.4%	14.6%	0.0%	11.0%	5.5%	4.3%	4.3%	31.5%	0.4%	1.2%	1.2%	1.2%	0.0%	0.0%	0.0%	
				14:00	15:00	300	0.4%	22.8%	19.7%	0.0%	10.2%	4.3%	3.1%	3.1%	33.6%	0.4%	0.8%	0.8%	0.8%	0.0%	0.0%	0.0%	
				15:00	16:00	300	0.7%	26.9%	20.6%	0.0%	10.1%	4.5%	3.8%	3.8%	26.8%	0.7%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	
				16:00	17:00	300	0.7%	32.9%	18.1%	0.0%	9.0%	1.8%	1.1%	1.4%	31.0%	0.4%	1.1%	1.1%	1.4%	0.0%	0.0%	0.0%	
				17:00	18:00	350	0.9%	39.5%	18.1%	0.0%	5.7%	2.9%	2.3%	2.3%	25.9%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	
				18:00	19:00	300	1.1%	48.5%	22.3%	0.0%	5.3%	2.3%	1.9%	1.9%	13.6%	0.4%	0.8%	0.8%	1.1%	0.0%	0.0%	0.0%	
				19:00	20:00	300	0.7%	40.9%	21.9%	0.0%	1.5%	0.7%	0.4%	0.4%	31.9%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				20:00	21:00	300	0.8%	29.0%	23.1%	0.0%	1.2%	0.4%	0.4%	0.4%	44.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	250	0.5%	26.2%	26.7%	0.0%	1.8%	0.5%	0.5%	0.5%	43.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	250	0.5%	21.6%	28.4%	0.0%	1.0%	0.0%	0.0%	0.0%	48.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	200	0.6%	20.7%	28.7%	0.0%	1.2%	0.0%	0.0%	0.0%	48.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L095	Sheung Heung Street (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	200	1.0%	35.8%	52.9%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	8.8%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%
				1:00	2:00	150	0.9%	38.7%	59.5%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	1.0%	35.1%	63.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	1.4%	38.9%	58.3%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	1.6%	34.9%	63.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.4%	32.9%	60.1%	0.0%	1.4%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	1.4%	0.0%	0.0%
				6:00	7:00	150	0.7%	30.3%	55.6%	0.0%	0.7%	0.0%	0.0%	0.0%	0.7%	10.6%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				7:00	8:00	400	0.5%	41.8%	39.6%	0.0%	0.8%	0.3%	0.8%	0.8%	13.4%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%	
				8:00	9:00	600	1.2%	56.5%	27.2%	0.0%	1.2%	0.8%	2.4%	2.4%	6.9%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%	
				9:00	10:00	550	1.0%	49.8%	32.0%	0.0%	1.4%	1.0%	2.8%	2.9%	7.1%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%	
				10:00	11:00	450	1.2%	43.8%	32.9%	0.2%	2.8%	1.6%	4.4%	4.7%	7.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%	
				11:00	12:00	400	0.8%	45.2%	30.1%	0.3%	3.8%	0.8%	2.0%	1.8%	13.4%	0.0%	0.0%	1.0%	0.0%	0.8%	0.0%	0.0%	
				12:00	13:00	500	0.9%	51.5%	32.2%	0.2%	2.2%	0.4%	1.8%	1.6%	8.6%	0.0%	0.0%	0.4%	0.0%	0.2%	0.0%	0.0%	
				13:00	14:00	400	0.8%	41.5%	25.8%	0.3%	3.2%	2.1%	6.1%	6.6%	10.0%	0.0%	0.0%	1.8%	0.0%	1.8%	0.0%	0.0%	
				14:00	15:00	400	0.5%	38.2%	34.7%	0.3%	2.9%	1.6%	4.4%	4.7%	10.4%	0.0%	0.0%	1.3%	0.0%	1.0%	0.0%	0.0%	
				15:00	16:00	500	0.9%	41.6%	33.7%	0.2%	2.6%	1.7%	4.9%	4.9%	7.7%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%	
				16:00	17:00	400	1.0%	50.2%	28.8%	0.3%	2.8%	0.8%	2.0%	2.0%	9.8%	0.0%	0.0%	1.3%	0.0%	1.0%	0.0%	0.0%	
				17:00	18:00	600	1.1%	54.6%	26.2%	0.2%	1.6%	1.1%	3.4%	3.4%	7.4%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	
				18:00	19:00	500	1.0%	58.8%	28.8%	0.2%	1.3%	0.6%	2.3%	2.5%	3.3%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%	
				19:00	20:00	500	0.8%	56.8%	31.9%	0.0%	0.4%	0.2%	0.4%	0.6%	8.3%	0.0%	0.0%	0.4%	0.0%	0.2%	0.0%	0.0%	
				20:00	21:00	450	1.0%	45.9%	38.4%	0.0%	0.2%	0.0%	0.5%	0.5%	13.1%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	0.0%	
				21:00	22:00	400	0.6%	40.9%	43.7%	0.0%	0.6%	0.3%	0.6%	0.6%	12.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	350	0.9%	34.6%	49.0%	0.0%	0.3%	0.0%	0.0%	0.0%	14.6%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	
23:00	0:00	300	1.2%	33.7%	49.2%	0.0%	0.4%	0.0%	0.0%	0.0%	14.7%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%					
L096	Sheung Heung Street (EB)	Ha Heung Road	Cheung Ning Street	0:00	1:00	150	0.9%	24.8%	35.2%	0.0%	1.8%	0.0%	0.0%	0.0%	31.9%	0.9%	0.9%	1.8%	0.9%	0.9%	0.0%	0.0%	
				1:00	2:00	50	0.0%	37.0%	52.1%	0.0%	2.2%	0.0%	0.0%	0.0%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	32.6%	55.8%	0.0%	2.3%	0.0%	0.0%	0.0%	9.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	39.3%	57.1%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	36.0%	60.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	29.4%	50.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	2.9%	2.9%	5.9%	2.9%	2.9%	0.0%	0.0%	
				6:00	7:00	100	0.0%	18.8%	32.3%	0.0%	2.1%	1.0%	0.0%	0.0%	33.3%	2.1%	2.1%	4.2%	3.1%	1.0%	0.0%	0.0%	
				7:00	8:00	250	0.4%	21.0%	18.9%	0.4%	3.4%	0.8%	0.8%	0.8%	44.7%	1.7%	1.3%	2.5%	2.5%	0.8%	0.0%	0.0%	
				8:00	9:00	350	0.7%	33.9%	15.4%	0.3%	6.2%	3.3%	2.6%	2.6%	27.1%	1.3%	1.3%	2.3%	2.3%	0.7%	0.0%	0.0%	
				9:00	10:00	300	0.4%	28.1%	17.0%	0.4%	6.5%	4.0%	2.9%	2.9%	26.1%	2.2%	1.8%	3.3%	3.3%	1.1%	0.0%	0.0%	
				10:00	11:00	300	0.7%	28.0%	19.6%	0.4%	9.4%	4.3%	3.3%	3.3%	22.8%	1.1%	1.4%	2.5%	2.5%	0.7%	0.0%	0.0%	
				11:00	12:00	350	0.3%	23.6%	14.7%	0.6%	10.2%	1.3%	1.3%	1.0%	35.8%	2.9%	1.6%	2.9%	3.2%	0.6%	0.0%	0.0%	
				12:00	13:00	300	0.3%	33.2%	19.4%	0.3%	7.3%	1.4%	1.4%	1.0%	28.5%	3.1%	0.7%	1.4%	1.7%	0.3%	0.0%	0.0%	
				13:00	14:00	350	0.3%	21.6%	12.6%	0.3%	8.6%	4.7%	3.7%	3.7%	26.6%	1.0%	3.3%	5.6%	6.3%	1.7%	0.0%	0.0%	
				14:00	15:00	300	0.3%	21.0%	17.8%	0.3%	8.4%	3.8%	2.8%	2.8%	29.9%	1.4%	2.1%	4.2%	4.2%	1.0%	0.0%	0.0%	
				15:00	16:00	350	0.6%	25.3%	19.1%	0.3%	8.5%	4.1%	3.4%	3.4%	24.1%	2.5%	1.6%	3.1%	3.1%	0.9%	0.0%	0.0%	
				16:00	17:00	350	0.6%	31.6%	17.1%	0.3%	6.0%	1.5%	0.9%	0.9%	25.8%	0.6%	2.7%	5.1%	5.7%	1.2%	0.0%	0.0%	
				17:00	18:00	450	0.7%	40.0%	18.0%	0.2%	4.0%	2.2%	1.7%	1.7%	22.4%	1.5%	1.5%	2.7%	2.7%	0.7%	0.0%	0.0%	
				18:00	19:00	350	0.9%	46.0%	21.3%	0.3%	3.4%	1.6%	1.3%	1.3%	11.3%	1.3%	2.2%	3.8%	4.4%	0.9%	0.0%	0.0%	
				19:00	20:00	300	0.7%	40.2%	21.3%	0.0%	1.4%	0.7%	0.3%	0.3%	30.0%	1.0%	0.7%	1.4%	1.7%	0.3%	0.0%	0.0%	
				20:00	21:00	300	0.4%	29.1%	23.0%	0.0%	1.1%	0.4%	0.4%	0.4%	42.8%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%	
				21:00	22:00	250	0.4%	27.0%	27.0%	0.0%	1.3%	0.4%	0.4%	0.4%	42.7%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	250	0.5%	21.5%	28.0%	0.0%	0.9%	0.0%	0.0%	0.0%	46.2%	0.5%	0.5%	0.9%	0.5%	0.5%	0.0%	0.0%	
23:00	0:00	200	0.6%	20.7%	28.2%	0.0%	1.1%	0.0%	0.0%	0.0%	45.9%	0.6%	0.6%	1.1%	0.6%	0.6%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L097	Sheung Heung Street (EB)	Cheung Ning Street	To Kwa Wan Road	0:00	1:00	150	2.4%	30.4%	32.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	28.8%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%
				1:00	2:00	100	3.6%	41.1%	44.6%	0.0%	3.6%	0.0%	0.0%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.0%	38.8%	49.0%	0.0%	2.0%	0.0%	0.0%	0.0%	8.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	2.9%	44.1%	47.1%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.3%	40.0%	53.4%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.6%	33.3%	43.5%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	2.6%	2.6%	5.1%	2.6%	2.6%	0.0%	0.0%	0.0%
				6:00	7:00	150	1.9%	22.9%	29.5%	0.0%	2.9%	1.0%	0.0%	0.0%	30.3%	1.9%	1.9%	3.8%	2.9%	1.0%	0.0%	0.0%	0.0%
				7:00	8:00	300	1.1%	25.7%	17.2%	0.4%	4.2%	1.1%	0.8%	0.8%	40.7%	1.5%	1.1%	2.3%	2.3%	0.8%	0.0%	0.0%	0.0%
				8:00	9:00	400	2.2%	38.5%	13.1%	0.3%	7.3%	3.6%	2.5%	2.5%	23.2%	1.1%	1.1%	2.0%	2.0%	0.6%	0.0%	0.0%	0.0%
				9:00	10:00	350	1.9%	32.9%	14.8%	0.3%	7.5%	4.4%	2.8%	2.8%	22.6%	1.9%	1.6%	2.8%	2.8%	0.9%	0.0%	0.0%	0.0%
				10:00	11:00	350	2.2%	32.1%	17.0%	0.3%	11.1%	4.6%	3.1%	3.1%	19.4%	0.9%	1.2%	2.2%	2.2%	0.6%	0.0%	0.0%	0.0%
				11:00	12:00	400	1.4%	27.7%	13.2%	0.6%	12.3%	1.7%	1.1%	0.8%	31.4%	2.5%	1.4%	2.5%	2.8%	0.6%	0.0%	0.0%	0.0%
				12:00	13:00	350	1.8%	38.0%	17.0%	0.3%	8.6%	1.5%	1.2%	0.9%	24.4%	2.7%	0.6%	1.2%	1.5%	0.3%	0.0%	0.0%	0.0%
				13:00	14:00	350	1.2%	25.3%	11.1%	0.3%	10.5%	5.2%	3.5%	3.8%	23.3%	0.9%	2.9%	5.0%	5.5%	1.5%	0.0%	0.0%	0.0%
				14:00	15:00	350	1.2%	25.0%	16.0%	0.3%	10.2%	4.0%	2.8%	2.8%	26.3%	1.2%	1.9%	3.7%	3.7%	0.9%	0.0%	0.0%	0.0%
				15:00	16:00	400	1.6%	29.3%	16.8%	0.3%	10.1%	4.6%	3.3%	3.3%	20.9%	2.2%	1.4%	2.7%	2.7%	0.8%	0.0%	0.0%	0.0%
				16:00	17:00	400	2.1%	37.0%	14.9%	0.3%	7.2%	1.5%	0.8%	1.0%	22.1%	0.5%	2.3%	4.4%	4.9%	1.0%	0.0%	0.0%	0.0%
				17:00	18:00	500	2.5%	45.0%	15.4%	0.2%	4.6%	2.3%	1.7%	1.7%	18.8%	1.3%	1.3%	2.3%	2.3%	0.6%	0.0%	0.0%	0.0%
				18:00	19:00	400	2.8%	51.6%	17.9%	0.3%	3.8%	1.5%	1.3%	1.3%	9.2%	1.0%	1.8%	3.1%	3.6%	0.8%	0.0%	0.0%	0.0%
				19:00	20:00	350	2.1%	46.2%	18.5%	0.0%	1.5%	0.6%	0.3%	0.6%	25.7%	0.9%	0.6%	1.2%	1.5%	0.3%	0.0%	0.0%	0.0%
				20:00	21:00	300	1.7%	34.9%	20.8%	0.0%	1.3%	0.3%	0.3%	0.3%	38.5%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	0.0%	0.0%
				21:00	22:00	300	1.6%	32.4%	24.5%	0.0%	2.0%	0.4%	0.4%	0.4%	37.9%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	2.1%	26.3%	26.3%	0.0%	0.8%	0.0%	0.0%	0.0%	42.1%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%	0.0%
				23:00	0:00	200	2.1%	25.1%	26.2%	0.0%	1.6%	0.0%	0.0%	0.0%	42.0%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%	0.0%
L098	Cheung Ning Street (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	50	14.3%	64.3%	14.3%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	12.5%	62.5%	12.5%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	14.3%	71.4%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	14.3%	57.1%	14.3%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	20.0%	60.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	16.7%	49.9%	16.7%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	10.0%	60.0%	20.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	5.9%	67.7%	8.8%	0.0%	14.7%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	100	9.7%	65.2%	4.2%	0.0%	15.3%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	8.8%	61.4%	5.3%	0.0%	17.5%	7.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	9.4%	47.2%	5.7%	0.0%	30.2%	7.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	50	6.0%	48.0%	4.0%	0.0%	38.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	7.7%	59.6%	5.8%	0.0%	25.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	50	6.4%	44.7%	4.3%	0.0%	34.0%	10.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	50	4.7%	44.1%	7.0%	0.0%	34.9%	9.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	7.4%	48.1%	5.6%	0.0%	29.6%	9.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	50	8.7%	54.4%	4.3%	0.0%	28.3%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	100	10.0%	63.3%	5.0%	0.0%	16.7%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	9.6%	67.3%	5.8%	0.0%	13.5%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	50	8.5%	78.7%	6.4%	0.0%	4.3%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	50	11.8%	73.5%	8.8%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	7.4%	74.1%	11.1%	0.0%	7.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	13.6%	68.3%	13.6%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	50	16.7%	61.0%	16.7%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L099	Ha Heung Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	250	3.5%	38.1%	44.5%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	8.4%	0.5%	0.5%	2.0%	1.0%	0.5%	0.0%	0.0%	
				1:00	2:00	150	3.8%	43.4%	51.9%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.1%	39.6%	56.3%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.2%	43.1%	51.3%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.1%	39.1%	54.6%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.6%	35.1%	49.3%	0.0%	1.3%	0.0%	0.0%	0.0%	1.3%	0.0%	1.3%	1.3%	3.9%	2.6%	1.3%	0.0%	0.0%	0.0%
				6:00	7:00	200	2.5%	29.8%	42.9%	0.0%	1.9%	0.6%	0.0%	0.6%	9.3%	1.2%	1.9%	4.3%	3.1%	1.9%	0.0%	0.0%	0.0%	
				7:00	8:00	450	1.9%	41.1%	30.5%	0.0%	1.6%	0.5%	0.9%	0.7%	12.1%	1.4%	1.4%	3.5%	3.0%	1.4%	0.0%	0.0%	0.0%	
				8:00	9:00	700	3.2%	53.9%	20.3%	0.0%	2.6%	1.2%	2.6%	2.6%	6.0%	1.0%	1.0%	2.5%	2.1%	1.0%	0.0%	0.0%	0.0%	
				9:00	10:00	600	2.7%	46.3%	23.2%	0.0%	2.8%	1.3%	3.0%	3.2%	6.0%	1.8%	1.5%	3.5%	3.2%	1.5%	0.0%	0.0%	0.0%	
				10:00	11:00	550	3.0%	41.8%	24.6%	0.2%	5.8%	2.2%	4.4%	4.6%	6.0%	0.8%	1.0%	2.4%	2.2%	1.0%	0.0%	0.0%	0.0%	
				11:00	12:00	500	2.1%	41.6%	21.4%	0.2%	7.4%	0.8%	1.9%	1.6%	10.9%	2.5%	1.4%	3.5%	3.3%	1.4%	0.0%	0.0%	0.0%	
				12:00	13:00	550	2.3%	50.1%	24.7%	0.2%	4.5%	0.8%	1.7%	1.6%	7.6%	2.5%	0.6%	1.4%	1.4%	0.6%	0.0%	0.0%	0.0%	
				13:00	14:00	550	1.8%	35.0%	17.2%	0.2%	5.8%	2.6%	5.4%	5.8%	7.6%	0.8%	2.8%	6.4%	6.0%	2.6%	0.0%	0.0%	0.0%	
				14:00	15:00	500	1.7%	34.7%	24.7%	0.2%	5.7%	2.1%	4.3%	4.5%	8.5%	1.3%	1.9%	4.5%	4.0%	1.9%	0.0%	0.0%	0.0%	
				15:00	16:00	600	2.3%	39.0%	24.5%	0.2%	5.3%	2.1%	4.8%	4.8%	6.4%	2.0%	1.4%	3.2%	2.8%	1.2%	0.0%	0.0%	0.0%	
				16:00	17:00	550	2.6%	45.5%	20.3%	0.2%	5.1%	1.0%	1.8%	1.8%	7.7%	0.4%	2.2%	4.7%	4.9%	1.8%	0.0%	0.0%	0.0%	
				17:00	18:00	700	3.0%	52.5%	19.7%	0.2%	3.2%	1.4%	3.0%	3.0%	6.2%	1.1%	1.1%	2.4%	2.1%	1.1%	0.0%	0.0%	0.0%	
				18:00	19:00	600	3.1%	55.5%	21.3%	0.2%	2.6%	0.9%	1.9%	2.1%	2.7%	0.9%	1.4%	3.1%	3.1%	1.2%	0.0%	0.0%	0.0%	
				19:00	20:00	550	2.6%	57.5%	25.5%	0.0%	0.7%	0.4%	0.4%	0.6%	7.6%	0.7%	0.6%	1.5%	1.3%	0.6%	0.0%	0.0%	0.0%	
				20:00	21:00	450	2.6%	48.8%	32.1%	0.0%	0.7%	0.2%	0.5%	0.7%	12.6%	0.2%	0.2%	0.7%	0.5%	0.2%	0.0%	0.0%	0.0%	
				21:00	22:00	400	2.2%	44.5%	37.6%	0.0%	1.1%	0.3%	0.5%	0.8%	12.4%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	0.0%	
				22:00	23:00	350	3.0%	37.3%	41.3%	0.0%	0.6%	0.0%	0.0%	0.0%	14.2%	0.6%	0.6%	1.2%	0.6%	0.6%	0.0%	0.0%	0.0%	
				23:00	0:00	300	3.0%	36.2%	41.6%	0.0%	0.7%	0.0%	0.0%	0.0%	14.2%	0.7%	0.7%	1.5%	0.7%	0.7%	0.0%	0.0%	0.0%	
L100	Kowloon City Road (SB)	Sheung Heung Street	Lok Shan road	0:00	1:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				1:00	2:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				6:00	7:00	50	0.0%	40.0%	60.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				7:00	8:00	50	0.0%	50.0%	30.0%	5.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	5.0%	0.0%	0.0%		
				8:00	9:00	50	0.0%	61.9%	17.6%	2.9%	5.9%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	0.0%	2.9%	0.0%	0.0%		
				9:00	10:00	50	0.0%	55.3%	20.7%	3.4%	6.9%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	0.0%	3.4%	0.0%	0.0%		
				10:00	11:00	50	0.0%	45.8%	20.8%	4.2%	16.7%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%	0.0%	0.0%		
				11:00	12:00	50	0.0%	41.6%	16.7%	8.3%	20.8%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%	0.0%	4.2%	0.0%	0.0%		
				12:00	13:00	50	0.0%	56.7%	21.7%	4.3%	13.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				13:00	14:00	50	0.0%	39.3%	13.0%	4.3%	17.4%	13.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%	0.0%	8.7%	0.0%	0.0%		
				14:00	15:00	50	0.0%	40.0%	20.0%	5.0%	15.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	5.0%	0.0%	0.0%		
				15:00	16:00	50	0.0%	42.5%	19.2%	3.8%	15.4%	11.5%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	3.8%	0.0%	0.0%		
				16:00	17:00	50	0.0%	50.0%	15.0%	5.0%	20.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.0%	0.0%	0.0%		
				17:00	18:00	50	0.0%	57.8%	15.4%	3.8%	11.5%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%		
				18:00	19:00	50	0.0%	61.0%	17.4%	4.3%	8.7%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%	0.0%	0.0%		
				19:00	20:00	50	0.0%	72.8%	22.7%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				20:00	21:00	50	0.0%	68.7%	31.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				21:00	22:00	50	0.0%	61.5%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				22:00	23:00	50	0.0%	54.5%	45.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				23:00	0:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L101	Kowloon City Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	50	6.5%	43.5%	39.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	2.2%	2.2%	0.0%	0.0%
				1:00	2:00	50	7.7%	46.2%	42.3%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	4.3%	43.5%	47.9%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.9%	47.0%	41.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	6.7%	40.0%	46.6%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	5.3%	36.7%	36.8%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	5.3%	5.3%	0.0%	0.0%
				6:00	7:00	50	5.3%	31.6%	34.2%	0.0%	7.9%	2.6%	0.0%	0.0%	0.0%	2.6%	2.6%	5.3%	5.3%	2.6%	0.0%	0.0%
				7:00	8:00	100	2.4%	43.5%	24.1%	1.2%	9.6%	2.4%	0.0%	0.0%	0.0%	2.4%	2.4%	4.8%	4.8%	2.4%	0.0%	0.0%
				8:00	9:00	200	4.6%	49.8%	13.9%	0.7%	13.2%	7.3%	0.7%	0.7%	0.0%	1.3%	1.3%	2.6%	2.6%	1.3%	0.0%	0.0%
				9:00	10:00	150	3.7%	42.3%	15.6%	0.7%	14.1%	8.9%	0.7%	0.7%	0.0%	2.2%	1.5%	3.7%	3.7%	2.2%	0.0%	0.0%
				10:00	11:00	150	5.0%	38.8%	17.1%	0.7%	20.0%	9.3%	0.7%	0.7%	0.0%	0.7%	1.4%	2.1%	2.1%	1.4%	0.0%	0.0%
				11:00	12:00	150	2.9%	38.2%	14.7%	1.5%	25.7%	3.7%	0.0%	0.0%	0.0%	2.9%	1.5%	3.7%	3.7%	1.5%	0.0%	0.0%
				12:00	13:00	150	3.7%	49.3%	18.4%	0.7%	16.9%	2.9%	0.0%	0.0%	0.0%	3.7%	0.7%	1.5%	1.5%	0.7%	0.0%	0.0%
				13:00	14:00	150	2.8%	31.8%	11.7%	0.7%	20.0%	10.3%	0.7%	1.4%	0.0%	0.7%	3.4%	6.2%	6.9%	3.4%	0.0%	0.0%
				14:00	15:00	150	2.3%	32.8%	17.2%	0.8%	20.3%	9.4%	0.8%	0.8%	0.0%	1.6%	2.3%	4.7%	4.7%	2.3%	0.0%	0.0%
				15:00	16:00	200	3.8%	36.1%	17.3%	0.6%	18.6%	9.6%	0.6%	0.6%	0.0%	2.6%	1.9%	3.2%	3.2%	1.9%	0.0%	0.0%
				16:00	17:00	200	4.4%	45.7%	15.0%	0.6%	14.4%	3.1%	0.0%	0.0%	0.0%	0.6%	3.1%	5.0%	5.6%	2.5%	0.0%	0.0%
				17:00	18:00	250	5.4%	54.4%	15.3%	0.5%	8.9%	5.0%	0.5%	0.5%	0.0%	1.5%	1.5%	2.5%	2.5%	1.5%	0.0%	0.0%
				18:00	19:00	200	5.4%	55.4%	15.8%	0.5%	7.1%	3.3%	0.5%	0.5%	0.0%	1.1%	2.2%	3.3%	3.3%	1.6%	0.0%	0.0%
				19:00	20:00	150	4.7%	63.7%	21.3%	0.0%	3.1%	1.6%	0.0%	0.0%	0.0%	0.8%	0.8%	1.6%	1.6%	0.8%	0.0%	0.0%
				20:00	21:00	100	5.3%	57.3%	28.7%	0.0%	3.2%	1.1%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%
				21:00	22:00	100	3.8%	55.2%	34.6%	0.0%	5.1%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	5.8%	46.6%	37.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	1.4%	1.4%	1.4%	0.0%	0.0%
				23:00	0:00	100	7.0%	43.7%	36.8%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	1.8%	1.8%	1.8%	1.8%	1.8%	0.0%	0.0%
L102	Pau Chung Street (SB)	Sheung Heung Street	Kowloon City Road	0:00	1:00	50	8.0%	60.0%	28.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	6.7%	53.3%	33.3%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	9.1%	54.5%	27.3%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	11.1%	55.6%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	10.0%	50.0%	30.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	5.3%	47.3%	31.6%	0.0%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%
				7:00	8:00	50	4.5%	56.7%	20.5%	2.3%	11.4%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	0.0%
				8:00	9:00	100	7.1%	61.1%	10.6%	1.2%	12.9%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%
				9:00	10:00	100	5.8%	56.7%	13.0%	1.4%	14.5%	7.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				10:00	11:00	100	6.3%	49.2%	12.7%	1.3%	20.3%	8.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				11:00	12:00	100	5.3%	48.8%	11.8%	2.6%	26.3%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				12:00	13:00	100	5.0%	59.8%	13.8%	1.3%	16.3%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				13:00	14:00	100	4.2%	45.8%	9.7%	1.4%	23.6%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%	0.0%	0.0%
				14:00	15:00	100	4.5%	44.7%	14.9%	1.5%	22.4%	9.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%
				15:00	16:00	100	5.8%	47.6%	14.0%	1.2%	19.8%	9.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	0.0%
				16:00	17:00	100	6.5%	58.6%	12.0%	1.1%	15.2%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	0.0%	0.0%
				17:00	18:00	150	7.3%	65.3%	11.3%	0.8%	8.9%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%
				18:00	19:00	150	7.3%	68.2%	11.8%	0.9%	7.3%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%
				19:00	20:00	100	6.3%	73.4%	15.2%	0.0%	2.5%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				20:00	21:00	100	7.0%	68.4%	21.1%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	6.1%	63.3%	24.5%	0.0%	4.1%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	10.0%	57.5%	30.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	9.1%	57.6%	30.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L103	Pau Chung Street (NB)	Kowloon City Road	Sheung Heung Street	0:00	1:00	100	1.8%	42.1%	52.5%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	
				1:00	2:00	50	2.9%	40.0%	54.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	3.1%	37.5%	56.3%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.2%	41.7%	49.9%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	38.1%	57.1%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	37.5%	54.1%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%	0.0%	0.0%
				6:00	7:00	50	2.3%	34.1%	52.3%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	4.5%	0.0%	0.0%
				7:00	8:00	150	1.5%	48.6%	37.7%	0.8%	4.6%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.5%	0.0%	3.8%	0.0%	0.0%
				8:00	9:00	250	2.2%	58.8%	23.3%	0.9%	6.3%	3.6%	0.9%	0.9%	0.0%	0.0%	0.0%	0.9%	0.0%	2.2%	0.0%	0.0%	
				9:00	10:00	200	2.1%	51.6%	27.1%	1.0%	6.8%	4.2%	1.0%	1.0%	0.0%	0.0%	0.0%	1.6%	0.0%	3.6%	0.0%	0.0%	
				10:00	11:00	200	1.9%	43.3%	27.1%	1.9%	14.2%	6.5%	1.3%	1.3%	0.0%	0.0%	0.0%	0.6%	0.0%	1.9%	0.0%	0.0%	
				11:00	12:00	150	1.4%	44.7%	24.5%	2.1%	18.9%	2.8%	0.7%	0.7%	0.0%	0.0%	0.0%	1.4%	0.0%	2.8%	0.0%	0.0%	
				12:00	13:00	200	1.9%	53.1%	27.7%	1.3%	11.0%	1.9%	0.6%	0.6%	0.0%	0.0%	0.0%	0.6%	0.0%	1.3%	0.0%	0.0%	
				13:00	14:00	150	1.4%	39.1%	20.3%	2.1%	15.4%	8.4%	2.1%	2.1%	0.0%	0.0%	0.0%	2.8%	0.0%	6.3%	0.0%	0.0%	
				14:00	15:00	150	1.4%	37.8%	28.3%	2.2%	14.5%	6.5%	1.4%	1.4%	0.0%	0.0%	0.0%	2.2%	0.0%	4.3%	0.0%	0.0%	
				15:00	16:00	200	1.8%	41.3%	27.8%	1.8%	13.0%	6.5%	1.8%	1.8%	0.0%	0.0%	0.0%	1.2%	0.0%	3.0%	0.0%	0.0%	
				16:00	17:00	150	2.3%	47.9%	23.3%	1.5%	14.3%	3.0%	0.8%	0.8%	0.0%	0.0%	0.0%	2.3%	0.0%	3.8%	0.0%	0.0%	
				17:00	18:00	200	2.3%	55.5%	22.3%	1.1%	8.6%	4.6%	1.1%	1.1%	0.0%	0.0%	0.0%	1.1%	0.0%	2.3%	0.0%	0.0%	
				18:00	19:00	200	2.6%	58.1%	23.9%	0.6%	6.5%	3.2%	0.6%	0.6%	0.0%	0.0%	0.0%	1.3%	0.0%	2.6%	0.0%	0.0%	
				19:00	20:00	200	1.9%	63.8%	29.9%	0.0%	1.9%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	1.3%	0.0%	0.0%	
				20:00	21:00	150	1.7%	56.3%	39.5%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%	
				21:00	22:00	150	1.9%	50.1%	44.3%	0.0%	2.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	100	2.2%	44.4%	51.2%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	
23:00	0:00	100	2.7%	42.5%	50.7%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%					
L104	Ma Tau Wai Road (SB)	Sheung Heung Street	Lok Shan road	0:00	1:00	400	4.0%	23.9%	47.3%	0.0%	2.4%	0.0%	0.0%	0.0%	12.4%	0.5%	0.3%	1.1%	0.5%	0.3%	0.0%	7.3%	
				1:00	2:00	200	4.4%	29.4%	60.1%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	
				2:00	3:00	200	4.2%	26.7%	64.3%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	
				3:00	4:00	150	5.6%	28.8%	57.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	
				4:00	5:00	150	4.5%	26.1%	61.3%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%	0.0%	3.6%	
				5:00	6:00	150	3.9%	24.4%	59.0%	0.0%	5.5%	0.0%	0.0%	0.8%	0.0%	0.8%	0.8%	2.4%	1.6%	0.8%	0.0%	0.0%	
				6:00	7:00	350	2.5%	17.8%	43.4%	0.6%	4.1%	1.0%	0.0%	0.3%	13.7%	1.0%	1.0%	2.2%	2.2%	1.0%	0.0%	9.2%	
				7:00	8:00	1,000	2.0%	23.7%	27.4%	0.7%	5.2%	1.3%	0.4%	0.3%	17.4%	1.0%	0.9%	2.0%	2.0%	0.6%	0.0%	15.1%	
				8:00	9:00	1,400	3.9%	34.6%	20.3%	0.9%	8.7%	4.8%	1.3%	1.3%	9.7%	0.8%	0.8%	1.5%	1.5%	0.5%	0.0%	9.4%	
				9:00	10:00	1,300	3.0%	28.5%	22.4%	0.9%	9.0%	5.5%	1.4%	1.5%	9.2%	1.4%	1.0%	2.1%	2.2%	0.7%	0.0%	11.2%	
				10:00	11:00	1,000	3.3%	24.5%	24.1%	1.5%	14.3%	6.5%	1.7%	1.7%	8.4%	0.5%	0.6%	1.3%	1.4%	0.4%	0.0%	9.8%	
				11:00	12:00	1,050	2.2%	22.6%	20.0%	2.0%	17.4%	2.5%	0.7%	0.6%	14.5%	1.5%	0.9%	1.9%	2.0%	0.6%	0.0%	10.6%	
				12:00	13:00	1,000	2.8%	30.9%	25.6%	1.4%	11.8%	2.3%	0.7%	0.6%	11.1%	1.8%	0.4%	0.8%	0.9%	0.3%	0.0%	8.6%	
				13:00	14:00	1,050	2.0%	20.3%	16.7%	1.7%	14.3%	7.8%	2.0%	2.1%	10.5%	0.5%	1.8%	3.5%	3.8%	1.1%	0.0%	11.9%	
				14:00	15:00	1,000	1.8%	19.6%	23.7%	1.6%	13.7%	6.1%	1.6%	1.6%	11.6%	0.8%	1.1%	2.4%	2.6%	0.8%	0.0%	11.0%	
				15:00	16:00	1,150	2.5%	22.8%	24.4%	1.5%	13.3%	6.8%	1.8%	1.8%	9.1%	1.3%	0.9%	1.8%	1.8%	0.5%	0.0%	9.7%	
				16:00	17:00	850	3.0%	28.5%	23.5%	1.3%	10.6%	2.4%	0.6%	0.5%	10.7%	0.2%	1.3%	2.8%	3.1%	0.8%	0.0%	10.7%	
				17:00	18:00	1,050	3.8%	34.7%	24.4%	1.0%	6.9%	3.9%	1.1%	1.0%	9.3%	0.7%	0.7%	1.5%	1.5%	0.5%	0.0%	9.0%	
				18:00	19:00	850	4.0%	39.0%	27.7%	0.7%	5.8%	2.7%	0.7%	0.7%	4.5%	0.6%	1.0%	2.0%	2.3%	0.6%	0.0%	7.7%	
				19:00	20:00	950	3.2%	39.0%	29.1%	0.2%	2.3%	1.0%	0.2%	0.3%	12.2%	0.5%	0.4%	0.9%	1.0%	0.3%	0.0%	9.4%	
				20:00	21:00	850	3.1%	30.2%	33.8%	0.2%	1.9%	0.4%	0.2%	0.2%	18.6%	0.2%	0.1%	0.4%	0.2%	0.1%	0.0%	10.4%	
				21:00	22:00	750	2.3%	26.3%	37.5%	0.3%	2.5%	0.8%	0.3%	0.3%	17.4%	0.0%	0.0%	0.1%	0.0%	0.1%	0.0%	12.1%	
				22:00	23:00	750	3.1%	20.4%	37.7%	0.3%	1.4%	0.0%	0.0%	0.0%	18.4%	0.4%	0.3%	0.6%	0.3%	0.3%	0.0%	16.8%	
23:00	0:00	600	3.3%	19.7%	37.8%	0.3%	1.7%	0.0%	0.0%	0.0%	18.4%	0.5%	0.3%	0.7%	0.3%	0.3%	0.0%	16.7%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L105	Ma Tau Wai Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	300	6.9%	16.5%	45.8%	0.0%	1.4%	0.0%	0.0%	0.0%	18.2%	0.3%	0.3%	1.4%	0.7%	0.3%	0.0%	8.2%
				1:00	2:00	150	8.3%	21.2%	62.2%	0.0%	2.3%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%
				2:00	3:00	150	7.3%	19.4%	65.3%	0.0%	1.6%	0.0%	0.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%
				3:00	4:00	100	10.1%	21.3%	60.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%
				4:00	5:00	100	8.6%	19.8%	63.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	4.9%
				5:00	6:00	100	7.6%	18.5%	61.8%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	3.3%	2.2%	1.1%	0.0%	0.0%
				6:00	7:00	250	4.5%	12.1%	41.9%	0.4%	2.4%	0.4%	0.0%	0.0%	19.8%	1.2%	1.2%	2.4%	2.0%	0.8%	0.0%	10.9%
				7:00	8:00	750	2.1%	14.2%	34.2%	0.7%	1.9%	0.4%	0.1%	0.1%	27.0%	0.7%	0.7%	1.4%	1.2%	0.6%	0.0%	14.7%
				8:00	9:00	900	4.8%	24.4%	29.9%	1.1%	3.9%	1.8%	0.7%	0.7%	17.6%	0.7%	0.7%	1.3%	1.1%	0.5%	0.0%	10.8%
				9:00	10:00	850	3.7%	19.9%	32.7%	1.1%	4.0%	2.1%	0.7%	0.7%	16.6%	1.1%	0.9%	1.7%	1.6%	0.6%	0.0%	12.6%
				10:00	11:00	700	6.5%	18.9%	26.2%	2.0%	8.8%	3.8%	0.9%	0.9%	13.7%	0.7%	0.9%	1.6%	1.6%	0.6%	0.0%	12.9%
				11:00	12:00	750	3.9%	16.8%	20.8%	2.4%	10.2%	1.3%	0.3%	0.3%	22.7%	1.9%	1.1%	2.2%	2.2%	0.7%	0.0%	13.2%
				12:00	13:00	700	5.2%	23.3%	27.0%	1.9%	7.1%	1.3%	0.3%	0.3%	17.8%	2.2%	0.4%	1.0%	1.0%	0.3%	0.0%	10.9%
				13:00	14:00	750	3.8%	15.6%	17.9%	2.1%	8.8%	4.4%	1.0%	1.1%	17.1%	0.6%	2.3%	4.1%	4.4%	1.4%	0.0%	15.4%
				14:00	15:00	700	3.3%	14.8%	25.3%	2.0%	8.2%	3.3%	0.7%	0.7%	18.5%	1.0%	1.4%	2.9%	2.9%	1.0%	0.0%	14.0%
				15:00	16:00	800	4.9%	17.5%	26.5%	1.9%	8.1%	3.9%	0.9%	0.9%	15.0%	1.5%	1.2%	2.2%	2.1%	0.8%	0.0%	12.6%
				16:00	17:00	750	7.7%	22.1%	15.4%	1.9%	8.5%	1.9%	0.3%	0.3%	14.5%	0.4%	2.1%	4.1%	4.7%	1.3%	0.0%	14.8%
				17:00	18:00	850	9.9%	28.4%	16.6%	1.3%	5.8%	3.2%	0.5%	0.5%	13.0%	1.2%	1.2%	2.3%	2.3%	0.8%	0.0%	13.0%
				18:00	19:00	700	10.9%	32.6%	19.3%	0.9%	5.1%	2.2%	0.3%	0.3%	6.4%	1.0%	1.8%	3.1%	3.6%	1.0%	0.0%	11.5%
				19:00	20:00	700	5.9%	28.4%	29.8%	0.1%	1.3%	0.6%	0.1%	0.1%	18.8%	0.6%	0.4%	1.0%	1.0%	0.3%	0.0%	11.6%
				20:00	21:00	650	5.3%	20.6%	32.0%	0.2%	0.9%	0.2%	0.2%	0.2%	27.0%	0.3%	0.2%	0.5%	0.3%	0.2%	0.0%	11.9%
				21:00	22:00	600	4.0%	18.0%	36.0%	0.2%	1.4%	0.3%	0.2%	0.2%	25.3%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	14.0%
				22:00	23:00	600	4.9%	13.3%	34.8%	0.2%	0.7%	0.0%	0.0%	0.0%	25.6%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	18.6%
				23:00	0:00	500	5.2%	12.8%	34.6%	0.2%	0.8%	0.0%	0.0%	0.0%	25.6%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	18.4%
L106	Maidstone Road (SB)	Maidstone Lane	Lok Shan road	0:00	1:00	50	14.6%	47.9%	20.8%	0.0%	2.1%	0.0%	0.0%	0.0%	10.4%	0.0%	0.0%	2.1%	0.0%	2.1%	0.0%	0.0%
				1:00	2:00	50	15.4%	53.9%	26.9%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	13.6%	54.6%	27.3%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	17.6%	53.0%	23.5%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	14.3%	57.1%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	11.8%	47.0%	29.4%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.9%	0.0%	0.0%
				6:00	7:00	50	11.1%	41.6%	22.2%	0.0%	5.6%	0.0%	0.0%	0.0%	13.9%	0.0%	0.0%	2.8%	0.0%	2.8%	0.0%	0.0%
				7:00	8:00	150	7.4%	53.2%	15.6%	0.0%	4.1%	0.8%	0.0%	0.0%	13.9%	0.0%	0.0%	2.5%	0.0%	2.5%	0.0%	0.0%
				8:00	9:00	250	12.0%	61.6%	9.2%	0.0%	5.1%	2.3%	0.5%	0.5%	6.0%	0.0%	0.0%	1.4%	0.0%	1.4%	0.0%	0.0%
				9:00	10:00	200	10.3%	57.6%	11.4%	0.0%	5.7%	2.9%	0.6%	0.6%	6.3%	0.0%	0.0%	2.3%	0.0%	2.3%	0.0%	0.0%
				10:00	11:00	150	11.4%	48.4%	11.4%	0.0%	12.9%	4.5%	0.8%	0.8%	6.8%	0.0%	0.0%	1.5%	0.0%	1.5%	0.0%	0.0%
				11:00	12:00	150	7.8%	47.6%	9.4%	0.0%	16.4%	1.6%	0.0%	0.0%	13.3%	0.0%	0.0%	2.3%	0.0%	1.6%	0.0%	0.0%
				12:00	13:00	150	8.8%	58.2%	11.0%	0.0%	10.3%	1.5%	0.0%	0.0%	8.8%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				13:00	14:00	150	7.4%	44.4%	8.2%	0.0%	13.9%	5.7%	0.8%	1.6%	9.8%	0.0%	0.0%	4.1%	0.0%	4.1%	0.0%	0.0%
				14:00	15:00	150	7.0%	43.4%	12.2%	0.0%	13.9%	5.2%	0.9%	0.9%	11.3%	0.0%	0.0%	2.6%	0.0%	2.6%	0.0%	0.0%
				15:00	16:00	150	9.2%	47.6%	11.3%	0.0%	12.8%	5.0%	0.7%	0.7%	8.5%	0.0%	0.0%	2.1%	0.0%	2.1%	0.0%	0.0%
				16:00	17:00	150	9.8%	50.0%	8.0%	0.0%	13.4%	2.7%	0.0%	0.0%	10.7%	0.0%	0.0%	2.7%	0.0%	2.7%	0.0%	0.0%
				17:00	18:00	150	10.8%	56.6%	8.1%	0.0%	8.1%	3.4%	0.7%	0.7%	8.8%	0.0%	0.0%	1.4%	0.0%	1.4%	0.0%	0.0%
				18:00	19:00	150	11.2%	62.4%	8.8%	0.0%	6.4%	2.4%	0.8%	0.8%	4.0%	0.0%	0.0%	1.6%	0.0%	1.6%	0.0%	0.0%
				19:00	20:00	150	9.7%	66.0%	11.1%	0.0%	2.1%	0.7%	0.0%	0.0%	9.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				20:00	21:00	150	9.9%	57.7%	14.4%	0.0%	1.8%	0.0%	0.0%	0.0%	15.3%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%
				21:00	22:00	100	8.8%	54.9%	17.6%	0.0%	2.2%	1.1%	0.0%	0.0%	15.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	12.2%	46.4%	19.5%	0.0%	1.2%	0.0%	0.0%	0.0%	18.3%	0.0%	0.0%	1.2%	0.0%	1.2%	0.0%	0.0%
				23:00	0:00	100	11.9%	44.8%	20.9%	0.0%	1.5%	0.0%	0.0%	0.0%	17.9%	0.0%	0.0%	1.5%	0.0%	1.5%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L107	Kau Pui Lung Road (SB)	Maidstone Lane	Lok Shan road	0:00	1:00	200	2.6%	31.6%	58.4%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	1.3%	0.7%	0.0%	0.0%
				1:00	2:00	100	3.4%	31.5%	61.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	2.4%	29.3%	65.9%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.4%	32.2%	61.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.7%	29.6%	64.8%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.0%	25.4%	56.6%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	3.0%	3.0%	1.5%	0.0%	0.0%
				6:00	7:00	150	2.4%	23.8%	54.6%	0.0%	4.0%	0.8%	0.0%	0.0%	0.0%	0.0%	1.6%	2.4%	4.8%	4.0%	1.6%	0.0%	0.0%
				7:00	8:00	350	1.7%	35.0%	41.1%	0.3%	5.9%	1.7%	0.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.3%	4.3%	1.7%	0.0%	0.0%
				8:00	9:00	500	2.9%	45.3%	27.3%	0.4%	8.9%	4.8%	0.4%	0.4%	0.0%	0.0%	1.4%	1.4%	2.9%	2.9%	1.0%	0.0%	0.0%
				9:00	10:00	450	2.3%	37.2%	30.1%	0.5%	9.0%	5.4%	0.5%	0.5%	0.0%	0.0%	2.5%	2.0%	4.1%	4.3%	1.6%	0.0%	0.0%
				10:00	11:00	400	2.8%	34.1%	31.5%	0.5%	14.7%	6.5%	0.5%	0.5%	0.0%	0.0%	1.0%	1.3%	2.8%	2.8%	1.0%	0.0%	0.0%
				11:00	12:00	400	1.9%	33.1%	27.5%	0.8%	18.7%	2.7%	0.3%	0.3%	0.0%	0.0%	3.2%	1.9%	4.0%	4.3%	1.3%	0.0%	0.0%
				12:00	13:00	400	2.1%	41.8%	32.8%	0.5%	12.0%	2.1%	0.3%	0.3%	0.0%	0.0%	3.4%	0.8%	1.6%	1.8%	0.5%	0.0%	0.0%
				13:00	14:00	400	1.6%	28.8%	22.4%	0.5%	15.0%	7.9%	0.5%	0.5%	0.0%	0.0%	1.1%	3.7%	7.4%	8.2%	2.4%	0.0%	0.0%
				14:00	15:00	400	1.4%	27.9%	32.0%	0.5%	14.5%	6.3%	0.5%	0.5%	0.0%	0.0%	1.6%	2.5%	5.2%	5.5%	1.6%	0.0%	0.0%
				15:00	16:00	450	2.1%	31.4%	31.8%	0.5%	13.6%	6.7%	0.5%	0.5%	0.0%	0.0%	2.5%	1.8%	3.7%	3.7%	1.2%	0.0%	0.0%
				16:00	17:00	400	2.6%	38.7%	27.6%	0.3%	10.8%	2.3%	0.0%	0.0%	0.0%	0.0%	0.5%	2.8%	5.9%	6.7%	1.8%	0.0%	0.0%
				17:00	18:00	500	3.1%	47.1%	28.3%	0.2%	6.9%	3.8%	0.2%	0.2%	0.0%	0.0%	1.5%	1.5%	3.1%	3.1%	1.0%	0.0%	0.0%
				18:00	19:00	450	3.0%	47.6%	29.1%	0.2%	5.2%	2.3%	0.2%	0.2%	0.0%	0.0%	1.1%	1.8%	3.9%	4.3%	1.1%	0.0%	0.0%
				19:00	20:00	400	2.4%	51.6%	36.6%	0.0%	2.1%	1.1%	0.0%	0.0%	0.0%	0.0%	1.1%	0.8%	1.9%	1.9%	0.5%	0.0%	0.0%
				20:00	21:00	300	2.8%	45.0%	47.5%	0.0%	2.1%	0.3%	0.0%	0.0%	0.0%	0.0%	0.3%	0.3%	0.7%	0.7%	0.3%	0.0%	0.0%
				21:00	22:00	300	1.9%	40.1%	53.7%	0.0%	2.7%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%
				22:00	23:00	250	3.0%	32.8%	57.6%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	1.3%	0.9%	0.9%	0.0%	0.0%
				23:00	0:00	200	3.1%	31.4%	57.8%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.6%	1.0%	1.0%	0.0%	0.0%
L108	Kau Pui Lung Road (NB)	Lok Shan road	Maidstone Lane	0:00	1:00	150	1.8%	19.5%	67.1%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	4.4%	0.9%	0.0%	1.8%	0.9%	0.9%	0.0%	0.0%
				1:00	2:00	100	1.6%	20.6%	74.6%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	1.7%	18.6%	78.0%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	2.3%	20.9%	72.1%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.6%	18.4%	76.4%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.2%	17.4%	69.5%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	2.2%	2.2%	0.0%	0.0%
				6:00	7:00	100	1.1%	15.1%	63.3%	0.0%	4.3%	1.1%	0.0%	0.0%	5.4%	1.1%	1.1%	3.2%	3.2%	1.1%	0.0%	0.0%	0.0%
				7:00	8:00	250	0.9%	23.5%	50.7%	0.5%	4.7%	1.4%	0.0%	0.0%	8.0%	1.4%	1.4%	3.3%	2.8%	1.4%	0.0%	0.0%	0.0%
				8:00	9:00	350	2.0%	34.6%	37.9%	0.3%	8.0%	4.3%	0.3%	0.3%	4.3%	1.0%	1.0%	2.7%	2.3%	1.0%	0.0%	0.0%	0.0%
				9:00	10:00	300	1.4%	27.9%	41.0%	0.4%	8.2%	5.0%	0.4%	0.4%	3.9%	1.8%	1.4%	3.6%	3.2%	1.4%	0.0%	0.0%	0.0%
				10:00	11:00	300	1.5%	22.9%	38.8%	0.4%	17.7%	7.9%	0.4%	0.4%	3.4%	0.8%	0.8%	2.3%	1.9%	0.8%	0.0%	0.0%	0.0%
				11:00	12:00	300	1.1%	22.2%	33.8%	0.8%	22.2%	3.1%	0.0%	0.0%	6.5%	1.9%	1.1%	3.1%	3.1%	1.1%	0.0%	0.0%	0.0%
				12:00	13:00	300	1.2%	29.0%	41.6%	0.4%	14.7%	2.7%	0.0%	0.0%	4.6%	2.3%	0.4%	1.2%	1.5%	0.4%	0.0%	0.0%	0.0%
				13:00	14:00	300	1.2%	19.8%	27.8%	0.4%	18.2%	9.7%	0.4%	0.8%	4.7%	0.8%	2.3%	5.8%	5.8%	2.3%	0.0%	0.0%	0.0%
				14:00	15:00	300	0.8%	18.2%	38.1%	0.4%	17.1%	7.4%	0.4%	0.4%	5.0%	1.2%	1.6%	3.9%	3.9%	1.6%	0.0%	0.0%	0.0%
				15:00	16:00	300	1.3%	21.0%	39.1%	0.3%	16.3%	8.0%	0.3%	0.3%	4.0%	1.7%	1.0%	3.0%	2.7%	1.0%	0.0%	0.0%	0.0%
				16:00	17:00	300	1.5%	25.6%	33.3%	0.4%	18.1%	4.1%	0.0%	0.0%	4.4%	0.4%	1.9%	4.4%	4.4%	1.5%	0.0%	0.0%	0.0%
				17:00	18:00	350	1.9%	32.1%	35.2%	0.3%	12.0%	6.5%	0.3%	0.3%	4.0%	0.9%	0.9%	2.5%	2.2%	0.9%	0.0%	0.0%	0.0%
				18:00	19:00	300	1.8%	33.9%	38.0%	0.4%	9.5%	4.2%	0.4%	0.4%	1.8%	0.7%	1.4%	3.2%	3.2%	1.1%	0.0%	0.0%	0.0%
				19:00	20:00	250	1.6%	36.8%	47.5%	0.0%	2.8%	1.2%	0.0%	0.0%	5.3%	0.8%	0.4%	1.6%	1.6%	0.4%	0.0%	0.0%	0.0%
				20:00	21:00	250	1.4%	29.3%	56.2%	0.0%	2.4%	0.5%	0.0%	0.0%	8.2%	0.5%	0.0%	0.5%	0.5%	0.5%	0.0%	0.0%	0.0%
				21:00	22:00	200	1.0%	25.1%	62.0%	0.0%	3.1%	1.0%	0.0%	0.0%	7.3%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	200	1.7%	20.1%	64.6%	0.0%	1.7%	0.0%	0.0%	0.0%	8.4%	0.6%	0.6%	1.1%	0.6%	0.6%	0.0%	0.0%	0.0%
				23:00	0:00	150	2.1%	19.9%	63.5%	0.0%	2.1%	0.0%	0.0%	0.0%	8.2%	0.7%	0.7%	1.4%	0.7%	0.7%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L109	Lok Shan road (EB)	Kau Pui Lung Road	Maidstone Road	0:00	1:00	100	2.9%	32.9%	55.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	1.4%	1.4%	0.0%	0.0%	
				1:00	2:00	50	2.5%	35.0%	60.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.8%	30.6%	63.8%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	3.7%	33.3%	59.3%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	4.2%	29.2%	62.4%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	3.4%	27.6%	55.4%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	3.4%	3.4%	0.0%	0.0%
				6:00	7:00	100	1.8%	25.5%	54.7%	0.0%	3.6%	1.8%	0.0%	0.0%	0.0%	1.8%	1.8%	3.6%	3.6%	1.8%	0.0%	0.0%	
				7:00	8:00	150	1.4%	36.8%	39.4%	0.7%	4.9%	1.4%	0.7%	0.7%	0.0%	2.1%	2.1%	3.5%	4.2%	2.1%	0.0%	0.0%	
				8:00	9:00	250	2.6%	47.1%	25.8%	0.4%	7.7%	3.9%	1.7%	1.7%	0.0%	1.3%	1.3%	2.6%	2.6%	1.3%	0.0%	0.0%	
				9:00	10:00	250	1.9%	39.2%	28.4%	0.5%	8.1%	4.3%	1.9%	1.9%	0.0%	2.4%	1.9%	3.8%	3.8%	1.9%	0.0%	0.0%	
				10:00	11:00	200	2.2%	34.9%	29.1%	0.5%	13.7%	6.0%	2.7%	2.7%	0.0%	1.1%	1.1%	2.2%	2.7%	1.1%	0.0%	0.0%	
				11:00	12:00	200	1.7%	34.7%	26.0%	1.2%	17.9%	2.3%	1.2%	1.2%	0.0%	2.9%	1.7%	3.5%	4.0%	1.7%	0.0%	0.0%	
				12:00	13:00	200	1.7%	43.2%	30.9%	0.6%	11.2%	2.2%	1.1%	1.1%	0.0%	3.4%	0.6%	1.7%	1.7%	0.6%	0.0%	0.0%	
				13:00	14:00	200	1.6%	28.7%	20.1%	0.5%	14.1%	7.1%	3.3%	3.3%	0.0%	1.1%	3.3%	6.5%	7.1%	3.3%	0.0%	0.0%	
				14:00	15:00	200	1.2%	28.7%	29.3%	0.6%	14.0%	5.8%	2.3%	2.3%	0.0%	1.8%	2.3%	4.7%	4.7%	2.3%	0.0%	0.0%	
				15:00	16:00	250	2.0%	31.6%	29.3%	0.5%	12.7%	5.9%	2.9%	2.9%	0.0%	2.4%	1.5%	3.4%	3.4%	1.5%	0.0%	0.0%	
				16:00	17:00	200	2.3%	39.1%	25.0%	0.6%	11.4%	2.3%	1.1%	1.1%	0.0%	0.6%	2.8%	5.1%	6.3%	2.3%	0.0%	0.0%	
				17:00	18:00	250	2.7%	47.2%	25.5%	0.5%	7.3%	3.6%	1.8%	1.8%	0.0%	1.4%	1.4%	2.7%	2.7%	1.4%	0.0%	0.0%	
				18:00	19:00	200	2.5%	48.4%	26.6%	0.5%	5.5%	2.5%	1.0%	1.0%	0.0%	1.0%	2.0%	3.5%	4.0%	1.5%	0.0%	0.0%	
				19:00	20:00	200	2.3%	53.7%	34.3%	0.0%	2.3%	1.1%	0.0%	0.6%	0.0%	1.1%	0.6%	1.7%	1.7%	0.6%	0.0%	0.0%	
				20:00	21:00	150	2.2%	47.4%	44.0%	0.0%	2.2%	0.7%	0.0%	0.7%	0.0%	0.7%	0.0%	0.7%	0.7%	0.7%	0.0%	0.0%	
				21:00	22:00	150	1.7%	42.2%	51.7%	0.0%	2.6%	0.9%	0.0%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	150	2.8%	34.9%	55.9%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	0.9%	0.9%	0.9%	0.9%	0.0%	0.0%
				23:00	0:00	100	3.4%	33.3%	55.5%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	1.1%	1.1%	1.1%	0.0%	0.0%	
L110	Lok Shan road (EB)	Maidstone Road	Ma Tau Wai Road	0:00	1:00	100	13.7%	35.6%	46.6%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	
				1:00	2:00	50	14.0%	34.9%	48.8%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	12.5%	32.5%	52.5%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	16.7%	33.3%	46.7%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	12.0%	32.0%	52.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	10.3%	31.0%	51.9%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	0.0%	0.0%	
				6:00	7:00	100	10.9%	29.1%	49.1%	0.0%	5.5%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.6%	0.0%	0.0%	
				7:00	8:00	150	8.4%	41.9%	35.9%	0.0%	6.9%	1.5%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%	
				8:00	9:00	250	12.7%	48.0%	20.7%	0.0%	9.3%	4.6%	1.3%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%	
				9:00	10:00	250	10.4%	42.5%	24.3%	0.0%	10.4%	5.9%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	3.5%	0.0%	0.0%	
				10:00	11:00	200	11.9%	36.1%	24.2%	0.0%	16.0%	6.7%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%	
				11:00	12:00	200	8.7%	38.9%	23.3%	0.0%	22.1%	2.9%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	0.0%	0.0%	
				12:00	13:00	200	9.6%	46.6%	26.2%	0.0%	13.4%	2.1%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	
				13:00	14:00	200	8.3%	34.9%	19.5%	0.0%	18.3%	8.9%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%	
				14:00	15:00	200	6.7%	33.6%	27.4%	0.0%	17.7%	7.3%	1.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	3.7%	0.0%	0.0%	
				15:00	16:00	250	9.3%	35.5%	25.9%	0.0%	15.6%	7.3%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	0.0%	
				16:00	17:00	200	11.8%	44.7%	23.1%	0.0%	12.9%	2.7%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%	
				17:00	18:00	300	13.1%	50.0%	21.8%	0.0%	7.5%	4.0%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%	
				18:00	19:00	250	13.3%	52.1%	23.1%	0.0%	5.8%	2.7%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	0.0%	0.0%	
				19:00	20:00	200	11.2%	55.7%	28.2%	0.0%	2.7%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	
				20:00	21:00	150	11.7%	48.2%	36.6%	0.0%	2.1%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	
				21:00	22:00	150	8.7%	43.6%	42.9%	0.0%	3.2%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%	
				22:00	23:00	150	13.2%	36.8%	46.4%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	
				23:00	0:00	100	14.1%	34.8%	46.7%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L111	Lok Shan road (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	100	3.1%	40.0%	47.8%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	1.5%	0.0%	1.5%	1.5%	1.5%	0.0%	0.0%
				1:00	2:00	50	5.3%	39.5%	49.9%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.9%	38.2%	56.0%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	3.8%	38.5%	50.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	4.5%	36.4%	54.6%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	3.6%	32.1%	46.4%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.6%	3.6%	3.6%	0.0%	0.0%
				6:00	7:00	100	3.7%	29.6%	44.3%	0.0%	7.4%	1.9%	0.0%	0.0%	0.0%	1.9%	1.9%	3.7%	3.7%	1.9%	0.0%	0.0%
				7:00	8:00	150	2.1%	43.3%	32.9%	0.7%	7.0%	1.4%	0.0%	0.0%	0.0%	2.1%	2.1%	3.5%	2.8%	2.1%	0.0%	0.0%
				8:00	9:00	250	3.7%	52.0%	20.4%	0.4%	9.4%	4.9%	0.8%	0.8%	0.0%	1.2%	1.2%	2.4%	1.6%	1.2%	0.0%	0.0%
				9:00	10:00	250	2.8%	44.0%	22.9%	0.5%	10.1%	6.0%	0.9%	0.9%	0.0%	2.3%	1.8%	3.7%	2.3%	1.8%	0.0%	0.0%
				10:00	11:00	200	3.1%	36.2%	22.0%	0.5%	19.9%	8.4%	1.6%	1.6%	0.0%	1.0%	1.0%	2.1%	1.6%	1.0%	0.0%	0.0%
				11:00	12:00	200	2.2%	35.8%	19.5%	1.1%	25.4%	3.2%	0.5%	0.5%	0.0%	2.7%	1.6%	3.2%	2.7%	1.6%	0.0%	0.0%
				12:00	13:00	200	2.7%	46.1%	23.8%	0.5%	16.8%	2.7%	0.5%	0.5%	0.0%	3.2%	0.5%	1.1%	1.1%	0.5%	0.0%	0.0%
				13:00	14:00	200	2.1%	30.3%	15.6%	0.5%	19.8%	9.9%	2.1%	2.1%	0.0%	1.0%	3.1%	5.7%	4.7%	3.1%	0.0%	0.0%
				14:00	15:00	200	1.7%	30.1%	22.3%	0.6%	20.1%	8.4%	1.7%	1.7%	0.0%	1.7%	2.2%	3.9%	3.4%	2.2%	0.0%	0.0%
				15:00	16:00	250	2.3%	33.9%	22.5%	0.5%	18.3%	8.5%	1.9%	1.9%	0.0%	2.3%	1.4%	2.8%	2.3%	1.4%	0.0%	0.0%
				16:00	17:00	200	2.8%	39.5%	18.8%	0.6%	19.3%	3.9%	0.6%	0.6%	0.0%	0.6%	2.8%	4.4%	3.9%	2.2%	0.0%	0.0%
				17:00	18:00	250	3.5%	48.2%	18.9%	0.4%	12.3%	6.2%	1.3%	1.3%	0.0%	1.3%	1.3%	2.2%	1.8%	1.3%	0.0%	0.0%
				18:00	19:00	250	3.5%	49.8%	20.3%	0.5%	9.9%	4.0%	1.0%	1.0%	0.0%	1.0%	2.0%	3.0%	2.5%	1.5%	0.0%	0.0%
				19:00	20:00	200	3.4%	59.4%	27.6%	0.0%	3.4%	1.1%	0.0%	0.0%	0.0%	1.1%	0.6%	1.7%	1.1%	0.6%	0.0%	0.0%
				20:00	21:00	150	3.8%	52.5%	36.6%	0.0%	3.1%	0.8%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.8%	0.8%	0.0%	0.0%
				21:00	22:00	150	2.7%	48.6%	42.5%	0.0%	4.4%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	4.0%	41.0%	47.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%
				23:00	0:00	100	4.9%	39.0%	46.4%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%
L112	Lok Shan road (WB)	Cheung Ning Street	Ha Heung Road	0:00	1:00	200	1.7%	34.7%	46.2%	0.0%	1.7%	0.0%	0.0%	0.0%	9.8%	0.6%	0.6%	2.3%	1.2%	1.2%	0.0%	0.0%
				1:00	2:00	100	2.3%	39.8%	55.6%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	1.2%	36.6%	59.8%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	1.7%	40.0%	55.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	1.9%	35.8%	58.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.5%	31.3%	50.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	1.5%	1.5%	4.5%	3.0%	3.0%	0.0%	0.0%
				6:00	7:00	150	1.4%	25.9%	43.3%	0.0%	3.5%	0.7%	0.0%	0.0%	10.5%	2.1%	2.1%	4.9%	3.5%	2.1%	0.0%	0.0%
				7:00	8:00	400	1.0%	36.6%	31.2%	0.3%	3.0%	0.8%	0.3%	0.3%	13.2%	1.8%	1.8%	4.1%	3.8%	1.8%	0.0%	0.0%
				8:00	9:00	600	1.7%	50.5%	21.7%	0.2%	5.1%	2.7%	0.8%	0.8%	6.9%	1.3%	1.3%	3.0%	2.7%	1.3%	0.0%	0.0%
				9:00	10:00	550	1.3%	42.4%	24.5%	0.2%	5.3%	3.2%	0.9%	0.9%	6.8%	2.4%	1.9%	4.3%	4.0%	1.9%	0.0%	0.0%
				10:00	11:00	450	1.6%	37.6%	25.2%	0.5%	11.5%	5.0%	1.4%	1.6%	6.9%	0.9%	1.1%	2.8%	2.5%	1.4%	0.0%	0.0%
				11:00	12:00	450	0.9%	35.0%	20.7%	0.7%	14.0%	1.8%	0.7%	0.5%	11.9%	2.9%	1.6%	4.1%	3.6%	1.6%	0.0%	0.0%
				12:00	13:00	450	1.3%	45.1%	25.2%	0.4%	8.9%	1.6%	0.7%	0.4%	8.7%	3.1%	0.7%	1.6%	1.6%	0.7%	0.0%	0.0%
				13:00	14:00	450	0.9%	30.8%	17.0%	0.4%	11.2%	5.8%	1.8%	1.8%	8.5%	0.9%	3.4%	7.4%	7.0%	3.1%	0.0%	0.0%
				14:00	15:00	450	0.7%	30.2%	24.7%	0.5%	11.2%	4.8%	1.4%	1.4%	9.5%	1.4%	2.1%	5.2%	4.8%	2.1%	0.0%	0.0%
				15:00	16:00	500	1.2%	34.7%	24.9%	0.4%	10.5%	5.1%	1.6%	1.6%	7.3%	2.4%	1.6%	3.9%	3.2%	1.6%	0.0%	0.0%
				16:00	17:00	450	1.4%	38.9%	19.9%	0.5%	10.8%	2.3%	0.7%	0.7%	8.9%	0.5%	2.5%	5.5%	5.3%	2.1%	0.0%	0.0%
				17:00	18:00	550	1.7%	47.8%	20.5%	0.4%	6.8%	3.7%	1.1%	1.1%	7.6%	1.3%	1.3%	3.0%	2.4%	1.3%	0.0%	0.0%
				18:00	19:00	500	1.7%	51.3%	22.4%	0.2%	5.6%	2.4%	0.6%	0.9%	3.4%	1.1%	1.7%	3.8%	3.4%	1.5%	0.0%	0.0%
				19:00	20:00	500	1.3%	53.9%	27.3%	0.0%	1.6%	0.7%	0.2%	0.2%	9.1%	0.9%	0.7%	1.8%	1.6%	0.7%	0.0%	0.0%
				20:00	21:00	400	1.4%	44.6%	33.9%	0.0%	1.4%	0.3%	0.3%	0.3%	14.9%	0.6%	0.3%	0.8%	0.6%	0.6%	0.0%	0.0%
				21:00	22:00	350	1.0%	41.2%	39.5%	0.0%	1.9%	0.6%	0.3%	0.3%	14.6%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%
				22:00	23:00	300	1.4%	34.0%	42.8%	0.0%	1.1%	0.0%	0.0%	0.0%	16.5%	0.7%	0.7%	1.4%	0.7%	0.7%	0.0%	0.0%
				23:00	0:00	250	1.7%	32.6%	42.4%	0.0%	1.7%	0.0%	0.0%	0.0%	16.3%	0.9%	0.9%	1.7%	0.9%	0.9%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L113	Lok Shan road (WB)	To Kwa Wan Road	Cheung Ning Street	0:00	1:00	200	3.8%	38.9%	41.1%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	9.2%	0.5%	0.5%	2.2%	1.1%	0.5%	0.0%	0.0%
				1:00	2:00	100	4.2%	44.8%	47.9%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.4%	41.4%	52.9%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.5%	43.9%	47.1%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.5%	40.4%	50.8%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.9%	35.7%	45.7%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	4.3%	2.9%	1.4%	0.0%	0.0%
				6:00	7:00	150	2.7%	30.6%	39.4%	0.0%	4.1%	0.7%	0.0%	0.0%	10.2%	1.4%	1.4%	4.1%	3.4%	2.0%	0.0%	0.0%	
				7:00	8:00	450	1.9%	41.9%	27.7%	0.2%	3.8%	0.9%	0.2%	0.2%	12.3%	1.4%	1.4%	3.6%	3.1%	1.4%	0.0%	0.0%	
				8:00	9:00	700	3.4%	54.1%	18.2%	0.1%	5.8%	3.0%	0.9%	0.9%	6.1%	1.0%	1.0%	2.4%	2.1%	1.0%	0.0%	0.0%	
				9:00	10:00	600	2.7%	46.5%	21.1%	0.2%	6.3%	3.6%	1.0%	1.0%	6.1%	1.9%	1.5%	3.4%	3.2%	1.5%	0.0%	0.0%	
				10:00	11:00	500	3.3%	40.4%	21.0%	0.4%	13.1%	5.5%	1.6%	1.6%	6.1%	0.8%	1.0%	2.2%	2.0%	1.0%	0.0%	0.0%	
				11:00	12:00	500	2.0%	38.5%	17.8%	0.6%	16.1%	2.0%	0.6%	0.6%	10.8%	2.2%	1.2%	3.3%	3.1%	1.2%	0.0%	0.0%	
				12:00	13:00	550	2.6%	48.3%	21.3%	0.4%	10.2%	1.8%	0.6%	0.6%	7.8%	2.4%	0.6%	1.4%	1.4%	0.6%	0.0%	0.0%	
				13:00	14:00	500	1.9%	33.7%	14.8%	0.4%	13.2%	6.6%	1.9%	2.1%	7.8%	0.6%	2.7%	6.0%	5.8%	2.5%	0.0%	0.0%	
				14:00	15:00	500	1.7%	33.6%	21.4%	0.4%	12.9%	5.5%	1.5%	1.5%	8.7%	1.1%	1.7%	4.4%	3.9%	1.7%	0.0%	0.0%	
				15:00	16:00	550	2.4%	37.6%	21.4%	0.4%	12.2%	5.7%	1.7%	1.7%	6.6%	1.8%	1.3%	3.1%	2.8%	1.3%	0.0%	0.0%	
				16:00	17:00	500	2.7%	42.9%	17.1%	0.4%	12.2%	2.5%	0.6%	0.6%	8.2%	0.4%	1.9%	4.4%	4.4%	1.7%	0.0%	0.0%	
				17:00	18:00	650	3.3%	51.1%	17.1%	0.3%	7.7%	4.0%	1.2%	1.2%	6.8%	1.0%	1.0%	2.3%	2.0%	1.0%	0.0%	0.0%	
				18:00	19:00	550	3.5%	54.6%	18.7%	0.2%	6.2%	2.7%	0.8%	0.8%	3.1%	0.8%	1.4%	3.1%	2.9%	1.2%	0.0%	0.0%	
				19:00	20:00	550	2.8%	58.1%	23.0%	0.0%	2.0%	0.8%	0.2%	0.2%	8.1%	0.8%	0.6%	1.4%	1.4%	0.6%	0.0%	0.0%	
				20:00	21:00	400	3.0%	49.2%	29.3%	0.0%	1.8%	0.3%	0.3%	0.3%	13.6%	0.3%	0.3%	0.8%	0.5%	0.3%	0.0%	0.0%	
				21:00	22:00	350	2.4%	45.5%	34.1%	0.0%	2.4%	0.9%	0.3%	0.3%	13.5%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	
				22:00	23:00	350	3.3%	38.4%	37.8%	0.0%	1.3%	0.0%	0.0%	0.0%	15.5%	0.7%	0.3%	1.3%	0.7%	0.7%	0.0%	0.0%	
				23:00	0:00	250	3.2%	37.2%	37.8%	0.0%	2.0%	0.0%	0.0%	0.0%	15.4%	0.8%	0.4%	1.6%	0.8%	0.8%	0.0%	0.0%	
L114	Ha Heung Road (NB)	Chi Kiang Street	Lok Shan road	0:00	1:00	100	5.4%	43.0%	45.0%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%
				1:00	2:00	100	5.5%	43.6%	47.3%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	4.2%	41.7%	52.0%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	5.4%	43.2%	46.0%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	6.3%	40.6%	50.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	5.0%	35.0%	45.0%	0.0%	5.0%	0.0%	0.0%	2.5%	0.0%	0.0%	0.0%	2.5%	2.5%	2.5%	0.0%	0.0%	
				6:00	7:00	100	4.2%	34.7%	44.3%	0.0%	4.2%	1.4%	0.0%	1.4%	0.0%	1.4%	1.4%	2.8%	2.8%	1.4%	0.0%	0.0%	
				7:00	8:00	200	3.1%	47.7%	31.4%	0.5%	4.6%	1.0%	1.0%	1.0%	0.0%	1.5%	1.5%	2.6%	2.6%	1.5%	0.0%	0.0%	
				8:00	9:00	350	4.9%	54.4%	18.4%	0.3%	6.3%	3.5%	3.2%	3.2%	0.0%	0.9%	0.9%	1.7%	1.4%	0.9%	0.0%	0.0%	
				9:00	10:00	350	4.0%	47.0%	21.1%	0.3%	6.9%	4.3%	3.6%	3.6%	0.0%	1.7%	1.3%	2.6%	2.3%	1.3%	0.0%	0.0%	
				10:00	11:00	300	4.4%	40.5%	21.0%	0.4%	12.9%	5.5%	5.1%	5.1%	0.0%	0.7%	0.7%	1.5%	1.5%	0.7%	0.0%	0.0%	
				11:00	12:00	250	3.3%	42.9%	19.9%	0.8%	17.5%	2.4%	2.0%	2.0%	0.0%	2.0%	1.2%	2.4%	2.4%	1.2%	0.0%	0.0%	
				12:00	13:00	300	3.8%	51.6%	22.5%	0.4%	10.7%	1.9%	1.9%	1.9%	0.0%	2.3%	0.4%	1.1%	1.1%	0.4%	0.0%	0.0%	
				13:00	14:00	300	2.7%	34.8%	15.2%	0.4%	13.3%	6.8%	6.1%	6.5%	0.0%	0.8%	2.3%	4.6%	4.2%	2.3%	0.0%	0.0%	
				14:00	15:00	250	2.4%	34.8%	22.4%	0.4%	13.5%	5.7%	4.9%	5.3%	0.0%	1.2%	1.6%	3.3%	2.9%	1.6%	0.0%	0.0%	
				15:00	16:00	350	3.7%	37.8%	21.6%	0.3%	12.0%	6.0%	5.3%	5.3%	0.0%	1.7%	1.0%	2.3%	2.0%	1.0%	0.0%	0.0%	
				16:00	17:00	300	4.2%	47.2%	18.9%	0.4%	12.0%	2.7%	1.9%	1.9%	0.0%	0.4%	1.9%	3.5%	3.5%	1.5%	0.0%	0.0%	
				17:00	18:00	350	4.9%	53.1%	17.9%	0.3%	7.2%	3.8%	3.5%	3.5%	0.0%	0.9%	0.9%	1.7%	1.4%	0.9%	0.0%	0.0%	
				18:00	19:00	350	4.9%	55.7%	19.3%	0.3%	5.6%	2.3%	2.3%	2.3%	0.0%	0.7%	1.3%	2.3%	2.0%	1.0%	0.0%	0.0%	
				19:00	20:00	300	4.2%	62.8%	25.1%	0.0%	1.9%	0.8%	0.4%	0.8%	0.0%	0.8%	0.4%	1.2%	1.2%	0.4%	0.0%	0.0%	
				20:00	21:00	200	4.6%	56.3%	33.0%	0.0%	2.1%	0.5%	0.5%	1.0%	0.0%	0.5%	0.0%	0.5%	0.5%	0.5%	0.0%	0.0%	
				21:00	22:00	200	3.6%	51.8%	39.2%	0.0%	3.0%	0.6%	0.6%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	150	5.6%	45.1%	44.4%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.7%	0.7%	0.7%	0.7%	0.7%	0.0%	0.0%	
				23:00	0:00	150	5.9%	43.2%	44.4%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.8%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L115	Sze Chuen Street (EB)	Kowloon City Road	Ha Heung Road	0:00	1:00	50	0.0%	73.4%	13.3%	0.0%	13.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	75.0%	12.5%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	71.4%	14.3%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	66.6%	16.7%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	60.0%	20.0%	0.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	66.6%	16.7%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	58.3%	16.7%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	69.5%	11.1%	0.0%	19.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	0.0%	71.2%	5.5%	0.0%	23.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	0.0%	66.1%	6.8%	0.0%	27.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	0.0%	49.1%	5.1%	0.0%	45.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	0.0%	43.8%	4.7%	0.0%	51.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	0.0%	59.0%	4.9%	0.0%	36.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	0.0%	46.3%	3.7%	0.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	0.0%	45.1%	5.9%	0.0%	49.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	0.0%	49.3%	6.3%	0.0%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	0.0%	55.2%	3.4%	0.0%	41.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	0.0%	68.6%	4.3%	0.0%	27.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	0.0%	73.3%	5.0%	0.0%	21.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	0.0%	84.6%	7.7%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	80.6%	11.1%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	76.7%	13.3%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	73.9%	17.4%	0.0%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	0.0%	73.7%	15.8%	0.0%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
L116	Sze Chuen Street (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	50	0.0%	91.7%	0.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	87.5%	0.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	83.3%	0.0%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	80.0%	0.0%	0.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	77.8%	0.0%	0.0%	11.1%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	0.0%	84.3%	0.0%	0.0%	9.4%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	100	0.0%	77.8%	0.0%	0.0%	11.1%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	0.0%	72.4%	0.0%	0.0%	13.8%	13.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	0.0%	56.4%	0.0%	0.0%	23.6%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	50	0.0%	60.0%	0.0%	0.0%	32.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	0.0%	73.6%	0.0%	0.0%	18.9%	7.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	0.0%	50.0%	0.0%	0.0%	25.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	50	0.0%	52.2%	0.0%	0.0%	26.1%	21.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	0.0%	56.9%	0.0%	0.0%	22.4%	20.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	50	0.0%	68.0%	0.0%	0.0%	22.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	100	0.0%	74.0%	0.0%	0.0%	13.0%	13.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	0.0%	81.1%	0.0%	0.0%	10.3%	8.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	0.0%	92.2%	0.0%	0.0%	3.9%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	50	0.0%	94.0%	0.0%	0.0%	3.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	0.0%	89.3%	0.0%	0.0%	7.1%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	0.0%	95.0%	0.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	50	0.0%	93.7%	0.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L117	Kowloon City Road (WB)	Ha Heung Road	Lok Shan road	0:00	1:00	50	8.3%	58.4%	25.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				1:00	2:00	50	0.0%	57.1%	28.6%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	60.0%	20.0%	0.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	25.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	36.3%	27.3%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	9.1%	9.1%	0.0%	0.0%	
				7:00	8:00	50	2.9%	45.6%	14.3%	0.0%	11.4%	2.9%	0.0%	0.0%	0.0%	2.9%	2.9%	5.7%	5.7%	5.7%	0.0%	0.0%	0.0%	
				8:00	9:00	100	3.0%	51.6%	7.6%	0.0%	15.2%	7.6%	1.5%	1.5%	0.0%	1.5%	1.5%	3.0%	3.0%	3.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	1.7%	43.3%	8.3%	0.0%	15.0%	8.3%	1.7%	1.7%	0.0%	3.3%	1.7%	5.0%	5.0%	5.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	1.8%	33.8%	8.9%	0.0%	28.6%	12.5%	1.8%	1.8%	0.0%	1.8%	1.8%	1.8%	3.6%	3.6%	1.8%	0.0%	0.0%	0.0%
				11:00	12:00	100	1.9%	33.3%	7.4%	0.0%	35.1%	5.6%	0.0%	0.0%	0.0%	3.7%	1.9%	3.7%	3.7%	3.7%	0.0%	0.0%	0.0%	
				12:00	13:00	50	2.0%	48.0%	10.0%	0.0%	26.0%	4.0%	0.0%	0.0%	0.0%	4.0%	0.0%	2.0%	2.0%	2.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	1.6%	25.8%	4.8%	0.0%	25.8%	12.9%	1.6%	3.2%	0.0%	1.6%	3.2%	6.5%	6.5%	6.5%	0.0%	0.0%	0.0%	
				14:00	15:00	100	1.9%	27.4%	7.4%	0.0%	27.8%	11.1%	1.9%	1.9%	0.0%	1.9%	1.9%	5.6%	5.6%	5.6%	0.0%	0.0%	0.0%	
				15:00	16:00	100	1.6%	32.9%	8.2%	0.0%	26.2%	13.1%	1.6%	1.6%	0.0%	3.3%	1.6%	3.3%	3.3%	3.3%	0.0%	0.0%	0.0%	
				16:00	17:00	100	1.8%	38.0%	7.3%	0.0%	25.5%	5.5%	0.0%	0.0%	0.0%	0.0%	3.6%	5.5%	7.3%	5.5%	0.0%	0.0%	0.0%	
				17:00	18:00	100	3.0%	48.6%	7.6%	0.0%	16.7%	9.1%	1.5%	1.5%	0.0%	1.5%	1.5%	3.0%	3.0%	3.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	3.4%	50.7%	8.5%	0.0%	13.6%	5.1%	1.7%	1.7%	0.0%	1.7%	1.7%	3.4%	5.1%	3.4%	0.0%	0.0%	0.0%	
				19:00	20:00	50	2.4%	68.9%	11.9%	0.0%	4.8%	2.4%	0.0%	0.0%	0.0%	2.4%	0.0%	2.4%	2.4%	2.4%	0.0%	0.0%	0.0%	
				20:00	21:00	50	3.7%	70.4%	18.5%	0.0%	7.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	4.2%	62.5%	20.8%	0.0%	8.3%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	5.3%	57.8%	26.3%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%	0.0%	
				23:00	0:00	50	6.3%	56.1%	25.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.3%	0.0%	0.0%	
L118	Ma Tau Wai Road (SB)	Lok Shan road	Chi Kiang Street	0:00	1:00	450	5.3%	25.5%	49.1%	0.0%	2.3%	0.0%	0.0%	0.0%	7.4%	0.5%	0.5%	1.1%	0.7%	0.5%	0.0%	7.1%		
				1:00	2:00	250	5.9%	29.7%	59.4%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%		
				2:00	3:00	250	4.9%	27.0%	63.6%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%		
				3:00	4:00	200	6.5%	28.8%	57.5%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%		
				4:00	5:00	150	5.9%	26.5%	61.1%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	2.9%		
				5:00	6:00	200	5.0%	24.5%	57.3%	0.0%	5.0%	0.0%	0.0%	0.6%	0.0%	0.6%	1.3%	2.5%	1.9%	1.3%	0.0%	0.0%		
				6:00	7:00	400	3.6%	19.0%	45.7%	0.6%	4.1%	1.1%	0.0%	0.3%	8.3%	1.1%	1.1%	2.5%	2.2%	0.8%	0.0%	9.6%		
				7:00	8:00	1,100	2.7%	26.1%	28.9%	0.7%	5.0%	1.4%	0.5%	0.5%	11.2%	1.1%	1.0%	2.1%	2.1%	0.7%	0.0%	16.0%		
				8:00	9:00	1,600	5.2%	36.5%	20.5%	0.8%	8.3%	4.6%	1.6%	1.6%	5.9%	0.8%	0.8%	1.6%	1.6%	0.6%	0.0%	9.6%		
				9:00	10:00	1,450	4.0%	30.3%	22.6%	0.8%	8.5%	5.3%	1.8%	1.8%	5.7%	1.5%	1.1%	2.1%	2.3%	0.8%	0.0%	11.4%		
				10:00	11:00	1,200	4.4%	25.9%	25.0%	1.3%	13.6%	6.3%	2.1%	2.1%	4.9%	0.6%	0.7%	1.4%	1.4%	0.5%	0.0%	9.8%		
				11:00	12:00	1,200	3.0%	25.1%	21.5%	1.7%	17.2%	2.4%	0.9%	0.7%	9.0%	1.7%	1.0%	2.0%	2.2%	0.7%	0.0%	10.9%		
				12:00	13:00	1,150	3.7%	33.1%	26.9%	1.2%	11.4%	2.2%	0.9%	0.7%	6.7%	1.9%	0.4%	0.9%	1.0%	0.3%	0.0%	8.7%		
				13:00	14:00	1,200	2.7%	21.8%	17.6%	1.5%	13.8%	7.6%	2.6%	2.6%	6.4%	0.5%	1.9%	3.7%	4.0%	1.3%	0.0%	12.0%		
				14:00	15:00	1,150	2.3%	21.1%	25.2%	1.3%	13.4%	6.0%	2.0%	2.0%	7.1%	0.9%	1.3%	2.6%	2.7%	0.9%	0.0%	11.2%		
				15:00	16:00	1,350	3.5%	24.2%	25.4%	1.3%	12.7%	6.5%	2.3%	2.2%	5.5%	1.4%	0.9%	1.8%	1.9%	0.6%	0.0%	9.8%		
				16:00	17:00	1,050	4.1%	29.9%	25.0%	1.1%	10.5%	2.4%	0.7%	0.7%	6.3%	0.3%	1.5%	2.9%	3.3%	0.9%	0.0%	10.4%		
				17:00	18:00	1,250	5.0%	36.4%	25.7%	0.8%	6.7%	3.8%	1.3%	1.2%	5.3%	0.8%	0.8%	1.5%	1.5%	0.6%	0.0%	8.6%		
				18:00	19:00	1,100	5.2%	39.6%	28.4%	0.6%	5.5%	2.5%	0.8%	0.8%	2.5%	0.7%	1.1%	2.0%	2.3%	0.7%	0.0%	7.3%		
				19:00	20:00	1,100	4.3%	41.3%	30.4%	0.2%	2.2%	1.0%	0.2%	0.4%	7.3%	0.6%	0.5%	0.9%	1.0%	0.3%	0.0%	9.4%		
				20:00	21:00	950	4.3%	33.0%	36.2%	0.2%	1.9%	0.4%	0.2%	0.3%	11.5%	0.2%	0.2%	0.4%	0.3%	0.2%	0.0%	10.7%		
				21:00	22:00	850	3.1%	28.8%	40.3%	0.2%	2.5%	0.8%	0.2%	0.4%	10.8%	0.0%	0.1%	0.1%	0.0%	0.1%	0.0%	12.6%		
				22:00	23:00	850	4.2%	22.3%	40.9%	0.2%	1.4%	0.0%	0.0%	0.0%	11.5%	0.4%	0.2%	0.6%	0.4%	0.4%	0.0%	17.5%		
				23:00	0:00	700	4.4%	21.5%	40.6%	0.3%	1.8%	0.0%	0.0%	0.0%	11.5%	0.5%	0.3%	0.8%	0.5%	0.5%	0.0%	17.3%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L119	Ma Tau Wai Road (NB)	Kiang Su Street	Lok Shan road	0:00	1:00	300	9.1%	15.7%	44.1%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	16.8%	0.7%	0.7%	1.4%	0.7%	0.3%	0.0%	9.1%	
				1:00	2:00	150	10.9%	20.9%	59.7%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	
				2:00	3:00	150	10.0%	18.3%	63.4%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%
				3:00	4:00	100	13.6%	20.5%	58.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%
				4:00	5:00	100	11.4%	19.0%	60.7%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	5.1%
				5:00	6:00	100	10.0%	17.8%	59.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	2.2%	3.3%	2.2%	1.1%	0.0%	0.0%	0.0%
				6:00	7:00	250	5.8%	11.5%	40.0%	0.4%	2.5%	0.4%	0.0%	0.0%	18.1%	1.2%	1.6%	2.9%	2.5%	1.2%	0.0%	11.9%		
				7:00	8:00	750	2.1%	14.2%	34.2%	0.7%	1.9%	0.4%	0.1%	0.1%	27.0%	0.7%	0.7%	1.4%	1.2%	0.6%	0.0%	14.7%		
				8:00	9:00	900	4.8%	24.4%	29.9%	1.1%	3.9%	1.8%	0.7%	0.7%	17.6%	0.7%	0.7%	1.3%	1.1%	0.5%	0.0%	10.8%		
				9:00	10:00	850	3.7%	19.9%	32.7%	1.1%	4.0%	2.1%	0.7%	0.7%	16.6%	1.1%	0.9%	1.7%	1.6%	0.6%	0.0%	12.6%		
				10:00	11:00	700	8.4%	17.9%	25.1%	2.0%	8.9%	3.6%	0.9%	1.0%	12.5%	0.9%	1.0%	1.7%	1.7%	0.6%	0.0%	13.8%		
				11:00	12:00	750	5.2%	16.0%	19.8%	2.5%	10.2%	1.2%	0.4%	0.3%	20.9%	2.2%	1.2%	2.5%	2.6%	0.8%	0.0%	14.2%		
				12:00	13:00	700	6.7%	22.1%	26.1%	1.9%	7.2%	1.2%	0.4%	0.3%	16.4%	2.6%	0.6%	1.0%	1.2%	0.4%	0.0%	11.9%		
				13:00	14:00	750	5.0%	14.6%	16.7%	2.1%	8.7%	4.1%	1.1%	1.1%	15.5%	0.7%	2.6%	4.7%	5.0%	1.6%	0.0%	16.5%		
				14:00	15:00	700	4.3%	13.9%	23.6%	2.1%	8.2%	3.2%	0.9%	0.9%	17.0%	1.2%	1.8%	3.2%	3.4%	1.2%	0.0%	15.1%		
				15:00	16:00	800	6.4%	16.6%	25.1%	1.9%	8.2%	3.6%	1.0%	1.0%	13.6%	1.9%	1.3%	2.5%	2.5%	0.8%	0.0%	13.6%		
				16:00	17:00	700	11.1%	19.4%	12.8%	1.9%	8.5%	1.7%	0.3%	0.3%	11.2%	0.6%	2.9%	5.0%	5.7%	1.6%	0.0%	17.0%		
				17:00	18:00	850	14.4%	25.4%	14.0%	1.4%	5.8%	3.0%	0.6%	0.6%	10.1%	1.6%	1.6%	2.8%	2.8%	1.0%	0.0%	14.9%		
				18:00	19:00	700	15.7%	28.6%	16.0%	0.9%	4.9%	2.1%	0.4%	0.4%	4.9%	1.3%	2.4%	3.9%	4.3%	1.2%	0.0%	13.0%		
				19:00	20:00	700	7.7%	27.1%	28.8%	0.1%	1.3%	0.6%	0.1%	0.1%	17.5%	0.7%	0.6%	1.2%	1.2%	0.4%	0.0%	12.6%		
				20:00	21:00	650	7.0%	19.9%	31.2%	0.2%	1.0%	0.2%	0.2%	0.2%	25.3%	0.3%	0.3%	0.5%	0.3%	0.2%	0.0%	13.2%		
				21:00	22:00	600	5.3%	17.6%	34.8%	0.2%	1.4%	0.4%	0.2%	0.2%	23.7%	0.0%	0.2%	0.2%	0.0%	0.2%	0.0%	15.6%		
				22:00	23:00	600	6.6%	12.8%	33.5%	0.2%	0.7%	0.0%	0.0%	0.0%	23.8%	0.5%	0.3%	0.7%	0.3%	0.3%	0.0%	20.3%		
				23:00	0:00	500	6.9%	12.1%	33.2%	0.2%	0.9%	0.0%	0.0%	0.0%	23.7%	0.6%	0.4%	0.9%	0.4%	0.4%	0.0%	20.3%		
L120	Maidstone Road (SB)	Lok Shan road	Kiang Su Street	0:00	1:00	50	3.2%	42.0%	35.5%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	16.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				1:00	2:00	50	5.9%	47.0%	41.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	6.7%	46.6%	46.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	50	8.3%	41.7%	41.7%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	50	0.0%	45.4%	45.5%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				6:00	7:00	50	3.7%	29.6%	33.4%	0.0%	3.7%	0.0%	0.0%	0.0%	18.5%	0.0%	0.0%	3.7%	3.7%	3.7%	0.0%	0.0%		
				7:00	8:00	100	1.4%	38.4%	20.5%	0.0%	4.1%	1.4%	0.0%	0.0%	23.3%	1.4%	1.4%	2.7%	2.7%	2.7%	0.0%	0.0%		
				8:00	9:00	150	3.5%	49.8%	14.0%	0.0%	7.0%	3.5%	1.8%	1.8%	11.4%	0.9%	0.9%	1.8%	1.8%	1.8%	0.0%	0.0%		
				9:00	10:00	150	3.0%	42.4%	15.8%	0.0%	7.9%	4.0%	2.0%	2.0%	10.9%	2.0%	1.0%	3.0%	3.0%	3.0%	0.0%	0.0%		
				10:00	11:00	100	3.2%	38.4%	17.2%	0.0%	14.0%	6.5%	2.2%	2.2%	9.7%	1.1%	1.1%	1.1%	2.2%	1.1%	0.0%	0.0%		
				11:00	12:00	100	2.1%	35.7%	13.7%	0.0%	16.8%	2.1%	1.1%	1.1%	17.9%	2.1%	1.1%	2.1%	2.1%	2.1%	0.0%	0.0%		
				12:00	13:00	100	3.2%	46.7%	17.0%	0.0%	10.6%	2.1%	1.1%	1.1%	12.8%	2.1%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%		
				13:00	14:00	100	2.1%	31.2%	11.5%	0.0%	13.5%	7.3%	3.1%	3.1%	12.5%	1.0%	2.1%	4.2%	4.2%	4.2%	0.0%	0.0%		
				14:00	15:00	100	2.2%	31.3%	16.7%	0.0%	13.3%	5.6%	2.2%	2.2%	14.4%	1.1%	1.1%	3.3%	3.3%	3.3%	0.0%	0.0%		
				15:00	16:00	150	2.9%	36.1%	16.5%	0.0%	12.6%	5.8%	2.9%	2.9%	11.7%	1.9%	1.0%	1.9%	1.9%	1.9%	0.0%	0.0%		
				16:00	17:00	100	3.0%	43.0%	14.0%	0.0%	11.0%	3.0%	1.0%	1.0%	12.0%	0.0%	2.0%	3.0%	4.0%	3.0%	0.0%	0.0%		
				17:00	18:00	150	3.9%	51.1%	14.2%	0.0%	7.1%	3.9%	1.6%	1.6%	10.2%	0.8%	0.8%	1.6%	1.6%	1.6%	0.0%	0.0%		
				18:00	19:00	150	3.8%	56.8%	16.0%	0.0%	5.7%	2.8%	0.9%	0.9%	4.7%	0.9%	0.9%	1.9%	2.8%	1.9%	0.0%	0.0%		
				19:00	20:00	100	3.2%	56.8%	18.3%	0.0%	2.2%	1.1%	0.0%	0.0%	14.0%	1.1%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%		
				20:00	21:00	100	2.7%	49.3%	23.3%	0.0%	1.4%	0.0%	0.0%	0.0%	23.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				21:00	22:00	100	3.1%	43.1%	27.7%	0.0%	3.1%	1.5%	0.0%	0.0%	21.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				22:00	23:00	100	3.5%	36.8%	29.8%	0.0%	1.8%	0.0%	0.0%	0.0%	26.3%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%		
				23:00	0:00	50	4.3%	36.2%	29.8%	0.0%	2.1%	0.0%	0.0%	0.0%	25.5%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L121	Kau Pui Lung Road (SB)	Lok Shan road	Kiang Su Street	0:00	1:00	100	3.3%	29.3%	60.8%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	2.2%	1.1%	0.0%	0.0%	0.0%
				1:00	2:00	100	3.8%	30.2%	64.1%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.9%	27.5%	66.6%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.4%	29.7%	62.2%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.1%	28.1%	65.7%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.6%	23.1%	61.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	2.6%	2.6%	2.6%	2.6%	0.0%	0.0%	0.0%
				6:00	7:00	100	2.6%	22.4%	56.8%	0.0%	2.6%	1.3%	0.0%	0.0%	0.0%	2.6%	2.6%	3.9%	3.9%	1.3%	0.0%	0.0%
				7:00	8:00	200	2.4%	33.5%	43.1%	0.6%	4.8%	1.2%	0.0%	0.0%	0.0%	2.4%	2.4%	4.2%	4.2%	1.2%	0.0%	0.0%
				8:00	9:00	300	3.8%	44.1%	28.9%	0.4%	7.2%	4.2%	0.4%	0.4%	0.0%	1.9%	1.9%	3.0%	3.0%	0.8%	0.0%	0.0%
				9:00	10:00	250	2.9%	36.1%	31.8%	0.4%	7.4%	5.0%	0.4%	0.4%	0.0%	3.3%	2.5%	4.1%	4.5%	1.2%	0.0%	0.0%
				10:00	11:00	250	3.7%	33.9%	34.6%	0.5%	11.5%	5.5%	0.5%	0.5%	0.0%	1.4%	1.8%	2.8%	2.8%	0.5%	0.0%	0.0%
				11:00	12:00	250	2.4%	33.6%	30.2%	0.9%	14.6%	1.9%	0.0%	0.0%	0.0%	4.2%	2.4%	4.2%	4.7%	0.9%	0.0%	0.0%
				12:00	13:00	250	2.7%	41.2%	35.3%	0.5%	9.0%	1.8%	0.0%	0.0%	0.0%	4.5%	0.9%	1.8%	1.8%	0.5%	0.0%	0.0%
				13:00	14:00	250	2.3%	28.8%	24.7%	0.5%	11.6%	6.5%	0.5%	0.9%	0.0%	1.4%	5.1%	7.4%	8.4%	1.9%	0.0%	0.0%
				14:00	15:00	250	1.9%	27.8%	34.4%	0.5%	11.0%	5.3%	0.5%	0.5%	0.0%	2.4%	3.3%	5.3%	5.7%	1.4%	0.0%	0.0%
				15:00	16:00	250	2.5%	31.6%	34.8%	0.4%	10.2%	5.3%	0.4%	0.4%	0.0%	3.3%	2.5%	3.7%	4.1%	0.8%	0.0%	0.0%
				16:00	17:00	250	3.0%	38.2%	30.2%	0.4%	7.7%	1.7%	0.0%	0.0%	0.0%	0.9%	3.8%	6.0%	6.8%	1.3%	0.0%	0.0%
				17:00	18:00	300	3.7%	46.4%	30.6%	0.3%	4.8%	2.7%	0.3%	0.3%	0.0%	2.0%	2.0%	3.1%	3.1%	0.7%	0.0%	0.0%
				18:00	19:00	300	3.7%	46.0%	31.1%	0.4%	3.7%	1.8%	0.4%	0.4%	0.0%	1.5%	2.6%	3.7%	4.0%	0.7%	0.0%	0.0%
				19:00	20:00	250	3.2%	49.4%	38.3%	0.0%	1.8%	0.9%	0.0%	0.0%	0.0%	1.4%	0.9%	1.8%	1.8%	0.5%	0.0%	0.0%
				20:00	21:00	200	3.5%	42.8%	49.0%	0.0%	1.7%	0.6%	0.0%	0.0%	0.0%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%
				21:00	22:00	200	2.6%	37.9%	56.1%	0.0%	2.0%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	3.5%	31.0%	59.2%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	1.4%	0.7%	1.4%	0.7%	0.7%	0.0%	0.0%
				23:00	0:00	150	4.3%	29.3%	58.6%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	1.7%	0.9%	1.7%	0.9%	0.9%	0.0%	0.0%
L122	Kau Pui Lung Road (NB)	Kiang Su Street	Lok Shan road	0:00	1:00	200	1.8%	22.3%	66.3%	0.0%	2.4%	0.0%	0.0%	0.0%	3.0%	0.6%	0.6%	1.7%	0.9%	0.9%	0.0%	0.0%
				1:00	2:00	100	2.1%	22.3%	72.4%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	1.1%	20.5%	76.1%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	1.6%	22.2%	71.4%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	1.8%	21.1%	73.6%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.4%	18.6%	67.2%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	1.4%	1.4%	2.9%	1.4%	1.4%	0.0%	0.0%
				6:00	7:00	150	1.5%	16.8%	62.0%	0.0%	4.4%	0.7%	0.0%	0.0%	3.6%	1.5%	1.5%	3.6%	2.9%	1.5%	0.0%	0.0%
				7:00	8:00	350	1.2%	27.1%	48.7%	0.6%	5.2%	1.2%	0.3%	0.3%	5.2%	1.5%	1.5%	3.3%	2.7%	1.2%	0.0%	0.0%
				8:00	9:00	500	2.1%	37.6%	34.5%	0.6%	8.2%	4.5%	1.0%	1.0%	2.7%	1.2%	1.2%	2.5%	2.1%	0.8%	0.0%	0.0%
				9:00	10:00	450	1.6%	30.8%	37.7%	0.7%	8.5%	5.1%	1.1%	1.1%	2.5%	2.0%	1.6%	3.3%	2.9%	1.1%	0.0%	0.0%
				10:00	11:00	450	1.7%	24.5%	37.3%	0.7%	16.3%	7.2%	1.5%	1.7%	2.2%	1.0%	1.0%	2.2%	2.0%	0.7%	0.0%	0.0%
				11:00	12:00	400	1.3%	24.4%	32.9%	0.8%	21.0%	2.8%	0.8%	0.5%	4.4%	2.6%	1.6%	3.1%	2.8%	1.0%	0.0%	0.0%
				12:00	13:00	400	1.5%	31.2%	39.5%	0.5%	13.6%	2.6%	0.8%	0.5%	3.1%	2.8%	0.8%	1.3%	1.3%	0.5%	0.0%	0.0%
				13:00	14:00	400	1.0%	21.2%	26.8%	0.8%	16.9%	9.0%	2.0%	2.0%	3.1%	0.8%	3.1%	5.9%	5.4%	2.0%	0.0%	0.0%
				14:00	15:00	400	1.0%	20.0%	36.8%	0.8%	15.8%	6.8%	1.6%	1.6%	3.4%	1.3%	1.8%	4.2%	3.6%	1.3%	0.0%	0.0%
				15:00	16:00	500	1.3%	22.7%	37.3%	0.7%	15.0%	7.3%	1.8%	1.8%	2.6%	2.0%	1.3%	2.9%	2.4%	0.9%	0.0%	0.0%
				16:00	17:00	400	1.8%	26.8%	33.5%	0.3%	15.8%	3.4%	0.8%	0.8%	3.2%	0.5%	2.4%	4.7%	1.3%	0.0%	0.0%	0.0%
				17:00	18:00	500	2.2%	33.3%	34.8%	0.2%	10.4%	5.4%	1.3%	1.3%	2.8%	1.3%	1.3%	2.6%	2.2%	0.9%	0.0%	0.0%
				18:00	19:00	450	2.2%	35.2%	37.6%	0.2%	8.4%	3.4%	0.7%	1.0%	1.2%	1.0%	1.7%	3.2%	3.2%	1.0%	0.0%	0.0%
				19:00	20:00	400	1.9%	39.8%	45.4%	0.0%	2.7%	1.1%	0.3%	0.3%	3.5%	0.8%	0.8%	1.6%	1.3%	0.5%	0.0%	0.0%
				20:00	21:00	350	1.6%	32.5%	55.2%	0.0%	2.3%	0.3%	0.3%	0.3%	5.6%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	0.0%
				21:00	22:00	300	1.4%	27.8%	60.3%	0.0%	3.2%	1.1%	0.4%	0.4%	5.0%	0.0%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	300	1.9%	22.8%	64.4%	0.0%	1.9%	0.0%	0.0%	0.0%	5.8%	0.8%	0.4%	1.2%	0.4%	0.4%	0.0%	0.0%
				23:00	0:00	250	1.9%	21.5%	64.6%	0.0%	2.4%	0.0%	0.0%	0.0%	5.7%	1.0%	0.5%	1.4%	0.5%	0.5%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L123	Klang Su Street (WB)	Maidstone Road	Kau Pui Lung Road	0:00	1:00	50	2.7%	43.3%	35.1%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	13.5%	0.0%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	5.3%	47.3%	42.1%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	5.9%	47.0%	47.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.1%	42.9%	42.9%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	46.1%	46.2%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	3.3%	33.3%	36.8%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	16.7%	0.0%	0.0%	3.3%	3.3%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	100	1.2%	41.7%	22.1%	2.3%	3.5%	1.2%	0.0%	0.0%	19.8%	1.2%	1.2%	2.3%	2.3%	1.2%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	150	2.2%	54.5%	14.7%	2.2%	5.9%	4.4%	0.7%	0.7%	9.6%	0.7%	0.7%	1.5%	1.5%	0.7%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	150	1.7%	47.8%	16.9%	2.5%	6.8%	5.1%	0.8%	0.8%	9.3%	1.7%	0.8%	2.5%	2.5%	0.8%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	150	2.8%	39.7%	17.6%	2.8%	12.0%	6.5%	1.9%	1.9%	8.3%	0.9%	0.9%	1.9%	1.9%	0.9%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	150	1.8%	37.0%	14.4%	2.7%	15.3%	2.7%	0.9%	0.9%	15.3%	1.8%	0.9%	2.7%	2.7%	0.9%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	150	2.8%	49.0%	17.6%	1.9%	10.2%	1.9%	0.9%	0.9%	11.1%	1.9%	0.0%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	150	1.9%	33.5%	12.1%	2.8%	13.1%	7.5%	1.9%	1.9%	11.2%	0.9%	1.9%	4.7%	4.7%	1.9%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	150	2.0%	33.4%	17.6%	2.9%	12.7%	5.9%	2.0%	2.0%	12.7%	1.0%	1.0%	2.9%	2.9%	1.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	150	2.5%	37.4%	17.5%	2.5%	11.7%	6.7%	1.7%	1.7%	10.0%	1.7%	0.8%	2.5%	2.5%	0.8%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	150	3.5%	42.5%	15.0%	0.9%	11.5%	2.7%	0.9%	0.9%	10.6%	0.0%	1.8%	4.4%	4.4%	0.9%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	150	4.2%	51.5%	14.8%	0.7%	7.0%	3.5%	1.4%	1.4%	9.2%	0.7%	0.7%	2.1%	2.1%	0.7%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	150	4.1%	56.4%	16.5%	0.8%	5.8%	2.5%	0.8%	0.8%	4.1%	0.8%	0.8%	2.5%	3.3%	0.8%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	150	2.8%	60.0%	19.6%	0.0%	1.9%	0.9%	0.0%	0.0%	12.1%	0.9%	0.0%	0.9%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	2.4%	51.2%	25.0%	0.0%	1.2%	0.0%	0.0%	0.0%	20.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	2.7%	45.9%	28.4%	0.0%	2.7%	1.4%	0.0%	0.0%	18.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.0%	39.5%	31.8%	0.0%	1.5%	0.0%	0.0%	0.0%	22.7%	0.0%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L124	Maidstone Road (NB)	Chi Kiang Street	Klang Su Street	23:00	0:00	100	3.8%	37.7%	32.1%	0.0%	1.9%	0.0%	0.0%	0.0%	22.6%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	
				0:00	1:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	66.7%	22.2%	0.0%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	75.0%	12.5%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	69.2%	15.4%	0.0%	15.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	57.1%	14.3%	0.0%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	53.4%	13.3%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	66.7%	13.3%	0.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	53.8%	15.4%	0.0%	30.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	54.5%	18.2%	0.0%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	53.3%	20.0%	0.0%	26.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	62.5%	12.5%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	71.4%	14.3%	0.0%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	73.7%	15.8%	0.0%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	74.9%	18.8%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	72.7%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
23:00	0:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L125	Maidstone Road (SB)	Kiang Su Street	Chi Kiang Street	0:00	1:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	63.6%	27.3%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	70.0%	15.0%	0.0%	15.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	64.8%	17.6%	0.0%	17.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	53.4%	13.3%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	46.7%	13.3%	0.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	60.0%	13.3%	0.0%	26.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	46.1%	15.4%	0.0%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	46.1%	15.4%	0.0%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	49.9%	18.8%	0.0%	31.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	53.4%	13.3%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	66.7%	11.1%	0.0%	22.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	68.7%	12.5%	0.0%	18.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	73.3%	20.0%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	66.7%	25.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	60.0%	30.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L126	Ma Tau Wai Road (NB)	Chi Kiang Street	Kiang Su Street	0:00	1:00	300	8.4%	16.0%	44.6%	0.0%	1.4%	0.0%	0.0%	0.0%	16.7%	0.7%	1.4%	0.7%	1.4%	0.7%	0.3%	0.0%	9.1%
				1:00	2:00	150	10.7%	20.6%	60.2%	0.0%	2.3%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	0.0%
				2:00	3:00	150	9.1%	19.0%	63.6%	0.0%	1.7%	0.0%	0.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.3%	0.0%
				3:00	4:00	100	12.5%	20.5%	59.1%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%	0.0%
				4:00	5:00	100	10.1%	19.0%	62.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	5.1%	0.0%
				5:00	6:00	100	8.9%	17.8%	60.1%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	1.1%	2.2%	3.3%	2.2%	1.1%	0.0%	0.0%	0.0%
				6:00	7:00	250	5.7%	11.5%	40.2%	0.8%	2.5%	0.4%	0.0%	0.0%	18.0%	1.2%	1.6%	2.9%	2.5%	0.8%	0.0%	11.9%	0.0%
				7:00	8:00	750	1.8%	14.2%	34.9%	0.8%	1.9%	0.4%	0.1%	0.1%	26.7%	0.7%	0.7%	1.5%	1.2%	0.4%	0.0%	14.6%	0.0%
				8:00	9:00	900	4.1%	24.5%	30.5%	1.4%	3.8%	1.9%	0.7%	0.7%	17.5%	0.7%	0.7%	1.4%	1.1%	0.3%	0.0%	10.7%	0.0%
				9:00	10:00	850	3.1%	19.9%	33.3%	1.4%	3.8%	2.2%	0.7%	0.7%	16.5%	1.1%	0.9%	1.8%	1.6%	0.5%	0.0%	12.5%	0.0%
				10:00	11:00	700	8.1%	17.8%	25.2%	2.2%	8.8%	3.8%	0.9%	1.0%	12.4%	0.9%	1.0%	1.9%	1.7%	0.6%	0.0%	13.7%	0.0%
				11:00	12:00	750	4.9%	16.1%	20.0%	2.7%	10.2%	1.4%	0.4%	0.3%	20.8%	2.2%	1.2%	2.4%	2.6%	0.7%	0.0%	14.1%	0.0%
				12:00	13:00	700	6.6%	22.1%	26.2%	2.0%	7.1%	1.3%	0.4%	0.3%	16.3%	2.6%	0.6%	1.2%	1.2%	0.3%	0.0%	11.8%	0.0%
				13:00	14:00	750	4.7%	14.5%	17.1%	2.4%	8.6%	4.4%	1.1%	1.1%	15.4%	0.7%	2.5%	4.8%	4.9%	1.4%	0.0%	16.4%	0.0%
				14:00	15:00	700	4.1%	14.0%	24.0%	2.2%	8.2%	3.3%	0.9%	0.9%	16.9%	1.2%	1.7%	3.3%	3.3%	1.0%	0.0%	15.0%	0.0%
				15:00	16:00	800	6.1%	16.5%	25.4%	2.2%	8.1%	3.9%	1.0%	1.0%	13.5%	1.9%	1.3%	2.4%	2.4%	0.8%	0.0%	13.5%	0.0%
				16:00	17:00	700	11.0%	19.3%	12.8%	1.9%	8.6%	1.9%	0.3%	0.3%	11.2%	0.6%	2.9%	5.0%	5.7%	1.6%	0.0%	16.9%	0.0%
				17:00	18:00	850	14.4%	25.3%	13.9%	1.4%	5.9%	3.1%	0.6%	0.6%	10.1%	1.6%	1.6%	2.8%	2.8%	1.0%	0.0%	14.9%	0.0%
				18:00	19:00	700	15.7%	28.4%	16.0%	0.9%	5.1%	2.1%	0.4%	0.4%	4.9%	1.3%	2.4%	3.9%	4.3%	1.2%	0.0%	13.0%	0.0%
				19:00	20:00	700	7.4%	27.2%	29.0%	0.3%	1.3%	0.6%	0.1%	0.1%	17.4%	0.7%	0.6%	1.2%	1.2%	0.3%	0.0%	12.6%	0.0%
				20:00	21:00	650	6.8%	19.9%	31.6%	0.3%	1.0%	0.2%	0.2%	0.2%	25.1%	0.3%	0.3%	0.5%	0.3%	0.2%	0.0%	13.1%	0.0%
				21:00	22:00	600	5.0%	17.4%	35.3%	0.4%	1.4%	0.4%	0.2%	0.2%	23.6%	0.0%	0.2%	0.2%	0.0%	0.2%	0.0%	15.5%	0.0%
				22:00	23:00	600	6.3%	12.7%	34.0%	0.3%	0.7%	0.0%	0.0%	0.0%	23.7%	0.5%	0.3%	0.7%	0.3%	0.3%	0.0%	20.2%	0.0%
				23:00	0:00	500	6.6%	12.2%	33.6%	0.4%	0.9%	0.0%	0.0%	0.0%	23.5%	0.6%	0.4%	0.9%	0.4%	0.4%	0.0%	20.1%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L127	Chi Kiang Street (WB)	Ma Tau Wai Road	Ko Shan Road	0:00	1:00	50	0.0%	61.9%	33.3%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	61.5%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	63.6%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	53.3%	40.0%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	67.5%	25.0%	0.0%	5.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	0.0%	70.5%	14.1%	0.0%	7.7%	5.1%	1.3%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	0.0%	63.9%	17.2%	0.0%	9.4%	6.3%	1.6%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	0.0%	57.0%	15.4%	0.0%	15.4%	9.2%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	0.0%	61.4%	14.0%	0.0%	21.1%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	0.0%	69.7%	15.2%	0.0%	12.1%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	0.0%	53.5%	12.1%	0.0%	17.2%	12.1%	1.7%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	0.0%	52.7%	18.2%	0.0%	16.4%	9.1%	1.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	0.0%	56.7%	16.4%	0.0%	14.9%	9.0%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	0.0%	69.2%	13.2%	0.0%	13.2%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	0.0%	74.0%	11.5%	0.0%	7.3%	5.2%	1.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	0.0%	76.7%	11.6%	0.0%	5.8%	3.5%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	0.0%	80.8%	16.2%	0.0%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	75.6%	22.4%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	69.0%	26.2%	0.0%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	64.7%	32.4%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	60.7%	35.7%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L128	Kau Pui Lung Road (SB)	Kiang Su Street	Ko Shan Road	0:00	1:00	100	2.0%	30.3%	54.6%	0.0%	2.0%	0.0%	0.0%	0.0%	5.1%	1.0%	1.0%	2.0%	1.0%	1.0%	0.0%	0.0%	
				1:00	2:00	100	1.9%	33.3%	62.9%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.0%	30.0%	66.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.8%	33.3%	61.1%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	3.0%	30.3%	63.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.4%	26.2%	54.6%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	2.4%	2.4%	4.8%	2.4%	2.4%	0.0%	0.0%	
				6:00	7:00	100	1.2%	22.9%	50.7%	0.0%	2.4%	1.2%	0.0%	0.0%	6.0%	2.4%	2.4%	4.8%	4.8%	1.2%	0.0%	0.0%	
				7:00	8:00	200	1.1%	32.6%	36.9%	0.5%	4.3%	1.1%	0.5%	0.0%	9.1%	2.1%	1.6%	4.3%	4.3%	1.6%	0.0%	0.0%	
				8:00	9:00	300	2.5%	44.3%	25.9%	0.4%	6.7%	3.5%	1.1%	0.7%	4.6%	1.4%	1.4%	3.2%	3.2%	1.1%	0.0%	0.0%	
				9:00	10:00	300	1.9%	36.5%	28.8%	0.4%	7.0%	4.3%	1.2%	0.8%	4.3%	2.3%	1.9%	4.3%	4.7%	1.6%	0.0%	0.0%	
				10:00	11:00	250	2.5%	34.6%	31.4%	0.4%	10.5%	4.6%	1.3%	0.8%	3.8%	1.3%	1.3%	2.9%	3.3%	1.3%	0.0%	0.0%	
				11:00	12:00	250	1.7%	33.2%	26.5%	0.8%	13.0%	1.7%	0.4%	0.4%	7.1%	3.4%	1.7%	4.2%	4.6%	1.3%	0.0%	0.0%	
				12:00	13:00	250	2.1%	42.1%	31.7%	0.4%	8.2%	1.6%	0.4%	0.4%	4.9%	3.3%	0.8%	1.6%	2.1%	0.4%	0.0%	0.0%	
				13:00	14:00	250	1.3%	28.7%	21.7%	0.4%	10.8%	5.4%	1.7%	0.8%	5.0%	0.8%	3.8%	7.9%	8.8%	2.9%	0.0%	0.0%	
				14:00	15:00	250	1.3%	27.6%	30.7%	0.4%	10.3%	4.3%	1.3%	0.9%	5.6%	1.7%	2.6%	5.6%	6.0%	1.7%	0.0%	0.0%	
				15:00	16:00	300	1.8%	31.7%	31.0%	0.4%	9.6%	4.8%	1.5%	0.7%	4.4%	2.6%	1.8%	4.1%	4.1%	1.5%	0.0%	0.0%	
				16:00	17:00	300	2.2%	38.3%	26.9%	0.4%	7.1%	1.5%	0.4%	0.0%	4.5%	0.4%	3.0%	6.3%	7.1%	1.9%	0.0%	0.0%	
				17:00	18:00	350	2.7%	47.1%	27.4%	0.3%	4.5%	2.4%	0.6%	0.3%	3.9%	1.5%	1.5%	3.3%	3.3%	1.2%	0.0%	0.0%	
				18:00	19:00	350	2.6%	48.1%	28.5%	0.3%	3.3%	1.7%	0.3%	0.3%	1.7%	1.3%	2.0%	4.0%	4.6%	1.3%	0.0%	0.0%	
				19:00	20:00	250	2.0%	50.2%	34.1%	0.0%	1.6%	0.8%	0.0%	0.0%	5.3%	0.8%	0.8%	2.0%	2.0%	0.4%	0.0%	0.0%	
				20:00	21:00	200	2.0%	41.6%	42.8%	0.0%	1.5%	0.5%	0.0%	0.0%	8.6%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%	
				21:00	22:00	200	1.7%	37.8%	49.5%	0.0%	1.7%	0.6%	0.0%	0.0%	8.1%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	200	2.5%	30.8%	52.3%	0.0%	1.3%	0.0%	0.0%	0.0%	9.4%	0.6%	0.6%	1.3%	0.6%	0.6%	0.0%	0.0%	
				23:00	0:00	150	2.3%	29.5%	52.5%	0.0%	1.6%	0.0%	0.0%	0.0%	9.3%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L129	Kau Pui Lung Road (NB)	Ko Shan Road	Kiang Su Street	0:00	1:00	150	2.2%	19.0%	68.7%	0.0%	2.2%	0.0%	0.0%	0.0%	3.6%	0.7%	0.7%	1.5%	0.7%	0.7%	0.0%	0.0%
				1:00	2:00	100	2.6%	19.2%	74.4%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	2.7%	17.6%	77.0%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.8%	19.2%	73.2%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.2%	17.4%	78.2%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.7%	15.3%	67.7%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	1.7%	1.7%	3.4%	1.7%	1.7%	0.0%	0.0%
				6:00	7:00	150	1.8%	14.0%	63.9%	0.0%	4.4%	0.9%	0.0%	0.0%	4.4%	0.9%	1.8%	3.5%	2.6%	1.8%	0.0%	0.0%
				7:00	8:00	300	1.5%	22.3%	52.3%	0.4%	5.2%	1.1%	0.4%	0.4%	6.3%	1.5%	1.1%	3.0%	2.6%	1.9%	0.0%	0.0%
				8:00	9:00	400	2.9%	32.0%	38.6%	0.3%	8.9%	4.4%	1.0%	0.8%	3.4%	1.0%	1.0%	2.3%	2.1%	1.3%	0.0%	0.0%
				9:00	10:00	400	2.2%	25.9%	41.5%	0.3%	8.9%	5.0%	1.1%	0.8%	3.1%	1.7%	1.4%	3.1%	3.1%	1.9%	0.0%	0.0%
				10:00	11:00	350	2.4%	21.1%	39.7%	0.3%	17.4%	7.0%	1.5%	1.2%	2.8%	0.6%	0.9%	2.1%	1.8%	1.2%	0.0%	0.0%
				11:00	12:00	350	1.6%	21.0%	34.8%	0.6%	22.3%	2.9%	0.6%	0.3%	5.4%	2.2%	1.3%	2.9%	2.5%	1.6%	0.0%	0.0%
				12:00	13:00	350	1.9%	27.1%	42.6%	0.3%	14.6%	2.5%	0.6%	0.3%	3.8%	2.5%	0.6%	1.3%	1.3%	0.6%	0.0%	0.0%
				13:00	14:00	350	1.6%	18.5%	28.7%	0.3%	18.2%	8.9%	1.9%	1.6%	3.8%	0.6%	2.5%	5.4%	5.1%	2.9%	0.0%	0.0%
				14:00	15:00	350	1.3%	17.3%	38.9%	0.3%	16.9%	6.7%	1.6%	1.3%	4.2%	1.0%	1.6%	3.8%	3.2%	1.9%	0.0%	0.0%
				15:00	16:00	400	1.6%	19.6%	39.7%	0.3%	16.1%	7.4%	1.6%	1.4%	3.3%	1.6%	1.1%	2.7%	2.2%	1.4%	0.0%	0.0%
				16:00	17:00	350	2.3%	24.3%	34.3%	0.3%	17.2%	3.6%	0.6%	0.6%	3.9%	0.3%	1.9%	4.5%	3.9%	2.3%	0.0%	0.0%
				17:00	18:00	400	2.7%	30.3%	36.1%	0.3%	11.3%	5.6%	1.3%	1.1%	3.5%	1.1%	1.1%	2.4%	1.9%	1.3%	0.0%	0.0%
				18:00	19:00	350	2.8%	32.3%	39.2%	0.3%	8.9%	3.7%	0.9%	0.6%	1.5%	0.9%	1.5%	3.1%	2.8%	1.5%	0.0%	0.0%
				19:00	20:00	300	2.4%	34.8%	49.2%	0.0%	2.7%	1.0%	0.3%	0.3%	4.4%	0.7%	0.7%	1.4%	1.4%	0.7%	0.0%	0.0%
				20:00	21:00	300	2.4%	27.5%	57.7%	0.0%	2.4%	0.4%	0.4%	0.0%	6.8%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%
				21:00	22:00	250	1.7%	23.8%	63.3%	0.0%	3.0%	0.9%	0.4%	0.0%	6.1%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%
				22:00	23:00	250	2.3%	19.0%	66.6%	0.0%	1.9%	0.0%	0.0%	0.0%	6.9%	0.5%	0.5%	0.9%	0.5%	0.9%	0.0%	0.0%
				23:00	0:00	200	2.8%	18.1%	66.0%	0.0%	2.3%	0.0%	0.0%	0.0%	6.8%	0.6%	0.6%	1.1%	0.6%	1.1%	0.0%	0.0%
L130	Ko Shan Road (NB)	Chi Kiang Street	Anhui Street	0:00	1:00	150	2.7%	12.9%	73.5%	0.0%	2.7%	0.0%	0.0%	0.0%	3.4%	0.7%	0.7%	2.0%	0.7%	0.7%	0.0%	0.0%
				1:00	2:00	100	2.4%	13.4%	80.5%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	2.6%	11.5%	83.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.5%	14.0%	77.2%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	2.0%	11.8%	82.3%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.6%	11.1%	72.9%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	1.6%	1.6%	3.2%	1.6%	1.6%	0.0%	0.0%
				6:00	7:00	150	1.6%	9.8%	67.5%	0.0%	4.9%	0.8%	0.0%	0.0%	4.1%	1.6%	1.6%	3.3%	2.4%	2.4%	0.0%	0.0%
				7:00	8:00	300	1.8%	17.1%	55.8%	0.4%	5.5%	1.1%	0.4%	0.0%	6.2%	1.5%	1.5%	3.6%	2.9%	2.2%	0.0%	0.0%
				8:00	9:00	400	3.4%	25.3%	42.8%	0.3%	9.2%	4.5%	1.1%	0.5%	3.4%	1.3%	1.3%	2.9%	2.4%	1.6%	0.0%	0.0%
				9:00	10:00	400	2.5%	19.9%	45.2%	0.3%	9.1%	5.0%	1.1%	0.6%	3.0%	2.2%	1.7%	3.9%	3.3%	2.2%	0.0%	0.0%
				10:00	11:00	350	2.7%	15.6%	44.2%	0.3%	17.4%	6.9%	1.5%	0.9%	2.7%	0.9%	1.2%	2.4%	2.1%	1.2%	0.0%	0.0%
				11:00	12:00	350	1.9%	15.2%	38.3%	0.6%	22.3%	2.8%	0.6%	0.3%	5.3%	2.8%	1.5%	3.4%	3.1%	1.9%	0.0%	0.0%
				12:00	13:00	350	2.2%	20.1%	47.9%	0.3%	14.8%	2.5%	0.6%	0.3%	3.8%	2.8%	0.6%	1.6%	1.6%	0.9%	0.0%	0.0%
				13:00	14:00	350	1.5%	13.2%	31.8%	0.3%	18.2%	8.6%	1.8%	1.2%	3.7%	0.9%	3.1%	6.5%	5.8%	3.4%	0.0%	0.0%
				14:00	15:00	350	1.5%	12.3%	43.2%	0.3%	16.6%	6.5%	1.2%	0.9%	4.0%	1.2%	1.8%	4.3%	3.7%	2.5%	0.0%	0.0%
				15:00	16:00	400	2.1%	14.2%	43.8%	0.3%	15.8%	7.1%	1.6%	1.1%	3.2%	2.1%	1.3%	3.2%	2.6%	1.6%	0.0%	0.0%
				16:00	17:00	350	2.5%	16.6%	40.0%	0.3%	16.9%	3.4%	0.6%	0.3%	3.8%	0.3%	2.5%	5.3%	5.0%	2.5%	0.0%	0.0%
				17:00	18:00	400	3.2%	21.4%	43.1%	0.3%	11.5%	5.6%	1.1%	0.8%	3.5%	1.3%	1.3%	2.9%	2.4%	1.6%	0.0%	0.0%
				18:00	19:00	350	3.4%	22.6%	46.3%	0.3%	9.1%	3.7%	0.6%	0.6%	1.5%	1.2%	1.8%	3.7%	3.4%	1.8%	0.0%	0.0%
				19:00	20:00	300	2.7%	26.2%	56.5%	0.0%	3.1%	1.0%	0.0%	0.0%	4.4%	1.0%	0.7%	1.7%	1.7%	1.0%	0.0%	0.0%
				20:00	21:00	300	2.8%	20.5%	64.8%	0.0%	2.4%	0.4%	0.0%	0.0%	6.7%	0.4%	0.4%	0.8%	0.4%	0.4%	0.0%	0.0%
				21:00	22:00	250	2.1%	17.2%	69.8%	0.0%	3.4%	0.8%	0.0%	0.0%	5.9%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%
				22:00	23:00	250	2.6%	13.6%	72.0%	0.0%	1.8%	0.0%	0.0%	0.0%	6.6%	0.4%	0.4%	1.3%	0.4%	0.9%	0.0%	0.0%
				23:00	0:00	200	2.7%	12.9%	71.5%	0.0%	2.2%	0.0%	0.0%	0.0%	6.5%	0.5%	0.5%	1.6%	0.5%	1.1%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L131	Ko Shan Road (SB)	Anhui Street	Chi Kiang Street	0:00	1:00	100	2.0%	30.3%	54.6%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	5.1%	1.0%	1.0%	2.0%	1.0%	1.0%	0.0%	0.0%	
				1:00	2:00	100	1.9%	33.3%	62.9%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.0%	30.0%	66.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	2.8%	33.3%	61.1%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.0%	30.3%	63.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	2.4%	26.2%	54.6%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	2.4%	4.8%	2.4%	2.4%	0.0%	0.0%	0.0%
				6:00	7:00	100	1.2%	22.9%	50.7%	0.0%	2.4%	1.2%	0.0%	0.0%	6.0%	2.4%	2.4%	4.8%	4.8%	1.2%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	200	1.1%	32.6%	36.9%	0.5%	4.3%	1.1%	0.5%	0.0%	9.1%	2.1%	1.6%	4.3%	4.3%	1.6%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	300	2.5%	44.3%	25.9%	0.4%	6.7%	3.5%	1.1%	0.7%	4.6%	1.4%	1.4%	3.2%	3.2%	1.1%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	300	1.9%	36.5%	28.8%	0.4%	7.0%	4.3%	1.2%	0.8%	4.3%	2.3%	1.9%	4.3%	4.7%	1.6%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	250	2.5%	34.6%	31.4%	0.4%	10.5%	4.6%	1.3%	0.8%	3.8%	1.3%	1.3%	2.9%	3.3%	1.3%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	250	1.7%	33.2%	26.5%	0.8%	13.0%	1.7%	0.4%	0.4%	7.1%	3.4%	1.7%	4.2%	4.6%	1.3%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	250	2.1%	42.1%	31.7%	0.4%	8.2%	1.6%	0.4%	0.4%	4.9%	3.3%	0.8%	1.6%	2.1%	0.4%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	250	1.3%	28.7%	21.7%	0.4%	10.8%	5.4%	1.7%	0.8%	5.0%	0.8%	3.8%	7.9%	8.8%	2.9%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	250	1.3%	27.6%	30.7%	0.4%	10.3%	4.3%	1.3%	0.9%	5.6%	1.7%	2.6%	5.6%	6.0%	1.7%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	300	1.8%	31.7%	31.0%	0.4%	9.6%	4.8%	1.5%	0.7%	4.4%	2.6%	1.8%	4.1%	4.1%	1.5%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	300	2.2%	38.3%	26.9%	0.4%	7.1%	1.5%	0.4%	0.0%	4.5%	0.4%	3.0%	6.3%	7.1%	1.9%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	350	2.7%	47.1%	27.4%	0.3%	4.5%	2.4%	0.6%	0.3%	3.9%	1.5%	1.5%	3.3%	3.3%	1.2%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	350	2.6%	48.1%	28.5%	0.3%	3.3%	1.7%	0.3%	0.3%	1.7%	1.3%	2.0%	4.0%	4.6%	1.3%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	2.0%	50.2%	34.1%	0.0%	1.6%	0.8%	0.0%	0.0%	5.3%	0.8%	0.8%	2.0%	2.0%	0.4%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	200	2.0%	41.6%	42.8%	0.0%	1.5%	0.5%	0.0%	0.0%	8.6%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	200	1.7%	37.8%	49.5%	0.0%	1.7%	0.6%	0.0%	0.0%	8.1%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	200	2.5%	30.8%	52.3%	0.0%	1.3%	0.0%	0.0%	0.0%	9.4%	0.6%	0.6%	1.3%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	150	2.3%	29.5%	52.5%	0.0%	1.6%	0.0%	0.0%	0.0%	9.3%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%	0.0%	0.0%
L132	Chatham Road North (NB)	Anhui Street	Chi Kiang Street	0:00	1:00	500	9.8%	17.6%	49.7%	0.0%	2.4%	0.0%	0.0%	0.0%	10.8%	0.6%	0.6%	1.6%	0.8%	0.4%	0.0%	5.7%		
				1:00	2:00	250	11.1%	20.5%	61.5%	0.0%	3.7%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	
				2:00	3:00	250	9.3%	19.0%	65.4%	0.0%	2.7%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	
				3:00	4:00	200	12.7%	20.5%	59.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	
				4:00	5:00	150	10.7%	18.8%	63.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%	2.7%	
				5:00	6:00	200	9.0%	16.9%	58.4%	0.0%	5.6%	0.0%	0.0%	0.6%	0.0%	1.1%	1.7%	3.4%	2.2%	1.1%	0.0%	0.0%	0.0%	
				6:00	7:00	450	6.5%	12.7%	45.2%	0.5%	4.6%	0.7%	0.0%	0.2%	11.5%	1.4%	1.7%	3.4%	2.9%	1.0%	0.0%	0.0%	7.7%	
				7:00	8:00	1,200	2.1%	16.0%	42.7%	0.7%	3.5%	0.6%	0.3%	0.3%	18.2%	0.9%	0.9%	1.9%	1.6%	0.5%	0.0%	0.0%	9.8%	
				8:00	9:00	1,550	4.7%	25.4%	34.4%	1.0%	6.6%	2.4%	1.4%	1.4%	11.0%	0.8%	0.8%	1.6%	1.4%	0.4%	0.0%	0.0%	6.7%	
				9:00	10:00	1,450	3.6%	20.6%	37.3%	0.9%	6.6%	2.7%	1.5%	1.6%	10.2%	1.4%	1.1%	2.1%	2.0%	0.6%	0.0%	0.0%	7.8%	
				10:00	11:00	1,300	8.3%	18.2%	25.9%	1.6%	15.3%	5.1%	1.8%	1.8%	7.3%	0.9%	1.0%	2.0%	2.0%	0.6%	0.0%	0.0%	8.2%	
				11:00	12:00	1,300	5.4%	17.1%	21.5%	2.0%	18.7%	1.9%	0.7%	0.6%	12.9%	2.4%	1.4%	2.8%	2.9%	0.8%	0.0%	0.0%	8.9%	
				12:00	13:00	1,250	7.0%	23.5%	28.1%	1.5%	12.9%	1.8%	0.7%	0.7%	10.1%	2.8%	0.7%	1.2%	1.4%	0.3%	0.0%	0.0%	7.3%	
				13:00	14:00	1,300	4.9%	15.1%	17.9%	1.7%	15.2%	6.1%	2.2%	2.2%	9.3%	0.7%	2.7%	5.2%	5.4%	1.5%	0.0%	0.0%	9.9%	
				14:00	15:00	1,250	4.4%	14.6%	25.5%	1.6%	14.8%	4.9%	1.7%	1.7%	10.3%	1.2%	1.8%	3.6%	3.6%	1.1%	0.0%	0.0%	9.2%	
				15:00	16:00	1,450	6.4%	17.0%	26.1%	1.5%	14.2%	5.3%	2.0%	1.9%	8.1%	2.0%	1.3%	2.7%	2.6%	0.8%	0.0%	0.0%	8.1%	
				16:00	17:00	1,300	11.4%	21.2%	12.7%	1.3%	15.9%	2.8%	0.6%	0.6%	6.8%	0.5%	2.8%	5.4%	6.1%	1.6%	0.0%	0.0%	10.3%	
				17:00	18:00	1,550	14.9%	27.2%	13.8%	1.0%	10.8%	4.7%	1.2%	1.1%	6.1%	1.6%	1.6%	3.0%	3.0%	1.0%	0.0%	0.0%	9.0%	
				18:00	19:00	1,300	15.9%	30.2%	15.5%	0.7%	9.0%	3.3%	0.8%	0.8%	2.9%	1.3%	2.3%	4.0%	4.4%	1.2%	0.0%	0.0%	7.7%	
				19:00	20:00	1,150	8.5%	30.4%	32.8%	0.2%	2.5%	0.8%	0.2%	0.3%	11.3%	0.9%	0.7%	1.4%	1.5%	0.3%	0.0%	0.0%	8.2%	
				20:00	21:00	1,000	8.0%	23.3%	37.7%	0.2%	2.0%	0.3%	0.2%	0.3%	17.1%	0.4%	0.3%	0.6%	0.4%	0.2%	0.0%	0.0%	9.0%	
				21:00	22:00	900	6.0%	20.5%	42.3%	0.2%	2.8%	0.7%	0.2%	0.3%	16.0%	0.0%	0.1%	0.2%	0.0%	0.1%	0.0%	0.0%	10.6%	
				22:00	23:00	900	7.8%	15.4%	41.6%	0.2%	1.6%	0.0%	0.0%	0.0%	16.5%	0.6%	0.4%	0.9%	0.4%	0.3%	0.0%	0.0%	14.3%	
				23:00	0:00	750	8.1%	14.8%	41.1%	0.3%	1.9%	0.0%	0.0%	0.0%	16.4%	0.7%	0.5%	1.1%	0.5%	0.4%	0.0%	0.0%	14.2%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L133	Chi Kiang Street (EB)	Chatham Road North	Ha Heung Road	0:00	1:00	300	3.4%	30.4%	53.8%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	200	3.0%	32.9%	59.8%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	2.7%	30.2%	64.4%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	150	3.6%	32.4%	58.6%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.1%	29.9%	62.9%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	150	2.5%	27.1%	57.9%	0.0%	5.9%	0.0%	0.0%	0.8%	0.0%	0.8%	0.8%	1.7%	1.7%	0.8%	0.0%	0.0%	0.0%
				6:00	7:00	250	2.1%	23.6%	50.8%	0.4%	5.4%	1.2%	0.0%	0.4%	3.7%	0.8%	1.2%	2.5%	2.1%	0.8%	0.0%	5.0%	
				7:00	8:00	700	1.8%	33.2%	37.0%	0.6%	5.4%	1.3%	0.7%	0.6%	4.8%	1.0%	1.0%	2.1%	2.1%	0.9%	0.0%	7.5%	
				8:00	9:00	1,100	3.1%	43.0%	24.5%	0.7%	8.2%	4.4%	2.4%	2.2%	2.4%	0.8%	0.8%	1.4%	1.4%	0.6%	0.0%	4.1%	
				9:00	10:00	1,000	2.4%	36.2%	27.4%	0.6%	8.6%	5.1%	2.6%	2.5%	2.3%	1.4%	1.0%	2.0%	2.1%	0.8%	0.0%	5.0%	
				10:00	11:00	900	2.6%	28.8%	25.5%	1.1%	16.6%	7.3%	3.8%	3.6%	2.1%	0.5%	0.6%	1.2%	1.3%	0.5%	0.0%	4.5%	
				11:00	12:00	800	1.8%	29.7%	23.0%	1.5%	22.0%	3.0%	1.6%	1.3%	4.0%	1.5%	0.9%	1.8%	2.0%	0.6%	0.0%	5.3%	
				12:00	13:00	800	2.1%	38.1%	28.1%	1.0%	14.3%	2.6%	1.6%	1.3%	2.9%	1.6%	0.4%	0.8%	0.9%	0.3%	0.0%	4.0%	
				13:00	14:00	850	1.6%	24.9%	18.3%	1.2%	17.2%	8.9%	4.6%	4.6%	2.8%	0.5%	1.7%	3.3%	3.6%	1.2%	0.0%	5.6%	
				14:00	15:00	800	1.4%	24.4%	26.3%	1.1%	16.8%	7.1%	3.7%	3.4%	3.1%	0.8%	1.1%	2.3%	2.4%	0.9%	0.0%	5.2%	
				15:00	16:00	950	2.0%	27.4%	26.0%	1.1%	15.5%	7.5%	4.0%	3.7%	2.3%	1.2%	0.9%	1.6%	1.7%	0.6%	0.0%	4.5%	
				16:00	17:00	800	2.4%	33.4%	22.6%	1.1%	17.0%	3.6%	1.6%	1.6%	3.1%	0.3%	1.2%	2.7%	3.1%	0.9%	0.0%	5.4%	
				17:00	18:00	1,000	2.8%	40.3%	22.6%	0.7%	10.7%	5.7%	3.0%	2.8%	2.5%	0.6%	0.6%	1.4%	1.4%	0.5%	0.0%	4.4%	
				18:00	19:00	850	3.0%	43.7%	25.2%	0.5%	8.8%	3.8%	2.1%	2.1%	1.2%	0.5%	0.9%	1.9%	2.0%	0.6%	0.0%	3.7%	
				19:00	20:00	800	2.7%	48.9%	32.5%	0.1%	2.8%	1.2%	0.4%	0.7%	3.2%	0.5%	0.4%	0.9%	0.9%	0.3%	0.0%	4.5%	
				20:00	21:00	650	2.7%	40.8%	40.5%	0.2%	2.5%	0.5%	0.5%	0.5%	5.3%	0.2%	0.2%	0.3%	0.3%	0.2%	0.0%	5.3%	
				21:00	22:00	550	2.0%	35.7%	44.8%	0.2%	3.3%	1.1%	0.6%	0.6%	5.0%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	6.3%	
				22:00	23:00	500	2.8%	29.3%	48.7%	0.2%	2.0%	0.0%	0.0%	0.0%	5.6%	0.4%	0.4%	0.6%	0.4%	0.4%	0.0%	9.2%	
				L134	Chi Kiang Street (EB)	Ha Heung Road	To Kwa Wan Road	23:00	0:00	450	3.0%	28.3%	48.2%	0.2%	2.5%	0.0%	0.0%	0.0%	5.7%	0.5%	0.5%	0.7%	0.5%
0:00	1:00	250	2.5%					26.4%	55.1%	0.0%	3.0%	0.0%	0.0%	0.0%	5.0%	0.5%	0.5%	1.0%	0.5%	0.5%	0.0%	5.0%	
1:00	2:00	150	2.8%					29.0%	63.5%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	100	2.0%					26.5%	68.4%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	100	2.8%					29.2%	62.4%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	100	3.1%					26.2%	66.1%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	100	2.5%					24.1%	59.3%	0.0%	6.3%	0.0%	0.0%	1.3%	0.0%	1.3%	1.3%	1.3%	1.3%	1.3%	0.0%	0.0%	
6:00	7:00	200	1.8%					19.6%	51.1%	0.6%	6.0%	1.2%	0.0%	0.6%	5.4%	0.6%	1.2%	1.8%	1.8%	1.2%	0.0%	7.1%	
7:00	8:00	500	1.3%					28.5%	37.9%	0.6%	5.5%	1.5%	0.6%	0.6%	6.8%	0.8%	0.8%	1.7%	1.7%	1.1%	0.0%	10.6%	
8:00	9:00	750	2.4%					38.6%	26.4%	0.8%	8.8%	4.9%	2.0%	1.8%	3.5%	0.7%	0.7%	1.3%	1.3%	0.7%	0.0%	6.1%	
9:00	10:00	700	1.8%					32.2%	29.0%	0.8%	9.0%	5.6%	2.1%	2.1%	3.4%	1.2%	0.9%	1.7%	1.8%	1.1%	0.0%	7.3%	
10:00	11:00	600	1.9%					25.2%	26.4%	1.4%	17.9%	8.0%	3.1%	3.1%	3.1%	0.3%	0.5%	1.0%	1.0%	0.5%	0.0%	6.6%	
11:00	12:00	550	1.3%					25.2%	23.4%	1.8%	23.2%	3.3%	1.3%	1.1%	5.9%	1.3%	0.7%	1.5%	1.6%	0.7%	0.0%	7.7%	
12:00	13:00	550	1.7%					33.3%	29.3%	1.3%	15.3%	2.8%	1.3%	1.1%	4.3%	1.5%	0.4%	0.6%	0.7%	0.4%	0.0%	6.0%	
13:00	14:00	600	1.2%					21.3%	18.8%	1.6%	18.3%	9.8%	3.9%	3.9%	4.1%	0.4%	1.4%	2.7%	3.0%	1.4%	0.0%	8.2%	
14:00	15:00	550	0.9%					21.0%	26.7%	1.5%	17.7%	7.8%	3.0%	3.0%	4.5%	0.6%	0.9%	1.9%	2.0%	0.9%	0.0%	7.6%	
15:00	16:00	650	1.4%					23.7%	27.0%	1.4%	16.6%	8.3%	3.5%	3.1%	3.5%	0.9%	0.6%	1.4%	1.4%	0.6%	0.0%	6.6%	
16:00	17:00	500	1.8%					28.9%	22.8%	1.4%	18.8%	4.0%	1.4%	1.4%	4.6%	0.2%	1.0%	2.2%	2.4%	0.8%	0.0%	8.3%	
17:00	18:00	650	2.1%					35.1%	23.3%	1.0%	12.1%	6.5%	2.8%	2.6%	3.9%	0.5%	0.5%	1.1%	1.1%	0.5%	0.0%	6.9%	
18:00	19:00	550	2.4%					38.7%	26.5%	0.6%	10.2%	4.5%	2.0%	2.0%	2.0%	0.4%	0.8%	1.6%	1.8%	0.6%	0.0%	5.9%	
19:00	20:00	500	2.0%					43.6%	34.4%	0.2%	3.0%	1.4%	0.4%	0.6%	4.8%	0.4%	0.4%	0.8%	0.8%	0.4%	0.0%	6.8%	
20:00	21:00	450	2.0%					35.2%	41.8%	0.2%	2.7%	0.5%	0.5%	0.5%	7.8%	0.2%	0.2%	0.2%	0.2%	0.2%	0.0%	7.8%	
21:00	22:00	400	1.3%					30.5%	45.7%	0.3%	3.5%	1.1%	0.5%	0.5%	7.2%	0.0%	0.0%	0.3%	0.0%	0.0%	0.0%	9.1%	
22:00	23:00	350	2.0%					24.6%	48.2%	0.3%	2.0%	0.0%	0.0%	0.0%	8.0%	0.3%	0.3%	0.6%	0.3%	0.3%	0.0%	13.1%	
23:00	0:00	300	2.1%	23.5%	47.8%	0.4%	2.5%	0.0%	0.0%	0.0%	8.1%	0.4%	0.4%	0.7%	0.4%	0.4%	0.0%	13.3%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L135	Anhui Street (WB)	Chatham Road North	Ko Shan Road	0:00	1:00	100	1.9%	24.5%	69.8%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	3.0%	24.2%	69.8%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	3.2%	22.6%	71.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.5%	22.7%	68.3%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	21.1%	73.6%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	21.7%	69.6%	0.0%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	2.3%	18.6%	67.5%	0.0%	7.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	2.3%	0.0%	0.0%
				7:00	8:00	150	1.8%	41.1%	43.7%	0.0%	7.1%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%	1.8%	0.0%	1.8%	0.0%	0.0%
				8:00	9:00	200	3.7%	49.3%	27.4%	0.0%	10.5%	1.6%	3.2%	2.1%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%
				9:00	10:00	200	3.0%	42.9%	31.3%	0.0%	11.4%	1.8%	3.6%	2.4%	0.0%	0.0%	0.0%	1.8%	0.0%	1.8%	0.0%	0.0%
				10:00	11:00	150	2.1%	25.0%	35.4%	0.0%	21.5%	6.9%	4.9%	2.8%	0.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				11:00	12:00	150	1.5%	26.9%	33.1%	0.0%	30.0%	3.1%	2.3%	0.8%	0.0%	0.0%	0.0%	1.5%	0.0%	0.8%	0.0%	0.0%
				12:00	13:00	150	1.5%	33.8%	38.9%	0.0%	18.8%	2.3%	2.3%	0.8%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.0%	0.0%
				13:00	14:00	150	1.5%	23.7%	26.7%	0.0%	24.4%	9.2%	6.1%	3.8%	0.0%	0.0%	0.0%	2.3%	0.0%	2.3%	0.0%	0.0%
				14:00	15:00	150	0.8%	21.7%	37.0%	0.0%	22.5%	7.0%	4.7%	3.1%	0.0%	0.0%	0.0%	1.6%	0.0%	1.6%	0.0%	0.0%
				15:00	16:00	200	1.3%	24.2%	36.2%	0.0%	20.4%	7.0%	5.1%	3.2%	0.0%	0.0%	0.0%	1.3%	0.0%	1.3%	0.0%	0.0%
				16:00	17:00	150	0.9%	18.3%	42.3%	0.0%	25.7%	6.4%	2.8%	0.9%	0.0%	0.0%	0.0%	1.8%	0.0%	0.9%	0.0%	0.0%
				17:00	18:00	150	0.7%	22.1%	43.4%	0.0%	16.2%	9.6%	4.4%	2.2%	0.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				18:00	19:00	150	0.9%	24.6%	49.0%	0.0%	13.2%	6.1%	2.6%	1.8%	0.0%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%
				19:00	20:00	150	2.4%	43.5%	46.1%	0.0%	4.0%	0.8%	0.8%	0.8%	0.0%	0.0%	0.0%	0.8%	0.0%	0.8%	0.0%	0.0%
				20:00	21:00	100	2.0%	36.4%	57.6%	0.0%	3.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	1.1%	31.2%	61.2%	0.0%	4.3%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	2.4%	26.8%	68.4%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				L136	Chatham Road North (SB)	Chi Kiang Street	Shek Tong Street	23:00	0:00	100	3.0%	25.8%	68.2%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	300	6.4%					28.5%	47.8%	0.0%	2.3%	0.0%	0.0%	0.0%	3.7%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	9.4%
1:00	2:00	200	6.9%					31.4%	55.4%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%
2:00	3:00	150	6.2%					29.0%	59.2%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%
3:00	4:00	150	8.2%					30.9%	52.8%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.6%
4:00	5:00	100	7.3%					28.1%	57.3%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%
5:00	6:00	150	6.4%					26.6%	56.1%	0.0%	5.5%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	1.8%	0.9%	0.9%	0.0%	0.0%
6:00	7:00	250	4.5%					21.8%	45.5%	0.4%	4.5%	1.2%	0.0%	0.0%	4.1%	0.8%	0.8%	1.6%	1.2%	0.4%	0.0%	13.2%
7:00	8:00	750	3.3%					23.7%	32.5%	0.5%	5.9%	1.6%	0.3%	0.3%	3.4%	0.8%	0.8%	1.6%	1.4%	0.5%	0.0%	23.4%
8:00	9:00	1,100	6.4%					33.1%	23.1%	0.7%	9.5%	5.5%	1.0%	1.0%	1.9%	0.6%	0.6%	1.2%	1.0%	0.4%	0.0%	14.0%
9:00	10:00	1,000	4.9%					27.1%	25.3%	0.7%	9.8%	6.2%	1.1%	1.1%	1.7%	1.1%	0.9%	1.6%	1.5%	0.5%	0.0%	16.5%
10:00	11:00	850	5.3%					27.8%	23.6%	1.1%	14.5%	6.5%	1.6%	1.6%	2.4%	0.4%	0.5%	0.8%	0.7%	0.4%	0.0%	12.8%
11:00	12:00	800	3.6%					28.0%	20.9%	1.5%	18.8%	2.5%	0.6%	0.5%	4.6%	1.1%	0.6%	1.1%	1.1%	0.4%	0.0%	14.7%
12:00	13:00	800	4.4%					36.2%	25.5%	1.0%	12.2%	2.3%	0.6%	0.5%	3.4%	1.1%	0.3%	0.5%	0.5%	0.1%	0.0%	11.4%
13:00	14:00	800	3.3%					24.4%	17.2%	1.3%	15.2%	8.1%	2.0%	2.0%	3.3%	0.4%	1.3%	2.1%	2.1%	0.9%	0.0%	16.4%
14:00	15:00	800	2.9%					23.6%	24.3%	1.2%	14.6%	6.5%	1.6%	1.5%	3.7%	0.5%	0.8%	1.6%	1.5%	0.5%	0.0%	15.2%
15:00	16:00	900	4.1%					26.5%	24.4%	1.1%	13.8%	6.8%	1.8%	1.7%	2.8%	0.9%	0.6%	1.1%	1.0%	0.4%	0.0%	13.0%
16:00	17:00	750	4.8%					40.0%	19.8%	1.0%	10.2%	2.0%	0.7%	0.6%	4.8%	0.1%	0.7%	1.1%	1.3%	0.6%	0.0%	12.3%
17:00	18:00	950	5.6%					46.9%	19.5%	0.7%	6.3%	3.1%	1.2%	1.1%	3.9%	0.3%	0.3%	0.5%	0.5%	0.3%	0.0%	9.8%
18:00	19:00	800	5.9%					50.6%	21.5%	0.4%	5.1%	2.1%	0.8%	0.8%	1.8%	0.3%	0.5%	0.8%	0.8%	0.4%	0.0%	8.2%
19:00	20:00	800	5.1%					44.7%	28.6%	0.1%	2.3%	1.0%	0.1%	0.3%	3.6%	0.4%	0.3%	0.5%	0.5%	0.1%	0.0%	12.4%
20:00	21:00	650	5.2%					35.9%	34.7%	0.2%	1.9%	0.5%	0.2%	0.2%	5.9%	0.2%	0.2%	0.3%	0.2%	0.2%	0.0%	14.2%
21:00	22:00	600	3.8%					31.5%	38.3%	0.2%	2.6%	0.9%	0.2%	0.2%	5.4%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	16.7%
22:00	23:00	600	5.0%					24.4%	38.8%	0.2%	1.4%	0.0%	0.0%	0.0%	5.7%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	23.3%
23:00	0:00	500	5.3%	23.4%	39.0%	0.2%	2.0%	0.0%	0.0%	0.0%	5.7%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	23.2%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L137	Chatham Road North (NB)	Kiang Hsi Street	Anhui Street	0:00	1:00	550	5.1%	25.7%	50.7%	0.0%	2.4%	0.0%	0.0%	0.0%	8.1%	0.4%	0.2%	0.8%	0.4%	0.2%	0.0%	6.0%	
				1:00	2:00	300	5.3%	28.8%	59.5%	0.0%	3.6%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	
				2:00	3:00	300	4.6%	26.5%	63.2%	0.0%	2.7%	0.0%	0.0%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.5%
				3:00	4:00	200	6.3%	28.8%	57.6%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%
				4:00	5:00	200	5.3%	26.5%	61.7%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	2.4%
				5:00	6:00	200	4.7%	25.0%	60.0%	0.0%	5.7%	0.0%	0.0%	0.5%	0.0%	0.5%	0.5%	1.6%	1.0%	0.5%	0.0%	0.0%	0.0%
				6:00	7:00	450	3.4%	19.7%	47.9%	0.5%	4.8%	0.9%	0.0%	0.2%	9.2%	0.7%	0.7%	1.6%	1.4%	0.7%	0.0%	8.3%	0.0%
				7:00	8:00	1,250	1.4%	23.2%	41.1%	0.6%	3.9%	0.7%	0.4%	0.4%	13.6%	0.5%	0.5%	1.2%	1.0%	0.5%	0.0%	11.0%	0.0%
				8:00	9:00	1,750	2.9%	34.9%	31.2%	0.9%	6.9%	2.7%	1.5%	1.4%	7.7%	0.4%	0.4%	0.9%	0.8%	0.4%	0.0%	7.0%	0.0%
				9:00	10:00	1,600	2.2%	28.8%	34.5%	0.9%	7.1%	3.1%	1.6%	1.6%	7.4%	0.7%	0.6%	1.3%	1.2%	0.6%	0.0%	8.4%	0.0%
				10:00	11:00	1,450	4.2%	26.4%	26.1%	1.5%	15.6%	5.3%	2.2%	2.2%	5.4%	0.4%	0.4%	0.9%	0.9%	0.3%	0.0%	8.2%	0.0%
				11:00	12:00	1,400	2.8%	25.6%	22.4%	2.0%	19.7%	2.0%	0.9%	0.7%	9.8%	1.1%	0.6%	1.4%	1.4%	0.4%	0.0%	9.2%	0.0%
				12:00	13:00	1,400	3.5%	33.3%	27.8%	1.4%	13.0%	1.8%	0.9%	0.7%	7.3%	1.2%	0.3%	0.6%	0.7%	0.2%	0.0%	7.3%	0.0%
				13:00	14:00	1,400	2.6%	23.0%	18.9%	1.7%	16.4%	6.5%	2.7%	2.8%	7.2%	0.4%	1.3%	2.6%	2.6%	0.8%	0.0%	10.5%	0.0%
				14:00	15:00	1,350	2.3%	22.0%	26.4%	1.6%	15.6%	5.1%	2.1%	2.1%	7.8%	0.6%	0.8%	1.7%	1.7%	0.6%	0.0%	9.6%	0.0%
				15:00	16:00	1,600	3.3%	25.0%	26.7%	1.5%	14.7%	5.4%	2.4%	2.3%	6.0%	0.9%	0.6%	1.3%	1.2%	0.4%	0.0%	8.3%	0.0%
				16:00	17:00	1,400	5.5%	33.3%	16.0%	1.3%	16.6%	2.7%	0.9%	0.9%	5.5%	0.2%	1.3%	2.3%	2.6%	0.5%	0.0%	10.4%	0.0%
				17:00	18:00	1,700	6.8%	40.2%	16.4%	1.0%	10.6%	4.4%	1.7%	1.6%	4.6%	0.7%	0.7%	1.2%	1.2%	0.3%	0.0%	8.6%	0.0%
				18:00	19:00	1,450	7.2%	44.7%	18.5%	0.6%	8.9%	3.0%	1.1%	1.2%	2.2%	0.6%	0.9%	1.6%	1.8%	0.4%	0.0%	7.3%	0.0%
				19:00	20:00	1,350	4.1%	42.2%	31.6%	0.2%	2.5%	0.8%	0.2%	0.4%	7.9%	0.4%	0.3%	0.6%	0.7%	0.2%	0.0%	7.9%	0.0%
				20:00	21:00	1,150	4.0%	33.5%	37.0%	0.2%	2.1%	0.4%	0.3%	0.3%	12.4%	0.2%	0.1%	0.3%	0.2%	0.1%	0.0%	8.9%	0.0%
				21:00	22:00	1,050	3.0%	29.2%	41.1%	0.2%	2.9%	0.7%	0.3%	0.3%	11.6%	0.0%	0.0%	0.1%	0.0%	0.1%	0.0%	10.5%	0.0%
				22:00	23:00	1,000	4.0%	22.9%	42.8%	0.2%	1.5%	0.0%	0.0%	0.0%	12.5%	0.3%	0.2%	0.4%	0.2%	0.2%	0.0%	14.8%	0.0%
L138	Ko Shan Road (SB)	Anhui Street	Kiang Hsi Street	23:00	0:00	800	4.2%	22.0%	42.3%	0.3%	2.0%	0.0%	0.0%	0.0%	12.6%	0.4%	0.3%	0.5%	0.3%	0.3%	0.0%	14.8%	
				0:00	1:00	150	1.8%	25.4%	61.2%	0.0%	1.8%	0.0%	0.0%	0.0%	4.4%	0.9%	0.9%	1.8%	0.9%	0.9%	0.0%	0.0%	
				1:00	2:00	100	3.2%	27.0%	68.2%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	1.7%	25.0%	71.6%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.3%	27.9%	67.5%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	2.6%	23.7%	71.1%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.1%	21.3%	64.0%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	2.1%	2.1%	2.1%	2.1%	0.0%	0.0%	0.0%	
				6:00	7:00	100	2.2%	19.4%	57.8%	0.0%	2.2%	1.1%	0.0%	0.0%	5.4%	1.1%	2.2%	3.2%	3.2%	2.2%	0.0%	0.0%	
				7:00	8:00	200	1.5%	33.3%	38.0%	0.5%	4.1%	1.0%	0.5%	0.5%	8.7%	2.1%	1.5%	3.1%	3.1%	2.1%	0.0%	0.0%	
				8:00	9:00	300	3.0%	45.1%	26.4%	0.3%	6.8%	3.0%	1.0%	1.0%	4.4%	1.4%	1.4%	2.4%	2.4%	1.4%	0.0%	0.0%	
				9:00	10:00	300	2.3%	37.9%	29.7%	0.4%	7.1%	3.4%	1.1%	1.1%	4.1%	2.3%	1.9%	3.4%	3.4%	1.9%	0.0%	0.0%	
				10:00	11:00	300	2.4%	31.4%	37.4%	0.4%	10.2%	4.3%	1.2%	1.2%	3.5%	0.8%	1.2%	2.4%	2.4%	1.2%	0.0%	0.0%	
				11:00	12:00	250	1.6%	30.5%	32.6%	0.8%	12.9%	1.6%	0.4%	0.4%	6.8%	2.8%	1.6%	3.2%	3.2%	1.6%	0.0%	0.0%	
				12:00	13:00	300	1.9%	37.4%	38.1%	0.4%	8.0%	1.5%	0.4%	0.4%	4.6%	3.1%	0.8%	1.1%	1.5%	0.8%	0.0%	0.0%	
				13:00	14:00	250	1.6%	27.2%	27.1%	0.4%	10.5%	5.3%	1.6%	1.6%	4.9%	0.8%	3.2%	6.1%	6.5%	3.2%	0.0%	0.0%	
				14:00	15:00	250	1.2%	25.5%	37.6%	0.4%	9.9%	4.1%	1.2%	1.2%	5.3%	1.2%	2.1%	4.1%	4.1%	2.1%	0.0%	0.0%	
				15:00	16:00	300	1.7%	29.0%	37.9%	0.3%	9.4%	4.2%	1.4%	1.4%	4.2%	2.1%	1.4%	2.8%	2.8%	1.4%	0.0%	0.0%	
				16:00	17:00	300	1.8%	32.9%	37.7%	0.4%	6.9%	1.4%	0.4%	0.4%	4.3%	0.4%	2.2%	4.3%	5.1%	1.8%	0.0%	0.0%	
				17:00	18:00	350	2.3%	39.4%	38.3%	0.3%	4.3%	2.3%	0.6%	0.6%	3.7%	1.2%	1.2%	2.3%	2.3%	1.2%	0.0%	0.0%	
				18:00	19:00	350	2.2%	40.4%	40.1%	0.3%	3.2%	1.6%	0.3%	0.3%	1.6%	1.0%	1.6%	2.9%	3.2%	1.3%	0.0%	0.0%	
				19:00	20:00	300	2.3%	44.5%	40.6%	0.0%	1.5%	0.8%	0.0%	0.0%	4.9%	0.8%	0.8%	1.5%	1.5%	0.8%	0.0%	0.0%	
				20:00	21:00	250	2.3%	36.2%	49.3%	0.0%	1.4%	0.5%	0.0%	0.0%	7.8%	0.5%	0.5%	0.5%	0.5%	0.5%	0.0%	0.0%	
				21:00	22:00	200	1.5%	32.5%	56.3%	0.0%	1.5%	0.5%	0.0%	0.0%	7.2%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	200	2.2%	26.0%	58.9%	0.0%	1.1%	0.0%	0.0%	0.0%	8.3%	0.6%	0.6%	1.1%	0.6%	0.6%	0.0%	0.0%					
23:00	0:00	150	2.7%	25.0%	58.6%	0.0%	1.4%	0.0%	0.0%	0.0%	8.1%	0.7%	0.7%	1.4%	0.7%	0.7%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L139	Ko Shan Road (NB)	Kiang Hsi Street	Anhui Street	0:00	1:00	100	3.1%	10.4%	74.2%	0.0%	1.0%	0.0%	0.0%	0.0%	5.2%	1.0%	1.0%	2.1%	1.0%	1.0%	0.0%	0.0%		
				1:00	2:00	100	3.8%	11.3%	83.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	50	2.0%	10.0%	86.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	2.9%	11.4%	82.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	3.0%	9.1%	84.9%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	2.6%	7.7%	76.7%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	2.6%	2.6%	0.0%	0.0%	0.0%	
				6:00	7:00	100	2.5%	7.6%	69.6%	0.0%	2.5%	0.0%	0.0%	0.0%	6.3%	1.3%	1.3%	3.8%	3.8%	1.3%	0.0%	0.0%	0.0%	
				7:00	8:00	200	1.3%	12.7%	56.2%	0.6%	3.8%	1.3%	0.0%	0.0%	10.8%	1.9%	1.9%	3.8%	3.8%	1.9%	0.0%	0.0%	0.0%	
				8:00	9:00	250	3.4%	20.7%	46.3%	0.5%	6.9%	3.9%	0.5%	0.5%	6.4%	1.5%	1.5%	3.4%	3.0%	1.5%	0.0%	0.0%	0.0%	
				9:00	10:00	200	2.5%	16.2%	48.3%	0.5%	6.6%	4.1%	0.5%	0.5%	5.6%	2.5%	2.0%	4.6%	4.1%	2.0%	0.0%	0.0%	0.0%	
				10:00	11:00	200	3.6%	13.5%	51.2%	0.5%	11.5%	4.7%	0.5%	0.5%	4.7%	1.0%	1.0%	3.1%	2.6%	1.6%	0.0%	0.0%	0.0%	
				11:00	12:00	200	2.1%	13.1%	42.9%	1.0%	14.7%	2.1%	0.0%	0.0%	8.9%	3.1%	1.6%	4.7%	4.2%	1.6%	0.0%	0.0%	0.0%	
				12:00	13:00	200	2.6%	17.3%	52.9%	0.5%	9.4%	1.6%	0.0%	0.0%	6.3%	3.7%	1.0%	2.1%	2.1%	0.5%	0.0%	0.0%	0.0%	
				13:00	14:00	200	2.1%	11.5%	35.6%	0.5%	12.0%	5.8%	0.5%	1.0%	6.3%	1.0%	3.7%	8.4%	7.9%	3.7%	0.0%	0.0%	0.0%	
				14:00	15:00	200	1.5%	10.8%	48.0%	0.5%	10.8%	4.1%	0.5%	0.5%	6.7%	1.5%	2.1%	5.7%	5.2%	2.1%	0.0%	0.0%	0.0%	
				15:00	16:00	250	2.7%	12.2%	49.2%	0.5%	10.4%	5.0%	0.5%	0.5%	5.4%	2.3%	1.8%	4.1%	3.6%	1.8%	0.0%	0.0%	0.0%	
				16:00	17:00	250	3.3%	15.1%	44.7%	0.5%	9.4%	1.9%	0.0%	0.0%	5.7%	0.5%	2.8%	7.1%	6.6%	2.4%	0.0%	0.0%	0.0%	
				17:00	18:00	250	4.4%	19.4%	48.5%	0.4%	6.5%	2.8%	0.4%	0.4%	5.2%	1.6%	1.6%	4.0%	3.2%	1.6%	0.0%	0.0%	0.0%	
				18:00	19:00	250	4.5%	19.7%	51.0%	0.4%	4.9%	1.8%	0.4%	0.4%	2.2%	1.3%	2.2%	4.9%	4.5%	1.8%	0.0%	0.0%	0.0%	
				19:00	20:00	200	3.2%	21.1%	59.5%	0.0%	1.6%	0.5%	0.0%	0.0%	7.0%	1.1%	1.1%	2.2%	2.2%	0.5%	0.0%	0.0%	0.0%	
				20:00	21:00	200	3.0%	15.8%	66.7%	0.0%	1.2%	0.0%	0.0%	0.0%	10.3%	0.6%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	
				21:00	22:00	200	2.0%	13.7%	71.7%	0.0%	2.0%	0.7%	0.0%	0.0%	9.2%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	200	2.6%	10.5%	71.6%	0.0%	1.3%	0.0%	0.0%	0.0%	9.9%	0.7%	0.7%	1.3%	0.7%	0.7%	0.0%	0.0%	0.0%	
L140	Kiang Hsi Street (EB)	Ko Shan Road	Chatham Road North	23:00	0:00	150	3.2%	9.7%	71.0%	0.0%	1.6%	0.0%	0.0%	0.0%	9.7%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%		
				0:00	1:00	100	3.3%	42.6%	45.9%	0.0%	1.6%	0.0%	0.0%	0.0%	6.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	2.6%	39.5%	44.8%	0.0%	2.6%	0.0%	0.0%	0.0%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	2.9%	37.1%	48.6%	0.0%	0.0%	0.0%	0.0%	0.0%	11.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	4.5%	45.5%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	5.0%	40.0%	55.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	4.3%	39.1%	52.3%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	2.2%	34.8%	45.5%	0.0%	2.2%	2.2%	0.0%	0.0%	8.7%	0.0%	0.0%	2.2%	2.2%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	200	0.0%	28.8%	50.9%	0.0%	2.6%	0.0%	1.3%	1.3%	8.5%	1.3%	1.3%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	250	0.0%	41.2%	37.1%	0.0%	4.5%	0.0%	4.5%	3.6%	4.5%	0.9%	0.9%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	250	0.0%	34.3%	41.2%	0.0%	4.5%	0.0%	5.0%	4.0%	4.5%	1.5%	1.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	200	3.0%	40.4%	22.0%	0.0%	6.5%	13.7%	4.2%	3.6%	4.2%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	150	2.0%	43.7%	21.6%	0.0%	9.5%	6.1%	2.0%	1.4%	8.8%	1.4%	0.7%	1.4%	1.4%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	200	2.5%	51.7%	23.9%	0.0%	5.5%	4.9%	1.8%	1.2%	6.1%	1.2%	0.0%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	200	1.9%	35.5%	16.3%	0.0%	6.9%	16.9%	5.0%	5.0%	5.6%	0.6%	1.3%	2.5%	1.9%	0.6%	0.0%	0.0%	0.0%	
				14:00	15:00	150	1.3%	35.3%	23.3%	0.0%	6.7%	14.0%	4.0%	3.3%	6.7%	0.7%	0.7%	2.0%	1.3%	0.7%	0.0%	0.0%	0.0%	
				15:00	16:00	200	2.1%	38.0%	22.5%	0.0%	6.4%	13.9%	4.3%	3.7%	4.8%	1.1%	0.5%	1.1%	1.1%	0.5%	0.0%	0.0%	0.0%	
				16:00	17:00	150	6.3%	65.7%	0.0%	0.0%	4.2%	13.3%	0.7%	0.7%	7.0%	0.0%	0.0%	1.4%	0.0%	0.7%	0.0%	0.0%	0.0%	
				17:00	18:00	250	6.1%	66.7%	0.0%	0.0%	2.3%	17.4%	0.9%	0.9%	4.7%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	0.0%	
				18:00	19:00	200	6.8%	74.9%	0.0%	0.0%	1.7%	11.9%	0.6%	0.6%	2.3%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%	0.0%	
				19:00	20:00	200	2.4%	60.6%	25.0%	0.0%	1.2%	1.8%	0.6%	0.6%	6.0%	0.6%	0.0%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	150	3.1%	51.7%	32.1%	0.0%	0.8%	0.8%	0.8%	0.8%	9.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	150	1.8%	47.3%	36.8%	0.0%	0.9%	1.8%	0.9%	0.9%	9.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.1%	41.2%	43.4%	0.0%	1.0%	0.0%	0.0%	0.0%	11.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	100	3.9%	40.3%	42.8%	0.0%	1.3%	0.0%	0.0%	0.0%	11.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L141	Chatham Road North (NB)	Shan Si Street	Kiang Hsi Street	0:00	1:00	500	5.7%	23.5%	51.9%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	7.6%	0.2%	0.2%	0.8%	0.4%	0.2%	0.0%	7.0%
				1:00	2:00	250	6.1%	26.9%	61.7%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%
				2:00	3:00	250	5.3%	24.8%	65.4%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%
				3:00	4:00	200	7.1%	26.0%	59.2%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%
				4:00	5:00	150	6.0%	24.2%	63.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	2.7%
				5:00	6:00	200	5.3%	22.8%	60.7%	0.0%	5.8%	0.0%	0.0%	0.6%	0.0%	0.6%	0.6%	1.8%	1.2%	0.6%	0.0%	0.0%	
				6:00	7:00	400	3.9%	17.8%	48.6%	0.5%	4.9%	0.8%	0.0%	0.3%	8.5%	0.8%	0.8%	1.5%	1.3%	0.8%	0.0%	9.5%	
				7:00	8:00	1,100	2.0%	21.8%	40.3%	0.6%	4.0%	0.8%	0.4%	0.3%	13.3%	0.5%	0.5%	1.1%	0.9%	0.4%	0.0%	13.1%	
				8:00	9:00	1,500	4.1%	33.1%	30.9%	0.9%	7.0%	2.9%	1.2%	1.2%	7.6%	0.4%	0.4%	0.9%	0.7%	0.3%	0.0%	8.4%	
				9:00	10:00	1,400	3.2%	27.3%	34.0%	0.9%	7.3%	3.4%	1.3%	1.4%	7.3%	0.7%	0.5%	1.2%	1.1%	0.4%	0.0%	10.0%	
				10:00	11:00	1,250	4.7%	24.4%	26.8%	1.6%	16.1%	4.6%	2.0%	2.1%	5.2%	0.3%	0.4%	0.9%	0.9%	0.3%	0.0%	9.7%	
				11:00	12:00	1,250	3.2%	23.5%	22.9%	2.1%	20.2%	1.8%	0.8%	0.7%	9.3%	1.1%	0.6%	1.3%	1.3%	0.5%	0.0%	10.7%	
				12:00	13:00	1,250	3.9%	31.1%	28.7%	1.5%	13.4%	1.6%	0.8%	0.7%	7.0%	1.2%	0.2%	0.6%	0.6%	0.2%	0.0%	8.5%	
				13:00	14:00	1,200	3.0%	21.3%	19.4%	1.8%	16.8%	5.7%	2.5%	2.7%	6.9%	0.3%	1.3%	2.5%	2.6%	0.9%	0.0%	12.3%	
				14:00	15:00	1,200	2.6%	20.2%	27.0%	1.7%	16.0%	4.4%	2.0%	2.0%	7.5%	0.5%	0.8%	1.7%	1.7%	0.7%	0.0%	11.2%	
				15:00	16:00	1,400	3.6%	23.0%	27.5%	1.6%	15.2%	4.8%	2.2%	2.2%	5.8%	0.9%	0.6%	1.2%	1.2%	0.4%	0.0%	9.8%	
				16:00	17:00	1,250	5.5%	29.7%	17.8%	1.5%	17.2%	2.1%	0.9%	0.9%	5.1%	0.2%	1.2%	2.3%	2.7%	0.9%	0.0%	12.0%	
				17:00	18:00	1,500	6.9%	37.2%	18.5%	1.1%	11.2%	3.3%	1.7%	1.6%	4.4%	0.6%	0.6%	1.2%	1.2%	0.5%	0.0%	10.0%	
				18:00	19:00	1,250	7.3%	41.0%	20.8%	0.7%	9.3%	2.3%	1.1%	1.2%	2.1%	0.6%	0.9%	1.6%	1.9%	0.7%	0.0%	8.5%	
				19:00	20:00	1,200	4.6%	39.3%	32.8%	0.2%	2.6%	0.7%	0.3%	0.3%	7.7%	0.3%	0.3%	0.6%	0.6%	0.3%	0.0%	9.4%	
				20:00	21:00	1,000	4.5%	30.8%	38.2%	0.2%	2.1%	0.3%	0.3%	0.3%	11.9%	0.2%	0.1%	0.3%	0.2%	0.1%	0.0%	10.5%	
				21:00	22:00	900	3.3%	26.7%	42.3%	0.2%	2.9%	0.6%	0.3%	0.3%	11.0%	0.0%	0.0%	0.1%	0.0%	0.1%	0.0%	12.2%	
				22:00	23:00	900	4.5%	20.7%	43.0%	0.2%	1.6%	0.0%	0.0%	0.0%	11.7%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	17.0%	
				L142	Ko Shan Road (SB)	Kiang Hsi Street	Shan Si Street	23:00	0:00	750	4.7%	20.0%	42.5%	0.3%	2.0%	0.0%	0.0%	0.0%	11.8%	0.3%	0.3%	0.6%	0.3%
0:00	1:00	150	1.9%					29.8%	60.6%	0.0%	1.9%	0.0%	0.0%	0.0%	4.8%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	100	1.7%					30.5%	66.1%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	100	1.8%					27.3%	69.1%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	2.5%					30.0%	65.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	2.8%					27.8%	66.6%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	2.4%					26.8%	66.0%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	100	1.3%					23.8%	60.9%	0.0%	3.8%	0.0%	0.0%	0.0%	6.3%	0.0%	0.0%	1.3%	1.3%	1.3%	0.0%	0.0%	
7:00	8:00	200	3.0%					39.2%	29.9%	0.6%	5.5%	1.2%	0.0%	0.0%	10.4%	1.2%	1.2%	3.0%	2.4%	2.4%	0.0%	0.0%	
8:00	9:00	300	5.0%					50.7%	19.8%	0.4%	8.0%	4.2%	0.0%	0.0%	5.0%	0.8%	0.8%	2.3%	1.5%	1.5%	0.0%	0.0%	
9:00	10:00	250	3.9%					43.9%	22.8%	0.4%	8.8%	5.3%	0.0%	0.0%	4.8%	1.3%	0.9%	3.5%	2.2%	2.2%	0.0%	0.0%	
10:00	11:00	250	2.2%					36.4%	37.7%	0.4%	11.7%	3.9%	0.4%	0.4%	3.9%	0.4%	0.4%	0.9%	0.9%	0.4%	0.0%	0.0%	
11:00	12:00	250	1.4%					36.1%	33.0%	0.9%	14.9%	1.4%	0.0%	0.0%	7.7%	0.9%	0.5%	1.4%	0.9%	0.9%	0.0%	0.0%	
12:00	13:00	250	1.7%					43.3%	37.3%	0.4%	9.1%	1.2%	0.0%	0.0%	5.0%	0.8%	0.0%	0.4%	0.4%	0.4%	0.0%	0.0%	
13:00	14:00	250	1.5%					34.8%	29.6%	0.5%	13.3%	4.9%	0.5%	0.5%	5.9%	0.5%	1.0%	3.0%	2.0%	2.0%	0.0%	0.0%	
14:00	15:00	250	0.9%					31.3%	38.8%	0.5%	11.8%	3.8%	0.5%	0.5%	6.2%	0.5%	0.5%	1.9%	1.4%	1.4%	0.0%	0.0%	
15:00	16:00	300	1.6%					34.8%	38.6%	0.4%	11.1%	4.0%	0.4%	0.4%	4.7%	0.8%	0.4%	1.2%	0.8%	0.8%	0.0%	0.0%	
16:00	17:00	250	0.0%					41.6%	44.5%	0.4%	7.8%	0.8%	0.0%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
17:00	18:00	350	0.0%					47.3%	42.3%	0.3%	4.6%	0.9%	0.3%	0.3%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
18:00	19:00	300	0.0%					48.9%	44.4%	0.3%	3.4%	0.7%	0.3%	0.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
19:00	20:00	250	1.6%					50.4%	39.2%	0.0%	1.6%	0.4%	0.0%	0.0%	5.2%	0.4%	0.0%	0.4%	0.4%	0.4%	0.0%	0.0%	
20:00	21:00	250	1.9%					40.8%	47.0%	0.0%	1.5%	0.0%	0.0%	0.0%	8.3%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	200	1.1%					35.9%	53.3%	0.0%	1.6%	0.5%	0.0%	0.0%	7.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	200	1.8%					29.6%	57.3%	0.0%	1.2%	0.0%	0.0%	0.0%	8.9%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%	
23:00	0:00	150	2.2%	28.7%	57.4%	0.0%	1.5%	0.0%	0.0%	0.0%	8.8%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L143	Ko Shan Road (NB)	Shan Si Street	Klang Hsi Street	0:00	1:00	200	2.6%	23.2%	64.0%	0.0%	1.9%	0.0%	0.0%	0.0%	6.5%	0.0%	0.0%	0.6%	0.6%	0.6%	0.0%	0.0%
				1:00	2:00	100	3.3%	23.1%	67.0%	0.0%	2.2%	0.0%	0.0%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	2.3%	20.9%	69.8%	0.0%	2.3%	0.0%	0.0%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.4%	24.1%	69.1%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.8%	22.6%	71.7%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.3%	19.7%	68.9%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	1.6%	1.6%	0.0%	0.0%
				6:00	7:00	150	2.4%	17.7%	62.2%	0.0%	4.0%	0.8%	0.0%	0.0%	7.3%	0.8%	0.8%	1.6%	1.6%	0.8%	0.0%	0.0%
				7:00	8:00	350	1.0%	19.5%	56.1%	0.3%	4.0%	1.0%	0.3%	0.3%	9.6%	1.3%	1.0%	2.3%	2.3%	1.0%	0.0%	0.0%
				8:00	9:00	450	2.2%	29.6%	43.3%	0.2%	7.0%	3.6%	1.0%	1.0%	5.6%	1.0%	1.0%	1.9%	1.9%	0.7%	0.0%	0.0%
				9:00	10:00	400	1.6%	23.9%	46.4%	0.3%	7.0%	4.2%	1.0%	1.0%	5.2%	1.6%	1.3%	2.6%	2.9%	1.0%	0.0%	0.0%
				10:00	11:00	400	3.1%	27.1%	38.1%	0.6%	13.4%	7.5%	1.1%	1.1%	4.7%	0.3%	0.3%	1.1%	0.8%	0.8%	0.0%	0.0%
				11:00	12:00	350	2.1%	27.7%	33.8%	0.9%	17.6%	3.0%	0.3%	0.3%	8.9%	0.9%	0.6%	1.5%	1.5%	0.9%	0.0%	0.0%
				12:00	13:00	400	2.3%	34.1%	39.5%	0.6%	11.1%	2.6%	0.3%	0.3%	6.3%	1.1%	0.3%	0.6%	0.6%	0.3%	0.0%	0.0%
				13:00	14:00	350	1.9%	25.0%	29.0%	0.6%	14.8%	9.9%	1.2%	1.5%	6.5%	0.3%	1.2%	3.1%	2.8%	2.2%	0.0%	0.0%
				14:00	15:00	350	1.5%	23.0%	38.9%	0.6%	13.6%	7.3%	0.9%	0.9%	7.0%	0.6%	0.6%	2.1%	1.8%	1.2%	0.0%	0.0%
				15:00	16:00	400	2.3%	25.8%	39.0%	0.5%	12.5%	7.7%	1.0%	1.0%	5.1%	0.8%	0.5%	1.5%	1.3%	1.0%	0.0%	0.0%
				16:00	17:00	350	3.9%	40.8%	27.9%	0.6%	13.1%	4.2%	0.3%	0.3%	6.5%	0.0%	0.0%	0.9%	0.0%	1.5%	0.0%	0.0%
				17:00	18:00	450	4.5%	47.0%	26.7%	0.4%	7.8%	6.3%	0.4%	0.4%	5.2%	0.0%	0.0%	0.4%	0.0%	0.9%	0.0%	0.0%
				18:00	19:00	400	4.7%	50.7%	29.3%	0.3%	6.3%	4.2%	0.3%	0.3%	2.4%	0.0%	0.0%	0.5%	0.0%	1.0%	0.0%	0.0%
				19:00	20:00	350	2.6%	41.5%	43.8%	0.0%	2.0%	1.1%	0.0%	0.3%	6.6%	0.3%	0.3%	0.6%	0.6%	0.3%	0.0%	0.0%
				20:00	21:00	300	2.7%	32.8%	51.5%	0.0%	1.7%	0.3%	0.0%	0.0%	10.1%	0.0%	0.0%	0.3%	0.3%	0.3%	0.0%	0.0%
				21:00	22:00	300	1.9%	28.3%	56.8%	0.0%	2.2%	1.1%	0.0%	0.0%	9.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	2.8%	23.0%	60.9%	0.0%	1.2%	0.0%	0.0%	0.0%	10.5%	0.4%	0.0%	0.4%	0.4%	0.4%	0.0%	0.0%
				23:00	0:00	250	3.0%	22.4%	60.7%	0.0%	1.5%	0.0%	0.0%	0.0%	10.4%	0.5%	0.0%	0.5%	0.5%	0.5%	0.0%	0.0%
L144	Shan Si Street (EB)	Ko Shan Road	Chatham Road North	0:00	1:00	100	4.1%	24.3%	56.7%	0.0%	1.4%	0.0%	0.0%	0.0%	13.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	5.3%	26.3%	68.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	2.8%	25.0%	72.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.0%	28.0%	68.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	4.3%	26.1%	69.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	3.8%	23.1%	69.3%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	3.5%	19.3%	57.8%	0.0%	1.8%	0.0%	0.0%	0.0%	15.8%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%
				7:00	8:00	200	1.3%	23.8%	55.4%	0.0%	2.5%	0.0%	0.0%	0.0%	14.4%	0.0%	0.0%	1.3%	0.0%	1.3%	0.0%	0.0%
				8:00	9:00	250	2.3%	36.7%	43.9%	0.0%	4.2%	0.9%	0.9%	0.9%	8.4%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.0%
				9:00	10:00	200	2.1%	30.8%	48.8%	0.0%	4.1%	1.0%	1.0%	1.0%	8.2%	0.0%	0.0%	1.5%	0.0%	1.5%	0.0%	0.0%
				10:00	11:00	150	4.7%	32.4%	39.0%	0.0%	6.8%	1.4%	1.4%	1.4%	11.5%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				11:00	12:00	150	3.4%	31.1%	32.9%	0.0%	8.8%	0.7%	0.7%	0.7%	20.3%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%
				12:00	13:00	200	3.8%	37.1%	37.9%	0.0%	5.0%	0.6%	0.6%	0.6%	13.8%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				13:00	14:00	150	3.1%	31.1%	31.3%	0.0%	8.6%	1.6%	1.6%	1.6%	17.2%	0.0%	0.0%	1.6%	0.0%	2.3%	0.0%	0.0%
				14:00	15:00	150	2.9%	26.8%	40.1%	0.0%	7.2%	1.4%	1.4%	1.4%	16.7%	0.0%	0.0%	0.7%	0.0%	1.4%	0.0%	0.0%
				15:00	16:00	200	3.7%	30.9%	40.2%	0.0%	6.8%	1.2%	1.2%	1.2%	13.0%	0.0%	0.0%	0.6%	0.0%	1.2%	0.0%	0.0%
				16:00	17:00	150	7.8%	43.7%	20.3%	0.0%	4.7%	0.8%	0.0%	0.0%	21.9%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%
				17:00	18:00	200	8.8%	49.4%	19.4%	0.0%	2.9%	0.6%	0.6%	0.6%	17.1%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				18:00	19:00	150	9.2%	55.4%	22.0%	0.0%	2.1%	0.7%	0.7%	0.7%	8.5%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				19:00	20:00	200	4.1%	42.0%	38.5%	0.0%	1.2%	0.0%	0.0%	0.0%	13.6%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				20:00	21:00	150	3.3%	32.0%	43.3%	0.0%	0.7%	0.0%	0.0%	0.0%	20.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	3.0%	28.4%	48.5%	0.0%	0.7%	0.0%	0.0%	0.0%	19.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	4.0%	22.2%	51.6%	0.0%	0.8%	0.0%	0.0%	0.0%	21.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	150	4.0%	21.8%	51.4%	0.0%	1.0%	0.0%	0.0%	0.0%	21.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L145	Shek Tong Street (EB)	Chatham Road North	Ma Tau Wai Road	0:00	1:00	50	3.4%	34.5%	58.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	5.9%	35.3%	58.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	6.3%	31.3%	62.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	8.3%	33.3%	58.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	30.0%	70.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	30.0%	70.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	4.5%	27.3%	59.2%	0.0%	4.5%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	100	0.0%	20.0%	73.9%	0.0%	3.1%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	0.0%	28.3%	55.5%	0.0%	5.4%	5.4%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	0.0%	23.3%	59.3%	0.0%	5.8%	5.8%	5.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	3.9%	34.2%	30.3%	0.0%	13.2%	14.5%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	3.1%	38.9%	31.3%	0.0%	18.8%	6.3%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	4.2%	44.4%	33.3%	0.0%	11.1%	5.6%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	3.0%	32.8%	23.9%	0.0%	14.9%	19.4%	6.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	3.0%	30.3%	33.4%	0.0%	13.6%	15.2%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	3.7%	32.9%	31.7%	0.0%	12.2%	14.6%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	9.5%	65.1%	0.0%	0.0%	15.9%	9.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	9.9%	68.1%	0.0%	0.0%	8.8%	13.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	10.3%	73.0%	0.0%	0.0%	7.7%	9.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	4.2%	55.0%	36.6%	0.0%	1.4%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	3.6%	46.4%	46.4%	0.0%	1.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	4.0%	40.0%	52.0%	0.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	4.5%	34.1%	59.1%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	5.6%	33.3%	58.3%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L146	Chatham Road North (SB)	Shek Tong Street	San Lau Street	0:00	1:00	300	6.6%	27.9%	47.7%	0.0%	2.4%	0.0%	0.0%	0.0%	3.8%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	9.7%
				1:00	2:00	200	6.6%	31.6%	55.9%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%
				2:00	3:00	150	5.7%	29.3%	59.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%
				3:00	4:00	150	7.5%	31.1%	52.9%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%
				4:00	5:00	100	6.5%	28.3%	57.6%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%
				5:00	6:00	150	5.7%	27.4%	55.7%	0.0%	5.7%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	1.9%	0.9%	0.9%	0.0%	0.0%
				6:00	7:00	250	4.3%	21.7%	45.0%	0.4%	4.7%	1.3%	0.0%	0.0%	4.3%	0.4%	0.9%	1.7%	1.3%	0.4%	0.0%	13.6%
				7:00	8:00	750	3.2%	23.4%	32.4%	0.6%	5.7%	1.7%	0.3%	0.3%	3.5%	0.7%	0.7%	1.7%	1.4%	0.6%	0.0%	23.8%
				8:00	9:00	1,100	6.2%	32.7%	23.3%	0.8%	9.4%	5.5%	1.1%	1.0%	1.9%	0.6%	0.6%	1.2%	1.0%	0.4%	0.0%	14.3%
				9:00	10:00	1,000	4.8%	27.2%	25.5%	0.7%	9.5%	6.3%	1.2%	1.1%	1.7%	0.9%	0.7%	1.6%	1.5%	0.5%	0.0%	16.8%
				10:00	11:00	850	5.2%	28.1%	23.5%	1.1%	14.2%	6.7%	1.6%	1.5%	2.5%	0.2%	0.4%	0.9%	0.7%	0.2%	0.0%	13.2%
				11:00	12:00	800	3.5%	27.7%	20.8%	1.6%	18.5%	2.6%	0.7%	0.5%	4.7%	0.9%	0.5%	1.2%	1.2%	0.4%	0.0%	15.2%
				12:00	13:00	800	4.3%	35.9%	25.5%	1.0%	12.0%	2.4%	0.7%	0.5%	3.5%	1.0%	0.3%	0.5%	0.5%	0.1%	0.0%	11.8%
				13:00	14:00	800	3.3%	24.3%	17.1%	1.3%	14.8%	8.3%	2.1%	2.0%	3.4%	0.3%	1.0%	2.2%	2.2%	0.8%	0.0%	16.9%
				14:00	15:00	750	2.8%	23.4%	24.4%	1.2%	14.4%	6.6%	1.6%	1.5%	3.8%	0.4%	0.7%	1.6%	1.5%	0.5%	0.0%	15.6%
				15:00	16:00	900	4.1%	26.8%	24.3%	1.1%	13.4%	7.0%	1.8%	1.6%	2.9%	0.7%	0.5%	1.1%	1.0%	0.3%	0.0%	13.4%
				16:00	17:00	700	4.7%	40.3%	19.5%	1.0%	9.9%	2.2%	0.6%	0.6%	5.0%	0.1%	0.4%	1.2%	1.3%	0.4%	0.0%	12.8%
				17:00	18:00	900	5.6%	47.0%	19.1%	0.7%	6.1%	3.3%	1.1%	1.0%	4.1%	0.2%	0.2%	0.6%	0.6%	0.2%	0.0%	10.2%
				18:00	19:00	750	5.9%	50.8%	21.2%	0.4%	5.0%	2.3%	0.8%	0.8%	1.9%	0.1%	0.3%	0.8%	0.8%	0.3%	0.0%	8.6%
				19:00	20:00	750	5.1%	44.5%	28.4%	0.1%	2.3%	1.1%	0.1%	0.3%	3.7%	0.3%	0.3%	0.5%	0.5%	0.1%	0.0%	12.7%
				20:00	21:00	650	5.0%	35.9%	34.3%	0.2%	2.0%	0.5%	0.2%	0.2%	6.0%	0.2%	0.2%	0.3%	0.2%	0.2%	0.0%	14.6%
				21:00	22:00	600	3.8%	31.3%	37.9%	0.2%	2.7%	0.9%	0.2%	0.2%	5.5%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	17.1%
				22:00	23:00	550	4.9%	24.2%	38.3%	0.2%	1.5%	0.0%	0.0%	0.0%	5.9%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	23.8%
				23:00	0:00	450	5.2%	23.3%	38.5%	0.2%	1.8%	0.0%	0.0%	0.0%	5.9%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	23.8%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L147	Chatham Road North (NB)	San Lau Street	Shan Si Street	0:00	1:00	400	5.8%	23.6%	50.0%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	7.8%	0.3%	0.3%	0.8%	0.5%	0.3%	0.0%	7.8%	
				1:00	2:00	250	6.3%	26.8%	60.5%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	
				2:00	3:00	200	5.3%	24.9%	64.5%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	
				3:00	4:00	150	7.1%	26.2%	58.2%	0.0%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	
				4:00	5:00	150	6.4%	24.0%	62.4%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	
				5:00	6:00	150	5.6%	22.9%	59.6%	0.0%	6.3%	0.0%	0.0%	0.7%	0.0%	0.7%	0.7%	1.4%	1.4%	0.7%	0.0%	0.0%		
				6:00	7:00	350	3.9%	17.4%	46.6%	0.6%	5.4%	0.9%	0.0%	0.3%	8.7%	0.9%	0.9%	1.8%	1.5%	0.6%	0.0%	10.5%		
				7:00	8:00	950	2.3%	21.4%	37.2%	0.8%	4.3%	1.0%	0.3%	0.3%	13.5%	0.6%	0.6%	1.1%	1.1%	0.2%	0.0%	15.3%		
				8:00	9:00	1,300	4.6%	32.0%	28.5%	1.1%	7.5%	3.4%	1.2%	1.2%	7.7%	0.5%	0.5%	0.9%	0.9%	0.2%	0.0%	9.8%		
				9:00	10:00	1,200	3.6%	26.5%	31.3%	1.0%	7.7%	3.8%	1.4%	1.4%	7.3%	0.9%	0.8%	1.2%	1.3%	0.3%	0.0%	11.5%		
				10:00	11:00	1,100	4.7%	23.2%	24.9%	1.7%	17.1%	5.2%	2.2%	2.1%	5.1%	0.5%	0.5%	1.0%	1.0%	0.3%	0.0%	10.5%		
				11:00	12:00	1,100	3.1%	22.3%	21.3%	2.3%	21.3%	2.0%	0.9%	0.7%	9.2%	1.3%	0.7%	1.4%	1.5%	0.4%	0.0%	11.6%		
				12:00	13:00	1,050	4.0%	29.9%	27.0%	1.6%	14.4%	1.8%	1.0%	0.8%	7.0%	1.4%	0.3%	0.6%	0.7%	0.2%	0.0%	9.3%		
				13:00	14:00	1,100	2.9%	20.1%	17.9%	2.0%	17.6%	6.3%	2.7%	2.7%	6.6%	0.4%	1.5%	2.6%	2.9%	0.7%	0.0%	13.1%		
				14:00	15:00	1,050	2.5%	19.2%	25.2%	1.8%	16.9%	5.0%	2.1%	2.0%	7.3%	0.7%	1.0%	1.8%	1.9%	0.5%	0.0%	12.1%		
				15:00	16:00	1,250	3.7%	22.0%	25.8%	1.7%	16.0%	5.5%	2.4%	2.2%	5.6%	1.0%	0.7%	1.3%	1.3%	0.3%	0.0%	10.5%		
				16:00	17:00	1,100	5.2%	28.5%	17.5%	1.6%	18.2%	2.3%	1.0%	0.9%	4.8%	0.2%	1.3%	2.6%	3.0%	0.7%	0.0%	12.2%		
				17:00	18:00	1,300	6.5%	35.4%	18.3%	1.2%	11.9%	3.8%	1.9%	1.8%	4.2%	0.7%	0.7%	1.4%	1.4%	0.5%	0.0%	10.3%		
				18:00	19:00	1,100	7.0%	39.3%	20.7%	0.8%	10.0%	2.7%	1.3%	1.3%	2.0%	0.6%	1.0%	1.8%	2.1%	0.6%	0.0%	8.8%		
				19:00	20:00	1,000	4.8%	38.9%	31.5%	0.2%	2.8%	0.8%	0.2%	0.3%	7.8%	0.4%	0.3%	0.7%	0.7%	0.2%	0.0%	10.4%		
				20:00	21:00	850	4.6%	30.4%	37.0%	0.2%	2.3%	0.4%	0.2%	0.4%	12.0%	0.2%	0.1%	0.2%	0.2%	0.1%	0.0%	11.7%		
				21:00	22:00	800	3.4%	26.3%	40.6%	0.3%	3.1%	0.7%	0.3%	0.4%	11.2%	0.0%	0.0%	0.1%	0.0%	0.0%	0.0%	13.6%		
				22:00	23:00	750	4.6%	20.3%	41.1%	0.3%	1.7%	0.0%	0.0%	0.0%	11.8%	0.3%	0.3%	0.4%	0.3%	0.1%	0.0%	18.8%		
L148	Ko Shan Road (SB)	Shan Si Street	San Lau Street	23:00	0:00	650	4.8%	19.5%	41.1%	0.3%	2.1%	0.0%	0.0%	0.0%	11.9%	0.3%	0.3%	0.5%	0.3%	0.2%	0.0%	18.7%		
				0:00	1:00	150	2.0%	30.4%	59.7%	0.0%	2.0%	0.0%	0.0%	0.0%	4.9%	0.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%		
				1:00	2:00	100	1.7%	31.0%	65.6%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	100	1.8%	29.1%	67.3%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	50	2.6%	30.8%	64.0%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	50	2.8%	27.8%	66.6%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	50	2.5%	27.5%	65.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%		
				6:00	7:00	100	1.3%	24.7%	61.0%	0.0%	2.6%	0.0%	0.0%	0.0%	6.5%	0.0%	0.0%	1.3%	1.3%	1.3%	0.0%	0.0%		
				7:00	8:00	200	3.1%	39.0%	30.2%	0.6%	4.9%	1.2%	0.0%	0.0%	10.5%	1.2%	1.2%	3.1%	2.5%	2.5%	0.0%	0.0%		
				8:00	9:00	300	5.0%	50.6%	20.1%	0.4%	7.3%	3.9%	0.4%	0.4%	5.0%	0.8%	0.8%	2.3%	1.5%	1.5%	0.0%	0.0%		
				9:00	10:00	250	4.0%	43.9%	23.0%	0.4%	8.0%	4.9%	0.4%	0.4%	4.9%	1.3%	0.9%	3.5%	2.2%	2.2%	0.0%	0.0%		
				10:00	11:00	250	2.7%	37.8%	36.7%	0.4%	11.1%	3.5%	0.4%	0.4%	4.0%	0.4%	0.4%	0.9%	0.9%	0.4%	0.0%	0.0%		
				11:00	12:00	250	1.8%	36.9%	32.4%	0.9%	14.2%	1.4%	0.0%	0.0%	7.8%	0.9%	0.5%	1.4%	0.9%	0.9%	0.0%	0.0%		
				12:00	13:00	250	2.1%	44.4%	36.3%	0.4%	8.4%	1.3%	0.0%	0.0%	5.1%	0.8%	0.0%	0.4%	0.4%	0.4%	0.0%	0.0%		
				13:00	14:00	250	1.5%	35.2%	28.9%	0.5%	12.9%	5.0%	0.5%	1.0%	6.0%	0.5%	1.0%	3.0%	2.0%	2.0%	0.0%	0.0%		
				14:00	15:00	250	1.4%	31.9%	38.2%	0.5%	11.6%	3.4%	0.5%	0.5%	6.3%	0.5%	0.5%	1.9%	1.4%	1.4%	0.0%	0.0%		
				15:00	16:00	250	2.0%	35.9%	38.0%	0.4%	10.5%	3.6%	0.4%	0.4%	4.8%	0.8%	0.4%	1.2%	0.8%	0.8%	0.0%	0.0%		
				16:00	17:00	250	0.8%	43.1%	42.2%	0.4%	7.8%	0.8%	0.0%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				17:00	18:00	350	0.9%	48.6%	40.1%	0.3%	4.6%	0.9%	0.3%	0.3%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				18:00	19:00	300	1.0%	50.1%	42.2%	0.3%	3.4%	0.7%	0.3%	0.3%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				19:00	20:00	250	2.0%	51.3%	37.9%	0.0%	1.6%	0.4%	0.0%	0.0%	5.2%	0.4%	0.0%	0.4%	0.4%	0.4%	0.0%	0.0%		
				20:00	21:00	250	2.0%	41.7%	46.0%	0.0%	1.5%	0.0%	0.0%	0.0%	8.3%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%		
				21:00	22:00	200	1.6%	36.8%	51.8%	0.0%	1.6%	0.5%	0.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
22:00	23:00	200	2.4%	30.1%	56.1%	0.0%	1.2%	0.0%	0.0%	0.0%	9.0%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%						
23:00	0:00	150	2.2%	29.9%	56.0%	0.0%	1.5%	0.0%	0.0%	0.0%	9.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%						

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L149	Ko Shan Road (NB)	San Lau Street	Shan Si Street	0:00	1:00	250	3.0%	22.4%	63.5%	0.0%	1.7%	0.0%	0.0%	0.0%	8.2%	0.4%	0.0%	0.4%	0.4%	0.0%	0.0%	0.0%	
				1:00	2:00	150	3.0%	23.5%	68.2%	0.0%	2.3%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.4%	21.0%	71.8%	0.0%	1.6%	0.0%	0.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.4%	24.1%	69.1%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.8%	21.8%	71.8%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.4%	20.5%	70.5%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	200	2.2%	18.2%	62.3%	0.0%	3.3%	0.6%	0.0%	0.0%	9.4%	0.6%	0.6%	1.1%	1.1%	0.6%	0.0%	0.0%	0.0%
				7:00	8:00	500	1.1%	20.6%	56.4%	0.2%	3.6%	0.6%	0.2%	0.2%	11.1%	0.9%	0.6%	1.9%	1.7%	0.9%	0.0%	0.0%	0.0%
				8:00	9:00	650	2.2%	31.4%	44.1%	0.3%	6.4%	2.4%	1.1%	0.9%	6.4%	0.6%	0.6%	1.6%	1.4%	0.6%	0.0%	0.0%	0.0%
				9:00	10:00	600	1.7%	25.6%	48.0%	0.3%	6.6%	2.7%	1.2%	1.0%	6.1%	1.0%	0.8%	2.2%	2.0%	0.8%	0.0%	0.0%	0.0%
				10:00	11:00	550	3.3%	28.3%	39.4%	0.4%	12.6%	4.3%	1.2%	1.2%	6.7%	0.4%	0.4%	0.8%	0.8%	0.2%	0.0%	0.0%	0.0%
				11:00	12:00	500	2.2%	27.9%	34.2%	0.6%	16.0%	1.6%	0.6%	0.4%	12.1%	1.0%	0.6%	1.2%	1.2%	0.4%	0.0%	0.0%	0.0%
				12:00	13:00	550	2.7%	34.1%	39.6%	0.4%	9.8%	1.3%	0.6%	0.4%	8.4%	1.1%	0.2%	0.6%	0.6%	0.2%	0.0%	0.0%	0.0%
				13:00	14:00	500	2.2%	26.5%	30.4%	0.4%	14.0%	5.7%	1.8%	1.8%	9.4%	0.4%	1.3%	2.6%	2.6%	0.9%	0.0%	0.0%	0.0%
				14:00	15:00	500	1.9%	23.6%	40.0%	0.4%	12.4%	4.2%	1.3%	1.1%	9.7%	0.6%	0.8%	1.7%	1.7%	0.6%	0.0%	0.0%	0.0%
				15:00	16:00	600	2.7%	26.7%	40.1%	0.4%	11.7%	4.4%	1.4%	1.2%	7.3%	0.9%	0.5%	1.2%	1.1%	0.4%	0.0%	0.0%	0.0%
				16:00	17:00	500	4.7%	41.2%	26.5%	0.2%	11.7%	2.3%	0.4%	0.4%	10.4%	0.2%	0.6%	0.6%	0.8%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	650	5.5%	47.3%	25.7%	0.2%	7.1%	3.4%	0.6%	0.6%	8.4%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	550	5.7%	51.0%	28.5%	0.2%	5.9%	2.3%	0.4%	0.4%	4.0%	0.2%	0.4%	0.4%	0.6%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	550	3.0%	40.8%	42.6%	0.0%	1.9%	0.6%	0.2%	0.2%	8.7%	0.4%	0.2%	0.6%	0.6%	0.2%	0.0%	0.0%	0.0%
				20:00	21:00	500	2.9%	31.6%	49.4%	0.0%	1.5%	0.2%	0.2%	0.2%	13.4%	0.2%	0.0%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	450	2.2%	27.4%	55.0%	0.0%	1.9%	0.5%	0.2%	0.2%	12.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	400	2.9%	22.3%	58.4%	0.0%	1.0%	0.0%	0.0%	0.0%	13.9%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%
L150	San Lau Street (WB)	Chatham Road North	Ko Shan Road	23:00	0:00	350	3.2%	21.6%	58.2%	0.0%	1.6%	0.0%	0.0%	0.0%	13.9%	0.3%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	
				0:00	1:00	100	4.3%	22.3%	59.6%	0.0%	2.1%	0.0%	0.0%	0.0%	10.6%	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	100	5.5%	23.6%	61.8%	0.0%	1.8%	0.0%	0.0%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.8%	21.2%	65.4%	0.0%	1.9%	0.0%	0.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	5.9%	23.5%	67.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	6.5%	22.6%	67.7%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	5.7%	20.0%	68.6%	0.0%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	100	4.1%	17.6%	57.8%	0.0%	4.1%	1.4%	0.0%	0.0%	12.2%	0.0%	0.0%	1.4%	1.4%	0.0%	0.0%	0.0%	
				7:00	8:00	200	2.1%	21.9%	48.5%	0.5%	4.7%	1.0%	0.5%	0.5%	14.1%	1.0%	1.0%	2.1%	2.1%	0.0%	0.0%	0.0%	
				8:00	9:00	300	4.1%	32.1%	36.7%	0.4%	7.8%	3.7%	1.5%	1.5%	7.8%	0.7%	0.7%	1.5%	1.5%	0.0%	0.0%	0.0%	
				9:00	10:00	250	3.3%	26.5%	40.6%	0.4%	8.2%	4.5%	1.6%	1.6%	7.3%	1.2%	0.8%	2.0%	2.0%	0.0%	0.0%	0.0%	
				10:00	11:00	250	4.6%	26.5%	34.4%	0.5%	14.2%	5.5%	1.4%	1.4%	8.2%	0.5%	0.5%	0.9%	0.9%	0.5%	0.0%	0.0%	
				11:00	12:00	250	3.3%	25.6%	29.7%	0.9%	18.1%	2.3%	0.5%	0.5%	14.9%	0.9%	0.5%	1.4%	0.9%	0.5%	0.0%	0.0%	
				12:00	13:00	250	3.7%	32.9%	35.9%	0.5%	11.6%	1.9%	0.5%	0.5%	10.6%	0.9%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%	
				13:00	14:00	250	3.0%	24.4%	26.3%	0.5%	15.9%	7.5%	2.0%	2.0%	11.4%	0.5%	1.0%	2.5%	2.0%	1.0%	0.0%	0.0%	
				14:00	15:00	250	2.5%	22.3%	35.5%	0.5%	14.4%	5.4%	1.5%	1.5%	11.9%	0.5%	0.5%	1.5%	1.5%	0.5%	0.0%	0.0%	
				15:00	16:00	250	3.8%	25.0%	35.4%	0.4%	13.3%	5.8%	1.7%	1.7%	9.2%	0.8%	0.4%	1.3%	0.8%	0.4%	0.0%	0.0%	
				16:00	17:00	250	5.4%	36.1%	26.7%	0.5%	12.9%	2.5%	0.0%	0.0%	13.4%	0.0%	0.0%	1.0%	0.0%	1.5%	0.0%	0.0%	
				17:00	18:00	300	6.5%	42.5%	26.1%	0.4%	8.0%	3.8%	0.4%	0.4%	10.7%	0.0%	0.0%	0.4%	0.0%	0.8%	0.0%	0.0%	
				18:00	19:00	250	6.8%	46.7%	29.1%	0.5%	6.8%	2.7%	0.5%	0.5%	5.0%	0.0%	0.0%	0.5%	0.0%	0.9%	0.0%	0.0%	
				19:00	20:00	250	4.2%	40.2%	39.7%	0.0%	2.3%	0.9%	0.0%	0.0%	11.2%	0.5%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%	
				20:00	21:00	200	4.3%	31.0%	45.5%	0.0%	1.6%	0.5%	0.0%	0.0%	17.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	200	3.0%	27.2%	50.8%	0.0%	2.4%	0.6%	0.0%	0.0%	16.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	200	4.5%	21.8%	53.9%	0.0%	1.3%	0.0%	0.0%	0.0%	17.9%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	150	4.7%	21.3%	53.5%	0.0%	1.6%	0.0%	0.0%	0.0%	18.1%	0.0%	0.0%	0.8%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID																							
Road Section		From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L151	San Lau Street (WB)	Gillies Avenue North	Chatham Road North	0:00	1:00	250	4.0%	28.2%	55.3%	0.0%	2.0%	0.0%	0.0%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	
				1:00	2:00	150	4.3%	30.0%	60.7%	0.0%	2.1%	0.0%	0.0%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	3.9%	27.1%	64.3%	0.0%	1.6%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	100	5.4%	30.4%	60.9%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	100	3.7%	28.4%	65.4%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	3.3%	27.8%	64.5%	0.0%	4.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	200	3.1%	22.9%	55.3%	0.5%	3.6%	0.5%	0.0%	0.0%	8.9%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	4.7%	
				7:00	8:00	550	1.6%	27.6%	43.0%	1.2%	4.0%	0.8%	0.2%	0.2%	14.7%	0.0%	0.0%	0.0%	0.0%	0.2%	0.0%	6.5%	
				8:00	9:00	750	3.1%	40.1%	31.9%	1.5%	6.9%	2.9%	0.7%	0.6%	8.1%	0.0%	0.0%	0.0%	0.0%	0.1%	0.0%	4.1%	
				9:00	10:00	650	2.5%	34.2%	36.5%	1.6%	7.3%	3.5%	0.8%	0.6%	7.9%	0.0%	0.0%	0.0%	0.0%	0.2%	0.0%	4.9%	
				10:00	11:00	600	3.9%	32.0%	31.5%	2.3%	12.6%	4.4%	1.2%	1.2%	5.5%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	5.0%	
				11:00	12:00	600	2.6%	31.8%	27.3%	3.1%	15.9%	1.7%	0.5%	0.3%	10.2%	0.2%	0.0%	0.3%	0.2%	0.2%	0.0%	5.7%	
				12:00	13:00	650	3.0%	39.0%	32.1%	2.1%	9.9%	1.5%	0.5%	0.3%	7.1%	0.2%	0.0%	0.2%	0.0%	0.0%	0.0%	4.1%	
				13:00	14:00	550	2.7%	30.4%	24.8%	2.8%	14.2%	5.9%	1.5%	1.5%	8.0%	0.0%	0.2%	0.6%	0.2%	0.4%	0.0%	6.8%	
				14:00	15:00	550	2.0%	27.6%	33.1%	2.6%	13.0%	4.3%	1.1%	1.1%	8.3%	0.0%	0.2%	0.4%	0.2%	0.2%	0.0%	5.9%	
				15:00	16:00	650	2.9%	30.8%	32.7%	2.3%	11.9%	4.6%	1.2%	1.2%	6.2%	0.2%	0.2%	0.3%	0.2%	0.2%	0.0%	5.1%	
				16:00	17:00	600	4.5%	41.7%	24.7%	1.9%	11.0%	1.9%	0.5%	0.5%	5.3%	0.0%	0.3%	0.9%	0.3%	0.2%	0.0%	6.3%	
				17:00	18:00	800	5.2%	48.2%	24.0%	1.3%	6.7%	2.7%	0.9%	0.9%	4.3%	0.1%	0.1%	0.4%	0.1%	0.1%	0.0%	5.0%	
				18:00	19:00	700	5.5%	51.4%	26.3%	0.9%	5.5%	1.8%	0.6%	0.6%	2.0%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	4.1%	
				19:00	20:00	650	3.3%	46.9%	34.7%	0.2%	1.8%	0.7%	0.2%	0.2%	7.4%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	4.4%	
				20:00	21:00	550	3.3%	37.1%	40.6%	0.2%	1.6%	0.2%	0.2%	0.2%	11.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.9%	
				21:00	22:00	500	2.4%	32.3%	45.5%	0.2%	2.1%	0.4%	0.2%	0.2%	10.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.8%	
				22:00	23:00	450	3.5%	26.2%	48.4%	0.2%	1.2%	0.0%	0.0%	0.0%	12.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	8.4%	
				23:00	0:00	350	3.7%	25.4%	48.3%	0.3%	1.4%	0.0%	0.0%	0.0%	12.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	8.6%	
L152	Gillies Avenue North (WB)	Ma Tau Wai Road	San Lau Street	0:00	1:00	250	3.9%	30.6%	51.6%	0.0%	2.2%	0.0%	0.0%	0.0%	7.0%	0.0%	0.0%	0.4%	0.4%	0.0%	3.9%		
				1:00	2:00	150	3.9%	32.8%	57.1%	0.0%	3.1%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				2:00	3:00	150	3.4%	29.9%	60.7%	0.0%	2.6%	0.0%	0.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				3:00	4:00	100	4.8%	33.3%	57.1%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				4:00	5:00	100	4.1%	31.5%	61.7%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%		
				5:00	6:00	100	3.6%	29.8%	59.4%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	0.0%	0.0%		
				6:00	7:00	200	2.7%	24.2%	50.2%	0.5%	4.4%	1.1%	0.0%	0.0%	7.7%	0.5%	0.5%	1.1%	1.1%	0.5%	5.5%		
				7:00	8:00	550	1.5%	30.3%	37.5%	1.2%	4.2%	1.0%	0.4%	0.2%	11.4%	0.8%	0.6%	1.3%	1.3%	0.6%	7.7%		
				8:00	9:00	800	3.0%	42.4%	26.8%	1.6%	6.9%	3.4%	1.0%	0.9%	6.0%	0.5%	0.5%	1.0%	1.0%	0.4%	4.6%		
				9:00	10:00	700	2.3%	35.6%	30.2%	1.6%	7.3%	4.0%	1.2%	1.0%	5.9%	0.9%	0.7%	1.5%	1.6%	0.6%	5.6%		
				10:00	11:00	600	3.5%	32.0%	27.0%	2.5%	14.6%	5.4%	1.5%	1.5%	4.7%	0.2%	0.3%	0.7%	0.5%	0.2%	5.4%		
				11:00	12:00	600	2.2%	31.8%	23.4%	3.3%	18.4%	2.1%	0.7%	0.5%	8.6%	0.7%	0.3%	0.9%	0.9%	0.2%	6.0%		
				12:00	13:00	600	2.9%	39.8%	27.9%	2.2%	11.7%	1.8%	0.7%	0.5%	6.2%	0.8%	0.2%	0.3%	0.3%	0.2%	4.5%		
				13:00	14:00	600	2.2%	29.1%	20.3%	2.9%	15.8%	6.9%	2.0%	2.0%	6.5%	0.2%	0.9%	1.8%	1.8%	0.5%	7.1%		
				14:00	15:00	550	1.8%	27.4%	28.0%	2.7%	14.8%	5.3%	1.5%	1.5%	6.9%	0.4%	0.5%	1.3%	1.1%	0.4%	6.4%		
				15:00	16:00	700	2.8%	30.4%	27.9%	2.5%	13.8%	5.7%	1.7%	1.5%	5.2%	0.6%	0.5%	0.9%	0.8%	0.3%	5.4%		
				16:00	17:00	550	4.1%	40.5%	21.7%	2.0%	14.6%	2.4%	0.6%	0.6%	5.4%	0.0%	0.4%	0.6%	0.4%	0.0%	6.7%		
				17:00	18:00	750	4.9%	47.5%	21.1%	1.4%	9.0%	3.7%	1.1%	1.0%	4.4%	0.1%	0.1%	0.3%	0.1%	0.0%	5.3%		
				18:00	19:00	600	5.0%	51.8%	23.5%	1.0%	7.4%	2.5%	0.8%	0.7%	2.0%	0.2%	0.2%	0.3%	0.2%	0.0%	4.4%		
				19:00	20:00	600	3.3%	49.2%	31.3%	0.2%	2.2%	0.9%	0.2%	0.2%	6.5%	0.2%	0.2%	0.3%	0.3%	0.2%	4.8%		
				20:00	21:00	500	3.3%	39.7%	37.4%	0.2%	1.9%	0.4%	0.2%	0.2%	10.5%	0.2%	0.0%	0.2%	0.2%	0.0%	5.6%		
				21:00	22:00	450	2.3%	35.0%	42.2%	0.2%	2.5%	0.7%	0.2%	0.2%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.7%		
				22:00	23:00	400	3.3%	28.5%	44.4%	0.3%	1.5%	0.0%	0.0%	0.0%	11.0%	0.3%	0.3%	0.3%	0.3%	0.0%	9.8%		
				23:00	0:00	350	3.4%	27.6%	44.9%	0.3%	1.9%	0.0%	0.0%	0.0%	11.1%	0.3%	0.3%	0.3%	0.3%	0.0%	9.6%		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L153	Gillies Avenue North (SB)	San Lau Street	San Wai Street	0:00	1:00	100	4.5%	31.5%	54.0%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	5.6%	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	1.1%
				1:00	2:00	50	4.0%	34.0%	60.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	4.3%	30.4%	63.1%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.9%	32.4%	58.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	3.3%	30.0%	63.4%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	3.0%	30.3%	60.7%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	2.8%	25.4%	52.2%	0.0%	4.2%	1.4%	0.0%	0.0%	5.6%	1.4%	1.4%	1.4%	1.4%	1.4%	0.0%	1.4%	
				7:00	8:00	200	1.1%	31.8%	37.6%	1.1%	4.0%	1.7%	0.6%	0.6%	8.0%	1.7%	1.7%	2.8%	3.4%	1.1%	0.0%	2.8%	
				8:00	9:00	300	1.8%	42.4%	25.5%	1.5%	6.6%	5.8%	1.8%	1.8%	4.0%	1.1%	1.1%	2.2%	2.2%	0.7%	0.0%	1.5%	
				9:00	10:00	300	1.6%	34.4%	28.2%	1.6%	6.7%	6.7%	2.0%	2.0%	4.0%	2.0%	1.6%	3.2%	3.2%	1.2%	0.0%	1.6%	
				10:00	11:00	250	3.9%	33.3%	28.6%	1.3%	12.6%	8.2%	2.2%	1.7%	3.9%	0.4%	0.4%	0.9%	0.9%	0.4%	0.0%	1.3%	
				11:00	12:00	250	2.8%	34.0%	25.7%	1.9%	16.8%	3.3%	0.9%	0.5%	7.5%	1.4%	0.5%	1.4%	1.4%	0.5%	0.0%	1.4%	
				12:00	13:00	250	3.5%	41.7%	30.1%	1.3%	10.2%	2.7%	0.9%	0.4%	4.9%	1.3%	0.4%	0.4%	0.9%	0.4%	0.0%	0.9%	
				13:00	14:00	250	2.8%	29.5%	21.3%	1.9%	13.4%	10.6%	2.8%	2.3%	5.1%	0.5%	1.4%	2.8%	2.8%	1.4%	0.0%	1.4%	
				14:00	15:00	250	2.4%	28.6%	29.4%	1.4%	12.9%	8.1%	1.9%	1.9%	5.7%	0.5%	1.0%	1.9%	1.9%	1.0%	0.0%	1.4%	
				15:00	16:00	300	3.1%	31.2%	29.0%	1.6%	11.8%	8.6%	2.4%	2.0%	4.3%	0.8%	0.8%	1.2%	1.2%	0.8%	0.0%	1.2%	
				16:00	17:00	250	6.4%	43.4%	26.4%	0.5%	11.8%	3.6%	0.5%	0.5%	5.5%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.9%	
				17:00	18:00	300	7.0%	48.8%	24.8%	0.3%	7.0%	5.0%	1.0%	0.7%	4.4%	0.0%	0.0%	0.0%	0.0%	0.3%	0.0%	0.7%	
				18:00	19:00	300	7.4%	51.9%	27.1%	0.4%	5.8%	3.5%	0.8%	0.4%	1.9%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.4%	
				19:00	20:00	250	4.0%	50.9%	32.9%	0.0%	1.8%	1.3%	0.0%	0.4%	5.3%	0.4%	0.4%	0.4%	0.9%	0.4%	0.0%	0.9%	
				20:00	21:00	200	3.9%	42.1%	41.1%	0.0%	1.7%	0.6%	0.0%	0.0%	8.9%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	1.1%	
				21:00	22:00	200	3.1%	37.5%	46.2%	0.0%	2.5%	1.3%	0.0%	0.0%	8.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	
				22:00	23:00	150	4.2%	31.3%	50.6%	0.0%	1.4%	0.0%	0.0%	0.0%	9.7%	0.0%	0.0%	0.7%	0.0%	0.0%	0.0%	2.1%	
				L154	Gillies Avenue North (NB)	San Wai Street	San Lau Street	23:00	0:00	150	5.1%	29.9%	50.4%	0.0%	1.7%	0.0%	0.0%	0.0%	9.4%	0.0%	0.0%	0.9%	0.0%
0:00	1:00	100	4.3%					22.8%	62.0%	0.0%	1.1%	0.0%	0.0%	0.0%	9.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	100	5.9%					23.5%	68.6%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	4.3%					21.3%	74.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	5.9%					23.5%	67.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	6.5%					22.6%	70.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	5.9%					20.6%	70.6%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	100	4.2%					18.3%	62.0%	0.0%	1.4%	0.0%	0.0%	0.0%	11.3%	0.0%	0.0%	1.4%	0.0%	1.4%	0.0%	0.0%	
7:00	8:00	150	0.8%					24.0%	46.4%	0.8%	4.0%	1.6%	0.0%	0.0%	21.6%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%	
8:00	9:00	200	1.7%					35.7%	35.3%	1.2%	6.4%	5.8%	0.6%	0.6%	12.1%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
9:00	10:00	200	1.3%					30.5%	39.8%	1.3%	6.5%	7.1%	0.6%	0.6%	11.7%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
10:00	11:00	200	5.9%					30.3%	41.5%	1.1%	6.4%	4.8%	0.5%	0.5%	8.0%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	
11:00	12:00	200	3.9%					30.0%	36.7%	1.7%	8.3%	2.2%	0.0%	0.0%	15.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	
12:00	13:00	200	4.1%					35.7%	41.4%	1.0%	5.1%	1.5%	0.0%	0.0%	10.2%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	
13:00	14:00	200	3.7%					29.6%	33.4%	1.2%	7.4%	6.8%	0.6%	1.2%	11.7%	0.0%	0.0%	1.9%	0.0%	2.5%	0.0%	0.0%	
14:00	15:00	200	2.9%					25.6%	42.9%	1.2%	6.4%	4.7%	0.6%	0.6%	12.2%	0.0%	0.0%	1.2%	0.0%	1.7%	0.0%	0.0%	
15:00	16:00	250	4.4%					28.6%	42.7%	1.0%	5.8%	5.3%	0.5%	0.5%	9.2%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%	
16:00	17:00	250	7.4%					37.9%	37.8%	0.4%	2.6%	1.3%	0.0%	0.0%	8.7%	0.0%	0.0%	2.2%	0.0%	1.7%	0.0%	0.0%	
17:00	18:00	350	8.4%					43.0%	35.7%	0.3%	1.6%	1.6%	0.3%	0.3%	6.8%	0.0%	0.0%	1.0%	0.0%	1.0%	0.0%	0.0%	
18:00	19:00	300	8.5%					44.8%	38.2%	0.4%	1.1%	1.1%	0.4%	0.4%	2.9%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	
19:00	20:00	250	4.3%					40.9%	42.2%	0.0%	1.0%	0.5%	0.0%	0.0%	10.1%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%	
20:00	21:00	200	4.4%					31.3%	48.4%	0.0%	0.5%	0.0%	0.0%	0.0%	15.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	200	3.0%					27.4%	53.8%	0.0%	1.2%	0.6%	0.0%	0.0%	14.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	200	4.5%					22.1%	56.6%	0.0%	0.6%	0.0%	0.0%	0.0%	15.6%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
23:00	0:00	150	4.9%	21.1%	57.0%	0.0%	0.8%	0.0%	0.0%	0.0%	15.4%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L155	Gillies Avenue North (SB)	San Wai Street	Pak Kung Street	0:00	1:00	100	5.0%	33.0%	54.0%	0.0%	2.0%	0.0%	0.0%	0.0%	4.0%	0.0%	0.0%	1.0%	1.0%	0.0%	0.0%	0.0%	
				1:00	2:00	100	5.3%	33.3%	57.9%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	100	3.8%	30.8%	63.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.1%	33.3%	56.5%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	5.7%	31.4%	60.0%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	4.9%	29.3%	56.1%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	2.4%	0.0%	0.0%	0.0%
				6:00	7:00	100	3.7%	25.6%	51.3%	0.0%	4.9%	1.2%	0.0%	0.0%	3.7%	1.2%	1.2%	2.4%	2.4%	1.2%	0.0%	1.2%	0.0%
				7:00	8:00	250	1.9%	33.8%	36.2%	1.4%	5.7%	1.9%	0.5%	0.5%	4.8%	1.9%	1.9%	3.8%	3.3%	1.0%	0.0%	1.4%	0.0%
				8:00	9:00	350	2.9%	43.0%	23.3%	1.5%	8.7%	5.5%	1.7%	1.7%	2.3%	1.5%	1.5%	2.6%	2.3%	0.6%	0.0%	0.9%	0.0%
				9:00	10:00	350	2.2%	35.6%	26.0%	1.3%	9.0%	6.4%	1.9%	1.9%	2.2%	2.6%	1.9%	3.5%	3.5%	1.0%	0.0%	1.0%	0.0%
				10:00	11:00	300	4.1%	33.7%	27.8%	1.9%	13.9%	7.1%	2.3%	2.3%	2.3%	0.4%	0.8%	1.1%	1.1%	0.4%	0.0%	0.8%	0.0%
				11:00	12:00	250	2.8%	34.9%	25.0%	2.4%	18.5%	2.8%	0.8%	0.8%	4.4%	1.6%	0.8%	2.0%	2.0%	0.4%	0.0%	0.8%	0.0%
				12:00	13:00	300	3.4%	42.4%	29.1%	1.5%	11.5%	2.3%	0.8%	0.8%	3.1%	1.9%	0.4%	0.8%	0.8%	0.4%	0.0%	0.8%	0.0%
				13:00	14:00	300	2.8%	29.6%	20.6%	2.0%	15.1%	9.1%	2.8%	2.8%	3.2%	0.4%	2.0%	3.6%	3.6%	1.2%	0.0%	1.2%	0.0%
				14:00	15:00	250	2.1%	29.1%	29.0%	2.1%	14.5%	7.1%	2.1%	2.1%	3.3%	0.8%	1.2%	2.5%	2.5%	0.8%	0.0%	0.8%	0.0%
				15:00	16:00	300	3.1%	31.8%	28.3%	1.7%	13.3%	7.5%	2.4%	2.0%	2.7%	1.4%	1.0%	1.7%	1.7%	0.7%	0.0%	0.7%	0.0%
				16:00	17:00	250	5.6%	45.5%	28.1%	0.9%	10.8%	2.6%	0.9%	0.4%	3.9%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.9%	0.0%
				17:00	18:00	350	6.4%	50.6%	26.1%	0.6%	6.4%	3.8%	1.3%	1.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.3%	0.0%	0.6%	0.0%
				18:00	19:00	300	6.6%	53.5%	28.1%	0.4%	5.1%	2.6%	0.7%	0.7%	1.5%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%
				19:00	20:00	300	3.9%	52.5%	32.5%	0.0%	2.4%	1.2%	0.4%	0.4%	3.1%	0.4%	0.4%	0.8%	0.8%	0.4%	0.0%	0.8%	0.0%
				20:00	21:00	250	3.9%	44.0%	40.7%	0.0%	2.0%	0.5%	0.5%	0.5%	5.4%	0.5%	0.0%	0.5%	0.5%	0.0%	0.0%	1.0%	0.0%
				21:00	22:00	200	2.8%	39.7%	46.3%	0.0%	2.8%	1.1%	0.6%	0.6%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%
				22:00	23:00	200	4.3%	32.7%	50.6%	0.0%	1.9%	0.0%	0.0%	0.0%	6.2%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	1.9%	0.0%
				L156	Gillies Avenue North (NB)	Pak Kung Street	San Wai Street	23:00	0:00	150	4.5%	31.8%	50.6%	0.0%	2.3%	0.0%	0.0%	0.0%	6.1%	0.8%	0.8%	0.8%	0.8%
0:00	1:00	100	3.3%					21.1%	64.5%	0.0%	1.1%	0.0%	0.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	50	4.0%					22.0%	72.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	4.3%					21.3%	74.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
3:00	4:00	50	5.9%					23.5%	70.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
4:00	5:00	50	3.4%					20.7%	75.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
5:00	6:00	50	2.9%					20.6%	73.6%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
6:00	7:00	100	2.9%					17.4%	65.3%	0.0%	1.4%	0.0%	0.0%	0.0%	11.6%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	
7:00	8:00	150	0.9%					23.9%	45.1%	0.9%	4.3%	0.9%	0.0%	0.0%	23.1%	0.0%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%	
8:00	9:00	200	1.9%					35.6%	35.0%	1.3%	6.9%	4.4%	0.6%	0.6%	13.1%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
9:00	10:00	150	1.4%					30.5%	39.7%	1.4%	7.1%	5.0%	0.7%	0.7%	12.8%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	
10:00	11:00	200	4.5%					29.5%	44.9%	1.1%	6.3%	3.4%	0.6%	0.6%	8.5%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%	
11:00	12:00	200	2.9%					29.4%	39.4%	1.8%	8.2%	1.2%	0.0%	0.0%	15.9%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	
12:00	13:00	200	3.7%					34.2%	43.9%	1.1%	4.8%	1.1%	0.0%	0.0%	10.7%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%	
13:00	14:00	150	3.3%					29.3%	36.7%	1.3%	7.3%	4.7%	0.7%	1.3%	12.7%	0.0%	0.0%	0.0%	0.0%	2.7%	0.0%	0.0%	
14:00	15:00	200	2.5%					24.7%	46.2%	1.2%	6.2%	3.1%	0.6%	0.6%	13.0%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	
15:00	16:00	200	3.6%					28.0%	46.3%	1.0%	6.2%	3.1%	0.5%	0.5%	9.8%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	
16:00	17:00	250	6.0%					36.6%	42.9%	0.5%	2.3%	0.5%	0.0%	0.0%	9.3%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%	
17:00	18:00	300	6.9%					41.4%	40.5%	0.3%	1.4%	0.7%	0.3%	0.3%	7.2%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%	
18:00	19:00	300	7.0%					42.9%	43.0%	0.4%	1.2%	0.4%	0.4%	0.4%	3.1%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	
19:00	20:00	200	4.0%					38.7%	44.7%	0.0%	1.0%	0.5%	0.0%	0.0%	10.6%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%	
20:00	21:00	200	3.4%					29.5%	50.6%	0.0%	0.6%	0.0%	0.0%	0.0%	15.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	200	2.5%					25.6%	56.3%	0.0%	0.6%	0.6%	0.0%	0.0%	14.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	150	3.3%					20.7%	58.6%	0.0%	0.7%	0.0%	0.0%	0.0%	16.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	
23:00	0:00	150	4.1%	19.8%	58.8%	0.0%	0.8%	0.0%	0.0%	0.0%	15.7%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	0.0%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L157	San Wai Street (EB)	Chatham Road North	Gillies Avenue North	0:00	1:00	50	5.7%	28.6%	62.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	4.5%	27.3%	63.7%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	5.0%	25.0%	70.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	6.7%	26.7%	59.9%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	7.7%	23.1%	69.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	6.7%	20.0%	66.6%	0.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	3.7%	22.2%	66.7%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	100	3.8%	33.3%	44.9%	1.3%	6.4%	2.6%	0.0%	0.0%	0.0%	1.3%	1.3%	3.8%	1.3%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	150	6.3%	41.2%	28.9%	0.8%	10.2%	6.3%	0.8%	0.8%	0.0%	0.8%	0.8%	2.3%	0.8%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	150	5.3%	34.9%	32.5%	0.9%	10.5%	7.0%	0.9%	0.9%	0.0%	1.8%	0.9%	3.5%	0.9%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	5.5%	29.7%	34.0%	1.1%	16.5%	8.8%	1.1%	1.1%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	0.0%
				11:00	12:00	100	4.8%	30.8%	31.0%	2.4%	21.4%	3.6%	0.0%	0.0%	0.0%	1.2%	0.0%	2.4%	1.2%	1.2%	0.0%	0.0%	0.0%
				12:00	13:00	100	4.6%	38.1%	36.8%	1.1%	13.8%	3.4%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	3.6%	27.2%	26.2%	1.2%	17.9%	11.9%	1.2%	2.4%	0.0%	0.0%	1.2%	3.6%	1.2%	2.4%	0.0%	0.0%	0.0%
				14:00	15:00	100	3.7%	25.6%	35.5%	1.2%	17.1%	8.5%	1.2%	1.2%	0.0%	0.0%	1.2%	2.4%	1.2%	1.2%	0.0%	0.0%	0.0%
				15:00	16:00	150	5.0%	27.7%	34.5%	1.0%	14.9%	8.9%	1.0%	1.0%	0.0%	1.0%	1.0%	2.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	7.0%	36.7%	33.8%	1.4%	12.7%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.2%	0.0%	0.0%	0.0%
				17:00	18:00	100	7.4%	41.5%	33.0%	1.1%	7.4%	5.3%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.1%	0.0%	0.0%
				18:00	19:00	100	7.1%	42.8%	34.5%	1.2%	6.0%	3.6%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	0.0%	0.0%	0.0%
				19:00	20:00	100	6.0%	47.5%	41.7%	0.0%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	5.9%	39.7%	51.5%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	4.8%	33.9%	56.5%	0.0%	3.2%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	7.1%	28.6%	62.5%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	6.7%	28.9%	62.2%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L158	San Wai Street (WB)	Gillies Avenue North	Chatham Road North	0:00	1:00	50	4.8%	14.3%	66.6%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	9.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	8.3%	16.7%	75.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	9.1%	18.2%	72.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	11.1%	22.2%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	16.7%	83.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	14.3%	85.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	5.9%	11.8%	64.6%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	11.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	2.9%	22.9%	57.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	17.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	6.4%	34.0%	44.8%	0.0%	2.1%	2.1%	0.0%	0.0%	10.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	4.9%	29.3%	51.2%	0.0%	2.4%	2.4%	0.0%	0.0%	9.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	6.3%	22.9%	39.5%	0.0%	18.8%	4.2%	0.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	4.3%	21.7%	34.9%	0.0%	23.9%	2.2%	0.0%	0.0%	13.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	4.2%	27.1%	41.6%	0.0%	14.6%	2.1%	0.0%	0.0%	10.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	5.0%	22.5%	32.5%	0.0%	22.5%	5.0%	0.0%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	2.4%	19.0%	42.9%	0.0%	19.0%	4.8%	0.0%	0.0%	11.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	4.0%	22.0%	44.0%	0.0%	18.0%	4.0%	0.0%	0.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	17:00	100	5.7%	24.5%	32.1%	0.0%	26.4%	1.9%	0.0%	0.0%	9.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	6.3%	31.7%	33.4%	0.0%	17.5%	3.2%	0.0%	0.0%	7.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	7.4%	35.2%	37.0%	0.0%	14.8%	1.9%	0.0%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	4.3%	34.8%	47.8%	0.0%	2.2%	0.0%	0.0%	0.0%	10.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	4.8%	26.2%	49.9%	0.0%	2.4%	0.0%	0.0%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	2.6%	21.1%	57.9%	0.0%	2.6%	0.0%	0.0%	0.0%	15.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	5.6%	16.7%	58.2%	0.0%	2.8%	0.0%	0.0%	0.0%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	6.5%	16.1%	58.1%	0.0%	3.2%	0.0%	0.0%	0.0%	16.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L159	Chatham Road North (SB)	San Lau Street	San Wai Street	0:00	1:00	300	6.6%	27.9%	47.7%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.3%	0.3%	0.7%	0.3%	0.3%	0.0%	9.7%	
				1:00	2:00	200	6.6%	31.6%	55.9%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	
				2:00	3:00	150	5.7%	29.3%	59.2%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	
				3:00	4:00	150	7.5%	31.1%	52.9%	0.0%	4.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.8%	
				4:00	5:00	100	6.5%	28.3%	57.6%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%	
				5:00	6:00	150	5.7%	27.4%	55.7%	0.0%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.9%	0.9%	1.9%	0.9%	0.9%	0.0%	0.0%
				6:00	7:00	250	4.3%	21.7%	45.0%	0.4%	4.7%	1.3%	0.0%	0.0%	4.3%	0.4%	0.9%	1.7%	1.3%	0.4%	0.0%	13.6%	
				7:00	8:00	750	3.2%	23.4%	32.4%	0.6%	5.7%	1.7%	0.3%	0.3%	3.5%	0.7%	0.7%	1.7%	1.4%	0.6%	0.0%	23.8%	
				8:00	9:00	1,100	6.2%	32.7%	23.3%	0.8%	9.4%	5.5%	1.1%	1.0%	1.9%	0.6%	0.6%	1.2%	1.0%	0.4%	0.0%	14.3%	
				9:00	10:00	1,000	4.8%	27.2%	25.5%	0.7%	9.5%	6.3%	1.2%	1.1%	1.7%	0.9%	0.7%	1.6%	1.5%	0.5%	0.0%	16.8%	
				10:00	11:00	850	5.2%	28.1%	23.5%	1.1%	14.2%	6.7%	1.6%	1.5%	2.5%	0.2%	0.4%	0.9%	0.7%	0.2%	0.0%	13.2%	
				11:00	12:00	800	3.5%	27.7%	20.8%	1.6%	18.5%	2.6%	0.7%	0.5%	4.7%	0.9%	0.5%	1.2%	1.2%	0.4%	0.0%	15.2%	
				12:00	13:00	800	4.3%	35.9%	25.5%	1.0%	12.0%	2.4%	0.7%	0.5%	3.5%	1.0%	0.3%	0.5%	0.5%	0.1%	0.0%	11.8%	
				13:00	14:00	800	3.3%	24.3%	17.1%	1.3%	14.8%	8.3%	2.1%	2.0%	3.4%	0.3%	1.0%	2.2%	2.2%	0.8%	0.0%	16.9%	
				14:00	15:00	750	2.8%	23.4%	24.4%	1.2%	14.4%	6.6%	1.6%	1.5%	3.8%	0.4%	0.7%	1.6%	1.5%	0.5%	0.0%	15.6%	
				15:00	16:00	900	4.1%	26.8%	24.3%	1.1%	13.4%	7.0%	1.8%	1.6%	2.9%	0.7%	0.5%	1.1%	1.0%	0.3%	0.0%	13.4%	
				16:00	17:00	700	4.7%	40.3%	19.5%	1.0%	9.9%	2.2%	0.6%	0.6%	5.0%	0.1%	0.4%	1.2%	1.3%	0.4%	0.0%	12.8%	
				17:00	18:00	900	5.6%	47.0%	19.1%	0.7%	6.1%	3.3%	1.1%	1.0%	4.1%	0.2%	0.2%	0.6%	0.6%	0.2%	0.0%	10.2%	
				18:00	19:00	750	5.9%	50.8%	21.2%	0.4%	5.0%	2.3%	0.8%	0.8%	1.9%	0.1%	0.3%	0.8%	0.8%	0.3%	0.0%	8.6%	
				19:00	20:00	750	5.1%	44.5%	28.4%	0.1%	2.3%	1.1%	0.1%	0.3%	3.7%	0.3%	0.3%	0.5%	0.5%	0.1%	0.0%	12.7%	
				20:00	21:00	650	5.0%	35.9%	34.3%	0.2%	2.0%	0.5%	0.2%	0.2%	6.0%	0.2%	0.2%	0.3%	0.2%	0.2%	0.0%	14.6%	
				21:00	22:00	600	3.8%	31.3%	37.9%	0.2%	2.7%	0.9%	0.2%	0.2%	5.5%	0.0%	0.0%	0.2%	0.0%	0.0%	0.0%	17.1%	
				22:00	23:00	550	4.9%	24.2%	38.3%	0.2%	1.5%	0.0%	0.0%	0.0%	5.9%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	23.8%	
				L160	Chatham Road North (SB)	San Wai Street	Pak Kung Street	23:00	0:00	450	5.2%	23.3%	38.5%	0.2%	1.8%	0.0%	0.0%	0.0%	5.9%	0.2%	0.2%	0.5%	0.2%
0:00	1:00	300	6.2%					26.8%	47.7%	0.0%	2.2%	0.0%	0.0%	0.0%	4.3%	0.4%	0.0%	0.7%	0.4%	0.4%	0.0%	10.9%	
1:00	2:00	150	6.9%					30.3%	56.6%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.8%	
2:00	3:00	150	6.1%					28.0%	60.6%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.0%	
3:00	4:00	100	8.0%					30.0%	54.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.0%	
4:00	5:00	100	6.8%					27.3%	58.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%	
5:00	6:00	100	6.3%					27.1%	58.4%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	1.0%	1.0%	0.0%	0.0%	
6:00	7:00	250	4.5%					20.6%	46.0%	0.4%	4.5%	0.9%	0.0%	0.0%	5.4%	0.4%	0.4%	1.3%	0.9%	0.4%	0.0%	14.3%	
7:00	8:00	700	3.0%					21.7%	33.3%	0.6%	5.1%	1.5%	0.3%	0.3%	4.4%	0.6%	0.5%	1.1%	1.1%	0.6%	0.0%	25.9%	
8:00	9:00	1,000	6.1%					31.4%	24.4%	0.7%	8.7%	5.4%	1.2%	1.0%	2.4%	0.4%	0.4%	0.8%	0.8%	0.4%	0.0%	15.9%	
9:00	10:00	900	4.6%					25.5%	26.7%	0.7%	8.8%	6.2%	1.2%	1.1%	2.3%	0.7%	0.6%	1.1%	1.2%	0.6%	0.0%	18.7%	
10:00	11:00	750	5.2%					26.7%	24.2%	1.1%	13.8%	6.4%	1.6%	1.5%	3.1%	0.3%	0.3%	0.7%	0.5%	0.3%	0.0%	14.3%	
11:00	12:00	750	3.5%					27.0%	21.3%	1.4%	17.7%	2.5%	0.7%	0.6%	5.7%	0.7%	0.4%	1.0%	0.8%	0.4%	0.0%	16.3%	
12:00	13:00	750	4.3%					34.8%	26.2%	1.0%	11.5%	2.2%	0.7%	0.6%	4.2%	0.8%	0.1%	0.4%	0.4%	0.1%	0.0%	12.7%	
13:00	14:00	750	3.2%					23.7%	17.6%	1.3%	14.4%	8.0%	2.0%	2.0%	4.1%	0.3%	0.8%	1.8%	1.7%	0.8%	0.0%	18.3%	
14:00	15:00	700	2.8%					22.8%	24.7%	1.2%	13.8%	6.4%	1.6%	1.4%	4.5%	0.4%	0.6%	1.2%	1.2%	0.6%	0.0%	16.8%	
15:00	16:00	850	4.1%					25.7%	25.1%	1.1%	13.0%	6.8%	1.7%	1.6%	3.4%	0.6%	0.4%	0.9%	0.7%	0.4%	0.0%	14.5%	
16:00	17:00	700	4.9%					38.7%	19.6%	0.9%	10.3%	2.0%	0.6%	0.6%	5.8%	0.2%	0.5%	1.2%	0.8%	0.5%	0.0%	13.4%	
17:00	18:00	850	5.7%					45.9%	19.4%	0.6%	6.4%	3.1%	1.1%	0.9%	4.7%	0.2%	0.2%	0.6%	0.4%	0.2%	0.0%	10.6%	
18:00	19:00	750	6.0%					50.0%	21.6%	0.4%	5.3%	2.1%	0.7%	0.7%	2.2%	0.1%	0.3%	0.8%	0.6%	0.3%	0.0%	8.9%	
19:00	20:00	750	5.0%					43.0%	29.0%	0.1%	2.1%	1.0%	0.1%	0.3%	4.4%	0.3%	0.1%	0.4%	0.4%	0.1%	0.0%	13.7%	
20:00	21:00	600	5.0%					34.4%	34.4%	0.2%	1.9%	0.3%	0.2%	0.2%	7.0%	0.2%	0.0%	0.2%	0.2%	0.2%	0.0%	15.6%	
21:00	22:00	550	3.5%					29.7%	38.3%	0.2%	2.4%	0.9%	0.2%	0.2%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	18.1%	
22:00	23:00	550	4.7%					22.6%	38.3%	0.2%	1.3%	0.0%	0.0%	0.0%	6.8%	0.2%	0.2%	0.4%	0.2%	0.2%	0.0%	24.9%	
23:00	0:00	450	4.9%	21.8%	38.6%	0.2%	1.6%	0.0%	0.0%	0.0%	7.0%	0.2%	0.2%	0.5%	0.2%	0.2%	0.0%	24.6%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L161	Chatham Road North (NB)	Pak Kung Street	San Lau Street	0:00	1:00	250	7.4%	18.6%	48.4%	0.0%	3.3%	0.0%	0.0%	0.0%	8.7%	0.4%	0.4%	1.7%	0.8%	0.4%	0.0%	9.9%
				1:00	2:00	150	8.5%	22.0%	61.0%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%
				2:00	3:00	150	7.3%	20.2%	65.1%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.7%
				3:00	4:00	100	9.6%	21.7%	56.7%	0.0%	7.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.8%
				4:00	5:00	100	8.2%	19.2%	61.6%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	5.5%
				5:00	6:00	100	6.9%	17.2%	57.8%	0.0%	8.0%	0.0%	0.0%	1.1%	0.0%	1.1%	1.1%	3.4%	2.3%	1.1%	0.0%	0.0%
				6:00	7:00	250	4.7%	13.1%	42.3%	0.5%	6.1%	0.9%	0.0%	0.5%	8.9%	1.4%	1.4%	3.3%	2.8%	1.4%	0.0%	12.7%
				7:00	8:00	650	2.8%	15.9%	35.1%	0.3%	3.8%	1.2%	0.7%	0.5%	12.5%	1.3%	1.3%	2.8%	2.5%	1.0%	0.0%	18.3%
				8:00	9:00	850	5.8%	24.5%	27.2%	0.5%	6.8%	4.2%	2.2%	2.2%	7.3%	1.1%	1.1%	2.3%	2.0%	0.9%	0.0%	11.9%
				9:00	10:00	800	4.3%	19.5%	29.1%	0.5%	6.7%	4.7%	2.3%	2.5%	6.6%	1.8%	1.4%	3.1%	2.7%	1.2%	0.0%	13.6%
				* 0:00	11:00	750	5.5%	16.8%	22.3%	1.0%	18.9%	6.5%	3.1%	2.9%	5.2%	0.7%	0.8%	1.7%	1.7%	0.6%	0.0%	12.3%
				11:00	12:00	750	3.5%	16.2%	18.9%	1.3%	23.6%	2.5%	1.3%	1.0%	9.2%	2.1%	1.1%	2.5%	2.4%	0.8%	0.0%	13.6%
				12:00	13:00	650	4.8%	22.8%	25.3%	0.9%	16.6%	2.3%	1.4%	1.1%	7.4%	2.5%	0.6%	1.1%	1.2%	0.5%	0.0%	11.5%
				13:00	14:00	750	3.1%	13.7%	15.1%	1.0%	18.4%	7.5%	3.5%	3.5%	6.4%	0.5%	2.3%	4.5%	4.5%	1.5%	0.0%	14.5%
				14:00	15:00	700	2.7%	13.6%	22.0%	1.0%	18.1%	6.1%	2.9%	2.7%	7.2%	1.0%	1.6%	3.2%	3.0%	1.2%	0.0%	13.7%
				15:00	16:00	800	4.2%	15.8%	22.5%	0.9%	17.5%	6.7%	3.3%	3.2%	5.7%	1.6%	1.1%	2.4%	2.1%	0.8%	0.0%	12.2%
				16:00	17:00	700	5.7%	20.3%	14.5%	1.0%	22.8%	3.0%	1.2%	1.2%	5.8%	0.3%	1.7%	3.3%	3.8%	1.0%	0.0%	14.4%
				17:00	18:00	800	7.5%	26.3%	15.9%	0.8%	15.6%	5.1%	2.4%	2.1%	5.3%	1.0%	1.0%	1.9%	1.9%	0.6%	0.0%	12.6%
				18:00	19:00	650	8.3%	30.2%	18.5%	0.5%	13.4%	3.5%	1.7%	1.5%	2.6%	0.9%	1.5%	2.6%	2.9%	0.8%	0.0%	11.1%
				19:00	20:00	600	6.0%	30.7%	30.8%	0.2%	3.4%	1.2%	0.3%	0.5%	8.6%	0.9%	0.7%	1.4%	1.4%	0.5%	0.0%	13.4%
				20:00	21:00	500	5.8%	24.0%	36.0%	0.2%	2.8%	0.4%	0.4%	0.4%	13.2%	0.4%	0.2%	0.6%	0.4%	0.2%	0.0%	15.0%
				21:00	22:00	500	4.2%	20.8%	39.4%	0.2%	3.7%	0.9%	0.4%	0.4%	12.3%	0.0%	0.0%	0.2%	0.0%	0.2%	0.0%	17.3%
				22:00	23:00	500	5.4%	15.4%	38.6%	0.2%	2.0%	0.0%	0.0%	0.0%	12.6%	0.4%	0.4%	0.9%	0.4%	0.4%	0.0%	23.3%
				23:00	0:00	400	5.6%	14.9%	37.8%	0.3%	2.7%	0.0%	0.0%	0.0%	12.5%	0.5%	0.5%	1.1%	0.5%	0.5%	0.0%	23.1%
L162	Ko Shan Road (SB)	San Lau Street	Wai Yin Path	0:00	1:00	200	3.8%	28.7%	61.2%	0.0%	1.9%	0.0%	0.0%	0.0%	3.2%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%
				1:00	2:00	100	3.3%	28.9%	65.6%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.6%	26.2%	69.0%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.8%	29.0%	63.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.7%	25.9%	68.5%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.2%	25.8%	64.6%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	1.6%	0.0%	0.0%
				6:00	7:00	150	2.5%	23.5%	61.6%	0.0%	3.4%	0.8%	0.0%	0.0%	4.2%	0.8%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%
				7:00	8:00	300	2.4%	35.3%	40.5%	0.4%	5.5%	1.2%	0.4%	0.4%	6.7%	1.2%	1.2%	2.0%	2.0%	0.8%	0.0%	0.0%
				8:00	9:00	450	4.5%	46.1%	27.4%	0.2%	8.5%	3.5%	1.0%	1.0%	3.2%	0.7%	0.7%	1.5%	1.2%	0.5%	0.0%	0.0%
				9:00	10:00	400	3.6%	39.0%	31.1%	0.3%	9.0%	4.2%	1.1%	1.1%	3.1%	1.4%	1.1%	2.2%	2.0%	0.8%	0.0%	0.0%
				10:00	11:00	400	3.7%	34.4%	36.7%	0.3%	13.4%	4.0%	1.1%	1.1%	2.6%	0.3%	0.3%	0.9%	0.6%	0.6%	0.0%	0.0%
				11:00	12:00	350	2.7%	35.0%	33.1%	0.6%	17.5%	1.5%	0.3%	0.3%	5.1%	0.9%	0.3%	1.2%	0.9%	0.6%	0.0%	0.0%
				12:00	13:00	400	3.1%	41.2%	37.2%	0.3%	10.6%	1.4%	0.3%	0.3%	3.3%	0.8%	0.3%	0.6%	0.3%	0.3%	0.0%	0.0%
				13:00	14:00	350	2.6%	33.2%	29.5%	0.3%	15.3%	5.5%	1.3%	1.6%	3.9%	0.3%	1.0%	2.3%	1.6%	1.6%	0.0%	0.0%
				14:00	15:00	350	2.2%	29.8%	39.0%	0.3%	14.0%	4.1%	1.0%	1.0%	4.1%	0.3%	0.6%	1.6%	1.0%	1.0%	0.0%	0.0%
				15:00	16:00	400	2.9%	32.8%	38.3%	0.3%	12.8%	4.2%	1.0%	1.0%	3.1%	0.5%	0.5%	1.0%	0.8%	0.8%	0.0%	0.0%
				16:00	17:00	400	3.6%	41.3%	37.8%	0.3%	10.1%	1.4%	0.3%	0.3%	3.3%	0.0%	0.0%	0.5%	0.0%	1.1%	0.0%	0.0%
				17:00	18:00	500	3.9%	47.4%	36.3%	0.2%	6.0%	1.9%	0.4%	0.4%	2.7%	0.0%	0.0%	0.2%	0.0%	0.6%	0.0%	0.0%
				18:00	19:00	450	4.0%	49.0%	38.4%	0.2%	4.7%	1.2%	0.2%	0.2%	1.2%	0.0%	0.0%	0.2%	0.0%	0.7%	0.0%	0.0%
				19:00	20:00	400	3.3%	49.0%	39.8%	0.0%	1.9%	0.5%	0.0%	0.3%	3.5%	0.3%	0.3%	0.5%	0.3%	0.3%	0.0%	0.0%
				20:00	21:00	350	3.3%	40.1%	48.4%	0.0%	1.7%	0.3%	0.0%	0.0%	5.6%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%
				21:00	22:00	300	2.6%	35.2%	54.4%	0.0%	2.2%	0.4%	0.0%	0.0%	5.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	3.7%	29.3%	58.9%	0.0%	1.2%	0.0%	0.0%	0.0%	6.1%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%
				23:00	0:00	200	3.6%	28.4%	59.4%	0.0%	1.5%	0.0%	0.0%	0.0%	6.1%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L163	Ko Shan Road (NB)	Wai Yin Path	San Lau Street	0:00	1:00	200	3.1%	23.8%	65.8%	0.0%	1.6%	0.0%	0.0%	0.0%	4.7%	0.0%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%	
				1:00	2:00	150	2.7%	24.5%	71.0%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.9%	21.9%	73.3%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.0%	24.0%	69.3%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.0%	22.4%	73.1%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.6%	20.8%	70.1%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	1.3%	0.0%	0.0%	0.0%
				6:00	7:00	150	2.0%	18.7%	65.3%	0.0%	3.3%	0.7%	0.0%	0.0%	5.3%	0.7%	0.7%	1.3%	1.3%	0.7%	0.0%	0.0%	0.0%
				7:00	8:00	400	0.8%	22.3%	59.5%	0.3%	3.7%	0.5%	0.3%	0.3%	6.5%	0.8%	0.8%	1.8%	1.6%	0.8%	0.0%	0.0%	0.0%
				8:00	9:00	550	1.5%	33.5%	46.1%	0.4%	6.5%	1.7%	1.1%	1.0%	3.8%	0.6%	0.6%	1.5%	1.1%	0.6%	0.0%	0.0%	0.0%
				9:00	10:00	500	1.2%	27.4%	50.4%	0.4%	6.6%	1.9%	1.2%	1.0%	3.5%	1.0%	0.8%	2.1%	1.7%	0.8%	0.0%	0.0%	0.0%
				10:00	11:00	450	3.3%	29.7%	41.1%	0.5%	12.4%	4.0%	1.4%	1.2%	3.8%	0.2%	0.5%	1.0%	0.7%	0.2%	0.0%	0.0%	0.0%
				11:00	12:00	400	2.3%	30.1%	36.6%	0.8%	16.2%	1.5%	0.5%	0.5%	7.1%	1.0%	0.5%	1.3%	1.3%	0.3%	0.0%	0.0%	0.0%
				12:00	13:00	450	2.6%	35.7%	41.5%	0.5%	9.8%	1.4%	0.5%	0.5%	4.9%	1.2%	0.2%	0.5%	0.5%	0.2%	0.0%	0.0%	0.0%
				13:00	14:00	400	2.2%	28.3%	33.0%	0.5%	14.1%	5.4%	1.9%	1.6%	5.4%	0.3%	1.4%	2.7%	2.4%	0.8%	0.0%	0.0%	0.0%
				14:00	15:00	400	1.8%	25.3%	42.8%	0.5%	12.5%	3.9%	1.3%	1.0%	5.7%	0.5%	0.8%	1.8%	1.6%	0.5%	0.0%	0.0%	0.0%
				15:00	16:00	500	2.6%	27.9%	42.2%	0.4%	11.4%	4.1%	1.5%	1.3%	4.3%	0.9%	0.6%	1.3%	1.1%	0.4%	0.0%	0.0%	0.0%
				16:00	17:00	400	5.2%	42.6%	28.5%	0.3%	11.4%	2.3%	0.5%	0.3%	6.0%	0.3%	0.8%	0.8%	1.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	550	5.9%	48.7%	27.2%	0.2%	6.8%	3.5%	0.8%	0.6%	4.7%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	450	6.1%	52.0%	29.4%	0.2%	5.4%	2.2%	0.4%	0.4%	2.2%	0.2%	0.4%	0.4%	0.7%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	450	2.8%	42.9%	44.9%	0.0%	1.8%	0.5%	0.2%	0.2%	5.1%	0.2%	0.2%	0.5%	0.5%	0.2%	0.0%	0.0%	0.0%
				20:00	21:00	400	2.7%	33.7%	52.5%	0.0%	1.4%	0.3%	0.3%	0.3%	7.9%	0.3%	0.0%	0.3%	0.3%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	350	2.1%	29.3%	58.1%	0.0%	2.1%	0.6%	0.3%	0.3%	7.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	350	2.9%	23.9%	62.6%	0.0%	1.3%	0.0%	0.0%	0.0%	8.1%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%	0.0%
L164	Wai Yin Path (EB)	Cul-de-sac	Ko Shan Road	23:00	0:00	250	3.2%	23.0%	62.5%	0.0%	1.6%	0.0%	0.0%	0.0%	8.1%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%	
				0:00	1:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	0.0%	77.7%	11.1%	5.6%	0.0%	0.0%	5.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	73.7%	5.3%	2.6%	2.6%	5.3%	7.9%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	67.7%	6.5%	3.2%	3.2%	6.5%	9.7%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	0.0%	50.0%	17.9%	3.6%	7.1%	7.1%	10.7%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	56.7%	17.4%	4.3%	13.0%	4.3%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	0.0%	63.0%	18.5%	3.7%	7.4%	3.7%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	46.3%	11.5%	3.8%	7.7%	11.5%	15.4%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	0.0%	44.0%	20.0%	4.0%	8.0%	8.0%	12.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	0.0%	46.8%	18.8%	3.1%	6.3%	9.4%	12.5%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	52.0%	28.0%	0.0%	12.0%	4.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	57.2%	25.7%	0.0%	5.7%	5.7%	5.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	63.4%	26.7%	0.0%	3.3%	3.3%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	0.0%	77.8%	22.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	70.0%	30.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	64.7%	35.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	58.3%	41.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L165	Wai Yin Path (WB)	Ko Shan Road	Cul-de-sac	0:00	1:00	50	30.0%	60.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	25.0%	75.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	28.6%	57.1%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	22.2%	74.1%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	27.3%	62.2%	1.5%	0.0%	0.0%	3.0%	3.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	25.5%	60.8%	2.0%	0.0%	0.0%	3.9%	3.9%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*10:00	11:00	50	22.6%	54.8%	3.2%	0.0%	3.2%	6.5%	6.5%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*11:00	12:00	50	16.0%	64.0%	4.0%	0.0%	8.0%	4.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*12:00	13:00	50	16.7%	70.1%	3.3%	0.0%	3.3%	3.3%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*13:00	14:00	50	13.8%	48.4%	3.4%	0.0%	6.9%	10.3%	10.3%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*14:00	15:00	50	13.0%	56.7%	4.3%	0.0%	4.3%	8.7%	8.7%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*15:00	16:00	50	18.2%	51.5%	3.0%	0.0%	6.1%	9.1%	9.1%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	0.0%	64.7%	5.9%	0.0%	17.6%	5.9%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	0.0%	69.6%	4.3%	0.0%	8.7%	8.7%	8.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	0.0%	78.8%	5.3%	0.0%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	18.8%	78.1%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	21.7%	74.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	17.6%	76.5%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	26.7%	66.6%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	30.8%	61.5%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L166	Ko Shan Road (NB)	Pak Kung Street	Wai Yin Path	0:00	1:00	250	3.4%	23.7%	66.2%	0.0%	1.4%	0.0%	0.0%	0.0%	4.3%	0.0%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%
				1:00	2:00	150	3.3%	24.0%	70.2%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	150	2.7%	21.4%	74.1%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	3.7%	24.7%	69.1%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	2.8%	22.2%	73.6%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	2.4%	20.7%	70.8%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	0.0%	0.0%	0.0%
				6:00	7:00	200	2.5%	18.6%	66.0%	0.0%	3.1%	0.6%	0.0%	0.0%	5.0%	0.6%	1.2%	1.2%	0.6%	0.0%	0.0%	0.0%
				7:00	8:00	450	1.0%	22.6%	60.4%	0.2%	3.8%	0.5%	0.2%	0.2%	5.9%	0.7%	0.7%	1.7%	1.4%	0.7%	0.0%	0.0%
				8:00	9:00	600	1.7%	33.9%	46.4%	0.3%	6.6%	1.7%	0.9%	0.9%	3.5%	0.5%	0.5%	1.4%	1.2%	0.5%	0.0%	0.0%
				9:00	10:00	550	1.3%	27.7%	50.6%	0.4%	6.8%	2.1%	0.9%	0.9%	3.2%	0.9%	0.8%	1.9%	1.7%	0.8%	0.0%	0.0%
				10:00	11:00	450	3.4%	29.8%	42.1%	0.4%	12.3%	3.8%	1.1%	1.1%	3.6%	0.2%	0.4%	0.9%	0.7%	0.2%	0.0%	0.0%
				11:00	12:00	450	2.4%	30.1%	37.4%	0.7%	16.1%	1.7%	0.5%	0.5%	6.6%	0.9%	0.5%	1.2%	1.2%	0.2%	0.0%	0.0%
				12:00	13:00	500	2.6%	35.7%	42.5%	0.4%	9.8%	1.3%	0.4%	0.4%	4.6%	1.1%	0.2%	0.4%	0.4%	0.2%	0.0%	0.0%
				13:00	14:00	400	2.3%	28.6%	33.2%	0.5%	14.3%	5.4%	1.5%	1.5%	5.1%	0.3%	1.3%	2.6%	2.6%	0.8%	0.0%	0.0%
				14:00	15:00	450	1.7%	25.4%	43.3%	0.5%	12.7%	3.9%	1.2%	1.0%	5.4%	0.5%	0.7%	1.7%	1.5%	0.5%	0.0%	0.0%
				15:00	16:00	500	2.6%	28.1%	43.0%	0.4%	11.5%	4.0%	1.2%	1.2%	4.0%	0.8%	0.6%	1.2%	1.0%	0.4%	0.0%	0.0%
				16:00	17:00	400	5.1%	42.9%	28.5%	0.3%	11.4%	2.3%	0.5%	0.3%	5.8%	0.3%	0.8%	1.0%	1.0%	0.0%	0.0%	0.0%
				17:00	18:00	550	5.9%	48.9%	27.3%	0.2%	6.8%	3.4%	0.8%	0.6%	4.5%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%
				18:00	19:00	500	6.1%	52.1%	29.4%	0.2%	5.4%	2.2%	0.4%	0.4%	2.2%	0.2%	0.4%	0.4%	0.6%	0.0%	0.0%	0.0%
				19:00	20:00	500	2.8%	42.9%	45.7%	0.0%	1.7%	0.4%	0.2%	0.2%	4.7%	0.2%	0.2%	0.4%	0.4%	0.2%	0.0%	0.0%
				20:00	21:00	400	2.8%	33.7%	52.9%	0.0%	1.5%	0.3%	0.3%	0.3%	7.3%	0.3%	0.0%	0.3%	0.3%	0.0%	0.0%	0.0%
				21:00	22:00	400	1.9%	29.2%	59.1%	0.0%	1.9%	0.6%	0.3%	0.3%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	350	3.0%	23.9%	63.1%	0.0%	1.2%	0.0%	0.0%	0.0%	7.6%	0.3%	0.3%	0.3%	0.3%	0.0%	0.0%	0.0%
				23:00	0:00	300	3.0%	23.2%	63.2%	0.0%	1.5%	0.0%	0.0%	0.0%	7.5%	0.4%	0.4%	0.4%	0.4%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L167	Ko Shan Road (SB)	Wai Yin Path	Pak Kung Street	0:00	1:00	200	3.8%	28.9%	61.1%	0.0%	1.9%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	0.0%
				1:00	2:00	100	3.3%	29.3%	65.2%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.5%	26.7%	68.6%	0.0%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	4.8%	29.0%	63.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	3.6%	27.3%	67.3%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	3.2%	25.4%	65.0%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	1.6%	0.0%	0.0%
				6:00	7:00	150	2.5%	23.0%	61.5%	0.0%	3.3%	0.8%	0.0%	0.0%	4.1%	0.8%	0.8%	1.6%	0.8%	0.8%	0.0%	0.0%
				7:00	8:00	300	2.4%	34.8%	40.6%	0.4%	5.9%	1.2%	0.4%	0.4%	6.7%	1.2%	1.2%	2.0%	2.0%	0.8%	0.0%	0.0%
				8:00	9:00	400	4.3%	45.3%	27.1%	0.3%	9.0%	3.8%	1.0%	1.0%	3.3%	0.8%	0.8%	1.5%	1.3%	0.5%	0.0%	0.0%
				9:00	10:00	400	3.4%	38.5%	30.8%	0.3%	9.6%	4.5%	1.1%	1.1%	3.1%	1.4%	1.1%	2.3%	2.0%	0.8%	0.0%	0.0%
				10:00	11:00	400	3.6%	34.5%	36.9%	0.3%	13.2%	4.2%	1.1%	1.1%	2.5%	0.3%	0.3%	0.8%	0.6%	0.6%	0.0%	0.0%
				11:00	12:00	350	2.7%	34.7%	33.3%	0.6%	17.3%	1.8%	0.3%	0.3%	5.1%	0.9%	0.3%	1.2%	0.9%	0.6%	0.0%	0.0%
				12:00	13:00	400	3.0%	41.4%	37.4%	0.3%	10.4%	1.4%	0.3%	0.3%	3.3%	0.8%	0.3%	0.5%	0.3%	0.3%	0.0%	0.0%
				13:00	14:00	350	2.6%	32.9%	29.5%	0.3%	15.1%	5.8%	1.3%	1.6%	3.8%	0.3%	1.0%	2.6%	1.6%	1.6%	0.0%	0.0%
				14:00	15:00	350	2.2%	30.1%	39.3%	0.3%	13.8%	4.1%	0.9%	0.9%	4.1%	0.3%	0.6%	1.6%	0.9%	0.9%	0.0%	0.0%
				15:00	16:00	400	2.8%	32.9%	38.3%	0.3%	12.6%	4.4%	1.0%	1.0%	3.1%	0.5%	0.5%	1.0%	0.8%	0.8%	0.0%	0.0%
				16:00	17:00	400	3.5%	41.9%	38.2%	0.3%	9.1%	1.3%	0.3%	0.3%	3.2%	0.0%	0.0%	0.8%	0.0%	1.1%	0.0%	0.0%
				17:00	18:00	500	4.0%	47.7%	36.5%	0.2%	5.4%	1.8%	0.4%	0.4%	2.6%	0.0%	0.0%	0.4%	0.0%	0.6%	0.0%	0.0%
				18:00	19:00	450	4.0%	49.3%	38.5%	0.2%	4.3%	1.1%	0.2%	0.2%	1.1%	0.0%	0.0%	0.4%	0.0%	0.7%	0.0%	0.0%
				19:00	20:00	400	3.2%	49.0%	39.9%	0.0%	1.9%	0.5%	0.0%	0.3%	3.5%	0.3%	0.3%	0.5%	0.3%	0.3%	0.0%	0.0%
				20:00	21:00	350	3.3%	40.2%	48.4%	0.0%	1.6%	0.3%	0.0%	0.0%	5.6%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%
				21:00	22:00	300	2.6%	35.4%	54.3%	0.0%	2.2%	0.4%	0.0%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	250	3.6%	29.3%	59.1%	0.0%	1.2%	0.0%	0.0%	0.0%	6.0%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%
L168	Pak Kung Street (EB)	Shun Yung Street	Chatham Road North	23:00	0:00	200	3.5%	28.5%	59.5%	0.0%	1.5%	0.0%	0.0%	0.0%	6.0%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%
				0:00	1:00	100	3.9%	37.3%	54.8%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%
				1:00	2:00	50	3.4%	37.9%	58.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	3.7%	33.3%	63.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	5.0%	40.0%	55.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	5.6%	33.3%	61.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	4.5%	31.8%	54.7%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%	0.0%	0.0%
				6:00	7:00	50	2.6%	31.6%	55.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	5.3%	0.0%	0.0%
				7:00	8:00	150	2.2%	36.5%	48.3%	0.7%	2.2%	0.7%	0.7%	0.7%	0.0%	0.0%	0.0%	2.9%	0.0%	5.1%	0.0%	0.0%
				8:00	9:00	250	3.7%	48.7%	32.2%	0.9%	3.7%	1.9%	1.9%	1.4%	0.0%	0.0%	0.0%	1.9%	0.0%	3.7%	0.0%	0.0%
				9:00	10:00	200	3.2%	41.3%	36.5%	1.1%	4.2%	2.1%	2.1%	1.6%	0.0%	0.0%	0.0%	2.6%	0.0%	5.3%	0.0%	0.0%
				10:00	11:00	150	4.1%	42.4%	30.1%	2.4%	8.1%	5.7%	2.4%	1.6%	0.0%	0.0%	0.0%	0.8%	0.0%	2.4%	0.0%	0.0%
				11:00	12:00	150	2.7%	44.2%	28.8%	2.7%	11.7%	2.7%	0.9%	0.9%	0.0%	0.0%	0.0%	1.8%	0.0%	3.6%	0.0%	0.0%
				12:00	13:00	150	3.2%	51.6%	31.5%	1.6%	6.5%	1.6%	0.8%	0.8%	0.0%	0.0%	0.0%	0.8%	0.0%	1.6%	0.0%	0.0%
				13:00	14:00	150	2.7%	38.0%	23.0%	2.7%	9.7%	8.0%	3.5%	1.8%	0.0%	0.0%	0.0%	3.5%	0.0%	7.1%	0.0%	0.0%
				14:00	15:00	150	2.7%	36.1%	31.5%	2.7%	9.0%	6.3%	2.7%	1.8%	0.0%	0.0%	0.0%	2.7%	0.0%	4.5%	0.0%	0.0%
				15:00	16:00	150	3.0%	40.3%	31.3%	2.2%	8.2%	6.0%	3.0%	1.5%	0.0%	0.0%	0.0%	1.5%	0.0%	3.0%	0.0%	0.0%
				16:00	17:00	100	5.3%	63.1%	13.2%	2.6%	10.5%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	150	5.6%	66.6%	12.0%	1.9%	5.6%	7.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	5.3%	70.4%	12.6%	1.1%	4.2%	5.3%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	150	3.8%	59.3%	32.3%	0.0%	1.5%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.0%	1.5%	0.0%	0.0%
				20:00	21:00	100	4.0%	52.0%	42.0%	0.0%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.0%	0.0%	0.0%
				21:00	22:00	100	3.4%	46.6%	47.8%	0.0%	1.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.8%	39.7%	53.9%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				23:00	0:00	100	4.8%	38.7%	53.3%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.6%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L169	Pak Kung Street (WB)	Chatham Road North	Shun Yung Street	0:00	1:00	100	7.6%	30.4%	56.5%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	1.1%	0.0%	1.1%	
				1:00	2:00	100	7.3%	30.9%	58.2%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	6.1%	28.6%	63.3%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	8.1%	29.7%	56.8%	0.0%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	6.3%	28.1%	62.5%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	5.3%	26.3%	57.9%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	0.0%	2.6%	0.0%	0.0%
				6:00	7:00	100	5.6%	23.9%	56.5%	0.0%	4.2%	1.4%	0.0%	0.0%	0.0%	1.4%	1.4%	1.4%	1.4%	1.4%	0.0%	1.4%	
				7:00	8:00	200	4.6%	35.3%	43.3%	0.6%	2.9%	0.6%	0.6%	0.6%	0.0%	1.2%	1.2%	1.7%	1.7%	1.7%	0.0%	4.0%	
				8:00	9:00	300	8.3%	45.7%	28.6%	0.4%	4.7%	1.8%	1.8%	1.8%	0.0%	0.7%	0.7%	1.1%	1.1%	1.1%	0.0%	2.2%	
				9:00	10:00	250	6.6%	39.0%	33.1%	0.4%	5.0%	2.1%	2.1%	2.1%	0.0%	1.2%	0.8%	1.7%	1.7%	1.7%	0.0%	2.5%	
				10:00	11:00	250	6.3%	32.2%	30.0%	0.4%	15.2%	5.9%	2.1%	2.1%	0.4%	0.4%	0.4%	1.3%	0.8%	0.8%	0.0%	1.7%	
				11:00	12:00	250	4.2%	33.6%	27.8%	0.9%	20.4%	2.3%	0.9%	0.9%	0.5%	1.4%	0.5%	1.9%	1.4%	1.4%	0.0%	1.9%	
				12:00	13:00	250	5.2%	40.6%	31.6%	0.4%	12.6%	2.2%	0.9%	0.9%	0.4%	1.3%	0.4%	0.9%	0.9%	0.4%	0.0%	1.3%	
				13:00	14:00	250	4.2%	29.4%	22.7%	0.5%	16.7%	7.4%	2.8%	2.8%	0.5%	0.5%	1.4%	3.2%	2.8%	2.8%	0.0%	2.3%	
				14:00	15:00	250	3.3%	28.2%	32.1%	0.5%	15.8%	5.7%	2.4%	1.9%	0.5%	0.5%	1.0%	2.4%	1.9%	1.9%	0.0%	1.9%	
				15:00	16:00	300	4.7%	30.9%	31.0%	0.4%	14.5%	6.3%	2.3%	2.3%	0.4%	0.8%	0.8%	1.6%	1.2%	1.2%	0.0%	1.6%	
				16:00	17:00	250	4.6%	37.3%	25.1%	0.4%	18.4%	3.8%	0.8%	0.4%	0.8%	0.0%	0.8%	2.5%	2.1%	1.7%	0.0%	1.3%	
				17:00	18:00	350	5.6%	44.1%	25.0%	0.3%	11.5%	5.6%	1.3%	1.0%	0.7%	0.3%	0.3%	1.3%	1.0%	1.0%	0.0%	1.0%	
				18:00	19:00	300	5.6%	46.4%	27.1%	0.4%	9.0%	3.8%	0.8%	0.8%	0.4%	0.4%	0.4%	1.5%	1.5%	1.1%	0.0%	0.8%	
				19:00	20:00	250	5.7%	50.1%	35.1%	0.0%	2.2%	0.9%	0.4%	0.4%	0.4%	0.4%	0.4%	0.9%	0.9%	0.4%	0.0%	1.8%	
				20:00	21:00	200	6.1%	42.5%	43.9%	0.0%	2.2%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.6%	0.0%	0.6%	0.0%	1.7%	
				21:00	22:00	200	4.4%	37.5%	50.1%	0.0%	3.1%	0.6%	0.6%	0.6%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.5%
				22:00	23:00	150	6.3%	31.5%	55.2%	0.0%	1.4%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%	0.7%	0.0%	0.7%	0.0%	0.0%	3.5%
				23:00	0:00	150	6.8%	29.9%	54.6%	0.0%	2.6%	0.0%	0.0%	0.0%	0.9%	0.0%	0.0%	0.9%	0.0%	0.9%	0.0%	0.9%	3.4%
L170	Pak Kung Street (EB)	Chatham Road North	Gillies Avenue North	0:00	1:00	50	4.1%	34.7%	57.2%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	3.4%	34.5%	58.7%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	3.8%	30.8%	65.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	5.0%	35.0%	55.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	5.9%	29.4%	64.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	4.8%	28.6%	57.0%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	2.6%	25.6%	53.6%	0.0%	2.6%	2.6%	0.0%	0.0%	0.0%	2.6%	2.6%	2.6%	2.6%	2.6%	0.0%	0.0%	
				7:00	8:00	100	2.4%	38.6%	38.6%	1.2%	3.6%	1.2%	0.0%	0.0%	0.0%	2.4%	2.4%	3.6%	3.6%	2.4%	0.0%	0.0%	
				8:00	9:00	150	4.4%	47.2%	24.8%	1.5%	5.1%	5.1%	1.5%	1.5%	0.0%	1.5%	1.5%	2.2%	2.2%	1.5%	0.0%	0.0%	
				9:00	10:00	150	3.3%	39.9%	27.6%	1.6%	5.7%	5.7%	1.6%	1.6%	0.0%	2.4%	1.6%	3.3%	3.3%	2.4%	0.0%	0.0%	
				10:00	11:00	150	3.1%	34.7%	29.5%	2.3%	11.6%	9.3%	1.6%	1.6%	0.0%	0.8%	0.8%	2.3%	1.6%	0.8%	0.0%	0.0%	
				11:00	12:00	150	2.5%	36.2%	26.9%	2.5%	15.1%	3.4%	0.8%	0.8%	0.0%	2.5%	1.7%	3.4%	2.5%	1.7%	0.0%	0.0%	
				12:00	13:00	150	2.4%	43.3%	30.7%	1.6%	9.4%	3.1%	0.8%	0.8%	0.0%	3.1%	0.8%	1.6%	1.6%	0.8%	0.0%	0.0%	
				13:00	14:00	150	1.6%	30.4%	20.8%	2.4%	12.0%	11.2%	2.4%	1.6%	0.0%	0.8%	3.2%	5.6%	4.8%	3.2%	0.0%	0.0%	
				14:00	15:00	150	1.7%	28.9%	29.5%	2.5%	11.6%	9.1%	1.7%	1.7%	0.0%	1.7%	1.7%	4.1%	3.3%	2.5%	0.0%	0.0%	
				15:00	16:00	150	2.1%	32.8%	30.1%	2.1%	10.5%	9.1%	2.1%	1.4%	0.0%	2.1%	1.4%	2.8%	2.1%	1.4%	0.0%	0.0%	
				16:00	17:00	150	2.1%	40.5%	27.3%	1.4%	11.2%	4.2%	0.7%	0.0%	0.0%	0.7%	2.1%	4.2%	3.5%	2.1%	0.0%	0.0%	
				17:00	18:00	200	2.7%	47.5%	26.5%	1.1%	7.0%	6.5%	1.1%	0.5%	0.0%	1.1%	1.1%	2.2%	1.6%	1.1%	0.0%	0.0%	
				18:00	19:00	200	2.4%	50.2%	28.0%	0.6%	5.5%	4.3%	0.6%	0.6%	0.0%	0.6%	1.2%	2.4%	2.4%	1.2%	0.0%	0.0%	
				19:00	20:00	150	3.2%	53.6%	34.4%	0.0%	1.6%	1.6%	0.0%	0.0%	0.0%	0.8%	0.8%	1.6%	1.6%	0.8%	0.0%	0.0%	
				20:00	21:00	100	3.2%	47.8%	44.7%	0.0%	2.1%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	100	2.4%	42.2%	51.8%	0.0%	2.4%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	100	3.9%	35.5%	55.4%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	1.3%	0.0%	1.3%	0.0%	0.0%	
				23:00	0:00	100	3.3%	33.3%	56.6%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	1.7%	0.0%	1.7%	0.0%	1.7%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L171	Pak Kung Street (WB)	Gillies Avenue North	Chatham Road North	0:00	1:00	50	4.8%	33.3%	38.1%	0.0%	0.0%	0.0%	0.0%	0.0%	23.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	44.4%	55.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	37.5%	62.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	40.0%	60.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	26.7%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	2.4%	26.8%	9.8%	2.4%	2.4%	0.0%	0.0%	0.0%	56.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	4.0%	44.0%	8.0%	2.0%	4.0%	2.0%	0.0%	0.0%	36.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	2.4%	40.4%	9.5%	2.4%	4.8%	2.4%	0.0%	0.0%	38.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	4.3%	38.3%	23.4%	2.1%	8.5%	2.1%	0.0%	0.0%	21.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	1.9%	32.8%	19.2%	3.8%	9.6%	0.0%	0.0%	0.0%	32.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	1.9%	42.3%	23.1%	1.9%	5.8%	0.0%	0.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	2.3%	35.0%	18.6%	2.3%	9.3%	2.3%	0.0%	0.0%	30.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	2.3%	31.8%	25.0%	2.3%	6.8%	2.3%	0.0%	0.0%	29.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	3.8%	36.6%	25.0%	1.9%	7.7%	1.9%	0.0%	0.0%	23.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	3.3%	43.3%	28.3%	1.7%	6.7%	1.7%	0.0%	0.0%	15.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	3.8%	51.4%	26.9%	1.3%	3.8%	1.3%	0.0%	0.0%	11.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	4.4%	54.4%	29.4%	1.5%	2.9%	1.5%	0.0%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	3.6%	48.2%	23.2%	0.0%	1.8%	0.0%	0.0%	0.0%	23.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	2.0%	36.0%	26.0%	0.0%	0.0%	0.0%	0.0%	0.0%	36.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	2.3%	32.6%	30.2%	0.0%	0.0%	0.0%	0.0%	0.0%	34.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	2.4%	26.8%	31.7%	0.0%	0.0%	0.0%	0.0%	0.0%	39.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	3.0%	24.2%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	39.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L172	Pak Kung Street (EB)	Gillies Avenue North	Ma Tau Wai Road	0:00	1:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	0.0%	38.4%	38.5%	7.7%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	7.7%	0.0%	0.0%	
				8:00	9:00	50	4.5%	50.2%	22.7%	4.5%	9.1%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.5%	0.0%	0.0%
				9:00	10:00	50	5.3%	42.0%	26.3%	5.3%	10.5%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%
				10:00	11:00	50	6.3%	43.5%	18.8%	6.3%	18.8%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	0.0%	43.7%	18.8%	12.5%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	6.3%	56.1%	18.8%	6.3%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	0.0%	43.0%	14.3%	7.1%	21.4%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	7.1%	0.0%	0.0%
				14:00	15:00	50	0.0%	39.9%	20.0%	6.7%	20.0%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.7%	0.0%	0.0%
				15:00	16:00	50	5.3%	41.9%	21.1%	5.3%	15.8%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%	0.0%	0.0%
				16:00	17:00	50	5.9%	52.9%	11.8%	5.9%	17.6%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	4.8%	66.6%	9.5%	4.8%	9.5%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	5.3%	68.3%	10.5%	5.3%	5.3%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	6.3%	68.7%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	8.3%	58.4%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID																								Road Section		From Road		To Road		1-hour interval		Traffic	Vehicle Composition																	
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End		Flows	MC	PC	Taxi	LGV	LGV	LGV	HGV	HGV	PLB	PrLB	PrLB	NFB	NFB	NFB	FBSD	FBDD
Link ID																								Road Section		From Road		To Road		Start		End																		

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L175	Hok Yuen Street (EB)	Gillies Avenue North	Ma Tau Wai Road	0:00	1:00	150	3.2%	30.6%	59.0%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.8%	0.8%	2.4%	0.8%	0.8%	0.0%	0.0%
				1:00	2:00	100	2.8%	31.0%	63.4%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	100	3.0%	28.8%	66.7%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.1%	30.6%	61.2%	0.0%	4.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	2.4%	28.6%	66.6%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	1.9%	24.5%	58.4%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	1.9%	1.9%	3.8%	1.9%	1.9%	0.0%	0.0%
				6:00	7:00	100	2.0%	23.2%	56.8%	0.0%	3.0%	2.0%	0.0%	0.0%	0.0%	2.0%	2.0%	4.0%	3.0%	2.0%	0.0%	0.0%
				7:00	8:00	300	1.9%	34.5%	37.2%	0.4%	4.2%	1.9%	0.4%	0.4%	0.0%	2.7%	2.7%	5.7%	5.3%	2.7%	0.0%	0.0%
				8:00	9:00	450	3.5%	43.6%	24.3%	0.5%	6.1%	5.6%	1.6%	1.6%	0.0%	1.9%	1.9%	4.0%	3.5%	1.9%	0.0%	0.0%
				9:00	10:00	400	2.8%	35.5%	26.5%	0.5%	6.1%	6.3%	1.8%	1.8%	0.0%	3.3%	2.5%	5.3%	5.1%	2.5%	0.0%	0.0%
				10:00	11:00	350	2.8%	31.7%	31.1%	0.9%	11.5%	9.9%	1.9%	2.2%	0.0%	0.9%	1.2%	2.5%	2.2%	1.2%	0.0%	0.0%
				11:00	12:00	300	2.1%	33.2%	28.8%	1.0%	15.4%	4.1%	1.0%	0.7%	0.0%	3.1%	1.7%	3.8%	3.4%	1.7%	0.0%	0.0%
				12:00	13:00	350	2.6%	41.2%	33.4%	0.6%	9.7%	3.6%	1.0%	0.6%	0.0%	2.9%	0.6%	1.6%	1.6%	0.6%	0.0%	0.0%
				13:00	14:00	350	1.9%	27.0%	22.0%	0.9%	11.6%	11.9%	2.5%	2.5%	0.0%	0.9%	3.1%	6.6%	6.0%	3.1%	0.0%	0.0%
				14:00	15:00	300	1.7%	26.3%	31.7%	1.0%	11.3%	9.7%	2.0%	2.0%	0.0%	1.3%	2.0%	4.7%	4.0%	2.3%	0.0%	0.0%
				15:00	16:00	400	2.2%	29.4%	31.3%	0.8%	10.5%	10.0%	2.2%	2.2%	0.0%	2.2%	1.4%	3.3%	2.8%	1.7%	0.0%	0.0%
				16:00	17:00	300	2.6%	38.7%	33.2%	0.7%	10.6%	5.1%	0.7%	0.7%	0.0%	0.4%	1.1%	2.9%	1.8%	1.5%	0.0%	0.0%
				17:00	18:00	400	3.0%	44.2%	31.9%	0.6%	6.4%	7.5%	1.1%	1.1%	0.0%	0.6%	0.6%	1.4%	0.8%	0.8%	0.0%	0.0%
				18:00	19:00	350	3.1%	46.5%	34.2%	0.3%	5.0%	4.7%	0.6%	0.6%	0.0%	0.3%	0.6%	1.9%	1.3%	0.9%	0.0%	0.0%
				19:00	20:00	350	3.0%	49.9%	37.3%	0.0%	1.7%	1.7%	0.3%	0.3%	0.0%	1.0%	0.7%	1.7%	1.7%	0.7%	0.0%	0.0%
				20:00	21:00	250	3.0%	43.4%	47.7%	0.0%	1.7%	0.9%	0.4%	0.4%	0.0%	0.4%	0.4%	0.9%	0.4%	0.4%	0.0%	0.0%
				21:00	22:00	250	2.4%	38.1%	53.7%	0.0%	2.4%	1.4%	0.5%	0.5%	0.0%	0.0%	0.0%	0.5%	0.0%	0.5%	0.0%	0.0%
				22:00	23:00	200	3.2%	31.9%	59.1%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.5%	0.5%	1.6%	0.5%	1.1%	0.0%	0.0%
				23:00	0:00	200	3.9%	30.5%	58.7%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.6%	0.6%	1.9%	0.6%	1.3%	0.0%	0.0%
L176	Hok Yuen Street (WB)	Gillies Avenue North	Hok Yuen Lane	0:00	1:00	100	9.0%	34.6%	47.4%	0.0%	2.6%	0.0%	0.0%	0.0%	3.8%	0.0%	0.0%	1.3%	0.0%	0.0%	0.0%	1.3%
				1:00	2:00	50	9.1%	36.4%	52.2%	0.0%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	7.7%	33.3%	56.4%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	10.0%	36.7%	50.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	7.7%	34.6%	53.9%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	6.9%	31.0%	55.3%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	6.7%	26.7%	48.1%	0.0%	5.0%	0.0%	0.0%	0.0%	5.0%	1.7%	1.7%	1.7%	1.7%	0.0%	0.0%	1.7%
				7:00	8:00	150	4.8%	39.2%	28.0%	1.6%	8.0%	0.8%	0.8%	0.8%	4.8%	1.6%	1.6%	1.6%	1.6%	0.0%	0.0%	4.8%
				8:00	9:00	250	8.3%	46.7%	17.0%	1.8%	10.6%	2.8%	2.3%	2.3%	2.3%	0.9%	0.9%	0.9%	0.9%	0.0%	0.0%	2.3%
				9:00	10:00	200	6.9%	40.8%	19.6%	2.1%	11.6%	3.2%	2.6%	2.6%	2.1%	1.6%	1.1%	1.6%	1.6%	0.0%	0.0%	2.6%
				10:00	11:00	250	6.7%	34.7%	24.5%	1.9%	14.9%	3.8%	2.9%	2.9%	2.4%	0.5%	0.5%	1.4%	1.0%	0.5%	0.0%	1.4%
				11:00	12:00	200	4.6%	35.4%	21.9%	2.6%	19.4%	1.5%	1.0%	1.0%	4.6%	1.5%	1.0%	2.0%	1.5%	0.5%	0.0%	1.5%
				12:00	13:00	250	5.3%	43.2%	25.2%	1.9%	12.1%	1.5%	1.0%	1.0%	3.4%	1.9%	0.5%	1.0%	0.5%	0.0%	0.0%	1.5%
				13:00	14:00	200	4.7%	31.1%	18.1%	2.1%	16.1%	5.2%	3.6%	4.1%	3.1%	0.5%	2.1%	3.6%	2.6%	1.0%	0.0%	2.1%
				14:00	15:00	200	3.8%	30.5%	26.1%	2.2%	15.8%	3.8%	2.7%	2.7%	3.8%	1.1%	1.1%	2.7%	1.6%	0.5%	0.0%	1.6%
				15:00	16:00	250	5.3%	33.5%	25.3%	1.8%	14.2%	4.0%	3.1%	3.1%	2.7%	1.3%	0.9%	1.8%	1.3%	0.4%	0.0%	1.3%
				16:00	17:00	250	6.1%	40.9%	25.1%	0.9%	10.0%	1.7%	0.9%	0.9%	3.9%	0.4%	1.3%	3.5%	2.2%	1.3%	0.0%	0.9%
				17:00	18:00	300	7.0%	47.4%	24.7%	0.7%	6.0%	2.3%	1.7%	1.7%	3.0%	0.7%	0.7%	1.7%	1.0%	0.7%	0.0%	0.7%
				18:00	19:00	300	7.2%	49.7%	26.4%	0.4%	4.9%	1.5%	1.1%	1.1%	1.5%	0.4%	0.8%	2.3%	1.5%	0.8%	0.0%	0.4%
				19:00	20:00	200	6.5%	53.5%	28.5%	0.0%	2.5%	0.5%	0.5%	0.5%	3.5%	0.5%	0.5%	1.0%	0.5%	0.0%	0.0%	1.5%
				20:00	21:00	200	7.0%	45.6%	36.1%	0.0%	1.9%	0.0%	0.6%	0.6%	5.7%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	1.9%
				21:00	22:00	150	5.0%	41.0%	41.0%	0.0%	2.9%	0.7%	0.7%	0.7%	5.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%
				22:00	23:00	150	7.3%	34.7%	45.1%	0.0%	1.6%	0.0%	0.0%	0.0%	6.5%	0.8%	0.0%	0.8%	0.0%	0.0%	0.0%	3.2%
				23:00	0:00	100	8.1%	33.3%	45.5%	0.0%	2.0%	0.0%	0.0%	0.0%	6.1%	1.0%	0.0%	1.0%	0.0%	0.0%	0.0%	3.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L177	Hok Yuen Lane (SB)	Hok Yuen Street	Cui-de-sac	0:00	1:00	50	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	33.3%	66.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	37.5%	56.2%	0.0%	0.0%	0.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	33.3%	58.4%	0.0%	0.0%	0.0%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	50.0%	42.9%	0.0%	0.0%	0.0%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	40.0%	60.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	38.5%	61.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	40.0%	50.0%	0.0%	0.0%	0.0%	0.0%	10.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	33.3%	55.6%	0.0%	0.0%	0.0%	0.0%	11.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	46.1%	46.2%	0.0%	0.0%	0.0%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	40.0%	60.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	45.5%	54.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	37.5%	62.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L178	Hok Yuen Lane (NB)	Cui-de-sac	Hok Yuen Street	0:00	1:00	50	81.8%	9.1%	9.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	71.4%	14.3%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	66.6%	16.7%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	66.6%	16.7%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	75.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	75.0%	0.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	71.4%	14.3%	14.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	20.0%	0.0%	80.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	50.0%	0.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	42.9%	0.0%	57.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	66.7%	13.3%	6.7%	0.0%	13.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	52.0%	16.0%	8.0%	0.0%	24.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	59.3%	18.5%	7.4%	0.0%	14.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	57.1%	14.3%	4.8%	0.0%	23.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	52.6%	15.8%	10.5%	0.0%	21.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	60.7%	14.3%	7.1%	0.0%	17.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	66.7%	17.6%	0.0%	0.0%	15.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	72.8%	18.6%	0.0%	0.0%	8.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	74.1%	19.4%	0.0%	0.0%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	66.7%	22.2%	7.4%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	71.5%	19.0%	9.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	62.4%	18.8%	12.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	76.4%	11.8%	11.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	73.4%	13.3%	13.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L179	Hok Yuen Street (WB)	Hok Yuen Lane	Chatham Road North	0:00	1:00	100	8.5%	34.1%	48.9%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	3.7%	0.0%	0.0%	1.2%	0.0%	0.0%	0.0%	1.2%
				1:00	2:00	50	8.7%	34.8%	54.3%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	7.1%	33.3%	57.2%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	9.7%	35.5%	51.6%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	7.4%	33.3%	55.6%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	6.3%	31.3%	53.0%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	100	6.3%	26.6%	48.1%	0.0%	4.7%	0.0%	0.0%	0.0%	4.7%	1.6%	1.6%	1.6%	1.6%	1.6%	0.0%	1.6%	
				7:00	8:00	150	5.0%	39.4%	30.0%	1.7%	8.3%	0.8%	0.8%	0.8%	5.0%	0.8%	0.8%	0.8%	0.8%	0.0%	0.0%	5.0%	
				8:00	9:00	250	7.7%	46.1%	18.3%	1.9%	12.0%	2.9%	2.4%	1.9%	2.4%	0.5%	0.5%	0.5%	0.5%	0.5%	0.0%	0.0%	2.4%
				9:00	10:00	200	6.2%	40.5%	21.3%	2.2%	13.5%	3.4%	2.8%	2.2%	2.2%	1.1%	0.6%	0.6%	0.6%	0.0%	0.0%	2.8%	
				10:00	11:00	250	7.4%	34.5%	25.3%	1.8%	14.7%	3.7%	2.8%	2.8%	2.3%	0.5%	0.5%	0.9%	0.9%	0.5%	0.0%	1.4%	
				11:00	12:00	250	4.9%	34.8%	22.7%	2.5%	19.7%	1.5%	1.0%	1.0%	4.4%	1.5%	1.0%	1.5%	1.5%	0.5%	0.0%	1.5%	
				12:00	13:00	250	6.0%	42.3%	26.3%	1.8%	12.0%	1.4%	0.9%	0.9%	3.2%	1.8%	0.5%	0.5%	0.5%	0.5%	0.0%	1.4%	
				13:00	14:00	200	5.0%	31.5%	19.0%	2.0%	16.0%	5.0%	3.5%	3.5%	3.0%	0.5%	2.0%	3.0%	2.5%	1.5%	0.0%	2.0%	
				14:00	15:00	200	4.2%	30.3%	27.1%	2.1%	15.6%	3.6%	2.6%	2.6%	3.6%	1.0%	1.0%	2.1%	1.6%	1.0%	0.0%	1.6%	
				15:00	16:00	250	6.0%	32.9%	26.4%	1.7%	14.0%	3.8%	3.0%	2.6%	2.6%	1.3%	0.9%	1.3%	1.3%	0.9%	0.0%	1.3%	
				16:00	17:00	300	7.0%	40.8%	25.3%	0.8%	8.9%	1.6%	0.8%	0.8%	3.5%	0.4%	1.9%	3.1%	2.7%	1.6%	0.0%	0.8%	
				17:00	18:00	350	8.4%	47.2%	24.6%	0.6%	5.4%	2.1%	1.5%	1.5%	2.7%	0.9%	0.9%	1.5%	1.2%	0.9%	0.0%	0.6%	
				18:00	19:00	300	8.4%	49.4%	25.9%	0.3%	4.4%	1.3%	1.0%	1.0%	1.3%	0.7%	1.3%	2.0%	1.7%	1.0%	0.0%	0.3%	
				19:00	20:00	250	6.7%	52.7%	29.5%	0.0%	2.4%	0.5%	0.5%	0.5%	3.3%	0.5%	0.5%	0.5%	0.5%	0.5%	0.0%	1.4%	
				20:00	21:00	200	7.3%	44.8%	37.0%	0.0%	1.8%	0.0%	0.6%	0.6%	5.5%	0.0%	0.0%	0.6%	0.0%	0.0%	0.0%	1.8%	
				21:00	22:00	150	5.4%	40.1%	42.3%	0.0%	2.7%	0.7%	0.7%	0.7%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.0%	
				22:00	23:00	150	7.6%	33.6%	46.5%	0.0%	1.5%	0.0%	0.0%	0.0%	6.1%	0.8%	0.0%	0.8%	0.0%	0.0%	0.0%	3.1%	
				L180	Chatham Road North (SB)	Hok Yuen Street	Wo Chung Street	23:00	0:00	150	8.6%	32.4%	46.5%	0.0%	1.9%	0.0%	0.0%	0.0%	5.7%	1.0%	0.0%	1.0%	0.0%
0:00	1:00	350	7.0%					28.9%	45.0%	0.0%	2.1%	0.0%	0.0%	0.0%	7.6%	0.3%	0.0%	0.3%	0.3%	0.0%	0.0%	8.5%	
1:00	2:00	200	7.6%					32.9%	53.6%	0.0%	3.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.4%	
2:00	3:00	200	7.1%					30.1%	57.6%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.6%	
3:00	4:00	150	9.3%					32.2%	50.9%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.4%	
4:00	5:00	150	7.8%					30.1%	55.3%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.9%	
5:00	6:00	150	7.1%					29.2%	55.7%	0.0%	5.3%	0.0%	0.0%	0.9%	0.0%	0.0%	0.9%	0.9%	0.0%	0.0%	0.0%		
6:00	7:00	300	4.9%					22.4%	43.1%	0.4%	4.6%	0.8%	0.0%	0.4%	8.4%	0.4%	0.4%	0.8%	0.8%	0.4%	0.0%	12.2%	
7:00	8:00	800	3.3%					24.2%	28.8%	0.8%	5.9%	1.3%	0.4%	0.4%	9.3%	0.5%	0.4%	0.7%	0.8%	0.3%	0.0%	22.9%	
8:00	9:00	1,100	6.5%					34.1%	20.8%	1.1%	9.6%	4.5%	1.3%	1.3%	5.0%	0.4%	0.4%	0.5%	0.5%	0.2%	0.0%	13.8%	
9:00	10:00	1,000	5.0%					28.4%	23.1%	1.1%	9.9%	5.3%	1.4%	1.5%	4.8%	0.6%	0.5%	0.8%	0.8%	0.3%	0.0%	16.5%	
10:00	11:00	900	6.0%					29.0%	23.0%	1.4%	13.9%	4.6%	1.8%	1.8%	5.0%	0.2%	0.2%	0.5%	0.5%	0.1%	0.0%	12.0%	
11:00	12:00	900	4.1%					28.4%	19.8%	1.9%	17.7%	1.9%	0.7%	0.6%	9.0%	0.6%	0.3%	0.7%	0.7%	0.2%	0.0%	13.4%	
12:00	13:00	900	5.0%					36.7%	24.2%	1.3%	11.5%	1.6%	0.7%	0.6%	6.6%	0.7%	0.1%	0.2%	0.3%	0.1%	0.0%	10.4%	
13:00	14:00	850	3.8%					25.9%	16.9%	1.6%	14.9%	5.9%	2.3%	2.4%	6.7%	0.2%	0.7%	1.3%	1.3%	0.5%	0.0%	15.6%	
14:00	15:00	850	3.3%					24.7%	23.7%	1.5%	14.2%	4.6%	1.7%	1.7%	7.3%	0.4%	0.5%	0.9%	0.9%	0.4%	0.0%	14.2%	
15:00	16:00	1,000	4.7%					28.1%	23.8%	1.4%	13.3%	4.9%	2.0%	1.9%	5.5%	0.5%	0.3%	0.6%	0.6%	0.2%	0.0%	12.2%	
16:00	17:00	850	6.1%					40.1%	20.6%	0.9%	9.2%	1.0%	0.7%	0.6%	7.5%	0.1%	0.4%	1.0%	0.9%	0.4%	0.0%	10.5%	
17:00	18:00	1,100	7.1%					46.8%	20.2%	0.6%	5.6%	1.5%	1.2%	1.1%	6.1%	0.2%	0.2%	0.5%	0.4%	0.2%	0.0%	8.3%	
18:00	19:00	950	7.5%					51.0%	22.4%	0.3%	4.6%	1.0%	0.8%	0.8%	2.9%	0.1%	0.2%	0.7%	0.6%	0.2%	0.0%	6.9%	
19:00	20:00	900	5.6%					45.1%	26.7%	0.1%	2.1%	0.7%	0.2%	0.2%	7.0%	0.2%	0.1%	0.4%	0.4%	0.1%	0.0%	11.1%	
20:00	21:00	750	5.6%					36.0%	31.7%	0.1%	1.8%	0.3%	0.3%	0.3%	11.0%	0.1%	0.0%	0.1%	0.1%	0.0%	0.0%	12.6%	
21:00	22:00	650	4.2%					31.3%	35.3%	0.2%	2.5%	0.6%	0.3%	0.3%	10.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	14.9%	
22:00	23:00	650	5.6%					24.2%	35.9%	0.2%	1.4%	0.0%	0.0%	0.0%	11.0%	0.2%	0.2%	0.2%	0.2%	0.2%	0.0%	20.7%	
23:00	0:00	550	5.9%	23.3%	35.9%	0.2%	1.8%	0.0%	0.0%	0.0%	11.1%	0.2%	0.2%	0.2%	0.2%	0.2%	0.0%	20.8%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L181	Chatham Road North (SB)	Ha Heung Road	Wo Chung Street	0:00	1:00	750	8.1%	26.0%	60.4%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.3%	0.3%	1.0%	0.4%	0.3%	0.0%	1.5%	
				1:00	2:00	450	7.8%	26.3%	63.8%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	400	6.7%	23.9%	67.9%	0.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	300	9.0%	26.0%	61.9%	0.0%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	300	7.8%	23.8%	65.6%	0.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.4%	0.0%	0.4%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	300	6.8%	22.0%	62.3%	0.0%	3.4%	0.0%	0.0%	0.0%	0.7%	0.0%	0.7%	0.7%	1.7%	1.0%	0.7%	0.0%	0.0%
				6:00	7:00	600	5.9%	20.8%	59.6%	0.2%	3.4%	0.9%	0.0%	0.4%	0.2%	0.9%	0.9%	2.0%	1.8%	0.7%	0.0%	2.3%	
				7:00	8:00	1,500	5.0%	25.4%	51.3%	0.1%	3.6%	1.3%	0.8%	0.7%	0.3%	1.1%	1.0%	2.2%	2.0%	0.8%	0.0%	4.4%	
				8:00	9:00	2,300	9.0%	33.6%	34.7%	0.2%	5.5%	4.1%	2.6%	2.5%	0.1%	0.8%	0.8%	1.6%	1.4%	0.6%	0.0%	2.5%	
				9:00	10:00	2,100	7.0%	27.8%	38.3%	0.2%	5.7%	4.7%	2.8%	2.9%	0.1%	1.3%	1.1%	2.2%	2.1%	0.8%	0.0%	3.0%	
				*10:00	11:00	1,800	7.4%	28.8%	33.3%	0.3%	11.4%	6.0%	3.2%	3.3%	0.1%	0.5%	0.6%	1.2%	1.1%	0.4%	0.0%	2.4%	
				*11:00	12:00	1,600	5.4%	31.1%	32.2%	0.5%	15.9%	2.5%	1.5%	1.2%	0.1%	1.5%	0.9%	1.8%	1.8%	0.6%	0.0%	3.0%	
				*12:00	13:00	1,700	6.2%	37.1%	36.4%	0.3%	9.6%	2.1%	1.4%	1.1%	0.1%	1.5%	0.4%	0.7%	0.8%	0.2%	0.0%	2.1%	
				*13:00	14:00	1,650	4.9%	26.7%	25.8%	0.4%	12.6%	7.8%	4.2%	4.4%	0.1%	0.4%	1.7%	3.3%	3.3%	1.2%	0.0%	3.2%	
				14:00	15:00	1,600	4.1%	25.1%	35.8%	0.4%	11.9%	6.1%	3.2%	3.2%	0.1%	0.8%	1.1%	2.3%	2.2%	0.8%	0.0%	2.9%	
				15:00	16:00	1,950	5.7%	27.4%	34.6%	0.4%	10.7%	6.2%	3.5%	3.4%	0.1%	1.1%	0.8%	1.6%	1.5%	0.6%	0.0%	2.4%	
				16:00	17:00	1,550	6.6%	41.9%	26.5%	0.4%	11.7%	2.5%	1.0%	1.0%	0.0%	0.2%	1.0%	2.0%	2.1%	0.6%	0.0%	2.5%	
				17:00	18:00	2,050	7.5%	47.5%	25.2%	0.2%	7.0%	3.8%	1.9%	1.8%	0.0%	0.5%	0.5%	1.0%	0.9%	0.3%	0.0%	1.9%	
				18:00	19:00	1,850	7.6%	49.5%	26.8%	0.2%	5.5%	2.5%	1.2%	1.2%	0.0%	0.4%	0.7%	1.2%	1.3%	0.4%	0.0%	1.5%	
				19:00	20:00	1,700	7.0%	44.7%	39.6%	0.1%	1.8%	0.9%	0.4%	0.5%	0.1%	0.5%	0.4%	0.8%	0.8%	0.2%	0.0%	2.2%	
				20:00	21:00	1,400	7.2%	37.2%	49.1%	0.1%	1.5%	0.4%	0.4%	0.4%	0.1%	0.2%	0.1%	0.4%	0.2%	0.1%	0.0%	2.6%	
				21:00	22:00	1,250	5.3%	32.6%	54.4%	0.1%	2.1%	0.8%	0.5%	0.5%	0.2%	0.0%	0.1%	0.2%	0.0%	0.1%	0.0%	3.1%	
				22:00	23:00	1,150	7.5%	26.5%	58.0%	0.1%	1.2%	0.0%	0.0%	0.0%	0.2%	0.4%	0.3%	0.6%	0.3%	0.3%	0.0%	4.6%	
L182	Chatham Road North (NB)	Wo Chung Street	Ha Heung Road	23:00	0:00	950	7.9%	25.5%	58.1%	0.1%	1.6%	0.0%	0.0%	0.0%	0.2%	0.4%	0.3%	0.8%	0.3%	0.3%	0.0%	4.5%	
				0:00	1:00	1,050	6.6%	27.9%	59.8%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.4%	0.3%	1.0%	0.5%	0.3%	0.0%	1.0%	
				1:00	2:00	650	6.4%	28.0%	62.8%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	600	5.5%	25.7%	66.7%	0.0%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	450	7.4%	27.9%	60.9%	0.0%	3.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	400	6.2%	25.5%	65.0%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.3%	0.0%	0.3%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	450	5.3%	23.6%	61.3%	0.0%	4.2%	0.0%	0.0%	0.7%	0.0%	0.7%	0.7%	1.6%	1.2%	0.7%	0.0%	0.0%	
				6:00	7:00	850	4.8%	22.3%	59.0%	0.1%	4.4%	0.9%	0.0%	0.4%	0.0%	0.9%	1.0%	2.1%	2.0%	0.9%	0.0%	1.2%	
				7:00	8:00	2,050	2.7%	30.5%	52.0%	0.1%	3.5%	1.0%	0.7%	0.7%	0.1%	0.9%	0.9%	2.0%	1.9%	0.7%	0.0%	2.3%	
				8:00	9:00	3,150	4.9%	40.4%	35.0%	0.2%	5.4%	3.2%	2.4%	2.4%	0.1%	0.7%	0.7%	1.4%	1.4%	0.5%	0.0%	1.3%	
				9:00	10:00	2,850	3.8%	33.7%	38.9%	0.1%	5.7%	3.7%	2.7%	2.8%	0.1%	1.2%	1.0%	2.0%	2.0%	0.7%	0.0%	1.6%	
				10:00	11:00	2,700	5.9%	30.0%	32.1%	0.4%	14.2%	5.7%	3.2%	3.3%	0.0%	0.5%	0.6%	1.2%	1.2%	0.4%	0.0%	1.3%	
				11:00	12:00	2,400	4.2%	32.1%	30.4%	0.5%	19.5%	2.4%	1.4%	1.2%	0.0%	1.5%	0.8%	1.8%	2.0%	0.6%	0.0%	1.6%	
				12:00	13:00	2,550	4.9%	38.8%	34.8%	0.4%	11.9%	2.0%	1.3%	1.1%	0.0%	1.5%	0.4%	0.7%	0.8%	0.3%	0.0%	1.1%	
				13:00	14:00	2,450	3.9%	27.4%	24.5%	0.5%	15.5%	7.4%	4.2%	4.4%	0.0%	0.5%	1.7%	3.4%	3.7%	1.2%	0.0%	1.7%	
				14:00	15:00	2,400	3.3%	26.1%	34.2%	0.4%	14.7%	5.7%	3.2%	3.3%	0.0%	0.8%	1.1%	2.4%	2.4%	0.8%	0.0%	1.6%	
				15:00	16:00	2,950	4.6%	28.5%	33.2%	0.4%	13.4%	5.9%	3.5%	3.5%	0.0%	1.1%	0.8%	1.6%	1.6%	0.6%	0.0%	1.3%	
				16:00	17:00	2,550	7.2%	37.6%	24.3%	0.4%	16.4%	2.8%	1.2%	1.2%	0.0%	0.2%	1.2%	2.5%	2.8%	0.8%	0.0%	1.4%	
				17:00	18:00	3,350	8.4%	43.7%	23.7%	0.3%	10.1%	4.4%	2.1%	2.1%	0.0%	0.6%	0.6%	1.2%	1.2%	0.5%	0.0%	1.1%	
				18:00	19:00	2,950	8.5%	46.2%	25.5%	0.2%	8.0%	2.8%	1.3%	1.4%	0.0%	0.5%	0.8%	1.6%	1.8%	0.5%	0.0%	0.9%	
				19:00	20:00	2,500	5.6%	47.9%	38.7%	0.0%	2.2%	0.9%	0.3%	0.5%	0.0%	0.4%	0.4%	0.8%	0.8%	0.3%	0.0%	1.2%	
				20:00	21:00	2,000	5.9%	39.9%	47.9%	0.1%	2.0%	0.4%	0.4%	0.5%	0.1%	0.2%	0.2%	0.4%	0.3%	0.2%	0.0%	1.5%	
				21:00	22:00	1,800	4.4%	35.0%	53.8%	0.1%	2.7%	0.8%	0.4%	0.6%	0.1%	0.0%	0.1%	0.2%	0.0%	0.1%	0.0%	1.7%	
22:00	23:00	1,650	6.2%	28.9%	58.6%	0.1%	1.6%	0.0%	0.0%	0.0%	0.1%	0.4%	0.3%	0.6%	0.3%	0.3%	0.0%	2.6%					
23:00	0:00	1,350	6.6%	27.9%	58.3%	0.1%	2.0%	0.0%	0.0%	0.0%	0.1%	0.5%	0.4%	0.8%	0.4%	0.4%	0.0%	2.5%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L183	Chatham Road North (SB)	Pak Kung Street	Hok Yeun Street	0:00	1:00	250	6.9%	27.0%	44.3%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	150	7.3%	31.5%	54.8%	0.0%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	
				2:00	3:00	150	6.1%	28.9%	58.9%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.5%
				3:00	4:00	100	8.0%	31.0%	51.8%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.6%
				4:00	5:00	100	7.9%	28.9%	55.3%	0.0%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.3%
				5:00	6:00	100	7.4%	28.4%	58.0%	0.0%	6.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	200	4.5%	21.0%	42.5%	0.5%	4.5%	1.0%	0.0%	0.0%	8.5%	0.0%	0.0%	0.5%	0.5%	0.0%	0.0%	0.0%	16.5%
				7:00	8:00	650	3.0%	21.3%	29.7%	0.6%	5.2%	1.6%	0.3%	0.3%	8.9%	0.3%	0.3%	0.6%	0.6%	0.3%	0.0%	27.0%	
				8:00	9:00	900	6.1%	31.3%	22.0%	0.8%	9.0%	5.4%	1.0%	0.9%	5.0%	0.2%	0.2%	0.5%	0.5%	0.2%	0.0%	16.9%	
				9:00	10:00	850	4.7%	25.9%	24.2%	0.7%	9.3%	6.2%	1.1%	1.0%	4.7%	0.4%	0.2%	0.6%	0.6%	0.4%	0.0%	20.0%	
				10:00	11:00	700	5.5%	27.5%	22.4%	1.0%	14.0%	5.1%	1.6%	1.3%	4.8%	0.1%	0.1%	0.1%	0.3%	0.1%	0.0%	16.1%	
				11:00	12:00	700	3.7%	26.2%	19.4%	1.5%	17.9%	2.0%	0.6%	0.5%	8.9%	0.3%	0.2%	0.3%	0.3%	0.2%	0.0%	18.0%	
				12:00	13:00	650	4.6%	34.5%	23.9%	1.1%	11.7%	1.7%	0.6%	0.5%	6.5%	0.3%	0.0%	0.2%	0.2%	0.0%	0.0%	14.2%	
				13:00	14:00	650	3.5%	24.1%	16.6%	1.3%	15.1%	6.3%	2.1%	1.7%	6.5%	0.2%	0.3%	0.6%	0.6%	0.3%	0.0%	20.8%	
				14:00	15:00	650	3.1%	22.9%	23.0%	1.1%	14.3%	5.0%	1.6%	1.3%	7.1%	0.2%	0.2%	0.5%	0.5%	0.2%	0.0%	19.0%	
				15:00	16:00	750	4.4%	26.1%	23.3%	1.1%	13.4%	5.4%	1.8%	1.5%	5.5%	0.3%	0.1%	0.3%	0.3%	0.1%	0.0%	16.4%	
				16:00	17:00	600	5.6%	39.6%	18.5%	0.9%	10.1%	0.7%	0.7%	0.5%	7.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	15.9%	
				17:00	18:00	750	6.5%	46.5%	18.2%	0.5%	6.3%	1.0%	1.2%	1.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	12.7%	
				18:00	19:00	650	6.9%	51.4%	20.5%	0.3%	5.2%	0.6%	0.8%	0.6%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	10.8%	
				19:00	20:00	650	5.2%	42.2%	26.5%	0.2%	2.2%	0.8%	0.2%	0.2%	6.9%	0.2%	0.0%	0.2%	0.2%	0.0%	0.0%	15.0%	
				20:00	21:00	550	5.2%	33.3%	31.0%	0.2%	1.8%	0.4%	0.2%	0.2%	10.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	17.0%	
				21:00	22:00	500	3.8%	28.8%	34.3%	0.2%	2.4%	0.6%	0.2%	0.2%	9.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	19.6%	
				22:00	23:00	500	4.9%	22.1%	34.2%	0.2%	1.4%	0.0%	0.0%	0.0%	10.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	26.8%	
				L184	Chatham Road North (NB)	Wo Chung Street	Pak Kung Street	23:00	0:00	400	5.1%	21.3%	34.1%	0.3%	1.8%	0.0%	0.0%	0.0%	10.6%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	300	7.5%					19.6%	49.0%	0.0%	3.6%	0.0%	0.0%	0.0%	7.5%	0.7%	0.7%	1.4%	0.7%	0.4%	0.0%	8.9%	
1:00	2:00	150	8.6%					22.9%	60.6%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.9%	
2:00	3:00	150	7.0%					20.9%	65.1%	0.0%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.1%	
3:00	4:00	100	9.2%					22.4%	57.2%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.1%	
4:00	5:00	100	8.0%					20.7%	61.0%	0.0%	4.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%	0.0%	4.6%	
5:00	6:00	150	6.8%					18.4%	57.3%	0.0%	7.8%	0.0%	0.0%	1.0%	0.0%	1.0%	1.9%	2.9%	1.9%	1.0%	0.0%	0.0%	
6:00	7:00	250	4.9%					14.0%	44.2%	0.4%	6.2%	0.8%	0.0%	0.4%	7.8%	1.2%	1.6%	2.9%	2.9%	1.2%	0.0%	11.5%	
7:00	8:00	650	3.1%					18.1%	33.9%	0.3%	3.9%	0.9%	0.6%	0.6%	11.8%	1.3%	1.3%	2.4%	2.5%	0.9%	0.0%	18.4%	
8:00	9:00	900	6.6%					27.2%	25.9%	0.3%	6.8%	3.6%	2.4%	2.3%	6.7%	1.0%	1.0%	1.9%	1.9%	0.7%	0.0%	11.7%	
9:00	10:00	850	5.0%					21.7%	27.9%	0.4%	6.8%	4.1%	2.5%	2.5%	6.2%	1.7%	1.3%	2.5%	2.8%	1.0%	0.0%	13.6%	
10:00	11:00	850	5.7%					18.1%	22.9%	0.7%	19.4%	6.2%	3.0%	2.9%	4.6%	0.7%	0.8%	1.7%	1.6%	0.6%	0.0%	11.1%	
11:00	12:00	850	3.8%					17.5%	19.6%	1.0%	24.4%	2.4%	1.2%	1.0%	8.2%	2.0%	1.1%	2.3%	2.4%	0.7%	0.0%	12.4%	
12:00	13:00	800	5.0%					24.4%	25.7%	0.7%	17.1%	2.3%	1.3%	1.1%	6.5%	2.4%	0.5%	1.1%	1.2%	0.4%	0.0%	10.3%	
13:00	14:00	850	3.3%					14.9%	15.7%	0.8%	19.3%	7.2%	3.6%	3.4%	5.7%	0.6%	2.1%	4.3%	4.4%	1.4%	0.0%	13.3%	
14:00	15:00	800	3.0%					14.7%	22.7%	0.8%	19.0%	5.8%	2.8%	2.7%	6.4%	1.0%	1.5%	3.0%	3.0%	1.0%	0.0%	12.6%	
15:00	16:00	950	4.4%					17.0%	23.1%	0.8%	18.1%	6.4%	3.3%	2.9%	5.0%	1.6%	1.1%	2.2%	2.2%	0.8%	0.0%	11.1%	
16:00	17:00	900	5.5%					21.2%	17.2%	0.8%	22.8%	3.0%	1.1%	0.9%	4.9%	0.4%	1.8%	3.6%	3.7%	1.1%	0.0%	12.0%	
17:00	18:00	1,000	7.2%					27.5%	18.7%	0.6%	15.5%	5.1%	2.1%	1.9%	4.4%	1.0%	1.0%	2.0%	1.8%	0.7%	0.0%	10.5%	
18:00	19:00	850	7.8%					30.6%	21.5%	0.4%	13.1%	3.5%	1.5%	1.5%	2.2%	0.9%	1.5%	2.7%	2.8%	0.9%	0.0%	9.1%	
19:00	20:00	700	6.3%					33.0%	31.3%	0.1%	3.5%	1.0%	0.3%	0.6%	7.5%	0.7%	0.6%	1.3%	1.3%	0.4%	0.0%	12.1%	
20:00	21:00	600	6.1%					25.8%	37.0%	0.2%	2.9%	0.3%	0.3%	0.5%	11.8%	0.3%	0.3%	0.5%	0.3%	0.2%	0.0%	13.5%	
21:00	22:00	550	4.5%					22.3%	40.0%	0.2%	4.0%	0.9%	0.4%	0.6%	10.8%	0.0%	0.2%	0.2%	0.0%	0.2%	0.0%	15.7%	
22:00	23:00	550	5.9%					16.9%	39.8%	0.2%	2.1%	0.0%	0.0%	0.0%	11.2%	0.6%	0.4%	0.8%	0.4%	0.4%	0.0%	21.3%	
23:00	0:00	450	6.0%	16.0%	39.5%	0.2%	2.8%	0.0%	0.0%	0.0%	11.2%	0.7%	0.5%	0.9%	0.5%	0.5%	0.0%	21.2%					

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L185	Shun Yung Street (SB)	Pak Kung Street	Fat Kwong Street	0:00	1:00	250	4.9%	29.1%	58.2%	0.0%	1.9%	0.0%	0.0%	0.0%	2.9%	0.5%	0.5%	1.0%	0.5%	0.0%	0.0%	0.5%	
				1:00	2:00	150	5.1%	29.7%	62.7%	0.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	4.5%	27.3%	66.4%	0.0%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	100	6.2%	29.6%	60.5%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	100	5.6%	26.8%	64.8%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	100	4.8%	25.3%	61.5%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	200	3.7%	23.0%	58.0%	0.0%	4.3%	0.6%	0.0%	0.0%	3.7%	1.2%	1.2%	1.9%	1.2%	0.6%	0.0%	0.6%	0.6%
				7:00	8:00	400	3.0%	35.9%	40.4%	0.3%	4.5%	1.0%	0.5%	0.5%	5.5%	1.3%	1.3%	2.0%	2.0%	0.5%	0.0%	1.3%	1.3%
				8:00	9:00	650	5.5%	46.8%	26.8%	0.3%	6.8%	2.8%	1.4%	1.4%	2.7%	0.9%	0.9%	1.4%	1.4%	0.3%	0.0%	0.6%	0.6%
				9:00	10:00	600	4.5%	39.7%	30.6%	0.4%	7.2%	3.4%	1.6%	1.6%	2.7%	1.6%	1.3%	2.0%	2.2%	0.5%	0.0%	0.7%	0.7%
				10:00	11:00	500	4.9%	33.3%	33.3%	0.6%	14.0%	4.3%	1.6%	1.6%	2.4%	0.6%	0.6%	1.0%	1.0%	0.2%	0.0%	0.6%	0.6%
				11:00	12:00	500	3.2%	34.0%	30.0%	0.6%	18.4%	1.7%	0.6%	0.6%	4.5%	1.7%	0.9%	1.5%	1.5%	0.2%	0.0%	0.6%	0.6%
				12:00	13:00	500	3.8%	40.6%	34.3%	0.4%	11.1%	1.4%	0.6%	0.6%	3.2%	1.6%	0.4%	0.6%	0.6%	0.2%	0.0%	0.6%	0.6%
				13:00	14:00	450	3.2%	31.0%	25.9%	0.7%	15.6%	5.7%	2.0%	2.3%	3.4%	0.5%	2.0%	2.9%	3.2%	0.7%	0.0%	0.9%	0.9%
				14:00	15:00	450	2.7%	28.7%	35.1%	0.7%	14.4%	4.3%	1.6%	1.6%	3.6%	0.9%	1.4%	1.8%	2.0%	0.5%	0.0%	0.7%	0.7%
				15:00	16:00	550	3.7%	31.7%	34.3%	0.6%	13.2%	4.5%	1.7%	1.7%	2.8%	1.3%	0.9%	1.3%	1.3%	0.4%	0.0%	0.6%	0.6%
				16:00	17:00	500	4.4%	37.9%	32.8%	0.4%	13.6%	1.8%	0.4%	0.4%	3.3%	0.2%	1.1%	1.3%	1.5%	0.2%	0.0%	0.7%	0.7%
				17:00	18:00	600	5.1%	44.3%	32.1%	0.3%	8.3%	2.7%	0.7%	0.7%	2.7%	0.5%	0.5%	0.7%	0.7%	0.2%	0.0%	0.5%	0.5%
				18:00	19:00	550	5.2%	46.4%	34.4%	0.2%	6.5%	1.7%	0.4%	0.4%	1.2%	0.4%	0.8%	0.8%	1.0%	0.2%	0.0%	0.4%	0.4%
				19:00	20:00	500	4.2%	49.4%	37.4%	0.0%	2.0%	0.6%	0.2%	0.2%	3.2%	0.4%	0.4%	0.6%	0.6%	0.2%	0.0%	0.6%	0.6%
				20:00	21:00	450	4.5%	40.3%	46.0%	0.0%	1.7%	0.2%	0.2%	0.2%	5.4%	0.2%	0.2%	0.2%	0.2%	0.0%	0.0%	0.7%	0.7%
				21:00	22:00	400	3.3%	35.7%	51.5%	0.0%	2.5%	0.6%	0.3%	0.3%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.8%	0.8%
				22:00	23:00	350	4.6%	29.6%	55.8%	0.0%	1.5%	0.0%	0.0%	0.0%	5.8%	0.3%	0.3%	0.6%	0.3%	0.0%	0.0%	1.2%	1.2%
				23:00	0:00	300	4.9%	28.6%	55.5%	0.0%	1.9%	0.0%	0.0%	0.0%	6.0%	0.4%	0.4%	0.8%	0.4%	0.0%	0.0%	1.1%	1.1%
L186	Shun Yung Street (NB)	Fat Kwong Street	Pak Kung Street	0:00	1:00	250	2.8%	23.9%	65.9%	0.0%	1.4%	0.0%	0.0%	0.0%	4.1%	0.0%	0.0%	0.9%	0.5%	0.5%	0.0%	0.0%	
				1:00	2:00	150	2.4%	24.8%	71.2%	0.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	150	2.5%	21.8%	74.0%	0.0%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	100	3.5%	24.7%	69.4%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	100	2.6%	22.4%	73.7%	0.0%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	100	2.3%	20.7%	70.3%	0.0%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	1.1%	1.1%	0.0%	0.0%	
				6:00	7:00	200	1.8%	19.3%	65.3%	0.0%	2.9%	0.6%	0.0%	0.0%	4.7%	0.6%	0.6%	1.8%	1.2%	1.2%	0.0%	0.0%	
				7:00	8:00	550	0.4%	24.0%	60.8%	0.4%	3.1%	0.6%	0.4%	0.4%	4.6%	0.7%	0.6%	1.8%	1.5%	0.7%	0.0%	0.0%	
				8:00	9:00	750	0.9%	35.8%	46.5%	0.4%	5.3%	1.9%	1.2%	1.1%	2.7%	0.5%	0.5%	1.5%	1.2%	0.5%	0.0%	0.0%	
				9:00	10:00	700	0.7%	29.4%	50.8%	0.4%	5.5%	2.2%	1.3%	1.2%	2.5%	0.9%	0.7%	2.0%	1.7%	0.7%	0.0%	0.0%	
				10:00	11:00	500	2.8%	30.5%	42.2%	0.9%	11.1%	3.6%	1.5%	1.3%	3.4%	0.2%	0.2%	1.1%	0.6%	0.6%	0.0%	0.0%	
				11:00	12:00	450	2.0%	30.8%	37.7%	1.1%	14.5%	1.6%	0.7%	0.5%	6.3%	0.7%	0.5%	1.6%	1.1%	0.9%	0.0%	0.0%	
				12:00	13:00	500	2.3%	36.6%	42.6%	0.8%	8.7%	1.2%	0.6%	0.4%	4.4%	0.8%	0.2%	0.6%	0.4%	0.4%	0.0%	0.0%	
				13:00	14:00	450	1.9%	28.9%	33.4%	1.0%	12.5%	5.1%	1.9%	1.9%	4.8%	0.2%	1.0%	3.1%	2.4%	1.9%	0.0%	0.0%	
				14:00	15:00	450	1.6%	25.9%	43.5%	0.9%	11.2%	3.7%	1.4%	1.2%	5.1%	0.5%	0.5%	1.9%	1.4%	1.2%	0.0%	0.0%	
				15:00	16:00	550	2.1%	28.7%	43.1%	0.8%	10.3%	3.9%	1.6%	1.4%	3.9%	0.6%	0.4%	1.4%	1.0%	0.8%	0.0%	0.0%	
				16:00	17:00	350	6.4%	46.1%	21.8%	1.0%	11.9%	2.2%	0.3%	0.3%	7.4%	0.0%	0.0%	1.0%	0.0%	1.6%	0.0%	0.0%	
				17:00	18:00	450	7.2%	52.5%	20.7%	0.7%	7.0%	3.4%	0.7%	0.5%	5.8%	0.0%	0.0%	0.5%	0.0%	1.0%	0.0%	0.0%	
				18:00	19:00	400	7.5%	56.1%	22.6%	0.6%	5.6%	2.2%	0.6%	0.3%	2.8%	0.0%	0.0%	0.6%	0.0%	1.1%	0.0%	0.0%	
				19:00	20:00	500	2.4%	43.4%	45.5%	0.0%	1.6%	0.4%	0.2%	0.2%	4.5%	0.2%	0.2%	0.6%	0.4%	0.4%	0.0%	0.0%	
				20:00	21:00	450	2.4%	34.3%	53.9%	0.0%	1.2%	0.2%	0.2%	0.2%	7.0%	0.0%	0.0%	0.2%	0.2%	0.2%	0.0%	0.0%	
				21:00	22:00	400	1.9%	29.7%	59.0%	0.0%	1.9%	0.5%	0.3%	0.3%	6.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	350	2.6%	24.2%	63.3%	0.0%	1.2%	0.0%	0.0%	0.0%	7.2%	0.3%	0.0%	0.6%	0.3%	0.3%	0.0%	0.0%	
				23:00	0:00	300	2.5%	23.6%	63.5%	0.0%	1.4%	0.0%	0.0%	0.0%	7.1%	0.4%	0.0%	0.7%	0.4%	0.4%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L187	Sheung Lok Street (SB)	Sheung Wo Street	Fat Kwong Street	0:00	1:00	50	3.0%	51.5%	36.4%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	5.3%	52.6%	36.8%	0.0%	5.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	5.9%	47.0%	41.2%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	7.1%	50.1%	35.7%	0.0%	7.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	50.0%	41.7%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	41.7%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	3.4%	38.2%	31.0%	0.0%	6.9%	0.0%	0.0%	0.0%	6.9%	3.4%	3.4%	3.4%	3.4%	0.0%	0.0%	0.0%
				7:00	8:00	100	1.1%	46.1%	22.0%	0.0%	7.7%	2.2%	0.0%	0.0%	8.8%	2.2%	2.2%	3.3%	3.3%	1.1%	0.0%	0.0%
				8:00	9:00	200	2.0%	56.1%	13.7%	0.0%	10.5%	5.2%	1.3%	0.0%	3.9%	1.3%	1.3%	2.0%	2.0%	0.7%	0.0%	0.0%
				9:00	10:00	150	1.5%	49.2%	15.9%	0.0%	11.4%	6.1%	1.5%	0.0%	3.8%	2.3%	1.5%	3.0%	3.0%	0.8%	0.0%	0.0%
				10:00	11:00	150	2.9%	44.7%	15.5%	0.0%	18.4%	7.8%	1.9%	0.0%	3.9%	1.0%	1.0%	1.0%	1.9%	0.0%	0.0%	0.0%
				11:00	12:00	150	1.9%	42.8%	12.6%	0.0%	23.3%	2.9%	1.0%	0.0%	7.8%	2.9%	1.0%	1.9%	1.9%	0.0%	0.0%	0.0%
				12:00	13:00	150	1.9%	52.8%	15.7%	0.0%	14.8%	2.8%	0.9%	0.0%	5.6%	2.8%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%
				13:00	14:00	150	1.9%	37.9%	10.7%	0.0%	19.4%	9.7%	1.9%	0.0%	5.8%	1.0%	2.9%	3.9%	3.9%	1.0%	0.0%	0.0%
				14:00	15:00	100	1.1%	37.7%	15.8%	0.0%	18.9%	7.4%	2.1%	0.0%	6.3%	1.1%	2.1%	3.2%	3.2%	1.1%	0.0%	0.0%
				15:00	16:00	150	1.8%	42.3%	15.9%	0.0%	17.7%	8.0%	1.8%	0.0%	4.4%	1.8%	1.8%	1.8%	1.8%	0.9%	0.0%	0.0%
				16:00	17:00	100	3.3%	53.5%	12.2%	0.0%	14.4%	3.3%	0.0%	0.0%	6.7%	0.0%	2.2%	2.2%	2.2%	0.0%	0.0%	0.0%
				17:00	18:00	150	4.3%	61.3%	12.0%	0.0%	8.5%	4.3%	0.9%	0.0%	5.1%	0.9%	0.9%	0.9%	0.9%	0.0%	0.0%	0.0%
				18:00	19:00	150	4.0%	66.2%	12.9%	0.0%	6.9%	3.0%	1.0%	0.0%	2.0%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				19:00	20:00	150	2.9%	66.1%	17.3%	0.0%	2.9%	1.0%	0.0%	0.0%	5.8%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	2.6%	60.6%	23.7%	0.0%	2.6%	0.0%	0.0%	0.0%	10.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	1.5%	54.6%	27.3%	0.0%	4.5%	1.5%	0.0%	0.0%	10.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.6%	49.2%	32.7%	0.0%	1.8%	0.0%	0.0%	0.0%	12.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L188	Sheung Lok Street (NB)	Fat Kwong Street	Sheung Wo Street	23:00	0:00	50	4.4%	46.8%	33.3%	0.0%	2.2%	0.0%	0.0%	0.0%	13.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				0:00	1:00	50	0.0%	54.8%	32.3%	0.0%	3.2%	0.0%	0.0%	0.0%	6.5%	0.0%	0.0%	3.2%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	63.6%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	54.5%	36.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	40.8%	29.6%	0.0%	3.7%	3.7%	0.0%	0.0%	7.4%	3.7%	3.7%	3.7%	3.7%	0.0%	0.0%	
				7:00	8:00	100	0.0%	48.2%	23.7%	0.0%	3.2%	2.2%	0.0%	0.0%	8.6%	2.2%	2.2%	5.4%	3.2%	1.1%	0.0%	0.0%
				8:00	9:00	200	0.0%	60.7%	15.0%	0.0%	4.6%	5.9%	1.3%	0.0%	3.9%	1.3%	1.3%	3.3%	2.0%	0.7%	0.0%	0.0%
				9:00	10:00	150	0.0%	53.1%	17.4%	0.0%	5.3%	6.8%	1.5%	0.0%	3.8%	2.3%	1.5%	4.5%	3.0%	0.8%	0.0%	0.0%
				10:00	11:00	100	0.0%	50.0%	14.9%	0.0%	10.6%	11.7%	2.1%	0.0%	4.3%	1.1%	1.1%	2.1%	2.1%	0.0%	0.0%	0.0%
				11:00	12:00	100	0.0%	49.4%	13.2%	0.0%	13.2%	4.4%	1.1%	0.0%	8.8%	3.3%	1.1%	3.3%	2.2%	0.0%	0.0%	0.0%
				12:00	13:00	100	0.0%	59.2%	15.3%	0.0%	8.2%	4.1%	1.0%	0.0%	6.1%	3.1%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	0.0%	40.7%	10.4%	0.0%	10.4%	13.5%	3.1%	0.0%	6.3%	1.0%	3.1%	6.3%	4.2%	1.0%	0.0%	0.0%
				14:00	15:00	100	0.0%	42.1%	14.8%	0.0%	10.2%	11.4%	2.3%	0.0%	6.8%	1.1%	2.3%	4.5%	3.4%	1.1%	0.0%	0.0%
				15:00	16:00	150	0.0%	46.3%	15.1%	0.0%	9.4%	12.3%	2.8%	0.0%	4.7%	1.9%	1.9%	2.8%	1.9%	0.9%	0.0%	0.0%
				16:00	17:00	100	0.0%	58.0%	7.9%	0.0%	10.5%	6.6%	1.3%	0.0%	7.9%	0.0%	2.6%	2.6%	2.6%	0.0%	0.0%	0.0%
				17:00	18:00	150	0.0%	65.6%	7.8%	0.0%	5.9%	8.8%	2.0%	0.0%	5.9%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	0.0%	71.9%	9.3%	0.0%	4.7%	5.8%	1.2%	0.0%	2.3%	1.2%	1.2%	1.2%	1.2%	0.0%	0.0%	0.0%
				19:00	20:00	100	0.0%	70.7%	16.2%	0.0%	1.0%	2.0%	0.0%	0.0%	6.1%	1.0%	1.0%	1.0%	1.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	0.0%	63.4%	21.6%	0.0%	1.4%	1.4%	0.0%	0.0%	10.8%	0.0%	0.0%	1.4%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	100	0.0%	59.7%	25.8%	0.0%	1.6%	1.6%	0.0%	0.0%	11.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	100	0.0%	52.8%	30.2%	0.0%	1.9%	0.0%	0.0%	0.0%	13.2%	0.0%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%				
23:00	0:00	50	0.0%	49.9%	31.0%	0.0%	2.4%	0.0%	0.0%	0.0%	14.3%	0.0%	0.0%	2.4%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L189	Kiang Su Street (EB)	Maidstone Road	Ma Tau Wai Road	0:00	1:00	50	10.0%	70.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	16.7%	66.6%	16.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	20.0%	60.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	20.0%	60.0%	20.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	14.3%	57.1%	28.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	10.0%	65.0%	20.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	12.5%	65.0%	10.0%	0.0%	7.5%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	12.1%	60.6%	12.1%	0.0%	9.1%	6.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	10.0%	56.6%	10.0%	0.0%	16.7%	6.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	6.9%	58.7%	10.3%	0.0%	20.7%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	9.4%	65.6%	9.4%	0.0%	12.5%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	7.7%	57.7%	7.7%	0.0%	19.2%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	8.0%	52.0%	12.0%	0.0%	20.0%	8.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	9.4%	56.2%	12.5%	0.0%	15.6%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	9.1%	66.6%	6.1%	0.0%	15.2%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	8.9%	73.3%	6.7%	0.0%	8.9%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	9.5%	73.9%	7.1%	0.0%	7.1%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	8.8%	76.5%	11.8%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	8.3%	70.8%	16.7%	0.0%	4.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	9.5%	66.7%	19.0%	0.0%	4.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	12.5%	62.5%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	15.4%	61.5%	23.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L190	Kiang Su Street (WB)	Ma Tau Wai Road	Maidstone Road	0:00	1:00	50	9.1%	63.6%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	75.0%	25.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	50.0%	37.5%	0.0%	12.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	55.6%	29.6%	7.4%	3.7%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	50	0.0%	64.4%	16.7%	6.3%	6.3%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	50	0.0%	57.5%	20.0%	7.5%	7.5%	7.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	50	2.7%	51.4%	13.5%	5.4%	18.9%	5.4%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	50	2.9%	51.3%	11.4%	8.6%	22.9%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	50	2.7%	62.2%	13.5%	5.4%	13.5%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	50	3.0%	48.5%	9.1%	6.1%	21.2%	9.1%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	50	3.1%	46.8%	15.6%	6.3%	18.8%	6.3%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	50	2.5%	50.0%	15.0%	5.0%	17.5%	7.5%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	50	8.3%	61.1%	5.6%	0.0%	22.2%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	50	8.3%	68.7%	6.3%	0.0%	12.5%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	50	9.1%	70.4%	6.8%	0.0%	9.1%	2.3%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	50	2.8%	77.7%	16.7%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	3.7%	70.4%	22.2%	0.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	4.3%	65.3%	26.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	5.6%	61.1%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	7.1%	57.2%	35.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L191	Access Road (NB)	Hung Fook Street	Ngan Hon Street	0:00	1:00	50	0.0%	64.0%	32.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	64.3%	35.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	61.5%	38.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	62.5%	37.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	56.2%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	69.5%	22.4%	2.0%	4.1%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	100	0.0%	71.6%	12.2%	2.0%	6.1%	7.1%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	100	0.0%	65.5%	14.8%	2.5%	7.4%	8.6%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	100	0.0%	56.1%	15.1%	4.1%	12.3%	11.0%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	100	0.0%	59.7%	13.4%	6.0%	16.4%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	100	0.0%	68.0%	14.7%	4.0%	9.3%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	100	0.0%	52.3%	11.9%	6.0%	13.4%	14.9%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	100	0.0%	52.5%	16.4%	4.9%	13.1%	11.5%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	100	0.0%	55.2%	15.4%	5.1%	11.5%	11.5%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	100	0.0%	67.1%	12.9%	4.3%	11.4%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	100	0.0%	71.7%	12.1%	3.0%	6.1%	6.1%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	100	0.0%	76.0%	12.6%	2.3%	4.6%	3.4%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	100	0.0%	81.6%	15.8%	0.0%	1.3%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	100	0.0%	75.9%	22.2%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	70.3%	25.5%	0.0%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	64.9%	32.4%	0.0%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	0.0%	63.4%	33.3%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
L192	Access Road (NB)	Kai Ming Street	Hung Fook Street	0:00	1:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	58.3%	41.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	54.5%	45.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	57.1%	42.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	43.6%	43.8%	6.3%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	50	0.0%	60.0%	27.5%	5.0%	5.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	100	0.0%	67.6%	16.2%	5.4%	5.4%	5.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	0.0%	61.1%	19.4%	6.5%	6.5%	6.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	0.0%	51.5%	18.8%	7.8%	10.9%	9.4%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	100	0.0%	53.5%	17.2%	12.1%	13.8%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	0.0%	62.5%	18.8%	7.8%	7.8%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	0.0%	48.2%	14.3%	10.7%	12.5%	12.5%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	100	0.0%	47.2%	20.8%	9.4%	11.3%	9.4%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	0.0%	50.7%	19.4%	9.0%	10.4%	9.0%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	100	0.0%	61.6%	16.9%	7.7%	9.2%	4.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	100	0.0%	67.7%	15.6%	4.4%	5.6%	5.6%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	100	0.0%	72.1%	16.5%	2.5%	3.8%	3.8%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	0.0%	75.5%	20.0%	1.5%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	50	0.0%	68.1%	27.7%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	50	0.0%	61.8%	31.0%	2.4%	2.4%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	57.6%	39.4%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	0.0%	55.6%	40.7%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L193	Yuk Shing Street (NB)	Kai Ming Street	Hung Fook Street	0:00	1:00	50	0.0%	56.2%	37.5%	0.0%	6.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	55.6%	44.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	0.0%	60.0%	40.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	0.0%	50.0%	50.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	0.0%	50.0%	41.7%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	50	0.0%	59.4%	28.1%	0.0%	3.1%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	6.3%	0.0%	0.0%
				8:00	9:00	100	0.0%	64.6%	14.5%	0.0%	4.8%	8.1%	3.2%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	3.2%	0.0%	0.0%
				9:00	10:00	100	0.0%	56.5%	17.0%	0.0%	5.7%	9.4%	3.8%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	5.7%	0.0%	0.0%
				10:00	11:00	100	0.0%	50.1%	15.4%	0.0%	17.3%	9.6%	3.8%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%	0.0%	0.0%
				*1:00	12:00	50	0.0%	52.2%	15.2%	0.0%	23.9%	4.3%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.2%	0.0%	0.0%
				*2:00	13:00	50	0.0%	63.3%	16.3%	0.0%	14.3%	4.1%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*3:00	14:00	50	0.0%	44.6%	12.8%	0.0%	19.1%	12.8%	4.3%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	4.3%	0.0%	0.0%
				*4:00	15:00	50	0.0%	45.4%	18.2%	0.0%	18.2%	9.1%	4.5%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	2.3%	0.0%	0.0%
				*5:00	16:00	100	0.0%	49.1%	16.4%	0.0%	16.4%	10.9%	3.6%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.8%	0.0%	0.0%
				*6:00	17:00	100	0.0%	60.8%	13.7%	0.0%	21.6%	3.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*7:00	18:00	100	0.0%	68.3%	13.0%	0.0%	13.0%	4.3%	1.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*8:00	19:00	100	0.0%	72.2%	13.1%	0.0%	9.8%	3.3%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				*9:00	20:00	50	0.0%	77.6%	18.4%	0.0%	2.0%	2.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	50	0.0%	71.4%	25.7%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	50	0.0%	66.7%	30.0%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	50	0.0%	60.0%	36.0%	0.0%	4.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	50	0.0%	60.0%	35.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L194	Access Road (NB)	Wing Kwong Street	Kai Ming Street	0:00	1:00	50	0.0%	67.8%	28.6%	0.0%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				1:00	2:00	50	0.0%	64.7%	29.4%	0.0%	5.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				2:00	3:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				3:00	4:00	50	0.0%	72.7%	27.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				4:00	5:00	50	0.0%	66.7%	33.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				5:00	6:00	50	0.0%	58.4%	33.3%	0.0%	8.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				6:00	7:00	50	0.0%	60.0%	30.0%	0.0%	5.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				7:00	8:00	100	0.0%	71.3%	17.9%	1.8%	5.4%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				8:00	9:00	150	0.0%	74.1%	9.8%	0.9%	6.3%	7.1%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				9:00	10:00	100	0.0%	68.4%	12.0%	1.1%	7.6%	8.7%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				10:00	11:00	100	0.0%	58.4%	12.4%	2.2%	12.4%	12.4%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				11:00	12:00	100	0.0%	60.9%	12.2%	3.7%	17.1%	4.9%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				12:00	13:00	100	0.0%	69.6%	13.0%	2.2%	9.8%	4.3%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				13:00	14:00	100	0.0%	54.3%	9.9%	2.5%	13.6%	16.0%	3.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				14:00	15:00	100	0.0%	53.3%	14.7%	2.7%	13.3%	13.3%	2.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				15:00	16:00	100	0.0%	56.3%	13.5%	2.1%	12.5%	12.5%	3.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				16:00	17:00	100	0.0%	68.1%	12.1%	2.2%	11.0%	5.5%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				17:00	18:00	150	0.0%	72.7%	10.9%	1.6%	6.2%	7.0%	1.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				18:00	19:00	150	0.0%	77.0%	11.5%	0.9%	5.3%	4.4%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				19:00	20:00	100	0.0%	82.0%	13.8%	0.0%	2.1%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				20:00	21:00	100	0.0%	77.6%	19.4%	0.0%	1.5%	1.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				21:00	22:00	100	0.0%	73.2%	23.2%	0.0%	1.8%	1.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				22:00	23:00	50	0.0%	68.9%	28.9%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
				23:00	0:00	50	0.0%	66.6%	30.6%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

TABLE A – TRAFFIC FLOW AND VEHICLE COMPOSITION

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Traffic Flows (veh/hr)	Vehicle Composition															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L195	Wing Kwong Street (WB)	Kai Ming Street	Wan On Street	0:00	1:00	100	10.4%	55.3%	31.3%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	9.8%	53.6%	31.7%	0.0%	4.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	8.6%	51.4%	37.1%	0.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	10.3%	51.8%	31.0%	0.0%	6.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	8.7%	52.2%	34.8%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	7.7%	50.0%	34.6%	0.0%	7.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	8.3%	47.9%	33.3%	2.1%	6.3%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	150	5.5%	57.5%	23.3%	3.4%	6.8%	2.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.4%	0.0%	0.0%
				8:00	9:00	300	8.5%	61.1%	12.7%	3.5%	8.1%	4.6%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.7%	0.0%	0.0%
				9:00	10:00	250	7.3%	55.8%	15.5%	3.9%	9.4%	6.0%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	1.3%	0.0%	0.0%
				10:00	11:00	250	7.7%	48.2%	13.9%	5.3%	16.7%	6.7%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				11:00	12:00	200	5.1%	49.2%	12.8%	7.7%	22.1%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				12:00	13:00	250	6.2%	59.0%	14.3%	4.8%	13.3%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	200	4.9%	46.3%	11.0%	6.6%	19.2%	9.3%	0.5%	1.1%	0.0%	0.0%	0.0%	0.0%	0.0%	1.1%	0.0%	0.0%
				14:00	15:00	200	4.6%	44.7%	16.1%	6.3%	19.0%	7.5%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.6%	0.0%	0.0%
				15:00	16:00	250	6.0%	48.1%	15.1%	5.5%	16.5%	7.3%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.5%	0.0%	0.0%
				16:00	17:00	200	6.8%	59.0%	11.6%	4.2%	15.8%	2.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	300	7.3%	65.4%	10.8%	2.7%	9.2%	3.8%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	250	7.4%	68.7%	11.4%	1.7%	7.4%	2.6%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	250	6.8%	73.2%	16.1%	0.5%	2.4%	1.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	200	7.9%	66.2%	21.9%	0.7%	2.6%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	6.3%	62.2%	26.0%	0.8%	3.9%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	150	9.4%	56.7%	31.1%	0.9%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	100	9.5%	54.7%	31.0%	1.2%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
L196	Wing Kwong Street (WB)	Wan On Street	Ma Tau Wai Road	0:00	1:00	100	3.7%	55.5%	38.9%	0.0%	1.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				1:00	2:00	50	3.0%	54.6%	39.4%	0.0%	3.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				2:00	3:00	50	3.3%	50.1%	43.3%	0.0%	3.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				3:00	4:00	50	4.3%	52.3%	39.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				4:00	5:00	50	5.0%	50.0%	40.0%	0.0%	5.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				5:00	6:00	50	4.5%	50.1%	40.9%	0.0%	4.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				6:00	7:00	50	2.5%	47.5%	40.0%	2.5%	5.0%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				7:00	8:00	150	1.6%	58.6%	27.8%	4.8%	4.8%	2.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				8:00	9:00	250	2.1%	64.6%	15.6%	5.1%	6.3%	5.5%	0.4%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				9:00	10:00	200	2.0%	58.4%	18.8%	5.6%	7.1%	7.1%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				10:00	11:00	200	3.0%	49.0%	17.2%	7.7%	13.6%	8.3%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				11:00	12:00	200	1.9%	50.5%	15.2%	10.8%	18.4%	3.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				12:00	13:00	200	2.3%	59.6%	17.3%	6.9%	11.0%	2.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				13:00	14:00	150	2.0%	46.7%	13.3%	10.0%	15.3%	10.7%	0.7%	1.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				14:00	15:00	150	2.1%	45.1%	18.8%	9.0%	15.3%	8.3%	0.7%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				15:00	16:00	200	2.2%	48.3%	17.8%	8.3%	13.3%	8.9%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				16:00	17:00	150	4.1%	59.2%	13.6%	6.1%	13.6%	3.4%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				17:00	18:00	250	4.5%	65.1%	12.9%	4.0%	8.0%	4.5%	0.5%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				18:00	19:00	200	4.5%	68.4%	14.1%	2.8%	6.2%	2.8%	0.6%	0.6%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				19:00	20:00	200	3.0%	74.2%	19.2%	0.6%	1.8%	1.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				20:00	21:00	150	3.3%	67.5%	26.0%	0.8%	1.6%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				21:00	22:00	150	2.8%	62.4%	30.2%	0.9%	2.8%	0.9%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				22:00	23:00	100	3.4%	56.4%	36.8%	1.1%	2.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
				23:00	0:00	100	4.2%	55.0%	36.6%	1.4%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L001	Long Yuet Street (SB)	San Ma Tau Street	Kwei Chow Street	0:00	1:00	7.70	0.23	2.23	4.62	0.00	0.23	0.00	0.00	0.00	0.08	0.08	0.08	0.08	0.08	0.00	0.00	0.00	
				1:00	2:00	3.85	0.08	1.23	2.46	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.85	0.08	1.08	2.62	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.85	0.15	1.23	2.31	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.85	0.15	1.00	2.54	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.85	0.15	1.00	2.08	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.15	0.15	0.00	0.00	0.00
				6:00	7:00	7.70	0.15	1.69	4.39	0.00	0.39	0.00	0.00	0.00	0.15	0.15	0.15	0.23	0.23	0.15	0.00	0.00	0.00
				7:00	8:00	11.55	0.15	3.31	5.47	0.08	0.62	0.15	0.00	0.00	0.00	0.31	0.23	0.54	0.46	0.23	0.00	0.00	0.00
				8:00	9:00	19.25	0.54	7.47	6.24	0.08	1.62	0.85	0.15	0.15	0.00	0.31	0.31	0.69	0.62	0.23	0.00	0.00	0.00
				9:00	10:00	19.25	0.39	6.01	6.70	0.08	1.62	1.00	0.15	0.15	0.00	0.54	0.46	0.92	0.85	0.39	0.00	0.00	0.00
				*0:00	11:00	15.40	0.54	5.01	5.16	0.08	2.39	0.77	0.15	0.15	0.15	0.15	0.15	0.31	0.31	0.08	0.00	0.00	0.00
				*1:00	12:00	15.40	0.31	4.93	4.31	0.15	3.00	0.31	0.08	0.08	0.23	0.54	0.23	0.54	0.54	0.15	0.00	0.00	0.00
				*2:00	13:00	15.40	0.39	6.08	5.08	0.08	1.93	0.23	0.08	0.08	0.15	0.62	0.15	0.23	0.23	0.08	0.00	0.00	0.00
				*3:00	14:00	15.40	0.23	4.47	3.54	0.08	2.54	0.92	0.23	0.15	0.15	0.15	0.62	1.00	1.00	0.31	0.00	0.00	0.00
				14:00	15:00	15.40	0.23	4.08	5.31	0.08	2.31	0.69	0.15	0.15	0.15	0.23	0.39	0.69	0.69	0.23	0.00	0.00	0.00
				15:00	16:00	19.25	0.46	5.78	6.01	0.08	2.77	1.08	0.31	0.23	0.23	0.46	0.39	0.69	0.54	0.23	0.00	0.00	0.00
				16:00	17:00	15.40	0.62	6.39	3.70	0.08	2.08	0.23	0.08	0.00	0.46	0.08	0.46	0.54	0.62	0.08	0.00	0.00	0.00
				17:00	18:00	19.25	0.85	9.55	4.70	0.08	1.62	0.46	0.15	0.08	0.46	0.23	0.23	0.39	0.39	0.08	0.00	0.00	0.00
				18:00	19:00	19.25	0.85	9.93	4.93	0.08	1.23	0.31	0.08	0.08	0.23	0.23	0.39	0.39	0.46	0.08	0.00	0.00	0.00
				19:00	20:00	15.40	0.46	7.70	5.78	0.00	0.39	0.08	0.00	0.00	0.15	0.15	0.15	0.23	0.23	0.08	0.00	0.00	0.00
				20:00	21:00	11.55	0.31	4.93	5.54	0.00	0.23	0.00	0.00	0.00	0.23	0.08	0.08	0.08	0.08	0.00	0.00	0.00	0.00
				21:00	22:00	11.55	0.31	4.31	6.16	0.00	0.39	0.08	0.00	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	11.55	0.39	3.54	6.70	0.00	0.23	0.00	0.00	0.00	0.31	0.08	0.08	0.08	0.08	0.08	0.00	0.00	0.00
L002	Wai King Street (SB)	San Ma Tau Street	Kwei Chow Street	23:00	0:00	7.70	0.23	2.31	4.39	0.00	0.15	0.00	0.00	0.00	0.23	0.08	0.08	0.08	0.08	0.08	0.00	0.00	
				0:00	1:00	8.60	0.09	1.03	6.11	0.00	0.17	0.00	0.00	0.00	0.77	0.09	0.09	0.17	0.09	0.00	0.00	0.00	
				1:00	2:00	8.60	0.17	1.20	6.88	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.30	0.09	0.52	3.61	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.30	0.09	0.60	3.35	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.30	0.17	0.52	3.44	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.30	0.09	0.43	3.18	0.00	0.26	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.00
				6:00	7:00	8.60	0.09	0.69	5.59	0.00	0.43	0.00	0.00	0.00	0.69	0.17	0.17	0.34	0.34	0.09	0.00	0.00	0.00
				7:00	8:00	17.20	0.26	2.92	8.08	0.09	0.95	0.17	0.09	0.09	2.32	0.34	0.34	0.69	0.69	0.17	0.00	0.00	0.00
				8:00	9:00	25.80	0.77	6.79	9.37	0.09	2.32	1.03	0.34	0.34	1.98	0.52	0.52	0.77	0.77	0.17	0.00	0.00	0.00
				9:00	10:00	21.50	0.52	4.47	8.26	0.09	1.98	0.95	0.26	0.26	1.46	0.69	0.52	0.86	0.95	0.26	0.00	0.00	0.00
				10:00	11:00	21.50	0.43	3.10	8.77	0.09	3.87	0.95	0.43	0.43	1.46	0.26	0.43	0.60	0.60	0.09	0.00	0.00	0.00
				11:00	12:00	21.50	0.17	2.92	7.31	0.17	4.73	0.43	0.09	0.09	2.49	0.86	0.52	0.77	0.77	0.17	0.00	0.00	0.00
				12:00	13:00	21.50	0.34	3.96	9.55	0.09	3.27	0.34	0.09	0.09	1.89	0.86	0.17	0.34	0.43	0.09	0.00	0.00	0.00
				13:00	14:00	21.50	0.17	2.58	6.11	0.09	3.96	1.20	0.43	0.52	1.81	0.26	0.95	1.46	1.55	0.43	0.00	0.00	0.00
				14:00	15:00	21.50	0.17	2.41	8.60	0.09	3.70	0.86	0.26	0.26	1.98	0.43	0.60	0.95	0.95	0.26	0.00	0.00	0.00
				15:00	16:00	25.80	0.34	3.44	10.58	0.09	4.30	1.12	0.43	0.43	1.81	0.86	0.52	0.86	0.86	0.17	0.00	0.00	0.00
				16:00	17:00	21.50	0.09	2.75	8.77	0.09	3.87	0.34	0.09	0.09	1.89	0.09	0.77	1.12	1.20	0.34	0.00	0.00	0.00
				17:00	18:00	21.50	0.17	3.70	9.89	0.09	2.67	0.52	0.26	0.26	1.72	0.43	0.43	0.60	0.60	0.17	0.00	0.00	0.00
				18:00	19:00	21.50	0.17	3.96	10.66	0.09	2.24	0.34	0.17	0.17	0.77	0.43	0.60	0.77	0.95	0.17	0.00	0.00	0.00
				19:00	20:00	17.20	0.26	4.13	9.20	0.00	0.52	0.09	0.00	0.09	1.72	0.26	0.17	0.34	0.34	0.09	0.00	0.00	0.00
				20:00	21:00	17.20	0.26	3.18	10.41	0.00	0.43	0.00	0.00	0.00	2.58	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.00
				21:00	22:00	17.20	0.17	2.67	11.35	0.00	0.52	0.09	0.00	0.00	2.32	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00
22:00	23:00	17.20	0.26	2.06	11.44	0.00	0.34	0.00	0.00	0.00	2.49	0.09	0.09	0.26	0.09	0.09	0.00	0.00	0.00				
23:00	0:00	12.90	0.17	1.46	8.60	0.00	0.26	0.00	0.00	0.00	1.89	0.09	0.09	0.17	0.09	0.09	0.00	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L003	Mei King Street (NB)	Kwei Chow Street	San Ma Tau Street	0:00	1:00	3.95	0.00	2.21	1.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	3.95	0.00	1.98	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.95	0.00	1.98	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.95	0.00	1.98	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.95	0.00	1.98	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.95	0.00	1.98	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	3.95	0.00	1.34	1.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.47	0.47	0.00	0.00	0.00	0.00
				7:00	8:00	3.95	0.00	1.74	1.03	0.00	0.24	0.00	0.00	0.00	0.00	0.24	0.24	0.24	0.24	0.00	0.00	0.00	0.00
				8:00	9:00	3.95	0.00	2.13	0.63	0.00	0.40	0.16	0.00	0.00	0.00	0.16	0.16	0.16	0.16	0.00	0.00	0.00	0.00
				9:00	10:00	3.95	0.00	1.90	0.71	0.00	0.40	0.16	0.00	0.00	0.00	0.32	0.16	0.16	0.16	0.00	0.00	0.00	0.00
				10:00	11:00	3.95	0.00	1.66	0.79	0.00	0.63	0.24	0.00	0.00	0.00	0.16	0.16	0.16	0.16	0.00	0.00	0.00	0.00
				11:00	12:00	3.95	0.00	1.58	0.63	0.00	0.71	0.16	0.00	0.00	0.00	0.24	0.16	0.24	0.24	0.00	0.00	0.00	0.00
				12:00	13:00	3.95	0.00	1.90	0.87	0.00	0.47	0.16	0.00	0.00	0.00	0.24	0.00	0.16	0.16	0.00	0.00	0.00	0.00
				13:00	14:00	3.95	0.00	1.42	0.47	0.00	0.63	0.24	0.00	0.00	0.00	0.16	0.24	0.40	0.40	0.00	0.00	0.00	0.00
				14:00	15:00	3.95	0.00	1.50	0.79	0.00	0.63	0.24	0.00	0.00	0.00	0.16	0.16	0.24	0.24	0.00	0.00	0.00	0.00
				15:00	16:00	3.95	0.00	1.58	0.79	0.00	0.55	0.24	0.00	0.00	0.00	0.24	0.08	0.24	0.24	0.00	0.00	0.00	0.00
				16:00	17:00	3.95	0.08	1.82	0.63	0.00	0.47	0.08	0.00	0.00	0.00	0.00	0.16	0.32	0.40	0.00	0.00	0.00	0.00
				17:00	18:00	7.90	0.16	4.50	1.34	0.00	0.63	0.32	0.00	0.00	0.00	0.16	0.16	0.32	0.32	0.00	0.00	0.00	0.00
				18:00	19:00	3.95	0.08	2.37	0.63	0.00	0.24	0.08	0.00	0.00	0.00	0.08	0.08	0.16	0.24	0.00	0.00	0.00	0.00
				19:00	20:00	3.95	0.00	2.45	0.87	0.00	0.16	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.16	0.00	0.00	0.00	0.00
				20:00	21:00	3.95	0.00	2.53	1.26	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	3.95	0.00	2.29	1.42	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	3.95	0.00	2.13	1.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				L004	To Kwa Wan Road (SB)	San Ma Tau Street	Kwei Chow Street	23:00	0:00	3.95	0.00	2.13	1.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
0:00	1:00	24.80	1.12					6.20	13.70	0.00	0.56	0.00	0.00	0.00	0.37	0.12	0.12	0.43	0.19	0.12	0.00	1.86	
1:00	2:00	12.40	0.56					3.47	8.00	0.00	0.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	12.40	0.50					3.22	8.43	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	9.30	0.50					2.60	5.77	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	9.30	0.37					2.36	6.20	0.00	0.25	0.00	0.00	0.00	0.00	0.06	0.00	0.06	0.00	0.00	0.00	0.00	0.00
5:00	6:00	9.30	0.31					2.11	5.58	0.00	0.43	0.00	0.00	0.06	0.00	0.12	0.12	0.25	0.19	0.12	0.00	0.00	0.00
6:00	7:00	21.70	0.68					4.09	10.85	0.06	0.93	0.12	0.00	0.06	0.31	0.31	0.37	0.74	0.68	0.31	0.00	2.17	0.00
7:00	8:00	52.70	0.87					13.70	18.48	0.25	2.98	0.25	0.12	0.12	1.86	0.81	0.74	1.61	1.43	0.50	0.00	8.99	0.00
8:00	9:00	74.40	2.36					27.90	19.34	0.50	7.07	1.18	0.68	0.68	1.43	0.81	0.81	1.74	1.55	0.50	0.00	7.87	0.00
9:00	10:00	68.20	1.61					20.83	19.28	0.50	6.63	1.24	0.62	0.62	1.24	1.36	1.05	2.11	2.05	0.62	0.00	8.43	0.00
10:00	11:00	58.90	2.17					15.81	17.17	0.62	8.56	2.11	0.99	1.05	0.56	0.43	0.56	1.12	1.12	0.37	0.00	6.26	0.00
11:00	12:00	58.90	1.43					15.31	15.00	0.74	10.79	0.81	0.43	0.43	0.87	1.36	0.74	1.67	1.74	0.56	0.00	7.01	0.00
12:00	13:00	58.90	1.80					20.03	18.35	0.56	7.01	0.68	0.43	0.43	0.62	1.49	0.37	0.68	0.74	0.25	0.00	5.46	0.00
13:00	14:00	58.90	1.36					13.39	12.28	0.62	8.68	2.60	1.24	1.30	0.62	0.43	1.49	2.98	3.16	0.99	0.00	7.75	0.00
14:00	15:00	55.80	1.05					12.28	16.55	0.56	7.94	1.98	0.87	0.93	0.68	0.62	0.93	1.98	1.98	0.68	0.00	6.76	0.00
15:00	16:00	65.10	1.80					16.37	19.28	0.68	8.74	2.48	1.18	1.18	0.68	1.18	0.87	1.67	1.61	0.56	0.00	6.82	0.00
16:00	17:00	55.80	2.42					17.42	14.94	0.43	5.58	1.18	0.43	0.50	0.00	0.19	1.05	2.17	2.54	0.74	0.00	6.20	0.00
17:00	18:00	71.30	3.72					27.16	19.47	0.37	4.59	2.36	1.05	1.05	0.00	0.74	0.74	1.49	1.49	0.56	0.00	6.51	0.00
18:00	19:00	62.00	3.29					24.68	18.10	0.19	3.16	1.36	0.62	0.62	0.00	0.50	0.81	1.67	1.86	0.56	0.00	4.59	0.00
19:00	20:00	55.80	1.98					23.50	19.59	0.06	1.30	0.31	0.12	0.12	0.68	0.43	0.31	0.68	0.81	0.31	0.00	5.58	0.00
20:00	21:00	46.50	1.67					16.18	19.96	0.06	0.93	0.12	0.12	0.12	0.93	0.19	0.12	0.31	0.19	0.12	0.00	5.46	0.00
21:00	22:00	43.40	1.12					13.08	20.58	0.06	1.12	0.25	0.12	0.12	0.81	0.00	0.06	0.12	0.00	0.06	0.00	5.89	0.00
22:00	23:00	40.30	1.36					9.36	19.28	0.06	0.56	0.00	0.00	0.00	0.81	0.25	0.19	0.37	0.19	0.19	0.00	7.69	0.00
23:00	0:00	34.10	1.24	7.63	16.31	0.06	0.62	0.00	0.00	0.00	0.62	0.25	0.19	0.43	0.19	0.19	0.00	6.39	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L005	To Kwa Wan Road (NB)	Kwei Chow Street	San Ma Tau Street	0:00	1:00	20.65	0.65	3.89	11.45	0.00	0.35	0.00	0.00	0.00	0.77	0.24	0.24	0.53	0.30	0.12	0.00	2.12
				1:00	2:00	8.85	0.30	2.07	6.25	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	8.85	0.24	1.77	6.67	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.90	0.24	1.36	4.13	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.90	0.18	1.24	4.25	0.00	0.12	0.00	0.00	0.00	0.00	0.06	0.00	0.06	0.00	0.00	0.00	0.00
				5:00	6:00	8.85	0.24	1.53	5.49	0.00	0.30	0.00	0.00	0.06	0.00	0.18	0.24	0.35	0.30	0.18	0.00	0.00
				6:00	7:00	17.70	0.35	2.36	8.61	0.00	0.53	0.18	0.00	0.06	0.65	0.41	0.41	0.83	0.77	0.30	0.00	2.24
				7:00	8:00	41.30	0.71	7.26	13.92	0.06	1.00	0.83	0.24	0.24	0.83	0.83	0.83	1.71	1.77	0.59	0.00	10.50
				8:00	9:00	53.10	1.83	13.98	14.04	0.06	2.30	3.78	0.94	0.94	0.59	0.89	0.89	1.77	1.77	0.59	0.00	8.73
				9:00	10:00	53.10	1.36	10.86	14.46	0.06	2.24	4.01	0.94	1.00	0.53	1.42	1.06	2.30	2.42	0.83	0.00	9.62
				10:00	11:00	50.15	1.24	9.74	14.57	0.12	5.02	3.84	1.18	1.24	1.18	0.59	0.77	1.53	1.53	0.53	0.00	7.08
				*1:00	12:00	47.20	0.83	8.97	11.98	0.18	5.96	1.48	0.47	0.41	2.01	1.71	0.94	2.01	2.24	0.59	0.00	7.43
				*2:00	13:00	44.25	1.00	11.45	14.10	0.12	3.84	1.24	0.47	0.41	1.53	1.89	0.41	0.83	1.00	0.24	0.00	5.72
				*3:00	14:00	53.10	0.77	7.97	9.97	0.12	5.02	4.54	1.42	1.53	1.48	0.47	1.95	3.89	4.25	1.24	0.00	8.50
				*4:00	15:00	50.15	0.59	7.55	13.87	0.12	4.66	3.54	1.06	1.12	1.59	0.83	1.30	2.60	2.83	0.83	0.00	7.67
				*5:00	16:00	56.05	1.00	9.85	16.11	0.12	5.07	4.31	1.42	1.42	1.42	1.53	1.12	2.18	2.24	0.65	0.00	7.61
				*6:00	17:00	53.10	1.00	11.98	13.45	0.12	5.37	1.12	0.41	0.47	2.48	0.30	1.71	3.36	3.78	0.94	0.00	6.61
				*7:00	18:00	59.00	1.42	17.29	16.23	0.12	4.01	2.12	1.00	1.00	2.48	1.06	1.06	2.07	2.07	0.65	0.00	6.43
				*8:00	19:00	53.10	1.30	16.40	15.99	0.06	2.89	1.30	0.65	0.65	1.06	0.83	1.36	2.42	2.71	0.71	0.00	4.78
				19:00	20:00	44.25	1.12	14.34	15.99	0.00	0.77	0.59	0.12	0.18	1.59	0.59	0.47	0.94	1.00	0.30	0.00	6.25
				20:00	21:00	35.40	0.89	9.26	15.64	0.00	0.47	0.18	0.12	0.12	2.01	0.18	0.18	0.30	0.24	0.12	0.00	5.72
				21:00	22:00	35.40	0.65	7.85	16.93	0.00	0.65	0.41	0.12	0.12	1.89	0.00	0.06	0.12	0.00	0.06	0.00	6.55
				22:00	23:00	35.40	0.83	5.78	16.40	0.00	0.41	0.00	0.00	0.00	1.89	0.30	0.24	0.47	0.24	0.18	0.00	8.67
				23:00	0:00	29.50	0.71	4.66	13.28	0.00	0.41	0.00	0.00	0.00	1.53	0.35	0.30	0.53	0.30	0.24	0.00	7.20
L006	Kwei Chow Street (WB)	Mai King Street	To Kwa Wan Road	0:00	1:00	4.30	0.09	0.60	2.75	0.00	0.09	0.00	0.00	0.00	0.43	0.04	0.04	0.13	0.09	0.04	0.00	
				1:00	2:00	2.15	0.04	0.39	1.63	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.15	0.04	0.30	1.76	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.15	0.09	0.34	1.63	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.15	0.09	0.30	1.68	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.15	0.04	0.26	1.33	0.00	0.13	0.00	0.00	0.00	0.00	0.04	0.04	0.13	0.13	0.04	0.00	
				6:00	7:00	4.30	0.04	0.43	2.45	0.00	0.17	0.04	0.00	0.00	0.39	0.09	0.09	0.26	0.26	0.09	0.00	
				7:00	8:00	8.60	0.09	1.29	3.83	0.00	0.43	0.09	0.04	0.00	1.12	0.22	0.22	0.56	0.52	0.22	0.00	
				8:00	9:00	12.90	0.34	3.01	4.39	0.00	1.20	0.60	0.17	0.09	0.99	0.30	0.30	0.65	0.60	0.26	0.00	
				9:00	10:00	12.90	0.26	2.32	4.56	0.00	1.16	0.69	0.17	0.09	0.86	0.47	0.34	0.82	0.82	0.34	0.00	
				10:00	11:00	10.75	0.26	1.72	3.83	0.00	1.68	0.69	0.22	0.09	0.73	0.22	0.22	0.47	0.43	0.22	0.00	
				11:00	12:00	10.75	0.13	1.59	3.18	0.00	1.98	0.26	0.04	0.04	1.25	0.47	0.30	0.60	0.65	0.26	0.00	
				12:00	13:00	10.75	0.22	2.19	4.21	0.00	1.38	0.26	0.04	0.04	0.99	0.56	0.17	0.30	0.30	0.09	0.00	
				13:00	14:00	10.75	0.13	1.33	2.49	0.00	1.51	0.77	0.17	0.13	0.86	0.13	0.56	1.12	1.16	0.39	0.00	
				14:00	15:00	10.75	0.13	1.29	3.57	0.00	1.46	0.60	0.13	0.09	0.95	0.26	0.34	0.82	0.82	0.30	0.00	
				15:00	16:00	12.90	0.22	1.89	4.43	0.00	1.76	0.82	0.22	0.17	0.90	0.47	0.30	0.73	0.73	0.26	0.00	
				16:00	17:00	10.75	0.26	1.89	3.27	0.00	1.25	0.30	0.04	0.04	0.90	0.09	0.43	0.95	0.99	0.34	0.00	
				17:00	18:00	12.90	0.34	3.05	4.52	0.00	1.08	0.56	0.17	0.09	0.99	0.30	0.30	0.65	0.60	0.26	0.00	
				18:00	19:00	10.75	0.30	2.71	3.91	0.00	0.73	0.30	0.09	0.04	0.39	0.22	0.34	0.73	0.73	0.26	0.00	
				19:00	20:00	8.60	0.22	2.32	4.04	0.00	0.22	0.09	0.00	0.00	0.86	0.13	0.13	0.26	0.26	0.09	0.00	
				20:00	21:00	8.60	0.22	1.81	4.73	0.00	0.17	0.04	0.00	0.00	1.33	0.04	0.04	0.09	0.09	0.04	0.00	
				21:00	22:00	8.60	0.17	1.59	5.25	0.00	0.22	0.04	0.00	0.00	1.25	0.00	0.00	0.04	0.00	0.04	0.00	
				22:00	23:00	6.45	0.13	0.90	3.91	0.00	0.09	0.00	0.00	0.00	0.99	0.09	0.04	0.13	0.09	0.09	0.00	
				23:00	0:00	6.45	0.17	0.90	3.83	0.00	0.09	0.00	0.00	0.00	0.99	0.09	0.04	0.17	0.09	0.09	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L007	Kwei Chow Street (WB)	Wai King Street	Mei King Street	0:00	1:00	10.80	0.22	2.21	6.64	0.00	0.27	0.00	0.00	0.00	0.65	0.16	0.16	0.27	0.16	0.05	0.00	0.00		
				1:00	2:00	5.40	0.11	1.24	3.83	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.40	0.16	1.13	3.94	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.40	0.22	1.24	3.67	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.40	0.11	1.13	3.83	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.40	0.11	0.92	3.29	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.11	0.16	0.27	0.16	0.11	0.00	0.00	0.00
				6:00	7:00	8.10	0.11	1.19	4.43	0.00	0.38	0.05	0.00	0.00	0.49	0.16	0.22	0.49	0.43	0.16	0.00	0.00	0.00	0.00
				7:00	8:00	21.60	0.22	4.86	9.13	0.05	1.19	0.32	0.05	0.05	1.94	0.59	0.54	1.13	1.13	0.38	0.00	0.00	0.00	0.00
				8:00	9:00	29.70	0.70	9.72	9.13	0.11	2.75	1.40	0.22	0.22	1.40	0.65	0.65	1.19	1.19	0.38	0.00	0.00	0.00	0.00
				9:00	10:00	27.00	0.43	6.97	8.69	0.11	2.48	1.46	0.22	0.22	1.24	0.97	0.70	1.46	1.57	0.49	0.00	0.00	0.00	0.00
				10:00	11:00	24.30	0.49	5.35	8.05	0.16	4.10	1.84	0.27	0.27	0.97	0.38	0.43	0.86	0.86	0.27	0.00	0.00	0.00	0.00
				11:00	12:00	24.30	0.27	5.08	6.75	0.16	5.02	0.65	0.11	0.11	1.73	1.03	0.59	1.19	1.24	0.38	0.00	0.00	0.00	0.00
				12:00	13:00	21.60	0.32	6.16	7.56	0.11	3.08	0.54	0.11	0.11	1.19	1.08	0.22	0.43	0.54	0.16	0.00	0.00	0.00	0.00
				13:00	14:00	24.30	0.27	4.32	5.35	0.16	3.89	2.00	0.32	0.32	1.24	0.27	1.08	2.11	2.27	0.70	0.00	0.00	0.00	0.00
				14:00	15:00	21.60	0.22	3.73	6.75	0.16	3.40	1.46	0.22	0.22	1.19	0.43	0.65	1.35	1.40	0.43	0.00	0.00	0.00	0.00
				15:00	16:00	27.00	0.38	5.45	8.53	0.16	4.10	2.00	0.38	0.38	1.19	0.86	0.65	1.24	1.24	0.43	0.00	0.00	0.00	0.00
				16:00	17:00	21.60	0.32	5.29	6.10	0.11	3.13	0.70	0.11	0.11	1.08	0.11	0.81	1.51	1.73	0.49	0.00	0.00	0.00	0.00
				17:00	18:00	24.30	0.49	7.88	7.29	0.11	2.43	1.30	0.22	0.22	1.13	0.49	0.49	0.97	0.97	0.32	0.00	0.00	0.00	0.00
				18:00	19:00	21.60	0.43	7.29	7.02	0.05	1.73	0.76	0.11	0.11	0.43	0.43	0.65	1.08	1.19	0.32	0.00	0.00	0.00	0.00
				19:00	20:00	21.60	0.43	8.05	9.07	0.00	0.59	0.22	0.00	0.05	1.35	0.32	0.22	0.54	0.59	0.16	0.00	0.00	0.00	0.00
				20:00	21:00	16.20	0.32	4.97	8.21	0.00	0.38	0.05	0.00	0.05	1.67	0.11	0.11	0.16	0.11	0.05	0.00	0.00	0.00	0.00
				21:00	22:00	16.20	0.27	4.32	9.13	0.00	0.54	0.16	0.00	0.05	1.57	0.00	0.05	0.05	0.00	0.05	0.00	0.00	0.00	0.00
				22:00	23:00	13.50	0.27	2.86	7.99	0.00	0.27	0.00	0.00	0.00	1.40	0.16	0.11	0.22	0.11	0.11	0.00	0.00	0.00	0.00
				L008	Kwei Chow Street (WB)	Long Yuet Street	Wai King Street	23:00	0:00	13.50	0.32	2.81	7.67	0.00	0.32	0.00	0.00	0.00	1.40	0.22	0.16	0.27	0.16	0.16
0:00	1:00	4.20	0.08					0.92	2.52	0.00	0.17	0.00	0.00	0.00	0.00	0.08	0.08	0.17	0.08	0.08	0.00	0.00	0.00	
1:00	2:00	2.10	0.08					0.50	1.43	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	2.10	0.08					0.42	1.51	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	2.10	0.08					0.50	1.43	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	2.10	0.00					0.46	1.51	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	2.10	0.00					0.34	1.26	0.00	0.08	0.00	0.00	0.00	0.00	0.08	0.08	0.08	0.08	0.08	0.00	0.00	0.00	0.00
6:00	7:00	2.10	0.04					0.29	1.18	0.00	0.13	0.04	0.00	0.00	0.00	0.04	0.08	0.13	0.13	0.04	0.00	0.00	0.00	0.00
7:00	8:00	6.30	0.04					1.47	2.90	0.04	0.42	0.13	0.00	0.00	0.00	0.21	0.21	0.38	0.38	0.13	0.00	0.00	0.00	0.00
8:00	9:00	10.50	0.21					3.32	3.28	0.04	1.13	0.67	0.13	0.13	0.00	0.25	0.25	0.46	0.46	0.17	0.00	0.00	0.00	0.00
9:00	10:00	8.40	0.13					2.06	2.77	0.04	0.92	0.59	0.08	0.08	0.00	0.34	0.25	0.46	0.50	0.17	0.00	0.00	0.00	0.00
10:00	11:00	6.30	0.13					1.34	2.06	0.04	1.18	0.59	0.08	0.08	0.00	0.08	0.13	0.25	0.25	0.08	0.00	0.00	0.00	0.00
11:00	12:00	6.30	0.08					1.34	1.85	0.04	1.47	0.21	0.04	0.04	0.00	0.29	0.17	0.34	0.34	0.08	0.00	0.00	0.00	0.00
12:00	13:00	6.30	0.08					1.76	2.31	0.04	1.01	0.21	0.04	0.04	0.00	0.38	0.08	0.13	0.17	0.04	0.00	0.00	0.00	0.00
13:00	14:00	8.40	0.13					1.43	1.68	0.04	1.47	0.88	0.17	0.17	0.00	0.13	0.42	0.80	0.84	0.25	0.00	0.00	0.00	0.00
14:00	15:00	6.30	0.04					1.09	1.97	0.04	1.13	0.55	0.08	0.08	0.00	0.13	0.21	0.42	0.42	0.13	0.00	0.00	0.00	0.00
15:00	16:00	8.40	0.08					1.64	2.65	0.04	1.43	0.80	0.17	0.17	0.00	0.29	0.21	0.38	0.38	0.17	0.00	0.00	0.00	0.00
16:00	17:00	6.30	0.13					1.60	1.72	0.00	1.01	0.25	0.04	0.04	0.00	0.04	0.25	0.46	0.59	0.17	0.00	0.00	0.00	0.00
17:00	18:00	6.30	0.13					2.10	1.85	0.00	0.67	0.46	0.08	0.08	0.00	0.13	0.13	0.29	0.29	0.08	0.00	0.00	0.00	0.00
18:00	19:00	6.30	0.17					2.18	1.93	0.00	0.50	0.29	0.04	0.04	0.00	0.08	0.21	0.34	0.42	0.08	0.00	0.00	0.00	0.00
19:00	20:00	6.30	0.08					2.48	2.81	0.00	0.21	0.08	0.00	0.00	0.00	0.08	0.08	0.21	0.21	0.04	0.00	0.00	0.00	0.00
20:00	21:00	4.20	0.08					1.39	2.35	0.00	0.13	0.04	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.04	0.00	0.00	0.00	0.00
21:00	22:00	4.20	0.04					1.22	2.65	0.00	0.21	0.04	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	4.20	0.13					1.01	2.65	0.00	0.13	0.00	0.00	0.00	0.00	0.04	0.04	0.13	0.04	0.04	0.00	0.00	0.00	0.00
23:00	0:00	4.20	0.13	0.92	2.56	0.00	0.13	0.00	0.00	0.00	0.00	0.08	0.08	0.13	0.08	0.08	0.00	0.00	0.00	0.00	0.00			

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L009	King Wan Street (SB)	Kwei Chow Street	Shun Fung Street	0:00	1:00	30.40	1.52	14.29	11.55	0.00	1.22	0.00	0.00	0.00	0.30	0.30	0.30	0.61	0.30	0.00	0.00	0.00
				1:00	2:00	15.20	0.91	7.30	6.38	0.00	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	15.20	0.61	7.30	6.99	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	15.20	0.91	7.30	6.08	0.00	0.91	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	15.20	1.22	6.99	6.38	0.00	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	15.20	0.91	5.78	6.38	0.00	0.91	0.00	0.00	0.00	0.00	0.00	0.30	0.30	0.30	0.30	0.00	0.00
				6:00	7:00	30.40	1.22	11.55	10.94	0.00	1.82	0.30	0.00	0.00	0.30	0.91	0.91	1.22	1.22	0.00	0.00	0.00
				7:00	8:00	60.80	0.61	25.84	19.76	0.30	3.95	0.91	0.30	0.00	0.00	1.82	1.82	2.74	2.74	0.00	0.00	0.00
				8:00	9:00	106.40	2.43	54.72	21.58	0.30	9.42	4.86	0.91	0.61	0.00	2.43	2.43	3.34	3.34	0.00	0.00	0.00
				9:00	10:00	91.20	1.52	39.22	20.67	0.30	8.21	5.17	0.91	0.61	0.00	3.34	2.74	4.26	4.26	0.00	0.00	0.00
				10:00	11:00	76.00	3.34	33.74	13.68	0.30	13.38	4.26	0.91	0.61	0.61	0.91	0.91	1.52	1.52	0.30	0.00	0.00
				11:00	12:00	76.00	2.13	33.14	11.86	0.61	16.72	1.52	0.30	0.30	0.91	2.43	1.22	2.13	2.43	0.30	0.00	0.00
				12:00	13:00	76.00	2.43	41.04	13.98	0.30	10.34	1.52	0.30	0.30	0.61	2.43	0.61	0.91	1.22	0.00	0.00	0.00
				13:00	14:00	76.00	1.82	28.58	9.73	0.30	13.68	5.47	1.22	0.91	0.61	0.61	3.04	4.56	4.86	0.61	0.00	0.00
				14:00	15:00	76.00	1.82	28.58	14.29	0.30	13.68	4.56	1.22	0.61	0.61	1.52	2.13	3.04	3.34	0.30	0.00	0.00
				15:00	16:00	91.20	3.04	37.39	17.02	0.30	14.90	5.47	1.52	0.91	0.61	2.43	1.82	2.74	2.74	0.30	0.00	0.00
				16:00	17:00	76.00	4.26	42.56	7.90	0.30	11.55	1.22	0.30	0.30	1.52	0.30	0.91	1.82	2.13	0.91	0.00	0.00
				17:00	18:00	106.40	7.30	67.79	10.64	0.30	9.73	2.74	0.61	0.61	1.82	0.61	0.61	1.52	1.52	0.61	0.00	0.00
				18:00	19:00	91.20	6.38	61.41	9.73	0.30	6.38	1.52	0.30	0.30	0.61	0.30	0.61	1.22	1.52	0.61	0.00	0.00
				19:00	20:00	76.00	2.74	51.07	15.81	0.00	1.82	0.61	0.00	0.00	0.61	0.61	0.61	0.91	1.22	0.00	0.00	0.00
				20:00	21:00	60.80	2.74	37.39	17.02	0.00	1.22	0.30	0.00	0.00	0.91	0.30	0.30	0.30	0.30	0.00	0.00	0.00
				21:00	22:00	45.60	1.52	25.84	15.20	0.00	1.82	0.30	0.00	0.00	0.91	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	45.60	2.43	22.80	17.02	0.00	0.91	0.00	0.00	0.00	0.91	0.30	0.30	0.61	0.30	0.00	0.00	0.00
				L010	Kiang Su Street (EB)	Kau Pui Lung Road	Maidstone Road	23:00	0:00	45.60	2.43	21.89	17.02	0.00	1.22	0.00	0.00	0.00	1.22	0.30	0.30	0.91
0:00	1:00	2.10	0.00					0.71	1.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	2.10	0.00					1.05	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.10	0.00					1.05	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.10	0.00					0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.10	0.00					0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.10	0.00					0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	2.10	0.00					1.05	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
7:00	8:00	2.10	0.29					0.88	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
8:00	9:00	2.10	0.34					1.09	0.50	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
9:00	10:00	2.10	0.21					1.05	0.63	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
10:00	11:00	2.10	0.29					0.92	0.59	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
11:00	12:00	2.10	0.25					0.76	0.55	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
12:00	13:00	2.10	0.25					1.05	0.55	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
13:00	14:00	2.10	0.25					0.76	0.55	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
14:00	15:00	2.10	0.29					0.92	0.59	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
15:00	16:00	2.10	0.25					0.67	0.71	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
16:00	17:00	2.10	0.29					0.92	0.59	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
17:00	18:00	2.10	0.25					1.05	0.55	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
18:00	19:00	2.10	0.25					1.05	0.55	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
19:00	20:00	2.10	0.25					1.13	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
20:00	21:00	2.10	0.29					0.88	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	2.10	0.29					0.88	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	2.10	0.34					0.71	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	2.10	0.42	0.84	0.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L011	Yuk Yat Street (NB)	Shun Fung Street	Kwei Chow Street	0:00	1:00	5.90	0.12	1.42	3.42	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.12	0.12	0.24	0.12	0.12	0.00	0.00
				1:00	2:00	5.90	0.24	1.53	3.89	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	5.90	0.24	1.42	4.01	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.90	0.35	1.65	3.54	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.90	0.00	1.53	4.01	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.90	0.00	1.06	3.42	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.24	0.24	0.24	0.24	0.24	0.00	0.00
				6:00	7:00	5.90	0.12	1.06	3.07	0.00	0.35	0.12	0.00	0.00	0.00	0.00	0.12	0.24	0.35	0.35	0.12	0.00	0.00
				7:00	8:00	17.70	0.12	4.48	6.73	0.00	1.42	0.47	0.00	0.00	0.00	0.00	0.59	0.59	1.42	1.30	0.59	0.00	0.00
				8:00	9:00	23.60	0.24	8.02	6.25	0.00	2.83	1.89	0.24	0.00	0.00	0.00	0.59	0.59	1.30	1.18	0.47	0.00	0.00
				9:00	10:00	23.60	0.12	6.14	6.49	0.00	2.83	2.12	0.24	0.00	0.00	0.00	0.94	0.71	1.77	1.65	0.59	0.00	0.00
				10:00	11:00	17.70	0.35	4.25	4.84	0.12	3.30	2.24	0.24	0.12	0.00	0.00	0.24	0.35	0.71	0.71	0.24	0.00	0.00
				11:00	12:00	17.70	0.24	4.01	4.01	0.12	4.13	0.94	0.12	0.00	0.00	0.00	0.94	0.47	1.18	1.18	0.35	0.00	0.00
				12:00	13:00	17.70	0.24	5.55	5.31	0.12	2.95	0.83	0.12	0.00	0.00	0.00	1.06	0.24	0.59	0.59	0.12	0.00	0.00
				13:00	14:00	23.60	0.24	4.37	4.01	0.12	4.01	3.07	0.47	0.24	0.00	0.00	0.24	1.18	2.36	2.48	0.83	0.00	0.00
				14:00	15:00	17.70	0.12	3.19	4.60	0.12	3.07	2.01	0.24	0.12	0.00	0.00	0.35	0.59	1.42	1.42	0.47	0.00	0.00
				15:00	16:00	23.60	0.24	5.07	6.25	0.12	4.01	2.83	0.47	0.12	0.00	0.00	0.83	0.59	1.30	1.30	0.47	0.00	0.00
				16:00	17:00	17.70	0.47	5.19	3.89	0.12	2.36	1.06	0.12	0.12	0.00	0.00	0.12	0.71	1.42	1.65	0.47	0.00	0.00
				17:00	18:00	17.70	0.59	6.61	4.13	0.12	1.53	1.65	0.24	0.24	0.00	0.00	0.35	0.35	0.83	0.83	0.24	0.00	0.00
				18:00	19:00	17.70	0.59	6.61	4.37	0.12	1.18	1.06	0.12	0.12	0.00	0.00	0.35	0.59	1.06	1.18	0.35	0.00	0.00
				19:00	20:00	17.70	0.35	7.67	6.61	0.00	0.59	0.47	0.00	0.00	0.00	0.00	0.35	0.35	0.59	0.59	0.12	0.00	0.00
				20:00	21:00	11.80	0.24	4.60	5.90	0.00	0.35	0.12	0.00	0.00	0.00	0.00	0.12	0.12	0.12	0.12	0.12	0.00	0.00
				21:00	22:00	11.80	0.12	4.13	6.61	0.00	0.47	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00	0.00	0.00
				22:00	23:00	11.80	0.35	3.19	7.08	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.12	0.12	0.35	0.12	0.12	0.00	0.00
				L012	Mei Kwong Street (NB)	Mei King Street	Kwei Chow Street	23:00	0:00	11.80	0.35	3.19	6.61	0.00	0.35	0.00	0.00	0.00	0.00	0.24	0.24	0.35	0.24
0:00	1:00	7.45	0.00					4.62	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.75	0.00	0.00
1:00	2:00	7.45	0.00					4.92	2.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	7.45	0.00					4.92	2.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	7.45	0.00					5.51	1.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	7.45	0.00					4.92	2.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	7.45	0.00					3.73	2.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.19	0.00	0.00
6:00	7:00	7.45	0.00					3.73	2.24	0.00	0.75	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.75	0.00	0.00
7:00	8:00	7.45	0.00					4.17	1.49	0.00	0.45	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.30	0.00	0.75	0.00	0.00
8:00	9:00	14.90	0.30					9.54	1.64	0.00	1.64	0.75	0.00	0.00	0.00	0.00	0.00	0.00	0.30	0.00	0.75	0.00	0.00
9:00	10:00	7.45	0.15					4.32	0.89	0.00	0.89	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.60	0.00	0.00
10:00	11:00	7.45	0.15					3.73	0.89	0.00	1.49	0.75	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.30	0.00	0.00
11:00	12:00	7.45	0.00					3.87	0.75	0.00	2.09	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.45	0.00	0.00
12:00	13:00	7.45	0.15					4.77	0.89	0.00	1.34	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
13:00	14:00	7.45	0.00					3.28	0.60	0.00	1.49	0.75	0.00	0.00	0.00	0.00	0.00	0.00	0.45	0.00	0.89	0.00	0.00
14:00	15:00	7.45	0.00					3.43	1.04	0.00	1.64	0.60	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.60	0.00	0.00
15:00	16:00	7.45	0.15					3.58	1.04	0.00	1.34	0.75	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.45	0.00	0.00
16:00	17:00	7.45	0.15					3.87	0.75	0.00	1.19	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.45	0.00	0.60	0.00	0.00
17:00	18:00	7.45	0.15					4.92	0.75	0.00	0.75	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.30	0.00	0.00
18:00	19:00	7.45	0.15					5.07	0.89	0.00	0.60	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.30	0.00	0.00
19:00	20:00	7.45	0.15					5.66	1.19	0.00	0.15	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
20:00	21:00	7.45	0.30					5.07	1.49	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.30	0.00	0.00
21:00	22:00	7.45	0.00					5.22	1.94	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	7.45	0.00					4.32	2.24	0.00	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.45	0.00	0.00
23:00	0:00	7.45	0.00	4.32	2.24	0.00	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.45	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L013	Mei King Street (SB)	Kwei Chow Street	Mei Kwong Street	0:00	1:00	12.00	0.24	3.96	7.08	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	0.00	0.00
				1:00	2:00	6.00	0.12	1.92	3.84	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	6.00	0.00	1.80	3.96	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	6.00	0.00	1.92	3.84	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	6.00	0.00	1.80	3.84	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	6.00	0.00	1.56	3.72	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	0.00	0.00
				6:00	7:00	6.00	0.12	1.44	3.48	0.00	0.36	0.12	0.00	0.00	0.00	0.12	0.12	0.12	0.12	0.00	0.00	0.00	0.00
				7:00	8:00	18.00	0.12	6.96	8.16	0.12	1.20	0.24	0.00	0.00	0.00	0.24	0.24	0.48	0.24	0.00	0.00	0.00	0.00
				8:00	9:00	30.00	0.48	14.64	8.88	0.12	3.00	1.56	0.12	0.00	0.00	0.24	0.24	0.48	0.24	0.00	0.00	0.00	0.00
				9:00	10:00	30.00	0.48	12.24	9.96	0.12	3.12	1.92	0.12	0.00	0.00	0.48	0.36	0.72	0.48	0.00	0.00	0.00	0.00
				10:00	11:00	18.00	0.24	5.88	5.52	0.12	3.84	1.68	0.12	0.00	0.00	0.12	0.12	0.24	0.12	0.00	0.00	0.00	0.00
				11:00	12:00	18.00	0.12	6.00	4.92	0.24	5.04	0.60	0.00	0.00	0.00	0.36	0.12	0.36	0.24	0.00	0.00	0.00	0.00
				12:00	13:00	18.00	0.24	7.32	5.76	0.12	3.24	0.60	0.00	0.00	0.00	0.36	0.12	0.12	0.12	0.00	0.00	0.00	0.00
				13:00	14:00	18.00	0.12	5.52	4.20	0.12	4.20	2.16	0.12	0.00	0.00	0.12	0.36	0.72	0.36	0.00	0.00	0.00	0.00
				14:00	15:00	18.00	0.12	5.04	5.88	0.12	3.96	1.68	0.12	0.00	0.00	0.12	0.24	0.48	0.24	0.00	0.00	0.00	0.00
				15:00	16:00	24.00	0.24	7.68	7.44	0.12	4.80	2.40	0.12	0.00	0.00	0.24	0.24	0.48	0.24	0.00	0.00	0.00	0.00
				*6:00	17:00	12.00	0.12	4.08	3.00	0.12	3.12	0.72	0.00	0.00	0.00	0.00	0.24	0.36	0.24	0.00	0.00	0.00	0.00
				*7:00	18:00	18.00	0.24	7.92	4.56	0.12	2.88	1.56	0.12	0.00	0.00	0.12	0.12	0.24	0.12	0.00	0.00	0.00	0.00
				*8:00	19:00	18.00	0.36	8.28	4.92	0.12	2.40	1.08	0.12	0.00	0.00	0.12	0.12	0.36	0.12	0.00	0.00	0.00	0.00
				*9:00	20:00	18.00	0.24	9.72	6.72	0.00	0.60	0.24	0.00	0.00	0.00	0.12	0.12	0.12	0.12	0.00	0.00	0.00	0.00
				20:00	21:00	18.00	0.36	8.28	8.64	0.00	0.48	0.12	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	12.00	0.12	4.80	6.48	0.00	0.48	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	12.00	0.12	4.20	7.32	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00	0.00	0.00	0.00
				L014	To Kwa Wan Road (SB)	Kwei Chow Street	Lok Shan road	23:00	0:00	12.00	0.24	4.08	7.08	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00
0:00	1:00	77.85	2.42					18.34	41.69	0.00	1.73	0.00	0.00	0.00	5.19	0.35	0.35	1.21	0.52	0.35	0.00	5.71	
1:00	2:00	43.25	1.73					11.76	27.51	0.00	1.38	0.00	0.00	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	34.60	1.04					8.48	23.70	0.00	0.69	0.00	0.00	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	25.95	1.04					7.09	16.78	0.00	1.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	25.95	1.04					6.40	17.30	0.00	0.87	0.00	0.00	0.00	0.00	0.17	0.00	0.17	0.00	0.00	0.00	0.00	
5:00	6:00	34.60	1.21					7.61	20.93	0.00	1.56	0.00	0.00	0.17	0.00	0.52	0.52	0.87	0.69	0.52	0.00	0.00	
6:00	7:00	60.55	1.38					10.55	29.76	0.17	2.25	0.35	0.00	0.17	4.50	0.69	0.87	1.73	1.56	0.69	0.00	5.88	
7:00	8:00	164.35	2.08					38.23	53.98	0.69	8.30	0.69	0.35	0.35	21.11	1.90	1.73	4.15	4.15	1.73	0.00	24.91	
8:00	9:00	216.25	5.71					75.43	54.67	1.38	19.20	3.98	1.73	1.73	16.09	2.25	2.25	4.33	4.33	1.73	0.00	21.45	
9:00	10:00	198.95	3.98					56.92	55.19	1.04	17.99	3.98	1.73	1.73	14.01	3.46	2.60	5.36	5.71	2.25	0.00	23.01	
10:00	11:00	181.65	5.54					46.36	52.42	1.56	24.91	6.75	2.77	2.94	8.82	1.21	1.56	3.29	3.46	1.04	0.00	19.03	
11:00	12:00	181.65	3.46					43.77	44.29	2.08	30.62	2.60	1.04	0.87	15.40	3.63	2.25	4.50	4.84	1.38	0.00	20.93	
12:00	13:00	181.65	4.33					57.61	55.19	1.38	20.41	2.42	1.04	0.87	11.59	4.15	0.87	2.08	2.42	0.69	0.00	16.61	
13:00	14:00	181.65	3.29					38.58	36.85	1.90	25.26	8.13	3.29	3.63	11.25	1.04	4.15	8.48	9.52	2.94	0.00	23.36	
14:00	15:00	173.00	2.60					35.47	49.31	1.56	23.01	6.06	2.42	2.42	11.59	1.73	2.77	5.71	5.88	1.90	0.00	20.59	
15:00	16:00	198.95	4.50					47.23	57.96	1.73	25.26	7.61	3.11	3.11	10.38	3.46	2.42	4.84	4.84	1.56	0.00	20.93	
16:00	17:00	181.65	6.23					53.80	47.58	1.21	17.65	3.63	1.21	1.21	7.44	0.69	3.63	6.75	7.79	1.56	0.00	21.28	
17:00	18:00	216.25	8.82					78.20	58.65	1.04	13.67	6.92	2.77	2.77	7.61	2.25	2.25	4.33	4.33	1.38	0.00	21.28	
18:00	19:00	190.30	8.13					73.53	56.40	0.52	9.52	3.98	1.56	1.73	3.29	1.56	2.94	5.02	5.54	1.21	0.00	15.40	
19:00	20:00	173.00	4.67					68.34	58.65	0.17	3.63	0.87	0.35	0.35	11.94	1.21	0.87	2.08	2.25	0.69	0.00	16.95	
20:00	21:00	147.05	4.15					45.85	58.99	0.17	2.60	0.35	0.35	0.35	15.57	0.52	0.35	0.69	0.52	0.35	0.00	16.26	
21:00	22:00	138.40	2.77					37.37	61.24	0.17	3.29	0.69	0.35	0.35	13.84	0.00	0.17	0.35	0.00	0.17	0.00	17.65	
22:00	23:00	129.75	3.46					26.82	57.78	0.17	1.73	0.00	0.00	0.00	13.49	0.69	0.52	1.04	0.52	0.52	0.00	23.01	
23:00	0:00	112.45	3.11	22.32	50.00	0.17	1.73	0.00	0.00	0.00	11.76	0.87	0.52	1.21	0.52	0.52	0.00	19.72					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L015	To Kwa Wan Road (NB)	Lok Shan road	Kwei Chow Street	0:00	1:00	25.35	0.51	3.21	14.87	0.00	0.17	0.00	0.00	0.00	0.17	0.17	0.17	0.34	0.17	0.17	0.00	5.41
				1:00	2:00	16.90	0.51	2.70	13.35	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	16.90	0.51	2.54	13.52	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	8.45	0.34	1.35	6.59	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	8.45	0.17	1.35	6.76	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	8.45	0.17	1.01	6.42	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.17	0.17	0.17	0.17	0.00	0.00
				6:00	7:00	25.35	0.34	2.37	13.52	0.00	0.34	0.17	0.00	0.00	0.17	0.17	0.17	0.68	0.34	0.17	0.00	6.93
				7:00	8:00	59.15	1.01	5.75	20.28	0.00	0.34	0.85	0.17	0.17	0.00	0.51	0.51	1.01	0.85	0.68	0.00	27.04
				8:00	9:00	76.05	3.21	13.01	22.82	0.00	0.85	4.73	0.85	0.85	0.00	0.51	0.51	1.35	0.85	0.85	0.00	25.69
				9:00	10:00	76.05	2.37	9.80	23.15	0.00	0.85	4.90	0.85	0.85	0.00	0.85	0.85	1.69	1.35	0.85	0.00	27.72
				10:00	11:00	67.60	1.69	8.79	20.96	0.17	4.56	4.56	1.52	1.52	0.51	0.34	0.34	1.01	1.01	0.51	0.00	20.11
				11:00	12:00	59.15	1.01	7.94	16.22	0.17	5.41	1.52	0.51	0.51	0.85	1.01	0.51	1.35	1.18	0.51	0.00	20.45
				12:00	13:00	59.15	1.18	10.65	21.13	0.17	3.55	1.35	0.51	0.51	0.68	1.18	0.34	0.51	0.51	0.17	0.00	16.73
				13:00	14:00	67.60	0.85	7.10	14.03	0.17	4.39	5.07	1.69	1.69	0.68	0.34	1.18	2.70	2.54	1.18	0.00	24.00
				14:00	15:00	67.60	0.68	6.93	20.11	0.17	4.39	4.06	1.35	1.35	0.68	0.51	0.68	1.86	1.69	0.68	0.00	22.48
				15:00	16:00	76.05	1.35	9.30	23.15	0.17	4.56	5.24	1.86	1.69	0.85	0.85	0.85	1.69	1.35	0.85	0.00	22.31
				16:00	17:00	67.60	0.85	11.49	17.75	0.17	6.42	1.35	0.68	0.68	1.35	0.17	1.01	2.54	2.54	0.68	0.00	19.94
				17:00	18:00	76.05	1.18	16.56	22.31	0.17	4.90	2.54	1.52	1.35	1.35	0.68	0.68	1.52	1.35	0.51	0.00	19.44
				18:00	19:00	67.60	1.01	16.22	21.97	0.17	3.72	1.69	1.01	1.01	0.51	0.51	1.01	1.86	1.86	0.51	0.00	14.53
				19:00	20:00	59.15	1.35	12.84	23.15	0.00	0.68	0.68	0.17	0.17	0.68	0.34	0.34	0.68	0.51	0.17	0.00	17.41
				20:00	21:00	50.70	1.18	8.45	22.14	0.00	0.51	0.17	0.17	0.17	0.85	0.17	0.17	0.17	0.17	0.17	0.00	16.22
				21:00	22:00	50.70	0.68	6.93	23.32	0.00	0.51	0.34	0.17	0.17	0.68	0.00	0.00	0.17	0.00	0.00	0.00	17.75
				22:00	23:00	59.15	1.18	5.41	24.34	0.00	0.34	0.00	0.00	0.00	1.01	0.17	0.17	0.34	0.17	0.17	0.00	25.86
				23:00	0:00	42.25	0.85	3.72	17.41	0.00	0.34	0.00	0.00	0.00	0.68	0.17	0.17	0.34	0.17	0.17	0.00	18.25
L016	Mei King Street (SB)	Mei Kwong Street	Lok Shan road	0:00	1:00	4.10	0.08	0.98	2.95	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	4.10	0.00	1.07	3.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.10	0.00	0.90	3.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.10	0.00	1.07	3.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.10	0.00	0.98	3.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.10	0.00	0.90	3.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.10	0.00	0.82	3.03	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00
				7:00	8:00	12.30	0.08	3.94	7.22	0.00	0.33	0.08	0.00	0.00	0.00	0.00	0.00	0.33	0.00	0.33	0.00	0.00
				8:00	9:00	16.40	0.25	7.38	7.05	0.00	0.74	0.33	0.16	0.00	0.00	0.00	0.00	0.25	0.00	0.25	0.00	0.00
				9:00	10:00	16.40	0.16	6.15	7.87	0.00	0.82	0.41	0.16	0.00	0.00	0.00	0.00	0.41	0.00	0.41	0.00	0.00
				10:00	11:00	8.20	0.08	2.62	4.10	0.00	0.74	0.33	0.16	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00
				11:00	12:00	8.20	0.08	2.79	3.77	0.00	1.07	0.08	0.08	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.00	0.00
				12:00	13:00	8.20	0.08	3.20	4.02	0.00	0.57	0.08	0.08	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00
				13:00	14:00	8.20	0.08	2.62	3.20	0.00	0.90	0.41	0.16	0.00	0.00	0.00	0.00	0.41	0.00	0.41	0.00	0.00
				14:00	15:00	8.20	0.08	2.21	4.26	0.00	0.74	0.25	0.16	0.00	0.00	0.00	0.00	0.25	0.00	0.25	0.00	0.00
				15:00	16:00	12.30	0.16	3.77	6.07	0.00	1.07	0.49	0.25	0.00	0.00	0.00	0.00	0.25	0.00	0.25	0.00	0.00
				16:00	17:00	4.10	0.08	1.39	1.80	0.00	0.49	0.08	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.08	0.00	0.00
				17:00	18:00	8.20	0.16	3.36	3.28	0.00	0.57	0.33	0.16	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.00	0.00
				18:00	19:00	8.20	0.16	3.36	3.53	0.00	0.49	0.16	0.16	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.00	0.00
				19:00	20:00	12.30	0.16	5.49	5.99	0.00	0.16	0.16	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.00	0.00
				20:00	21:00	8.20	0.08	3.03	5.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	8.20	0.08	2.46	5.58	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	8.20	0.08	2.05	5.90	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				23:00	0:00	8.20	0.16	1.97	5.74	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L017	Yuk Yat Street (NB)	Lok Shan road	Shun Fung Street	0:00	1:00	3.80	0.08	0.91	2.20	0.00	0.15	0.00	0.00	0.00	0.00	0.08	0.08	0.15	0.08	0.08	0.00	0.00	
				1:00	2:00	3.80	0.15	0.99	2.51	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.80	0.15	0.91	2.58	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.80	0.23	1.06	2.28	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.80	0.00	0.99	2.58	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.80	0.00	0.68	2.20	0.00	0.15	0.00	0.00	0.00	0.00	0.15	0.15	0.15	0.15	0.15	0.00	0.00	
				6:00	7:00	3.80	0.08	0.68	1.98	0.00	0.23	0.08	0.00	0.00	0.00	0.08	0.15	0.23	0.23	0.08	0.00	0.00	
				7:00	8:00	11.40	0.08	2.89	4.33	0.00	0.91	0.30	0.00	0.00	0.00	0.38	0.38	0.91	0.84	0.38	0.00	0.00	
				8:00	9:00	15.20	0.15	5.17	4.03	0.00	1.82	1.22	0.15	0.00	0.00	0.38	0.38	0.84	0.76	0.30	0.00	0.00	
				9:00	10:00	15.20	0.08	3.95	4.18	0.00	1.82	1.37	0.15	0.00	0.00	0.61	0.46	1.14	1.06	0.38	0.00	0.00	
				10:00	11:00	11.40	0.23	2.74	3.12	0.08	2.13	1.44	0.15	0.08	0.00	0.15	0.23	0.46	0.46	0.15	0.00	0.00	
				11:00	12:00	11.40	0.15	2.58	2.58	0.08	2.66	0.61	0.08	0.00	0.00	0.61	0.30	0.76	0.76	0.23	0.00	0.00	
				12:00	13:00	11.40	0.15	3.57	3.42	0.08	1.90	0.53	0.08	0.00	0.00	0.68	0.15	0.38	0.38	0.08	0.00	0.00	
				13:00	14:00	15.20	0.15	2.81	2.58	0.08	2.58	1.98	0.30	0.15	0.00	0.15	0.76	1.52	1.60	0.53	0.00	0.00	
				14:00	15:00	11.40	0.08	2.05	2.96	0.08	1.98	1.29	0.15	0.08	0.00	0.23	0.38	0.91	0.91	0.30	0.00	0.00	
				15:00	16:00	15.20	0.15	3.27	4.03	0.08	2.58	1.82	0.30	0.08	0.00	0.53	0.38	0.84	0.84	0.30	0.00	0.00	
				16:00	17:00	11.40	0.30	3.34	2.51	0.08	1.52	0.68	0.08	0.08	0.00	0.08	0.46	0.91	1.06	0.30	0.00	0.00	
				17:00	18:00	11.40	0.38	4.26	2.66	0.08	0.99	1.06	0.15	0.15	0.00	0.23	0.23	0.53	0.53	0.15	0.00	0.00	
				18:00	19:00	11.40	0.38	4.26	2.81	0.08	0.76	0.68	0.08	0.08	0.00	0.23	0.38	0.68	0.76	0.23	0.00	0.00	
				19:00	20:00	11.40	0.23	4.94	4.26	0.00	0.38	0.30	0.00	0.00	0.00	0.23	0.23	0.38	0.38	0.08	0.00	0.00	
				20:00	21:00	7.60	0.15	2.96	3.80	0.00	0.23	0.08	0.00	0.00	0.00	0.08	0.08	0.08	0.08	0.08	0.00	0.00	
				21:00	22:00	7.60	0.08	2.66	4.26	0.00	0.30	0.23	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	
				22:00	23:00	7.60	0.23	2.05	4.56	0.00	0.23	0.00	0.00	0.00	0.00	0.08	0.08	0.23	0.08	0.08	0.00	0.00	
L018	Yuk Yat Street (SB)	Shun Fung Street	Lok Shan road	23:00	0:00	7.60	0.23	2.05	4.26	0.00	0.23	0.00	0.00	0.00	0.00	0.15	0.15	0.23	0.15	0.15	0.00	0.00	
				0:00	1:00	7.60	0.38	3.57	2.89	0.00	0.30	0.00	0.00	0.00	0.08	0.08	0.15	0.08	0.00	0.00	0.00		
				1:00	2:00	3.80	0.23	1.82	1.60	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.80	0.15	1.82	1.75	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.80	0.23	1.82	1.52	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.80	0.30	1.75	1.60	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.80	0.23	1.44	1.60	0.00	0.23	0.00	0.00	0.00	0.00	0.08	0.08	0.08	0.08	0.00	0.00	0.00	
				6:00	7:00	7.60	0.30	2.89	2.74	0.00	0.46	0.08	0.00	0.00	0.08	0.23	0.23	0.30	0.30	0.00	0.00	0.00	
				7:00	8:00	15.20	0.15	6.46	4.94	0.08	0.99	0.23	0.08	0.00	0.00	0.46	0.46	0.68	0.68	0.00	0.00	0.00	
				8:00	9:00	26.60	0.61	13.68	5.40	0.08	2.36	1.22	0.23	0.15	0.00	0.61	0.61	0.84	0.84	0.00	0.00	0.00	
				9:00	10:00	22.80	0.38	9.80	5.17	0.08	2.05	1.29	0.23	0.15	0.00	0.84	0.68	1.06	1.06	0.00	0.00	0.00	
				10:00	11:00	19.00	0.84	8.44	3.42	0.08	3.34	1.06	0.23	0.15	0.15	0.23	0.23	0.38	0.38	0.08	0.00	0.00	
				11:00	12:00	19.00	0.53	8.28	2.96	0.15	4.18	0.38	0.08	0.08	0.23	0.61	0.30	0.53	0.61	0.08	0.00	0.00	
				12:00	13:00	19.00	0.61	10.26	3.50	0.08	2.58	0.38	0.08	0.08	0.15	0.61	0.15	0.23	0.30	0.00	0.00	0.00	
				13:00	14:00	19.00	0.46	7.14	2.43	0.08	3.42	1.37	0.30	0.23	0.15	0.15	0.76	1.14	1.22	0.15	0.00	0.00	
				14:00	15:00	19.00	0.46	7.14	3.57	0.08	3.42	1.14	0.30	0.15	0.15	0.38	0.53	0.76	0.84	0.08	0.00	0.00	
				15:00	16:00	22.80	0.76	9.35	4.26	0.08	3.72	1.37	0.38	0.23	0.15	0.61	0.46	0.68	0.68	0.08	0.00	0.00	
				16:00	17:00	19.00	1.06	10.64	1.98	0.08	2.89	0.30	0.08	0.08	0.38	0.08	0.23	0.46	0.53	0.23	0.00	0.00	
				17:00	18:00	26.60	1.82	16.95	2.66	0.08	2.43	0.68	0.15	0.15	0.46	0.15	0.15	0.38	0.38	0.15	0.00	0.00	
				18:00	19:00	22.80	1.60	15.35	2.43	0.08	1.60	0.38	0.08	0.08	0.15	0.08	0.15	0.30	0.38	0.15	0.00	0.00	
				19:00	20:00	19.00	0.68	12.77	3.95	0.00	0.46	0.15	0.00	0.00	0.15	0.15	0.15	0.23	0.30	0.00	0.00	0.00	
				20:00	21:00	15.20	0.68	9.35	4.26	0.00	0.30	0.08	0.00	0.00	0.23	0.08	0.08	0.08	0.08	0.00	0.00	0.00	
				21:00	22:00	11.40	0.38	6.46	3.80	0.00	0.46	0.08	0.00	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	11.40	0.61	5.70	4.26	0.00	0.23	0.00	0.00	0.00	0.23	0.08	0.08	0.15	0.08	0.00	0.00	0.00					
23:00	0:00	11.40	0.61	5.47	4.26	0.00	0.30	0.00	0.00	0.00	0.30	0.08	0.08	0.23	0.08	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L019	Lok Shan road (WB)	Yuk Yat Street	Mei King Street	0:00	1:00	10.60	1.17	4.03	3.39	0.00	0.53	0.00	0.00	0.00	0.00	0.21	0.21	0.53	0.32	0.21	0.00	0.00	
				1:00	2:00	5.30	0.64	2.33	2.01	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.30	0.53	2.23	2.23	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.30	0.64	2.44	1.80	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.30	0.74	2.33	2.01	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.30	0.53	1.59	1.59	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.21	0.21	0.32	0.32	0.21	0.00	0.00
				6:00	7:00	10.60	0.74	2.97	2.76	0.00	0.85	0.21	0.00	0.00	0.00	0.32	0.32	1.06	0.85	0.53	0.00	0.00	
				7:00	8:00	15.90	0.85	6.15	2.97	0.11	1.27	0.32	0.00	0.00	0.00	0.53	0.53	1.27	1.27	0.64	0.00	0.00	
				8:00	9:00	31.80	2.76	13.89	3.60	0.11	3.71	2.01	0.21	0.11	0.00	0.74	0.74	1.59	1.59	0.74	0.00	0.00	
				9:00	10:00	26.50	1.80	9.43	3.29	0.11	3.18	1.91	0.21	0.11	0.00	1.06	0.74	1.80	1.91	0.95	0.00	0.00	
				10:00	11:00	26.50	2.01	8.27	3.29	0.11	5.72	2.54	0.21	0.11	0.00	0.53	0.64	1.17	1.27	0.64	0.00	0.00	
				11:00	12:00	26.50	1.27	7.84	2.76	0.21	7.00	0.95	0.11	0.11	0.00	1.27	0.74	1.59	1.91	0.74	0.00	0.00	
				12:00	13:00	26.50	1.80	11.13	3.60	0.11	4.88	0.85	0.11	0.11	0.00	1.48	0.42	0.74	0.85	0.42	0.00	0.00	
				13:00	14:00	31.80	1.27	7.53	2.44	0.11	6.04	3.18	0.42	0.21	0.00	0.42	1.59	3.39	3.71	1.48	0.00	0.00	
				14:00	15:00	26.50	1.06	6.78	3.29	0.11	5.41	2.44	0.21	0.11	0.00	0.64	0.95	2.23	2.33	0.95	0.00	0.00	
				15:00	16:00	31.80	1.80	9.22	4.03	0.11	6.25	3.07	0.42	0.11	0.00	1.27	0.85	1.80	2.01	0.85	0.00	0.00	
				16:00	17:00	31.80	2.01	10.81	3.18	0.11	5.41	1.27	0.11	0.11	0.00	0.21	1.38	2.86	3.18	1.17	0.00	0.00	
				17:00	18:00	31.80	2.65	14.10	3.50	0.11	3.71	2.01	0.21	0.11	0.00	0.74	0.74	1.59	1.59	0.74	0.00	0.00	
				18:00	19:00	31.80	2.65	14.31	3.71	0.11	2.86	1.27	0.11	0.11	0.00	0.64	0.95	2.01	2.23	0.85	0.00	0.00	
				19:00	20:00	21.20	1.70	12.30	3.60	0.00	0.74	0.32	0.00	0.00	0.00	0.42	0.32	0.74	0.74	0.32	0.00	0.00	
				20:00	21:00	15.90	1.59	8.90	3.92	0.00	0.64	0.11	0.00	0.00	0.00	0.11	0.11	0.21	0.21	0.11	0.00	0.00	
				21:00	22:00	15.90	1.27	8.59	4.66	0.00	0.85	0.32	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.11	0.00	0.00	
				22:00	23:00	10.60	1.17	4.66	3.39	0.00	0.32	0.00	0.00	0.00	0.00	0.21	0.11	0.32	0.21	0.21	0.00	0.00	
				23:00	0:00	10.60	1.27	4.35	3.39	0.00	0.42	0.00	0.00	0.00	0.00	0.21	0.11	0.42	0.21	0.21	0.00	0.00	
L020	Lok Shan road (WB)	Mei King Street	To Kwa Wan Road	0:00	1:00	5.85	0.31	1.79	3.32	0.00	0.12	0.00	0.00	0.00	0.00	0.04	0.04	0.12	0.04	0.08	0.00	0.00	
				1:00	2:00	3.90	0.23	1.25	2.30	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.90	0.20	1.17	2.46	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.95	0.12	0.62	1.13	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.95	0.08	0.59	1.25	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.90	0.16	1.01	2.03	0.00	0.16	0.00	0.00	0.00	0.00	0.08	0.08	0.16	0.08	0.16	0.00	0.00	
				6:00	7:00	5.85	0.23	1.37	3.04	0.00	0.23	0.08	0.00	0.00	0.00	0.12	0.12	0.27	0.23	0.16	0.00	0.00	
				7:00	8:00	11.70	0.35	4.17	4.72	0.04	0.51	0.12	0.00	0.00	0.00	0.20	0.20	0.59	0.47	0.35	0.00	0.00	
				8:00	9:00	17.55	0.94	8.11	4.72	0.04	1.13	0.59	0.08	0.08	0.00	0.23	0.23	0.59	0.47	0.35	0.00	0.00	
				9:00	10:00	17.55	0.74	6.71	5.27	0.04	1.17	0.70	0.08	0.08	0.00	0.39	0.31	0.82	0.70	0.55	0.00	0.00	
				10:00	11:00	13.65	0.70	4.56	4.17	0.04	1.83	0.82	0.08	0.08	0.00	0.12	0.20	0.43	0.35	0.27	0.00	0.00	
				11:00	12:00	13.65	0.47	4.52	3.63	0.08	2.42	0.31	0.04	0.04	0.00	0.39	0.23	0.62	0.55	0.35	0.00	0.00	
				12:00	13:00	13.65	0.55	5.69	4.33	0.04	1.52	0.27	0.04	0.04	0.00	0.43	0.08	0.27	0.27	0.12	0.00	0.00	
				13:00	14:00	13.65	0.39	3.90	2.89	0.04	1.91	0.94	0.12	0.12	0.00	0.12	0.47	1.13	1.01	0.62	0.00	0.00	
				14:00	15:00	11.70	0.31	3.24	3.59	0.04	1.56	0.66	0.08	0.08	0.00	0.20	0.27	0.70	0.59	0.39	0.00	0.00	
				15:00	16:00	15.60	0.59	4.84	4.80	0.04	1.99	0.94	0.12	0.12	0.00	0.35	0.27	0.66	0.55	0.35	0.00	0.00	
				16:00	17:00	11.70	0.51	4.29	2.89	0.04	1.48	0.31	0.04	0.04	0.00	0.04	0.31	0.70	0.70	0.35	0.00	0.00	
				17:00	18:00	15.60	0.82	6.94	4.02	0.04	1.25	0.66	0.08	0.08	0.00	0.23	0.23	0.51	0.43	0.31	0.00	0.00	
				18:00	19:00	13.65	0.70	6.24	3.67	0.04	0.82	0.35	0.04	0.04	0.00	0.16	0.23	0.55	0.55	0.27	0.00	0.00	
				19:00	20:00	13.65	0.62	6.86	4.80	0.00	0.27	0.16	0.00	0.00	0.00	0.16	0.08	0.27	0.27	0.16	0.00	0.00	
				20:00	21:00	9.75	0.51	4.37	4.41	0.00	0.16	0.04	0.00	0.00	0.00	0.04	0.04	0.08	0.04	0.08	0.00	0.00	
				21:00	22:00	9.75	0.39	3.90	5.03	0.00	0.27	0.08	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				22:00	23:00	7.80	0.39	2.54	4.37	0.00	0.12	0.00	0.00	0.00	0.00	0.08	0.04	0.12	0.04	0.12	0.00	0.00	
				23:00	0:00	7.80	0.39	2.42	4.33	0.00	0.16	0.00	0.00	0.00	0.00	0.12	0.04	0.16	0.04	0.16	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L021	To Kwa Wan Road (SB)	Lok Shan road	Chi Kiang Street	0:00	1:00	55.20	1.79	11.59	30.64	0.00	1.10	0.00	0.00	0.00	3.59	0.28	0.28	0.69	0.41	0.28	0.00	4.55	
				1:00	2:00	27.60	0.97	6.62	18.63	0.00	0.83	0.00	0.00	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	27.60	0.97	6.07	19.46	0.00	0.55	0.00	0.00	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	20.70	0.97	5.11	13.66	0.00	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	20.70	0.69	4.55	14.63	0.00	0.55	0.00	0.00	0.00	0.00	0.14	0.00	0.14	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	20.70	0.55	4.00	13.25	0.00	1.10	0.00	0.00	0.00	0.00	0.28	0.28	0.55	0.41	0.28	0.00	0.00	0.00
				6:00	7:00	48.30	1.10	7.45	23.87	0.14	1.93	0.28	0.00	0.00	3.45	0.69	0.83	1.38	1.38	0.69	0.00	5.11	
				7:00	8:00	117.30	1.38	24.29	39.19	0.55	6.07	0.55	0.28	0.28	13.80	1.38	1.24	2.90	2.76	1.24	0.00	21.39	
				8:00	9:00	158.70	4.14	50.78	41.54	0.97	14.90	3.17	1.10	1.10	10.90	1.66	1.66	3.31	3.17	1.24	0.00	19.04	
				9:00	10:00	144.90	2.76	37.54	40.99	0.83	13.80	3.17	1.10	1.10	9.38	2.35	1.93	4.00	4.00	1.79	0.00	20.15	
				10:00	11:00	131.10	4.00	29.95	39.19	1.38	18.49	5.11	1.66	1.66	6.21	0.97	1.24	2.35	2.35	0.83	0.00	15.73	
				11:00	12:00	138.00	2.62	29.67	34.50	1.66	23.87	2.07	0.69	0.55	11.32	3.04	1.79	3.45	3.73	1.10	0.00	17.94	
				12:00	13:00	131.10	3.17	37.54	41.54	1.24	15.18	1.66	0.69	0.55	8.28	3.31	0.69	1.38	1.66	0.55	0.00	13.66	
				13:00	14:00	131.10	2.35	24.98	27.32	1.38	18.63	6.21	1.93	2.07	7.73	0.83	3.17	6.35	6.76	2.21	0.00	19.18	
				14:00	15:00	131.10	1.93	23.87	38.36	1.38	17.66	4.83	1.52	1.52	8.56	1.38	2.07	4.42	4.55	1.52	0.00	17.53	
				15:00	16:00	151.80	3.31	32.15	45.82	1.38	19.73	6.07	1.93	1.93	7.73	2.76	1.79	3.86	3.86	1.38	0.00	18.08	
				16:00	17:00	131.10	4.28	34.50	36.02	0.97	12.97	2.62	0.69	0.69	5.93	0.55	2.76	5.11	5.93	1.52	0.00	16.56	
				17:00	18:00	158.70	6.49	52.03	45.68	0.83	10.35	5.24	1.79	1.79	6.21	1.79	1.79	3.31	3.31	1.10	0.00	16.97	
				18:00	19:00	138.00	5.93	48.85	43.47	0.41	7.31	3.04	0.97	0.97	2.48	1.24	2.21	3.73	4.14	1.10	0.00	12.14	
				19:00	20:00	124.20	3.45	44.71	44.30	0.14	2.76	0.69	0.14	0.28	8.28	0.97	0.69	1.52	1.66	0.55	0.00	14.08	
				20:00	21:00	110.40	3.04	31.05	46.23	0.14	1.93	0.28	0.14	0.14	11.59	0.41	0.28	0.55	0.41	0.28	0.00	13.94	
				21:00	22:00	96.60	1.93	23.32	44.16	0.14	2.35	0.55	0.14	0.14	9.25	0.00	0.14	0.28	0.00	0.14	0.00	14.08	
				22:00	23:00	96.60	2.48	17.53	43.75	0.14	1.24	0.00	0.00	0.00	9.80	0.55	0.41	0.69	0.41	0.41	0.00	19.18	
				23:00	0:00	82.80	2.35	14.49	37.40	0.14	1.52	0.00	0.00	0.00	8.14	0.55	0.41	0.69	0.41	0.41	0.00	16.28	
L022	To Kwa Wan Road (NB)	Chi Kiang Street	Lok Shan road	0:00	1:00	27.60	0.41	4.28	13.66	0.00	0.28	0.00	0.00	0.00	3.59	0.14	0.14	0.28	0.14	0.14	0.00	4.55	
				1:00	2:00	13.80	0.41	3.04	9.94	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	13.80	0.41	2.90	10.21	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	6.90	0.28	1.52	4.97	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	6.90	0.14	1.38	5.24	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	13.80	0.28	2.35	9.25	0.00	0.55	0.00	0.00	0.00	0.00	0.28	0.28	0.28	0.28	0.28	0.00	0.00	
				6:00	7:00	27.60	0.41	3.04	11.73	0.00	0.55	0.41	0.00	0.00	3.59	0.14	0.41	0.55	0.55	0.41	0.00	5.80	
				7:00	8:00	62.10	0.69	7.87	16.84	0.14	0.41	0.97	0.14	0.14	8.97	0.41	0.41	0.97	0.97	0.69	0.00	22.49	
				8:00	9:00	69.00	2.07	15.18	16.42	0.14	1.10	4.42	0.83	0.83	6.49	0.41	0.41	0.97	0.83	0.55	0.00	18.35	
				9:00	10:00	69.00	1.66	11.73	17.11	0.14	1.24	4.83	0.97	0.97	5.80	0.69	0.55	1.38	1.24	0.69	0.00	20.01	
				10:00	11:00	62.10	1.10	10.21	16.15	0.28	4.69	4.42	1.38	1.38	5.38	0.28	0.41	0.83	0.83	0.41	0.00	14.35	
				11:00	12:00	69.00	0.83	10.76	14.90	0.55	6.62	1.79	0.69	0.55	10.49	1.10	0.69	1.38	1.24	0.69	0.00	16.70	
				12:00	13:00	62.10	0.83	13.25	17.66	0.28	4.14	1.66	0.55	0.41	7.45	1.24	0.28	0.55	0.55	0.28	0.00	12.97	
				13:00	14:00	69.00	0.83	9.11	11.87	0.28	5.11	5.52	1.79	1.79	7.31	0.28	1.24	2.35	2.35	1.24	0.00	17.94	
				14:00	15:00	69.00	0.69	8.83	16.84	0.28	4.97	4.42	1.38	1.38	8.14	0.55	0.83	1.66	1.52	0.83	0.00	16.70	
				15:00	16:00	75.90	1.10	11.59	19.46	0.28	5.38	5.52	1.79	1.66	7.18	0.97	0.55	1.38	1.24	0.55	0.00	17.25	
				16:00	17:00	69.00	0.69	13.66	15.04	0.28	6.90	1.66	0.69	0.55	8.28	0.14	1.24	2.21	2.35	0.69	0.00	14.63	
				17:00	18:00	82.80	0.97	21.11	20.01	0.28	5.52	3.04	1.52	1.52	8.83	0.69	0.69	1.52	1.38	0.55	0.00	15.18	
				18:00	19:00	69.00	0.97	19.73	19.60	0.14	4.14	1.79	0.97	0.97	3.73	0.69	0.97	1.66	1.66	0.69	0.00	11.32	
				19:00	20:00	62.10	1.10	16.28	19.32	0.00	0.69	0.69	0.14	0.14	7.87	0.28	0.28	0.69	0.69	0.28	0.00	13.66	
				20:00	21:00	55.20	0.83	10.76	18.91	0.00	0.55	0.28	0.14	0.14	10.07	0.14	0.14	0.14	0.14	0.14	0.00	12.83	
				21:00	22:00	55.20	0.55	8.97	20.56	0.00	0.55	0.41	0.14	0.14	9.11	0.00	0.00	0.14	0.00	0.00	0.00	14.63	
				22:00	23:00	55.20	0.69	6.49	19.18	0.00	0.28	0.00	0.00	0.00	8.97	0.14	0.14	0.28	0.14	0.14	0.00	18.77	
				23:00	0:00	48.30	0.83	5.38	16.84	0.00	0.28	0.00	0.00	0.00	7.87	0.14	0.14	0.28	0.14	0.14	0.00	16.28	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L023	Yuk Yat Street (NB)	Chi Kiang Street	Lok Shan road	0:00	1:00	12.00	0.32	3.12	6.64	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.24	0.24	0.48	0.32	0.24	0.00	0.00
				1:00	2:00	8.00	0.24	2.32	5.04	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	8.00	0.24	2.08	5.44	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.00	0.16	1.12	2.48	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.00	0.08	1.04	2.56	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				5:00	6:00	8.00	0.16	1.60	3.92	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.16	0.32	0.56	0.48	0.32	0.00	0.00
				6:00	7:00	12.00	0.24	2.16	5.52	0.00	0.64	0.24	0.00	0.00	0.00	0.48	0.48	1.04	0.88	0.32	0.00	0.00	
				7:00	8:00	24.00	0.24	5.84	9.20	0.08	1.76	0.56	0.08	0.00	0.00	0.96	0.88	1.92	1.84	0.64	0.00	0.00	
				8:00	9:00	36.00	0.72	11.76	9.36	0.16	4.08	2.48	0.40	0.16	0.00	1.04	1.04	2.08	2.00	0.72	0.00	0.00	
				9:00	10:00	36.00	0.56	9.20	9.68	0.16	3.92	2.56	0.40	0.16	0.00	1.68	1.36	2.64	2.80	0.88	0.00	0.00	
				10:00	11:00	28.00	0.72	6.80	7.20	0.16	5.20	3.12	0.40	0.32	0.00	0.48	0.56	1.28	1.28	0.48	0.00	0.00	
				11:00	12:00	28.00	0.48	6.56	6.16	0.24	6.56	1.20	0.16	0.08	0.00	1.44	0.80	1.76	2.00	0.56	0.00	0.00	
				12:00	13:00	28.00	0.64	8.96	8.08	0.16	4.56	1.12	0.16	0.08	0.00	1.76	0.48	0.80	0.96	0.24	0.00	0.00	
				13:00	14:00	32.00	0.40	5.84	5.12	0.16	5.36	3.84	0.48	0.40	0.00	0.40	1.68	3.52	3.68	1.12	0.00	0.00	
				14:00	15:00	28.00	0.32	5.28	6.96	0.16	4.88	2.80	0.32	0.24	0.00	0.72	1.04	2.24	2.32	0.72	0.00	0.00	
				15:00	16:00	32.00	0.56	6.96	8.08	0.16	5.28	3.52	0.48	0.32	0.00	1.28	0.88	1.92	1.92	0.64	0.00	0.00	
				16:00	17:00	28.00	0.96	8.16	5.76	0.08	4.08	1.44	0.16	0.16	0.00	0.16	1.20	2.40	2.72	0.72	0.00	0.00	
				17:00	18:00	32.00	1.28	12.08	7.04	0.08	3.12	2.72	0.32	0.32	0.00	0.72	0.72	1.52	1.52	0.56	0.00	0.00	
				18:00	19:00	32.00	1.28	12.48	7.44	0.08	2.40	1.68	0.16	0.16	0.00	0.64	0.96	1.92	2.16	0.64	0.00	0.00	
				19:00	20:00	24.00	0.64	10.56	8.48	0.00	0.88	0.40	0.00	0.08	0.00	0.48	0.40	0.88	0.96	0.24	0.00	0.00	
				20:00	21:00	20.00	0.64	7.92	9.44	0.00	0.64	0.16	0.00	0.00	0.00	0.16	0.16	0.40	0.32	0.16	0.00	0.00	
				21:00	22:00	16.00	0.32	5.68	8.72	0.00	0.72	0.32	0.00	0.00	0.00	0.00	0.08	0.08	0.00	0.08	0.00	0.00	
				22:00	23:00	16.00	0.56	4.56	9.20	0.00	0.40	0.00	0.00	0.00	0.00	0.24	0.16	0.48	0.24	0.16	0.00	0.00	
				23:00	0:00	12.00	0.40	3.28	6.72	0.00	0.40	0.00	0.00	0.00	0.00	0.24	0.16	0.40	0.24	0.16	0.00	0.00	
L024	Yuk Yat Street (SB)	Lok Shan road	Chi Kiang Street	0:00	1:00	8.00	0.32	4.40	2.96	0.00	0.24	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	4.00	0.16	2.16	1.52	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.00	0.08	2.16	1.68	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.00	0.16	2.08	1.52	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.00	0.16	2.08	1.60	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	4.00	0.16	2.00	1.60	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	8.00	0.32	3.84	3.12	0.00	0.40	0.16	0.00	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	16.00	0.00	9.36	5.52	0.00	0.72	0.32	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	24.00	0.00	15.92	4.80	0.00	1.36	1.36	0.40	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	20.00	0.00	12.00	4.72	0.00	1.28	1.44	0.40	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	20.00	0.64	10.72	3.52	0.00	3.36	1.04	0.40	0.16	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	20.00	0.40	10.88	3.28	0.00	4.40	0.40	0.24	0.08	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	20.00	0.48	12.56	3.60	0.00	2.64	0.32	0.16	0.08	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	16.00	0.32	8.40	2.24	0.00	3.12	1.12	0.40	0.24	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	16.00	0.24	8.08	3.20	0.00	2.96	0.88	0.32	0.16	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	20.00	0.56	10.40	3.84	0.00	3.28	1.04	0.48	0.24	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	20.00	1.12	12.48	2.00	0.00	3.76	0.00	0.08	0.08	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	24.00	1.52	16.88	2.24	0.00	2.64	0.00	0.16	0.16	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	24.00	1.52	17.76	2.32	0.00	2.08	0.00	0.08	0.08	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	20.00	0.56	14.96	3.76	0.00	0.40	0.16	0.00	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	16.00	0.48	10.72	4.08	0.00	0.40	0.08	0.00	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	12.00	0.24	7.52	3.52	0.00	0.40	0.08	0.00	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	12.00	0.40	6.72	4.24	0.00	0.32	0.00	0.00	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	8.00	0.32	4.40	2.80	0.00	0.24	0.00	0.00	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L025	Chi Kiang Street (SB)	Yuk Yat Street	Bailey Street	0:00	1:00	16.85	0.00	9.44	0.00	0.00	1.01	0.00	0.00	0.00	0.00	0.00	1.01	1.01	1.69	1.01	1.69	0.00	0.00	
				1:00	2:00	16.85	0.00	14.83	0.00	0.00	2.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	16.85	0.00	16.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	16.85	0.00	14.15	0.00	0.00	2.70	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	16.85	0.00	16.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	16.85	0.00	5.39	0.00	0.00	1.69	0.00	0.00	0.00	0.00	0.00	1.69	1.69	1.69	1.69	3.03	0.00	0.00	0.00
				6:00	7:00	16.85	0.00	6.07	0.00	0.00	0.67	0.67	0.00	0.00	0.00	0.67	1.69	2.36	2.36	2.36	0.00	0.00	0.00	
				7:00	8:00	33.70	0.00	17.52	0.00	0.00	0.67	1.01	0.67	0.67	0.00	1.69	1.69	3.03	3.37	3.37	0.00	0.00	0.00	
				8:00	9:00	50.55	0.00	30.67	0.00	0.00	1.35	5.06	1.35	1.35	0.00	1.35	1.35	2.70	2.70	2.70	0.00	0.00	0.00	
				9:00	10:00	50.55	0.00	23.93	0.00	0.00	1.35	6.07	1.35	1.35	0.00	2.36	2.02	4.04	4.04	4.04	0.00	0.00	0.00	
				10:00	11:00	33.70	0.00	12.81	0.00	0.00	5.73	4.72	0.67	0.67	0.00	0.67	1.35	2.36	2.36	2.36	0.00	0.00	0.00	
				11:00	12:00	33.70	0.00	11.46	0.00	0.00	6.74	1.69	0.34	0.34	0.00	2.70	1.69	3.03	3.03	2.70	0.00	0.00	0.00	
				12:00	13:00	33.70	0.00	17.86	0.00	0.00	4.72	1.69	0.34	0.34	0.00	3.37	1.01	1.35	1.69	1.35	0.00	0.00	0.00	
				13:00	14:00	50.55	0.00	11.80	0.00	0.00	6.07	6.07	1.01	1.01	0.00	1.01	3.71	6.74	7.08	6.07	0.00	0.00	0.00	
				14:00	15:00	33.70	0.00	9.77	0.00	0.00	4.72	4.04	0.67	0.67	0.00	1.01	2.02	3.71	3.71	3.37	0.00	0.00	0.00	
				15:00	16:00	33.70	0.00	11.46	0.00	0.00	5.06	4.38	0.67	0.67	0.00	2.02	1.35	2.70	2.70	2.70	0.00	0.00	0.00	
				16:00	17:00	50.55	0.00	14.15	0.00	0.00	9.77	2.02	0.00	0.00	0.00	0.67	3.71	6.74	7.75	5.73	0.00	0.00	0.00	
				17:00	18:00	50.55	0.00	21.23	0.00	0.00	7.75	3.71	0.00	0.00	0.00	2.36	2.36	4.38	4.38	4.38	0.00	0.00	0.00	
				18:00	19:00	33.70	0.00	14.83	0.00	0.00	3.71	1.69	0.00	0.00	0.00	1.35	2.02	3.37	3.71	3.03	0.00	0.00	0.00	
				19:00	20:00	33.70	0.00	23.93	0.00	0.00	1.01	1.01	0.00	0.00	0.00	1.01	1.01	2.02	2.02	1.69	0.00	0.00	0.00	
				20:00	21:00	16.85	0.00	13.82	0.00	0.00	0.34	0.34	0.00	0.00	0.00	0.34	0.34	0.34	0.34	1.01	0.00	0.00	0.00	
				21:00	22:00	16.85	0.00	13.82	0.00	0.00	1.01	0.67	0.00	0.00	0.00	0.00	0.00	0.67	0.00	0.67	0.00	0.00	0.00	
				22:00	23:00	16.85	0.00	11.46	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.67	0.67	1.35	0.67	1.35	0.00	0.00	0.00	
				23:00	0:00	16.85	0.00	11.46	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.67	0.67	1.35	0.67	1.35	0.00	0.00	0.00	
L026	Chi Kiang Street (NB)	Bailey Street	Yuk Yat Street	0:00	1:00	16.50	0.00	9.90	0.00	0.00	1.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.65	1.65	1.65	0.00	0.00	
				1:00	2:00	16.50	0.00	16.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	16.50	0.00	16.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	16.50	0.00	16.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	16.50	0.00	16.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	16.50	0.00	8.58	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	2.64	2.64	2.64	0.00	0.00	0.00	
				6:00	7:00	16.50	0.00	4.62	0.00	0.00	1.32	0.00	0.00	0.00	0.00	1.32	1.32	2.64	2.64	2.64	0.00	0.00	0.00	
				7:00	8:00	16.50	0.00	8.91	0.00	0.00	0.00	0.66	0.00	0.00	0.00	0.99	0.99	1.65	1.65	1.65	0.00	0.00	0.00	
				8:00	9:00	33.00	0.00	19.80	0.00	0.00	0.66	3.63	0.66	0.66	0.00	1.32	1.32	1.65	1.65	1.65	0.00	0.00	0.00	
				9:00	10:00	33.00	0.00	15.84	0.00	0.00	0.66	3.96	0.66	0.66	0.00	1.98	1.32	2.64	2.64	2.64	0.00	0.00	0.00	
				*0:00	11:00	33.00	0.00	11.88	0.00	0.00	5.61	4.29	0.66	0.66	0.00	0.66	1.32	2.64	2.64	2.64	0.00	0.00	0.00	
				*1:00	12:00	33.00	0.00	11.22	0.00	0.00	6.60	1.98	0.00	0.00	0.00	2.31	1.32	2.97	3.63	2.97	0.00	0.00	0.00	
				*2:00	13:00	16.50	0.00	8.91	0.00	0.00	2.64	0.66	0.00	0.00	0.00	1.65	0.33	0.66	0.99	0.66	0.00	0.00	0.00	
				*3:00	14:00	33.00	0.00	7.26	0.00	0.00	4.29	4.29	0.33	0.33	0.00	0.33	2.31	4.62	4.95	4.29	0.00	0.00	0.00	
				*4:00	15:00	33.00	0.00	8.58	0.00	0.00	4.62	3.96	0.66	0.66	0.00	1.32	1.65	3.96	3.96	3.63	0.00	0.00	0.00	
				*5:00	16:00	33.00	0.00	9.90	0.00	0.00	4.62	4.29	0.66	0.66	0.00	1.98	1.65	3.30	3.30	2.64	0.00	0.00	0.00	
				*6:00	17:00	33.00	0.00	9.24	0.00	0.00	5.94	1.32	0.00	0.00	0.00	0.33	2.31	4.62	5.28	3.96	0.00	0.00	0.00	
				*7:00	18:00	33.00	0.00	14.19	0.00	0.00	4.62	2.64	0.00	0.00	0.00	1.32	1.32	2.97	2.97	2.97	0.00	0.00	0.00	
				*8:00	19:00	33.00	0.00	13.86	0.00	0.00	3.63	1.32	0.00	0.00	0.00	0.99	1.98	3.63	4.29	3.30	0.00	0.00	0.00	
				*9:00	20:00	16.50	0.00	11.88	0.00	0.00	0.33	0.33	0.00	0.00	0.00	0.33	0.33	0.99	1.32	0.99	0.00	0.00	0.00	
				20:00	21:00	16.50	0.00	13.20	0.00	0.00	0.66	0.00	0.00	0.00	0.00	0.66	0.00	0.66	0.66	0.66	0.00	0.00	0.00	
				21:00	22:00	16.50	0.00	13.53	0.00	0.00	0.99	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.99	0.00	0.00	0.00	
				22:00	23:00	16.50	0.00	9.90	0.00	0.00	0.99	0.00	0.00	0.00	0.00	0.99	0.99	0.99	0.99	1.65	0.00	0.00	0.00	
				23:00	0:00	16.50	0.00	9.57	0.00	0.00	0.99	0.00	0.00	0.00	0.00	0.99	0.99	0.99	0.99	1.98	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L027	Chi Kiang Street (EB)	Lung Tak Street	Yuk Yat Street	0:00	1:00	16.20	0.43	4.32	8.21	0.00	0.65	0.00	0.00	0.00	1.51	0.11	0.11	0.43	0.32	0.11	0.00	0.00
				1:00	2:00	10.80	0.32	3.35	6.59	0.00	0.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	10.80	0.43	3.13	6.80	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.40	0.22	1.62	3.13	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.40	0.11	1.51	3.46	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.40	0.11	1.19	2.92	0.00	0.43	0.00	0.00	0.00	0.00	0.11	0.11	0.22	0.22	0.11	0.00	0.00
				6:00	7:00	16.20	0.32	3.02	7.02	0.00	1.08	0.32	0.00	0.00	1.62	0.32	0.43	0.97	0.76	0.32	0.00	0.00
				7:00	8:00	32.40	0.54	8.53	10.26	0.11	2.48	0.65	0.11	0.11	4.21	0.76	0.76	1.62	1.62	0.65	0.00	0.00
				8:00	9:00	48.60	1.19	17.17	10.48	0.11	5.72	3.24	0.86	0.86	3.24	0.86	0.86	1.73	1.73	0.54	0.00	0.00
				9:00	10:00	43.20	0.86	12.10	10.04	0.11	5.08	3.24	0.86	0.86	2.59	1.30	1.08	2.05	2.16	0.86	0.00	0.00
				10:00	11:00	43.20	0.97	10.37	9.61	0.11	9.18	4.32	1.08	1.08	2.38	0.43	0.65	1.30	1.30	0.43	0.00	0.00
				11:00	12:00	43.20	0.65	9.72	8.21	0.22	11.02	1.73	0.43	0.32	4.21	1.40	0.86	1.84	1.94	0.65	0.00	0.00
				12:00	13:00	37.80	0.65	12.10	9.50	0.11	6.91	1.40	0.43	0.32	2.92	1.51	0.32	0.65	0.76	0.22	0.00	0.00
				13:00	14:00	43.20	0.54	7.99	6.26	0.11	8.53	4.86	1.30	1.30	2.81	0.43	1.51	3.13	3.46	0.97	0.00	0.00
				14:00	15:00	43.20	0.43	8.10	9.29	0.11	8.64	3.89	0.97	0.97	3.35	0.76	1.08	2.38	2.48	0.76	0.00	0.00
				15:00	16:00	48.60	0.76	10.69	10.69	0.11	9.29	4.86	1.30	1.30	2.92	1.30	0.97	1.94	1.94	0.54	0.00	0.00
				16:00	17:00	43.20	0.86	11.56	8.42	0.11	8.10	1.94	0.32	0.43	3.13	0.22	1.30	2.81	3.13	0.86	0.00	0.00
				17:00	18:00	48.60	1.30	16.74	10.15	0.11	6.16	3.56	0.97	0.97	3.13	0.76	0.76	1.73	1.73	0.54	0.00	0.00
				18:00	19:00	43.20	1.19	15.98	10.04	0.11	4.54	2.16	0.65	0.65	1.30	0.65	0.97	2.05	2.27	0.65	0.00	0.00
				19:00	20:00	32.40	0.76	14.26	9.94	0.00	1.19	0.54	0.11	0.11	3.02	0.43	0.32	0.76	0.76	0.22	0.00	0.00
				20:00	21:00	27.00	0.76	9.83	10.26	0.00	0.97	0.22	0.11	0.11	4.00	0.11	0.11	0.22	0.22	0.11	0.00	0.00
				21:00	22:00	27.00	0.54	8.64	11.77	0.00	1.30	0.43	0.11	0.11	3.89	0.00	0.00	0.11	0.00	0.11	0.00	0.00
				22:00	23:00	21.60	0.54	5.62	10.26	0.00	0.54	0.00	0.00	0.00	3.46	0.22	0.22	0.32	0.22	0.22	0.00	0.00
				23:00	0:00	21.60	0.65	5.51	9.61	0.00	0.65	0.00	0.00	0.00	3.46	0.32	0.32	0.43	0.32	0.32	0.00	0.00
L028	Chi Kiang Street (EB)	To Kwa Wan Road	Lung Tak Street	0:00	1:00	4.30	0.09	1.03	2.19	0.00	0.17	0.00	0.00	0.00	0.52	0.04	0.04	0.13	0.04	0.04	0.00	0.00
				1:00	2:00	2.15	0.09	0.60	1.33	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	0.04	0.56	1.46	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.09	0.60	1.25	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.09	0.56	1.33	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.04	0.47	1.16	0.00	0.17	0.00	0.00	0.00	0.00	0.04	0.04	0.13	0.04	0.04	0.00	0.00
				6:00	7:00	4.30	0.09	0.73	1.89	0.00	0.30	0.04	0.00	0.00	0.52	0.09	0.09	0.26	0.22	0.09	0.00	0.00
				7:00	8:00	10.75	0.17	2.49	3.31	0.04	0.90	0.22	0.04	0.04	1.76	0.26	0.26	0.52	0.52	0.22	0.00	0.00
				8:00	9:00	15.05	0.39	4.69	3.23	0.04	1.98	0.99	0.30	0.30	1.29	0.30	0.30	0.52	0.52	0.22	0.00	0.00
				9:00	10:00	12.90	0.26	3.31	2.92	0.04	1.72	0.99	0.26	0.26	0.99	0.39	0.30	0.60	0.65	0.22	0.00	0.00
				10:00	11:00	12.90	0.26	2.71	2.84	0.04	3.01	1.25	0.34	0.34	0.90	0.17	0.17	0.34	0.39	0.13	0.00	0.00
				11:00	12:00	12.90	0.17	2.54	2.32	0.09	3.61	0.47	0.13	0.13	1.51	0.43	0.26	0.52	0.56	0.17	0.00	0.00
				12:00	13:00	12.90	0.26	3.61	3.18	0.04	2.58	0.43	0.13	0.13	1.25	0.52	0.13	0.26	0.30	0.09	0.00	0.00
				13:00	14:00	15.05	0.22	2.41	2.15	0.04	3.23	1.59	0.47	0.47	1.20	0.13	0.56	1.03	1.16	0.39	0.00	0.00
				14:00	15:00	12.90	0.13	2.11	2.71	0.04	2.80	1.16	0.30	0.30	1.20	0.22	0.30	0.69	0.73	0.22	0.00	0.00
				15:00	16:00	15.05	0.22	2.88	3.31	0.04	3.14	1.46	0.47	0.39	1.12	0.39	0.26	0.60	0.60	0.17	0.00	0.00
				16:00	17:00	12.90	0.26	3.10	2.54	0.04	2.67	0.56	0.13	0.13	1.12	0.09	0.39	0.77	0.90	0.22	0.00	0.00
				17:00	18:00	15.05	0.39	4.73	3.18	0.04	2.11	1.08	0.34	0.30	1.16	0.26	0.26	0.52	0.52	0.17	0.00	0.00
				18:00	19:00	12.90	0.34	4.52	3.10	0.04	1.51	0.60	0.22	0.17	0.47	0.17	0.30	0.60	0.69	0.17	0.00	0.00
				19:00	20:00	10.75	0.30	4.21	3.35	0.00	0.47	0.17	0.04	0.04	1.25	0.13	0.13	0.26	0.30	0.09	0.00	0.00
				20:00	21:00	8.60	0.22	2.80	3.31	0.00	0.30	0.04	0.04	0.04	1.59	0.04	0.04	0.09	0.04	0.04	0.00	0.00
				21:00	22:00	8.60	0.17	2.45	3.66	0.00	0.47	0.17	0.04	0.04	1.55	0.00	0.00	0.04	0.00	0.00	0.00	0.00
				22:00	23:00	8.60	0.22	1.98	4.00	0.00	0.26	0.00	0.00	0.00	1.72	0.13	0.04	0.17	0.04	0.04	0.00	0.00
				23:00	0:00	6.45	0.22	1.42	2.88	0.00	0.26	0.00	0.00	0.00	1.29	0.09	0.04	0.17	0.04	0.04	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L029	Lung Tak Street (NB)	Ngan Hon Street	Chi Kiang Street	0:00	1:00	2.30	0.00	1.75	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.30	0.00	1.70	0.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.30	0.00	1.66	0.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.30	0.00	1.70	0.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.30	0.00	1.66	0.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.30	0.00	1.66	0.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.30	0.00	1.47	0.64	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.30	0.00	1.75	0.41	0.00	0.09	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.60	0.00	3.54	0.41	0.00	0.28	0.32	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.60	0.00	3.31	0.51	0.00	0.32	0.41	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	4.60	0.00	2.90	0.51	0.00	0.55	0.55	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	4.60	0.00	3.13	0.46	0.00	0.78	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	4.60	0.00	3.40	0.55	0.00	0.46	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	4.60	0.00	2.67	0.41	0.00	0.64	0.78	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	4.60	0.00	2.62	0.60	0.00	0.69	0.60	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*5:00	16:00	4.60	0.00	2.85	0.55	0.00	0.55	0.60	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*6:00	17:00	4.60	0.00	3.36	0.51	0.00	0.51	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*7:00	18:00	4.60	0.00	3.59	0.37	0.00	0.28	0.32	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*8:00	19:00	4.60	0.00	3.73	0.46	0.00	0.18	0.18	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*9:00	20:00	4.60	0.00	4.00	0.51	0.00	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.30	0.00	1.89	0.37	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.30	0.00	1.79	0.41	0.00	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.30	0.00	1.66	0.55	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.30	0.00	1.61	0.60	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L030	To Kwa Wan Road (SB)	Chi Kiang Street	Ngan Hon Street	0:00	1:00	27.00	1.08	6.53	14.47	0.00	0.59	0.00	0.00	0.00	1.84	0.11	0.11	0.22	0.11	0.05	0.00	1.89	
				1:00	2:00	13.50	0.59	3.67	8.59	0.00	0.43	0.00	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	13.50	0.49	3.40	9.02	0.00	0.32	0.00	0.00	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	10.80	0.54	2.97	6.80	0.00	0.49	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.10	0.32	2.05	5.45	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00	0.00	0.00	
				5:00	6:00	10.80	0.38	2.54	6.80	0.00	0.49	0.00	0.00	0.05	0.00	0.05	0.11	0.22	0.11	0.05	0.00	0.00	
				6:00	7:00	21.60	0.59	3.94	10.91	0.05	0.86	0.16	0.00	0.05	1.67	0.16	0.22	0.38	0.38	0.16	0.00	2.05	
				7:00	8:00	59.40	0.70	13.82	20.84	0.38	2.54	0.76	0.11	0.11	6.43	0.65	0.59	1.30	1.30	0.49	0.00	9.40	
				8:00	9:00	81.00	2.05	27.54	21.49	0.59	6.10	3.83	0.59	0.59	4.97	0.76	0.76	1.46	1.46	0.59	0.00	8.21	
				9:00	10:00	75.60	1.46	21.33	21.82	0.54	5.72	3.94	0.54	0.54	4.32	1.13	0.92	1.84	1.94	0.70	0.00	8.86	
				10:00	11:00	64.80	2.27	16.74	18.58	0.76	8.96	3.78	0.76	0.76	3.13	0.32	0.38	0.76	0.76	0.27	0.00	6.59	
				11:00	12:00	64.80	1.51	16.09	15.98	0.97	11.12	1.51	0.27	0.27	5.72	0.92	0.54	1.03	1.19	0.32	0.00	7.34	
				12:00	13:00	64.80	1.89	20.95	19.71	0.70	7.34	1.30	0.27	0.27	4.21	0.97	0.22	0.43	0.54	0.22	0.00	5.78	
				13:00	14:00	64.80	1.40	14.42	13.50	0.86	9.29	4.75	0.92	0.97	4.16	0.27	1.03	2.00	2.21	0.65	0.00	8.37	
				14:00	15:00	62.10	1.19	13.23	17.77	0.76	8.42	3.46	0.70	0.70	4.37	0.43	0.70	1.30	1.35	0.43	0.00	7.29	
				15:00	16:00	72.90	1.94	17.77	21.44	0.81	9.40	4.37	0.86	0.86	3.94	0.81	0.59	1.08	1.19	0.38	0.00	7.45	
				16:00	17:00	62.10	2.86	19.76	15.82	0.65	7.72	1.24	0.32	0.27	3.73	0.11	0.49	0.92	1.08	0.27	0.00	6.86	
				17:00	18:00	78.30	4.37	30.08	20.20	0.54	6.10	2.48	0.70	0.70	3.94	0.32	0.32	0.65	0.65	0.16	0.00	7.07	
				18:00	19:00	64.80	3.83	27.11	18.58	0.32	4.16	1.40	0.38	0.38	1.51	0.22	0.38	0.65	0.76	0.22	0.00	4.91	
				19:00	20:00	62.10	2.05	25.06	21.06	0.05	1.35	0.54	0.05	0.11	4.37	0.27	0.16	0.49	0.49	0.16	0.00	5.89	
				20:00	21:00	54.00	1.78	17.17	21.55	0.05	0.97	0.16	0.05	0.11	5.83	0.11	0.11	0.16	0.11	0.05	0.00	5.78	
				21:00	22:00	48.60	1.19	13.34	21.22	0.05	1.19	0.38	0.05	0.11	4.86	0.00	0.05	0.05	0.00	0.05	0.00	6.05	
				22:00	23:00	48.60	1.51	10.37	21.65	0.05	0.65	0.00	0.00	0.00	5.18	0.16	0.11	0.27	0.11	0.11	0.00	8.42	
				23:00	0:00	37.80	1.24	7.78	16.85	0.05	0.65	0.00	0.00	0.00	4.05	0.16	0.11	0.22	0.11	0.11	0.00	6.48	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L031	To Kwa Wan Road (SB)	Ngan Hon Street	Wing Kwong Street	0:00	1:00	69.20	2.08	13.67	39.10	0.00	1.21	0.00	0.00	0.00	0.00	5.54	0.35	0.17	0.69	0.35	0.17	0.00	5.88	
				1:00	2:00	34.60	1.21	7.96	23.87	0.00	0.87	0.00	0.00	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	34.60	0.87	7.27	25.09	0.00	0.69	0.00	0.00	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	25.95	1.04	6.06	17.82	0.00	1.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	25.95	0.87	5.54	18.68	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	25.95	0.69	5.02	17.99	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.17	0.17	0.52	0.35	0.17	0.00	0.00	0.00
				6:00	7:00	60.55	1.21	9.00	31.66	0.00	2.08	0.52	0.00	0.00	5.54	0.52	0.52	1.21	1.21	0.35	0.00	6.75	0.00	6.75
				7:00	8:00	164.35	1.38	32.01	57.78	0.35	5.71	2.08	0.35	0.35	20.76	1.90	1.73	3.98	3.98	1.38	0.00	30.62	0.00	30.62
				8:00	9:00	207.60	3.81	63.15	58.30	0.69	13.15	10.21	1.73	1.38	15.57	2.08	2.08	4.15	4.15	1.38	0.00	25.78	0.00	25.78
				9:00	10:00	198.95	2.60	48.79	60.03	0.52	12.46	10.90	1.56	1.38	13.67	3.46	2.60	5.36	5.54	1.73	0.00	28.37	0.00	28.37
				10:00	11:00	173.00	4.84	38.23	55.36	0.69	19.90	10.21	2.08	1.90	10.38	0.87	1.04	2.42	2.42	0.69	0.00	21.97	0.00	21.97
				11:00	12:00	173.00	3.29	36.68	46.71	0.87	24.74	3.98	0.87	0.69	18.86	2.77	1.56	3.29	3.63	0.87	0.00	24.22	0.00	24.22
				12:00	13:00	164.35	3.81	45.85	55.19	0.69	15.40	3.29	0.87	0.69	13.32	2.94	0.69	1.38	1.73	0.35	0.00	18.17	0.00	18.17
				13:00	14:00	164.35	2.77	31.14	37.20	0.69	19.20	11.76	2.42	2.42	12.80	0.87	3.11	5.88	6.40	1.73	0.00	25.95	0.00	25.95
				14:00	15:00	164.35	2.42	29.24	50.69	0.69	18.17	9.17	1.90	1.73	13.84	1.56	1.90	4.15	4.33	1.21	0.00	23.36	0.00	23.36
				15:00	16:00	190.30	3.98	39.27	60.90	0.69	20.24	11.59	2.42	2.25	12.63	2.42	1.73	3.46	3.63	1.21	0.00	23.87	0.00	23.87
				16:00	17:00	155.70	6.23	41.35	45.33	0.52	15.74	2.94	0.87	0.69	11.76	0.35	1.56	2.94	3.29	0.69	0.00	21.45	0.00	21.45
				17:00	18:00	198.95	9.69	64.18	59.69	0.52	12.98	6.23	2.08	1.73	12.46	1.04	1.04	2.08	2.08	0.52	0.00	22.66	0.00	22.66
				18:00	19:00	164.35	8.30	58.82	55.01	0.35	8.65	3.29	1.04	1.04	5.02	0.69	1.21	2.08	2.42	0.52	0.00	15.92	0.00	15.92
				19:00	20:00	164.35	4.50	56.05	60.20	0.00	2.94	1.56	0.17	0.35	14.19	0.87	0.69	1.56	1.73	0.35	0.00	19.20	0.00	19.20
				20:00	21:00	147.05	3.63	38.75	62.11	0.00	2.08	0.52	0.17	0.17	19.20	0.35	0.17	0.52	0.35	0.17	0.00	18.86	0.00	18.86
				21:00	22:00	129.75	2.42	29.06	59.86	0.00	2.42	0.87	0.17	0.17	15.40	0.00	0.00	0.17	0.00	0.17	0.00	19.03	0.00	19.03
				22:00	23:00	129.75	3.11	21.97	59.34	0.00	1.38	0.00	0.00	0.00	15.92	0.52	0.35	0.69	0.35	0.35	0.00	25.78	0.00	25.78
				23:00	0:00	112.45	2.77	18.34	51.21	0.00	1.38	0.00	0.00	0.00	14.01	0.52	0.35	0.87	0.35	0.35	0.00	22.32	0.00	22.32
L032	To Kwa Wan Road (NB)	Shek Tong Street	Chi Kiang Street	0:00	1:00	48.30	2.25	9.98	16.74	0.00	0.97	0.00	0.00	0.00	8.37	0.32	0.32	0.97	0.64	0.32	0.00	7.41		
				1:00	2:00	32.20	2.25	10.30	18.35	0.00	1.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	16.10	0.97	4.83	9.98	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	16.10	1.29	5.15	8.69	0.00	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	16.10	0.97	4.83	9.66	0.00	0.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	16.10	0.64	4.19	8.37	0.00	0.64	0.00	0.00	0.00	0.00	0.32	0.32	0.64	0.64	0.32	0.00	0.00	0.00	0.00
				6:00	7:00	48.30	1.61	7.08	14.49	0.00	1.61	0.32	0.00	0.00	8.37	0.64	0.64	1.93	1.93	0.64	0.00	9.02	0.00	9.02
				7:00	8:00	128.80	1.61	14.49	24.15	0.32	3.22	1.29	0.32	0.32	22.54	2.58	2.58	5.15	5.15	1.93	0.00	43.15	0.00	43.15
				8:00	9:00	144.90	4.51	28.66	24.15	0.32	7.08	6.12	1.61	1.61	16.74	2.58	2.58	5.15	5.15	1.93	0.00	36.71	0.00	36.71
				9:00	10:00	144.90	3.22	21.90	24.79	0.32	6.76	6.76	1.61	1.61	14.49	4.51	3.22	6.76	7.08	2.58	0.00	39.28	0.00	39.28
				10:00	11:00	144.90	5.47	29.62	25.12	0.64	15.78	7.41	1.61	1.61	16.10	1.61	3.22	3.22	0.97	0.00	30.91	0.00	30.91	
				11:00	12:00	144.90	3.22	26.08	19.64	0.97	18.35	2.58	0.64	0.64	26.40	3.54	1.93	3.86	4.51	1.29	0.00	31.23	0.00	31.23
				12:00	13:00	128.80	3.86	33.81	23.83	0.64	11.59	2.25	0.64	0.64	19.32	3.86	0.97	1.61	1.93	0.64	0.00	23.18	0.00	23.18
				13:00	14:00	144.90	2.90	22.86	16.10	0.64	14.49	8.05	1.93	1.93	18.68	0.97	4.19	7.41	8.69	2.90	0.00	33.17	0.00	33.17
				14:00	15:00	144.90	2.90	22.86	23.18	0.64	14.49	6.76	1.61	1.61	21.25	1.61	2.90	5.80	5.80	1.93	0.00	31.56	0.00	31.56
				15:00	16:00	161.00	4.51	30.27	27.69	0.64	16.10	8.37	2.25	2.25	19.32	3.54	2.58	4.51	5.15	1.93	0.00	31.88	0.00	31.88
				16:00	17:00	161.00	6.44	45.40	22.22	0.64	17.07	2.90	0.64	0.32	24.15	0.32	2.90	4.19	5.15	1.61	0.00	27.05	0.00	27.05
				17:00	18:00	177.10	9.02	63.43	26.08	0.64	12.56	4.83	1.29	0.97	23.51	1.61	1.61	2.58	2.58	0.97	0.00	25.44	0.00	25.44
				18:00	19:00	144.90	8.37	60.21	24.79	0.32	8.69	2.90	0.64	0.64	9.66	1.29	1.93	2.90	3.22	0.97	0.00	18.35	0.00	18.35
				19:00	20:00	128.80	4.51	41.86	26.40	0.00	2.25	0.97	0.32	0.32	20.29	1.29	0.97	1.93	1.93	0.64	0.00	25.12	0.00	25.12
				20:00	21:00	112.70	3.54	28.01	26.08	0.00	1.61	0.32	0.32	0.32	26.73	0.32	0.32	0.64	0.64	0.32	0.00	23.51	0.00	23.51
				21:00	22:00	112.70	2.90	23.83	29.30	0.00	2.25	0.64	0.32	0.32	24.79	0.00	0.00	0.32	0.00	0.00	0.00	28.01	0.00	28.01
				22:00	23:00	112.70	3.54	17.07	27.37	0.00	0.97	0.00	0.00	0.00	24.79	0.64	0.32	0.97	0.64	0.32	0.00	36.06	0.00	36.06
				23:00	0:00	96.60	2.90	14.49	23.18	0.00	0.97	0.00	0.00	0.00	20.93	0.64	0.32	0.97	0.64	0.32	0.00	31.23	0.00	31.23

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L033	Ngan Hon Street (EB)	To Kwa Wan Road	Lung Tak Street	0:00	1:00	6.00	0.84	3.30	1.62	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	3.00	0.42	1.62	0.84	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.00	0.30	1.56	0.96	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.00	0.42	1.56	0.84	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.00	0.42	1.56	0.90	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.00	0.36	1.50	0.90	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	6.00	0.66	2.76	1.68	0.12	0.54	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00	
				7:00	8:00	9.00	0.78	5.04	1.80	0.24	0.72	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	
				8:00	9:00	18.00	2.22	10.32	1.92	0.42	1.86	0.84	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00
				9:00	10:00	15.00	1.56	7.92	1.98	0.36	1.80	0.90	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.30	0.00	0.00
				10:00	11:00	15.00	1.44	6.78	1.68	0.60	3.24	0.96	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				11:00	12:00	15.00	0.96	6.78	1.50	0.84	4.26	0.42	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				12:00	13:00	15.00	1.20	8.28	1.80	0.54	2.64	0.36	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				13:00	14:00	15.00	0.96	6.30	1.32	0.72	3.66	1.32	0.24	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.30	0.00	0.00
				14:00	15:00	12.00	0.66	5.10	1.56	0.54	2.94	0.78	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00
				15:00	16:00	15.00	1.20	6.60	1.86	0.60	3.24	1.08	0.18	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				16:00	17:00	15.00	1.08	8.22	1.44	0.54	3.18	0.42	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	18.00	1.50	11.28	1.62	0.42	2.28	0.66	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	18.00	1.56	12.06	1.74	0.30	1.80	0.42	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	15.00	1.38	10.80	2.10	0.06	0.48	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				20:00	21:00	12.00	1.32	7.92	2.28	0.06	0.36	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	9.00	0.78	5.64	2.04	0.06	0.42	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.00	1.14	5.10	2.40	0.06	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				23:00	0:00	6.00	0.78	3.24	1.62	0.06	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
L034	Ngan Hon Street (EB)	Lung Tak Street	Wing Kwong Street	0:00	1:00	6.10	0.55	3.42	1.95	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	3.05	0.24	1.65	1.04	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.05	0.24	1.59	1.16	0.00	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.05	0.31	1.59	0.98	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.05	0.24	1.53	1.16	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.05	0.24	1.40	1.16	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	6.10	0.37	2.87	2.14	0.12	0.49	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	9.15	0.43	5.25	2.20	0.37	0.61	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00	
				8:00	9:00	18.30	1.28	11.10	2.38	0.73	1.59	0.92	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				9:00	10:00	15.25	0.92	8.36	2.38	0.67	1.59	0.98	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00
				10:00	11:00	15.25	0.98	7.14	2.14	0.92	2.75	1.04	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				11:00	12:00	15.25	0.67	7.20	1.89	1.22	3.66	0.43	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				12:00	13:00	15.25	0.79	8.66	2.20	0.85	2.26	0.37	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	12.20	0.49	5.37	1.34	0.92	2.56	1.10	0.18	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				14:00	15:00	12.20	0.49	5.25	1.89	0.85	2.50	0.92	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				15:00	16:00	15.25	0.79	6.95	2.26	0.98	2.75	1.16	0.18	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				16:00	17:00	15.25	0.85	8.66	1.77	0.73	2.68	0.43	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	18.30	1.16	11.65	2.01	0.61	1.89	0.73	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	15.25	1.04	10.31	1.83	0.31	1.28	0.37	0.06	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	15.25	0.85	11.16	2.56	0.06	0.43	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	12.20	0.79	8.17	2.81	0.06	0.31	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	9.15	0.49	5.73	2.44	0.06	0.37	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.15	0.73	5.19	2.93	0.06	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	6.10	0.55	3.36	1.95	0.06	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L035	Ngan Hon Street (EB)	Wing Kwong Street	Sung On Street	0:00	1:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.10	0.00	1.97	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.10	0.00	1.76	0.00	0.00	0.08	0.08	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.10	0.00	1.60	0.00	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.10	0.00	1.51	0.00	0.00	0.21	0.13	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.10	0.00	1.51	0.00	0.00	0.34	0.08	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.10	0.00	1.72	0.00	0.00	0.13	0.08	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.10	0.00	1.26	0.00	0.00	0.21	0.21	0.21	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.10	0.00	1.34	0.00	0.00	0.25	0.17	0.17	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.10	0.00	1.26	0.00	0.00	0.21	0.21	0.21	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.10	0.00	1.64	0.00	0.00	0.21	0.08	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.10	0.00	1.76	0.00	0.00	0.08	0.08	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.10	0.00	1.93	0.00	0.00	0.04	0.04	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L036	Ngan Hon Street (WB)	Sung On Street	Wing Kwong Street	23:00	0:00	2.10	0.00	2.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				0:00	1:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.20	0.00	2.07	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	2.20	0.00	1.85	0.00	0.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.20	0.00	1.67	0.00	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.20	0.00	1.67	0.00	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.20	0.00	1.72	0.00	0.00	0.22	0.09	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.20	0.00	1.94	0.00	0.00	0.13	0.04	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.20	0.00	1.50	0.00	0.00	0.18	0.18	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.20	0.00	1.50	0.00	0.00	0.18	0.18	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.20	0.00	1.67	0.00	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.20	0.00	2.02	0.00	0.00	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	2.20	0.00	2.02	0.00	0.00	0.04	0.04	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	2.20	0.00	2.02	0.00	0.00	0.04	0.04	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					
23:00	0:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L037	Wing Kwong Street (SB)	Ngan Hon Street	Hung Fook Street	0:00	1:00	4.10	0.41	2.30	1.27	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.05	0.21	1.11	0.66	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.05	0.16	1.07	0.78	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.05	0.21	1.07	0.66	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.05	0.16	1.07	0.74	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.05	0.16	0.94	0.78	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.10	0.33	1.93	1.44	0.08	0.25	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	6.15	0.33	3.53	1.44	0.25	0.41	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				8:00	9:00	12.30	0.94	7.50	1.60	0.45	0.98	0.62	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				9:00	10:00	10.25	0.70	5.66	1.60	0.45	0.94	0.66	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				10:00	11:00	10.25	0.70	4.92	1.44	0.62	1.72	0.70	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				11:00	12:00	10.25	0.49	5.00	1.31	0.82	2.26	0.29	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				12:00	13:00	10.25	0.57	5.99	1.48	0.53	1.39	0.25	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	8.20	0.37	3.73	0.94	0.57	1.56	0.78	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				14:00	15:00	8.20	0.33	3.69	1.31	0.57	1.52	0.62	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				*5:00	16:00	10.25	0.57	4.84	1.56	0.62	1.68	0.82	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				*6:00	17:00	10.25	0.66	6.03	1.19	0.45	1.64	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*7:00	18:00	12.30	0.82	8.08	1.35	0.33	1.15	0.49	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*8:00	19:00	10.25	0.70	7.09	1.19	0.21	0.74	0.25	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*9:00	20:00	10.25	0.62	7.59	1.68	0.04	0.25	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	8.20	0.57	5.49	1.85	0.04	0.21	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	6.15	0.33	3.85	1.64	0.04	0.25	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	6.15	0.57	3.44	1.93	0.04	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L038	Wing Kwong Street (SB)	Hung Fook Street	Kai Ming Street	23:00	0:00	4.10	0.37	2.26	1.31	0.04	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				0:00	1:00	4.10	0.29	2.26	1.44	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.05	0.12	1.11	0.74	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.05	0.16	1.03	0.82	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.05	0.16	1.11	0.66	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.05	0.12	1.07	0.78	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.05	0.12	1.03	0.78	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	4.10	0.21	1.93	1.48	0.12	0.29	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	8.20	0.33	4.76	2.09	0.33	0.53	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				8:00	9:00	14.35	0.82	8.94	1.97	0.57	1.11	0.74	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				9:00	10:00	12.30	0.57	7.09	2.05	0.49	1.07	0.82	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				*0:00	11:00	12.30	0.66	5.99	1.80	0.74	2.01	0.94	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	10.25	0.41	5.00	1.39	0.86	2.21	0.29	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	12.30	0.53	7.22	1.89	0.70	1.60	0.29	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	10.25	0.37	4.72	1.19	0.78	1.89	1.03	0.16	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				14:00	15:00	10.25	0.33	4.63	1.68	0.74	1.85	0.82	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				15:00	16:00	12.30	0.49	5.95	1.93	0.78	1.93	0.98	0.12	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				16:00	17:00	10.25	0.53	5.99	1.23	0.49	1.64	0.33	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	14.35	0.82	9.43	1.64	0.45	1.31	0.57	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	12.30	0.70	8.49	1.52	0.29	0.90	0.33	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	10.25	0.49	7.54	1.76	0.08	0.25	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	8.20	0.45	5.58	1.89	0.08	0.16	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	8.20	0.37	5.08	2.26	0.12	0.25	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	6.15	0.45	3.49	2.01	0.08	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				
23:00	0:00	6.15	0.41	3.40	2.01	0.12	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L039	Hung Fook Street (EB)	Access Road	Wing Kwong Street	0:00	1:00	3.00	0.00	1.80	1.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	3.00	0.00	1.80	1.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.00	0.00	1.80	1.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.00	0.00	1.50	1.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.00	0.00	1.50	1.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.00	0.00	1.50	1.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	3.00	0.00	1.50	1.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	3.00	0.00	1.80	0.84	0.12	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	3.00	0.06	2.04	0.42	0.12	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	3.00	0.06	1.74	0.54	0.18	0.24	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	3.00	0.12	1.50	0.48	0.18	0.42	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	3.00	0.00	1.50	0.54	0.30	0.54	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	3.00	0.12	1.74	0.54	0.18	0.30	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	3.00	0.00	1.50	0.42	0.24	0.42	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	3.00	0.00	1.44	0.60	0.24	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	3.00	0.12	1.44	0.54	0.18	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	3.00	0.12	1.80	0.48	0.12	0.36	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	3.00	0.12	1.92	0.48	0.12	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	3.00	0.12	1.98	0.54	0.12	0.12	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	3.00	0.12	2.16	0.60	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	3.00	0.12	2.04	0.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	3.00	0.00	1.98	1.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	3.00	0.00	1.80	1.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				L040	Hung Fook Street (EB)	Yuk Shing Street	Access Road	23:00	0:00	3.00	0.00	1.74	1.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
0:00	1:00	0.90	0.00					0.58	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	0.90	0.00					0.52	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	0.90	0.00					0.52	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	0.90	0.00					0.54	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	0.90	0.00					0.45	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	0.90	0.00					0.45	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	0.90	0.00					0.45	0.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
7:00	8:00	0.90	0.00					0.50	0.25	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.00	
8:00	9:00	1.80	0.00					1.17	0.27	0.00	0.07	0.14	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.00	
9:00	10:00	0.90	0.00					0.54	0.16	0.00	0.04	0.07	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00	
10:00	11:00	0.90	0.00					0.49	0.14	0.00	0.13	0.09	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.02	0.00	0.00	
11:00	12:00	0.90	0.00					0.50	0.14	0.00	0.18	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
12:00	13:00	0.90	0.00					0.59	0.18	0.00	0.11	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
13:00	14:00	0.90	0.00					0.41	0.11	0.00	0.14	0.11	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00	
14:00	15:00	0.90	0.00					0.40	0.16	0.00	0.14	0.09	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
15:00	16:00	0.90	0.00					0.49	0.16	0.00	0.11	0.09	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.02	0.00	0.00	
16:00	17:00	0.90	0.00					0.56	0.14	0.00	0.16	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
17:00	18:00	0.90	0.00					0.63	0.13	0.00	0.09	0.04	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
18:00	19:00	0.90	0.00					0.67	0.13	0.00	0.07	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
19:00	20:00	0.90	0.00					0.70	0.16	0.00	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
20:00	21:00	0.90	0.00					0.63	0.23	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	0.90	0.00					0.59	0.27	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	0.90	0.00					0.56	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	0.90	0.00	0.54	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L041	Kai Ming Street (WB)	Wing Kwong Street	Access Road	0:00	1:00	2.40	0.14	1.25	0.86	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.40	0.24	1.30	0.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.40	0.00	1.34	1.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.40	0.00	1.39	1.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.40	0.00	1.39	1.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.40	0.00	1.20	0.91	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.40	0.19	1.15	0.86	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.40	0.05	1.39	0.62	0.14	0.14	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.80	0.19	3.02	0.67	0.19	0.38	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.80	0.14	2.78	0.82	0.24	0.43	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*0:00	11:00	4.80	0.14	2.40	0.72	0.24	0.82	0.38	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*1:00	12:00	4.80	0.10	2.40	0.67	0.34	1.10	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*2:00	13:00	4.80	0.14	2.93	0.72	0.24	0.62	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*3:00	14:00	4.80	0.10	2.26	0.53	0.34	0.96	0.53	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*4:00	15:00	4.80	0.10	2.11	0.86	0.29	0.96	0.38	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				*5:00	16:00	4.80	0.14	2.30	0.72	0.29	0.82	0.43	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	4.80	0.14	2.98	0.58	0.14	0.82	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.80	0.19	3.22	0.53	0.14	0.48	0.19	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.80	0.19	3.41	0.53	0.05	0.43	0.14	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	4.80	0.14	3.60	0.82	0.00	0.14	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.40	0.10	1.68	0.58	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.40	0.05	1.63	0.67	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.40	0.10	1.39	0.82	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.40	0.10	1.39	0.82	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L042	Kai Ming Street (WB)	Access Road	Yuk Shing Street	0:00	1:00	1.50	0.00	0.87	0.57	0.00	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	1.50	0.00	0.78	0.63	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	1.50	0.00	0.87	0.63	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.50	0.00	0.81	0.54	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.50	0.00	0.84	0.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	1.50	0.00	0.75	0.63	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	1.50	0.00	0.72	0.60	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	
				7:00	8:00	3.00	0.00	1.71	0.78	0.00	0.12	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.21	0.00	0.00
				8:00	9:00	4.50	0.00	2.88	0.69	0.00	0.21	0.33	0.12	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00
				9:00	10:00	3.00	0.00	1.68	0.54	0.00	0.18	0.27	0.09	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00
				10:00	11:00	3.00	0.00	1.56	0.54	0.00	0.54	0.24	0.06	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.03	0.00	0.00
				11:00	12:00	3.00	0.00	1.59	0.48	0.00	0.69	0.12	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00
				12:00	13:00	3.00	0.00	1.92	0.54	0.00	0.42	0.06	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.03	0.00	0.00
				13:00	14:00	3.00	0.00	1.35	0.39	0.00	0.57	0.33	0.12	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				14:00	15:00	3.00	0.00	1.35	0.57	0.00	0.57	0.27	0.09	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.00
				15:00	16:00	3.00	0.00	1.50	0.54	0.00	0.51	0.27	0.09	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00
				16:00	17:00	3.00	0.00	1.83	0.45	0.00	0.66	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.50	0.00	3.12	0.63	0.00	0.57	0.15	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.50	0.00	3.24	0.66	0.00	0.45	0.09	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	3.00	0.00	2.31	0.57	0.00	0.06	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.03	0.00	0.00
				20:00	21:00	3.00	0.00	2.13	0.78	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	3.00	0.00	1.92	0.90	0.00	0.12	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	1.50	0.00	0.90	0.54	0.00	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.03	0.00	0.00
				23:00	0:00	1.50	0.00	0.87	0.57	0.00	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.03	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L043	Sung On Street (NB)	Bailey Street	Chi Kiang Street	0:00	1:00	23.90	0.96	5.74	13.86	0.00	0.72	0.00	0.00	0.00	0.00	0.24	0.24	1.20	0.72	0.24	0.00	0.00
				1:00	2:00	11.95	0.48	3.11	8.13	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	11.95	0.24	2.87	8.60	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	11.95	0.48	3.11	7.89	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	11.95	0.48	2.87	7.65	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.48	0.00	0.00	0.00
				5:00	6:00	11.95	0.24	2.15	6.93	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.24	0.24	0.96	0.72	0.24	0.00
				6:00	7:00	23.90	0.72	3.82	10.99	0.00	0.72	0.24	0.00	0.00	0.00	0.00	0.96	0.96	2.39	2.15	0.96	0.00
				7:00	8:00	47.80	0.48	8.37	18.88	0.24	2.39	0.48	0.00	0.00	0.00	0.00	2.39	2.39	5.50	4.78	1.91	0.00
				8:00	9:00	59.75	1.20	15.30	17.21	0.24	4.78	2.87	0.48	0.48	0.00	2.39	2.39	5.50	4.78	2.15	0.00	0.00
				9:00	10:00	59.75	0.96	11.23	16.97	0.24	4.54	3.11	0.48	0.48	0.00	3.59	2.87	6.45	6.21	2.63	0.00	0.00
				10:00	11:00	59.75	2.15	14.58	17.21	0.24	7.65	5.02	0.72	0.24	0.00	1.43	1.91	3.82	3.59	1.20	0.00	0.00
				11:00	12:00	59.75	1.20	13.62	14.34	0.24	9.32	2.15	0.24	0.00	0.00	4.06	2.39	5.26	5.26	1.67	0.00	0.00
				12:00	13:00	47.80	1.43	14.82	15.30	0.24	5.26	1.43	0.24	0.00	0.00	3.82	0.72	1.91	1.91	0.72	0.00	0.00
				13:00	14:00	59.75	0.96	10.28	10.28	0.24	6.69	5.26	0.48	0.48	0.00	0.96	4.06	8.60	8.60	2.87	0.00	0.00
				14:00	15:00	59.75	0.96	10.76	15.77	0.24	6.93	4.54	0.48	0.24	0.00	1.91	2.87	6.45	6.21	2.39	0.00	0.00
				15:00	16:00	59.75	1.43	12.67	16.73	0.24	6.69	5.02	0.48	0.24	0.00	3.11	2.39	4.78	4.54	1.43	0.00	0.00
				16:00	17:00	59.75	2.39	19.12	13.38	0.00	5.50	2.39	0.00	0.00	0.00	0.48	3.11	5.74	6.21	1.43	0.00	0.00
				17:00	18:00	71.70	3.82	29.16	16.97	0.00	4.30	4.78	0.24	0.00	0.00	1.91	1.91	3.82	3.59	1.20	0.00	0.00
				18:00	19:00	59.75	3.35	24.14	14.58	0.00	2.87	2.39	0.24	0.00	0.00	1.20	1.91	4.06	4.06	0.96	0.00	0.00
				19:00	20:00	47.80	1.67	20.08	17.93	0.00	0.96	0.48	0.00	0.00	0.00	0.96	0.72	2.15	2.15	0.72	0.00	0.00
				20:00	21:00	35.85	1.43	13.15	18.16	0.00	0.72	0.24	0.00	0.00	0.00	0.48	0.24	0.72	0.48	0.24	0.00	0.00
				21:00	22:00	35.85	0.96	11.95	20.79	0.00	0.96	0.72	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.24	0.00	0.00
				22:00	23:00	35.85	1.20	9.32	20.55	0.00	0.72	0.00	0.00	0.00	0.00	0.72	0.72	1.20	0.72	0.72	0.00	0.00
				L044	Sung On Street (SB)	Chi Kiang Street	Bailey Street	23:00	0:00	23.90	0.96	5.50	14.10	0.00	0.48	0.00	0.00	0.00	0.00	0.48	0.48	0.96
0:00	1:00	35.40	1.18					12.04	16.05	0.00	0.94	0.00	0.00	0.00	3.54	0.24	0.24	0.71	0.24	0.24	0.00	0.00
1:00	2:00	23.60	0.71					9.20	12.51	0.00	1.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	23.60	0.94					8.50	13.22	0.00	0.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	11.80	0.47					4.48	6.37	0.00	0.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	11.80	0.24					4.25	7.08	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	11.80	0.24					3.54	5.90	0.00	0.71	0.00	0.00	0.00	0.00	0.24	0.24	0.47	0.24	0.24	0.00	0.00
6:00	7:00	23.60	0.47					6.14	9.91	0.00	1.18	0.24	0.00	0.00	2.60	0.47	0.47	0.94	0.94	0.24	0.00	0.00
7:00	8:00	70.80	1.18					21.24	21.95	0.24	3.54	1.18	0.24	0.24	8.97	1.89	1.89	3.54	3.54	1.18	0.00	0.00
8:00	9:00	106.20	2.60					43.90	23.36	0.24	8.26	5.43	1.18	1.18	7.08	2.12	2.12	3.78	3.78	1.18	0.00	0.00
9:00	10:00	94.40	1.89					31.86	22.89	0.24	7.32	5.43	1.18	1.18	5.90	3.30	2.60	4.72	4.72	1.18	0.00	0.00
10:00	11:00	82.60	2.60					28.32	18.17	0.24	13.92	4.72	1.42	1.42	5.43	0.94	0.94	1.89	1.89	0.71	0.00	0.00
11:00	12:00	82.60	1.42					25.72	15.10	0.47	16.99	1.65	0.71	0.47	9.44	2.60	1.42	2.83	3.07	0.71	0.00	0.00
12:00	13:00	82.60	1.89					34.46	18.88	0.24	11.33	1.42	0.71	0.47	7.08	2.83	0.71	1.18	1.18	0.24	0.00	0.00
13:00	14:00	82.60	1.42					22.89	12.51	0.24	13.69	5.66	1.89	1.89	6.61	0.71	3.07	4.96	5.43	1.65	0.00	0.00
14:00	15:00	82.60	1.42					21.95	18.17	0.24	13.69	4.48	1.65	1.42	7.55	1.42	1.89	3.78	3.78	1.18	0.00	0.00
15:00	16:00	94.40	2.12					29.03	21.00	0.24	14.87	5.66	2.12	1.89	6.61	2.36	1.42	3.07	3.07	0.94	0.00	0.00
16:00	17:00	82.60	2.83					33.98	13.69	0.24	13.45	1.42	0.71	0.47	7.32	0.24	1.42	2.83	3.07	0.94	0.00	0.00
17:00	18:00	106.20	4.25					52.86	17.94	0.24	11.09	2.83	1.42	1.18	8.02	0.94	0.94	1.89	1.89	0.71	0.00	0.00
18:00	19:00	94.40	3.78					51.21	17.46	0.24	8.02	1.65	0.71	0.71	3.30	0.71	1.42	2.12	2.36	0.71	0.00	0.00
19:00	20:00	82.60	2.36					42.72	21.48	0.00	2.12	0.94	0.24	0.24	7.55	0.94	0.94	1.42	1.42	0.24	0.00	0.00
20:00	21:00	70.80	1.89					31.39	22.89	0.00	1.65	0.24	0.24	0.24	10.86	0.24	0.24	0.47	0.24	0.24	0.00	0.00
21:00	22:00	59.00	1.42					23.36	22.18	0.00	1.89	0.47	0.24	0.24	8.97	0.00	0.00	0.24	0.00	0.00	0.00	0.00
22:00	23:00	47.20	1.42					15.58	19.35	0.00	0.94	0.00	0.00	0.00	8.26	0.47	0.24	0.47	0.24	0.24	0.00	0.00
23:00	0:00	47.20	1.65	14.87	19.59	0.00	1.18	0.00	0.00	0.00	8.26	0.47	0.24	0.47	0.24	0.24	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L045	Wan On Street (SB)	Wing Kwong Street	Bailey Street	0:00	1:00	5.30	0.21	2.86	2.01	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	5.30	0.21	2.65	2.23	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.30	0.21	2.65	2.23	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.30	0.32	2.54	2.12	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.30	0.32	2.65	2.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.30	0.32	2.23	2.12	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.32	0.00	0.00
				6:00	7:00	5.30	0.11	2.54	2.23	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				7:00	8:00	15.90	0.32	8.27	5.09	0.32	0.53	0.32	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.85	0.00	0.00
				8:00	9:00	26.50	0.85	15.79	4.98	0.53	1.38	1.17	0.53	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.85	0.00	0.00
				9:00	10:00	21.20	0.53	11.13	4.77	0.53	1.17	1.17	0.53	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.95	0.00	0.00
				10:00	11:00	15.90	0.53	7.95	2.86	0.53	2.12	1.17	0.32	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.21	0.00	0.00
				11:00	12:00	15.90	0.32	8.27	2.65	0.74	2.86	0.53	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.32	0.00	0.00
				12:00	13:00	15.90	0.42	9.86	2.86	0.42	1.70	0.32	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				13:00	14:00	15.90	0.32	7.31	2.23	0.64	2.33	1.48	0.53	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.74	0.00	0.00
				14:00	15:00	15.90	0.42	7.10	3.18	0.64	2.23	1.17	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.53	0.00	0.00
				15:00	16:00	21.20	0.53	10.28	4.03	0.74	2.65	1.59	0.53	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.42	0.00	0.00
				16:00	17:00	15.90	0.64	10.07	1.80	0.42	2.44	0.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	21.20	0.95	14.84	2.23	0.32	1.80	0.85	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	21.20	0.95	15.48	2.33	0.32	1.38	0.53	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	15.90	0.53	11.66	3.18	0.00	0.32	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				20:00	21:00	15.90	0.64	10.49	4.35	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				21:00	22:00	10.60	0.32	6.57	3.39	0.00	0.21	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	10.60	0.42	6.04	3.92	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				23:00	0:00	10.60	0.53	5.62	4.03	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.21	0.00	0.00
L046	Wan On Street (NB)	Bailey Street	Wing Kwong Street	0:00	1:00	10.60	0.00	5.94	4.45	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	5.30	0.00	2.86	2.23	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.30	0.00	2.76	2.33	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	5.30	0.00	2.97	2.12	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.30	0.00	2.54	2.44	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	5.30	0.00	2.86	2.23	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	5.30	0.00	2.54	2.23	0.11	0.21	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00	
				7:00	8:00	15.90	0.00	9.33	4.45	0.53	0.64	0.42	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.42	0.00	0.00
				8:00	9:00	26.50	0.00	17.38	4.24	0.85	1.38	1.59	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.42	0.00	0.00
				9:00	10:00	21.20	0.00	12.72	4.03	0.74	1.17	1.59	0.32	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.42	0.00	0.00
				10:00	11:00	21.20	0.00	10.92	3.92	1.17	2.65	1.80	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				11:00	12:00	15.90	0.00	8.37	2.76	1.17	2.76	0.53	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				12:00	13:00	21.20	0.00	13.04	4.03	1.06	2.12	0.64	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				13:00	14:00	15.90	0.00	7.53	2.33	1.06	2.23	1.80	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.32	0.00	0.00
				14:00	15:00	15.90	0.00	7.31	3.29	1.06	2.12	1.38	0.32	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.21	0.00	0.00
				15:00	16:00	21.20	0.00	10.49	4.13	1.17	2.54	1.91	0.53	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.21	0.00	0.00
				16:00	17:00	15.90	0.00	9.86	2.54	0.74	2.12	0.53	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	26.50	0.00	17.81	4.13	0.85	2.12	1.17	0.32	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	21.20	0.00	15.16	3.50	0.32	1.38	0.64	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	21.20	0.00	16.01	4.35	0.11	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00
				20:00	21:00	15.90	0.00	10.92	4.45	0.11	0.32	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	15.90	0.00	9.86	5.19	0.21	0.42	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	10.60	0.00	6.15	4.24	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	10.60	0.00	5.94	4.24	0.21	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L047	Ma Tau Wai Road (SB)	Wing Kwong Street	Bailey Street	0:00	1:00	48.60	1.84	12.20	25.60	0.00	0.97	0.00	0.00	0.00	0.43	0.11	0.11	0.43	0.22	0.11	0.00	3.56
				1:00	2:00	27.00	1.08	7.78	16.85	0.00	0.86	0.00	0.00	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	27.00	0.97	7.13	17.71	0.00	0.65	0.00	0.00	0.00	0.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	21.60	1.08	6.26	13.39	0.00	0.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	16.20	0.76	4.21	10.69	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00	0.00	0.00
				5:00	6:00	21.60	0.86	5.29	13.39	0.00	0.97	0.00	0.00	0.00	0.00	0.11	0.11	0.43	0.32	0.11	0.00	0.00
				6:00	7:00	43.20	1.08	8.21	21.38	0.22	1.62	0.32	0.00	0.00	3.56	0.32	0.32	0.86	0.65	0.32	0.00	4.32
				7:00	8:00	113.40	1.40	27.97	37.91	0.86	4.64	1.51	0.22	0.22	12.64	1.19	0.97	2.27	2.27	0.86	0.00	18.47
				8:00	9:00	156.60	3.56	57.02	39.74	1.62	11.02	7.02	0.97	0.97	9.83	1.30	1.30	2.48	2.48	0.97	0.00	16.31
				9:00	10:00	145.80	2.48	43.63	40.50	1.51	10.48	7.45	0.97	0.97	8.64	1.94	1.51	3.24	3.35	1.30	0.00	17.82
				10:00	11:00	124.20	4.21	33.59	34.34	1.84	15.98	7.13	1.40	1.40	6.37	0.54	0.65	1.40	1.40	0.54	0.00	13.39
				11:00	12:00	124.20	2.70	32.18	29.81	2.48	19.87	2.70	0.54	0.54	11.45	1.62	0.86	1.84	2.16	0.65	0.00	14.80
				12:00	13:00	124.20	3.35	41.69	36.61	1.73	13.07	2.48	0.54	0.54	8.42	1.73	0.54	0.76	0.86	0.32	0.00	11.56
				13:00	14:00	118.80	2.48	27.86	24.08	2.05	15.88	8.32	1.51	1.51	8.10	0.43	1.94	3.46	3.78	1.19	0.00	16.20
				14:00	15:00	118.80	2.16	26.24	33.37	1.94	15.12	6.59	1.19	1.08	8.64	0.86	1.19	2.38	2.48	0.86	0.00	14.69
				15:00	16:00	140.40	3.67	35.64	40.39	2.16	16.96	8.32	1.51	1.40	7.99	1.40	0.97	1.94	2.16	0.76	0.00	15.12
				16:00	17:00	118.80	5.40	38.56	29.81	1.40	13.28	2.27	0.65	0.43	7.56	0.22	1.08	1.62	2.05	0.43	0.00	14.04
				17:00	18:00	145.80	7.88	56.48	37.04	1.19	10.37	4.43	1.19	1.19	7.78	0.76	0.76	1.19	1.19	0.32	0.00	14.04
				18:00	19:00	124.20	7.13	52.81	34.88	0.65	7.34	2.48	0.65	0.65	3.13	0.54	0.76	1.30	1.51	0.32	0.00	10.04
				19:00	20:00	124.20	3.89	51.30	40.50	0.22	2.48	0.97	0.11	0.22	8.96	0.65	0.54	0.86	0.86	0.32	0.00	12.31
				20:00	21:00	102.60	3.13	33.70	39.42	0.22	1.62	0.32	0.11	0.11	11.56	0.22	0.11	0.32	0.22	0.11	0.00	11.45
				21:00	22:00	97.20	2.16	27.54	41.26	0.22	2.16	0.65	0.11	0.11	10.26	0.00	0.00	0.11	0.00	0.11	0.00	12.53
				22:00	23:00	91.80	2.81	20.09	39.64	0.22	1.08	0.00	0.00	0.00	10.15	0.22	0.22	0.43	0.22	0.22	0.00	16.52
				23:00	0:00	75.60	2.38	15.98	32.62	0.22	1.19	0.00	0.00	0.00	8.42	0.22	0.22	0.43	0.22	0.22	0.00	13.50
L048	Ma Tau Wai Road (NB)	Gillies Avenue North	Shek Tong Street	0:00	1:00	4.50	0.18	0.84	1.50	0.00	0.06	0.00	0.00	0.00	0.90	0.03	0.03	0.06	0.03	0.03	0.00	0.84
				1:00	2:00	1.50	0.12	0.51	0.84	0.00	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	1.50	0.09	0.48	0.90	0.00	0.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	1.50	0.12	0.48	0.84	0.00	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	1.50	0.12	0.45	0.87	0.00	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	1.50	0.09	0.36	0.75	0.00	0.09	0.00	0.00	0.00	0.00	0.03	0.03	0.09	0.03	0.03	0.00	0.00
				6:00	7:00	4.50	0.12	0.60	1.17	0.00	0.12	0.03	0.00	0.00	0.93	0.09	0.09	0.15	0.12	0.09	0.00	0.99
				7:00	8:00	10.50	0.12	1.02	1.53	0.03	0.21	0.09	0.03	0.03	2.13	0.18	0.18	0.36	0.36	0.18	0.00	4.05
				8:00	9:00	12.00	0.33	2.10	1.68	0.03	0.51	0.42	0.09	0.09	1.68	0.21	0.21	0.39	0.39	0.18	0.00	3.69
				9:00	10:00	12.00	0.24	1.59	1.68	0.03	0.48	0.48	0.09	0.09	1.47	0.30	0.24	0.51	0.54	0.24	0.00	4.02
				10:00	11:00	10.50	0.39	2.01	1.65	0.03	0.99	0.45	0.12	0.09	1.41	0.09	0.12	0.21	0.21	0.09	0.00	2.64
				11:00	12:00	12.00	0.27	1.98	1.44	0.06	1.26	0.15	0.03	0.03	2.58	0.30	0.15	0.30	0.33	0.12	0.00	3.00
				12:00	13:00	10.50	0.30	2.52	1.77	0.03	0.81	0.15	0.03	0.03	1.89	0.30	0.06	0.12	0.15	0.06	0.00	2.28
				13:00	14:00	12.00	0.24	1.71	1.20	0.03	1.02	0.51	0.12	0.12	1.86	0.09	0.33	0.54	0.60	0.27	0.00	3.36
				14:00	15:00	10.50	0.18	1.53	1.53	0.03	0.87	0.36	0.09	0.09	1.86	0.12	0.18	0.36	0.36	0.15	0.00	2.79
				15:00	16:00	12.00	0.33	2.10	1.92	0.03	1.02	0.51	0.12	0.12	1.74	0.24	0.15	0.33	0.33	0.12	0.00	2.94
				16:00	17:00	12.00	0.42	3.21	1.62	0.03	1.05	0.15	0.03	0.03	2.13	0.03	0.18	0.27	0.36	0.12	0.00	2.37
				17:00	18:00	15.00	0.72	4.92	2.16	0.03	0.90	0.30	0.12	0.06	2.37	0.15	0.15	0.21	0.21	0.12	0.00	2.58
				18:00	19:00	12.00	0.66	4.68	2.04	0.03	0.63	0.18	0.06	0.03	0.96	0.09	0.18	0.24	0.27	0.09	0.00	1.86
				19:00	20:00	10.50	0.33	3.12	1.95	0.00	0.15	0.06	0.00	0.00	1.98	0.09	0.06	0.12	0.15	0.06	0.00	2.43
				20:00	21:00	9.00	0.27	1.95	1.80	0.00	0.09	0.03	0.00	0.00	2.52	0.03	0.03	0.06	0.03	0.03	0.00	2.16
				21:00	22:00	9.00	0.21	1.68	2.04	0.00	0.15	0.03	0.00	0.00	2.31	0.00	0.00	0.03	0.00	0.00	0.00	2.55
				22:00	23:00	9.00	0.24	1.20	1.86	0.00	0.06	0.00	0.00	0.00	2.22	0.03	0.03	0.06	0.03	0.03	0.00	3.24
				23:00	0:00	7.50	0.24	0.96	1.56	0.00	0.06	0.00	0.00	0.00	1.86	0.03	0.03	0.06	0.03	0.03	0.00	2.64

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L049	Bailey Street (EB)	Ma Tau Wai Road	Wan On Street	0:00	1:00	19.00	0.53	5.17	11.86	0.00	0.53	0.00	0.00	0.00	0.15	0.08	0.08	0.38	0.15	0.08	0.00	0.00	
				1:00	2:00	11.40	0.30	3.12	7.60	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	11.40	0.30	2.89	7.90	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.60	0.30	2.05	4.86	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.60	0.23	1.90	5.17	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				5:00	6:00	7.60	0.15	1.67	4.86	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.23	0.15	0.08	0.00	0.00
				6:00	7:00	15.20	0.23	3.04	8.97	0.08	0.76	0.15	0.00	0.00	0.15	0.23	0.23	0.61	0.53	0.23	0.00	0.00	
				7:00	8:00	34.20	0.38	9.35	17.78	0.15	1.75	0.61	0.08	0.08	0.46	0.46	0.46	1.14	1.06	0.38	0.00	0.08	
				8:00	9:00	49.40	0.76	18.77	17.86	0.30	4.26	2.74	0.30	0.23	0.38	0.53	0.53	1.22	1.06	0.38	0.00	0.08	
				9:00	10:00	45.60	0.53	13.98	18.09	0.30	3.95	2.89	0.30	0.23	0.30	0.84	0.68	1.44	1.44	0.53	0.00	0.08	
				10:00	11:00	41.80	0.99	12.16	14.14	0.53	6.99	3.12	0.30	0.30	0.30	0.38	0.46	0.91	0.91	0.30	0.00	0.00	
				11:00	12:00	41.80	0.68	12.01	12.31	0.68	9.04	1.29	0.15	0.08	0.53	1.14	0.61	1.37	1.44	0.46	0.00	0.00	
				12:00	13:00	41.80	0.91	15.43	14.82	0.46	5.78	1.06	0.15	0.08	0.38	1.14	0.23	0.53	0.61	0.23	0.00	0.00	
				13:00	14:00	41.80	0.61	10.49	10.11	0.53	7.30	3.88	0.38	0.38	0.38	0.30	1.29	2.51	2.66	0.91	0.00	0.08	
				14:00	15:00	41.80	0.61	10.11	13.83	0.61	6.99	3.04	0.30	0.23	0.46	0.61	0.76	1.75	1.82	0.68	0.00	0.00	
				15:00	16:00	49.40	0.91	13.38	16.57	0.61	7.68	3.80	0.38	0.38	0.38	0.99	0.76	1.52	1.52	0.53	0.00	0.00	
				16:00	17:00	45.60	1.44	16.42	11.10	0.61	7.14	1.29	0.08	0.08	0.53	0.15	1.14	2.28	2.58	0.76	0.00	0.00	
				17:00	18:00	53.20	2.05	23.79	13.45	0.46	5.40	2.51	0.23	0.23	0.46	0.68	0.68	1.37	1.37	0.53	0.00	0.00	
				18:00	19:00	49.40	1.90	22.88	13.15	0.23	3.88	1.44	0.15	0.15	0.15	0.53	0.91	1.60	1.90	0.53	0.00	0.00	
				19:00	20:00	41.80	0.99	19.46	16.95	0.08	1.14	0.53	0.00	0.08	0.46	0.30	0.23	0.68	0.68	0.23	0.00	0.00	
				20:00	21:00	34.20	0.84	13.53	17.48	0.08	0.84	0.15	0.00	0.00	0.61	0.15	0.08	0.23	0.15	0.08	0.00	0.00	
				21:00	22:00	30.40	0.61	10.41	17.40	0.08	0.99	0.30	0.00	0.00	0.46	0.00	0.00	0.08	0.00	0.08	0.00	0.00	
				22:00	23:00	26.60	0.68	7.52	16.42	0.08	0.46	0.00	0.00	0.00	0.46	0.15	0.15	0.30	0.15	0.15	0.00	0.08	
				23:00	0:00	22.80	0.61	6.23	13.76	0.08	0.61	0.00	0.00	0.00	0.53	0.15	0.15	0.38	0.15	0.15	0.00	0.00	
L050	Bailey Street (WB)	Sung Chi Street	Ma Tau Wai Road	0:00	1:00	12.20	0.49	4.09	5.80	0.00	0.31	0.00	0.00	0.00	0.79	0.06	0.06	0.12	0.06	0.00	0.00	0.43	
				1:00	2:00	6.10	0.31	2.26	3.36	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	6.10	0.24	2.14	3.54	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	6.10	0.31	2.32	3.17	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	6.10	0.24	2.07	3.54	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	6.10	0.18	1.95	3.29	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.12	0.12	0.12	0.00	0.00	0.00	
				6:00	7:00	9.15	0.31	2.38	4.33	0.00	0.49	0.06	0.00	0.00	0.67	0.06	0.06	0.18	0.12	0.06	0.00	0.43	
				7:00	8:00	24.40	0.37	7.20	9.52	0.12	0.98	0.24	0.06	0.06	2.44	0.24	0.24	0.61	0.49	0.18	0.00	1.65	
				8:00	9:00	33.55	1.04	13.91	9.21	0.18	2.26	1.16	0.37	0.37	1.77	0.31	0.31	0.61	0.49	0.18	0.00	1.40	
				9:00	10:00	30.50	0.73	10.37	9.33	0.18	2.14	1.28	0.37	0.37	1.53	0.49	0.37	0.85	0.73	0.24	0.00	1.53	
				10:00	11:00	27.45	1.04	9.64	6.89	0.24	4.39	1.10	0.37	0.37	1.28	0.12	0.12	0.31	0.31	0.06	0.00	1.22	
				11:00	12:00	27.45	0.61	9.46	5.86	0.31	5.49	0.37	0.18	0.12	2.26	0.37	0.18	0.43	0.43	0.12	0.00	1.28	
				12:00	13:00	27.45	0.79	11.71	6.95	0.24	3.48	0.37	0.18	0.12	1.65	0.43	0.12	0.18	0.18	0.06	0.00	0.98	
				13:00	14:00	27.45	0.61	8.48	5.06	0.31	4.64	1.34	0.55	0.55	1.71	0.12	0.49	0.92	0.85	0.31	0.00	1.53	
				14:00	15:00	27.45	0.55	8.11	7.08	0.31	4.45	1.04	0.43	0.31	1.95	0.18	0.31	0.61	0.55	0.18	0.00	1.40	
				15:00	16:00	30.50	0.85	10.00	7.87	0.24	4.64	1.22	0.55	0.49	1.59	0.31	0.24	0.55	0.49	0.12	0.00	1.34	
				16:00	17:00	27.45	1.10	12.57	4.51	0.18	4.51	0.24	0.12	0.12	1.71	0.06	0.18	0.37	0.43	0.06	0.00	1.28	
				17:00	18:00	36.60	1.77	19.95	5.98	0.18	3.72	0.49	0.31	0.24	1.83	0.12	0.12	0.24	0.24	0.06	0.00	1.34	
				18:00	19:00	30.50	1.53	17.75	5.49	0.12	2.56	0.31	0.18	0.12	0.73	0.06	0.12	0.24	0.31	0.06	0.00	0.92	
				19:00	20:00	27.45	0.85	14.52	7.69	0.00	0.61	0.12	0.06	0.06	1.77	0.12	0.12	0.18	0.18	0.06	0.00	1.10	
				20:00	21:00	24.40	0.85	10.55	8.42	0.00	0.49	0.06	0.06	0.06	2.56	0.06	0.06	0.06	0.06	0.00	0.00	1.10	
				21:00	22:00	21.35	0.55	8.24	8.42	0.00	0.61	0.12	0.06	0.06	2.14	0.00	0.00	0.00	0.00	0.00	0.00	1.16	
				22:00	23:00	18.30	0.67	5.80	7.69	0.00	0.31	0.00	0.00	0.00	2.01	0.06	0.06	0.12	0.06	0.06	0.00	1.46	
				23:00	0:00	15.25	0.55	4.64	6.41	0.00	0.31	0.00	0.00	0.00	1.71	0.06	0.06	0.12	0.06	0.06	0.00	1.28	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L051	Bailey Street (EB)	Wan On Street	Sung On Street	0:00	1:00	19.00	0.53	4.86	12.08	0.00	0.46	0.00	0.00	0.00	0.23	0.08	0.08	0.38	0.23	0.08	0.00	0.00		
				1:00	2:00	11.40	0.30	3.04	7.68	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	11.40	0.30	2.74	8.06	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.60	0.30	1.90	5.02	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.60	0.23	1.75	5.32	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				5:00	6:00	7.60	0.15	1.60	4.94	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.23	0.15	0.08	0.00	0.00	0.00
				6:00	7:00	15.20	0.23	2.96	9.12	0.00	0.76	0.15	0.00	0.00	0.15	0.23	0.23	0.61	0.53	0.23	0.00	0.00	0.00	0.00
				7:00	8:00	34.20	0.30	9.12	17.94	0.15	1.90	0.53	0.08	0.08	0.46	0.53	0.53	1.06	1.06	0.38	0.00	0.08	0.00	0.08
				8:00	9:00	49.40	0.76	18.16	18.32	0.30	4.48	2.58	0.30	0.23	0.38	0.61	0.61	1.14	1.06	0.38	0.00	0.08	0.00	0.08
				9:00	10:00	45.60	0.53	13.60	18.16	0.30	4.18	2.74	0.30	0.23	0.30	0.99	0.76	1.44	1.44	0.53	0.00	0.08	0.00	0.08
				10:00	11:00	41.80	1.06	11.63	14.36	0.30	7.22	3.12	0.38	0.30	0.30	0.38	0.46	0.99	0.99	0.30	0.00	0.00	0.00	0.00
				11:00	12:00	41.80	0.76	11.48	12.62	0.46	9.20	1.22	0.15	0.08	0.61	1.22	0.68	1.37	1.44	0.53	0.00	0.00	0.00	0.00
				12:00	13:00	41.80	0.91	14.59	15.20	0.30	5.93	1.06	0.15	0.08	0.46	1.29	0.30	0.61	0.68	0.23	0.00	0.00	0.00	0.00
				13:00	14:00	41.80	0.68	9.96	10.34	0.30	7.37	3.72	0.53	0.46	0.46	0.30	1.37	2.58	2.74	0.91	0.00	0.08	0.00	0.08
				14:00	15:00	38.00	0.53	8.66	13.22	0.30	6.38	2.66	0.38	0.23	0.38	0.53	0.84	1.60	1.67	0.61	0.00	0.00	0.00	0.00
				15:00	16:00	45.60	0.84	11.78	15.88	0.30	7.22	3.42	0.46	0.30	0.38	0.99	0.68	1.44	1.44	0.46	0.00	0.00	0.00	0.00
				16:00	17:00	41.80	1.44	14.21	10.56	0.15	6.46	1.22	0.15	0.08	0.53	0.15	1.14	2.28	2.66	0.76	0.00	0.00	0.00	0.00
				17:00	18:00	49.40	2.13	20.75	13.07	0.15	5.02	2.36	0.38	0.23	0.53	0.68	0.68	1.44	1.44	0.53	0.00	0.00	0.00	0.00
				18:00	19:00	45.60	1.98	19.76	12.62	0.08	3.57	1.37	0.23	0.15	0.15	0.61	0.91	1.67	1.90	0.61	0.00	0.00	0.00	0.00
				19:00	20:00	38.00	0.91	16.87	15.96	0.00	1.14	0.46	0.08	0.08	0.38	0.38	0.30	0.61	0.61	0.23	0.00	0.00	0.00	0.00
				20:00	21:00	30.40	0.76	11.40	16.11	0.00	0.68	0.15	0.08	0.00	0.53	0.15	0.08	0.23	0.15	0.08	0.00	0.00	0.00	0.00
				21:00	22:00	30.40	0.61	9.88	17.78	0.00	1.06	0.30	0.08	0.00	0.53	0.00	0.00	0.08	0.00	0.08	0.00	0.00	0.00	0.00
				22:00	23:00	26.60	0.76	7.14	16.80	0.00	0.46	0.00	0.00	0.00	0.46	0.15	0.15	0.30	0.15	0.15	0.00	0.00	0.08	0.00
				23:00	0:00	22.80	0.61	5.85	14.21	0.00	0.61	0.00	0.00	0.00	0.53	0.15	0.15	0.38	0.15	0.15	0.00	0.00	0.00	0.00
L052	Bailey Street (WB)	Sung On Street	Sung Chi Street	0:00	1:00	11.40	0.30	3.50	5.78	0.00	0.23	0.00	0.00	0.00	0.91	0.08	0.00	0.08	0.08	0.00	0.00	0.46		
				1:00	2:00	7.60	0.23	2.66	4.41	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	7.60	0.23	2.43	4.71	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.80	0.15	1.29	2.20	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.80	0.08	1.22	2.43	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.80	0.08	1.14	2.20	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.00	0.00	0.00	0.00	
				6:00	7:00	11.40	0.23	2.74	5.47	0.00	0.53	0.08	0.00	0.00	1.06	0.08	0.08	0.23	0.23	0.08	0.00	0.61	0.00	
				7:00	8:00	26.60	0.15	6.92	10.49	0.08	1.06	0.30	0.08	0.08	3.12	0.38	0.30	0.68	0.53	0.30	0.00	2.13	0.00	
				8:00	9:00	34.20	0.53	13.22	10.18	0.08	2.36	1.22	0.30	0.30	2.20	0.30	0.30	0.68	0.53	0.23	0.00	1.75	0.00	
				9:00	10:00	30.40	0.38	9.58	10.11	0.08	2.13	1.22	0.30	0.30	1.82	0.46	0.38	0.84	0.68	0.30	0.00	1.82	0.00	
				10:00	11:00	30.40	0.76	10.26	8.06	0.15	4.86	1.14	0.46	0.30	1.75	0.15	0.15	0.30	0.30	0.08	0.00	1.67	0.00	
				11:00	12:00	30.40	0.53	9.65	6.84	0.23	5.85	0.46	0.15	0.08	3.04	0.46	0.23	0.53	0.53	0.15	0.00	1.67	0.00	
				12:00	13:00	30.40	0.61	12.31	8.21	0.15	3.80	0.30	0.15	0.08	2.28	0.53	0.08	0.23	0.23	0.08	0.00	1.37	0.00	
				13:00	14:00	26.60	0.38	7.83	5.17	0.15	4.41	1.14	0.46	0.38	2.05	0.15	0.46	0.99	0.91	0.30	0.00	1.82	0.00	
				14:00	15:00	26.60	0.38	7.45	7.14	0.15	4.18	0.99	0.30	0.30	2.20	0.23	0.30	0.61	0.53	0.23	0.00	1.60	0.00	
				15:00	16:00	30.40	0.61	9.58	8.21	0.15	4.48	1.14	0.46	0.38	1.98	0.38	0.23	0.53	0.46	0.15	0.00	1.67	0.00	
				16:00	17:00	30.40	1.06	13.45	5.17	0.15	4.86	0.23	0.15	0.08	2.28	0.08	0.23	0.46	0.46	0.08	0.00	1.67	0.00	
				17:00	18:00	38.00	1.52	19.68	6.61	0.15	3.80	0.53	0.38	0.30	2.36	0.15	0.15	0.30	0.30	0.08	0.00	1.67	0.00	
				18:00	19:00	30.40	1.29	17.40	5.85	0.08	2.43	0.23	0.15	0.15	0.91	0.08	0.15	0.23	0.30	0.08	0.00	1.06	0.00	
				19:00	20:00	30.40	0.68	15.12	9.04	0.00	0.68	0.15	0.00	0.08	2.43	0.15	0.08	0.23	0.23	0.08	0.00	1.44	0.00	
				20:00	21:00	22.80	0.53	9.20	8.21	0.00	0.46	0.08	0.00	0.00	2.89	0.08	0.00	0.08	0.08	0.00	0.00	1.22	0.00	
				21:00	22:00	22.80	0.46	8.13	9.27	0.00	0.61	0.08	0.00	0.00	2.74	0.00	0.00	0.00	0.00	0.00	0.00	1.52	0.00	
				22:00	23:00	19.00	0.46	5.47	8.06	0.00	0.30	0.00	0.00	0.00	2.51	0.08	0.08	0.08	0.08	0.08	0.00	1.82	0.00	
				23:00	0:00	15.20	0.38	4.18	6.54	0.00	0.30	0.00	0.00	0.00	1.98	0.08	0.08	0.08	0.08	0.08	0.00	1.44	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L053	Bailey Street (EB)	Sung On Street	Hung Hom Road	0:00	1:00	20.70	0.55	6.21	11.59	0.00	0.55	0.00	0.00	0.00	1.10	0.07	0.07	0.35	0.14	0.07	0.00	0.00		
				1:00	2:00	10.35	0.28	3.31	6.42	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.35	0.21	2.97	6.83	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	6.90	0.21	2.21	4.14	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	6.90	0.14	2.00	4.42	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	10.35	0.21	2.69	6.07	0.00	0.48	0.00	0.00	0.00	0.00	0.14	0.14	0.28	0.21	0.14	0.00	0.00	0.00	0.00
				6:00	7:00	17.25	0.35	4.00	8.97	0.07	0.90	0.21	0.00	0.00	1.10	0.28	0.28	0.48	0.41	0.21	0.00	0.00	0.00	0.00
				7:00	8:00	37.95	0.35	10.83	17.73	0.21	1.93	0.48	0.14	0.14	2.69	0.48	0.48	1.04	0.97	0.35	0.00	0.14	0.00	0.14
				8:00	9:00	55.20	1.04	21.67	18.01	0.35	4.55	2.48	0.69	0.62	2.07	0.55	0.55	1.10	1.04	0.35	0.00	0.14	0.00	0.14
				9:00	10:00	51.75	0.76	16.77	18.49	0.28	4.35	2.69	0.76	0.62	1.86	0.97	0.76	1.38	1.38	0.55	0.00	0.14	0.00	0.14
				10:00	11:00	48.30	1.24	15.18	14.49	0.41	8.28	2.83	0.69	0.69	1.86	0.28	0.41	0.83	0.76	0.28	0.00	0.07	0.00	0.07
				11:00	12:00	44.85	0.69	14.08	11.59	0.48	9.59	0.97	0.28	0.21	3.11	0.90	0.48	1.10	1.04	0.28	0.00	0.07	0.00	0.07
				12:00	13:00	48.30	0.97	18.98	14.90	0.41	6.56	0.97	0.28	0.28	2.35	1.04	0.28	0.55	0.55	0.14	0.00	0.07	0.00	0.07
				13:00	14:00	44.85	0.69	12.42	9.80	0.48	8.00	3.24	0.83	0.83	2.28	0.28	1.10	2.14	2.14	0.55	0.00	0.07	0.00	0.07
				14:00	15:00	44.85	0.55	12.08	13.80	0.35	7.66	2.48	0.55	0.55	2.42	0.48	0.69	1.45	1.38	0.35	0.00	0.07	0.00	0.07
				15:00	16:00	51.75	0.97	15.59	15.53	0.41	8.21	3.11	0.76	0.76	2.21	0.90	0.55	1.24	1.10	0.35	0.00	0.07	0.00	0.07
				16:00	17:00	44.85	1.38	18.70	9.32	0.21	7.38	0.90	0.21	0.14	2.55	0.14	0.76	1.45	1.45	0.28	0.00	0.00	0.00	0.00
				17:00	18:00	55.20	2.00	27.60	11.59	0.21	5.73	1.73	0.41	0.35	2.62	0.48	0.48	0.97	0.83	0.21	0.00	0.00	0.00	0.00
				18:00	19:00	48.30	1.79	25.67	11.04	0.14	4.00	0.97	0.21	0.21	1.04	0.35	0.55	1.10	1.04	0.21	0.00	0.00	0.00	0.00
				19:00	20:00	44.85	0.97	22.29	15.73	0.07	1.24	0.35	0.07	0.07	2.42	0.28	0.21	0.48	0.48	0.14	0.00	0.07	0.00	0.07
				20:00	21:00	37.95	0.90	15.46	16.42	0.07	0.83	0.14	0.07	0.07	3.31	0.14	0.07	0.21	0.14	0.07	0.00	0.07	0.00	0.07
				21:00	22:00	34.50	0.62	12.49	16.70	0.07	1.10	0.35	0.07	0.07	2.90	0.00	0.00	0.07	0.00	0.00	0.00	0.07	0.00	0.07
				22:00	23:00	31.05	0.76	9.32	16.42	0.07	0.62	0.00	0.00	0.00	2.97	0.14	0.14	0.35	0.14	0.07	0.00	0.07	0.00	0.07
23:00	0:00	24.15	0.62	7.04	12.77	0.07	0.55	0.00	0.00	0.00	2.28	0.14	0.14	0.28	0.14	0.07	0.00	0.07	0.00	0.07				
L054	Bailey Street (WB)	Hung Hom Road	Sung On Street	0:00	1:00	10.35	0.21	2.97	5.18	0.00	0.21	0.00	0.00	0.00	0.90	0.07	0.07	0.21	0.07	0.07	0.00	0.41		
				1:00	2:00	6.90	0.21	2.35	4.14	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	6.90	0.14	2.14	4.42	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.45	0.07	1.17	2.07	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.45	0.07	1.10	2.21	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	6.90	0.14	1.93	3.73	0.00	0.28	0.00	0.00	0.00	0.00	0.14	0.14	0.28	0.14	0.14	0.00	0.00	0.00	0.00
				6:00	7:00	10.35	0.21	2.21	4.55	0.00	0.41	0.07	0.00	0.00	0.97	0.21	0.21	0.41	0.35	0.21	0.00	0.55	0.00	0.55
				7:00	8:00	24.15	0.07	6.21	8.76	0.07	0.90	0.14	0.07	0.07	2.97	0.41	0.41	0.90	0.90	0.35	0.00	1.93	0.00	1.93
				8:00	9:00	34.50	0.35	13.04	9.52	0.07	2.14	0.90	0.41	0.35	2.35	0.55	0.55	1.04	1.04	0.41	0.00	1.79	0.00	1.79
				9:00	10:00	31.05	0.21	9.80	9.25	0.07	1.86	0.97	0.35	0.28	2.00	0.83	0.62	1.17	1.24	0.55	0.00	1.86	0.00	1.86
				10:00	11:00	27.60	0.55	8.83	7.45	0.07	3.93	0.90	0.41	0.28	1.66	0.28	0.28	0.62	0.69	0.21	0.00	1.45	0.00	1.45
				11:00	12:00	27.60	0.35	8.14	6.07	0.14	4.69	0.35	0.14	0.07	2.83	0.76	0.41	0.90	0.97	0.28	0.00	1.52	0.00	1.52
				12:00	13:00	27.60	0.41	10.63	7.52	0.07	3.11	0.28	0.14	0.07	2.14	0.83	0.21	0.41	0.41	0.14	0.00	1.24	0.00	1.24
				13:00	14:00	27.60	0.28	7.31	5.11	0.07	3.86	1.10	0.41	0.41	2.07	0.21	0.90	1.66	1.79	0.69	0.00	1.73	0.00	1.73
				14:00	15:00	24.15	0.28	6.07	6.35	0.07	3.24	0.76	0.28	0.28	2.00	0.35	0.48	1.04	1.10	0.41	0.00	1.45	0.00	1.45
				15:00	16:00	31.05	0.48	9.11	8.42	0.07	4.00	1.10	0.48	0.35	2.00	0.69	0.48	0.97	0.97	0.35	0.00	1.59	0.00	1.59
				16:00	17:00	27.60	0.83	10.42	5.11	0.07	3.73	0.28	0.07	0.07	2.00	0.07	0.62	1.10	1.38	0.35	0.00	1.52	0.00	1.52
				17:00	18:00	34.50	1.24	15.87	6.76	0.07	3.04	0.62	0.28	0.28	2.14	0.41	0.41	0.76	0.76	0.35	0.00	1.52	0.00	1.52
				18:00	19:00	27.60	0.97	13.87	5.80	0.07	1.93	0.35	0.14	0.14	0.76	0.28	0.41	0.76	0.90	0.28	0.00	0.97	0.00	0.97
				19:00	20:00	27.60	0.55	12.83	8.42	0.00	0.62	0.14	0.00	0.07	2.21	0.21	0.21	0.41	0.48	0.14	0.00	1.31	0.00	1.31
				20:00	21:00	20.70	0.35	8.00	7.66	0.00	0.35	0.07	0.00	0.00	2.76	0.07	0.07	0.14	0.07	0.07	0.00	1.10	0.00	1.10
				21:00	22:00	20.70	0.35	7.04	8.63	0.00	0.55	0.07	0.00	0.00	2.62	0.00	0.00	0.07	0.00	0.00	0.00	1.38	0.00	1.38
				22:00	23:00	17.25	0.35	4.62	7.52	0.00	0.28	0.00	0.00	0.00	2.35	0.14	0.07	0.21	0.07	0.07	0.00	1.59	0.00	1.59
23:00	0:00	17.25	0.35	4.49	7.45	0.00	0.35	0.00	0.00	0.00	2.35	0.21	0.07	0.28	0.07	0.07	0.00	1.59	0.00	1.59				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L055	Sung Ping Street (SB)	Bailey Street	Hok Yeun Street East	0:00	1:00	10.90	0.44	6.10	3.92	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	10.90	0.44	5.89	4.14	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.90	0.65	5.45	4.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	10.90	0.87	5.23	3.92	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	10.90	0.87	5.45	4.58	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	10.90	0.87	5.23	3.92	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	10.90	0.44	5.23	4.36	0.00	0.44	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	21.80	0.44	12.43	4.36	0.00	1.09	0.65	0.00	0.00	0.00	0.44	0.44	0.65	0.65	0.65	0.00	0.00	0.00	0.00
				8:00	9:00	32.70	0.87	21.15	3.71	0.00	2.18	2.83	0.22	0.00	0.00	0.22	0.22	0.44	0.44	0.44	0.00	0.00	0.00	0.00
				9:00	10:00	32.70	0.65	17.88	4.36	0.00	2.40	3.71	0.22	0.00	0.00	0.65	0.22	0.87	0.87	0.87	0.00	0.00	0.00	0.00
				10:00	11:00	32.70	1.31	15.70	5.45	0.00	4.36	3.49	0.65	0.44	0.00	0.00	0.00	0.44	0.44	0.44	0.00	0.00	0.00	0.00
				11:00	12:00	21.80	0.44	11.55	3.49	0.00	4.36	0.87	0.22	0.00	0.00	0.22	0.00	0.22	0.22	0.22	0.00	0.00	0.00	0.00
				12:00	13:00	32.70	0.87	20.06	5.67	0.00	3.49	1.31	0.44	0.00	0.00	0.44	0.00	0.44	0.00	0.44	0.00	0.00	0.00	0.00
				13:00	14:00	21.80	0.44	9.81	2.83	0.00	3.27	3.05	0.44	0.44	0.00	0.00	0.22	0.44	0.44	0.44	0.00	0.00	0.00	0.00
				14:00	15:00	21.80	0.44	10.03	3.92	0.00	3.27	2.62	0.44	0.22	0.00	0.00	0.22	0.22	0.22	0.22	0.00	0.00	0.00	0.00
				15:00	16:00	32.70	0.87	15.91	5.89	0.00	4.36	3.71	0.65	0.22	0.00	0.22	0.22	0.22	0.22	0.22	0.00	0.00	0.00	0.00
				16:00	17:00	32.70	1.53	19.18	5.89	0.00	4.36	1.31	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	32.70	1.53	21.15	5.23	0.00	2.40	1.53	0.44	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	32.70	1.53	22.02	5.67	0.00	1.96	1.09	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	32.70	0.87	23.76	6.32	0.00	0.65	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.44	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	21.80	0.87	15.04	5.45	0.00	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	21.80	0.65	13.30	6.76	0.00	0.65	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	21.80	0.87	12.43	8.07	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L056	Sung Ping Street (NB)	Hok Yeun Street East	Bailey Street	23:00	0:00	10.90	0.44	6.10	4.14	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				0:00	1:00	10.90	0.44	5.89	4.14	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	10.90	0.65	5.67	3.92	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	10.90	0.00	5.89	5.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	10.90	0.00	5.89	3.92	0.00	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	10.90	0.00	6.10	4.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	10.90	0.00	5.45	4.36	0.00	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	10.90	0.65	5.01	4.58	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	10.90	0.22	8.50	1.31	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	21.80	0.65	17.44	1.31	0.00	2.18	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	21.80	0.65	16.57	1.74	0.00	2.62	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	21.80	0.65	12.43	4.36	0.00	4.14	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	21.80	0.44	11.99	3.92	0.00	5.45	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	21.80	0.65	13.95	3.92	0.00	3.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	21.80	0.44	11.99	3.71	0.00	5.23	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	21.80	0.44	11.34	4.80	0.00	4.80	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	21.80	0.65	12.43	4.36	0.00	4.14	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	21.80	0.65	11.99	5.89	0.00	3.05	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	21.80	0.65	13.30	5.67	0.00	1.96	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	21.80	0.87	13.30	5.89	0.00	1.53	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	21.80	0.65	16.35	4.14	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	21.80	0.65	15.04	5.67	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	10.90	0.22	6.98	3.27	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	10.90	0.22	6.54	3.92	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				
23:00	0:00	10.90	0.44	6.10	3.92	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L057	Hung Hom Road (SB)	Bailey Street	Hok Yeun Street East	3:00	1:00	46.20	1.23	13.09	26.95	0.00	1.23	0.00	0.00	0.00	2.31	0.15	0.15	0.62	0.31	0.15	0.00	0.00	
				1:00	2:00	30.80	0.77	9.09	19.71	0.00	1.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	23.10	0.46	6.31	15.71	0.00	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	23.10	0.62	6.78	14.63	0.00	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	15.40	0.46	4.00	10.32	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00	0.00	0.00
				5:00	6:00	23.10	0.62	5.54	14.17	0.00	1.23	0.00	0.00	0.00	0.00	0.00	0.15	0.15	0.62	0.46	0.15	0.00	0.00
				6:00	7:00	38.50	0.77	8.16	21.41	0.15	1.85	0.31	0.00	0.00	2.31	0.46	0.46	1.23	1.08	0.31	0.00	0.00	
				7:00	8:00	92.40	0.77	23.41	46.66	0.46	4.47	1.23	0.31	0.31	6.01	1.23	1.23	2.46	2.62	0.92	0.00	0.31	
				8:00	9:00	138.60	2.46	49.59	49.59	0.77	11.24	6.31	1.85	1.69	4.93	1.54	1.54	2.93	2.93	0.92	0.00	0.31	
				9:00	10:00	123.20	1.69	36.04	48.36	0.62	10.16	6.47	1.69	1.54	4.00	2.16	1.69	3.54	3.70	1.23	0.00	0.31	
				10:00	11:00	107.80	2.62	32.49	34.50	0.92	18.48	5.85	1.54	1.39	3.85	0.77	0.92	1.85	1.85	0.62	0.00	0.15	
				11:00	12:00	107.80	1.69	31.72	29.41	1.23	23.56	2.16	0.62	0.46	6.93	2.31	1.23	2.77	2.77	0.77	0.00	0.15	
				12:00	13:00	107.80	2.00	40.50	35.57	0.77	15.09	2.00	0.62	0.46	4.93	2.46	0.62	1.08	1.23	0.31	0.00	0.15	
				13:00	14:00	107.80	1.69	28.03	25.10	1.23	19.56	7.39	2.00	1.85	5.24	0.62	2.77	5.24	5.39	1.54	0.00	0.15	
				14:00	15:00	107.80	1.23	27.10	35.27	0.92	18.48	5.54	1.54	1.23	5.54	1.23	1.85	3.39	3.39	0.92	0.00	0.15	
				15:00	16:00	123.20	2.16	34.96	39.73	1.08	19.71	6.93	2.00	1.69	4.77	2.16	1.54	2.77	2.77	0.77	0.00	0.15	
				16:00	17:00	107.80	3.23	44.04	22.33	0.77	18.63	1.69	0.46	0.31	5.85	0.31	2.00	3.70	3.70	0.77	0.00	0.00	
				17:00	18:00	130.90	4.77	64.53	27.72	0.62	14.48	3.39	0.92	0.77	5.85	1.39	1.39	2.31	2.16	0.62	0.00	0.00	
				18:00	19:00	115.50	4.47	60.21	26.33	0.31	10.16	2.16	0.46	0.46	2.46	0.92	1.69	2.62	2.62	0.62	0.00	0.00	
				19:00	20:00	107.80	2.31	51.13	40.35	0.15	2.93	0.77	0.15	0.15	5.54	0.77	0.62	1.23	1.23	0.31	0.00	0.15	
				20:00	21:00	84.70	2.00	32.80	38.65	0.15	1.85	0.31	0.15	0.15	7.08	0.31	0.15	0.46	0.31	0.15	0.00	0.15	
				21:00	22:00	77.00	1.23	26.18	39.42	0.15	2.62	0.62	0.15	0.15	6.01	0.00	0.00	0.15	0.00	0.15	0.00	0.15	
				22:00	23:00	69.30	1.69	19.56	38.50	0.15	1.23	0.00	0.00	0.00	6.16	0.31	0.31	0.62	0.31	0.31	0.00	0.15	
23:00	0:00	61.60	1.69	16.63	34.34	0.15	1.54	0.00	0.00	0.00	5.24	0.31	0.31	0.62	0.31	0.31	0.00	0.15					
L058	Hung Hom Road (NB)	Hok Yeun Street East	Bailey Street	0:00	1:00	30.80	0.62	8.47	15.71	0.00	0.77	0.00	0.00	0.00	2.46	0.15	0.15	0.62	0.46	0.15	0.00	1.23	
				1:00	2:00	15.40	0.46	5.08	9.24	0.00	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	15.40	0.15	4.62	10.16	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	15.40	0.31	5.08	9.09	0.00	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	7.70	0.15	2.31	4.93	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	15.40	0.31	4.00	8.47	0.00	0.77	0.00	0.00	0.00	0.00	0.31	0.31	0.46	0.46	0.31	0.00	0.00	
				6:00	7:00	23.10	0.31	4.62	10.78	0.00	1.08	0.15	0.00	0.00	2.00	0.31	0.46	1.08	0.92	0.31	0.00	1.08	
				7:00	8:00	53.90	0.00	12.17	20.48	0.15	1.85	0.77	0.15	0.15	6.31	1.08	1.08	2.31	2.46	0.92	0.00	4.00	
				8:00	9:00	77.00	0.15	25.26	22.48	0.15	4.77	3.85	0.92	0.92	5.08	1.39	1.39	2.77	2.77	1.23	0.00	3.85	
				9:00	10:00	77.00	0.15	20.64	23.25	0.15	4.62	4.16	0.92	0.92	4.62	2.31	1.69	3.70	4.00	1.54	0.00	4.31	
				10:00	11:00	61.60	1.08	18.02	16.17	0.31	9.55	3.23	0.77	0.92	3.39	0.62	0.77	1.54	1.69	0.62	0.00	2.93	
				11:00	12:00	69.30	0.77	18.94	15.09	0.46	12.78	1.39	0.31	0.31	6.47	2.16	1.23	2.31	2.77	0.77	0.00	3.54	
				12:00	13:00	61.60	0.92	22.48	16.94	0.31	7.70	1.08	0.31	0.31	4.31	2.00	0.46	0.92	1.08	0.31	0.00	2.46	
				13:00	14:00	69.30	0.77	16.17	12.32	0.31	10.32	4.16	1.08	1.23	4.62	0.77	2.31	4.62	5.08	1.69	0.00	3.85	
				14:00	15:00	61.60	0.62	14.17	15.71	0.31	8.93	3.08	0.77	0.77	4.62	0.92	1.39	2.77	3.08	1.08	0.00	3.39	
				15:00	16:00	69.30	0.92	18.79	18.17	0.31	9.70	3.70	0.92	0.92	4.00	1.69	1.23	2.31	2.46	0.92	0.00	3.23	
				16:00	17:00	69.30	2.16	25.87	12.01	0.31	10.93	0.92	0.31	0.31	4.47	0.31	1.54	2.77	3.23	0.77	0.00	3.39	
				17:00	18:00	84.70	3.23	39.27	15.40	0.31	8.78	1.85	0.62	0.62	4.77	1.08	1.08	1.85	1.85	0.62	0.00	3.39	
				18:00	19:00	69.30	2.77	34.65	13.86	0.15	5.85	0.92	0.31	0.31	1.85	0.62	1.08	2.00	2.16	0.62	0.00	2.16	
				19:00	20:00	61.60	1.08	28.18	19.10	0.00	1.54	0.46	0.15	0.15	4.62	0.62	0.46	1.08	1.08	0.31	0.00	2.77	
				20:00	21:00	53.90	0.92	20.17	20.64	0.00	1.08	0.15	0.15	0.15	6.78	0.15	0.15	0.31	0.31	0.15	0.00	2.77	
				21:00	22:00	46.20	0.62	15.25	19.71	0.00	1.39	0.31	0.15	0.15	5.54	0.00	0.00	0.15	0.00	0.15	0.00	2.77	
				22:00	23:00	46.20	0.92	12.01	20.79	0.00	0.77	0.00	0.00	0.00	6.01	0.31	0.31	0.46	0.31	0.31	0.00	4.00	
23:00	0:00	38.50	0.77	9.70	16.94	0.00	0.77	0.00	0.00	0.00	5.08	0.31	0.31	0.62	0.31	0.31	0.00	3.39					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L059	Sung On Street (SB)	Bailey Street	Access Road	0:00	1:00	4.75	0.00	2.47	1.33	0.00	0.29	0.00	0.00	0.00	0.00	0.10	0.10	0.29	0.10	0.10	0.00	0.00	
				1:00	2:00	4.75	0.00	2.85	1.62	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.75	0.00	2.66	1.81	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.75	0.00	2.95	1.43	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.75	0.00	2.57	1.71	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.75	0.00	1.71	1.05	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.29	0.29	0.57	0.29	0.29	0.00	0.00
				6:00	7:00	4.75	0.00	1.71	0.95	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.29	0.29	0.57	0.57	0.10	0.00	0.00
				7:00	8:00	14.25	0.00	6.27	2.85	0.00	0.86	0.29	0.00	0.00	0.00	0.48	0.48	1.24	1.24	0.57	0.00	0.00	0.00
				8:00	9:00	19.00	0.00	10.26	2.28	0.00	1.62	1.14	0.10	0.10	0.00	0.48	0.48	1.05	1.05	0.48	0.00	0.00	0.00
				9:00	10:00	19.00	0.00	8.17	2.47	0.00	1.62	1.33	0.10	0.10	0.00	0.86	0.67	1.43	1.52	0.76	0.00	0.00	0.00
				10:00	11:00	14.25	0.10	5.70	1.43	0.00	2.85	0.86	0.38	0.19	0.00	0.38	0.48	0.76	0.76	0.38	0.00	0.00	0.00
				11:00	12:00	14.25	0.10	5.23	1.14	0.00	3.33	0.29	0.10	0.10	0.00	0.95	0.57	1.05	1.14	0.29	0.00	0.00	0.00
				12:00	13:00	14.25	0.10	7.32	1.62	0.00	2.28	0.38	0.10	0.10	0.00	1.05	0.19	0.48	0.57	0.10	0.00	0.00	0.00
				13:00	14:00	19.00	0.10	5.32	1.14	0.00	3.23	1.24	0.48	0.38	0.00	0.38	1.24	2.19	2.47	0.86	0.00	0.00	0.00
				14:00	15:00	14.25	0.10	4.47	1.43	0.00	2.66	0.76	0.29	0.19	0.00	0.48	0.67	1.33	1.43	0.48	0.00	0.00	0.00
				15:00	16:00	14.25	0.10	5.13	1.43	0.00	2.47	0.86	0.38	0.29	0.00	0.76	0.48	0.95	1.05	0.38	0.00	0.00	0.00
				16:00	17:00	14.25	0.29	5.70	0.48	0.00	2.57	0.10	0.29	0.10	0.00	0.10	1.05	1.52	1.71	0.38	0.00	0.00	0.00
				17:00	18:00	14.25	0.29	7.60	0.57	0.00	1.90	0.10	0.48	0.29	0.00	0.57	0.57	0.86	0.86	0.19	0.00	0.00	0.00
				18:00	19:00	14.25	0.38	7.51	0.57	0.00	1.43	0.10	0.29	0.29	0.00	0.48	0.76	1.05	1.14	0.29	0.00	0.00	0.00
				19:00	20:00	14.25	0.10	9.79	1.90	0.00	0.48	0.10	0.00	0.00	0.00	0.38	0.29	0.48	0.67	0.10	0.00	0.00	0.00
				20:00	21:00	9.50	0.10	6.46	1.90	0.00	0.38	0.00	0.00	0.00	0.00	0.10	0.10	0.29	0.10	0.10	0.00	0.00	0.00
				21:00	22:00	9.50	0.19	6.08	2.38	0.00	0.48	0.19	0.00	0.00	0.00	0.00	0.00	0.19	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.50	0.19	5.13	2.66	0.00	0.38	0.00	0.00	0.00	0.00	0.19	0.19	0.38	0.19	0.19	0.00	0.00	0.00
				23:00	0:00	4.75	0.10	2.66	1.24	0.00	0.19	0.00	0.00	0.00	0.00	0.10	0.10	0.19	0.10	0.10	0.00	0.00	0.00
L060	Sung On Street (NB)	Access Road	Bailey Street	0:00	1:00	4.75	0.29	2.28	1.24	0.00	0.29	0.00	0.00	0.00	0.00	0.10	0.10	0.29	0.10	0.10	0.00	0.00	
				1:00	2:00	4.75	0.19	2.76	1.62	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.75	0.29	2.57	1.62	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.75	0.38	2.66	1.33	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.75	0.38	2.57	1.43	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.75	0.29	1.71	1.33	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.29	0.29	0.29	0.29	0.00	0.00	0.00
				6:00	7:00	4.75	0.19	1.71	1.24	0.00	0.29	0.00	0.00	0.00	0.00	0.19	0.19	0.48	0.29	0.19	0.00	0.00	0.00
				7:00	8:00	14.25	0.10	4.66	3.61	0.00	1.05	0.29	0.00	0.00	0.00	0.67	0.67	1.33	1.33	0.57	0.00	0.00	0.00
				8:00	9:00	19.00	0.38	8.17	2.95	0.00	2.09	1.05	0.19	0.19	0.00	0.67	0.67	1.14	1.14	0.38	0.00	0.00	0.00
				9:00	10:00	19.00	0.38	6.18	3.14	0.00	2.09	1.24	0.19	0.19	0.00	1.05	0.76	1.52	1.71	0.57	0.00	0.00	0.00
				10:00	11:00	14.25	0.48	6.37	1.71	0.00	2.85	0.86	0.19	0.10	0.00	0.19	0.19	0.57	0.57	0.19	0.00	0.00	0.00
				11:00	12:00	14.25	0.29	6.18	1.33	0.00	3.42	0.29	0.10	0.00	0.00	0.67	0.29	0.76	0.76	0.19	0.00	0.00	0.00
				12:00	13:00	14.25	0.38	7.60	1.71	0.00	2.28	0.38	0.10	0.00	0.00	0.76	0.19	0.38	0.38	0.10	0.00	0.00	0.00
				13:00	14:00	14.25	0.19	4.66	1.05	0.00	2.66	1.05	0.29	0.19	0.00	0.19	0.76	1.43	1.33	0.48	0.00	0.00	0.00
				14:00	15:00	14.25	0.29	5.04	1.62	0.00	2.85	0.86	0.29	0.10	0.00	0.38	0.48	1.05	0.95	0.38	0.00	0.00	0.00
				15:00	16:00	14.25	0.29	5.99	1.62	0.00	2.57	0.86	0.29	0.10	0.00	0.48	0.38	0.76	0.67	0.29	0.00	0.00	0.00
				16:00	17:00	14.25	0.57	9.50	0.38	0.00	2.19	0.29	0.10	0.00	0.00	0.00	0.29	0.57	0.29	0.10	0.00	0.00	0.00
				17:00	18:00	19.00	0.86	14.54	0.48	0.00	1.71	0.38	0.29	0.00	0.00	0.10	0.10	0.38	0.10	0.10	0.00	0.00	0.00
				18:00	19:00	14.25	0.57	11.31	0.38	0.00	1.05	0.19	0.10	0.00	0.00	0.10	0.10	0.29	0.10	0.10	0.00	0.00	0.00
				19:00	20:00	14.25	0.48	9.88	1.90	0.00	0.48	0.10	0.00	0.00	0.00	0.29	0.29	0.38	0.38	0.10	0.00	0.00	0.00
				20:00	21:00	9.50	0.38	6.46	1.81	0.00	0.38	0.00	0.00	0.00	0.00	0.10	0.10	0.10	0.10	0.10	0.00	0.00	0.00
				21:00	22:00	9.50	0.29	6.27	2.28	0.00	0.48	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.50	0.48	5.13	2.57	0.00	0.29	0.00	0.00	0.00	0.00	0.19	0.19	0.29	0.19	0.19	0.00	0.00	0.00
				23:00	0:00	4.75	0.19	2.47	1.33	0.00	0.19	0.00	0.00	0.00	0.00	0.10	0.10	0.19	0.10	0.10	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L061	Sung On Street (SB)	Access Road	Hok Yeun Street East	3:00	1:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.30	0.00	2.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.30	0.00	2.21	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.30	0.00	1.93	0.00	0.00	0.05	0.28	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.30	0.00	1.84	0.00	0.00	0.05	0.37	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.30	0.05	1.66	0.00	0.00	0.23	0.32	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.30	0.09	1.70	0.00	0.00	0.37	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.30	0.05	1.93	0.00	0.00	0.18	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.30	0.05	1.52	0.00	0.00	0.28	0.41	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.30	0.09	1.56	0.00	0.00	0.23	0.32	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.30	0.05	1.61	0.00	0.00	0.23	0.37	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.30	0.14	1.84	0.00	0.00	0.28	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.60	0.23	3.82	0.00	0.00	0.32	0.14	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.60	0.28	3.86	0.00	0.00	0.28	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.30	0.05	2.16	0.00	0.00	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.30	0.09	2.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.30	0.09	2.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.30	0.14	2.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.30	0.18	2.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L062	Sung On Street (NB)	Access Road	Hok Yeun Street East	0:00	1:00	2.30	0.09	1.10	1.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.30	0.18	0.97	1.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.30	0.00	0.97	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.30	0.00	1.15	1.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.30	0.00	1.15	1.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.30	0.00	1.01	1.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.30	0.14	0.83	0.92	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.14	0.14	0.00	0.00	0.00	0.00
				7:00	8:00	4.60	0.05	1.75	1.75	0.00	0.14	0.14	0.00	0.00	0.00	0.14	0.14	0.23	0.23	0.05	0.00	0.00	0.00
				8:00	9:00	6.90	0.09	3.45	1.75	0.00	0.28	0.46	0.09	0.09	0.00	0.09	0.09	0.23	0.23	0.05	0.00	0.00	0.00
				9:00	10:00	6.90	0.05	3.22	1.47	0.00	0.32	0.55	0.14	0.14	0.00	0.18	0.14	0.32	0.32	0.05	0.00	0.00	0.00
				10:00	11:00	4.60	0.14	2.07	1.01	0.00	0.41	0.60	0.05	0.05	0.00	0.05	0.05	0.05	0.14	0.00	0.00	0.00	0.00
				11:00	12:00	4.60	0.09	2.12	1.01	0.00	0.55	0.32	0.00	0.00	0.00	0.14	0.09	0.14	0.14	0.00	0.00	0.00	0.00
				12:00	13:00	4.60	0.14	2.44	1.10	0.00	0.37	0.23	0.00	0.00	0.00	0.14	0.00	0.09	0.09	0.00	0.00	0.00	0.00
				13:00	14:00	4.60	0.09	1.56	0.69	0.00	0.41	0.74	0.09	0.14	0.00	0.09	0.14	0.28	0.28	0.09	0.00	0.00	0.00
				14:00	15:00	4.60	0.09	1.52	1.01	0.00	0.46	0.60	0.09	0.09	0.00	0.09	0.09	0.23	0.23	0.09	0.00	0.00	0.00
				15:00	16:00	4.60	0.14	1.84	1.01	0.00	0.37	0.64	0.05	0.05	0.00	0.14	0.05	0.14	0.14	0.05	0.00	0.00	0.00
				16:00	17:00	2.30	0.23	1.56	0.00	0.00	0.28	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.30	0.18	1.70	0.00	0.00	0.14	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.30	0.23	1.79	0.00	0.00	0.09	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	4.60	0.18	3.96	0.00	0.00	0.09	0.09	0.00	0.00	0.00	0.09	0.00	0.09	0.09	0.00	0.00	0.00	0.00
				20:00	21:00	2.30	0.14	1.89	0.23	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.30	0.09	1.70	0.32	0.00	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.30	0.09	1.75	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.30	0.09	1.33	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L063	Sung Chi Street (SB)	Access Road	Hok Yeu Street	0:00	1:00	3.55	0.00	1.78	1.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	0.00																	
				2:00	3:00	0.00																	
				3:00	4:00	0.00																	
				4:00	5:00	0.00																	
				5:00	6:00	0.00																	
				6:00	7:00	3.55	0.00	0.00	3.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	3.55	0.00	1.70	0.92	0.00	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	3.55	0.00	2.20	0.43	0.00	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	3.55	0.00	2.06	0.50	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	3.55	0.00	1.42	0.71	0.00	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	3.55	0.00	1.21	0.57	0.00	1.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	3.55	0.00	1.78	0.57	0.00	1.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	3.55	0.00	1.42	0.71	0.00	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	3.55	0.00	1.42	0.71	0.00	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	3.55	0.00	1.42	0.71	0.00	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	3.55	0.00	1.14	1.21	0.00	1.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	3.55	0.00	1.70	0.92	0.00	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	3.55	0.00	1.70	0.92	0.00	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	3.55	0.00	2.63	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	3.55	0.00	2.34	1.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	3.55	0.00	2.34	1.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	3.55	0.00	1.78	1.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	3.55	0.00	1.78	1.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L064	Sung Chi Street (NB)	Hok Yeu Street	Bailey Street	0:00	1:00	8.45	1.86	4.73	1.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	8.45	1.86	4.73	1.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.45	2.20	4.06	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	8.45	2.87	4.23	1.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.45	1.69	5.07	1.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	8.45	1.35	4.23	2.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	8.45	1.52	3.72	2.37	0.00	0.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	8.45	1.35	4.90	1.35	0.34	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	16.90	3.38	9.63	1.18	0.51	1.18	0.51	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	16.90	3.04	8.79	1.52	0.68	1.52	0.68	0.68	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	8.45	1.35	3.89	0.85	0.51	1.35	0.34	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	8.45	1.01	4.23	0.68	0.51	1.86	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	8.45	1.01	4.90	0.85	0.34	1.18	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	8.45	1.01	3.55	0.68	0.68	1.69	0.68	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	8.45	0.85	3.72	1.01	0.68	1.69	0.34	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	16.90	2.37	7.61	2.03	1.01	2.54	1.01	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	8.45	1.01	4.39	1.01	0.34	1.52	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	16.90	2.20	10.31	1.69	0.51	1.69	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	16.90	2.20	10.82	1.86	0.34	1.35	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	8.45	1.18	6.08	1.01	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	8.45	1.35	5.58	1.35	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	8.45	1.18	5.24	1.69	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	8.45	1.69	4.39	2.03	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	8.45	1.86	4.39	1.86	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L065	Ma Tau Wai Road (SB)	Bailey Street	Pak Kung Street	0:00	1:00	26.75	0.86	5.56	13.05	0.00	0.54	0.00	0.00	0.00	2.46	0.11	0.11	0.21	0.11	0.11	0.00	3.64
				1:00	2:00	16.05	0.64	4.28	10.59	0.00	0.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	10.70	0.32	2.68	7.49	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	10.70	0.43	2.89	6.96	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	10.70	0.32	2.68	7.38	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	10.70	0.32	2.35	7.06	0.00	0.43	0.00	0.00	0.00	0.00	0.11	0.11	0.11	0.11	0.11	0.00	0.00
				6:00	7:00	21.40	0.43	3.32	9.74	0.11	0.75	0.11	0.00	0.00	2.14	0.11	0.21	0.32	0.32	0.11	0.00	3.75
				7:00	8:00	64.20	0.54	12.52	18.30	0.32	2.03	0.86	0.11	0.00	7.17	0.75	0.75	1.50	1.39	0.43	0.00	17.55
				8:00	9:00	80.25	1.50	24.72	18.62	0.54	4.82	3.75	0.32	0.21	5.46	0.86	0.86	1.61	1.50	0.54	0.00	14.98
				9:00	10:00	74.90	0.96	18.83	18.51	0.43	4.39	3.96	0.32	0.21	4.60	1.18	0.96	2.03	1.93	0.54	0.00	16.05
				10:00	11:00	58.85	1.61	13.27	15.94	0.54	6.53	3.21	0.43	0.32	3.64	0.21	0.32	0.64	0.54	0.21	0.00	11.45
				11:00	12:00	64.20	1.07	13.59	14.45	0.86	8.67	1.39	0.11	0.11	7.06	0.86	0.43	1.07	0.86	0.21	0.00	13.48
				12:00	13:00	58.85	1.28	16.69	16.80	0.54	5.35	1.07	0.11	0.11	4.92	0.86	0.21	0.43	0.43	0.11	0.00	9.95
				13:00	14:00	58.85	0.96	11.24	11.02	0.64	6.63	3.85	0.43	0.43	4.71	0.21	0.86	1.71	1.61	0.54	0.00	14.02
				14:00	15:00	58.85	0.75	10.70	15.52	0.54	6.21	3.00	0.32	0.32	5.14	0.32	0.54	1.28	1.07	0.32	0.00	12.84
				15:00	16:00	69.55	1.39	14.66	18.83	0.75	7.17	3.96	0.54	0.54	4.82	0.75	0.54	1.07	0.86	0.32	0.00	13.38
				16:00	17:00	53.50	2.03	14.12	13.59	0.32	5.24	0.86	0.11	0.11	4.71	0.00	0.21	0.54	0.21	0.11	0.00	11.34
				17:00	18:00	69.55	3.10	22.90	18.19	0.32	4.39	1.71	0.32	0.32	5.24	0.11	0.11	0.32	0.11	0.11	0.00	12.31
				18:00	19:00	53.50	2.57	19.58	15.94	0.21	2.89	0.96	0.21	0.21	1.93	0.11	0.11	0.32	0.11	0.11	0.00	8.24
				19:00	20:00	58.85	1.39	20.54	18.40	0.11	0.96	0.43	0.00	0.00	5.14	0.21	0.21	0.43	0.43	0.11	0.00	10.49
				20:00	21:00	53.50	1.28	14.12	18.94	0.11	0.75	0.11	0.00	0.00	7.17	0.11	0.11	0.11	0.11	0.11	0.00	10.49
				21:00	22:00	48.15	0.75	10.81	18.62	0.11	0.96	0.32	0.00	0.00	5.78	0.00	0.00	0.11	0.00	0.00	0.00	10.70
				22:00	23:00	48.15	0.96	7.92	18.08	0.11	0.43	0.00	0.00	0.00	5.89	0.11	0.11	0.21	0.11	0.11	0.00	14.12
				23:00	0:00	42.80	0.96	6.85	16.05	0.11	0.43	0.00	0.00	0.00	5.14	0.11	0.11	0.21	0.11	0.11	0.00	12.63
L066	Ma Tau Wai Road (NB)	Pak Kung Street	Bailey Street	0:00	1:00	21.40	0.64	3.85	10.70	0.00	0.32	0.00	0.00	0.00	2.57	0.11	0.11	0.32	0.11	0.11	0.00	2.57
				1:00	2:00	10.70	0.43	2.57	7.38	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.70	0.32	2.35	7.70	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	10.70	0.43	2.68	7.17	0.00	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.35	0.21	1.18	3.85	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	10.70	0.32	2.03	6.63	0.00	0.54	0.00	0.00	0.00	0.00	0.21	0.21	0.32	0.21	0.21	0.00	0.00
				6:00	7:00	21.40	0.43	2.78	9.31	0.11	0.64	0.32	0.00	0.00	2.57	0.32	0.32	0.64	0.54	0.32	0.00	3.10
				7:00	8:00	48.15	0.32	6.10	14.12	0.21	1.39	0.54	0.11	0.11	6.31	0.86	0.86	1.71	1.50	0.54	0.00	13.48
				8:00	9:00	58.85	0.96	12.73	15.41	0.43	3.32	2.35	0.32	0.32	5.03	0.96	0.96	1.93	1.61	0.43	0.00	12.09
				9:00	10:00	53.50	0.64	8.88	14.02	0.43	2.89	2.35	0.32	0.32	4.07	1.39	1.07	2.25	2.14	0.54	0.00	12.20
				10:00	11:00	53.50	1.28	10.27	13.38	0.86	6.21	3.85	0.43	0.43	4.28	0.54	0.54	1.07	0.96	0.43	0.00	8.99
				11:00	12:00	53.50	0.86	9.42	10.81	1.07	7.49	1.39	0.11	0.11	7.49	1.18	0.75	1.39	1.39	0.43	0.00	9.63
				12:00	13:00	48.15	0.96	11.77	13.38	0.64	4.60	1.18	0.11	0.11	5.35	1.18	0.32	0.54	0.64	0.21	0.00	7.17
				13:00	14:00	53.50	0.86	8.24	9.20	0.86	5.89	4.39	0.43	0.43	5.24	0.32	1.39	2.57	2.57	0.96	0.00	10.17
				14:00	15:00	53.50	0.64	8.03	12.52	0.86	5.67	3.53	0.43	0.43	5.78	0.64	0.86	1.93	1.82	0.64	0.00	9.74
				15:00	16:00	58.85	0.96	10.49	15.41	0.86	6.21	4.17	0.43	0.43	5.14	0.96	0.75	1.50	1.39	0.43	0.00	9.74
				16:00	17:00	53.50	1.61	13.48	11.34	0.75	5.99	1.93	0.11	0.11	6.21	0.11	0.64	1.39	1.61	0.54	0.00	7.70
				17:00	18:00	64.20	2.35	20.97	14.23	0.64	4.71	3.64	0.21	0.21	6.42	0.43	0.43	0.86	0.86	0.43	0.00	7.81
				18:00	19:00	53.50	2.25	19.47	13.80	0.32	3.32	2.14	0.11	0.11	2.68	0.32	0.54	1.07	1.18	0.43	0.00	5.78
				19:00	20:00	48.15	1.07	14.77	15.19	0.11	0.96	0.54	0.00	0.00	5.67	0.32	0.32	0.64	0.64	0.21	0.00	7.70
				20:00	21:00	42.80	0.86	9.95	15.41	0.11	0.54	0.21	0.00	0.00	7.60	0.11	0.11	0.21	0.11	0.11	0.00	7.49
				21:00	22:00	37.45	0.54	7.49	14.55	0.11	0.75	0.32	0.00	0.00	6.10	0.00	0.00	0.11	0.00	0.00	0.00	7.49
				22:00	23:00	42.80	0.86	6.21	15.94	0.11	0.43	0.00	0.00	0.00	7.06	0.21	0.11	0.32	0.11	0.11	0.00	11.34
				23:00	0:00	32.10	0.64	4.49	11.77	0.11	0.43	0.00	0.00	0.00	5.24	0.21	0.11	0.32	0.11	0.11	0.00	8.56

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L067	Ma Tau Wai Road (SB)	Pak Kung Street	Hok Yeun Street	0:00	1:00	10.00	0.32	2.12	4.84	0.00	0.20	0.00	0.00	0.00	0.92	0.04	0.04	0.08	0.04	0.04	0.00	1.36	
				1:00	2:00	6.00	0.24	1.64	3.96	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.00	0.12	1.00	2.80	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.00	0.16	1.08	2.60	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.00	0.12	1.00	2.76	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.00	0.12	0.88	2.64	0.00	0.16	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.04	0.00	0.00	0.00
				6:00	7:00	8.00	0.16	1.24	3.76	0.04	0.24	0.04	0.00	0.00	0.76	0.04	0.08	0.12	0.12	0.04	0.00	1.36	0.00
				7:00	8:00	24.00	0.20	4.76	6.80	0.12	0.76	0.32	0.04	0.00	2.68	0.28	0.28	0.56	0.52	0.16	0.00	6.52	0.00
				8:00	9:00	30.00	0.56	9.44	6.92	0.24	1.76	1.40	0.12	0.08	2.00	0.32	0.32	0.56	0.56	0.20	0.00	5.52	0.00
				9:00	10:00	28.00	0.44	7.08	6.88	0.20	1.64	1.48	0.12	0.08	1.72	0.44	0.36	0.72	0.72	0.20	0.00	5.92	0.00
				10:00	11:00	24.00	0.64	5.52	6.44	0.28	2.64	1.36	0.16	0.12	1.48	0.08	0.12	0.28	0.20	0.08	0.00	4.60	0.00
				11:00	12:00	24.00	0.44	5.20	5.40	0.32	3.24	0.52	0.04	0.04	2.60	0.28	0.16	0.40	0.32	0.08	0.00	4.96	0.00
				12:00	13:00	22.00	0.48	6.32	6.24	0.20	2.00	0.40	0.04	0.04	1.84	0.32	0.08	0.16	0.16	0.04	0.00	3.68	0.00
				13:00	14:00	24.00	0.40	4.64	4.52	0.32	2.68	1.60	0.16	0.16	1.88	0.08	0.32	0.68	0.64	0.20	0.00	5.72	0.00
				14:00	15:00	22.00	0.28	4.04	5.84	0.24	2.32	1.16	0.12	0.12	1.88	0.12	0.20	0.44	0.40	0.12	0.00	4.72	0.00
				15:00	16:00	26.00	0.56	5.60	7.12	0.28	2.64	1.48	0.16	0.16	1.76	0.28	0.16	0.40	0.32	0.12	0.00	4.96	0.00
				16:00	17:00	20.00	0.72	5.44	5.08	0.12	1.96	0.32	0.04	0.04	1.72	0.00	0.08	0.20	0.08	0.04	0.00	4.16	0.00
				17:00	18:00	26.00	1.16	8.68	6.80	0.12	1.64	0.68	0.12	0.12	1.88	0.04	0.04	0.12	0.04	0.04	0.00	4.52	0.00
				18:00	19:00	22.00	1.04	8.24	6.56	0.08	1.16	0.40	0.08	0.08	0.76	0.04	0.04	0.12	0.04	0.04	0.00	3.32	0.00
				19:00	20:00	22.00	0.52	7.72	6.88	0.04	0.36	0.16	0.00	0.00	1.92	0.08	0.08	0.16	0.16	0.04	0.00	3.88	0.00
				20:00	21:00	20.00	0.48	5.40	7.04	0.04	0.28	0.04	0.00	0.00	2.64	0.04	0.04	0.04	0.04	0.04	0.00	3.88	0.00
				21:00	22:00	18.00	0.28	4.12	6.96	0.04	0.32	0.12	0.00	0.00	2.16	0.00	0.00	0.04	0.00	0.00	0.00	3.96	0.00
				22:00	23:00	18.00	0.40	3.08	6.68	0.04	0.16	0.00	0.00	0.00	2.16	0.04	0.04	0.08	0.04	0.04	0.00	5.24	0.00
				23:00	0:00	16.00	0.36	2.60	6.04	0.04	0.16	0.00	0.00	0.00	1.92	0.04	0.04	0.08	0.04	0.04	0.00	4.64	0.00
L068	Ma Tau Wai Road (NB)	Hok Yeun Street	Pak Kung Street	0:00	1:00	16.00	0.60	3.40	8.68	0.00	0.24	0.00	0.00	0.00	1.68	0.08	0.08	0.16	0.08	0.04	0.00	0.96	
				1:00	2:00	8.00	0.36	2.04	5.40	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.00	0.28	1.84	5.72	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	6.00	0.32	1.52	3.96	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	6.00	0.28	1.36	4.20	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	0.00	0.00	
				5:00	6:00	6.00	0.24	1.24	3.92	0.00	0.20	0.00	0.00	0.00	0.00	0.04	0.08	0.16	0.08	0.04	0.00	0.00	
				6:00	7:00	12.00	0.32	1.88	6.04	0.04	0.32	0.08	0.00	0.00	1.40	0.12	0.16	0.28	0.28	0.08	0.00	1.00	
				7:00	8:00	30.00	0.44	5.68	9.96	0.12	0.96	0.32	0.08	0.08	4.88	0.44	0.40	0.80	0.80	0.32	0.00	4.72	0.00
				8:00	9:00	38.00	1.16	11.32	10.32	0.24	2.24	1.32	0.40	0.36	3.68	0.44	0.44	0.84	0.84	0.36	0.00	4.04	0.00
				9:00	10:00	36.00	0.84	8.64	10.56	0.20	2.12	1.44	0.36	0.32	3.24	0.68	0.56	1.08	1.16	0.40	0.00	4.40	0.00
				10:00	11:00	34.00	1.20	8.08	10.52	0.40	3.52	1.84	0.44	0.40	2.68	0.24	0.28	0.56	0.56	0.16	0.00	3.12	0.00
				11:00	12:00	36.00	0.84	8.12	9.16	0.56	4.52	0.72	0.20	0.16	5.00	0.72	0.44	0.80	0.92	0.20	0.00	3.64	0.00
				12:00	13:00	34.00	1.00	10.08	10.84	0.36	2.84	0.60	0.16	0.12	3.52	0.80	0.16	0.36	0.36	0.08	0.00	2.72	0.00
				13:00	14:00	34.00	0.72	6.92	7.28	0.44	3.52	2.16	0.56	0.56	3.44	0.20	0.80	1.48	1.60	0.40	0.00	3.92	0.00
				14:00	15:00	34.00	0.60	6.48	10.16	0.40	3.36	1.72	0.40	0.36	3.76	0.36	0.52	1.00	1.04	0.32	0.00	3.52	0.00
				15:00	16:00	38.00	1.04	8.40	11.80	0.44	3.60	2.08	0.52	0.48	3.24	0.60	0.44	0.80	0.84	0.24	0.00	3.48	0.00
				16:00	17:00	38.00	1.52	11.36	10.48	0.40	3.24	0.88	0.16	0.16	3.64	0.08	0.56	1.00	1.16	0.16	0.00	3.20	0.00
				17:00	18:00	46.00	2.20	16.72	12.88	0.36	2.48	1.64	0.36	0.36	3.72	0.36	0.36	0.64	0.64	0.12	0.00	3.16	0.00
				18:00	19:00	40.00	2.00	15.84	12.48	0.20	1.80	0.96	0.20	0.24	1.52	0.28	0.44	0.76	0.84	0.12	0.00	2.32	0.00
				19:00	20:00	34.00	1.08	12.28	11.92	0.04	0.56	0.28	0.04	0.08	3.72	0.20	0.16	0.36	0.36	0.08	0.00	2.84	0.00
				20:00	21:00	30.00	0.96	8.32	12.04	0.04	0.36	0.08	0.04	0.04	4.96	0.08	0.08	0.12	0.08	0.04	0.00	2.76	0.00
				21:00	22:00	28.00	0.64	6.72	12.44	0.04	0.48	0.16	0.04	0.04	4.32	0.00	0.04	0.04	0.00	0.04	0.00	3.00	0.00
				22:00	23:00	28.00	0.88	5.16	12.48	0.04	0.24	0.00	0.00	0.00	4.52	0.16	0.08	0.16	0.08	0.08	0.00	4.12	0.00
				23:00	0:00	22.00	0.72	3.88	9.80	0.04	0.24	0.00	0.00	0.00	3.56	0.12	0.08	0.16	0.08	0.08	0.00	3.24	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L069	Hok Yeun Street (EB)	Ma Tau Wai Road	Sung Chi Street	0:00	1:00	11.60	0.46	3.42	6.90	0.00	0.23	0.00	0.00	0.00	0.12	0.06	0.06	0.23	0.06	0.06	0.00	0.00	
				1:00	2:00	5.80	0.23	1.74	3.65	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.80	0.17	1.57	3.94	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	5.80	0.29	1.80	3.54	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.80	0.23	1.62	3.83	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	5.80	0.17	1.39	3.60	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.06	0.06	0.17	0.06	0.06	0.00	0.00
				6:00	7:00	8.70	0.29	1.97	4.87	0.00	0.35	0.12	0.00	0.00	0.12	0.12	0.12	0.35	0.29	0.12	0.00	0.00	
				7:00	8:00	20.30	0.41	6.55	7.31	0.12	0.87	0.41	0.06	0.06	0.46	0.64	0.58	1.22	1.16	0.46	0.00	0.00	
				8:00	9:00	31.90	1.10	13.46	7.42	0.17	2.15	2.15	0.29	0.29	0.35	0.70	0.70	1.33	1.28	0.52	0.00	0.00	
				9:00	10:00	31.90	0.81	10.79	8.00	0.17	2.20	2.44	0.35	0.35	0.35	1.16	0.93	1.80	1.80	0.75	0.00	0.00	
				10:00	11:00	26.10	0.93	8.18	8.41	0.23	3.42	2.32	0.29	0.35	0.23	0.23	0.29	0.52	0.52	0.17	0.00	0.00	
				11:00	12:00	23.20	0.58	7.42	6.61	0.29	4.06	0.87	0.12	0.12	0.46	0.64	0.35	0.70	0.75	0.23	0.00	0.00	
				12:00	13:00	26.10	0.75	10.38	8.76	0.29	2.84	0.81	0.12	0.12	0.35	0.75	0.17	0.29	0.35	0.12	0.00	0.00	
				13:00	14:00	26.10	0.58	7.08	5.92	0.29	3.54	2.90	0.41	0.46	0.41	0.17	0.81	1.45	1.51	0.58	0.00	0.00	
				14:00	15:00	23.20	0.41	6.09	7.54	0.23	3.02	2.03	0.29	0.29	0.35	0.29	0.46	0.93	0.93	0.35	0.00	0.00	
				15:00	16:00	29.00	0.81	8.53	8.93	0.23	3.54	2.73	0.41	0.41	0.35	0.64	0.46	0.81	0.81	0.35	0.00	0.00	
				16:00	17:00	23.20	0.87	9.28	8.18	0.17	2.96	0.81	0.12	0.12	0.35	0.00	0.12	0.12	0.12	0.00	0.00	0.00	
				17:00	18:00	29.00	1.28	13.11	9.74	0.17	2.20	1.45	0.23	0.23	0.35	0.06	0.06	0.06	0.06	0.00	0.00	0.00	
				18:00	19:00	26.10	1.22	12.35	9.40	0.12	1.62	0.81	0.12	0.12	0.12	0.06	0.06	0.06	0.06	0.00	0.00	0.00	
				19:00	20:00	26.10	0.81	12.82	9.74	0.00	0.52	0.29	0.06	0.06	0.41	0.29	0.17	0.41	0.41	0.12	0.00	0.00	
				20:00	21:00	20.30	0.70	8.47	9.63	0.00	0.41	0.12	0.06	0.06	0.52	0.06	0.06	0.12	0.06	0.06	0.00	0.00	
				21:00	22:00	17.40	0.46	6.38	9.22	0.00	0.46	0.23	0.06	0.06	0.46	0.00	0.00	0.06	0.00	0.00	0.00	0.00	
				22:00	23:00	17.40	0.70	5.34	10.09	0.00	0.29	0.00	0.00	0.00	0.46	0.12	0.06	0.23	0.06	0.06	0.00	0.00	
L070	Hok Yeun Street (EB)	Sung Chi Street	Sung On Street	23:00	0:00	14.50	0.58	4.29	8.35	0.00	0.29	0.00	0.00	0.00	0.41	0.17	0.06	0.23	0.06	0.06	0.00	0.00	
				0:00	1:00	12.75	0.43	3.57	7.74	0.00	0.26	0.00	0.00	0.00	0.17	0.09	0.09	0.26	0.09	0.09	0.00	0.00	
				1:00	2:00	8.50	0.34	2.47	5.53	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.50	0.26	2.21	5.78	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	8.50	0.26	2.47	5.53	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.50	0.34	2.30	5.70	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	8.50	0.26	1.96	4.93	0.00	0.43	0.00	0.00	0.00	0.00	0.17	0.17	0.26	0.17	0.17	0.00	0.00	
				6:00	7:00	12.75	0.34	2.72	7.40	0.00	0.51	0.17	0.00	0.00	0.17	0.17	0.17	0.51	0.43	0.17	0.00	0.00	
				7:00	8:00	29.75	0.51	9.27	10.63	0.09	1.45	0.68	0.09	0.09	0.77	0.94	0.85	1.96	1.79	0.68	0.00	0.00	
				8:00	9:00	42.50	1.36	17.17	10.20	0.17	3.06	2.89	0.43	0.34	0.51	0.94	0.94	1.96	1.87	0.68	0.00	0.00	
				9:00	10:00	42.50	1.02	13.60	10.97	0.17	3.06	3.23	0.51	0.43	0.51	1.62	1.19	2.64	2.64	0.94	0.00	0.00	
				10:00	11:00	34.00	1.02	10.29	11.31	0.26	4.68	3.06	0.43	0.34	0.34	0.34	0.34	0.68	0.68	0.26	0.00	0.00	
				11:00	12:00	34.00	0.77	10.54	9.95	0.26	6.21	1.19	0.17	0.09	0.77	0.94	0.60	1.11	1.11	0.34	0.00	0.00	
				12:00	13:00	34.00	0.85	13.18	11.73	0.17	3.91	1.02	0.17	0.09	0.51	1.02	0.26	0.43	0.51	0.17	0.00	0.00	
				13:00	14:00	34.00	0.68	8.84	7.99	0.26	4.85	3.66	0.51	0.43	0.51	0.26	1.11	2.04	2.13	0.77	0.00	0.00	
				14:00	15:00	34.00	0.60	8.50	11.31	0.26	4.59	2.89	0.34	0.34	0.60	0.51	0.68	1.45	1.45	0.51	0.00	0.00	
				15:00	16:00	38.25	0.85	10.80	12.67	0.26	4.93	3.49	0.51	0.43	0.43	0.77	0.51	1.19	1.11	0.34	0.00	0.00	
				16:00	17:00	29.75	1.02	11.39	10.97	0.17	4.00	1.02	0.09	0.09	0.51	0.00	0.17	0.17	0.17	0.00	0.00	0.00	
				17:00	18:00	38.25	1.45	16.92	13.43	0.17	3.06	1.87	0.26	0.26	0.51	0.09	0.09	0.09	0.09	0.00	0.00	0.00	
				18:00	19:00	34.00	1.28	15.81	12.75	0.09	2.13	1.11	0.17	0.17	0.17	0.09	0.09	0.09	0.09	0.00	0.00	0.00	
				19:00	20:00	34.00	0.94	16.15	13.18	0.00	0.77	0.51	0.00	0.09	0.60	0.26	0.26	0.51	0.60	0.17	0.00	0.00	
				20:00	21:00	25.50	0.77	10.37	12.58	0.00	0.43	0.17	0.00	0.00	0.68	0.09	0.09	0.17	0.09	0.09	0.00	0.00	
				21:00	22:00	25.50	0.60	9.10	14.03	0.00	0.68	0.34	0.00	0.00	0.68	0.00	0.00	0.09	0.00	0.00	0.00	0.00	
22:00	23:00	21.25	0.77	6.29	12.50	0.00	0.34	0.00	0.00	0.00	0.68	0.17	0.09	0.26	0.09	0.09	0.00	0.00					
23:00	0:00	17.00	0.60	4.76	10.12	0.00	0.34	0.00	0.00	0.00	0.51	0.17	0.09	0.26	0.09	0.09	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L071	Hok Yeun Street East (EB)	Sung On Street	Hung Hom Road	0:00	1:00	7.40	0.15	1.78	5.18	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	3.70	0.07	0.89	2.66	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.70	0.07	0.81	2.74	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.70	0.07	0.89	2.66	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.70	0.15	0.81	2.59	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.70	0.07	0.74	2.66	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.07	0.00	0.00
				6:00	7:00	7.40	0.15	1.41	4.74	0.00	0.22	0.15	0.00	0.00	0.00	0.00	0.15	0.15	0.15	0.15	0.15	0.00	0.00	
				7:00	8:00	11.10	0.15	3.63	5.03	0.07	0.59	0.15	0.07	0.00	0.00	0.22	0.22	0.37	0.37	0.22	0.00	0.00	0.00	
				8:00	9:00	18.50	0.44	7.62	5.48	0.07	1.55	1.04	0.30	0.15	0.00	0.30	0.30	0.52	0.44	0.30	0.00	0.00	0.00	
				9:00	10:00	18.50	0.37	6.29	5.92	0.07	1.55	1.18	0.30	0.22	0.00	0.44	0.37	0.74	0.67	0.37	0.00	0.00	0.00	
				10:00	11:00	14.80	0.37	4.22	6.07	0.07	2.00	0.89	0.30	0.22	0.00	0.07	0.07	0.22	0.15	0.15	0.00	0.00	0.00	
				11:00	12:00	14.80	0.22	4.51	5.70	0.15	2.66	0.44	0.07	0.07	0.00	0.22	0.07	0.30	0.22	0.15	0.00	0.00	0.00	
				12:00	13:00	14.80	0.30	5.33	6.44	0.07	1.63	0.30	0.07	0.07	0.00	0.22	0.07	0.15	0.07	0.07	0.00	0.00	0.00	
				13:00	14:00	14.80	0.22	3.92	4.66	0.07	2.15	1.26	0.37	0.37	0.00	0.07	0.22	0.59	0.44	0.44	0.00	0.00	0.00	
				14:00	15:00	14.80	0.22	3.63	6.44	0.07	2.00	0.89	0.22	0.22	0.00	0.07	0.15	0.44	0.22	0.22	0.00	0.00	0.00	
				15:00	16:00	18.50	0.37	5.03	7.55	0.07	2.29	1.18	0.37	0.37	0.00	0.15	0.15	0.37	0.30	0.30	0.00	0.00	0.00	
				16:00	17:00	14.80	0.44	5.03	6.81	0.07	1.63	0.30	0.07	0.07	0.00	0.00	0.00	0.15	0.00	0.22	0.00	0.00	0.00	
				17:00	18:00	22.20	0.81	8.66	9.62	0.07	1.48	0.74	0.30	0.30	0.00	0.00	0.00	0.07	0.00	0.15	0.00	0.00	0.00	
				18:00	19:00	18.50	0.67	7.40	8.44	0.07	0.96	0.44	0.15	0.15	0.00	0.00	0.00	0.07	0.00	0.15	0.00	0.00	0.00	
				19:00	20:00	14.80	0.37	6.51	7.03	0.00	0.30	0.15	0.00	0.00	0.00	0.07	0.07	0.15	0.07	0.07	0.00	0.00	0.00	
				20:00	21:00	14.80	0.37	5.33	8.58	0.00	0.30	0.07	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.07	0.00	0.00	0.00	
				21:00	22:00	11.10	0.22	3.48	7.10	0.00	0.22	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	11.10	0.22	2.81	7.77	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.07	0.00	0.00	0.00	
				23:00	0:00	11.10	0.30	2.74	7.55	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.15	0.00	0.00	0.00	
L072	Hok Yeun Street East (WB)	Hung Hom Road	Sung On Street	0:00	1:00	7.40	0.22	2.15	4.66	0.00	0.07	0.00	0.00	0.00	0.00	0.07	0.00	0.07	0.07	0.07	0.00	0.00		
				1:00	2:00	3.70	0.07	1.11	2.44	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				2:00	3:00	3.70	0.07	1.04	2.52	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				3:00	4:00	3.70	0.15	1.11	2.29	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				4:00	5:00	3.70	0.15	0.96	2.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				5:00	6:00	3.70	0.15	0.89	2.07	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.15	0.15	0.00	0.00		
				6:00	7:00	7.40	0.15	1.63	4.14	0.00	0.30	0.15	0.00	0.00	0.00	0.15	0.15	0.30	0.30	0.15	0.00	0.00		
				7:00	8:00	11.10	0.07	2.52	5.33	0.00	0.22	0.15	0.07	0.07	0.00	0.37	0.37	0.81	0.81	0.30	0.00	0.00		
				8:00	9:00	14.80	0.22	4.74	5.03	0.00	0.59	0.67	0.37	0.37	0.00	0.44	0.44	0.81	0.81	0.30	0.00	0.00		
				9:00	10:00	14.80	0.15	3.63	5.18	0.00	0.59	0.67	0.37	0.37	0.00	0.67	0.52	1.11	1.18	0.37	0.00	0.00		
				10:00	11:00	14.80	0.37	4.74	4.88	0.07	1.63	0.96	0.44	0.37	0.00	0.15	0.15	0.37	0.37	0.30	0.00	0.00		
				11:00	12:00	14.80	0.30	4.81	4.59	0.07	2.15	0.37	0.22	0.07	0.00	0.44	0.30	0.59	0.59	0.30	0.00	0.00		
				12:00	13:00	14.80	0.30	5.92	5.33	0.07	1.33	0.37	0.15	0.07	0.00	0.52	0.07	0.30	0.30	0.07	0.00	0.00		
				13:00	14:00	14.80	0.15	4.07	3.48	0.07	1.63	1.11	0.52	0.44	0.00	0.15	0.52	1.04	1.04	0.59	0.00	0.00		
				14:00	15:00	14.80	0.15	3.85	5.03	0.07	1.55	0.89	0.37	0.37	0.00	0.30	0.37	0.74	0.74	0.37	0.00	0.00		
				15:00	16:00	14.80	0.22	4.29	5.11	0.07	1.48	0.96	0.44	0.37	0.00	0.37	0.22	0.52	0.44	0.30	0.00	0.00		
				16:00	17:00	14.80	0.44	7.18	4.14	0.07	1.85	0.44	0.07	0.07	0.00	0.00	0.00	0.15	0.00	0.37	0.00	0.00		
				17:00	18:00	18.50	0.67	10.06	4.88	0.07	1.41	0.74	0.22	0.15	0.00	0.00	0.00	0.07	0.00	0.22	0.00	0.00		
				18:00	19:00	14.80	0.52	8.29	4.14	0.07	0.89	0.37	0.15	0.07	0.00	0.00	0.00	0.07	0.00	0.22	0.00	0.00		
				19:00	20:00	14.80	0.37	7.18	5.85	0.00	0.30	0.15	0.00	0.07	0.00	0.15	0.07	0.30	0.30	0.07	0.00	0.00		
				20:00	21:00	11.10	0.22	4.66	5.70	0.00	0.15	0.07	0.00	0.00	0.00	0.07	0.00	0.07	0.07	0.07	0.00	0.00		
				21:00	22:00	11.10	0.22	4.14	6.44	0.00	0.22	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				22:00	23:00	11.10	0.30	3.40	6.51	0.00	0.15	0.00	0.00	0.00	0.00	0.15	0.15	0.15	0.15	0.15	0.00	0.00		
				23:00	0:00	7.40	0.15	2.15	4.66	0.00	0.07	0.00	0.00	0.00	0.00	0.07	0.07	0.07	0.07	0.07	0.00	0.00		

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L073	Hok Yeun Street East (WB)	Sung Ping Street	Hung Hom Road	0:00	1:00	5.10	0.15	1.07	3.83	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	2.55	0.05	0.51	1.94	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.55	0.05	0.41	2.04	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.55	0.10	0.51	1.84	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.55	0.10	0.46	1.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.55	0.10	0.41	1.94	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	5.10	0.10	0.77	3.57	0.00	0.15	0.10	0.00	0.00	0.00	0.10	0.10	0.10	0.10	0.00	0.00	0.00
				7:00	8:00	7.65	0.00	0.61	5.81	0.00	0.31	0.10	0.05	0.05	0.00	0.15	0.15	0.20	0.20	0.00	0.00	0.00
				8:00	9:00	10.20	0.00	1.28	6.53	0.00	0.77	0.61	0.15	0.15	0.00	0.15	0.15	0.20	0.20	0.00	0.00	0.00
				9:00	10:00	10.20	0.00	0.97	6.58	0.00	0.71	0.66	0.15	0.15	0.00	0.26	0.20	0.26	0.26	0.00	0.00	0.00
				10:00	11:00	10.20	0.26	2.55	4.74	0.05	1.17	0.66	0.26	0.26	0.00	0.05	0.05	0.05	0.10	0.00	0.00	0.00
				11:00	12:00	7.65	0.15	2.09	3.42	0.05	1.22	0.20	0.05	0.05	0.00	0.15	0.05	0.10	0.10	0.00	0.00	0.00
				12:00	13:00	10.20	0.20	3.21	5.05	0.05	0.97	0.26	0.05	0.05	0.00	0.20	0.05	0.05	0.05	0.00	0.00	0.00
				13:00	14:00	7.65	0.10	1.84	2.86	0.05	1.02	0.71	0.20	0.26	0.00	0.05	0.15	0.20	0.20	0.00	0.00	0.00
				14:00	15:00	7.65	0.10	1.63	3.72	0.05	0.87	0.51	0.15	0.15	0.00	0.05	0.10	0.15	0.15	0.00	0.00	0.00
				15:00	16:00	10.20	0.15	2.40	4.95	0.05	1.07	0.77	0.20	0.20	0.00	0.10	0.10	0.10	0.10	0.00	0.00	0.00
				16:00	17:00	7.65	0.41	4.23	1.89	0.05	0.77	0.20	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	10.20	0.56	6.07	2.30	0.05	0.56	0.36	0.15	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	10.20	0.56	6.27	2.40	0.05	0.46	0.26	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	10.20	0.26	3.88	5.46	0.00	0.20	0.15	0.00	0.05	0.00	0.05	0.05	0.05	0.05	0.00	0.00	0.00
				20:00	21:00	7.65	0.15	2.40	4.95	0.00	0.10	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	7.65	0.15	2.04	5.25	0.00	0.15	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	7.65	0.20	1.63	5.76	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	5.10	0.10	1.07	3.88	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L074	Hok Cheung Street (NB)	Hok Yeun Street East	Man Yue Street	0:00	1:00	9.80	0.29	2.94	6.37	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	4.90	0.10	1.37	3.33	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.90	0.10	1.27	3.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.90	0.20	1.47	3.04	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.90	0.20	1.27	3.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	4.90	0.20	1.27	3.23	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	4.90	0.10	1.27	3.43	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	14.70	0.10	3.72	9.11	0.00	0.49	0.10	0.00	0.00	0.00	0.29	0.29	0.29	0.29	0.00	0.00	0.00
				8:00	9:00	19.60	0.20	7.06	9.41	0.00	1.18	0.69	0.20	0.10	0.00	0.20	0.20	0.20	0.20	0.00	0.00	0.00
				9:00	10:00	19.60	0.10	5.78	9.90	0.00	1.18	0.78	0.29	0.10	0.00	0.39	0.29	0.39	0.39	0.00	0.00	0.00
				10:00	11:00	14.70	0.39	5.49	6.08	0.00	1.37	0.59	0.20	0.20	0.00	0.10	0.10	0.10	0.10	0.00	0.00	0.00
				11:00	12:00	14.70	0.39	5.98	5.59	0.00	1.86	0.20	0.10	0.10	0.00	0.20	0.10	0.10	0.10	0.00	0.00	0.00
				12:00	13:00	14.70	0.29	6.66	5.98	0.00	1.08	0.20	0.10	0.10	0.00	0.20	0.00	0.00	0.10	0.00	0.00	0.00
				13:00	14:00	14.70	0.39	5.19	4.90	0.00	1.67	0.88	0.39	0.29	0.00	0.10	0.29	0.29	0.29	0.00	0.00	0.00
				14:00	15:00	14.70	0.29	4.90	6.27	0.00	1.47	0.78	0.29	0.29	0.00	0.10	0.10	0.10	0.10	0.00	0.00	0.00
				15:00	16:00	19.60	0.49	7.06	8.23	0.00	1.67	0.88	0.39	0.29	0.00	0.29	0.10	0.10	0.10	0.00	0.00	0.00
				16:00	17:00	14.70	0.78	8.62	3.92	0.00	0.98	0.20	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	19.60	1.08	12.25	4.70	0.00	0.78	0.39	0.20	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	19.60	1.08	12.64	4.90	0.00	0.59	0.20	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	14.70	0.39	7.64	6.17	0.00	0.20	0.10	0.00	0.00	0.00	0.10	0.00	0.00	0.10	0.00	0.00	0.00
				20:00	21:00	14.70	0.39	6.47	7.74	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	14.70	0.29	5.49	8.53	0.00	0.29	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.80	0.29	3.14	6.27	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	9.80	0.39	3.04	6.27	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L075	Hung Hom Road (SB)	Hok Yeun Street East	Man Yue Street	0:00	1:00	38.15	0.98	9.81	24.09	0.00	0.87	0.00	0.00	0.00	1.53	0.11	0.11	0.33	0.22	0.11	0.00	0.00	
				1:00	2:00	21.80	0.55	5.89	14.72	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	21.80	0.44	5.23	15.70	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	16.35	0.55	4.47	10.68	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	16.35	0.44	4.03	11.45	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	16.35	0.33	3.60	11.01	0.00	0.65	0.00	0.00	0.00	0.00	0.00	0.11	0.11	0.22	0.22	0.11	0.00	0.00
				6:00	7:00	32.70	0.55	6.76	20.27	0.11	1.42	0.22	0.00	0.00	1.53	0.22	0.33	0.55	0.55	0.22	0.00	0.00	
				7:00	8:00	76.30	0.65	19.73	40.22	0.33	3.92	0.98	0.22	0.22	4.25	0.87	0.87	1.64	1.64	0.55	0.00	0.22	
				8:00	9:00	114.45	2.07	41.42	43.49	0.55	9.59	4.91	1.42	1.31	3.27	0.98	0.98	1.85	1.85	0.55	0.00	0.22	
				9:00	10:00	103.55	1.42	30.74	43.16	0.44	9.05	5.12	1.31	1.20	2.94	1.53	1.20	2.18	2.29	0.76	0.00	0.22	
				10:00	11:00	92.65	2.18	27.47	33.79	0.65	14.28	5.12	1.42	1.42	2.83	0.44	0.55	0.98	0.98	0.44	0.00	0.11	
				11:00	12:00	87.20	1.53	25.94	27.80	0.87	17.44	1.96	0.55	0.44	4.91	1.31	0.76	1.53	1.53	0.55	0.00	0.11	
				12:00	13:00	92.65	1.64	34.01	35.32	0.55	11.55	1.64	0.55	0.44	3.60	1.53	0.33	0.55	0.65	0.22	0.00	0.11	
				13:00	14:00	87.20	1.42	23.76	23.87	0.76	14.72	6.32	1.74	1.64	3.71	0.44	1.64	2.83	3.16	1.09	0.00	0.11	
				14:00	15:00	87.20	1.09	21.91	32.70	0.65	13.63	4.69	1.31	1.09	3.82	0.65	1.09	1.85	1.96	0.65	0.00	0.11	
				15:00	16:00	103.55	1.85	28.99	38.15	0.87	14.93	6.00	1.74	1.42	3.38	1.20	0.98	1.64	1.64	0.65	0.00	0.11	
				16:00	17:00	81.75	2.51	33.14	22.67	0.55	11.45	1.64	0.44	0.44	3.92	0.11	0.87	1.53	1.74	0.76	0.00	0.00	
				17:00	18:00	109.00	3.92	52.32	29.87	0.44	9.37	3.27	0.98	0.87	4.36	0.55	0.55	0.98	0.98	0.55	0.00	0.00	
				18:00	19:00	92.65	3.38	46.76	27.36	0.22	6.43	1.85	0.55	0.55	1.64	0.44	0.65	1.09	1.20	0.55	0.00	0.00	
				19:00	20:00	87.20	1.85	39.57	36.52	0.11	2.07	0.65	0.11	0.22	3.71	0.44	0.33	0.65	0.65	0.22	0.00	0.11	
				20:00	21:00	76.30	1.64	28.01	38.48	0.11	1.53	0.22	0.11	0.11	5.23	0.11	0.11	0.22	0.22	0.11	0.00	0.11	
				21:00	22:00	65.40	1.09	20.82	36.52	0.11	1.74	0.44	0.11	0.11	4.14	0.00	0.00	0.11	0.00	0.11	0.00	0.11	
				22:00	23:00	59.95	1.42	15.81	36.08	0.11	0.98	0.00	0.00	0.00	4.25	0.22	0.22	0.33	0.22	0.22	0.00	0.11	
23:00	0:00	49.05	1.20	12.54	29.43	0.11	0.98	0.00	0.00	0.00	3.49	0.22	0.22	0.33	0.22	0.22	0.00	0.11					
L076	Hung Hom Road (NB)	Man Yue Street	Hok Yeun Street East	0:00	1:00	16.35	0.22	4.47	8.28	0.00	0.33	0.00	0.00	0.00	1.53	0.11	0.11	0.33	0.11	0.11	0.00	0.76	
				1:00	2:00	10.90	0.22	3.60	6.76	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.90	0.22	3.38	7.09	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	5.45	0.11	1.85	3.27	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.45	0.11	1.64	3.60	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	5.45	0.11	1.42	3.05	0.00	0.22	0.00	0.00	0.00	0.00	0.11	0.11	0.22	0.11	0.11	0.00	0.00	
				6:00	7:00	16.35	0.11	3.38	7.41	0.00	0.55	0.11	0.00	0.00	1.64	0.33	0.33	0.76	0.55	0.33	0.00	0.87	
				7:00	8:00	38.15	0.22	9.70	13.08	0.11	1.09	0.33	0.11	0.11	4.80	0.76	0.76	1.64	1.53	0.76	0.00	3.16	
				8:00	9:00	49.05	0.55	18.97	12.86	0.11	2.51	1.53	0.55	0.55	3.49	0.76	0.76	1.64	1.53	0.65	0.00	2.62	
				9:00	10:00	49.05	0.55	15.04	13.84	0.11	2.51	1.74	0.55	0.55	3.27	1.31	1.09	2.29	2.29	0.98	0.00	2.94	
				10:00	11:00	38.15	0.65	11.66	10.36	0.11	5.23	1.64	0.44	0.44	2.51	0.44	0.44	0.87	0.87	0.33	0.00	2.18	
				11:00	12:00	38.15	0.44	10.68	8.50	0.22	6.10	0.55	0.22	0.11	4.25	1.09	0.65	1.31	1.31	0.44	0.00	2.29	
				12:00	13:00	38.15	0.55	14.06	10.57	0.11	4.14	0.55	0.22	0.11	3.16	1.20	0.33	0.55	0.65	0.22	0.00	1.74	
				13:00	14:00	38.15	0.33	9.59	6.98	0.11	5.12	1.96	0.55	0.55	3.05	0.33	1.31	2.40	2.51	0.87	0.00	2.51	
				14:00	15:00	38.15	0.33	9.16	10.03	0.11	5.01	1.53	0.44	0.44	3.38	0.55	0.76	1.74	1.74	0.55	0.00	2.40	
				15:00	16:00	43.60	0.55	12.21	11.77	0.11	5.45	1.85	0.55	0.55	3.05	1.09	0.65	1.53	1.42	0.44	0.00	2.40	
				16:00	17:00	38.15	0.76	13.73	7.52	0.11	5.56	0.55	0.11	0.11	3.05	0.11	0.87	1.42	1.64	0.33	0.00	2.29	
				17:00	18:00	49.05	1.31	21.91	10.25	0.11	4.80	1.09	0.22	0.22	3.38	0.55	0.55	0.98	0.98	0.22	0.00	2.51	
				18:00	19:00	38.15	1.09	18.42	8.72	0.11	2.94	0.55	0.11	0.11	1.20	0.44	0.65	0.98	1.09	0.22	0.00	1.53	
				19:00	20:00	38.15	0.55	17.55	11.66	0.00	0.76	0.22	0.00	0.11	3.27	0.33	0.33	0.55	0.65	0.22	0.00	1.96	
				20:00	21:00	32.70	0.55	12.10	12.21	0.00	0.55	0.11	0.00	0.00	4.69	0.11	0.11	0.22	0.11	0.11	0.00	1.85	
				21:00	22:00	27.25	0.33	8.94	11.45	0.00	0.65	0.11	0.00	0.00	3.71	0.00	0.00	0.11	0.00	0.00	0.00	1.96	
				22:00	23:00	27.25	0.44	7.09	11.77	0.00	0.33	0.00	0.00	0.00	4.03	0.22	0.11	0.33	0.11	0.11	0.00	2.73	
23:00	0:00	21.80	0.33	5.45	9.37	0.00	0.33	0.00	0.00	0.00	3.27	0.22	0.11	0.33	0.11	0.11	0.00	2.18					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L077	Man Lok Street (SB)	Hok Yeun Street East	Man Yue Street	0:00	1:00	18.60	0.74	5.83	10.04	0.00	0.37	0.00	0.00	0.00	0.37	0.12	0.12	0.50	0.37	0.12	0.00	0.00		
				1:00	2:00	12.40	0.50	3.97	7.56	0.00	0.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	12.40	0.37	3.84	7.94	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	6.20	0.25	1.98	3.72	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	6.20	0.25	1.86	3.97	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	12.40	0.50	3.10	6.57	0.00	0.50	0.00	0.00	0.00	0.00	0.25	0.25	0.50	0.50	0.25	0.00	0.00	0.00	0.00
				6:00	7:00	18.60	0.50	4.34	9.05	0.00	0.74	0.25	0.00	0.00	0.37	0.50	0.50	1.12	0.87	0.37	0.00	0.00	0.00	0.00
				7:00	8:00	37.20	0.62	7.69	14.14	0.12	1.36	0.62	0.12	0.12	1.12	1.74	1.61	3.47	3.35	1.12	0.00	0.00	0.00	0.00
				8:00	9:00	49.60	1.49	14.51	13.76	0.12	2.73	2.73	0.99	0.99	0.74	1.74	1.74	3.60	3.22	1.24	0.00	0.00	0.00	0.00
				9:00	10:00	49.60	0.99	11.16	13.89	0.12	2.60	2.98	0.99	0.99	0.62	2.73	2.11	4.46	4.34	1.61	0.00	0.00	0.00	0.00
				10:00	11:00	43.40	1.49	13.76	12.15	0.12	5.46	3.10	1.12	0.99	0.62	0.62	0.74	1.49	1.36	0.37	0.00	0.00	0.00	0.00
				11:00	12:00	43.40	0.99	13.76	10.66	0.25	6.94	1.24	0.50	0.25	1.12	1.86	0.99	2.11	2.11	0.62	0.00	0.00	0.00	0.00
				12:00	13:00	43.40	1.24	17.24	12.90	0.12	4.59	1.12	0.50	0.25	0.87	1.98	0.50	0.87	0.99	0.25	0.00	0.00	0.00	0.00
				13:00	14:00	43.40	0.87	11.53	8.18	0.12	5.33	3.47	1.12	1.12	0.74	0.50	1.98	3.60	3.72	1.12	0.00	0.00	0.00	0.00
				14:00	15:00	43.40	0.87	11.16	11.90	0.12	5.33	2.98	0.99	0.99	0.87	0.87	1.24	2.60	2.60	0.87	0.00	0.00	0.00	0.00
				15:00	16:00	49.60	1.36	14.26	13.89	0.12	5.70	3.47	1.24	1.12	0.74	1.61	1.12	2.23	1.98	0.74	0.00	0.00	0.00	0.00
				16:00	17:00	43.40	1.74	21.70	10.04	0.12	5.70	1.12	0.37	0.25	0.87	0.12	0.37	0.37	0.62	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	55.80	2.60	31.25	12.28	0.12	4.34	2.11	0.74	0.62	0.74	0.25	0.25	0.25	0.25	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	49.60	2.36	29.26	11.66	0.12	2.98	1.24	0.37	0.37	0.25	0.12	0.25	0.25	0.37	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	43.40	1.49	21.58	14.51	0.00	0.87	0.50	0.12	0.12	0.87	0.62	0.50	0.99	0.99	0.25	0.00	0.00	0.00	0.00
				20:00	21:00	31.00	1.12	13.52	13.39	0.00	0.50	0.12	0.12	0.12	1.12	0.25	0.12	0.25	0.25	0.12	0.00	0.00	0.00	0.00
				21:00	22:00	31.00	0.87	12.15	15.38	0.00	0.74	0.37	0.12	0.12	1.12	0.00	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	24.80	0.87	8.06	13.39	0.00	0.37	0.00	0.00	0.00	0.87	0.25	0.25	0.37	0.25	0.12	0.00	0.00	0.00	0.00
				23:00	0:00	24.80	1.12	7.69	12.77	0.00	0.50	0.00	0.00	0.00	0.99	0.37	0.37	0.50	0.37	0.12	0.00	0.00	0.00	0.00
L078	Ma Tau Wai Road (SB)	Hok Yeun Street	Tsing Chau Street	0:00	1:00	11.25	0.23	2.18	5.40	0.00	0.15	0.00	0.00	0.00	0.98	0.00	0.00	0.08	0.00	0.00	0.00	2.25		
				1:00	2:00	7.50	0.23	1.95	5.10	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	7.50	0.23	1.73	5.40	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.75	0.15	0.98	2.48	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.75	0.08	0.90	2.70	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.75	0.08	0.83	2.63	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	11.25	0.15	1.58	4.88	0.00	0.23	0.08	0.00	0.00	1.05	0.08	0.08	0.08	0.08	0.00	0.00	0.00	3.00	
				7:00	8:00	33.75	0.15	5.78	9.00	0.15	0.98	0.30	0.08	0.08	4.35	0.15	0.15	0.30	0.30	0.08	0.00	0.00	11.93	
				8:00	9:00	41.25	0.53	12.00	9.38	0.30	2.18	1.43	0.30	0.30	3.38	0.15	0.15	0.30	0.30	0.08	0.00	0.00	10.50	
				9:00	10:00	37.50	0.38	8.63	9.15	0.30	2.03	1.43	0.30	0.30	2.85	0.23	0.15	0.38	0.38	0.08	0.00	0.00	10.95	
				10:00	11:00	30.00	0.60	6.00	7.50	0.38	2.70	1.43	0.30	0.23	1.80	0.08	0.08	0.23	0.15	0.00	0.00	0.00	8.55	
				11:00	12:00	30.00	0.38	5.70	6.23	0.45	3.30	0.53	0.08	0.08	3.30	0.23	0.08	0.30	0.23	0.00	0.00	0.00	9.15	
				12:00	13:00	30.00	0.45	7.73	8.10	0.30	2.33	0.45	0.08	0.08	2.48	0.23	0.08	0.15	0.08	0.00	0.00	0.00	7.50	
				13:00	14:00	30.00	0.38	5.03	5.18	0.38	2.78	1.65	0.30	0.30	2.40	0.08	0.23	0.53	0.38	0.08	0.00	0.00	10.35	
				14:00	15:00	30.00	0.30	4.80	7.28	0.38	2.55	1.35	0.23	0.23	2.55	0.08	0.15	0.38	0.23	0.08	0.00	0.00	9.45	
				15:00	16:00	33.75	0.53	6.38	8.55	0.38	2.85	1.65	0.30	0.30	2.25	0.15	0.15	0.30	0.23	0.08	0.00	0.00	9.68	
				16:00	17:00	26.25	0.75	5.93	5.70	0.23	1.95	0.45	0.08	0.00	1.73	0.00	0.15	0.38	0.15	0.00	0.00	0.00	8.78	
				17:00	18:00	33.75	1.28	9.68	7.80	0.23	1.65	0.98	0.15	0.08	1.88	0.08	0.08	0.23	0.08	0.00	0.00	0.00	9.60	
				18:00	19:00	26.25	1.05	8.63	6.98	0.15	1.13	0.53	0.08	0.08	0.68	0.08	0.08	0.23	0.08	0.00	0.00	0.00	6.53	
				19:00	20:00	30.00	0.53	9.38	8.70	0.00	0.38	0.23	0.00	0.00	2.63	0.08	0.08	0.15	0.08	0.00	0.00	0.00	7.80	
				20:00	21:00	26.25	0.45	6.08	8.55	0.00	0.30	0.08	0.00	0.00	3.38	0.00	0.00	0.08	0.00	0.00	0.00	0.00	7.35	
				21:00	22:00	26.25	0.38	5.10	8.85	0.00	0.45	0.15	0.00	0.00	3.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	8.25	
				22:00	23:00	26.25	0.38	3.68	8.48	0.00	0.15	0.00	0.00	0.00	3.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	10.50	
				23:00	0:00	22.50	0.38	3.00	7.28	0.00	0.23	0.00	0.00	0.00	2.55	0.00	0.00	0.08	0.00	0.00	0.00	0.00	9.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L079	Ma Tau Wai Road (NB)	Tsing Chau Street	Hok Yeun Street	0:00	1:00	26.25	0.98	5.40	14.25	0.00	0.38	0.00	0.00	0.00	2.93	0.08	0.08	0.30	0.15	0.08	0.00	1.65
				1:00	2:00	15.00	0.68	3.75	10.20	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	15.00	0.60	3.38	10.73	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	11.25	0.60	2.85	7.50	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.50	0.30	1.65	5.33	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				5:00	6:00	11.25	0.38	2.33	7.43	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.30	0.23	0.08	0.00
				6:00	7:00	22.50	0.60	3.53	11.10	0.08	0.68	0.15	0.00	0.00	2.78	0.23	0.23	0.53	0.53	0.15	0.00	1.95
				7:00	8:00	52.50	0.83	9.53	17.55	0.23	1.65	0.45	0.15	0.15	8.93	0.68	0.68	1.35	1.35	0.38	0.00	8.63
				8:00	9:00	67.50	2.10	19.50	18.53	0.45	3.90	2.33	0.60	0.60	6.98	0.75	0.75	1.50	1.50	0.38	0.00	7.65
				9:00	10:00	63.75	1.50	14.93	18.45	0.38	3.68	2.48	0.68	0.68	6.15	1.20	0.98	1.88	1.95	0.53	0.00	8.33
				10:00	11:00	60.00	2.18	14.18	18.60	0.68	6.08	3.15	0.75	0.75	5.03	0.38	0.45	0.83	0.90	0.23	0.00	5.85
				11:00	12:00	63.75	1.50	14.18	16.35	0.98	7.88	1.28	0.30	0.23	9.45	1.20	0.68	1.28	1.35	0.30	0.00	6.83
				12:00	13:00	60.00	1.73	17.48	19.35	0.60	4.88	1.05	0.30	0.23	6.60	1.20	0.30	0.53	0.60	0.15	0.00	5.03
				13:00	14:00	60.00	1.35	12.15	13.20	0.75	6.15	3.75	0.90	0.90	6.45	0.30	1.35	2.25	2.55	0.60	0.00	7.35
				14:00	15:00	60.00	1.05	11.33	18.08	0.75	5.85	2.85	0.75	0.75	7.05	0.53	0.83	1.58	1.65	0.38	0.00	6.60
				15:00	16:00	67.50	1.80	14.85	21.00	0.75	6.30	3.60	0.98	0.83	6.15	1.05	0.68	1.28	1.28	0.38	0.00	6.60
				16:00	17:00	63.75	2.55	19.05	17.70	0.68	5.33	1.43	0.30	0.30	6.53	0.15	0.83	1.35	1.58	0.30	0.00	5.70
				17:00	18:00	82.50	4.05	29.55	23.25	0.68	4.35	2.93	0.68	0.68	7.13	0.60	0.60	0.90	0.90	0.23	0.00	6.00
				18:00	19:00	67.50	3.53	26.78	21.30	0.38	2.93	1.58	0.38	0.38	2.78	0.38	0.60	0.98	1.13	0.23	0.00	4.20
				19:00	20:00	60.00	1.95	21.30	21.08	0.08	0.90	0.45	0.08	0.08	6.90	0.38	0.30	0.53	0.60	0.15	0.00	5.25
				20:00	21:00	52.50	1.65	14.10	21.00	0.08	0.60	0.15	0.08	0.08	9.08	0.15	0.08	0.23	0.15	0.08	0.00	5.03
				21:00	22:00	48.75	1.05	11.40	21.60	0.08	0.75	0.30	0.08	0.08	7.88	0.00	0.00	0.08	0.00	0.00	0.00	5.48
				22:00	23:00	48.75	1.50	8.70	21.45	0.08	0.53	0.00	0.00	0.00	8.18	0.15	0.15	0.30	0.15	0.08	0.00	7.50
L080	Ma Tau Wai Road (SB)	Tsing Chau Street	Man Yue Street	23:00	0:00	41.25	1.35	7.13	18.08	0.08	0.53	0.00	0.00	0.00	6.90	0.15	0.15	0.30	0.15	0.08	0.00	6.38
				0:00	1:00	3.30	0.07	0.62	1.54	0.00	0.04	0.00	0.00	0.00	0.31	0.00	0.00	0.02	0.00	0.00	0.00	0.70
				1:00	2:00	2.20	0.07	0.57	1.52	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.20	0.04	0.53	1.58	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	1.10	0.02	0.29	0.77	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	1.10	0.02	0.24	0.81	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	1.10	0.02	0.24	0.75	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.02	0.00	0.00	0.00	0.00
				6:00	7:00	3.30	0.04	0.44	1.36	0.00	0.09	0.02	0.00	0.00	0.33	0.02	0.02	0.02	0.02	0.00	0.00	0.92
				7:00	8:00	9.90	0.09	1.63	2.57	0.04	0.29	0.09	0.02	0.02	1.30	0.04	0.04	0.09	0.09	0.02	0.00	3.56
				8:00	9:00	12.10	0.22	3.41	2.71	0.09	0.66	0.42	0.09	0.09	1.01	0.04	0.04	0.09	0.09	0.02	0.00	3.12
				9:00	10:00	11.00	0.15	2.46	2.64	0.09	0.59	0.44	0.09	0.09	0.84	0.07	0.04	0.11	0.11	0.02	0.00	3.26
				10:00	11:00	8.80	0.18	1.67	2.11	0.09	0.77	0.40	0.09	0.07	0.57	0.02	0.02	0.04	0.04	0.00	0.00	2.73
				11:00	12:00	8.80	0.11	1.58	1.74	0.11	0.95	0.13	0.02	0.02	1.01	0.07	0.02	0.07	0.07	0.00	0.00	2.90
				12:00	13:00	8.80	0.15	2.16	2.29	0.09	0.66	0.15	0.02	0.02	0.77	0.09	0.02	0.02	0.02	0.00	0.00	2.33
				13:00	14:00	8.80	0.09	1.41	1.45	0.09	0.75	0.48	0.09	0.09	0.75	0.02	0.07	0.13	0.13	0.02	0.00	3.23
				14:00	15:00	8.80	0.09	1.34	2.02	0.09	0.73	0.37	0.07	0.07	0.79	0.02	0.04	0.09	0.07	0.02	0.00	2.99
				15:00	16:00	9.90	0.15	1.78	2.42	0.11	0.81	0.46	0.11	0.11	0.70	0.04	0.04	0.07	0.07	0.02	0.00	2.99
				16:00	17:00	6.60	0.15	1.39	1.34	0.04	0.42	0.11	0.02	0.00	0.48	0.00	0.04	0.07	0.04	0.00	0.00	2.49
				17:00	18:00	8.80	0.24	2.35	1.91	0.04	0.37	0.22	0.04	0.02	0.57	0.02	0.02	0.04	0.02	0.00	0.00	2.90
				18:00	19:00	6.60	0.22	2.02	1.69	0.02	0.24	0.11	0.02	0.02	0.22	0.02	0.02	0.04	0.02	0.00	0.00	1.91
				19:00	20:00	8.80	0.18	2.62	2.46	0.00	0.13	0.04	0.00	0.00	0.81	0.02	0.02	0.02	0.02	0.00	0.00	2.46
				20:00	21:00	7.70	0.13	1.72	2.40	0.00	0.07	0.02	0.00	0.00	1.06	0.00	0.00	0.02	0.00	0.00	0.00	2.29
				21:00	22:00	7.70	0.11	1.41	2.53	0.00	0.11	0.04	0.00	0.00	0.95	0.00	0.00	0.00	0.00	0.00	0.00	2.55
				22:00	23:00	7.70	0.13	1.01	2.35	0.00	0.04	0.00	0.00	0.00	0.90	0.00	0.00	0.02	0.00	0.00	0.00	3.23
				23:00	0:00	6.60	0.11	0.81	2.05	0.00	0.04	0.00	0.00	0.00	0.79	0.00	0.00	0.02	0.00	0.00	0.00	2.77

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L081	Ma Tau Wai Road (NB)	Man Yue Street	Tsing Chau Street	0:00	1:00	7.70	0.33	1.58	4.14	0.00	0.11	0.00	0.00	0.00	0.86	0.02	0.02	0.09	0.04	0.02	0.00	0.48	
				1:00	2:00	4.40	0.22	1.10	2.97	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.30	0.13	0.75	2.35	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.30	0.18	0.84	2.20	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.20	0.11	0.48	1.54	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.02	0.00	0.00	0.00	0.00
				5:00	6:00	3.30	0.13	0.68	2.16	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.02	0.02	0.09	0.07	0.02	0.00	0.00
				6:00	7:00	6.60	0.20	1.01	3.26	0.02	0.20	0.04	0.00	0.00	0.81	0.07	0.07	0.15	0.15	0.04	0.00	0.57	
				7:00	8:00	15.40	0.29	2.90	5.06	0.07	0.44	0.13	0.04	0.02	2.62	0.20	0.20	0.40	0.40	0.11	0.00	2.53	
				8:00	9:00	19.80	0.77	5.90	5.28	0.13	1.10	0.64	0.18	0.15	2.05	0.22	0.22	0.42	0.42	0.11	0.00	2.22	
				9:00	10:00	18.70	0.57	4.49	5.37	0.11	1.06	0.66	0.20	0.15	1.78	0.35	0.26	0.55	0.57	0.15	0.00	2.42	
				10:00	11:00	17.60	0.73	4.14	5.37	0.20	1.80	0.92	0.20	0.20	1.50	0.11	0.13	0.24	0.26	0.07	0.00	1.74	
				11:00	12:00	18.70	0.48	4.14	4.75	0.29	2.31	0.37	0.09	0.07	2.79	0.35	0.20	0.37	0.40	0.09	0.00	2.00	
				12:00	13:00	17.60	0.57	5.10	5.59	0.18	1.43	0.31	0.09	0.07	1.96	0.35	0.09	0.15	0.18	0.04	0.00	1.50	
				13:00	14:00	17.60	0.42	3.50	3.87	0.22	1.83	1.10	0.24	0.24	1.91	0.09	0.40	0.68	0.75	0.18	0.00	2.18	
				14:00	15:00	17.60	0.37	3.30	5.30	0.22	1.72	0.84	0.18	0.18	2.07	0.15	0.24	0.46	0.48	0.13	0.00	1.96	
				15:00	16:00	19.80	0.64	4.33	6.05	0.22	1.87	1.06	0.24	0.24	1.83	0.31	0.20	0.37	0.37	0.11	0.00	1.96	
				16:00	17:00	18.70	0.81	5.43	5.15	0.20	1.63	0.44	0.07	0.09	1.94	0.04	0.24	0.42	0.46	0.09	0.00	1.69	
				17:00	18:00	23.10	1.21	8.10	6.45	0.18	1.30	0.86	0.18	0.18	2.02	0.15	0.15	0.26	0.26	0.07	0.00	1.74	
				18:00	19:00	19.80	1.10	7.63	6.23	0.11	0.90	0.51	0.11	0.11	0.84	0.11	0.18	0.31	0.33	0.07	0.00	1.28	
				19:00	20:00	17.60	0.66	6.20	6.09	0.02	0.26	0.13	0.02	0.02	2.05	0.11	0.09	0.15	0.18	0.04	0.00	1.56	
				20:00	21:00	15.40	0.55	4.14	6.05	0.02	0.18	0.04	0.02	0.02	2.68	0.04	0.02	0.07	0.04	0.02	0.00	1.50	
				21:00	22:00	14.30	0.37	3.32	6.27	0.02	0.22	0.09	0.02	0.02	2.33	0.00	0.00	0.02	0.00	0.00	0.00	1.61	
				22:00	23:00	14.30	0.48	2.55	6.18	0.02	0.15	0.00	0.00	0.00	2.44	0.04	0.04	0.09	0.04	0.02	0.00	2.22	
				23:00	0:00	12.10	0.44	2.07	5.26	0.02	0.15	0.00	0.00	0.00	2.05	0.04	0.04	0.09	0.04	0.02	0.00	1.87	
L082	Tsing Chau Street (WB)	Ma Tau Wai Road	Wo Chung Street	0:00	1:00	1.10	0.18	0.59	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	1.10	0.18	0.64	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	1.10	0.20	0.59	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.10	0.29	0.53	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.10	0.15	0.64	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	1.10	0.15	0.64	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	1.10	0.15	0.55	0.31	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	1.10	0.09	0.73	0.22	0.02	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	2.20	0.24	1.47	0.24	0.02	0.13	0.04	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.20	0.22	1.39	0.31	0.02	0.15	0.07	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.20	0.29	1.03	0.29	0.04	0.29	0.18	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.20	0.20	1.17	0.29	0.04	0.42	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.20	0.22	1.32	0.29	0.04	0.26	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.20	0.22	1.03	0.22	0.04	0.33	0.26	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	1.10	0.09	0.53	0.15	0.02	0.18	0.09	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.20	0.24	1.03	0.31	0.04	0.29	0.20	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.20	0.33	1.17	0.24	0.00	0.33	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	2.20	0.35	1.30	0.20	0.00	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	2.20	0.37	1.34	0.24	0.00	0.13	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.20	0.24	1.56	0.31	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	1.10	0.15	0.70	0.22	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	1.10	0.11	0.70	0.26	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	1.10	0.18	0.57	0.31	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	1.10	0.20	0.57	0.29	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L083	Tsing Chau Street (EB)	Wo Chung Street	Ma Tau Wai Road	0:00	1:00	1.10	0.11	0.57	0.37	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	1.10	0.11	0.59	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	1.10	0.11	0.55	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	1.10	0.13	0.55	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	1.10	0.13	0.55	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	1.10	0.13	0.55	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	1.10	0.09	0.53	0.40	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	1.10	0.02	0.64	0.31	0.02	0.09	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.20	0.13	1.32	0.35	0.02	0.24	0.09	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.20	0.11	1.17	0.40	0.04	0.29	0.11	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.20	0.18	1.06	0.33	0.04	0.35	0.15	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.20	0.15	1.14	0.33	0.04	0.48	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.20	0.15	1.34	0.33	0.04	0.29	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.20	0.13	1.06	0.26	0.04	0.44	0.18	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	1.10	0.07	0.51	0.20	0.02	0.20	0.07	0.02	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.20	0.18	1.06	0.35	0.04	0.35	0.13	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.20	0.24	1.43	0.24	0.00	0.22	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.20	0.26	1.50	0.22	0.00	0.13	0.07	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.20	0.29	1.52	0.22	0.00	0.09	0.07	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.20	0.18	1.58	0.35	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	1.10	0.09	0.75	0.24	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	1.10	0.09	0.70	0.29	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	1.10	0.13	0.59	0.33	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	1.10	0.13	0.59	0.33	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L084	Cheung Ning Street (SB)	Cul-de-sac	Sheung Heung Street	0:00	1:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.15	0.00	1.03	0.00	0.00	0.56	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	2.15	0.00	1.08	0.00	0.00	0.43	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.15	0.00	0.95	0.00	0.00	0.47	0.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.15	0.00	1.29	0.00	0.00	0.30	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.15	0.00	1.42	0.00	0.00	0.56	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.15	0.00	1.63	0.00	0.00	0.34	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.15	0.00	1.16	0.00	0.00	0.34	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.15	0.00	1.25	0.00	0.00	0.34	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.15	0.00	1.29	0.00	0.00	0.30	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.15	0.00	1.81	0.00	0.00	0.13	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	2.15	0.00	1.85	0.00	0.00	0.09	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	2.15	0.00	1.89	0.00	0.00	0.09	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.15	0.00	1.98	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L085	Cheung Ning Street (NB)	Sheung Heung Street	Cul-de-sac	0:00	1:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.15	0.00	1.81	0.00	0.00	0.17	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.15	0.00	1.72	0.00	0.00	0.13	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.15	0.00	1.63	0.00	0.00	0.17	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.15	0.00	1.08	0.00	0.00	0.39	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.15	0.00	1.25	0.00	0.00	0.60	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.15	0.00	1.55	0.00	0.00	0.30	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.15	0.00	0.95	0.00	0.00	0.39	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.15	0.00	0.99	0.00	0.00	0.52	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.15	0.00	0.99	0.00	0.00	0.39	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.15	0.00	0.60	0.00	0.00	0.95	0.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.15	0.00	0.73	0.00	0.00	0.47	0.95	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.15	0.00	1.08	0.00	0.00	0.34	0.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.15	0.00	1.98	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.15	0.00	2.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L086	Ha Heung Road (WB)	Sheung Heung Street	Kowloon City Road	0:00	1:00	9.90	1.78	6.34	1.39	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	9.90	1.58	6.73	1.58	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	9.90	1.78	6.34	1.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	9.90	2.18	6.53	1.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	9.90	1.39	7.13	1.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	9.90	1.39	7.13	1.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	9.90	1.39	5.94	1.98	0.00	0.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	9.90	0.99	6.73	0.99	0.00	0.59	0.20	0.20	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	29.70	3.76	19.80	1.39	0.00	2.38	0.79	0.79	0.79	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	19.80	2.18	12.08	1.19	0.00	1.98	0.79	0.79	0.79	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	19.80	2.57	10.69	1.39	0.00	2.77	0.79	0.79	0.79	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	19.80	1.98	11.48	1.39	0.00	3.76	0.40	0.40	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	19.80	1.98	13.66	1.39	0.00	2.18	0.20	0.20	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	19.80	1.58	10.49	0.99	0.00	3.17	1.19	1.19	1.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	19.80	1.78	10.49	1.39	0.00	3.17	0.99	0.99	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	19.80	2.18	10.69	1.39	0.00	2.57	0.99	0.99	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	19.80	2.57	13.07	1.19	0.00	1.78	0.40	0.40	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	19.80	2.57	14.06	0.99	0.00	0.99	0.40	0.40	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	19.80	2.57	14.65	1.19	0.00	0.79	0.20	0.20	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	19.80	2.18	16.04	1.39	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	19.80	2.77	14.65	1.98	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	9.90	1.19	7.33	1.19	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	9.90	1.78	6.53	1.39	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	9.90	1.78	6.34	1.39	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L087	Kowloon City Road (NB)	Sheung Heung Street	Ma Hang Chung Road	0:00	1:00	17.75	0.99	6.11	8.17	0.00	0.43	0.00	0.00	0.00	1.35	0.07	0.07	0.28	0.14	0.14	0.00	0.00	
				1:00	2:00	10.65	0.64	3.98	5.68	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.65	0.57	3.76	6.11	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.10	0.50	2.63	3.69	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.10	0.43	2.49	3.98	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	7.10	0.36	2.13	3.69	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.07	0.07	0.14	0.14	0.14	0.00	0.00
				6:00	7:00	14.20	0.57	3.76	6.53	0.00	0.57	0.14	0.00	0.00	1.21	0.14	0.14	0.50	0.43	0.21	0.00	0.00	
				7:00	8:00	35.50	1.14	13.21	11.64	0.07	1.49	0.43	0.07	0.07	4.05	0.43	0.43	1.07	0.85	0.57	0.00	0.00	
				8:00	9:00	53.25	3.05	26.27	11.57	0.14	3.34	1.78	0.36	0.36	3.05	0.43	0.43	0.99	0.92	0.57	0.00	0.00	
				9:00	10:00	46.15	2.13	19.67	11.57	0.14	3.12	1.85	0.36	0.36	2.63	0.64	0.50	1.35	1.14	0.71	0.00	0.00	
				10:00	11:00	42.60	2.20	15.48	10.65	0.21	5.75	2.49	0.50	0.50	2.27	0.28	0.28	0.78	0.71	0.50	0.00	0.00	
				11:00	12:00	42.60	1.42	15.34	9.09	0.21	7.17	0.99	0.21	0.14	4.05	0.78	0.50	1.07	1.07	0.57	0.00	0.00	
				12:00	13:00	42.60	1.70	19.03	10.93	0.14	4.54	0.78	0.21	0.14	2.91	0.78	0.21	0.50	0.50	0.21	0.00	0.00	
				13:00	14:00	39.05	1.21	12.35	7.03	0.21	5.54	2.84	0.57	0.57	2.77	0.21	0.85	1.99	1.85	1.07	0.00	0.00	
				14:00	15:00	39.05	1.14	12.07	10.01	0.21	5.33	2.20	0.43	0.43	2.98	0.43	0.50	1.35	1.28	0.71	0.00	0.00	
				15:00	16:00	46.15	1.85	15.90	11.79	0.21	5.89	2.84	0.57	0.57	2.70	0.64	0.50	1.14	0.99	0.57	0.00	0.00	
				16:00	17:00	42.60	1.92	17.04	8.88	0.14	5.40	1.14	0.21	0.21	2.91	0.14	0.71	1.56	1.56	0.78	0.00	0.00	
				17:00	18:00	53.25	2.91	25.35	11.22	0.14	4.19	2.20	0.43	0.43	3.05	0.43	0.43	0.99	0.92	0.57	0.00	0.00	
				18:00	19:00	46.15	2.56	23.50	10.51	0.07	2.98	1.28	0.21	0.28	1.14	0.28	0.50	1.14	1.14	0.57	0.00	0.00	
				19:00	20:00	42.60	1.92	23.00	11.79	0.00	0.78	0.36	0.07	0.07	3.05	0.21	0.21	0.50	0.43	0.21	0.00	0.00	
				20:00	21:00	35.50	1.70	15.90	12.21	0.00	0.64	0.14	0.07	0.07	4.19	0.07	0.07	0.14	0.14	0.14	0.00	0.00	
				21:00	22:00	28.40	1.07	11.57	11.29	0.00	0.71	0.21	0.07	0.07	3.27	0.00	0.00	0.07	0.00	0.07	0.00	0.00	
				22:00	23:00	28.40	1.49	9.59	12.43	0.00	0.43	0.00	0.00	0.00	3.69	0.14	0.07	0.21	0.14	0.21	0.00	0.00	
				L088	Pau Chung Street (SB)	Ma Hang Chung Road	Sheung Heung Street	23:00	0:00	21.30	1.21	6.89	9.30	0.00	0.36	0.00	0.00	0.00	2.77	0.14	0.07	0.21	0.14
0:00	1:00	2.65	0.37					1.06	1.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	2.65	0.32					1.01	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.65	0.32					1.01	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.65	0.42					0.90	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.65	0.42					0.90	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.65	0.42					0.90	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	2.65	0.27					0.74	1.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.27	0.27	0.00	0.00	0.00	
7:00	8:00	2.65	0.16					1.06	0.95	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.16	0.16	0.00	0.00	0.00	
8:00	9:00	2.65	0.27					1.22	0.53	0.00	0.27	0.16	0.00	0.00	0.00	0.00	0.00	0.11	0.11	0.00	0.00	0.00	
9:00	10:00	2.65	0.21					1.06	0.64	0.00	0.32	0.21	0.00	0.00	0.00	0.00	0.00	0.11	0.11	0.00	0.00	0.00	
10:00	11:00	2.65	0.27					1.11	0.74	0.00	0.27	0.16	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.00	0.00	0.00	
11:00	12:00	2.65	0.21					1.01	0.58	0.00	0.42	0.05	0.00	0.00	0.00	0.05	0.00	0.16	0.16	0.00	0.00	0.00	
12:00	13:00	2.65	0.21					1.27	0.69	0.00	0.27	0.05	0.00	0.00	0.00	0.05	0.00	0.05	0.05	0.00	0.00	0.00	
13:00	14:00	2.65	0.16					0.90	0.48	0.00	0.37	0.21	0.00	0.00	0.00	0.00	0.05	0.21	0.21	0.05	0.00	0.00	
14:00	15:00	2.65	0.16					0.90	0.69	0.00	0.32	0.16	0.00	0.00	0.00	0.00	0.05	0.16	0.16	0.05	0.00	0.00	
15:00	16:00	2.65	0.16					1.01	0.64	0.00	0.32	0.16	0.00	0.00	0.00	0.05	0.05	0.11	0.11	0.05	0.00	0.00	
16:00	17:00	5.30	0.48					2.28	1.11	0.00	0.37	0.11	0.00	0.00	0.00	0.00	0.21	0.27	0.37	0.11	0.00	0.00	
17:00	18:00	5.30	0.58					2.76	1.11	0.00	0.21	0.16	0.00	0.00	0.00	0.05	0.05	0.16	0.16	0.05	0.00	0.00	
18:00	19:00	5.30	0.58					2.60	1.11	0.00	0.16	0.11	0.00	0.00	0.00	0.11	0.11	0.16	0.27	0.11	0.00	0.00	
19:00	20:00	2.65	0.27					1.54	0.69	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.00	0.00	0.00	
20:00	21:00	2.65	0.27					1.38	1.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	2.65	0.21					1.22	1.11	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	2.65	0.37					1.06	1.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	2.65	0.32	1.11	1.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L089	Pau Chung Street (NB)	Sheung Heung Street	Ma Hang Chung Road	0:00	1:00	5.10	0.15	1.53	3.21	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.05	0.00	0.00	0.00	0.00	
				1:00	2:00	5.10	0.20	1.48	3.21	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.10	0.20	1.43	3.37	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.55	0.15	0.71	1.53	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.55	0.10	0.71	1.63	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.55	0.05	0.61	1.68	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00	0.00	0.00
				6:00	7:00	5.10	0.15	1.17	3.11	0.00	0.31	0.05	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.15	0.05	0.00	0.00	0.00
				7:00	8:00	12.75	0.31	4.49	5.87	0.05	0.77	0.20	0.00	0.00	0.00	0.00	0.00	0.26	0.20	0.36	0.26	0.00	0.00	0.00
				8:00	9:00	17.85	0.71	8.06	5.41	0.10	1.63	0.82	0.10	0.05	0.00	0.00	0.20	0.20	0.36	0.20	0.00	0.00	0.00	0.00
				9:00	10:00	17.85	0.56	6.73	6.12	0.10	1.73	0.97	0.10	0.05	0.00	0.36	0.31	0.51	0.31	0.00	0.00	0.00	0.00	
				10:00	11:00	12.75	0.46	4.13	4.28	0.10	2.19	0.92	0.10	0.05	0.00	0.10	0.10	0.20	0.10	0.00	0.00	0.00	0.00	
				11:00	12:00	12.75	0.31	4.08	3.88	0.20	2.81	0.36	0.05	0.00	0.00	0.31	0.20	0.36	0.20	0.00	0.00	0.00	0.00	
				12:00	13:00	12.75	0.41	5.05	4.49	0.10	1.73	0.31	0.05	0.00	0.00	0.36	0.05	0.10	0.10	0.00	0.00	0.00	0.00	
				13:00	14:00	12.75	0.31	3.77	3.26	0.10	2.35	1.17	0.20	0.10	0.00	0.10	0.36	0.66	0.36	0.00	0.00	0.00	0.00	
				14:00	15:00	12.75	0.26	3.52	4.39	0.15	2.19	0.92	0.15	0.05	0.00	0.20	0.26	0.41	0.26	0.00	0.00	0.00	0.00	
				15:00	16:00	15.30	0.41	4.69	5.25	0.10	2.45	1.07	0.20	0.05	0.00	0.31	0.20	0.36	0.20	0.00	0.00	0.00	0.00	
				15:00	17:00	10.20	0.36	3.93	3.11	0.05	1.53	0.31	0.05	0.00	0.00	0.05	0.20	0.36	0.26	0.00	0.00	0.00	0.00	
				17:00	18:00	12.75	0.56	5.71	3.88	0.05	1.22	0.61	0.10	0.05	0.00	0.10	0.10	0.26	0.10	0.00	0.00	0.00	0.00	
				18:00	19:00	10.20	0.41	4.85	3.21	0.05	0.77	0.31	0.05	0.05	0.00	0.05	0.10	0.20	0.15	0.00	0.00	0.00	0.00	
				19:00	20:00	12.75	0.46	6.32	5.05	0.00	0.36	0.10	0.00	0.00	0.00	0.10	0.05	0.20	0.10	0.00	0.00	0.00	0.00	
				20:00	21:00	10.20	0.36	4.34	5.10	0.00	0.26	0.05	0.00	0.00	0.00	0.00	0.05	0.00	0.05	0.00	0.00	0.00	0.00	
				21:00	22:00	10.20	0.26	3.77	5.71	0.00	0.31	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	7.65	0.26	2.40	4.69	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.00	0.00	0.00	0.00	
L090	Sheung Heung Street (EB)	Ma Tau Wai Road	Pau Chung Street	23:00	0:00	7.65	0.31	2.24	4.74	0.00	0.20	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.00	0.00	0.00	0.00		
				0:00	1:00	16.20	0.32	3.97	7.21	0.00	0.49	0.00	0.00	0.00	3.65	0.08	0.08	0.24	0.08	0.08	0.00	0.00		
				1:00	2:00	8.10	0.24	2.43	4.62	0.00	0.41	0.00	0.00	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				2:00	3:00	8.10	0.08	2.27	4.94	0.00	0.32	0.00	0.00	0.00	0.49	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				3:00	4:00	4.05	0.08	1.30	2.43	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				4:00	5:00	4.05	0.08	1.22	2.59	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				5:00	6:00	8.10	0.16	2.11	4.46	0.00	0.57	0.00	0.00	0.00	0.00	0.00	0.16	0.32	0.16	0.16	0.00	0.00		
				6:00	7:00	12.15	0.16	2.27	4.94	0.00	0.73	0.16	0.00	0.00	2.92	0.08	0.08	0.41	0.24	0.16	0.00	0.00		
				7:00	8:00	32.40	0.32	8.51	9.72	0.08	2.03	0.49	0.00	0.00	8.83	0.32	0.24	0.81	0.57	0.49	0.00	0.00		
				8:00	9:00	48.60	0.97	18.63	10.77	0.16	5.10	2.59	0.16	0.08	7.29	0.32	0.32	0.97	0.65	0.57	0.00	0.00		
				9:00	10:00	40.50	0.65	12.80	10.04	0.16	4.54	2.59	0.16	0.08	5.91	0.49	0.41	1.13	0.89	0.65	0.00	0.00		
				10:00	11:00	36.45	0.65	9.48	8.51	0.24	6.97	3.08	0.16	0.08	5.51	0.16	0.16	0.65	0.41	0.41	0.00	0.00		
				11:00	12:00	36.45	0.32	8.34	6.64	0.24	8.02	1.05	0.08	0.00	9.15	0.49	0.24	0.81	0.65	0.41	0.00	0.00		
				12:00	13:00	36.45	0.49	11.42	8.75	0.16	5.59	1.05	0.08	0.00	7.13	0.65	0.16	0.41	0.41	0.16	0.00	0.00		
				13:00	14:00	36.45	0.32	7.70	5.83	0.24	6.97	3.56	0.24	0.16	6.89	0.16	0.65	1.54	1.30	0.89	0.00	0.00		
				14:00	15:00	36.45	0.24	7.45	8.10	0.24	6.64	2.75	0.16	0.08	7.53	0.24	0.41	1.05	0.89	0.65	0.00	0.00		
				15:00	16:00	40.50	0.57	9.32	9.56	0.24	7.21	3.48	0.24	0.08	6.72	0.49	0.41	0.89	0.73	0.57	0.00	0.00		
				16:00	17:00	32.40	0.41	8.91	6.16	0.16	5.27	1.13	0.08	0.00	7.29	0.08	0.41	0.97	0.97	0.57	0.00	0.00		
				17:00	18:00	40.50	0.73	13.85	8.18	0.16	4.46	2.35	0.16	0.08	8.10	0.32	0.32	0.73	0.57	0.49	0.00	0.00		
				18:00	19:00	32.40	0.65	13.37	7.86	0.08	3.08	1.38	0.08	0.08	3.32	0.16	0.32	0.81	0.73	0.49	0.00	0.00		
				19:00	20:00	32.40	0.49	13.28	9.07	0.00	0.97	0.41	0.00	0.00	7.05	0.16	0.16	0.32	0.32	0.16	0.00	0.00		
				20:00	21:00	32.40	0.49	9.88	10.04	0.00	0.73	0.16	0.00	0.00	10.61	0.08	0.08	0.16	0.08	0.08	0.00	0.00		
				21:00	22:00	28.35	0.32	7.78	10.04	0.00	0.89	0.32	0.00	0.00	8.83	0.00	0.00	0.08	0.00	0.08	0.00	0.00		
22:00	23:00	24.30	0.32	5.43	9.15	0.00	0.49	0.00	0.00	0.00	8.34	0.08	0.08	0.16	0.08	0.16	0.00	0.00						
23:00	0:00	20.25	0.32	4.29	7.61	0.00	0.57	0.00	0.00	0.00	6.89	0.08	0.08	0.16	0.08	0.16	0.00	0.00						

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L091	Sheung Heung Street (WB)	Pau Chung Street	Ma Tau Wai Road	0:00	1:00	4.05	0.16	2.11	1.46	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	4.05	0.24	2.03	1.54	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.05	0.32	2.19	1.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.05	0.32	2.03	1.38	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.05	0.00	2.51	1.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.05	0.00	1.78	1.46	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.41	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.05	0.16	1.70	1.22	0.00	0.16	0.00	0.00	0.00	0.00	0.16	0.16	0.16	0.16	0.16	0.00	0.00	
				7:00	8:00	4.05	0.08	2.11	0.81	0.08	0.41	0.08	0.00	0.00	0.00	0.08	0.08	0.16	0.08	0.08	0.00	0.00	
				8:00	9:00	8.10	0.16	4.70	0.97	0.08	0.97	0.57	0.08	0.08	0.00	0.08	0.08	0.16	0.08	0.08	0.00	0.00	
				9:00	10:00	8.10	0.08	4.05	1.13	0.08	1.05	0.73	0.08	0.08	0.00	0.24	0.08	0.32	0.08	0.08	0.00	0.00	
				10:00	11:00	8.10	0.32	3.89	1.22	0.08	1.22	0.65	0.08	0.08	0.00	0.08	0.08	0.24	0.08	0.08	0.00	0.00	
				11:00	12:00	8.10	0.24	3.65	1.05	0.24	1.62	0.24	0.00	0.00	0.00	0.32	0.08	0.32	0.24	0.08	0.00	0.00	
				12:00	13:00	8.10	0.16	4.78	1.30	0.08	0.97	0.16	0.00	0.00	0.00	0.32	0.08	0.08	0.08	0.08	0.00	0.00	
				13:00	14:00	8.10	0.24	3.16	0.89	0.08	1.22	0.73	0.08	0.08	0.00	0.08	0.32	0.57	0.32	0.32	0.00	0.00	
				14:00	15:00	8.10	0.08	3.32	1.30	0.08	1.22	0.65	0.08	0.08	0.00	0.08	0.24	0.49	0.24	0.24	0.00	0.00	
				15:00	16:00	8.10	0.16	3.73	1.30	0.08	1.13	0.65	0.08	0.08	0.00	0.16	0.16	0.24	0.16	0.16	0.00	0.00	
				16:00	17:00	8.10	0.24	4.54	1.05	0.08	0.65	0.16	0.00	0.00	0.00	0.08	0.24	0.49	0.32	0.24	0.00	0.00	
				17:00	18:00	12.15	0.49	7.86	1.62	0.08	0.57	0.41	0.08	0.00	0.00	0.16	0.16	0.41	0.16	0.16	0.00	0.00	
				18:00	19:00	12.15	0.41	8.02	1.62	0.08	0.41	0.24	0.08	0.00	0.00	0.08	0.24	0.41	0.32	0.24	0.00	0.00	
				19:00	20:00	8.10	0.24	5.67	1.46	0.00	0.24	0.08	0.00	0.00	0.00	0.08	0.08	0.08	0.08	0.08	0.00	0.00	
				20:00	21:00	8.10	0.32	5.43	2.03	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.00	0.00	0.00	
				21:00	22:00	4.05	0.08	2.51	1.22	0.00	0.16	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	4.05	0.24	2.19	1.46	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	
				L092	Sheung Heung Street (EB)	Pau Chung Street	Kowloon City Road	23:00	0:00	4.05	0.24	2.11	1.38	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.00
0:00	1:00	6.60	0.13					1.36	2.90	0.00	0.13	0.00	0.00	0.00	1.80	0.04	0.04	0.09	0.04	0.04	0.00	0.00	
1:00	2:00	4.40	0.13					1.23	2.60	0.00	0.13	0.00	0.00	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	4.40	0.18					1.06	2.68	0.00	0.18	0.00	0.00	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.20	0.13					0.62	1.32	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.20	0.09					0.62	1.41	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.20	0.04					0.48	1.36	0.00	0.09	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.04	0.00	0.00	
6:00	7:00	6.60	0.13					1.01	2.60	0.00	0.31	0.04	0.00	0.00	1.94	0.04	0.13	0.18	0.18	0.04	0.00	0.00	
7:00	8:00	13.20	0.18					2.77	3.65	0.04	0.57	0.13	0.04	0.04	4.80	0.18	0.13	0.26	0.26	0.13	0.00	0.00	
8:00	9:00	17.60	0.53					5.85	3.87	0.09	1.45	0.70	0.13	0.13	3.74	0.18	0.18	0.31	0.31	0.13	0.00	0.00	
9:00	10:00	17.60	0.40					4.84	4.36	0.09	1.50	0.84	0.18	0.18	3.61	0.31	0.26	0.44	0.44	0.18	0.00	0.00	
10:00	11:00	15.40	0.40					3.56	3.61	0.13	2.38	1.01	0.22	0.22	3.04	0.09	0.13	0.26	0.26	0.09	0.00	0.00	
11:00	12:00	17.60	0.26					3.43	3.17	0.13	3.04	0.40	0.04	0.04	5.68	0.35	0.18	0.35	0.40	0.13	0.00	0.00	
12:00	13:00	15.40	0.31					4.27	3.74	0.09	1.89	0.31	0.04	0.04	3.87	0.40	0.09	0.13	0.18	0.04	0.00	0.00	
13:00	14:00	15.40	0.22					2.82	2.46	0.13	2.33	1.19	0.18	0.22	3.78	0.09	0.35	0.66	0.70	0.26	0.00	0.00	
14:00	15:00	15.40	0.18					2.68	3.43	0.13	2.20	0.92	0.13	0.13	4.09	0.13	0.26	0.44	0.48	0.18	0.00	0.00	
15:00	16:00	17.60	0.35					3.65	4.22	0.13	2.51	1.19	0.18	0.18	3.78	0.31	0.18	0.40	0.40	0.13	0.00	0.00	
16:00	17:00	15.40	0.31					3.74	3.04	0.09	2.02	0.44	0.04	0.04	4.09	0.04	0.26	0.53	0.57	0.18	0.00	0.00	
17:00	18:00	17.60	0.48					5.50	3.70	0.09	1.54	0.84	0.13	0.13	4.09	0.18	0.18	0.31	0.31	0.13	0.00	0.00	
18:00	19:00	13.20	0.44					4.88	3.34	0.04	1.06	0.44	0.09	0.09	1.58	0.13	0.22	0.35	0.40	0.13	0.00	0.00	
19:00	20:00	15.40	0.40					5.28	4.27	0.00	0.35	0.18	0.00	0.04	4.27	0.09	0.09	0.18	0.22	0.04	0.00	0.00	
20:00	21:00	13.20	0.26					3.30	3.92	0.00	0.22	0.04	0.00	0.00	5.24	0.04	0.04	0.04	0.04	0.04	0.00	0.00	
21:00	22:00	13.20	0.22					2.99	4.53	0.00	0.31	0.09	0.00	0.00	5.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	11.00	0.22					1.98	3.87	0.00	0.13	0.00	0.00	0.00	4.53	0.04	0.04	0.09	0.04	0.04	0.00	0.00	
23:00	0:00	8.80	0.22	1.50	3.04	0.00	0.18	0.00	0.00	0.00	3.61	0.04	0.04	0.09	0.04	0.04	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L093	Sheung Heung Street (WB)	Kowloon City Road	Pau Chung Street	0:00	1:00	2.20	0.13	0.84	1.14	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	0.00	0.00	
				1:00	2:00	2.20	0.13	0.88	1.06	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.20	0.13	0.84	1.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.20	0.18	0.79	1.06	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.20	0.22	0.79	1.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.20	0.18	0.79	1.06	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.20	0.09	0.70	0.97	0.00	0.09	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.00
				7:00	8:00	4.40	0.18	1.72	1.32	0.00	0.40	0.09	0.00	0.00	0.00	0.18	0.18	0.18	0.18	0.00	0.00	0.00	0.00
				8:00	9:00	6.60	0.40	3.04	1.19	0.00	0.79	0.44	0.13	0.09	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00
				9:00	10:00	4.40	0.22	1.80	0.88	0.00	0.53	0.35	0.09	0.04	0.00	0.13	0.09	0.13	0.13	0.00	0.00	0.00	0.00
				10:00	11:00	4.40	0.26	1.67	1.01	0.00	0.70	0.31	0.09	0.04	0.00	0.04	0.09	0.09	0.09	0.00	0.00	0.00	0.00
				11:00	12:00	4.40	0.18	1.80	0.88	0.00	0.84	0.13	0.04	0.00	0.00	0.18	0.09	0.13	0.13	0.00	0.00	0.00	0.00
				12:00	13:00	4.40	0.22	2.11	1.06	0.00	0.53	0.09	0.04	0.00	0.00	0.22	0.04	0.04	0.04	0.00	0.00	0.00	0.00
				13:00	14:00	4.40	0.13	1.54	0.75	0.00	0.70	0.40	0.09	0.09	0.00	0.04	0.22	0.22	0.22	0.00	0.00	0.00	0.00
				14:00	15:00	4.40	0.13	1.58	1.06	0.00	0.70	0.31	0.09	0.04	0.00	0.09	0.13	0.13	0.13	0.00	0.00	0.00	0.00
				15:00	16:00	6.60	0.31	2.38	1.54	0.00	0.92	0.48	0.13	0.04	0.00	0.26	0.18	0.18	0.18	0.00	0.00	0.00	0.00
				16:00	17:00	6.60	0.40	3.12	1.41	0.00	0.57	0.13	0.00	0.00	0.00	0.04	0.31	0.31	0.31	0.00	0.00	0.00	0.00
				17:00	18:00	6.60	0.44	3.61	1.41	0.00	0.35	0.18	0.04	0.04	0.00	0.13	0.13	0.13	0.13	0.00	0.00	0.00	0.00
				18:00	19:00	6.60	0.44	3.61	1.41	0.00	0.31	0.09	0.04	0.04	0.00	0.09	0.22	0.13	0.22	0.00	0.00	0.00	0.00
				19:00	20:00	4.40	0.22	2.68	1.19	0.00	0.09	0.04	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.00	0.00	0.00	0.00
				20:00	21:00	4.40	0.26	2.38	1.58	0.00	0.13	0.00	0.00	0.00	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	4.40	0.22	2.11	1.85	0.00	0.13	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	4.40	0.31	1.76	1.98	0.00	0.09	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.00
				L094	Sheung Heung Street (EB)	Kowloon City Road	Ha Heung Road	23:00	0:00	2.20	0.13	0.88	1.01	0.00	0.04	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.00
0:00	1:00	6.60	0.09					1.72	2.42	0.00	0.13	0.00	0.00	0.00	2.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	2.20	0.04					0.75	1.19	0.00	0.04	0.00	0.00	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.20	0.00					0.75	1.19	0.00	0.04	0.00	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.20	0.00					0.88	1.23	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.20	0.00					0.79	1.32	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.20	0.00					0.84	1.28	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	4.40	0.04					0.84	1.50	0.00	0.13	0.04	0.00	0.00	1.67	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
7:00	8:00	11.00	0.04					2.73	2.38	0.00	0.35	0.09	0.04	0.04	5.15	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
8:00	9:00	13.20	0.13					5.10	2.33	0.00	0.75	0.40	0.31	0.31	3.70	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
9:00	10:00	13.20	0.09					4.49	2.68	0.00	0.84	0.48	0.35	0.35	3.70	0.09	0.04	0.04	0.04	0.00	0.00	0.00	
10:00	11:00	13.20	0.09					3.87	2.73	0.00	1.45	0.62	0.48	0.48	3.30	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
11:00	12:00	13.20	0.04					3.34	2.07	0.00	1.58	0.18	0.18	0.13	5.32	0.13	0.04	0.09	0.09	0.00	0.00	0.00	
12:00	13:00	13.20	0.09					4.49	2.64	0.00	1.10	0.22	0.22	0.13	4.05	0.13	0.04	0.04	0.04	0.00	0.00	0.00	
13:00	14:00	13.20	0.04					3.21	1.94	0.00	1.45	0.75	0.57	0.57	4.09	0.04	0.18	0.18	0.18	0.00	0.00	0.00	
14:00	15:00	13.20	0.04					2.99	2.60	0.00	1.36	0.57	0.40	0.40	4.53	0.04	0.09	0.09	0.09	0.00	0.00	0.00	
15:00	16:00	13.20	0.09					3.61	2.73	0.00	1.32	0.62	0.48	0.48	3.52	0.09	0.09	0.09	0.09	0.00	0.00	0.00	
16:00	17:00	13.20	0.09					4.44	2.38	0.00	1.19	0.22	0.13	0.18	4.09	0.04	0.13	0.13	0.18	0.00	0.00	0.00	
17:00	18:00	15.40	0.13					6.12	2.77	0.00	0.88	0.44	0.35	0.35	4.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00	
18:00	19:00	13.20	0.13					6.42	2.95	0.00	0.70	0.31	0.26	0.26	1.80	0.04	0.09	0.09	0.13	0.00	0.00	0.00	
19:00	20:00	13.20	0.09					5.41	2.90	0.00	0.22	0.09	0.04	0.04	4.22	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
20:00	21:00	13.20	0.09					3.83	3.04	0.00	0.18	0.04	0.04	0.04	5.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	11.00	0.04					2.90	2.95	0.00	0.22	0.04	0.04	0.04	4.75	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	11.00	0.04					2.38	3.12	0.00	0.13	0.00	0.00	0.00	5.32	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	8.80	0.04	1.80	2.51	0.00	0.09	0.00	0.00	0.00	4.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L095	Sheung Heung Street (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	8.80	0.09	3.17	4.62	0.00	0.04	0.00	0.00	0.00	0.79	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				1:00	2:00	6.60	0.04	2.55	3.96	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.40	0.04	1.54	2.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.40	0.04	1.72	2.60	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.40	0.09	1.54	2.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.40	0.04	1.45	2.73	0.00	0.04	0.00	0.00	0.04	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				6:00	7:00	6.60	0.04	1.98	3.70	0.00	0.04	0.00	0.00	0.04	0.70	0.00	0.00	0.04	0.00	0.04	0.00	0.00	0.00
				7:00	8:00	17.60	0.09	7.39	6.95	0.00	0.13	0.04	0.13	0.13	2.38	0.00	0.00	0.18	0.00	0.18	0.00	0.00	0.00
				8:00	9:00	26.40	0.31	15.00	7.17	0.00	0.31	0.22	0.62	0.62	1.80	0.00	0.00	0.18	0.00	0.18	0.00	0.00	0.00
				9:00	10:00	24.20	0.26	11.97	7.74	0.00	0.35	0.26	0.66	0.70	1.72	0.00	0.00	0.26	0.00	0.26	0.00	0.00	0.00
				10:00	11:00	19.80	0.22	8.67	6.51	0.04	0.57	0.31	0.88	0.92	1.41	0.00	0.00	0.13	0.00	0.13	0.00	0.00	0.00
				11:00	12:00	17.60	0.13	8.01	5.28	0.04	0.66	0.13	0.35	0.31	2.38	0.00	0.00	0.18	0.00	0.13	0.00	0.00	0.00
				12:00	13:00	22.00	0.22	11.31	7.08	0.04	0.48	0.09	0.40	0.35	1.89	0.00	0.00	0.09	0.00	0.04	0.00	0.00	0.00
				13:00	14:00	17.60	0.13	7.39	4.53	0.04	0.57	0.35	1.06	1.14	1.76	0.00	0.00	0.31	0.00	0.31	0.00	0.00	0.00
				14:00	15:00	17.60	0.09	6.69	6.12	0.04	0.53	0.26	0.79	0.84	1.85	0.00	0.00	0.22	0.00	0.18	0.00	0.00	0.00
				15:00	16:00	22.00	0.22	8.98	7.44	0.04	0.57	0.40	1.10	1.10	1.72	0.00	0.00	0.22	0.00	0.22	0.00	0.00	0.00
				16:00	17:00	17.60	0.18	8.89	5.06	0.04	0.48	0.13	0.35	0.35	1.72	0.00	0.00	0.22	0.00	0.18	0.00	0.00	0.00
				17:00	18:00	26.40	0.31	14.43	6.91	0.04	0.44	0.31	0.88	0.88	1.94	0.00	0.00	0.13	0.00	0.13	0.00	0.00	0.00
				18:00	19:00	22.00	0.22	12.85	6.34	0.04	0.31	0.13	0.53	0.57	0.75	0.00	0.00	0.13	0.00	0.13	0.00	0.00	0.00
				19:00	20:00	22.00	0.18	12.45	7.04	0.00	0.09	0.04	0.09	0.13	1.85	0.00	0.00	0.09	0.00	0.04	0.00	0.00	0.00
				20:00	21:00	19.80	0.22	9.06	7.61	0.00	0.04	0.00	0.09	0.09	2.60	0.00	0.00	0.04	0.00	0.04	0.00	0.00	0.00
				21:00	22:00	17.60	0.09	7.22	7.74	0.00	0.09	0.04	0.09	0.09	2.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	15.40	0.13	5.32	7.57	0.00	0.04	0.00	0.00	0.00	2.24	0.00	0.00	0.04	0.00	0.04	0.00	0.00	0.00
L096	Sheung Heung Street (EB)	Ha Heung Road	Cheung Ning Street	23:00	0:00	13.20	0.18	4.44	6.51	0.00	0.04	0.00	0.00	0.00	1.94	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				0:00	1:00	6.60	0.04	1.63	2.38	0.00	0.13	0.00	0.00	0.00	2.11	0.04	0.04	0.13	0.04	0.04	0.00	0.00	
				1:00	2:00	2.20	0.00	0.84	1.14	0.00	0.04	0.00	0.00	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.20	0.00	0.70	1.23	0.00	0.04	0.00	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.20	0.00	0.88	1.23	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.20	0.00	0.79	1.32	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.20	0.00	0.66	1.19	0.00	0.04	0.00	0.00	0.00	0.00	0.04	0.04	0.13	0.04	0.04	0.00	0.00	
				6:00	7:00	4.40	0.00	0.84	1.41	0.00	0.09	0.04	0.00	0.00	1.50	0.09	0.09	0.18	0.13	0.04	0.00	0.00	
				7:00	8:00	11.00	0.04	2.33	2.07	0.04	0.40	0.09	0.09	0.09	4.93	0.18	0.13	0.26	0.26	0.09	0.00	0.00	
				8:00	9:00	15.40	0.09	5.19	2.38	0.04	0.97	0.53	0.40	0.40	4.18	0.22	0.22	0.35	0.35	0.09	0.00	0.00	
				9:00	10:00	13.20	0.04	3.70	2.24	0.04	0.88	0.53	0.40	0.40	3.43	0.31	0.22	0.44	0.44	0.13	0.00	0.00	
				10:00	11:00	13.20	0.09	3.70	2.60	0.04	1.23	0.57	0.44	0.44	2.99	0.13	0.18	0.35	0.35	0.09	0.00	0.00	
				11:00	12:00	15.40	0.04	3.65	2.24	0.09	1.58	0.22	0.22	0.18	5.46	0.44	0.26	0.44	0.48	0.09	0.00	0.00	
				12:00	13:00	13.20	0.04	4.40	2.55	0.04	0.97	0.18	0.18	0.13	3.78	0.40	0.09	0.18	0.22	0.04	0.00	0.00	
				13:00	14:00	15.40	0.04	3.34	1.94	0.04	1.32	0.70	0.57	0.57	4.05	0.18	0.53	0.88	0.97	0.26	0.00	0.00	
				14:00	15:00	13.20	0.04	2.77	2.33	0.04	1.10	0.48	0.35	0.35	4.00	0.18	0.26	0.57	0.57	0.13	0.00	0.00	
				15:00	16:00	15.40	0.09	3.87	2.95	0.04	1.32	0.62	0.53	0.53	3.70	0.40	0.26	0.48	0.48	0.13	0.00	0.00	
				16:00	17:00	15.40	0.09	4.93	2.64	0.04	0.92	0.22	0.13	0.13	3.96	0.09	0.40	0.79	0.88	0.18	0.00	0.00	
				17:00	18:00	19.80	0.13	7.88	3.56	0.04	0.79	0.44	0.35	0.35	4.44	0.31	0.31	0.53	0.53	0.13	0.00	0.00	
				18:00	19:00	15.40	0.13	7.00	3.30	0.04	0.53	0.26	0.22	0.22	1.76	0.22	0.35	0.57	0.66	0.13	0.00	0.00	
				19:00	20:00	13.20	0.09	5.32	2.82	0.00	0.18	0.09	0.04	0.04	3.96	0.13	0.09	0.18	0.22	0.04	0.00	0.00	
				20:00	21:00	13.20	0.04	3.83	3.04	0.00	0.13	0.04	0.04	0.04	5.76	0.04	0.04	0.09	0.04	0.04	0.00	0.00	
				21:00	22:00	11.00	0.04	2.99	2.99	0.00	0.13	0.04	0.04	0.04	4.66	0.00	0.00	0.04	0.00	0.00	0.00	0.00	
22:00	23:00	11.00	0.04	2.38	3.08	0.00	0.09	0.00	0.00	0.00	5.15	0.04	0.04	0.09	0.04	0.04	0.00	0.00					
23:00	0:00	8.80	0.04	1.80	2.46	0.00	0.09	0.00	0.00	0.00	4.14	0.04	0.04	0.09	0.04	0.04	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L097	Sheung Heung Street (EB)	Cheung Ning Street	To Kwa Wan Road	0:00	1:00	9.90	0.26	3.04	3.23	0.00	0.13	0.00	0.00	0.00	2.84	0.07	0.07	0.13	0.07	0.07	0.00	0.00	
				1:00	2:00	6.60	0.26	2.71	2.90	0.00	0.26	0.00	0.00	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.30	0.07	1.25	1.65	0.00	0.07	0.00	0.00	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.30	0.07	1.45	1.58	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.30	0.13	1.32	1.72	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.30	0.07	1.12	1.45	0.00	0.20	0.00	0.00	0.00	0.00	0.07	0.07	0.20	0.07	0.07	0.00	0.00	0.00
				6:00	7:00	9.90	0.20	2.24	2.90	0.00	0.26	0.13	0.00	0.00	2.97	0.20	0.20	0.40	0.26	0.13	0.00	0.00	0.00
				7:00	8:00	19.80	0.20	5.08	3.43	0.07	0.86	0.20	0.13	0.13	8.12	0.33	0.20	0.46	0.46	0.13	0.00	0.00	0.00
				8:00	9:00	26.40	0.59	10.30	3.43	0.07	1.91	0.92	0.66	0.66	6.14	0.26	0.26	0.53	0.53	0.13	0.00	0.00	0.00
				9:00	10:00	23.10	0.46	7.52	3.43	0.07	1.72	0.99	0.66	0.66	5.21	0.46	0.40	0.66	0.66	0.20	0.00	0.00	0.00
				10:00	11:00	23.10	0.53	7.33	3.96	0.07	2.57	1.06	0.73	0.73	4.49	0.20	0.26	0.53	0.53	0.13	0.00	0.00	0.00
				11:00	12:00	26.40	0.40	7.33	3.50	0.13	3.23	0.46	0.26	0.20	8.32	0.66	0.40	0.66	0.73	0.13	0.00	0.00	0.00
				12:00	13:00	23.10	0.40	8.91	3.96	0.07	1.98	0.33	0.26	0.20	5.61	0.59	0.13	0.26	0.33	0.07	0.00	0.00	0.00
				13:00	14:00	23.10	0.26	5.87	2.57	0.07	2.44	1.19	0.79	0.86	5.41	0.20	0.66	1.19	1.25	0.33	0.00	0.00	0.00
				14:00	15:00	23.10	0.26	5.81	3.70	0.07	2.38	0.92	0.66	0.66	6.01	0.26	0.46	0.86	0.86	0.20	0.00	0.00	0.00
				15:00	16:00	26.40	0.40	7.79	4.42	0.07	2.64	1.19	0.86	0.86	5.54	0.59	0.40	0.73	0.73	0.20	0.00	0.00	0.00
				16:00	17:00	26.40	0.53	9.77	3.96	0.07	1.91	0.40	0.20	0.26	5.81	0.13	0.59	1.19	1.32	0.26	0.00	0.00	0.00
				17:00	18:00	33.00	0.86	14.59	5.08	0.07	1.52	0.79	0.59	0.59	6.20	0.46	0.46	0.79	0.79	0.20	0.00	0.00	0.00
				18:00	19:00	26.40	0.73	13.73	4.75	0.07	0.99	0.40	0.33	0.33	2.44	0.26	0.46	0.79	0.92	0.20	0.00	0.00	0.00
				19:00	20:00	23.10	0.46	10.76	4.29	0.00	0.33	0.13	0.07	0.13	5.94	0.20	0.13	0.26	0.33	0.07	0.00	0.00	0.00
				20:00	21:00	19.80	0.33	6.93	4.09	0.00	0.26	0.07	0.07	0.07	7.59	0.07	0.07	0.13	0.07	0.07	0.00	0.00	0.00
				21:00	22:00	19.80	0.33	6.40	4.88	0.00	0.40	0.07	0.07	0.07	7.52	0.00	0.00	0.07	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	16.50	0.33	4.36	4.36	0.00	0.13	0.00	0.00	0.00	6.93	0.07	0.07	0.13	0.07	0.07	0.00	0.00	0.00
23:00	0:00	13.20	0.26	3.30	3.43	0.00	0.20	0.00	0.00	0.00	5.61	0.07	0.07	0.13	0.07	0.07	0.00	0.00	0.00				
L098	Cheung Ning Street (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	7.80	1.09	4.99	1.09	0.00	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	7.80	0.94	4.99	0.94	0.00	0.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	7.80	1.09	5.62	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.80	1.09	4.52	1.09	0.00	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.80	1.56	4.68	1.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	7.80	1.25	4.06	1.25	0.00	1.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	7.80	0.78	4.68	1.56	0.00	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	7.80	0.47	5.46	0.62	0.00	1.09	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	15.60	1.56	10.14	0.62	0.00	2.34	0.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	15.60	1.40	9.52	0.78	0.00	2.81	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	15.60	1.40	7.33	0.94	0.00	4.68	1.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	7.80	0.47	3.74	0.31	0.00	2.96	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	15.60	1.25	9.20	0.94	0.00	3.90	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	7.80	0.47	3.59	0.31	0.00	2.65	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	7.80	0.31	3.43	0.62	0.00	2.65	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	15.60	1.09	7.49	0.94	0.00	4.68	1.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	7.80	0.62	4.37	0.31	0.00	2.18	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	15.60	1.56	9.83	0.78	0.00	2.65	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	15.60	1.56	10.30	0.94	0.00	2.18	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	7.80	0.62	6.24	0.47	0.00	0.31	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	7.80	0.94	5.77	0.62	0.00	0.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	7.80	0.62	5.62	0.94	0.00	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	7.80	1.09	5.30	1.09	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
23:00	0:00	7.80	1.25	4.84	1.25	0.00	0.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00			

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L099	Ha Heung Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	34.75	1.25	13.21	15.43	0.00	0.42	0.00	0.00	0.00	0.00	2.92	0.14	0.14	0.70	0.42	0.14	0.00	0.00
				1:00	2:00	20.85	0.83	9.04	10.84	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	13.90	0.42	5.56	7.78	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	13.90	0.56	5.98	7.23	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	13.90	0.42	5.42	7.51	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.28	0.00	0.00	0.00	0.00
				5:00	6:00	13.90	0.42	4.87	6.95	0.00	0.14	0.00	0.00	0.00	0.14	0.00	0.14	0.14	0.56	0.42	0.14	0.00	0.00
				6:00	7:00	27.80	0.70	8.34	11.82	0.00	0.56	0.14	0.00	0.14	2.64	0.28	0.56	1.25	0.83	0.56	0.00	0.00	
				7:00	8:00	62.55	1.25	25.85	19.04	0.00	0.97	0.28	0.56	0.42	7.51	0.83	0.83	2.22	1.95	0.83	0.00	0.00	
				8:00	9:00	97.30	3.06	52.54	19.74	0.00	2.50	1.11	2.50	2.50	5.84	0.97	0.97	2.50	2.09	0.97	0.00	0.00	
				9:00	10:00	83.40	2.22	38.64	19.32	0.00	2.36	1.11	2.50	2.64	5.00	1.53	1.25	2.92	2.64	1.25	0.00	0.00	
				10:00	11:00	76.45	2.36	31.97	18.77	0.14	4.45	1.67	3.34	3.48	4.59	0.56	0.83	1.81	1.67	0.83	0.00	0.00	
				11:00	12:00	69.50	1.53	28.50	14.87	0.14	5.14	0.56	1.39	1.11	7.65	1.81	0.97	2.50	2.36	0.97	0.00	0.00	
				12:00	13:00	76.45	1.81	38.23	18.90	0.14	3.48	0.56	1.25	1.25	5.84	1.95	0.42	1.11	1.11	0.42	0.00	0.00	
				13:00	14:00	76.45	1.39	26.83	13.21	0.14	4.45	1.95	4.17	4.45	5.84	0.56	2.09	4.87	4.59	1.95	0.00	0.00	
				14:00	15:00	69.50	1.25	23.35	17.24	0.14	4.03	1.53	3.06	3.20	5.98	0.97	1.39	3.20	2.78	1.39	0.00	0.00	
				15:00	16:00	83.40	1.95	32.53	20.43	0.14	4.45	1.81	4.03	4.03	5.28	1.67	1.11	2.64	2.36	0.97	0.00	0.00	
				16:00	17:00	76.45	1.95	34.75	15.57	0.14	3.89	0.83	1.39	1.39	5.84	0.28	1.67	3.61	3.75	1.39	0.00	0.00	
				17:00	18:00	97.30	2.92	51.01	19.18	0.14	3.06	1.39	2.92	2.92	5.98	1.11	1.11	2.36	2.09	1.11	0.00	0.00	
				18:00	19:00	83.40	2.64	46.29	17.79	0.14	2.22	0.70	1.53	1.81	2.22	0.70	1.11	2.64	2.64	0.97	0.00	0.00	
				19:00	20:00	76.45	1.95	44.20	19.46	0.00	0.56	0.28	0.28	0.42	5.84	0.56	0.42	1.11	0.97	0.42	0.00	0.00	
				20:00	21:00	62.55	1.67	30.58	20.02	0.00	0.42	0.14	0.28	0.42	7.92	0.14	0.14	0.42	0.28	0.14	0.00	0.00	
				21:00	22:00	55.60	1.25	24.88	20.85	0.00	0.56	0.14	0.28	0.42	6.95	0.00	0.00	0.14	0.00	0.14	0.00	0.00	
				22:00	23:00	48.65	1.53	18.21	20.02	0.00	0.28	0.00	0.00	0.00	6.95	0.28	0.28	0.56	0.28	0.28	0.00	0.00	
23:00	0:00	41.70	1.25	15.15	17.24	0.00	0.28	0.00	0.00	0.00	5.98	0.28	0.28	0.70	0.28	0.28	0.00	0.00					
L100	Kowloon City Road (SB)	Sheung Heung Street	Lok Shan road	0:00	1:00	6.00	0.00	3.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	6.00	0.00	3.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	6.00	0.00	3.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	6.00	0.00	3.96	2.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	6.00	0.00	3.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	6.00	0.00	3.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	6.00	0.00	2.40	3.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	6.00	0.00	2.76	1.80	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.36	0.00	0.36	0.00	0.00	
				8:00	9:00	6.00	0.00	3.84	1.08	0.12	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.12	0.00	0.12	0.00	0.00	
				9:00	10:00	6.00	0.00	3.36	1.20	0.24	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.24	0.00	0.00	
				10:00	11:00	6.00	0.00	2.88	1.20	0.24	0.96	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	
				11:00	12:00	6.00	0.00	2.64	0.96	0.48	1.20	0.24	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.24	0.00	0.00	
				12:00	13:00	6.00	0.00	3.36	1.32	0.24	0.84	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	6.00	0.00	2.28	0.84	0.24	1.08	0.84	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.48	0.00	0.00	
				14:00	15:00	6.00	0.00	2.16	1.20	0.36	0.96	0.60	0.00	0.00	0.00	0.00	0.00	0.36	0.00	0.36	0.00	0.00	
				15:00	16:00	6.00	0.00	2.40	1.20	0.24	0.96	0.72	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.24	0.00	0.00	
				16:00	17:00	6.00	0.00	2.76	0.96	0.36	1.20	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.36	0.00	0.00	
				17:00	18:00	6.00	0.00	3.36	0.96	0.24	0.72	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	
				18:00	19:00	6.00	0.00	3.72	1.08	0.24	0.48	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	0.00	
				19:00	20:00	6.00	0.00	4.44	1.32	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	6.00	0.00	4.08	1.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	6.00	0.00	3.72	2.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	6.00	0.00	3.24	2.76	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	6.00	0.00	3.36	2.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L101	Kowloon City Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	5.80	0.35	2.55	2.32	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.12	0.12	0.00	0.00
				1:00	2:00	5.80	0.46	2.67	2.44	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	5.80	0.23	2.55	2.78	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.80	0.35	2.67	2.44	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.80	0.35	2.32	2.78	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.80	0.35	2.09	1.97	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.35	0.35	0.35	0.00	0.00
				6:00	7:00	5.80	0.35	1.86	1.97	0.00	0.46	0.12	0.00	0.00	0.00	0.12	0.12	0.35	0.35	0.12	0.00	0.00
				7:00	8:00	11.60	0.23	5.22	2.78	0.12	1.16	0.23	0.00	0.00	0.00	0.23	0.23	0.58	0.58	0.23	0.00	0.00
				8:00	9:00	23.20	1.04	11.60	3.25	0.12	3.02	1.74	0.12	0.12	0.00	0.35	0.35	0.58	0.58	0.35	0.00	0.00
				9:00	10:00	17.40	0.70	7.42	2.67	0.12	2.44	1.51	0.12	0.12	0.00	0.35	0.23	0.70	0.70	0.35	0.00	0.00
				10:00	11:00	17.40	0.93	6.73	3.02	0.12	3.48	1.62	0.12	0.12	0.00	0.12	0.23	0.35	0.35	0.23	0.00	0.00
				11:00	12:00	17.40	0.46	6.61	2.55	0.23	4.52	0.70	0.00	0.00	0.00	0.46	0.23	0.70	0.70	0.23	0.00	0.00
				12:00	13:00	17.40	0.70	8.58	3.25	0.12	2.90	0.46	0.00	0.00	0.00	0.70	0.12	0.23	0.23	0.12	0.00	0.00
				13:00	14:00	17.40	0.46	5.68	2.09	0.12	3.48	1.74	0.12	0.23	0.00	0.12	0.58	1.04	1.16	0.58	0.00	0.00
				14:00	15:00	17.40	0.35	6.03	3.02	0.12	3.48	1.62	0.12	0.12	0.00	0.23	0.35	0.81	0.81	0.35	0.00	0.00
				15:00	16:00	23.20	0.93	8.47	4.06	0.12	4.29	2.20	0.12	0.12	0.00	0.58	0.46	0.70	0.70	0.46	0.00	0.00
				16:00	17:00	23.20	1.04	10.67	3.48	0.12	3.36	0.70	0.00	0.00	0.00	0.12	0.70	1.16	1.28	0.58	0.00	0.00
				17:00	18:00	29.00	1.62	15.78	4.41	0.12	2.55	1.51	0.12	0.12	0.00	0.46	0.46	0.70	0.70	0.46	0.00	0.00
				18:00	19:00	23.20	1.28	12.76	3.71	0.12	1.62	0.81	0.12	0.12	0.00	0.23	0.46	0.81	0.81	0.35	0.00	0.00
				19:00	20:00	17.40	0.81	11.25	3.71	0.00	0.58	0.23	0.00	0.00	0.00	0.12	0.12	0.23	0.23	0.12	0.00	0.00
				20:00	21:00	11.60	0.58	6.73	3.36	0.00	0.35	0.12	0.00	0.00	0.00	0.12	0.00	0.12	0.12	0.12	0.00	0.00
				21:00	22:00	11.60	0.46	6.38	4.06	0.00	0.58	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	11.60	0.70	5.57	4.41	0.00	0.35	0.00	0.00	0.00	0.00	0.12	0.12	0.12	0.12	0.12	0.00	0.00
L102	Pau Chung Street (SB)	Sheung Heung Street	Kowloon City Road	23:00	0:00	11.60	0.81	4.87	4.29	0.00	0.46	0.00	0.00	0.00	0.00	0.23	0.23	0.23	0.23	0.23	0.00	0.00
				0:00	1:00	7.65	0.61	4.59	2.14	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	7.65	0.46	4.13	2.60	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	7.65	0.61	3.67	2.75	0.00	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.65	0.77	3.98	2.14	0.00	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.65	0.92	4.13	2.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	7.65	0.77	3.83	2.30	0.00	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	7.65	0.46	3.52	2.45	0.00	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46	0.00	0.00
				7:00	8:00	7.65	0.31	4.44	1.53	0.15	0.92	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				8:00	9:00	15.30	1.07	9.33	1.68	0.15	1.99	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				9:00	10:00	15.30	0.92	8.72	1.99	0.15	2.30	1.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				10:00	11:00	15.30	0.92	7.65	1.99	0.15	3.06	1.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				11:00	12:00	15.30	0.77	7.50	1.84	0.46	3.98	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				12:00	13:00	15.30	0.77	9.18	2.14	0.15	2.45	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				13:00	14:00	15.30	0.61	7.04	1.53	0.15	3.67	1.68	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.61	0.00	0.00
				14:00	15:00	15.30	0.77	6.73	2.30	0.31	3.37	1.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46	0.00	0.00
				15:00	16:00	15.30	0.92	7.34	2.14	0.15	3.06	1.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.31	0.00	0.00
				16:00	17:00	15.30	1.07	9.03	1.84	0.15	2.30	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46	0.00	0.00
				17:00	18:00	22.95	1.68	15.15	2.60	0.15	1.99	1.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.31	0.00	0.00
				18:00	19:00	22.95	1.68	15.61	2.75	0.15	1.68	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.46	0.00	0.00
				19:00	20:00	15.30	0.92	11.32	2.30	0.00	0.46	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.15	0.00	0.00
				20:00	21:00	15.30	1.07	10.40	3.21	0.00	0.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	7.65	0.46	4.90	1.84	0.00	0.31	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	7.65	0.77	4.44	2.30	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	7.65	0.77	4.28	2.30	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																		
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD			
L103	Pau Chung Street (NB)	Kowloon City Road	Sheung Heung Street	0:00	1:00	16.50	0.33	6.93	8.58	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	8.25	0.17	3.30	4.62	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.25	0.33	3.14	4.46	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	8.25	0.33	3.47	4.13	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.25	0.00	3.14	4.79	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	8.25	0.00	3.14	4.46	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.33	0.00	0.00	0.00	
				6:00	7:00	8.25	0.17	2.81	4.46	0.00	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.33	0.00	0.00	0.00	
				7:00	8:00	24.75	0.33	12.05	9.41	0.17	1.16	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.33	0.00	0.99	0.00	0.00	0.00	
				8:00	9:00	41.25	0.99	24.26	9.57	0.33	2.64	1.49	0.33	0.33	0.00	0.00	0.00	0.33	0.00	0.99	0.00	0.00	0.00	0.00	
				9:00	10:00	33.00	0.66	17.16	8.91	0.33	2.31	1.32	0.33	0.33	0.00	0.00	0.00	0.50	0.00	1.16	0.00	0.00	0.00	0.00	
				10:00	11:00	33.00	0.66	14.19	8.91	0.66	4.62	2.15	0.50	0.50	0.00	0.00	0.00	0.17	0.00	0.66	0.00	0.00	0.00	0.00	
				11:00	12:00	24.75	0.33	11.22	6.11	0.50	4.62	0.66	0.17	0.17	0.00	0.00	0.00	0.33	0.00	0.66	0.00	0.00	0.00	0.00	
				12:00	13:00	33.00	0.66	17.49	9.08	0.50	3.63	0.66	0.17	0.17	0.00	0.00	0.00	0.17	0.00	0.50	0.00	0.00	0.00	0.00	
				13:00	14:00	24.75	0.33	9.90	4.95	0.50	3.80	2.15	0.50	0.50	0.00	0.00	0.00	0.66	0.00	1.49	0.00	0.00	0.00	0.00	
				14:00	15:00	24.75	0.33	9.57	6.93	0.50	3.63	1.65	0.33	0.33	0.00	0.00	0.00	0.50	0.00	0.99	0.00	0.00	0.00	0.00	
				15:00	16:00	33.00	0.66	13.37	9.24	0.66	4.29	2.15	0.66	0.66	0.00	0.00	0.00	0.33	0.00	0.99	0.00	0.00	0.00	0.00	
				16:00	17:00	24.75	0.50	12.05	5.78	0.33	3.47	0.83	0.17	0.17	0.00	0.00	0.00	0.50	0.00	0.99	0.00	0.00	0.00	0.00	
				17:00	18:00	33.00	0.83	18.32	7.43	0.33	2.81	1.49	0.33	0.33	0.00	0.00	0.00	0.33	0.00	0.83	0.00	0.00	0.00	0.00	
				18:00	19:00	33.00	0.83	19.31	7.92	0.17	2.15	0.99	0.17	0.17	0.00	0.00	0.00	0.50	0.00	0.83	0.00	0.00	0.00	0.00	
				19:00	20:00	33.00	0.66	20.96	9.90	0.00	0.66	0.17	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.50	0.00	0.00	0.00	0.00	
				20:00	21:00	24.75	0.50	13.86	9.74	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	
				21:00	22:00	24.75	0.50	12.54	10.89	0.00	0.66	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	16.50	0.33	7.26	8.58	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00
				L104	Ma Tau Wai Road (SB)	Sheung Heung Street	Lok Shan road	23:00	0:00	16.50	0.50	7.10	8.25	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00
0:00	1:00	39.60	1.58					9.50	18.71	0.00	0.99	0.00	0.00	0.00	0.00	4.95	0.20	0.10	0.40	0.20	0.10	0.00	2.87		
1:00	2:00	19.80	0.89					5.84	11.88	0.00	0.79	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.40		
2:00	3:00	19.80	0.79					5.25	12.77	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.50		
3:00	4:00	14.85	0.79					4.26	8.61	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.50		
4:00	5:00	14.85	0.69					3.86	9.21	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	0.00	0.50		
5:00	6:00	14.85	0.59					3.66	8.81	0.00	0.79	0.00	0.00	0.10	0.00	0.10	0.10	0.40	0.20	0.10	0.00	0.00	0.00		
6:00	7:00	34.65	0.89					6.14	14.85	0.20	1.39	0.40	0.00	0.10	4.75	0.40	0.40	0.79	0.79	0.40	0.00	0.00	3.17		
7:00	8:00	99.00	1.98					23.46	27.13	0.69	5.15	1.29	0.40	0.30	17.23	0.99	0.89	1.98	1.98	0.59	0.00	0.00	14.95		
8:00	9:00	138.60	5.45					47.92	28.12	1.29	12.08	6.63	1.78	1.78	13.46	1.09	1.09	2.08	2.08	0.69	0.00	0.00	13.07		
9:00	10:00	128.70	3.86					36.53	28.81	1.19	11.58	7.13	1.78	1.98	11.88	1.78	1.29	2.67	2.87	0.89	0.00	0.00	14.45		
10:00	11:00	99.00	3.27					24.26	23.86	1.49	14.16	6.44	1.68	1.68	8.32	0.50	0.59	1.29	1.39	0.40	0.00	0.00	9.70		
11:00	12:00	103.95	2.28					23.66	20.79	2.08	18.12	2.57	0.69	0.59	15.05	1.58	0.89	1.98	2.08	0.59	0.00	0.00	10.99		
12:00	13:00	99.00	2.77					30.59	25.34	1.39	11.68	2.28	0.69	0.59	10.99	1.78	0.40	0.79	0.89	0.30	0.00	0.00	8.51		
13:00	14:00	103.95	2.08					21.09	17.33	1.78	14.85	8.12	2.08	2.18	10.89	0.50	1.88	3.66	3.96	1.19	0.00	0.00	12.38		
14:00	15:00	99.00	1.78					19.40	23.46	1.58	13.56	6.04	1.58	1.58	11.48	0.79	1.09	2.38	2.57	0.79	0.00	0.00	10.89		
15:00	16:00	113.85	2.87					25.94	27.62	1.68	15.15	7.72	2.08	2.08	10.40	1.49	0.99	2.08	2.08	0.59	0.00	0.00	11.09		
16:00	17:00	84.15	2.57					23.96	19.80	1.09	8.91	1.98	0.50	0.40	9.01	0.20	1.09	2.38	2.57	0.69	0.00	0.00	9.01		
17:00	18:00	103.95	3.96					35.94	25.34	1.09	7.13	4.06	1.19	1.09	9.70	0.69	0.69	1.58	1.58	0.50	0.00	0.00	9.41		
18:00	19:00	84.15	3.37					32.87	23.27	0.59	4.85	2.28	0.59	0.59	3.76	0.50	0.89	1.68	1.98	0.50	0.00	0.00	6.44		
19:00	20:00	94.05	2.97					36.53	27.32	0.20	2.18	0.99	0.20	0.30	11.48	0.50	0.40	0.89	0.99	0.30	0.00	0.00	8.81		
20:00	21:00	84.15	2.57					25.44	28.41	0.20	1.58	0.30	0.20	0.20	15.64	0.20	0.10	0.30	0.20	0.10	0.00	0.00	8.71		
21:00	22:00	74.25	1.68					19.50	27.82	0.20	1.88	0.59	0.20	0.20	12.97	0.00	0.00	0.10	0.00	0.10	0.00	0.00	9.01		
22:00	23:00	74.25	2.28					15.15	28.02	0.20	1.09	0.00	0.00	0.00	13.66	0.30	0.20	0.50	0.20	0.20	0.00	0.00	12.47		
23:00	0:00	59.40	1.98	11.68	22.47	0.20	0.99	0.00	0.00	0.00	10.89	0.30	0.20	0.40	0.20	0.20	0.00	0.00	9.90						

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L105	Ma Tau Wai Road (NB)	Lok Shan road	Sheung Heung Street	0:00	1:00	30.30	2.12	5.05	13.74	0.00	0.40	0.00	0.00	0.00	5.56	0.10	0.10	0.40	0.20	0.10	0.00	2.53	
				1:00	2:00	15.15	1.21	3.23	9.39	0.00	0.30	0.00	0.00	0.00	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.51	
				2:00	3:00	15.15	1.11	2.93	9.90	0.00	0.20	0.00	0.00	0.00	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.51	
				3:00	4:00	10.10	1.01	2.12	6.16	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.51	
				4:00	5:00	10.10	0.91	2.02	6.26	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	0.51	
				5:00	6:00	10.10	0.81	1.92	6.26	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.10	0.10	0.30	0.20	0.10	0.00	0.00
				6:00	7:00	25.25	1.11	3.03	10.61	0.10	0.61	0.10	0.00	0.00	5.05	0.30	0.30	0.61	0.51	0.20	0.00	2.73	
				7:00	8:00	75.75	1.62	10.81	25.76	0.51	1.41	0.30	0.10	0.10	20.50	0.51	0.51	1.11	0.91	0.51	0.00	11.11	
				8:00	9:00	90.90	4.34	22.22	27.27	1.01	3.54	1.62	0.61	0.61	15.96	0.61	0.61	1.21	1.01	0.51	0.00	9.80	
				9:00	10:00	85.85	3.13	17.07	28.18	0.91	3.43	1.82	0.61	0.61	14.24	0.91	0.81	1.41	1.41	0.51	0.00	10.81	
				10:00	11:00	70.70	4.65	13.33	18.58	1.41	6.26	2.73	0.61	0.61	9.70	0.51	0.61	1.11	1.11	0.40	0.00	9.09	
				11:00	12:00	75.75	2.93	12.73	15.76	1.82	7.78	1.01	0.20	0.20	17.17	1.41	0.81	1.72	1.72	0.51	0.00	10.00	
				12:00	13:00	70.70	3.64	16.46	19.19	1.31	5.05	0.91	0.20	0.20	12.63	1.52	0.30	0.71	0.71	0.20	0.00	7.68	
				13:00	14:00	75.75	2.93	11.82	13.33	1.62	6.67	3.33	0.81	0.81	12.93	0.51	1.72	3.13	3.33	1.11	0.00	11.72	
				14:00	15:00	70.70	2.32	10.50	17.88	1.41	5.76	2.32	0.51	0.51	13.13	0.71	1.01	2.02	2.02	0.71	0.00	9.90	
				15:00	16:00	80.80	3.94	14.14	21.41	1.52	6.57	3.13	0.71	0.71	12.12	1.21	1.01	1.82	1.72	0.61	0.00	10.20	
				16:00	17:00	75.75	5.86	16.67	11.72	1.41	6.46	1.41	0.20	0.20	11.01	0.30	1.62	3.13	3.54	1.01	0.00	11.21	
				17:00	18:00	85.85	8.48	24.34	14.24	1.11	4.95	2.73	0.40	0.40	11.21	1.01	1.01	2.02	2.02	0.71	0.00	11.21	
				18:00	19:00	70.70	7.68	23.03	13.64	0.61	3.64	1.52	0.20	0.20	4.55	0.71	1.31	2.22	2.53	0.71	0.00	8.18	
				19:00	20:00	70.70	4.14	20.10	21.01	0.10	0.91	0.40	0.10	0.10	13.33	0.40	0.30	0.71	0.71	0.20	0.00	8.18	
				20:00	21:00	65.65	3.43	13.53	21.21	0.10	0.61	0.10	0.10	0.10	17.78	0.20	0.10	0.30	0.20	0.10	0.00	7.78	
				21:00	22:00	60.60	2.42	10.91	21.92	0.10	0.81	0.20	0.10	0.10	15.35	0.00	0.00	0.10	0.00	0.10	0.00	8.48	
				22:00	23:00	60.60	2.93	8.08	21.01	0.10	0.40	0.00	0.00	0.00	15.55	0.20	0.20	0.40	0.20	0.20	0.00	11.31	
				23:00	0:00	50.50	2.63	6.46	17.47	0.10	0.40	0.00	0.00	0.00	12.93	0.20	0.20	0.40	0.20	0.20	0.00	9.29	
L106	Maidstone Road (SB)	Maidstone Lane	Lok Shan road	0:00	1:00	5.20	0.73	2.60	1.04	0.00	0.10	0.00	0.00	0.00	0.52	0.00	0.00	0.10	0.00	0.10	0.00	0.00	
				1:00	2:00	5.20	0.83	2.81	1.35	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.20	0.73	2.81	1.46	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	5.20	0.94	2.70	1.25	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.20	0.73	3.02	1.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	5.20	0.62	2.39	1.56	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.31	0.00	0.00	
				6:00	7:00	5.20	0.62	2.18	1.14	0.00	0.31	0.00	0.00	0.00	0.73	0.00	0.00	0.10	0.00	0.10	0.00	0.00	
				7:00	8:00	15.60	1.14	8.32	2.39	0.00	0.62	0.10	0.00	0.00	2.18	0.00	0.00	0.42	0.00	0.42	0.00	0.00	
				8:00	9:00	26.00	3.12	15.91	2.39	0.00	1.35	0.62	0.10	0.10	1.56	0.00	0.00	0.42	0.00	0.42	0.00	0.00	
				9:00	10:00	20.80	2.18	11.86	2.39	0.00	1.14	0.62	0.10	0.10	1.35	0.00	0.00	0.52	0.00	0.52	0.00	0.00	
				10:00	11:00	15.60	1.77	7.70	1.77	0.00	1.98	0.73	0.10	0.10	1.04	0.00	0.00	0.21	0.00	0.21	0.00	0.00	
				11:00	12:00	15.60	1.25	7.49	1.46	0.00	2.60	0.21	0.00	0.00	2.08	0.00	0.00	0.31	0.00	0.21	0.00	0.00	
				12:00	13:00	15.60	1.35	9.15	1.77	0.00	1.56	0.21	0.00	0.00	1.35	0.00	0.00	0.10	0.00	0.10	0.00	0.00	
				13:00	14:00	15.60	1.14	6.97	1.25	0.00	2.18	0.94	0.10	0.21	1.56	0.00	0.00	0.62	0.00	0.62	0.00	0.00	
				14:00	15:00	15.60	1.14	6.76	1.87	0.00	2.18	0.83	0.10	0.10	1.77	0.00	0.00	0.42	0.00	0.42	0.00	0.00	
				15:00	16:00	15.60	1.46	7.38	1.77	0.00	1.98	0.83	0.10	0.10	1.35	0.00	0.00	0.31	0.00	0.31	0.00	0.00	
				16:00	17:00	15.60	1.56	7.80	1.25	0.00	2.08	0.42	0.00	0.00	1.66	0.00	0.00	0.42	0.00	0.42	0.00	0.00	
				17:00	18:00	15.60	1.66	8.94	1.25	0.00	1.25	0.52	0.10	0.10	1.35	0.00	0.00	0.21	0.00	0.21	0.00	0.00	
				18:00	19:00	15.60	1.77	9.78	1.35	0.00	1.04	0.42	0.10	0.10	0.62	0.00	0.00	0.21	0.00	0.21	0.00	0.00	
				19:00	20:00	15.60	1.56	10.19	1.77	0.00	0.31	0.10	0.00	0.00	1.46	0.00	0.00	0.10	0.00	0.10	0.00	0.00	
				20:00	21:00	15.60	1.56	8.94	2.29	0.00	0.31	0.00	0.00	0.00	2.39	0.00	0.00	0.00	0.00	0.10	0.00	0.00	
				21:00	22:00	10.40	0.94	5.72	1.87	0.00	0.21	0.10	0.00	0.00	1.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	10.40	1.25	4.89	2.08	0.00	0.10	0.00	0.00	0.00	1.87	0.00	0.00	0.10	0.00	0.10	0.00	0.00	
				23:00	0:00	10.40	1.25	4.47	2.18	0.00	0.21	0.00	0.00	0.00	1.87	0.00	0.00	0.21	0.00	0.21	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																		
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD			
L107	Kau Pui Lung Road (SB)	Maidstone Lane	Lok Shan road	0:00	1:00	16.00	0.40	5.04	9.44	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.32	0.24	0.08	0.00	0.00		
				1:00	2:00	8.00	0.24	2.56	4.96	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				2:00	3:00	8.00	0.16	2.32	5.36	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	8.00	0.24	2.56	4.96	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.00	0.32	2.40	5.12	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	8.00	0.24	2.00	4.40	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.16	0.16	0.24	0.24	0.16	0.00	0.00	0.00	
				6:00	7:00	12.00	0.32	2.88	6.56	0.00	0.48	0.08	0.00	0.00	0.00	0.00	0.16	0.32	0.56	0.48	0.16	0.00	0.00	0.00	
				7:00	8:00	28.00	0.48	9.84	11.44	0.08	1.68	0.48	0.00	0.00	0.00	0.00	0.56	0.56	1.20	1.20	0.48	0.00	0.00	0.00	
				8:00	9:00	40.00	1.20	17.92	10.96	0.16	3.60	1.92	0.16	0.16	0.00	0.56	0.56	1.20	1.20	0.40	0.00	0.00	0.00	0.00	
				9:00	10:00	36.00	0.80	13.60	10.80	0.16	3.28	1.92	0.16	0.16	0.00	0.88	0.72	1.44	1.52	0.56	0.00	0.00	0.00	0.00	
				10:00	11:00	32.00	0.88	10.96	10.08	0.16	4.72	2.08	0.16	0.16	0.00	0.32	0.40	0.88	0.88	0.32	0.00	0.00	0.00	0.00	
				11:00	12:00	32.00	0.64	10.56	8.80	0.24	6.00	0.88	0.08	0.08	0.00	1.04	0.64	1.28	1.36	0.40	0.00	0.00	0.00	0.00	
				12:00	13:00	32.00	0.64	13.52	10.48	0.16	3.84	0.64	0.08	0.08	0.00	1.12	0.24	0.48	0.56	0.16	0.00	0.00	0.00	0.00	
				13:00	14:00	32.00	0.48	9.12	7.20	0.16	4.80	2.56	0.16	0.16	0.00	0.32	1.20	2.40	2.64	0.80	0.00	0.00	0.00	0.00	
				14:00	15:00	32.00	0.48	8.96	10.24	0.16	4.64	2.00	0.16	0.16	0.00	0.48	0.80	1.68	1.76	0.48	0.00	0.00	0.00	0.00	
				15:00	16:00	36.00	0.72	11.28	11.60	0.16	4.88	2.40	0.16	0.16	0.00	0.88	0.64	1.36	1.36	0.40	0.00	0.00	0.00	0.00	
				16:00	17:00	32.00	0.80	12.48	8.80	0.08	3.44	0.72	0.00	0.00	0.00	0.16	0.88	1.92	2.16	0.56	0.00	0.00	0.00	0.00	
				17:00	18:00	40.00	1.28	18.56	11.36	0.08	2.80	1.52	0.08	0.08	0.00	0.64	0.64	1.28	1.28	0.40	0.00	0.00	0.00	0.00	
				18:00	19:00	36.00	1.12	17.12	10.48	0.08	1.84	0.80	0.08	0.08	0.00	0.40	0.64	1.44	1.52	0.40	0.00	0.00	0.00	0.00	
				19:00	20:00	32.00	0.80	16.56	11.68	0.00	0.64	0.32	0.00	0.00	0.00	0.32	0.24	0.64	0.64	0.16	0.00	0.00	0.00	0.00	
				20:00	21:00	24.00	0.64	10.80	11.44	0.00	0.48	0.08	0.00	0.00	0.00	0.08	0.08	0.16	0.16	0.08	0.00	0.00	0.00	0.00	
				21:00	22:00	24.00	0.48	9.60	12.96	0.00	0.64	0.16	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00	0.00	0.00	
				22:00	23:00	20.00	0.64	6.56	11.60	0.00	0.32	0.00	0.00	0.00	0.00	0.00	0.16	0.16	0.24	0.16	0.16	0.00	0.00	0.00	0.00
				L108	Kau Pui Lung Road (NB)	Lok Shan road	Maidstone Lane	23:00	0:00	16.00	0.48	5.04	9.28	0.00	0.32	0.00	0.00	0.00	0.00	0.16	0.16	0.24	0.16	0.16	0.00
0:00	1:00	12.00	0.24					2.32	8.08	0.00	0.32	0.00	0.00	0.00	0.56	0.08	0.00	0.24	0.08	0.08	0.00	0.00	0.00		
1:00	2:00	8.00	0.16					1.68	5.92	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
2:00	3:00	8.00	0.16					1.52	6.16	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
3:00	4:00	4.00	0.08					0.80	2.96	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
4:00	5:00	4.00	0.08					0.72	3.12	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
5:00	6:00	4.00	0.08					0.72	2.80	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.08	0.00	0.00	0.00		
6:00	7:00	8.00	0.08					1.20	5.20	0.00	0.32	0.08	0.00	0.00	0.40	0.08	0.08	0.24	0.24	0.08	0.00	0.00	0.00		
7:00	8:00	20.00	0.16					4.72	10.00	0.08	0.96	0.32	0.00	0.00	1.60	0.32	0.32	0.64	0.56	0.32	0.00	0.00	0.00		
8:00	9:00	28.00	0.56					9.68	10.56	0.08	2.24	1.20	0.08	0.08	1.20	0.32	0.32	0.72	0.64	0.32	0.00	0.00	0.00		
9:00	10:00	24.00	0.32					6.72	9.84	0.08	2.00	1.20	0.08	0.08	0.96	0.40	0.32	0.88	0.80	0.32	0.00	0.00	0.00		
10:00	11:00	24.00	0.40					5.52	9.36	0.08	4.24	1.92	0.08	0.08	0.80	0.16	0.16	0.56	0.48	0.16	0.00	0.00	0.00		
11:00	12:00	24.00	0.24					5.36	8.16	0.16	5.36	0.72	0.00	0.00	1.60	0.48	0.24	0.72	0.72	0.24	0.00	0.00	0.00		
12:00	13:00	24.00	0.32					6.96	9.92	0.08	3.52	0.64	0.00	0.00	1.12	0.56	0.08	0.32	0.40	0.08	0.00	0.00	0.00		
13:00	14:00	24.00	0.32					4.72	6.80	0.08	4.40	2.32	0.08	0.16	1.12	0.16	0.56	1.36	1.36	0.56	0.00	0.00	0.00		
14:00	15:00	24.00	0.16					4.40	9.12	0.08	4.08	1.76	0.08	0.08	1.20	0.32	0.40	0.96	0.96	0.40	0.00	0.00	0.00		
15:00	16:00	24.00	0.32					5.04	9.36	0.08	3.92	1.92	0.08	0.08	0.96	0.40	0.24	0.72	0.64	0.24	0.00	0.00	0.00		
16:00	17:00	24.00	0.40					6.16	8.00	0.08	4.32	0.96	0.00	0.00	1.04	0.08	0.48	1.04	1.04	0.40	0.00	0.00	0.00		
17:00	18:00	28.00	0.56					8.96	9.84	0.08	3.36	1.84	0.08	0.08	1.12	0.24	0.24	0.72	0.64	0.24	0.00	0.00	0.00		
18:00	19:00	24.00	0.40					8.16	9.12	0.08	2.32	1.04	0.08	0.08	0.40	0.16	0.32	0.80	0.80	0.24	0.00	0.00	0.00		
19:00	20:00	20.00	0.32					7.36	9.52	0.00	0.56	0.24	0.00	0.00	1.04	0.16	0.08	0.32	0.32	0.08	0.00	0.00	0.00		
20:00	21:00	20.00	0.32					5.84	11.28	0.00	0.48	0.08	0.00	0.00	1.68	0.08	0.00	0.08	0.08	0.08	0.00	0.00	0.00		
21:00	22:00	16.00	0.16					4.00	9.92	0.00	0.48	0.16	0.00	0.00	1.20	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00		
22:00	23:00	16.00	0.24					3.20	10.48	0.00	0.24	0.00	0.00	0.00	1.36	0.08	0.08	0.16	0.08	0.08	0.00	0.00	0.00		
23:00	0:00	12.00	0.24	2.40	7.68	0.00	0.24	0.00	0.00	0.00	0.96	0.08	0.08	0.16	0.08	0.08	0.00	0.00	0.00						

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L109	Lok Shan road (EB)	Kau Pui Lung Road	Maidstone Road	0:00	1:00	4.40	0.13	1.45	2.51	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.04	0.04	0.00	0.00
				1:00	2:00	2.20	0.04	0.79	1.32	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.20	0.04	0.66	1.45	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.20	0.09	0.75	1.28	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.20	0.09	0.66	1.36	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.20	0.09	0.62	1.14	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.00	0.00
				6:00	7:00	4.40	0.09	1.14	2.29	0.00	0.18	0.09	0.00	0.00	0.00	0.09	0.09	0.18	0.18	0.09	0.00	0.00	
				7:00	8:00	6.60	0.09	2.42	2.68	0.04	0.31	0.09	0.04	0.04	0.00	0.13	0.13	0.22	0.26	0.13	0.00	0.00	
				8:00	9:00	11.00	0.31	5.15	2.86	0.04	0.84	0.44	0.18	0.18	0.00	0.13	0.13	0.31	0.31	0.13	0.00	0.00	
				9:00	10:00	11.00	0.22	4.22	3.12	0.04	0.88	0.48	0.22	0.22	0.00	0.26	0.22	0.44	0.44	0.22	0.00	0.00	
				10:00	11:00	8.80	0.18	3.21	2.55	0.04	1.19	0.53	0.22	0.22	0.00	0.09	0.09	0.18	0.22	0.09	0.00	0.00	
				11:00	12:00	8.80	0.13	3.12	2.29	0.09	1.58	0.22	0.09	0.09	0.00	0.26	0.13	0.31	0.35	0.13	0.00	0.00	
				12:00	13:00	8.80	0.13	3.92	2.73	0.04	0.97	0.18	0.09	0.09	0.00	0.31	0.04	0.13	0.13	0.04	0.00	0.00	
				13:00	14:00	8.80	0.13	2.51	1.76	0.04	1.23	0.62	0.31	0.31	0.00	0.09	0.31	0.57	0.62	0.31	0.00	0.00	
				14:00	15:00	8.80	0.09	2.51	2.55	0.04	1.23	0.53	0.22	0.22	0.00	0.18	0.22	0.40	0.40	0.22	0.00	0.00	
				15:00	16:00	11.00	0.22	3.43	3.21	0.04	1.41	0.66	0.31	0.31	0.00	0.26	0.18	0.40	0.40	0.18	0.00	0.00	
				16:00	17:00	8.80	0.22	3.39	2.20	0.04	1.01	0.22	0.09	0.09	0.00	0.04	0.26	0.44	0.57	0.22	0.00	0.00	
				17:00	18:00	11.00	0.31	5.06	2.82	0.04	0.79	0.40	0.22	0.22	0.00	0.18	0.18	0.31	0.31	0.18	0.00	0.00	
				18:00	19:00	8.80	0.22	4.27	2.33	0.04	0.48	0.22	0.09	0.09	0.00	0.09	0.18	0.31	0.35	0.13	0.00	0.00	
				19:00	20:00	8.80	0.22	4.75	3.04	0.00	0.22	0.09	0.00	0.04	0.00	0.09	0.04	0.13	0.13	0.04	0.00	0.00	
				20:00	21:00	6.60	0.13	3.17	2.90	0.00	0.13	0.04	0.00	0.04	0.00	0.04	0.00	0.04	0.04	0.04	0.00	0.00	
				21:00	22:00	6.60	0.13	2.77	3.43	0.00	0.18	0.04	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	6.60	0.18	2.29	3.78	0.00	0.13	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.04	0.00	0.00	
23:00	0:00	4.40	0.13	1.45	2.51	0.00	0.09	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.04	0.04	0.00	0.00					
L110	Lok Shan road (EB)	Maidstone Road	Ma Tau Wai Road	0:00	1:00	4.40	0.62	1.58	2.02	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
				1:00	2:00	2.20	0.31	0.75	1.10	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.20	0.26	0.70	1.19	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.20	0.35	0.75	1.01	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.20	0.26	0.70	1.14	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.20	0.22	0.70	1.10	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	
				6:00	7:00	4.40	0.48	1.28	2.11	0.00	0.26	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00	
				7:00	8:00	6.60	0.57	2.77	2.38	0.00	0.44	0.09	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.26	0.00	0.00	
				8:00	9:00	11.00	1.41	5.28	2.29	0.00	1.01	0.53	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.22	0.00	0.00
				9:00	10:00	11.00	1.14	4.62	2.68	0.00	1.14	0.66	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.40	0.00	0.00	
				10:00	11:00	8.80	1.06	3.21	2.11	0.00	1.41	0.57	0.13	0.13	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00	
				11:00	12:00	8.80	0.75	3.43	2.07	0.00	1.94	0.26	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.26	0.00	0.00	
				12:00	13:00	8.80	0.84	4.14	2.29	0.00	1.19	0.18	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	
				13:00	14:00	8.80	0.75	2.99	1.72	0.00	1.63	0.79	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.48	0.00	0.00	
				14:00	15:00	8.80	0.57	2.95	2.42	0.00	1.54	0.66	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.31	0.00	0.00	
				15:00	16:00	11.00	1.01	3.92	2.86	0.00	1.72	0.79	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.26	0.00	0.00	
				16:00	17:00	8.80	1.06	3.92	2.02	0.00	1.14	0.22	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.35	0.00	0.00	
				17:00	18:00	13.20	1.72	6.64	2.86	0.00	1.01	0.53	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.26	0.00	0.00	
				18:00	19:00	11.00	1.45	5.68	2.55	0.00	0.66	0.31	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.26	0.00	0.00	
				19:00	20:00	8.80	0.97	4.97	2.46	0.00	0.22	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	
				20:00	21:00	6.60	0.79	3.17	2.42	0.00	0.13	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
				21:00	22:00	6.60	0.57	2.90	2.82	0.00	0.22	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
				22:00	23:00	6.60	0.88	2.42	3.04	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.13	0.00	0.00	
23:00	0:00	4.40	0.62	1.54	2.07	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L111	Lok Shan road (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	4.50	0.14	1.80	2.07	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.09	0.09	0.09	0.00	0.00
				1:00	2:00	2.25	0.14	0.90	1.08	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.25	0.05	0.86	1.31	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.25	0.09	0.86	1.13	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.25	0.09	0.81	1.26	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.25	0.09	0.72	0.99	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.00	0.00
				6:00	7:00	4.50	0.18	1.35	1.94	0.00	0.32	0.09	0.00	0.00	0.00	0.00	0.09	0.09	0.18	0.18	0.09	0.00	0.00
				7:00	8:00	6.75	0.14	2.97	2.21	0.05	0.50	0.09	0.00	0.00	0.00	0.00	0.14	0.14	0.23	0.18	0.14	0.00	0.00
				8:00	9:00	11.25	0.41	5.85	2.30	0.05	1.08	0.54	0.09	0.09	0.00	0.14	0.14	0.27	0.18	0.14	0.00	0.00	0.00
				9:00	10:00	11.25	0.32	4.95	2.57	0.05	1.13	0.68	0.09	0.09	0.00	0.27	0.23	0.41	0.27	0.23	0.00	0.00	0.00
				10:00	11:00	9.00	0.27	3.29	1.98	0.05	1.80	0.77	0.14	0.14	0.00	0.09	0.09	0.18	0.14	0.09	0.00	0.00	0.00
				11:00	12:00	9.00	0.18	3.33	1.76	0.09	2.30	0.27	0.05	0.05	0.00	0.23	0.14	0.27	0.23	0.14	0.00	0.00	0.00
				12:00	13:00	9.00	0.23	4.19	2.16	0.05	1.53	0.23	0.05	0.05	0.00	0.27	0.05	0.09	0.09	0.05	0.00	0.00	0.00
				13:00	14:00	9.00	0.18	2.79	1.40	0.05	1.80	0.90	0.18	0.18	0.00	0.09	0.27	0.50	0.41	0.27	0.00	0.00	0.00
				14:00	15:00	9.00	0.14	2.79	2.03	0.05	1.80	0.77	0.14	0.14	0.00	0.14	0.18	0.36	0.32	0.18	0.00	0.00	0.00
				15:00	16:00	11.25	0.27	3.74	2.52	0.05	2.07	0.95	0.23	0.23	0.00	0.27	0.18	0.32	0.27	0.18	0.00	0.00	0.00
				16:00	17:00	9.00	0.27	3.51	1.71	0.05	1.76	0.36	0.05	0.05	0.00	0.05	0.27	0.41	0.36	0.18	0.00	0.00	0.00
				17:00	18:00	11.25	0.41	5.40	2.12	0.05	1.40	0.72	0.14	0.14	0.00	0.14	0.14	0.27	0.23	0.14	0.00	0.00	0.00
				18:00	19:00	11.25	0.41	5.49	2.30	0.05	1.13	0.45	0.14	0.14	0.00	0.14	0.23	0.36	0.27	0.18	0.00	0.00	0.00
				19:00	20:00	9.00	0.32	5.40	2.48	0.00	0.32	0.09	0.00	0.00	0.00	0.09	0.05	0.14	0.09	0.05	0.00	0.00	0.00
				20:00	21:00	6.75	0.27	3.56	2.48	0.00	0.23	0.05	0.00	0.00	0.00	0.05	0.00	0.05	0.05	0.05	0.00	0.00	0.00
				21:00	22:00	6.75	0.18	3.24	2.88	0.00	0.32	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	4.50	0.18	1.85	2.12	0.00	0.14	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.05	0.05	0.00	0.00	0.00
				23:00	0:00	4.50	0.23	1.76	2.12	0.00	0.18	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.05	0.05	0.00	0.00	0.00
L112	Lok Shan road (WB)	Cheung Ning Street	Ha Heung Road	0:00	1:00	8.00	0.12	2.76	3.76	0.00	0.12	0.00	0.00	0.00	0.80	0.04	0.04	0.20	0.08	0.08	0.00	0.00	
				1:00	2:00	4.00	0.08	1.60	2.24	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.00	0.04	1.48	2.40	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.00	0.08	1.60	2.20	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.00	0.08	1.44	2.32	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	
				5:00	6:00	4.00	0.08	1.24	1.96	0.00	0.12	0.00	0.00	0.00	0.00	0.08	0.08	0.20	0.12	0.12	0.00	0.00	
				6:00	7:00	6.00	0.08	1.56	2.64	0.00	0.20	0.04	0.00	0.00	0.64	0.12	0.12	0.28	0.20	0.12	0.00	0.00	
				7:00	8:00	16.00	0.16	5.92	5.00	0.04	0.48	0.12	0.04	0.04	2.12	0.28	0.28	0.64	0.60	0.28	0.00	0.00	
				8:00	9:00	24.00	0.40	12.12	5.20	0.04	1.24	0.64	0.20	0.20	1.64	0.32	0.32	0.72	0.64	0.32	0.00	0.00	
				9:00	10:00	22.00	0.28	9.36	5.40	0.04	1.16	0.72	0.20	0.20	1.48	0.52	0.40	0.96	0.88	0.40	0.00	0.00	
				10:00	11:00	18.00	0.28	6.80	4.52	0.08	2.08	0.92	0.24	0.28	1.24	0.16	0.20	0.52	0.44	0.24	0.00	0.00	
				11:00	12:00	18.00	0.16	6.36	3.72	0.12	2.52	0.32	0.12	0.08	2.16	0.52	0.28	0.72	0.64	0.28	0.00	0.00	
				12:00	13:00	18.00	0.24	8.16	4.52	0.08	1.60	0.28	0.12	0.08	1.56	0.56	0.12	0.28	0.28	0.12	0.00	0.00	
				13:00	14:00	18.00	0.16	5.56	3.08	0.08	2.00	1.04	0.32	0.32	1.52	0.16	0.60	1.32	1.28	0.56	0.00	0.00	
				14:00	15:00	18.00	0.12	5.52	4.44	0.08	2.00	0.88	0.24	0.24	1.72	0.24	0.36	0.92	0.88	0.36	0.00	0.00	
				15:00	16:00	20.00	0.24	6.84	5.00	0.08	2.12	1.04	0.32	0.32	1.48	0.48	0.32	0.80	0.64	0.32	0.00	0.00	
				16:00	17:00	18.00	0.24	7.04	3.60	0.08	1.96	0.40	0.12	0.12	1.60	0.08	0.44	1.00	0.96	0.36	0.00	0.00	
				17:00	18:00	22.00	0.36	10.56	4.52	0.08	1.48	0.80	0.24	0.24	1.68	0.28	0.28	0.68	0.52	0.28	0.00	0.00	
				18:00	19:00	20.00	0.36	10.16	4.48	0.04	1.12	0.48	0.12	0.20	0.68	0.24	0.36	0.76	0.68	0.32	0.00	0.00	
				19:00	20:00	20.00	0.28	10.64	5.48	0.00	0.32	0.16	0.04	0.04	1.84	0.20	0.16	0.36	0.32	0.16	0.00	0.00	
				20:00	21:00	16.00	0.24	7.16	5.44	0.00	0.24	0.04	0.04	0.04	2.40	0.08	0.04	0.12	0.08	0.08	0.00	0.00	
				21:00	22:00	14.00	0.16	5.76	5.52	0.00	0.28	0.08	0.04	0.04	2.04	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				22:00	23:00	12.00	0.16	4.08	5.16	0.00	0.12	0.00	0.00	0.00	2.00	0.08	0.08	0.16	0.08	0.08	0.00	0.00	
				23:00	0:00	10.00	0.16	3.28	4.28	0.00	0.16	0.00	0.00	0.00	1.64	0.08	0.08	0.16	0.08	0.08	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L113	Lok Shan road (WB)	To Kwa Wan Road	Cheung Ning Street	0:00	1:00	4.60	0.18	1.79	1.91	0.00	0.09	0.00	0.00	0.00	0.41	0.02	0.02	0.09	0.05	0.02	0.00	0.00	
				1:00	2:00	2.30	0.09	1.04	1.10	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.30	0.07	0.94	1.24	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.30	0.12	1.01	1.06	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.30	0.09	0.92	1.15	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00	0.00	0.00
				5:00	6:00	2.30	0.07	0.83	1.08	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.02	0.02	0.09	0.07	0.02	0.00	0.00
				6:00	7:00	3.45	0.09	1.06	1.38	0.00	0.14	0.02	0.00	0.00	0.35	0.05	0.05	0.14	0.12	0.07	0.00	0.00	
				7:00	8:00	10.35	0.21	4.35	2.88	0.02	0.39	0.09	0.02	0.02	1.27	0.14	0.14	0.37	0.32	0.14	0.00	0.00	
				8:00	9:00	16.10	0.55	8.69	2.92	0.02	0.94	0.48	0.14	0.14	0.99	0.16	0.16	0.39	0.35	0.16	0.00	0.00	
				9:00	10:00	13.80	0.37	6.42	2.92	0.02	0.87	0.51	0.14	0.14	0.85	0.25	0.21	0.46	0.44	0.21	0.00	0.00	
				10:00	11:00	11.50	0.39	4.60	2.42	0.05	1.52	0.64	0.18	0.18	0.71	0.09	0.12	0.25	0.23	0.12	0.00	0.00	
				11:00	12:00	11.50	0.23	4.39	2.05	0.07	1.86	0.23	0.07	0.07	1.24	0.25	0.14	0.39	0.37	0.14	0.00	0.00	
				12:00	13:00	12.65	0.32	6.14	2.69	0.05	1.29	0.23	0.07	0.07	0.99	0.30	0.07	0.18	0.18	0.07	0.00	0.00	
				13:00	14:00	11.50	0.23	3.82	1.70	0.05	1.52	0.76	0.23	0.25	0.90	0.07	0.32	0.69	0.67	0.30	0.00	0.00	
				14:00	15:00	11.50	0.21	3.75	2.46	0.05	1.50	0.64	0.18	0.18	1.01	0.14	0.21	0.51	0.46	0.21	0.00	0.00	
				15:00	16:00	12.65	0.30	4.81	2.71	0.05	1.54	0.71	0.21	0.21	0.83	0.23	0.16	0.39	0.35	0.16	0.00	0.00	
				16:00	17:00	11.50	0.32	4.88	1.98	0.05	1.40	0.30	0.07	0.07	0.94	0.05	0.23	0.51	0.51	0.21	0.00	0.00	
				17:00	18:00	14.95	0.48	7.61	2.55	0.05	1.15	0.60	0.18	0.18	1.01	0.16	0.16	0.35	0.30	0.16	0.00	0.00	
				18:00	19:00	12.65	0.44	6.92	2.37	0.02	0.78	0.35	0.09	0.09	0.39	0.09	0.18	0.39	0.37	0.16	0.00	0.00	
				19:00	20:00	12.65	0.35	7.36	2.92	0.00	0.25	0.09	0.02	0.02	1.04	0.09	0.07	0.18	0.18	0.07	0.00	0.00	
				20:00	21:00	9.20	0.28	4.58	2.69	0.00	0.16	0.02	0.02	0.02	1.24	0.02	0.02	0.07	0.05	0.02	0.00	0.00	
				21:00	22:00	8.05	0.18	3.70	2.74	0.00	0.18	0.07	0.02	0.02	1.08	0.00	0.00	0.02	0.00	0.02	0.00	0.00	
				22:00	23:00	8.05	0.28	3.11	3.04	0.00	0.12	0.00	0.00	0.00	1.24	0.05	0.02	0.12	0.05	0.05	0.00	0.00	
L114	Ha Heung Road (NB)	Chi Kiang Street	Lok Shan road	23:00	0:00	5.75	0.18	2.14	2.16	0.00	0.12	0.00	0.00	0.00	0.90	0.05	0.02	0.09	0.05	0.05	0.00	0.00	
				0:00	1:00	15.70	0.79	6.75	7.22	0.00	0.31	0.00	0.00	0.00	0.00	0.16	0.00	0.16	0.16	0.00	0.00		
				1:00	2:00	15.70	0.94	6.91	7.22	0.00	0.63	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	7.85	0.31	3.30	4.08	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	7.85	0.47	3.45	3.45	0.00	0.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	7.85	0.47	3.14	3.93	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	7.85	0.47	2.83	3.45	0.00	0.47	0.00	0.00	0.16	0.00	0.00	0.00	0.16	0.16	0.16	0.00	0.00	
				6:00	7:00	15.70	0.63	5.50	7.22	0.00	0.63	0.16	0.00	0.16	0.00	0.16	0.16	0.47	0.47	0.16	0.00	0.00	
				7:00	8:00	31.40	0.94	15.07	9.89	0.16	1.41	0.31	0.31	0.31	0.00	0.47	0.47	0.79	0.79	0.47	0.00	0.00	
				8:00	9:00	54.95	2.67	30.14	10.05	0.16	3.45	1.88	1.73	1.73	0.00	0.47	0.47	0.94	0.79	0.47	0.00	0.00	
				9:00	10:00	54.95	2.20	25.59	11.62	0.16	3.77	2.36	2.04	2.04	0.00	0.94	0.79	1.41	1.26	0.79	0.00	0.00	
				10:00	11:00	47.10	2.04	19.00	9.89	0.16	6.12	2.67	2.36	2.36	0.00	0.31	0.31	0.79	0.79	0.31	0.00	0.00	
				11:00	12:00	39.25	1.26	16.80	7.85	0.31	6.91	0.94	0.79	0.79	0.00	0.79	0.47	0.94	0.94	0.47	0.00	0.00	
				12:00	13:00	47.10	1.73	24.34	10.68	0.16	5.02	0.94	0.94	0.94	0.00	1.10	0.16	0.47	0.47	0.16	0.00	0.00	
				13:00	14:00	47.10	1.26	16.33	7.22	0.16	6.28	3.14	2.83	3.14	0.00	0.31	1.10	2.20	2.04	1.10	0.00	0.00	
				14:00	15:00	39.25	0.94	13.82	8.79	0.16	5.34	2.20	1.88	2.04	0.00	0.47	0.63	1.26	1.10	0.63	0.00	0.00	
				15:00	16:00	54.95	2.04	20.41	11.93	0.16	6.59	3.30	2.98	2.98	0.00	0.94	0.63	1.26	1.10	0.63	0.00	0.00	
				16:00	17:00	47.10	2.04	21.82	8.95	0.16	5.65	1.26	0.94	0.94	0.00	0.16	0.94	1.73	1.73	0.79	0.00	0.00	
				17:00	18:00	54.95	2.67	29.36	9.89	0.16	3.93	2.04	1.88	1.88	0.00	0.47	0.47	0.94	0.79	0.47	0.00	0.00	
				18:00	19:00	54.95	2.67	30.46	10.68	0.16	3.14	1.26	1.26	1.26	0.00	0.31	0.79	1.26	1.10	0.63	0.00	0.00	
				19:00	20:00	47.10	2.04	29.67	11.78	0.00	0.94	0.31	0.16	0.31	0.00	0.31	0.16	0.63	0.63	0.16	0.00	0.00	
				20:00	21:00	31.40	1.41	17.74	10.36	0.00	0.63	0.16	0.16	0.31	0.00	0.16	0.00	0.16	0.16	0.16	0.00	0.00	
				21:00	22:00	31.40	1.10	16.49	12.25	0.00	0.94	0.16	0.16	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	23.55	1.26	10.68	10.52	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.16	0.16	0.16	0.16	0.00	0.00					
23:00	0:00	23.55	1.41	10.21	10.52	0.00	0.63	0.00	0.00	0.00	0.00	0.00	0.16	0.16	0.16	0.16	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

YEAR 2014 MONTHLY FORECAST																							
Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L115	Sze Chuen Street (EB)	Kowloon City Road	Ha Heung Road	0:00	1:00	2.20	0.00	1.58	0.31	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.20	0.00	1.67	0.26	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.20	0.00	1.58	0.31	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.20	0.00	1.50	0.35	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.20	0.00	1.32	0.44	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.20	0.00	1.50	0.35	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.20	0.00	1.28	0.35	0.00	0.57	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.20	0.00	1.50	0.26	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.40	0.00	3.12	0.26	0.00	1.01	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.40	0.00	2.90	0.31	0.00	1.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	4.40	0.00	2.16	0.22	0.00	2.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	4.40	0.00	1.94	0.22	0.00	2.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	4.40	0.00	2.60	0.22	0.00	1.58	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	4.40	0.00	2.02	0.18	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	4.40	0.00	1.98	0.26	0.00	2.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	4.40	0.00	2.20	0.26	0.00	1.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	4.40	0.00	2.46	0.13	0.00	1.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.40	0.00	3.04	0.18	0.00	1.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.40	0.00	3.21	0.22	0.00	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	4.40	0.00	3.70	0.35	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.20	0.00	1.76	0.26	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.20	0.00	1.67	0.31	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.20	0.00	1.63	0.40	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.20	0.00	1.63	0.35	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L116	Sze Chuen Street (WB)	Ha Heung Road	Kowloon City Road	0:00	1:00	2.20	0.00	2.02	0.00	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.20	0.00	1.94	0.00	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.20	0.00	1.85	0.00	0.00	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.20	0.00	2.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.20	0.00	1.76	0.00	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.20	0.00	1.67	0.00	0.00	0.26	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.20	0.00	1.85	0.00	0.00	0.22	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	4.40	0.00	3.43	0.00	0.00	0.48	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	4.40	0.00	3.17	0.00	0.00	0.62	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	4.40	0.00	2.46	0.00	0.00	1.06	0.88	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.20	0.00	1.32	0.00	0.00	0.70	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	4.40	0.00	3.21	0.00	0.00	0.84	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	4.40	0.00	2.20	0.00	0.00	1.10	1.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.20	0.00	1.14	0.00	0.00	0.57	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	4.40	0.00	2.51	0.00	0.00	0.97	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.20	0.00	1.50	0.00	0.00	0.48	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	4.40	0.00	3.26	0.00	0.00	0.57	0.57	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	4.40	0.00	3.56	0.00	0.00	0.44	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	4.40	0.00	4.05	0.00	0.00	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.20	0.00	2.02	0.00	0.00	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.20	0.00	1.94	0.00	0.00	0.18	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	2.20	0.00	2.07	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	2.20	0.00	2.07	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L117	Kowloon City Road (WB)	Ha Heung Road	Lok Shan road	0:00	1:00	14.80	1.18	8.58	3.85	0.00	1.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	14.80	0.00	8.58	4.14	0.00	2.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	14.80	0.00	9.77	5.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	14.80	0.00	8.88	2.96	0.00	2.96	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	14.80	0.00	9.77	5.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	14.80	0.00	7.10	3.85	0.00	3.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	14.80	0.00	4.74	4.14	0.00	1.48	0.00	0.00	0.00	0.00	0.00	0.00	1.48	1.48	1.48	0.00	0.00	
				7:00	8:00	14.80	0.30	7.10	2.07	0.00	1.78	0.30	0.00	0.00	0.00	0.30	0.30	0.89	0.89	0.89	0.00	0.00	
				8:00	9:00	29.60	0.89	14.50	2.37	0.00	4.44	2.37	0.59	0.59	0.00	0.59	0.59	0.89	0.89	0.89	0.00	0.00	
				9:00	10:00	29.60	0.59	12.73	2.37	0.00	4.44	2.37	0.59	0.59	0.00	0.89	0.59	1.48	1.48	1.48	0.00	0.00	
				10:00	11:00	29.60	0.59	9.18	2.66	0.00	8.58	3.85	0.59	0.59	0.00	0.59	0.59	0.59	1.18	1.18	0.59	0.00	0.00
				11:00	12:00	29.60	0.59	9.77	2.07	0.00	10.06	1.78	0.00	0.00	0.00	1.18	0.59	1.18	1.18	1.18	0.00	0.00	
				12:00	13:00	14.80	0.30	7.10	1.48	0.00	3.85	0.59	0.00	0.00	0.00	0.59	0.00	0.30	0.30	0.30	0.00	0.00	
				13:00	14:00	29.60	0.59	6.81	1.48	0.00	7.70	3.85	0.59	0.89	0.00	0.59	0.89	2.07	2.07	2.07	0.00	0.00	
				14:00	15:00	29.60	0.59	7.99	2.07	0.00	7.99	3.26	0.59	0.59	0.00	0.59	0.59	1.78	1.78	1.78	0.00	0.00	
				15:00	16:00	29.60	0.59	9.77	2.37	0.00	7.70	3.85	0.59	0.59	0.00	0.89	0.59	0.89	0.89	0.89	0.00	0.00	
				16:00	17:00	29.60	0.59	10.66	2.07	0.00	7.70	1.78	0.00	0.00	0.00	0.00	1.18	1.78	2.07	1.78	0.00	0.00	
				17:00	18:00	29.60	0.89	13.62	2.37	0.00	5.03	2.66	0.59	0.59	0.00	0.59	0.59	0.89	0.89	0.89	0.00	0.00	
				18:00	19:00	29.60	0.89	14.80	2.66	0.00	4.14	1.48	0.59	0.59	0.00	0.59	0.59	0.89	1.48	0.89	0.00	0.00	
				19:00	20:00	14.80	0.30	10.66	1.78	0.00	0.59	0.30	0.00	0.00	0.00	0.30	0.00	0.30	0.30	0.30	0.00	0.00	
				20:00	21:00	14.80	0.59	10.36	2.66	0.00	1.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	14.80	0.59	9.47	2.96	0.00	1.18	0.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	14.80	0.89	8.29	3.85	0.00	0.89	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.89	0.00	0.00	
				L118	Ma Tau Wai Road (SB)	Lok Shan road	Chi Kiang Street	23:00	0:00	14.80	0.89	8.29	3.85	0.00	0.89	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.89
0:00	1:00	111.60	5.95					28.52	55.06	0.00	2.48	0.00	0.00	0.00	8.18	0.50	0.50	1.24	0.74	0.50	0.00	7.94	
1:00	2:00	62.00	3.72					18.35	36.70	0.00	1.98	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.24	
2:00	3:00	62.00	2.98					16.86	39.43	0.00	1.49	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.24	
3:00	4:00	49.60	3.22					14.38	28.52	0.00	2.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.24	
4:00	5:00	37.20	2.23					9.92	22.82	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00	0.00	0.99	
5:00	6:00	49.60	2.48					12.15	28.27	0.00	2.48	0.00	0.00	0.25	0.00	0.25	0.74	1.24	0.99	0.74	0.00	0.00	
6:00	7:00	99.20	3.47					18.85	46.13	0.50	3.97	0.99	0.00	0.25	8.18	0.99	0.99	2.48	2.23	0.74	0.00	9.42	
7:00	8:00	272.80	7.44					71.18	78.62	1.98	13.64	3.72	1.49	1.49	30.50	2.98	2.73	5.70	5.70	1.98	0.00	43.65	
8:00	9:00	396.80	20.58					144.09	81.34	3.22	32.98	18.35	6.45	6.45	23.31	3.22	3.22	6.45	6.45	2.48	0.00	38.19	
9:00	10:00	359.60	14.38					108.87	81.34	2.98	30.50	19.10	6.45	6.45	20.58	5.46	3.97	7.44	8.18	2.98	0.00	40.92	
10:00	11:00	297.60	13.14					76.88	74.40	3.97	40.42	18.85	6.20	6.20	14.63	1.74	1.98	4.22	4.22	1.49	0.00	29.26	
11:00	12:00	297.60	8.93					75.14	63.98	4.96	51.09	7.19	2.73	1.98	26.78	4.96	2.98	5.95	6.45	1.98	0.00	32.49	
12:00	13:00	285.20	10.66					94.49	76.63	3.47	32.49	6.20	2.48	1.98	19.10	5.46	1.24	2.48	2.98	0.74	0.00	24.80	
13:00	14:00	297.60	7.94					64.98	52.33	4.46	41.17	22.57	7.69	7.69	19.10	1.49	5.70	10.91	11.90	3.97	0.00	35.71	
14:00	15:00	285.20	6.45					60.26	71.92	3.72	38.19	17.11	5.70	5.70	20.34	2.48	3.72	7.44	7.69	2.48	0.00	31.99	
15:00	16:00	334.80	11.66					81.10	85.06	4.46	42.41	21.82	7.69	7.44	18.35	4.71	2.98	5.95	6.45	1.98	0.00	32.74	
16:00	17:00	260.40	10.66					78.12	65.22	2.98	27.28	6.20	1.74	1.74	16.37	0.74	3.97	7.44	8.68	2.23	0.00	27.03	
17:00	18:00	310.00	15.62					112.34	79.61	2.48	20.83	11.90	3.97	3.72	16.37	2.48	2.48	4.71	4.71	1.98	0.00	26.78	
18:00	19:00	272.80	14.14					107.63	77.38	1.74	15.13	6.94	2.23	2.23	6.94	1.98	2.98	5.46	6.20	1.98	0.00	19.84	
19:00	20:00	272.80	11.66					113.09	82.83	0.50	5.95	2.73	0.50	0.99	19.84	1.74	1.49	2.48	2.73	0.74	0.00	25.54	
20:00	21:00	235.60	10.17					77.87	84.82	0.50	4.46	0.99	0.50	0.74	27.03	0.50	0.50	0.99	0.74	0.50	0.00	25.30	
21:00	22:00	210.80	6.45					60.76	84.82	0.50	5.21	1.74	0.50	0.74	22.82	0.00	0.25	0.25	0.00	0.25	0.00	26.54	
22:00	23:00	210.80	8.93					47.12	86.06	0.50	2.98	0.00	0.00	0.00	24.30	0.74	0.50	1.24	0.74	0.74	0.00	36.95	
23:00	0:00	173.60	7.69	37.45	69.69	0.50	3.22	0.00	0.00	0.00	20.09	0.99	0.50	1.49	0.99	0.99	0.00	30.01					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L119	Ma Tau Wai Road (NB)	Kiang Su Street	Lok Shan road	0:00	1:00	48.60	4.37	7.61	21.71	0.00	0.65	0.00	0.00	0.00	8.10	0.32	0.32	0.65	0.32	0.16	0.00	4.37
				1:00	2:00	24.30	2.59	5.02	14.58	0.00	0.49	0.00	0.00	0.00	0.81	0.00	0.00	0.00	0.00	0.00	0.00	0.81
				2:00	3:00	24.30	2.43	4.37	15.39	0.00	0.49	0.00	0.00	0.00	0.81	0.00	0.00	0.00	0.00	0.00	0.00	0.81
				3:00	4:00	16.20	2.27	3.40	9.23	0.00	0.49	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.81
				4:00	5:00	16.20	1.78	3.08	9.88	0.00	0.49	0.00	0.00	0.00	0.00	0.00	0.00	0.16	0.00	0.00	0.00	0.81
				5:00	6:00	16.20	1.62	2.92	9.72	0.00	0.49	0.00	0.00	0.00	0.00	0.16	0.32	0.49	0.32	0.16	0.00	0.00
				6:00	7:00	40.50	2.43	4.70	16.20	0.16	0.97	0.16	0.00	0.00	7.29	0.49	0.65	1.13	0.97	0.49	0.00	4.86
				7:00	8:00	121.50	2.59	17.33	41.31	0.81	2.27	0.49	0.16	0.16	32.89	0.81	0.81	1.78	1.46	0.81	0.00	17.82
				8:00	9:00	145.80	6.97	35.64	43.74	1.62	5.67	2.59	0.97	0.97	25.60	0.97	0.97	1.94	1.62	0.81	0.00	15.71
				9:00	10:00	137.70	5.02	27.38	45.20	1.46	5.51	2.92	0.97	0.97	22.84	1.46	1.30	2.27	2.27	0.81	0.00	17.33
				10:00	11:00	113.40	9.56	20.25	28.51	2.27	10.04	4.05	0.97	1.13	14.26	0.97	1.13	1.94	1.94	0.65	0.00	15.71
				11:00	12:00	121.50	6.32	19.44	24.14	3.08	12.47	1.46	0.49	0.32	24.95	2.75	1.46	3.08	3.24	0.97	0.00	17.33
				12:00	13:00	113.40	7.61	25.11	29.81	2.11	8.10	1.30	0.49	0.32	18.63	2.92	0.65	1.13	1.30	0.49	0.00	13.45
				13:00	14:00	121.50	6.16	17.82	20.09	2.59	10.53	5.02	1.30	1.30	18.79	0.81	3.24	5.67	6.16	1.94	0.00	20.09
				14:00	15:00	113.40	4.86	15.71	27.05	2.43	9.23	3.56	0.97	0.97	19.28	1.30	2.11	3.56	3.89	1.30	0.00	17.17
				15:00	16:00	129.60	8.26	21.55	32.56	2.43	10.69	4.70	1.30	1.30	17.66	2.43	1.62	3.24	3.24	0.97	0.00	17.66
				16:00	17:00	113.40	12.64	22.03	14.58	2.11	9.72	1.94	0.32	0.32	12.64	0.65	3.24	5.67	6.48	1.78	0.00	19.28
				17:00	18:00	137.70	19.76	34.67	19.28	1.94	7.94	4.21	0.81	0.81	13.93	2.27	2.27	3.89	3.89	1.46	0.00	20.57
				18:00	19:00	113.40	17.82	32.56	18.14	0.97	5.51	2.43	0.49	0.49	5.51	1.46	2.75	4.37	4.86	1.30	0.00	14.74
				19:00	20:00	113.40	8.75	30.78	32.56	0.16	1.46	0.65	0.16	0.16	19.93	0.81	0.65	1.30	1.30	0.49	0.00	14.26
				20:00	21:00	105.30	7.45	20.90	33.05	0.16	1.13	0.16	0.16	0.16	26.57	0.32	0.32	0.49	0.32	0.16	0.00	13.93
				21:00	22:00	97.20	5.18	17.17	34.02	0.16	1.30	0.32	0.16	0.16	23.00	0.00	0.16	0.16	0.00	0.16	0.00	15.23
				22:00	23:00	97.20	6.48	12.47	32.40	0.16	0.65	0.00	0.00	0.00	23.17	0.49	0.32	0.65	0.32	0.32	0.00	19.76
				23:00	0:00	81.00	5.67	9.88	26.41	0.16	0.81	0.00	0.00	0.00	19.28	0.49	0.32	0.81	0.32	0.32	0.00	16.52
L120	Maidstone Road (SB)	Lok Shan road	Kiang Su Street	0:00	1:00	8.00	0.32	3.20	2.88	0.00	0.32	0.00	0.00	0.00	1.28	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	8.00	0.48	3.68	3.36	0.00	0.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.00	0.48	3.68	3.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	8.00	0.64	3.36	3.36	0.00	0.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	8.00	0.00	4.00	4.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	8.00	0.00	3.68	3.52	0.00	0.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	8.00	0.32	2.40	2.56	0.00	0.32	0.00	0.00	0.00	1.44	0.00	0.00	0.32	0.32	0.32	0.00	
				7:00	8:00	16.00	0.16	6.24	3.36	0.00	0.64	0.16	0.00	0.00	3.68	0.16	0.16	0.48	0.48	0.48	0.00	
				8:00	9:00	24.00	0.80	11.84	3.36	0.00	1.76	0.80	0.48	0.48	2.72	0.16	0.16	0.48	0.48	0.48	0.00	
				9:00	10:00	24.00	0.80	9.76	3.84	0.00	1.92	0.96	0.48	0.48	2.56	0.48	0.32	0.80	0.80	0.80	0.00	
				10:00	11:00	16.00	0.48	6.24	2.72	0.00	2.24	1.12	0.32	0.32	1.60	0.16	0.16	0.16	0.32	0.16	0.00	
				11:00	12:00	16.00	0.32	5.76	2.24	0.00	2.72	0.32	0.16	0.16	2.88	0.32	0.16	0.32	0.32	0.32	0.00	
				12:00	13:00	16.00	0.48	7.52	2.72	0.00	1.76	0.32	0.16	0.16	2.08	0.32	0.00	0.16	0.16	0.16	0.00	
				13:00	14:00	16.00	0.32	4.96	1.92	0.00	2.24	1.12	0.48	0.48	2.08	0.16	0.32	0.64	0.64	0.64	0.00	
				14:00	15:00	16.00	0.32	5.28	2.72	0.00	2.08	0.96	0.32	0.32	2.24	0.16	0.16	0.48	0.48	0.48	0.00	
				15:00	16:00	24.00	0.64	8.48	4.00	0.00	3.04	1.44	0.64	0.64	2.88	0.48	0.32	0.48	0.48	0.48	0.00	
				16:00	17:00	16.00	0.48	6.88	2.24	0.00	1.76	0.48	0.16	0.16	1.92	0.00	0.32	0.48	0.64	0.48	0.00	
				17:00	18:00	24.00	0.96	12.64	3.36	0.00	1.76	0.96	0.32	0.32	2.40	0.16	0.16	0.32	0.32	0.32	0.00	
				18:00	19:00	24.00	0.96	13.76	3.84	0.00	1.44	0.64	0.16	0.16	1.12	0.16	0.16	0.48	0.64	0.48	0.00	
				19:00	20:00	16.00	0.48	9.28	2.88	0.00	0.32	0.16	0.00	0.00	2.24	0.16	0.00	0.16	0.16	0.16	0.00	
				20:00	21:00	16.00	0.48	8.00	3.68	0.00	0.16	0.00	0.00	0.00	3.68	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	16.00	0.48	6.72	4.48	0.00	0.48	0.32	0.00	0.00	3.52	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	16.00	0.64	5.76	4.80	0.00	0.32	0.00	0.00	0.00	4.16	0.00	0.00	0.00	0.00	0.32	0.00	
				23:00	0:00	8.00	0.32	2.88	2.40	0.00	0.16	0.00	0.00	0.00	2.08	0.00	0.00	0.00	0.00	0.16	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L121	Kau Pui Lung Road (SB)	Lok Shan road	Kiang Su Street	0:00	1:00	17.10	0.51	4.96	10.60	0.00	0.17	0.00	0.00	0.00	0.00	0.17	0.17	0.34	0.17	0.00	0.00	0.00
				1:00	2:00	17.10	0.68	5.13	10.94	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	17.10	0.68	4.79	11.29	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	8.55	0.51	2.57	5.30	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	8.55	0.34	2.39	5.47	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	8.55	0.17	2.05	5.47	0.00	0.17	0.00	0.00	0.00	0.00	0.17	0.17	0.17	0.17	0.00	0.00	0.00
				6:00	7:00	17.10	0.51	3.76	9.58	0.00	0.51	0.17	0.00	0.00	0.00	0.51	0.51	0.68	0.68	0.17	0.00	0.00
				7:00	8:00	34.20	0.86	11.46	14.88	0.17	1.71	0.34	0.00	0.00	0.00	0.86	0.86	1.37	1.37	0.34	0.00	0.00
				8:00	9:00	51.30	1.88	22.57	14.88	0.17	3.76	2.22	0.17	0.17	0.00	1.03	1.03	1.54	1.54	0.34	0.00	0.00
				9:00	10:00	42.75	1.20	15.39	13.68	0.17	3.25	2.22	0.17	0.17	0.00	1.37	1.03	1.71	1.88	0.51	0.00	0.00
				10:00	11:00	42.75	1.54	14.54	14.71	0.17	4.96	2.39	0.17	0.17	0.00	0.68	0.86	1.20	1.20	0.17	0.00	0.00
				11:00	12:00	42.75	1.03	14.02	13.00	0.34	6.33	0.86	0.00	0.00	0.00	1.88	1.03	1.88	2.05	0.34	0.00	0.00
				12:00	13:00	42.75	1.20	17.44	15.05	0.17	3.93	0.86	0.00	0.00	0.00	1.88	0.34	0.86	0.86	0.17	0.00	0.00
				13:00	14:00	42.75	1.03	12.14	10.60	0.17	4.96	2.74	0.17	0.34	0.00	0.68	2.22	3.25	3.59	0.86	0.00	0.00
				14:00	15:00	42.75	0.86	11.97	14.71	0.17	4.79	2.22	0.17	0.17	0.00	1.03	1.37	2.22	2.39	0.68	0.00	0.00
				15:00	16:00	42.75	1.03	13.51	15.05	0.17	4.45	2.22	0.17	0.17	0.00	1.37	1.03	1.54	1.71	0.34	0.00	0.00
				16:00	17:00	42.75	1.37	16.25	13.00	0.17	3.25	0.68	0.00	0.00	0.00	0.34	1.71	2.57	2.91	0.51	0.00	0.00
				17:00	18:00	51.30	1.88	23.94	15.73	0.17	2.39	1.37	0.17	0.17	0.00	1.03	1.03	1.54	1.54	0.34	0.00	0.00
				18:00	19:00	51.30	1.88	23.77	15.90	0.17	1.88	0.86	0.17	0.17	0.00	0.86	1.37	1.88	2.05	0.34	0.00	0.00
				19:00	20:00	42.75	1.37	20.86	16.42	0.00	0.86	0.34	0.00	0.00	0.00	0.68	0.34	0.86	0.86	0.17	0.00	0.00
				20:00	21:00	34.20	1.20	14.71	16.93	0.00	0.51	0.17	0.00	0.00	0.00	0.17	0.17	0.17	0.17	0.00	0.00	0.00
				21:00	22:00	34.20	0.86	13.00	19.32	0.00	0.68	0.17	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00
				22:00	23:00	25.65	0.86	8.04	15.22	0.00	0.34	0.00	0.00	0.00	0.00	0.34	0.17	0.34	0.17	0.17	0.00	0.00
L122	Kau Pui Lung Road (NB)	Kiang Su Street	Lok Shan road	23:00	0:00	25.65	1.03	7.52	15.05	0.00	0.51	0.00	0.00	0.00	0.00	0.51	0.17	0.51	0.17	0.17	0.00	0.00
				0:00	1:00	34.20	0.68	7.70	22.57	0.00	0.86	0.00	0.00	0.00	0.00	1.03	0.17	0.68	0.17	0.17	0.00	0.00
				1:00	2:00	17.10	0.34	3.76	12.48	0.00	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	17.10	0.17	3.59	13.00	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	17.10	0.34	3.76	12.14	0.00	0.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	17.10	0.34	3.59	12.48	0.00	0.68	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	17.10	0.17	3.25	11.80	0.00	0.68	0.00	0.00	0.00	0.00	0.17	0.17	0.51	0.17	0.17	0.00	0.00
				6:00	7:00	25.65	0.34	4.28	16.25	0.00	1.20	0.17	0.00	0.00	0.86	0.34	0.34	0.86	0.68	0.34	0.00	0.00
				7:00	8:00	59.85	0.68	16.25	29.41	0.34	3.08	0.68	0.17	0.17	3.08	0.86	0.86	2.05	1.54	0.68	0.00	0.00
				8:00	9:00	85.50	1.88	31.64	29.58	0.51	7.01	3.93	0.86	0.86	2.39	1.03	1.03	2.22	1.88	0.68	0.00	0.00
				9:00	10:00	76.95	1.20	23.77	29.07	0.51	6.50	3.93	0.86	0.86	1.88	1.54	1.20	2.57	2.22	0.86	0.00	0.00
				10:00	11:00	76.95	1.37	18.81	28.56	0.51	12.48	5.47	1.20	1.37	1.71	0.86	0.86	1.71	1.54	0.51	0.00	0.00
				11:00	12:00	68.40	0.86	16.76	22.74	0.51	14.36	1.88	0.51	0.34	3.08	1.71	1.03	2.05	1.88	0.68	0.00	0.00
				12:00	13:00	68.40	1.03	21.38	27.36	0.34	9.23	1.71	0.51	0.34	2.05	1.88	0.51	0.86	0.86	0.34	0.00	0.00
				13:00	14:00	68.40	0.68	14.54	18.30	0.51	11.63	6.16	1.37	1.37	2.05	0.51	2.05	4.10	3.76	1.37	0.00	0.00
				14:00	15:00	68.40	0.68	13.68	25.48	0.51	10.77	4.62	1.03	1.03	2.39	0.86	1.20	2.91	2.39	0.86	0.00	0.00
				15:00	16:00	85.50	1.20	19.49	31.29	0.68	12.83	6.33	1.54	1.54	2.22	1.71	1.20	2.57	2.05	0.86	0.00	0.00
				16:00	17:00	68.40	1.20	18.30	22.91	0.17	10.77	2.39	0.51	0.51	2.22	0.34	1.71	3.25	3.25	0.86	0.00	0.00
				17:00	18:00	85.50	1.88	28.56	29.24	0.17	8.89	4.62	1.20	1.20	2.39	1.20	1.20	2.22	1.88	0.86	0.00	0.00
				18:00	19:00	76.95	1.71	27.02	28.90	0.17	6.50	2.57	0.51	0.86	0.86	1.37	2.39	2.39	0.86	0.00	0.00	0.00
				19:00	20:00	68.40	1.37	27.19	31.29	0.00	1.88	0.68	0.17	0.17	2.39	0.51	0.51	1.03	0.86	0.34	0.00	0.00
				20:00	21:00	59.85	1.03	19.49	33.00	0.00	1.37	0.17	0.17	0.17	3.42	0.17	0.17	0.34	0.17	0.17	0.00	0.00
				21:00	22:00	51.30	0.68	14.19	31.12	0.00	1.71	0.51	0.17	0.17	2.57	0.00	0.00	0.17	0.00	0.00	0.00	0.00
				22:00	23:00	51.30	1.03	11.63	33.17	0.00	1.03	0.00	0.00	0.00	2.91	0.34	0.17	0.68	0.17	0.17	0.00	0.00
				23:00	0:00	42.75	0.86	9.23	27.53	0.00	1.03	0.00	0.00	0.00	2.39	0.51	0.17	0.68	0.17	0.17	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L123	Kiang Su Street (WB)	Maidstone Road	Kau Pui Lung Road	0:00	1:00	2.10	0.04	0.92	0.76	0.00	0.04	0.00	0.00	0.00	0.29	0.00	0.00	0.04	0.00	0.00	0.00	0.00	
				1:00	2:00	2.10	0.13	0.97	0.88	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.10	0.13	1.01	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.10	0.17	0.88	0.88	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.10	0.00	1.05	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.10	0.00	0.97	0.97	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.10	0.08	0.71	0.71	0.00	0.08	0.00	0.00	0.00	0.34	0.00	0.00	0.08	0.08	0.00	0.00	0.00	0.00
				7:00	8:00	4.20	0.04	1.81	0.92	0.08	0.17	0.04	0.00	0.00	0.84	0.04	0.04	0.08	0.08	0.04	0.00	0.00	0.00
				8:00	9:00	6.30	0.13	3.49	0.92	0.13	0.38	0.29	0.04	0.04	0.59	0.04	0.04	0.08	0.08	0.04	0.00	0.00	0.00
				9:00	10:00	6.30	0.13	2.98	1.05	0.17	0.42	0.34	0.04	0.04	0.59	0.13	0.04	0.17	0.17	0.04	0.00	0.00	0.00
				10:00	11:00	6.30	0.17	2.56	1.09	0.17	0.76	0.42	0.13	0.13	0.50	0.04	0.04	0.13	0.13	0.04	0.00	0.00	0.00
				11:00	12:00	6.30	0.13	2.35	0.92	0.17	0.97	0.17	0.04	0.04	0.97	0.13	0.04	0.17	0.17	0.04	0.00	0.00	0.00
				12:00	13:00	6.30	0.17	3.15	1.09	0.13	0.63	0.13	0.04	0.04	0.71	0.13	0.00	0.04	0.04	0.00	0.00	0.00	0.00
				13:00	14:00	6.30	0.13	2.10	0.76	0.17	0.84	0.46	0.13	0.13	0.71	0.04	0.13	0.29	0.29	0.13	0.00	0.00	0.00
				14:00	15:00	6.30	0.13	2.10	1.09	0.17	0.80	0.38	0.13	0.13	0.80	0.08	0.08	0.17	0.17	0.08	0.00	0.00	0.00
				15:00	16:00	6.30	0.17	2.27	1.09	0.17	0.76	0.42	0.13	0.13	0.63	0.13	0.04	0.17	0.17	0.04	0.00	0.00	0.00
				16:00	17:00	6.30	0.21	2.69	0.97	0.04	0.71	0.17	0.04	0.04	0.67	0.00	0.13	0.29	0.29	0.04	0.00	0.00	0.00
				17:00	18:00	6.30	0.25	3.28	0.92	0.04	0.46	0.21	0.08	0.08	0.59	0.04	0.04	0.13	0.13	0.04	0.00	0.00	0.00
				18:00	19:00	6.30	0.25	3.57	1.05	0.04	0.38	0.17	0.04	0.04	0.25	0.04	0.04	0.17	0.21	0.04	0.00	0.00	0.00
				19:00	20:00	6.30	0.17	3.86	1.22	0.00	0.13	0.04	0.00	0.00	0.76	0.04	0.00	0.04	0.04	0.00	0.00	0.00	0.00
				20:00	21:00	4.20	0.08	2.18	1.05	0.00	0.04	0.00	0.00	0.00	0.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	4.20	0.13	1.93	1.18	0.00	0.13	0.04	0.00	0.00	0.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	4.20	0.13	1.60	1.34	0.00	0.08	0.00	0.00	0.00	0.97	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00
L124	Maidstone Road (NB)	Chi Kiang Street	Kiang Su Street	23:00	0:00	4.20	0.17	1.55	1.34	0.00	0.08	0.00	0.00	0.00	0.97	0.00	0.00	0.08	0.00	0.00	0.00	0.00	
				0:00	1:00	2.55	0.00	1.53	1.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.55	0.00	1.68	0.56	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.55	0.00	1.94	0.31	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.55	0.00	1.73	0.41	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.55	0.00	1.48	0.36	0.00	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.55	0.00	1.33	0.36	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.55	0.00	1.68	0.36	0.00	0.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.55	0.00	1.38	0.41	0.00	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.55	0.00	1.38	0.46	0.00	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.55	0.00	1.38	0.51	0.00	0.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.55	0.00	1.58	0.31	0.00	0.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.55	0.00	1.84	0.36	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.55	0.00	1.89	0.41	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.55	0.00	1.94	0.46	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.55	0.00	1.84	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	2.55	0.00	1.58	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				
23:00	0:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00			

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L125	Maidstone Road (SB)	Kiang Su Street	Chi Kiang Street	0:00	1:00	2.55	0.00	1.53	1.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.55	0.00	1.58	0.71	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.55	0.00	1.73	0.41	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.55	0.00	1.63	0.46	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.55	0.00	1.33	0.36	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.55	0.00	1.17	0.36	0.00	1.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.55	0.00	1.53	0.36	0.00	0.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.55	0.00	1.17	0.41	0.00	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.55	0.00	1.17	0.41	0.00	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.55	0.00	1.28	0.46	0.00	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.55	0.00	1.33	0.36	0.00	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.55	0.00	1.68	0.31	0.00	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.55	0.00	1.79	0.31	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.55	0.00	1.89	0.51	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.55	0.00	1.68	0.66	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.55	0.00	1.53	0.77	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.55	0.00	1.58	0.97	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L126	Ma Tau Wai Road (NB)	Chi Kiang Street	Kiang Su Street	23:00	0:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				0:00	1:00	23.70	1.98	3.79	10.67	0.00	0.32	0.00	0.00	0.00	3.95	0.16	0.16	0.32	0.16	0.08	0.00	2.13	
				1:00	2:00	11.85	1.26	2.45	7.11	0.00	0.24	0.00	0.00	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.40	
				2:00	3:00	11.85	1.11	2.29	7.43	0.00	0.24	0.00	0.00	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.40	
				3:00	4:00	7.90	1.03	1.66	4.58	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.40	
				4:00	5:00	7.90	0.79	1.50	4.90	0.00	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.40	
				5:00	6:00	7.90	0.71	1.42	4.82	0.00	0.24	0.00	0.00	0.00	0.00	0.08	0.16	0.24	0.16	0.08	0.00	0.00	
				6:00	7:00	19.75	1.11	2.29	7.98	0.16	0.47	0.08	0.00	0.00	3.56	0.24	0.32	0.55	0.47	0.16	0.00	2.37	
				7:00	8:00	59.25	1.11	8.45	20.62	0.47	1.11	0.24	0.08	0.08	15.80	0.40	0.40	0.87	0.71	0.24	0.00	8.69	
				8:00	9:00	71.10	2.92	17.46	21.65	1.03	2.69	1.34	0.47	0.47	12.48	0.47	0.47	1.03	0.79	0.24	0.00	7.58	
				9:00	10:00	67.15	2.05	13.35	22.44	0.95	2.53	1.50	0.47	0.47	11.06	0.71	0.63	1.19	1.11	0.32	0.00	8.37	
				10:00	11:00	55.30	4.50	9.88	13.90	1.19	4.90	2.13	0.47	0.55	6.87	0.47	0.55	1.03	0.95	0.32	0.00	7.58	
				11:00	12:00	59.25	2.92	9.56	11.85	1.58	6.08	0.87	0.24	0.16	12.17	1.34	0.71	1.42	1.58	0.40	0.00	8.37	
				12:00	13:00	55.30	3.63	12.25	14.54	1.11	3.95	0.71	0.24	0.16	9.01	1.42	0.32	0.63	0.63	0.16	0.00	6.56	
				13:00	14:00	59.25	2.77	8.61	10.03	1.42	5.14	2.61	0.63	0.63	9.16	0.40	1.50	2.84	2.92	0.87	0.00	9.72	
				14:00	15:00	55.30	2.29	7.74	13.43	1.19	4.50	1.82	0.47	0.47	9.32	0.63	0.95	1.82	1.82	0.55	0.00	8.30	
				15:00	16:00	63.20	3.87	10.43	16.12	1.42	5.14	2.45	0.63	0.63	8.53	1.19	0.79	1.50	1.50	0.47	0.00	8.53	
				16:00	17:00	55.30	6.08	10.82	7.11	1.03	4.74	1.03	0.16	0.16	6.16	0.32	1.58	2.77	3.16	0.87	0.00	9.32	
				17:00	18:00	67.15	9.64	16.91	9.32	0.95	3.95	2.05	0.40	0.40	6.79	1.11	1.11	1.90	1.90	0.71	0.00	10.03	
				18:00	19:00	55.30	8.69	15.72	8.85	0.47	2.84	1.19	0.24	0.24	2.69	0.71	1.34	2.13	2.37	0.63	0.00	7.19	
				19:00	20:00	55.30	4.11	15.01	16.12	0.16	0.71	0.32	0.08	0.08	9.64	0.40	0.32	0.63	0.63	0.16	0.00	6.95	
				20:00	21:00	51.35	3.48	10.19	16.35	0.16	0.55	0.08	0.08	0.08	12.88	0.16	0.16	0.24	0.16	0.08	0.00	6.72	
				21:00	22:00	47.40	2.37	8.22	16.91	0.16	0.63	0.16	0.08	0.08	11.22	0.00	0.08	0.08	0.00	0.08	0.00	7.35	
22:00	23:00	47.40	3.00	6.00	16.12	0.16	0.32	0.00	0.00	0.00	11.22	0.24	0.16	0.32	0.16	0.16	0.00	9.56					
23:00	0:00	39.50	2.61	4.82	13.11	0.16	0.40	0.00	0.00	0.00	9.32	0.24	0.16	0.40	0.16	0.16	0.00	7.98					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L127	Chi Kiang Street (WB)	Ma Tau Wai Road	Ko Shan Road	0:00	1:00	2.15	0.00	1.33	0.73	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.15	0.00	1.33	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.15	0.00	1.38	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.00	1.33	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.00	1.25	0.90	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.00	1.33	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.15	0.00	1.16	0.86	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.15	0.00	1.42	0.56	0.00	0.13	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.30	0.00	3.05	0.60	0.00	0.34	0.22	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.30	0.00	2.75	0.73	0.00	0.39	0.26	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	4.30	0.00	2.45	0.65	0.00	0.65	0.39	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	4.30	0.00	2.62	0.60	0.00	0.90	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	4.30	0.00	3.01	0.65	0.00	0.52	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	4.30	0.00	2.32	0.52	0.00	0.73	0.52	0.09	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	4.30	0.00	2.28	0.77	0.00	0.69	0.39	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	4.30	0.00	2.41	0.69	0.00	0.65	0.39	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	4.30	0.00	3.01	0.56	0.00	0.56	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.30	0.00	3.18	0.52	0.00	0.30	0.22	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.30	0.00	3.27	0.52	0.00	0.26	0.17	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	4.30	0.00	3.44	0.69	0.00	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.15	0.00	1.63	0.47	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.15	0.00	1.51	0.56	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.15	0.00	1.42	0.69	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	2.15	0.00	1.29	0.77	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L128	Kau Pui Lung Road (SB)	Kiang Su Street	Ko Shan Road	0:00	1:00	12.50	0.25	3.75	6.88	0.00	0.25	0.00	0.00	0.00	0.63	0.13	0.13	0.25	0.13	0.13	0.00	0.00	
				1:00	2:00	12.50	0.25	4.13	7.88	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	6.25	0.13	1.88	4.13	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	6.25	0.13	2.13	3.88	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	6.25	0.25	1.88	3.88	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	6.25	0.13	1.63	3.63	0.00	0.13	0.00	0.00	0.00	0.00	0.13	0.13	0.25	0.13	0.13	0.00	0.00	0.00
				6:00	7:00	12.50	0.13	2.88	6.50	0.00	0.25	0.13	0.00	0.00	0.75	0.25	0.25	0.63	0.63	0.13	0.00	0.00	0.00
				7:00	8:00	25.00	0.25	8.13	9.25	0.13	1.13	0.25	0.13	0.00	2.25	0.50	0.38	1.13	1.13	0.38	0.00	0.00	0.00
				8:00	9:00	37.50	1.00	16.50	9.75	0.13	2.50	1.38	0.38	0.25	1.75	0.50	0.50	1.25	1.25	0.38	0.00	0.00	0.00
				9:00	10:00	37.50	0.75	13.63	10.75	0.13	2.63	1.63	0.50	0.25	1.63	0.88	0.75	1.63	1.75	0.63	0.00	0.00	0.00
				10:00	11:00	31.25	0.75	10.88	9.88	0.13	3.25	1.50	0.38	0.25	1.25	0.38	0.38	0.88	1.00	0.38	0.00	0.00	0.00
				11:00	12:00	31.25	0.50	10.25	8.25	0.25	4.13	0.50	0.13	0.13	2.25	1.13	0.50	1.38	1.50	0.38	0.00	0.00	0.00
				12:00	13:00	31.25	0.63	13.25	9.88	0.13	2.63	0.50	0.13	0.13	1.50	1.00	0.25	0.50	0.63	0.13	0.00	0.00	0.00
				13:00	14:00	31.25	0.38	8.88	6.75	0.13	3.38	1.75	0.50	0.25	1.63	0.25	1.25	2.50	2.75	0.88	0.00	0.00	0.00
				14:00	15:00	31.25	0.38	8.63	9.63	0.13	3.25	1.38	0.38	0.25	1.75	0.50	0.88	1.75	1.88	0.50	0.00	0.00	0.00
				15:00	16:00	37.50	0.63	12.00	11.63	0.13	3.63	1.75	0.63	0.25	1.63	1.00	0.63	1.50	1.50	0.63	0.00	0.00	0.00
				16:00	17:00	37.50	0.88	14.25	10.13	0.13	2.63	0.63	0.13	0.00	1.75	0.13	1.13	2.38	2.63	0.75	0.00	0.00	0.00
				17:00	18:00	43.75	1.13	20.63	12.00	0.13	2.00	1.00	0.25	0.13	1.75	0.63	0.63	1.50	1.50	0.50	0.00	0.00	0.00
				18:00	19:00	43.75	1.13	20.88	12.50	0.13	1.50	0.75	0.13	0.13	0.75	0.63	0.88	1.75	2.00	0.63	0.00	0.00	0.00
				19:00	20:00	31.25	0.63	15.75	10.63	0.00	0.50	0.25	0.00	0.00	1.63	0.25	0.25	0.63	0.63	0.13	0.00	0.00	0.00
				20:00	21:00	25.00	0.50	10.38	10.75	0.00	0.38	0.13	0.00	0.00	2.13	0.13	0.13	0.25	0.13	0.13	0.00	0.00	0.00
				21:00	22:00	25.00	0.38	9.50	12.50	0.00	0.38	0.13	0.00	0.00	2.00	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	25.00	0.63	7.75	13.00	0.00	0.38	0.00	0.00	0.00	2.38	0.13	0.13	0.38	0.13	0.13	0.00	0.00	0.00
				23:00	0:00	18.75	0.38	5.50	10.13	0.00	0.25	0.00	0.00	0.00	1.75	0.13	0.13	0.25	0.13	0.13	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L129	Kau Pui Lung Road (NB)	Ko Shan Road	Kiang Su Street	0:00	1:00	20.40	0.41	3.94	14.14	0.00	0.41	0.00	0.00	0.00	0.68	0.14	0.14	0.27	0.14	0.14	0.00	0.00		
				1:00	2:00	13.60	0.41	2.58	10.06	0.00	0.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	13.60	0.41	2.45	10.34	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	13.60	0.54	2.58	9.93	0.00	0.54	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	6.80	0.14	1.22	5.30	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	13.60	0.27	2.04	9.11	0.00	0.68	0.00	0.00	0.00	0.00	0.00	0.27	0.27	0.41	0.27	0.27	0.00	0.00	0.00
				6:00	7:00	20.40	0.41	2.86	12.92	0.00	0.95	0.14	0.00	0.00	0.95	0.14	0.41	0.68	0.54	0.41	0.00	0.00	0.00	0.00
				7:00	8:00	40.80	0.68	9.11	21.22	0.14	2.18	0.41	0.14	0.14	2.58	0.68	0.41	1.22	1.09	0.82	0.00	0.00	0.00	0.00
				8:00	9:00	54.40	1.63	17.41	20.94	0.14	4.90	2.45	0.54	0.41	1.90	0.54	0.54	1.22	1.09	0.68	0.00	0.00	0.00	0.00
				9:00	10:00	54.40	1.22	14.14	22.58	0.14	4.90	2.72	0.54	0.41	1.63	0.95	0.82	1.63	1.63	1.09	0.00	0.00	0.00	0.00
				10:00	11:00	47.60	1.09	10.06	19.04	0.14	8.30	3.40	0.68	0.54	1.36	0.27	0.41	0.95	0.82	0.54	0.00	0.00	0.00	0.00
				11:00	12:00	47.60	0.82	10.06	16.32	0.27	10.61	1.36	0.27	0.14	2.58	1.09	0.68	1.36	1.22	0.82	0.00	0.00	0.00	0.00
				12:00	13:00	47.60	0.95	12.92	20.13	0.14	6.94	1.22	0.27	0.14	1.77	1.22	0.27	0.68	0.68	0.27	0.00	0.00	0.00	0.00
				13:00	14:00	47.60	0.82	8.84	13.46	0.14	8.70	4.22	0.95	0.82	1.77	0.27	1.22	2.58	2.45	1.36	0.00	0.00	0.00	0.00
				14:00	15:00	47.60	0.68	8.30	18.22	0.14	8.02	3.13	0.82	0.68	2.04	0.54	0.82	1.77	1.50	0.95	0.00	0.00	0.00	0.00
				15:00	16:00	54.40	0.82	10.61	21.76	0.14	8.70	4.08	0.82	0.82	1.77	0.82	0.54	1.50	1.22	0.82	0.00	0.00	0.00	0.00
				16:00	17:00	47.60	1.09	11.56	16.18	0.14	8.16	1.77	0.27	0.27	1.90	0.14	0.95	2.18	1.90	1.09	0.00	0.00	0.00	0.00
				17:00	18:00	54.40	1.50	16.46	19.86	0.14	6.12	2.99	0.68	0.54	1.90	0.54	0.54	1.36	1.09	0.68	0.00	0.00	0.00	0.00
				18:00	19:00	47.60	1.36	15.37	18.77	0.14	4.22	1.77	0.41	0.27	0.68	0.41	0.68	1.50	1.36	0.68	0.00	0.00	0.00	0.00
				19:00	20:00	40.80	0.95	14.14	20.26	0.00	1.09	0.41	0.14	0.14	1.77	0.27	0.27	0.54	0.54	0.27	0.00	0.00	0.00	0.00
				20:00	21:00	40.80	0.95	11.29	23.80	0.00	0.95	0.14	0.14	0.00	2.72	0.14	0.14	0.27	0.14	0.14	0.00	0.00	0.00	0.00
				21:00	22:00	34.00	0.54	8.16	21.49	0.00	1.09	0.27	0.14	0.00	2.04	0.00	0.00	0.14	0.00	0.14	0.00	0.00	0.00	0.00
				22:00	23:00	34.00	0.82	6.53	22.71	0.00	0.68	0.00	0.00	0.00	2.31	0.14	0.14	0.27	0.14	0.27	0.00	0.00	0.00	0.00
				23:00	0:00	27.20	0.82	4.90	17.95	0.00	0.68	0.00	0.00	0.00	1.90	0.14	0.14	0.27	0.14	0.27	0.00	0.00	0.00	0.00
L130	Ko Shan Road (NB)	Chi Kiang Street	Anhui Street	0:00	1:00	6.00	0.16	0.76	4.44	0.00	0.16	0.00	0.00	0.00	0.20	0.04	0.04	0.12	0.04	0.04	0.00	0.00		
				1:00	2:00	4.00	0.08	0.52	3.24	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.00	0.12	0.48	3.28	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.00	0.16	0.56	3.08	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.00	0.08	0.48	3.28	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.00	0.08	0.44	2.84	0.00	0.20	0.00	0.00	0.00	0.00	0.08	0.08	0.12	0.08	0.08	0.00	0.00	0.00	0.00
				6:00	7:00	6.00	0.08	0.60	4.08	0.00	0.28	0.04	0.00	0.00	0.24	0.08	0.08	0.20	0.16	0.16	0.00	0.00	0.00	0.00
				7:00	8:00	12.00	0.20	2.04	6.64	0.04	0.68	0.12	0.04	0.00	0.76	0.20	0.20	0.44	0.36	0.28	0.00	0.00	0.00	0.00
				8:00	9:00	16.00	0.56	4.04	6.84	0.04	1.48	0.72	0.16	0.08	0.56	0.20	0.20	0.48	0.40	0.24	0.00	0.00	0.00	0.00
				9:00	10:00	16.00	0.40	3.20	7.24	0.04	1.44	0.80	0.16	0.08	0.48	0.36	0.28	0.64	0.52	0.36	0.00	0.00	0.00	0.00
				10:00	11:00	14.00	0.36	2.20	6.28	0.04	2.44	0.96	0.20	0.12	0.36	0.12	0.16	0.32	0.28	0.16	0.00	0.00	0.00	0.00
				11:00	12:00	14.00	0.28	2.12	5.32	0.08	3.12	0.40	0.08	0.04	0.76	0.40	0.20	0.48	0.44	0.28	0.00	0.00	0.00	0.00
				12:00	13:00	14.00	0.32	2.80	6.68	0.04	2.08	0.36	0.08	0.04	0.52	0.40	0.08	0.24	0.24	0.12	0.00	0.00	0.00	0.00
				13:00	14:00	14.00	0.20	1.84	4.48	0.04	2.56	1.20	0.24	0.16	0.52	0.12	0.44	0.92	0.80	0.48	0.00	0.00	0.00	0.00
				14:00	15:00	14.00	0.20	1.72	6.08	0.04	2.32	0.92	0.16	0.12	0.56	0.16	0.24	0.60	0.52	0.36	0.00	0.00	0.00	0.00
				15:00	16:00	16.00	0.32	2.28	7.12	0.04	2.52	1.12	0.24	0.16	0.52	0.32	0.20	0.52	0.40	0.24	0.00	0.00	0.00	0.00
				16:00	17:00	14.00	0.36	2.32	5.56	0.04	2.36	0.48	0.08	0.04	0.52	0.04	0.36	0.76	0.72	0.36	0.00	0.00	0.00	0.00
				17:00	18:00	16.00	0.52	3.44	6.92	0.04	1.84	0.88	0.16	0.12	0.56	0.20	0.20	0.48	0.40	0.24	0.00	0.00	0.00	0.00
				18:00	19:00	14.00	0.48	3.16	6.52	0.04	1.28	0.52	0.08	0.08	0.20	0.16	0.24	0.52	0.48	0.24	0.00	0.00	0.00	0.00
				19:00	20:00	12.00	0.32	3.16	6.80	0.00	0.36	0.12	0.00	0.00	0.52	0.12	0.08	0.20	0.20	0.12	0.00	0.00	0.00	0.00
				20:00	21:00	12.00	0.32	2.48	7.84	0.00	0.28	0.04	0.00	0.00	0.80	0.04	0.04	0.08	0.04	0.04	0.00	0.00	0.00	0.00
				21:00	22:00	10.00	0.20	1.72	6.96	0.00	0.36	0.08	0.00	0.00	0.60	0.00	0.00	0.04	0.00	0.04	0.00	0.00	0.00	0.00
				22:00	23:00	10.00	0.28	1.36	7.16	0.00	0.20	0.00	0.00	0.00	0.68	0.04	0.04	0.12	0.04	0.08	0.00	0.00	0.00	0.00
				23:00	0:00	8.00	0.20	1.04	5.76	0.00	0.16	0.00	0.00	0.00	0.52	0.04	0.04	0.12	0.04	0.08	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L131	Ko Shan Road (SB)	Anhui Street	Chi Kiang Street	0:00	1:00	4.00	0.08	1.20	2.20	0.00	0.08	0.00	0.00	0.00	0.20	0.04	0.04	0.08	0.04	0.04	0.00	0.00
				1:00	2:00	4.00	0.08	1.32	2.52	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.00	0.04	0.60	1.32	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.00	0.04	0.68	1.24	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.00	0.08	0.60	1.24	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.00	0.04	0.52	1.16	0.00	0.04	0.00	0.00	0.00	0.00	0.04	0.04	0.08	0.04	0.04	0.00	0.00
				6:00	7:00	4.00	0.04	0.92	2.08	0.00	0.08	0.04	0.00	0.00	0.24	0.08	0.08	0.20	0.20	0.04	0.00	0.00
				7:00	8:00	8.00	0.08	2.60	2.96	0.04	0.36	0.08	0.04	0.00	0.72	0.16	0.12	0.36	0.36	0.12	0.00	0.00
				8:00	9:00	12.00	0.32	5.28	3.12	0.04	0.80	0.44	0.12	0.08	0.56	0.16	0.16	0.40	0.40	0.12	0.00	0.00
				9:00	10:00	12.00	0.24	4.36	3.44	0.04	0.84	0.52	0.16	0.08	0.52	0.28	0.24	0.52	0.56	0.20	0.00	0.00
				10:00	11:00	10.00	0.24	3.48	3.16	0.04	1.04	0.48	0.12	0.08	0.40	0.12	0.12	0.28	0.32	0.12	0.00	0.00
				11:00	12:00	10.00	0.16	3.28	2.64	0.08	1.32	0.16	0.04	0.04	0.72	0.36	0.16	0.44	0.48	0.12	0.00	0.00
				12:00	13:00	10.00	0.20	4.24	3.16	0.04	0.84	0.16	0.04	0.04	0.48	0.32	0.08	0.16	0.20	0.04	0.00	0.00
				13:00	14:00	10.00	0.12	2.84	2.16	0.04	1.08	0.56	0.16	0.08	0.52	0.08	0.40	0.80	0.88	0.28	0.00	0.00
				14:00	15:00	10.00	0.12	2.76	3.08	0.04	1.04	0.44	0.12	0.08	0.56	0.16	0.28	0.56	0.60	0.16	0.00	0.00
				15:00	16:00	12.00	0.20	3.84	3.72	0.04	1.16	0.56	0.20	0.08	0.52	0.32	0.20	0.48	0.48	0.20	0.00	0.00
				16:00	17:00	12.00	0.28	4.56	3.24	0.04	0.84	0.20	0.04	0.00	0.56	0.04	0.36	0.76	0.84	0.24	0.00	0.00
				17:00	18:00	14.00	0.36	6.60	3.84	0.04	0.64	0.32	0.08	0.04	0.56	0.20	0.20	0.48	0.48	0.16	0.00	0.00
				18:00	19:00	14.00	0.36	6.68	4.00	0.04	0.48	0.24	0.04	0.04	0.24	0.20	0.28	0.56	0.64	0.20	0.00	0.00
				19:00	20:00	10.00	0.20	5.04	3.40	0.00	0.16	0.08	0.00	0.00	0.52	0.08	0.08	0.20	0.20	0.04	0.00	0.00
				20:00	21:00	8.00	0.16	3.32	3.44	0.00	0.12	0.04	0.00	0.00	0.68	0.04	0.04	0.08	0.04	0.04	0.00	0.00
				21:00	22:00	8.00	0.12	3.04	4.00	0.00	0.12	0.04	0.00	0.00	0.64	0.00	0.00	0.04	0.00	0.00	0.00	0.00
				22:00	23:00	8.00	0.20	2.48	4.16	0.00	0.12	0.00	0.00	0.00	0.76	0.04	0.04	0.12	0.04	0.04	0.00	0.00
				23:00	0:00	6.00	0.12	1.76	3.24	0.00	0.08	0.00	0.00	0.00	0.56	0.04	0.04	0.08	0.04	0.04	0.00	0.00
L132	Chatham Road North (NB)	Anhui Street	Chi Kiang Street	0:00	1:00	20.50	2.01	3.61	10.17	0.00	0.49	0.00	0.00	0.00	2.21	0.12	0.12	0.33	0.16	0.08	0.00	1.19
				1:00	2:00	10.25	1.15	2.09	6.31	0.00	0.37	0.00	0.00	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.16
				2:00	3:00	10.25	0.94	1.97	6.64	0.00	0.29	0.00	0.00	0.00	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.21
				3:00	4:00	8.20	1.03	1.68	4.88	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.21
				4:00	5:00	6.15	0.66	1.15	3.90	0.00	0.21	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	0.00	0.16
				5:00	6:00	8.20	0.74	1.39	4.84	0.00	0.45	0.00	0.00	0.04	0.00	0.08	0.12	0.29	0.16	0.08	0.00	0.00
				6:00	7:00	18.45	1.19	2.34	8.32	0.08	0.86	0.12	0.00	0.04	2.13	0.25	0.33	0.62	0.53	0.21	0.00	1.44
				7:00	8:00	49.20	1.03	7.87	20.99	0.33	1.72	0.29	0.16	0.16	8.94	0.45	0.45	0.94	0.78	0.25	0.00	4.84
				8:00	9:00	63.55	2.99	16.15	21.81	0.66	4.18	1.52	0.90	0.90	7.01	0.49	0.49	1.03	0.90	0.25	0.00	4.26
				9:00	10:00	59.45	2.13	12.26	22.18	0.53	3.94	1.60	0.90	0.94	6.07	0.82	0.66	1.23	1.19	0.37	0.00	4.63
				10:00	11:00	53.30	4.43	9.72	13.78	0.86	8.16	2.71	0.94	0.94	3.90	0.49	0.53	1.07	1.07	0.33	0.00	4.39
				11:00	12:00	53.30	2.87	9.10	11.48	1.07	9.96	1.03	0.37	0.33	6.89	1.27	0.74	1.48	1.56	0.41	0.00	4.76
				12:00	13:00	51.25	3.61	12.05	14.31	0.78	6.60	0.94	0.37	0.37	5.17	1.44	0.37	0.62	0.74	0.16	0.00	3.73
				13:00	14:00	53.30	2.62	8.04	9.47	0.90	8.12	3.24	1.19	1.19	4.96	0.37	1.44	2.79	2.87	0.82	0.00	5.29
				14:00	15:00	51.25	2.26	7.50	13.04	0.82	7.59	2.50	0.86	0.86	5.29	0.62	0.94	1.85	1.85	0.57	0.00	4.72
				15:00	16:00	59.45	3.81	10.13	15.46	0.90	8.45	3.16	1.19	1.15	4.80	1.19	0.78	1.60	1.56	0.49	0.00	4.80
				16:00	17:00	53.30	6.07	11.32	6.77	0.70	8.49	1.48	0.33	0.33	3.61	0.29	1.48	2.87	3.24	0.86	0.00	5.49
				17:00	18:00	63.55	9.47	17.14	8.77	0.66	6.85	2.99	0.78	0.70	3.90	1.03	1.03	1.93	1.93	0.66	0.00	5.74
				18:00	19:00	53.30	8.49	16.07	8.28	0.37	4.80	1.76	0.41	0.41	1.56	0.70	1.23	2.13	2.34	0.66	0.00	4.10
				19:00	20:00	47.15	4.02	14.35	15.54	0.08	1.19	0.37	0.08	0.12	5.33	0.41	0.33	0.66	0.70	0.12	0.00	3.85
				20:00	21:00	41.00	3.28	9.55	15.46	0.08	0.82	0.12	0.08	0.12	7.01	0.16	0.12	0.25	0.16	0.08	0.00	3.69
				21:00	22:00	36.90	2.21	7.59	15.58	0.08	1.03	0.25	0.08	0.12	5.90	0.00	0.04	0.08	0.00	0.04	0.00	3.90
				22:00	23:00	36.90	2.87	5.70	15.29	0.08	0.57	0.00	0.00	0.00	6.11	0.21	0.16	0.33	0.16	0.12	0.00	5.29
				23:00	0:00	30.75	2.50	4.55	12.63	0.08	0.57	0.00	0.00	0.00	5.04	0.21	0.16	0.33	0.16	0.12	0.00	4.39

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L133	Chi Kiang Street (EB)	Chatham Road North	Ha Heung Road	0:00	1:00	9.90	0.33	3.00	5.35	0.00	0.30	0.00	0.00	0.00	0.33	0.03	0.03	0.10	0.07	0.03	0.00	0.33
				1:00	2:00	6.60	0.20	2.18	3.93	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.95	0.13	1.49	3.20	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.95	0.17	1.62	2.90	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.30	0.10	0.99	2.08	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.95	0.13	1.35	2.84	0.00	0.30	0.00	0.00	0.03	0.00	0.03	0.03	0.10	0.10	0.03	0.00	0.00
				6:00	7:00	8.25	0.17	1.95	4.19	0.03	0.46	0.10	0.00	0.03	0.30	0.07	0.10	0.20	0.17	0.07	0.00	0.43
				7:00	8:00	23.10	0.43	7.66	8.51	0.13	1.25	0.30	0.17	0.13	1.12	0.23	0.23	0.50	0.50	0.20	0.00	1.75
				8:00	9:00	36.30	1.12	15.64	8.91	0.26	2.97	1.58	0.86	0.79	0.86	0.30	0.30	0.50	0.50	0.23	0.00	1.49
				9:00	10:00	33.00	0.79	11.95	9.04	0.20	2.84	1.68	0.86	0.83	0.76	0.46	0.33	0.66	0.69	0.26	0.00	1.65
				10:00	11:00	29.70	0.76	8.51	7.59	0.33	4.92	2.18	1.12	1.06	0.63	0.17	0.17	0.36	0.40	0.17	0.00	1.35
				11:00	12:00	26.40	0.46	7.89	6.07	0.40	5.81	0.79	0.43	0.33	1.06	0.40	0.23	0.46	0.53	0.17	0.00	1.39
				12:00	13:00	26.40	0.56	10.10	7.43	0.26	3.76	0.69	0.43	0.33	0.76	0.43	0.10	0.20	0.23	0.07	0.00	1.06
				13:00	14:00	28.05	0.46	6.96	5.15	0.33	4.82	2.51	1.29	1.29	0.79	0.13	0.46	0.92	1.02	0.33	0.00	1.58
				14:00	15:00	26.40	0.36	6.44	6.96	0.30	4.42	1.88	0.99	0.89	0.83	0.20	0.30	0.59	0.63	0.23	0.00	1.39
				15:00	16:00	31.35	0.63	8.61	8.15	0.33	4.85	2.34	1.25	1.16	0.73	0.36	0.30	0.50	0.53	0.20	0.00	1.42
				16:00	17:00	26.40	0.63	8.78	5.97	0.30	4.49	0.96	0.43	0.43	0.83	0.07	0.33	0.73	0.83	0.23	0.00	1.42
				17:00	18:00	33.00	0.92	13.30	7.46	0.23	3.53	1.88	0.99	0.92	0.83	0.20	0.20	0.46	0.46	0.17	0.00	1.45
				18:00	19:00	28.05	0.86	12.28	7.06	0.13	2.48	1.06	0.59	0.59	0.33	0.13	0.26	0.53	0.56	0.17	0.00	1.02
				19:00	20:00	26.40	0.73	12.90	8.58	0.03	0.73	0.33	0.10	0.20	0.86	0.13	0.10	0.23	0.23	0.07	0.00	1.19
				20:00	21:00	21.45	0.59	8.84	8.68	0.03	0.53	0.10	0.10	0.10	1.12	0.03	0.03	0.07	0.07	0.03	0.00	1.12
				21:00	22:00	18.15	0.36	6.47	8.15	0.03	0.59	0.20	0.10	0.10	0.92	0.00	0.00	0.03	0.00	0.03	0.00	1.16
				22:00	23:00	16.50	0.46	4.85	8.02	0.03	0.33	0.00	0.00	0.00	0.92	0.07	0.07	0.10	0.07	0.07	0.00	1.52
				L134	Chi Kiang Street (EB)	Ha Heung Road	To Kwa Wan Road	23:00	0:00	14.85	0.46	4.19	7.19	0.03	0.36	0.00	0.00	0.00	0.86	0.07	0.07	0.10
0:00	1:00	7.75	0.19					2.05	4.25	0.00	0.25	0.00	0.00	0.00	0.40	0.03	0.03	0.09	0.03	0.03	0.00	0.40
1:00	2:00	4.65	0.12					1.36	2.95	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	3.10	0.06					0.84	2.11	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	3.10	0.09					0.90	1.92	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	3.10	0.09					0.81	2.05	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	3.10	0.09					0.74	1.89	0.00	0.19	0.00	0.00	0.03	0.00	0.03	0.03	0.03	0.03	0.03	0.00	0.00
6:00	7:00	6.20	0.12					1.21	3.19	0.03	0.37	0.06	0.00	0.03	0.34	0.03	0.06	0.12	0.12	0.06	0.00	0.43
7:00	8:00	15.50	0.22					4.43	5.77	0.09	0.87	0.25	0.09	0.09	1.05	0.12	0.12	0.28	0.28	0.19	0.00	1.64
8:00	9:00	23.25	0.56					8.96	6.14	0.19	2.05	1.15	0.47	0.43	0.81	0.16	0.16	0.31	0.31	0.16	0.00	1.43
9:00	10:00	21.70	0.40					6.94	6.29	0.19	1.95	1.21	0.47	0.47	0.74	0.25	0.19	0.37	0.40	0.25	0.00	1.58
10:00	11:00	18.60	0.34					4.68	4.90	0.25	3.32	1.49	0.59	0.59	0.59	0.06	0.09	0.19	0.19	0.09	0.00	1.24
11:00	12:00	17.05	0.22					4.31	4.00	0.31	3.97	0.56	0.22	0.19	0.99	0.22	0.12	0.25	0.28	0.12	0.00	1.30
12:00	13:00	17.05	0.28					5.74	4.99	0.22	2.60	0.47	0.22	0.19	0.74	0.25	0.06	0.09	0.12	0.06	0.00	1.02
13:00	14:00	18.60	0.22					4.00	3.50	0.31	3.41	1.83	0.71	0.71	0.78	0.06	0.25	0.50	0.56	0.25	0.00	1.52
14:00	15:00	17.05	0.16					3.60	4.53	0.25	3.01	1.33	0.53	0.53	0.78	0.09	0.16	0.31	0.34	0.16	0.00	1.30
15:00	16:00	20.15	0.28					4.77	5.43	0.28	3.35	1.67	0.71	0.62	0.71	0.19	0.12	0.28	0.28	0.12	0.00	1.33
16:00	17:00	15.50	0.28					4.46	3.53	0.22	2.91	0.62	0.22	0.22	0.71	0.03	0.16	0.34	0.37	0.12	0.00	1.30
17:00	18:00	20.15	0.43					7.10	4.68	0.22	2.45	1.30	0.56	0.53	0.78	0.09	0.09	0.22	0.22	0.09	0.00	1.40
18:00	19:00	17.05	0.40					6.63	4.53	0.09	1.74	0.78	0.34	0.34	0.34	0.06	0.12	0.28	0.31	0.09	0.00	0.99
19:00	20:00	15.50	0.31					6.76	5.33	0.03	0.47	0.22	0.06	0.09	0.74	0.06	0.06	0.12	0.12	0.06	0.00	1.05
20:00	21:00	13.95	0.28					4.90	5.86	0.03	0.37	0.06	0.06	0.06	1.09	0.03	0.03	0.03	0.03	0.03	0.00	1.09
21:00	22:00	12.40	0.16					3.78	5.70	0.03	0.43	0.12	0.06	0.06	0.90	0.00	0.00	0.03	0.00	0.00	0.00	1.12
22:00	23:00	10.85	0.22					2.67	5.24	0.03	0.22	0.00	0.00	0.00	0.87	0.03	0.03	0.06	0.03	0.03	0.00	1.43
23:00	0:00	9.30	0.19	2.20	4.46	0.03	0.25	0.00	0.00	0.00	0.74	0.03	0.03	0.06	0.03	0.03	0.00	1.24				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L135	Anhui Street (WB)	Chatham Road North	Ko Shan Road	0:00	1:00	4.30	0.09	1.08	2.97	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.15	0.09	0.52	1.46	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	0.09	0.47	1.51	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.09	0.47	1.51	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.00	0.47	1.55	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.00	0.47	1.51	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.15	0.04	0.39	1.46	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				7:00	8:00	6.45	0.13	2.67	2.80	0.00	0.47	0.04	0.04	0.04	0.00	0.00	0.00	0.13	0.00	0.13	0.00	0.00	
				8:00	9:00	8.60	0.30	4.30	2.37	0.00	0.90	0.13	0.26	0.17	0.00	0.00	0.00	0.09	0.00	0.09	0.00	0.00	
				9:00	10:00	8.60	0.26	3.61	2.71	0.00	0.99	0.17	0.30	0.22	0.00	0.00	0.00	0.17	0.00	0.17	0.00	0.00	
				10:00	11:00	6.45	0.13	1.63	2.32	0.00	1.38	0.43	0.30	0.17	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				11:00	12:00	6.45	0.09	1.72	2.19	0.00	1.94	0.22	0.13	0.04	0.00	0.00	0.00	0.09	0.00	0.04	0.00	0.00	
				12:00	13:00	6.45	0.09	2.19	2.58	0.00	1.20	0.13	0.13	0.04	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				13:00	14:00	6.45	0.09	1.55	1.72	0.00	1.59	0.60	0.39	0.26	0.00	0.00	0.00	0.13	0.00	0.13	0.00	0.00	
				14:00	15:00	6.45	0.04	1.42	2.37	0.00	1.46	0.47	0.30	0.22	0.00	0.00	0.00	0.09	0.00	0.09	0.00	0.00	
				15:00	16:00	8.60	0.13	2.06	3.10	0.00	1.76	0.60	0.43	0.26	0.00	0.00	0.00	0.13	0.00	0.13	0.00	0.00	
				16:00	17:00	6.45	0.04	1.16	2.75	0.00	1.68	0.43	0.17	0.04	0.00	0.00	0.00	0.13	0.00	0.04	0.00	0.00	
				17:00	18:00	6.45	0.04	1.42	2.84	0.00	1.03	0.60	0.30	0.13	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				18:00	19:00	6.45	0.04	1.59	3.18	0.00	0.86	0.39	0.17	0.13	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				19:00	20:00	6.45	0.17	2.80	3.01	0.00	0.26	0.04	0.04	0.04	0.00	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				20:00	21:00	4.30	0.09	1.55	2.49	0.00	0.13	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	4.30	0.04	1.33	2.67	0.00	0.17	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	4.30	0.09	1.16	2.97	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	4.30	0.13	1.12	2.92	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
L136	Chatham Road North (SB)	Chi Kiang Street	Shek Tong Street	0:00	1:00	78.30	4.96	22.45	37.32	0.00	1.83	0.00	0.00	0.00	2.87	0.26	0.26	0.52	0.26	0.26	0.00	7.31	
				1:00	2:00	52.20	3.65	16.44	28.71	0.00	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.31	
				2:00	3:00	39.15	2.35	11.48	23.23	0.00	1.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.04	
				3:00	4:00	39.15	3.13	12.01	20.88	0.00	1.83	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.31	
				4:00	5:00	26.10	1.83	7.31	15.14	0.00	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.04	
				5:00	6:00	39.15	2.61	10.44	22.19	0.00	2.09	0.00	0.00	0.00	0.00	0.26	0.26	0.78	0.26	0.26	0.00	0.00	
				6:00	7:00	65.25	2.87	14.36	29.75	0.26	2.87	0.78	0.00	0.00	2.61	0.52	0.52	1.04	0.78	0.26	0.00	8.61	
				7:00	8:00	195.75	6.53	46.46	63.16	1.04	11.48	3.13	0.52	0.52	6.79	1.57	1.57	3.13	2.87	1.04	0.00	45.94	
				8:00	9:00	287.10	18.27	94.74	66.29	2.09	27.41	15.92	2.87	2.87	5.48	1.83	1.83	3.39	2.87	1.04	0.00	40.19	
				9:00	10:00	261.00	12.79	70.73	66.03	1.83	25.58	16.18	2.87	2.87	4.44	2.87	2.35	4.18	3.92	1.31	0.00	43.07	
				10:00	11:00	221.85	11.75	61.86	52.46	2.35	32.10	14.36	3.65	3.65	5.22	0.78	1.04	1.83	1.57	0.78	0.00	28.45	
				11:00	12:00	208.80	7.57	58.20	43.59	3.13	39.15	5.22	1.31	1.04	9.66	2.35	1.31	2.35	2.35	0.78	0.00	30.80	
				12:00	13:00	208.80	9.14	75.69	53.24	2.09	25.58	4.70	1.31	1.04	7.05	2.35	0.52	1.04	1.04	0.26	0.00	23.75	
				13:00	14:00	208.80	6.79	51.16	36.02	2.61	31.84	16.97	4.18	4.18	6.79	0.78	2.61	4.44	4.44	1.83	0.00	34.19	
				14:00	15:00	208.80	6.00	49.33	50.37	2.61	30.54	13.57	3.39	3.13	7.83	1.04	1.57	3.39	3.13	1.04	0.00	31.84	
				15:00	16:00	234.90	9.66	62.38	57.42	2.61	32.36	15.92	4.18	3.92	6.53	2.09	1.31	2.61	2.35	1.04	0.00	30.54	
				16:00	17:00	195.75	9.40	77.78	38.89	2.09	20.10	3.92	1.31	1.31	9.40	0.26	1.31	2.09	2.61	1.31	0.00	24.01	
				17:00	18:00	247.95	13.83	116.41	48.29	1.83	15.66	7.57	2.87	2.61	9.66	0.78	0.78	1.31	1.31	0.78	0.00	24.27	
				18:00	19:00	208.80	12.27	106.23	44.89	0.78	10.70	4.44	1.57	1.57	3.65	0.52	1.04	1.57	1.57	0.78	0.00	17.23	
				19:00	20:00	208.80	10.70	93.44	59.77	0.26	4.70	2.09	0.26	0.52	7.57	0.78	0.52	1.04	1.04	0.26	0.00	25.84	
				20:00	21:00	169.65	8.87	61.60	58.99	0.26	3.13	0.78	0.26	0.26	9.92	0.26	0.26	0.52	0.26	0.26	0.00	24.01	
				21:00	22:00	156.60	6.00	49.33	60.29	0.26	4.18	1.31	0.26	0.26	8.35	0.00	0.00	0.26	0.00	0.00	0.00	26.10	
				22:00	23:00	156.60	7.83	38.11	61.34	0.26	2.09	0.00	0.00	0.00	8.87	0.26	0.26	0.52	0.26	0.26	0.00	36.54	
				23:00	0:00	130.50	7.05	30.54	50.63	0.26	2.61	0.00	0.00	0.00	7.57	0.26	0.26	0.52	0.26	0.26	0.00	30.28	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L137	Chatham Road North (NB)	Kiang Hsi Street	Anhui Street	0:00	1:00	30.25	1.54	7.76	15.40	0.00	0.72	0.00	0.00	0.00	2.48	0.11	0.06	0.22	0.11	0.06	0.00	1.82
				1:00	2:00	16.50	0.88	4.73	9.85	0.00	0.61	0.00	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.22
				2:00	3:00	16.50	0.77	4.40	10.34	0.00	0.44	0.00	0.00	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.28
				3:00	4:00	11.00	0.72	3.19	6.33	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.22
				4:00	5:00	11.00	0.61	2.92	6.77	0.00	0.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.06	0.00	0.00	0.28
				5:00	6:00	11.00	0.50	2.75	6.66	0.00	0.61	0.00	0.00	0.06	0.00	0.06	0.06	0.17	0.11	0.06	0.00	0.00
				6:00	7:00	24.75	0.83	4.90	11.94	0.11	1.21	0.22	0.00	0.06	2.26	0.17	0.17	0.39	0.33	0.17	0.00	2.04
				7:00	8:00	68.75	0.99	15.95	28.16	0.44	2.70	0.50	0.28	0.28	9.35	0.33	0.33	0.83	0.72	0.33	0.00	7.59
				8:00	9:00	96.25	2.81	33.50	30.03	0.88	6.66	2.59	1.43	1.38	7.43	0.39	0.39	0.88	0.77	0.39	0.00	6.77
				9:00	10:00	88.00	1.93	25.36	30.31	0.77	6.27	2.75	1.43	1.43	6.49	0.61	0.55	1.16	1.05	0.55	0.00	7.37
				10:00	11:00	79.75	3.36	21.07	20.79	1.21	12.43	4.24	1.76	1.76	4.29	0.33	0.33	0.72	0.72	0.22	0.00	6.55
				11:00	12:00	77.00	2.15	19.64	17.27	1.54	15.18	1.54	0.72	0.55	7.54	0.83	0.44	1.10	1.10	0.33	0.00	7.10
				12:00	13:00	77.00	2.70	25.63	21.40	1.10	10.01	1.38	0.72	0.55	5.61	0.94	0.22	0.44	0.55	0.17	0.00	5.61
				13:00	14:00	77.00	1.98	17.71	14.58	1.32	12.65	5.01	2.09	2.15	5.56	0.33	0.99	1.98	1.98	0.61	0.00	8.09
				14:00	15:00	74.25	1.71	16.34	19.58	1.21	11.61	3.80	1.54	1.54	5.78	0.44	0.61	1.27	1.27	0.44	0.00	7.15
				15:00	16:00	88.00	2.92	22.00	23.54	1.32	12.93	4.73	2.09	2.04	5.28	0.77	0.55	1.16	1.05	0.33	0.00	7.32
				16:00	17:00	77.00	4.24	25.63	12.32	0.99	12.76	2.09	0.72	0.72	4.24	0.17	0.99	1.76	1.98	0.39	0.00	8.03
				17:00	18:00	93.50	6.38	37.62	15.35	0.94	9.90	4.13	1.60	1.49	4.29	0.66	0.66	1.10	1.10	0.28	0.00	8.03
				18:00	19:00	79.75	5.72	35.64	14.74	0.50	7.10	2.42	0.88	0.94	1.76	0.50	0.72	1.27	1.43	0.33	0.00	5.83
				19:00	20:00	74.25	3.03	31.30	23.49	0.17	1.87	0.61	0.17	0.28	5.89	0.28	0.22	0.44	0.50	0.17	0.00	5.89
				20:00	21:00	63.25	2.53	21.18	23.54	0.11	1.32	0.28	0.17	0.17	7.87	0.11	0.06	0.17	0.11	0.06	0.00	5.61
				21:00	22:00	57.75	1.76	16.89	23.76	0.11	1.65	0.39	0.17	0.17	6.71	0.00	0.00	0.06	0.00	0.06	0.00	6.05
				22:00	23:00	55.00	2.20	12.60	23.54	0.11	0.83	0.00	0.00	0.00	6.88	0.17	0.11	0.22	0.11	0.11	0.00	8.14
				L138	Ko Shan Road (SB)	Anhui Street	Kiang Hsi Street	23:00	0:00	44.00	1.87	9.68	18.70	0.11	0.88	0.00	0.00	0.00	5.56	0.17	0.11	0.22
0:00	1:00	7.50	0.15					1.90	4.60	0.00	0.15	0.00	0.00	0.00	0.35	0.05	0.05	0.15	0.05	0.05	0.00	0.00
1:00	2:00	5.00	0.15					1.35	3.40	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	5.00	0.10					1.25	3.55	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	2.50	0.05					0.70	1.70	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	2.50	0.05					0.60	1.80	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	2.50	0.05					0.55	1.60	0.00	0.05	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.05	0.05	0.00	0.00
6:00	7:00	5.00	0.10					0.95	3.00	0.00	0.10	0.05	0.00	0.00	0.25	0.05	0.10	0.15	0.15	0.10	0.00	0.00
7:00	8:00	10.00	0.15					3.35	3.85	0.05	0.40	0.10	0.05	0.05	0.85	0.20	0.15	0.30	0.30	0.20	0.00	0.00
8:00	9:00	15.00	0.45					6.85	3.95	0.05	1.00	0.45	0.15	0.15	0.65	0.20	0.20	0.35	0.35	0.20	0.00	0.00
9:00	10:00	15.00	0.35					5.75	4.45	0.05	1.05	0.50	0.15	0.15	0.60	0.35	0.30	0.50	0.50	0.30	0.00	0.00
10:00	11:00	15.00	0.35					4.70	5.55	0.05	1.55	0.65	0.20	0.20	0.55	0.10	0.20	0.35	0.35	0.20	0.00	0.00
11:00	12:00	12.50	0.20					3.80	4.10	0.10	1.60	0.20	0.05	0.05	0.85	0.35	0.20	0.40	0.40	0.20	0.00	0.00
12:00	13:00	15.00	0.30					5.60	5.75	0.05	1.20	0.25	0.05	0.05	0.70	0.45	0.10	0.15	0.25	0.10	0.00	0.00
13:00	14:00	12.50	0.20					3.45	3.40	0.05	1.30	0.65	0.20	0.20	0.60	0.10	0.40	0.75	0.80	0.40	0.00	0.00
14:00	15:00	12.50	0.15					3.20	4.75	0.05	1.25	0.50	0.15	0.15	0.65	0.15	0.25	0.50	0.50	0.25	0.00	0.00
15:00	16:00	15.00	0.25					4.35	5.75	0.05	1.40	0.65	0.20	0.20	0.65	0.30	0.20	0.40	0.40	0.20	0.00	0.00
16:00	17:00	15.00	0.25					4.95	5.70	0.05	1.05	0.20	0.05	0.05	0.65	0.05	0.35	0.65	0.75	0.25	0.00	0.00
17:00	18:00	17.50	0.40					6.95	6.70	0.05	0.75	0.40	0.10	0.10	0.65	0.20	0.20	0.40	0.40	0.20	0.00	0.00
18:00	19:00	17.50	0.40					7.00	7.00	0.05	0.55	0.30	0.05	0.05	0.30	0.20	0.30	0.50	0.55	0.25	0.00	0.00
19:00	20:00	15.00	0.35					6.65	6.10	0.00	0.25	0.10	0.00	0.00	0.75	0.10	0.10	0.25	0.25	0.10	0.00	0.00
20:00	21:00	12.50	0.30					4.55	6.15	0.00	0.20	0.05	0.00	0.00	1.00	0.05	0.05	0.05	0.05	0.05	0.00	0.00
21:00	22:00	10.00	0.15					3.25	5.65	0.00	0.15	0.05	0.00	0.00	0.70	0.00	0.00	0.05	0.00	0.00	0.00	0.00
22:00	23:00	10.00	0.20					2.60	5.95	0.00	0.10	0.00	0.00	0.00	0.85	0.05	0.05	0.10	0.05	0.05	0.00	0.00
23:00	0:00	7.50	0.20	1.90	4.40	0.00	0.10	0.00	0.00	0.00	0.60	0.05	0.05	0.10	0.05	0.05	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L139	Ko Shan Road (NB)	Kiang Hsi Street	Anhui Street	0:00	1:00	5.00	0.15	0.50	3.75	0.00	0.05	0.00	0.00	0.00	0.25	0.05	0.05	0.10	0.05	0.05	0.00	0.00	
				1:00	2:00	5.00	0.20	0.55	4.15	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.50	0.05	0.25	2.15	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.50	0.05	0.30	2.10	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.50	0.10	0.25	2.05	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.50	0.05	0.20	2.00	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.05	0.05	0.00	0.00
				6:00	7:00	5.00	0.15	0.40	3.45	0.00	0.15	0.00	0.00	0.00	0.30	0.05	0.05	0.20	0.20	0.05	0.00	0.00	
				7:00	8:00	10.00	0.15	1.25	5.50	0.05	0.40	0.15	0.00	0.00	1.10	0.20	0.20	0.40	0.40	0.20	0.00	0.00	
				8:00	9:00	12.50	0.45	2.60	5.70	0.05	0.85	0.50	0.05	0.05	0.80	0.20	0.20	0.45	0.40	0.20	0.00	0.00	
				9:00	10:00	10.00	0.25	1.60	4.90	0.05	0.65	0.40	0.05	0.05	0.55	0.25	0.20	0.45	0.40	0.20	0.00	0.00	
				10:00	11:00	10.00	0.35	1.35	5.20	0.05	1.15	0.45	0.05	0.05	0.45	0.10	0.10	0.30	0.25	0.15	0.00	0.00	
				11:00	12:00	10.00	0.20	1.30	4.40	0.10	1.45	0.20	0.00	0.00	0.90	0.30	0.15	0.45	0.40	0.15	0.00	0.00	
				12:00	13:00	10.00	0.25	1.75	5.30	0.05	0.95	0.15	0.00	0.00	0.65	0.35	0.10	0.20	0.20	0.05	0.00	0.00	
				13:00	14:00	10.00	0.20	1.15	3.55	0.05	1.20	0.60	0.05	0.10	0.65	0.10	0.35	0.85	0.80	0.35	0.00	0.00	
				14:00	15:00	10.00	0.15	1.10	4.85	0.05	1.10	0.40	0.05	0.05	0.65	0.15	0.20	0.55	0.50	0.20	0.00	0.00	
				15:00	16:00	12.50	0.35	1.55	6.05	0.05	1.30	0.65	0.05	0.05	0.70	0.30	0.25	0.50	0.45	0.25	0.00	0.00	
				16:00	17:00	12.50	0.40	1.90	5.55	0.05	1.20	0.25	0.00	0.00	0.70	0.05	0.35	0.90	0.85	0.30	0.00	0.00	
				17:00	18:00	12.50	0.55	2.45	6.05	0.05	0.80	0.35	0.05	0.05	0.65	0.20	0.20	0.50	0.40	0.20	0.00	0.00	
				18:00	19:00	12.50	0.55	2.45	6.35	0.05	0.60	0.25	0.05	0.05	0.30	0.15	0.30	0.60	0.55	0.25	0.00	0.00	
				19:00	20:00	10.00	0.30	2.10	6.05	0.00	0.15	0.05	0.00	0.00	0.70	0.10	0.10	0.20	0.20	0.05	0.00	0.00	
				20:00	21:00	10.00	0.30	1.60	6.70	0.00	0.10	0.00	0.00	0.00	1.05	0.05	0.05	0.05	0.05	0.05	0.00	0.00	
				21:00	22:00	10.00	0.20	1.35	7.25	0.00	0.20	0.05	0.00	0.00	0.90	0.00	0.00	0.05	0.00	0.00	0.00	0.00	
				22:00	23:00	10.00	0.25	1.05	7.20	0.00	0.15	0.00	0.00	0.00	1.00	0.05	0.05	0.15	0.05	0.05	0.00	0.00	
				23:00	0:00	7.50	0.25	0.75	5.35	0.00	0.10	0.00	0.00	0.00	0.75	0.05	0.05	0.10	0.05	0.05	0.00	0.00	
L140	Kiang Hsi Street (EB)	Ko Shan Road	Chatham Road North	0:00	1:00	3.80	0.11	1.63	1.71	0.00	0.08	0.00	0.00	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00		
				1:00	2:00	1.90	0.04	0.76	0.87	0.00	0.04	0.00	0.00	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	1.90	0.04	0.72	0.91	0.00	0.00	0.00	0.00	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.90	0.08	0.87	0.95	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.90	0.11	0.76	1.03	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	1.90	0.08	0.76	0.99	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	1.90	0.04	0.65	0.91	0.00	0.04	0.04	0.00	0.00	0.15	0.00	0.00	0.04	0.04	0.00	0.00	0.00	
				7:00	8:00	7.60	0.00	2.20	3.80	0.00	0.19	0.00	0.11	0.11	0.65	0.11	0.11	0.15	0.15	0.00	0.00	0.00	
				8:00	9:00	9.50	0.00	3.91	3.53	0.00	0.42	0.00	0.42	0.34	0.42	0.08	0.08	0.15	0.15	0.00	0.00	0.00	
				9:00	10:00	9.50	0.00	3.27	3.88	0.00	0.42	0.00	0.49	0.38	0.42	0.15	0.11	0.19	0.19	0.00	0.00	0.00	
				10:00	11:00	7.60	0.23	3.15	1.67	0.00	0.49	1.03	0.30	0.27	0.30	0.04	0.04	0.04	0.04	0.00	0.00	0.00	
				11:00	12:00	5.70	0.11	2.55	1.22	0.00	0.53	0.34	0.11	0.08	0.49	0.08	0.04	0.08	0.08	0.00	0.00	0.00	
				12:00	13:00	7.60	0.19	3.95	1.82	0.00	0.42	0.38	0.15	0.08	0.46	0.08	0.00	0.04	0.04	0.00	0.00	0.00	
				13:00	14:00	7.60	0.15	2.66	1.25	0.00	0.53	1.29	0.38	0.38	0.42	0.04	0.11	0.19	0.15	0.04	0.00	0.00	
				14:00	15:00	5.70	0.08	2.01	1.33	0.00	0.38	0.80	0.23	0.19	0.38	0.04	0.04	0.11	0.08	0.04	0.00	0.00	
				15:00	16:00	7.60	0.15	2.89	1.71	0.00	0.49	1.06	0.34	0.27	0.38	0.08	0.04	0.08	0.08	0.04	0.00	0.00	
				16:00	17:00	5.70	0.34	3.76	0.00	0.00	0.23	0.76	0.04	0.04	0.42	0.00	0.00	0.08	0.00	0.04	0.00	0.00	
				17:00	18:00	9.50	0.57	6.35	0.00	0.00	0.23	1.67	0.08	0.08	0.46	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				18:00	19:00	7.60	0.53	5.70	0.00	0.00	0.11	0.91	0.04	0.04	0.19	0.00	0.00	0.04	0.00	0.04	0.00	0.00	
				19:00	20:00	7.60	0.19	4.64	1.90	0.00	0.08	0.15	0.04	0.04	0.46	0.04	0.00	0.04	0.04	0.00	0.00	0.00	
				20:00	21:00	5.70	0.19	2.96	1.82	0.00	0.04	0.04	0.04	0.04	0.57	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	5.70	0.11	2.74	2.09	0.00	0.04	0.11	0.04	0.04	0.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	3.80	0.11	1.56	1.67	0.00	0.04	0.00	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	3.80	0.15	1.52	1.63	0.00	0.04	0.00	0.00	0.00	0.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L141	Chatham Road North (NB)	Shan Si Street	Kiang Hsi Street	0:00	1:00	55.00	3.19	12.98	28.38	0.00	1.43	0.00	0.00	0.00	4.18	0.11	0.11	0.44	0.22	0.11	0.00	3.85
				1:00	2:00	27.50	1.65	7.37	17.05	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.44
				2:00	3:00	27.50	1.43	6.82	17.93	0.00	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.55
				3:00	4:00	22.00	1.54	5.72	12.98	0.00	1.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.55
				4:00	5:00	16.50	0.99	3.96	10.45	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.11	0.00	0.00	0.44
				5:00	6:00	22.00	1.21	5.06	13.31	0.00	1.32	0.00	0.00	0.11	0.00	0.11	0.11	0.44	0.22	0.11	0.00	0.00
				6:00	7:00	44.00	1.76	7.81	21.45	0.22	2.20	0.33	0.00	0.11	3.74	0.33	0.33	0.66	0.55	0.33	0.00	4.18
				7:00	8:00	121.00	2.42	26.40	48.73	0.77	4.84	0.99	0.44	0.33	16.06	0.66	0.66	1.32	1.10	0.44	0.00	15.84
				8:00	9:00	165.00	6.82	54.23	51.04	1.54	11.55	4.84	1.98	1.98	12.54	0.66	0.66	1.54	1.21	0.55	0.00	13.86
				9:00	10:00	154.00	4.95	42.02	52.25	1.43	11.22	5.28	1.98	2.20	11.22	1.10	0.77	1.87	1.65	0.66	0.00	15.40
				10:00	11:00	137.50	6.49	33.55	36.85	2.20	22.11	6.38	2.75	2.86	7.15	0.44	0.55	1.21	1.21	0.44	0.00	13.31
				11:00	12:00	137.50	4.40	32.23	31.46	2.86	27.83	2.53	1.10	0.99	12.76	1.54	0.88	1.76	1.76	0.66	0.00	14.74
				12:00	13:00	137.50	5.39	42.35	39.49	2.09	18.48	2.20	1.10	0.99	9.68	1.65	0.33	0.88	0.88	0.33	0.00	11.66
				13:00	14:00	132.00	3.96	27.94	25.63	2.42	22.22	7.48	3.30	3.52	9.13	0.44	1.76	3.30	3.41	1.21	0.00	16.28
				14:00	15:00	132.00	3.41	26.62	35.86	2.20	21.12	5.83	2.64	2.64	9.90	0.66	1.10	2.20	2.20	0.88	0.00	14.74
				15:00	16:00	154.00	5.50	35.42	42.35	2.42	23.43	7.37	3.41	3.41	8.91	1.43	0.88	1.87	1.87	0.66	0.00	15.07
				16:00	17:00	137.50	7.59	40.70	24.53	2.09	23.65	2.86	1.21	1.21	7.04	0.33	1.65	3.19	3.74	1.21	0.00	16.50
				17:00	18:00	165.00	11.44	61.05	30.58	1.87	18.48	5.50	2.86	2.64	7.26	0.99	0.99	1.98	1.98	0.88	0.00	16.50
				18:00	19:00	137.50	10.01	56.32	28.60	0.99	12.76	3.19	1.54	1.65	2.86	0.88	1.21	2.20	2.64	0.99	0.00	11.66
				19:00	20:00	132.00	6.05	51.81	43.34	0.22	3.41	0.88	0.44	0.44	10.12	0.44	0.44	0.77	0.77	0.44	0.00	12.43
				20:00	21:00	110.00	4.95	33.88	42.02	0.22	2.31	0.33	0.33	0.33	13.09	0.22	0.11	0.33	0.22	0.11	0.00	11.55
				21:00	22:00	99.00	3.30	26.40	41.80	0.22	2.86	0.55	0.33	0.33	10.89	0.00	0.00	0.11	0.00	0.11	0.00	12.10
				22:00	23:00	99.00	4.51	20.46	42.46	0.22	1.54	0.00	0.00	0.00	11.55	0.22	0.22	0.55	0.22	0.22	0.00	16.83
				23:00	0:00	82.50	3.85	16.50	35.09	0.22	1.65	0.00	0.00	0.00	9.79	0.22	0.22	0.55	0.22	0.22	0.00	13.97
L142	Ko Shan Road (SB)	Kiang Hsi Street	Shan Si Street	0:00	1:00	15.00	0.30	4.50	9.00	0.00	0.30	0.00	0.00	0.00	0.70	0.00	0.00	0.20	0.00	0.00	0.00	
				1:00	2:00	10.00	0.20	3.10	6.50	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	10.00	0.20	2.70	6.90	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	5.00	0.10	1.50	3.30	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	5.00	0.10	1.40	3.40	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	5.00	0.10	1.30	3.40	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	
				6:00	7:00	10.00	0.10	2.40	6.20	0.00	0.40	0.00	0.00	0.00	0.60	0.00	0.00	0.10	0.10	0.10	0.00	
				7:00	8:00	20.00	0.60	7.90	6.00	0.10	1.10	0.20	0.00	0.00	2.10	0.20	0.20	0.60	0.50	0.50	0.00	
				8:00	9:00	30.00	1.50	15.20	5.90	0.10	2.40	1.30	0.00	0.00	1.50	0.20	0.20	0.70	0.50	0.50	0.00	
				9:00	10:00	25.00	1.00	10.90	5.70	0.10	2.20	1.30	0.00	0.00	1.20	0.30	0.20	0.90	0.60	0.60	0.00	
				10:00	11:00	25.00	0.60	9.10	9.40	0.10	2.90	1.00	0.10	0.10	1.00	0.10	0.10	0.20	0.20	0.10	0.00	
				11:00	12:00	25.00	0.40	9.00	8.30	0.20	3.70	0.40	0.00	0.00	1.90	0.20	0.10	0.40	0.20	0.20	0.00	
				12:00	13:00	25.00	0.40	10.80	9.30	0.10	2.30	0.30	0.00	0.00	1.30	0.20	0.00	0.10	0.10	0.10	0.00	
				13:00	14:00	25.00	0.40	8.70	7.40	0.10	3.30	1.20	0.10	0.10	1.50	0.10	0.30	0.80	0.50	0.50	0.00	
				14:00	15:00	25.00	0.20	7.80	9.60	0.10	3.00	1.00	0.10	0.10	1.60	0.10	0.10	0.50	0.40	0.40	0.00	
				15:00	16:00	30.00	0.50	10.40	11.80	0.10	3.30	1.20	0.10	0.10	1.40	0.20	0.10	0.40	0.20	0.20	0.00	
				16:00	17:00	25.00	0.00	10.40	11.10	0.10	2.00	0.20	0.00	0.00	1.20	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	35.00	0.00	16.60	14.80	0.10	1.60	0.30	0.10	0.10	1.40	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	30.00	0.00	14.70	13.30	0.10	1.00	0.20	0.10	0.10	0.50	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	25.00	0.40	12.60	9.80	0.00	0.40	0.10	0.00	0.00	1.30	0.10	0.00	0.10	0.10	0.10	0.00	
				20:00	21:00	25.00	0.50	10.20	11.70	0.00	0.40	0.00	0.00	0.00	2.10	0.00	0.00	0.10	0.00	0.00	0.00	
				21:00	22:00	20.00	0.20	7.20	10.70	0.00	0.30	0.10	0.00	0.00	1.50	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	20.00	0.40	5.90	11.50	0.00	0.20	0.00	0.00	0.00	1.80	0.00	0.00	0.10	0.00	0.10	0.00	
				23:00	0:00	15.00	0.30	4.30	8.70	0.00	0.20	0.00	0.00	0.00	1.30	0.00	0.00	0.10	0.00	0.10	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L143	Ko Shan Road (NB)	Shan Si Street	Kiang Hsi Street	0:00	1:00	20.00	0.50	4.60	12.90	0.00	0.40	0.00	0.00	0.00	1.30	0.00	0.00	0.10	0.10	0.10	0.00	0.00
				1:00	2:00	10.00	0.30	2.30	6.80	0.00	0.20	0.00	0.00	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	10.00	0.20	2.10	7.00	0.00	0.20	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	10.00	0.30	2.40	7.00	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	10.00	0.40	2.30	7.10	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	10.00	0.30	2.00	6.80	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.20	0.20	0.20	0.00	0.00
				6:00	7:00	15.00	0.40	2.70	9.40	0.00	0.60	0.10	0.00	0.00	1.10	0.10	0.10	0.20	0.20	0.10	0.00	0.00
				7:00	8:00	35.00	0.40	6.80	19.40	0.10	1.40	0.40	0.10	0.10	3.40	0.50	0.40	0.80	0.80	0.40	0.00	0.00
				8:00	9:00	45.00	1.00	13.30	19.20	0.10	3.20	1.60	0.50	0.50	2.50	0.50	0.50	0.90	0.90	0.30	0.00	0.00
				9:00	10:00	40.00	0.60	9.60	18.60	0.10	2.80	1.70	0.40	0.40	2.10	0.60	0.50	1.00	1.20	0.40	0.00	0.00
				10:00	11:00	40.00	1.20	10.80	15.50	0.20	5.40	3.00	0.40	0.40	1.90	0.10	0.10	0.40	0.30	0.30	0.00	0.00
				11:00	12:00	35.00	0.70	9.70	11.90	0.30	6.20	1.10	0.10	0.10	3.10	0.30	0.20	0.50	0.50	0.30	0.00	0.00
				12:00	13:00	40.00	0.90	13.60	16.20	0.20	4.40	1.00	0.10	0.10	2.50	0.40	0.10	0.20	0.20	0.10	0.00	0.00
				13:00	14:00	35.00	0.70	8.80	10.00	0.20	5.20	3.50	0.40	0.50	2.30	0.10	0.40	1.10	1.00	0.80	0.00	0.00
				14:00	15:00	35.00	0.50	8.10	13.60	0.20	4.80	2.60	0.30	0.30	2.50	0.20	0.20	0.70	0.60	0.40	0.00	0.00
				15:00	16:00	40.00	0.90	10.30	15.70	0.20	5.00	3.10	0.40	0.40	2.00	0.30	0.20	0.60	0.50	0.40	0.00	0.00
				16:00	17:00	35.00	1.40	14.20	9.80	0.20	4.60	1.50	0.10	0.10	2.30	0.00	0.00	0.30	0.00	0.50	0.00	0.00
				17:00	18:00	45.00	2.00	21.20	12.00	0.20	3.50	2.80	0.20	0.20	2.30	0.00	0.00	0.20	0.00	0.40	0.00	0.00
				18:00	19:00	40.00	1.90	20.30	11.70	0.10	2.50	1.70	0.10	0.10	1.00	0.00	0.00	0.20	0.00	0.40	0.00	0.00
				19:00	20:00	35.00	0.90	14.50	15.40	0.00	0.70	0.40	0.00	0.10	2.30	0.10	0.10	0.20	0.20	0.10	0.00	0.00
				20:00	21:00	30.00	0.80	9.80	15.50	0.00	0.50	0.10	0.00	0.00	3.00	0.00	0.00	0.10	0.10	0.10	0.00	0.00
				21:00	22:00	30.00	0.60	8.50	17.00	0.00	0.70	0.30	0.00	0.00	2.90	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	25.00	0.70	5.80	15.20	0.00	0.30	0.00	0.00	0.00	2.60	0.10	0.00	0.10	0.10	0.10	0.00	0.00
L144	Shan Si Street (EB)	Ko Shan Road	Chatham Road North	23:00	0:00	25.00	0.80	5.60	15.20	0.00	0.40	0.00	0.00	0.00	2.60	0.10	0.00	0.10	0.10	0.10	0.00	0.00
				0:00	1:00	4.30	0.17	1.03	2.45	0.00	0.04	0.00	0.00	0.00	0.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	2.15	0.13	0.56	1.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	0.04	0.56	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.09	0.60	1.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.09	0.56	1.51	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.09	0.52	1.46	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.30	0.17	0.82	2.45	0.00	0.09	0.00	0.00	0.00	0.69	0.00	0.00	0.00	0.00	0.09	0.00	0.00
				7:00	8:00	8.60	0.13	2.06	4.69	0.00	0.22	0.00	0.00	0.00	1.25	0.00	0.00	0.13	0.00	0.13	0.00	0.00
				8:00	9:00	10.75	0.26	3.96	4.73	0.00	0.47	0.09	0.09	0.09	0.90	0.00	0.00	0.09	0.00	0.09	0.00	0.00
				9:00	10:00	8.60	0.17	2.67	4.21	0.00	0.34	0.09	0.09	0.09	0.69	0.00	0.00	0.13	0.00	0.13	0.00	0.00
				10:00	11:00	6.45	0.30	2.11	2.54	0.00	0.43	0.09	0.09	0.09	0.73	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				11:00	12:00	6.45	0.22	2.02	2.15	0.00	0.56	0.04	0.04	0.04	1.29	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				12:00	13:00	8.60	0.34	3.18	3.27	0.00	0.43	0.04	0.04	0.04	1.20	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				13:00	14:00	6.45	0.22	2.02	2.06	0.00	0.56	0.09	0.09	0.09	1.12	0.00	0.00	0.09	0.00	0.13	0.00	0.00
				14:00	15:00	6.45	0.17	1.72	2.62	0.00	0.47	0.09	0.09	0.09	1.08	0.00	0.00	0.04	0.00	0.09	0.00	0.00
				15:00	16:00	8.60	0.30	2.67	3.53	0.00	0.60	0.09	0.09	0.09	1.12	0.00	0.00	0.04	0.00	0.09	0.00	0.00
				16:00	17:00	6.45	0.52	2.84	1.29	0.00	0.30	0.04	0.00	0.00	1.42	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				17:00	18:00	8.60	0.77	4.26	1.68	0.00	0.26	0.04	0.04	0.04	1.46	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				18:00	19:00	6.45	0.60	3.57	1.42	0.00	0.13	0.04	0.04	0.04	0.56	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				19:00	20:00	8.60	0.34	3.66	3.31	0.00	0.09	0.00	0.00	0.00	1.16	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				20:00	21:00	6.45	0.22	2.06	2.80	0.00	0.04	0.00	0.00	0.00	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	6.45	0.22	1.85	3.10	0.00	0.04	0.00	0.00	0.00	1.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	6.45	0.26	1.42	3.35	0.00	0.04	0.00	0.00	0.00	1.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	6.45	0.26	1.42	3.27	0.00	0.09	0.00	0.00	0.00	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L145	Shek Tong Street (EB)	Chatham Road North	Ma Tau Wai Road	0:00	1:00	3.60	0.14	1.22	2.09	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	3.60	0.22	1.30	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.60	0.22	1.15	2.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.60	0.29	1.22	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.60	0.00	1.08	2.52	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.60	0.00	1.08	2.52	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	3.60	0.14	1.01	2.16	0.00	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	7.20	0.00	1.44	5.26	0.00	0.22	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	7.20	0.00	2.02	4.10	0.00	0.36	0.36	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	7.20	0.00	1.66	4.25	0.00	0.43	0.43	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	7.20	0.29	2.45	2.16	0.00	0.94	1.08	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	7.20	0.22	2.81	2.23	0.00	1.37	0.43	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	7.20	0.29	3.24	2.38	0.00	0.79	0.43	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	7.20	0.22	2.38	1.73	0.00	1.08	1.37	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	7.20	0.22	2.16	2.38	0.00	1.01	1.08	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	7.20	0.29	2.30	2.30	0.00	0.86	1.08	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	7.20	0.72	4.61	0.00	0.00	1.15	0.72	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	7.20	0.72	4.90	0.00	0.00	0.65	0.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	7.20	0.72	5.26	0.00	0.00	0.58	0.65	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	7.20	0.29	3.96	2.66	0.00	0.07	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	7.20	0.29	3.31	3.31	0.00	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	3.60	0.14	1.44	1.87	0.00	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	3.60	0.14	1.22	2.16	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L146	Chatham Road North (SB)	Shek Tong Street	San Lau Street	23:00	0:00	3.60	0.22	1.22	2.09	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				0:00	1:00	10.80	0.72	3.02	5.15	0.00	0.25	0.00	0.00	0.00	0.40	0.04	0.04	0.07	0.04	0.04	0.00	1.04
				1:00	2:00	7.20	0.47	2.27	4.03	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.18
				2:00	3:00	5.40	0.32	1.58	3.20	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.14
				3:00	4:00	5.40	0.40	1.69	2.84	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.22
				4:00	5:00	3.60	0.25	1.01	2.09	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.14
				5:00	6:00	5.40	0.32	1.48	3.02	0.00	0.32	0.00	0.00	0.00	0.00	0.04	0.04	0.11	0.04	0.04	0.00	0.00
				6:00	7:00	9.00	0.40	1.94	4.07	0.04	0.43	0.11	0.00	0.00	0.40	0.04	0.07	0.14	0.11	0.04	0.00	1.22
				7:00	8:00	27.00	0.86	6.34	8.68	0.18	1.55	0.47	0.07	0.07	0.94	0.18	0.18	0.47	0.40	0.18	0.00	6.44
				8:00	9:00	39.60	2.45	12.96	9.22	0.32	3.71	2.20	0.43	0.40	0.76	0.25	0.25	0.47	0.40	0.14	0.00	5.65
				9:00	10:00	36.00	1.73	9.79	9.18	0.25	3.42	2.27	0.43	0.40	0.61	0.32	0.25	0.58	0.54	0.18	0.00	6.05
				10:00	11:00	30.60	1.58	8.57	7.20	0.32	4.36	2.05	0.50	0.47	0.76	0.07	0.11	0.29	0.22	0.07	0.00	4.03
				11:00	12:00	28.80	1.01	7.92	5.98	0.47	5.33	0.76	0.22	0.14	1.37	0.25	0.14	0.36	0.36	0.11	0.00	4.39
				12:00	13:00	28.80	1.22	10.37	7.34	0.29	3.46	0.68	0.22	0.14	1.01	0.29	0.07	0.14	0.14	0.04	0.00	3.38
				13:00	14:00	28.80	0.94	7.06	4.93	0.36	4.25	2.38	0.61	0.58	0.97	0.07	0.29	0.65	0.65	0.22	0.00	4.86
				14:00	15:00	27.00	0.76	6.34	6.55	0.32	3.89	1.80	0.43	0.40	1.04	0.11	0.18	0.43	0.40	0.14	0.00	4.21
				15:00	16:00	32.40	1.33	8.64	7.88	0.36	4.36	2.27	0.58	0.50	0.94	0.22	0.18	0.36	0.32	0.11	0.00	4.36
				16:00	17:00	25.20	1.19	10.15	4.93	0.25	2.48	0.54	0.14	0.14	1.26	0.04	0.11	0.29	0.32	0.11	0.00	3.24
				17:00	18:00	32.40	1.80	15.23	6.19	0.22	1.98	1.08	0.36	0.32	1.33	0.07	0.07	0.18	0.18	0.07	0.00	3.31
				18:00	19:00	27.00	1.58	13.72	5.72	0.11	1.37	0.61	0.22	0.22	0.50	0.04	0.07	0.22	0.22	0.07	0.00	2.34
				19:00	20:00	27.00	1.37	12.02	7.67	0.04	0.61	0.29	0.04	0.07	1.01	0.07	0.07	0.14	0.14	0.04	0.00	3.42
				20:00	21:00	23.40	1.19	8.46	8.03	0.04	0.47	0.11	0.04	0.04	1.40	0.04	0.04	0.07	0.04	0.04	0.00	3.42
				21:00	22:00	21.60	0.83	6.77	8.21	0.04	0.58	0.18	0.04	0.04	1.19	0.00	0.00	0.04	0.00	0.00	0.00	3.71
				22:00	23:00	19.80	0.97	4.79	7.63	0.04	0.29	0.00	0.00	0.00	1.15	0.04	0.04	0.07	0.04	0.04	0.00	4.72
				23:00	0:00	16.20	0.83	3.78	6.23	0.04	0.29	0.00	0.00	0.00	0.97	0.04	0.04	0.07	0.04	0.04	0.00	3.85

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L147	Chatham Road North (NB)	San Lau Street	Shan Si Street	0:00	1:00	29.60	1.70	6.96	14.95	0.00	0.81	0.00	0.00	0.00	2.29	0.07	0.07	0.22	0.15	0.07	0.00	2.29
				1:00	2:00	18.50	1.18	4.96	11.17	0.00	0.81	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.37
				2:00	3:00	14.80	0.81	3.70	9.55	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.30
				3:00	4:00	11.10	0.81	2.89	6.44	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.30
				4:00	5:00	11.10	0.74	2.66	6.88	0.00	0.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.37
				5:00	6:00	11.10	0.59	2.52	6.73	0.00	0.67	0.00	0.00	0.07	0.00	0.07	0.07	0.15	0.15	0.07	0.00	0.00
				6:00	7:00	25.90	1.04	4.51	12.14	0.15	1.41	0.22	0.00	0.07	2.22	0.22	0.22	0.44	0.37	0.15	0.00	2.74
				7:00	8:00	70.30	1.63	15.02	26.12	0.59	3.03	0.74	0.22	0.22	9.47	0.44	0.44	0.74	0.74	0.15	0.00	10.73
				8:00	9:00	96.20	4.44	30.56	27.45	1.04	7.25	3.26	1.18	1.18	7.40	0.52	0.52	0.89	0.89	0.22	0.00	9.40
				9:00	10:00	88.80	3.18	23.53	27.68	0.89	6.81	3.40	1.26	1.26	6.51	0.81	0.74	1.04	1.18	0.30	0.00	10.21
				10:00	11:00	81.40	3.85	18.87	20.20	1.41	13.91	4.22	1.78	1.70	4.14	0.44	0.44	0.81	0.81	0.22	0.00	8.58
				11:00	12:00	81.40	2.52	18.20	17.32	1.85	17.32	1.63	0.74	0.59	7.47	1.04	0.59	1.11	1.26	0.30	0.00	9.47
				12:00	13:00	77.70	3.11	23.16	21.02	1.26	11.17	1.41	0.81	0.59	5.48	1.11	0.22	0.44	0.52	0.15	0.00	7.25
				13:00	14:00	81.40	2.37	16.21	14.58	1.63	14.36	5.11	2.22	2.22	5.40	0.30	1.26	2.15	2.37	0.59	0.00	10.66
				14:00	15:00	77.70	1.92	14.95	19.54	1.41	13.10	3.92	1.63	1.55	5.70	0.52	0.81	1.41	1.48	0.37	0.00	9.40
				15:00	16:00	92.50	3.40	20.35	23.83	1.55	14.80	5.11	2.22	2.07	5.18	0.96	0.67	1.18	1.18	0.30	0.00	9.69
				16:00	17:00	81.40	4.22	23.16	14.28	1.33	14.80	1.85	0.81	0.74	3.92	0.15	1.04	2.15	2.44	0.59	0.00	9.92
				17:00	18:00	96.20	6.29	33.97	17.61	1.18	11.47	3.63	1.85	1.70	4.07	0.67	0.67	1.33	1.33	0.52	0.00	9.92
				18:00	19:00	81.40	5.70	31.89	16.87	0.67	8.14	2.22	1.04	1.04	1.63	0.52	0.81	1.48	1.70	0.52	0.00	7.18
				19:00	20:00	74.00	3.55	28.79	23.31	0.15	2.07	0.59	0.15	0.22	5.77	0.30	0.22	0.52	0.52	0.15	0.00	7.70
				20:00	21:00	62.90	2.89	19.09	23.24	0.15	1.48	0.22	0.15	0.22	7.55	0.15	0.07	0.15	0.15	0.07	0.00	7.33
				21:00	22:00	59.20	2.00	15.54	24.05	0.15	1.85	0.44	0.15	0.22	6.66	0.00	0.00	0.07	0.00	0.00	0.00	8.07
				22:00	23:00	55.50	2.59	11.25	22.79	0.15	0.96	0.00	0.00	0.00	6.59	0.15	0.15	0.22	0.15	0.07	0.00	10.43
L148	Ko Shan Road (SB)	Shan Si Street	San Lau Street	23:00	0:00	48.10	2.29	9.40	19.76	0.15	1.04	0.00	0.00	0.00	5.70	0.15	0.15	0.22	0.15	0.07	0.00	9.03
				0:00	1:00	11.25	0.23	3.45	6.68	0.00	0.23	0.00	0.00	0.00	0.53	0.00	0.00	0.15	0.00	0.00	0.00	0.00
				1:00	2:00	7.50	0.15	2.33	4.88	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	7.50	0.15	2.18	5.03	0.00	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.75	0.08	1.13	2.48	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.75	0.08	1.05	2.55	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.75	0.08	1.05	2.48	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				6:00	7:00	7.50	0.08	1.88	4.58	0.00	0.23	0.00	0.00	0.00	0.53	0.00	0.00	0.08	0.08	0.08	0.00	0.00
				7:00	8:00	15.00	0.45	6.00	4.50	0.08	0.75	0.15	0.00	0.00	1.58	0.15	0.15	0.45	0.38	0.38	0.00	0.00
				8:00	9:00	22.50	1.13	11.40	4.50	0.08	1.65	0.90	0.08	0.08	1.13	0.15	0.15	0.53	0.38	0.38	0.00	0.00
				9:00	10:00	18.75	0.75	8.18	4.35	0.08	1.50	0.90	0.08	0.08	0.90	0.23	0.15	0.68	0.45	0.45	0.00	0.00
				10:00	11:00	18.75	0.53	7.05	6.90	0.08	2.10	0.68	0.08	0.08	0.75	0.08	0.08	0.15	0.15	0.08	0.00	0.00
				11:00	12:00	18.75	0.38	6.83	6.08	0.15	2.70	0.30	0.00	0.00	1.50	0.15	0.08	0.30	0.15	0.15	0.00	0.00
				12:00	13:00	18.75	0.38	8.33	6.83	0.08	1.58	0.23	0.00	0.00	0.98	0.15	0.00	0.08	0.08	0.08	0.00	0.00
				13:00	14:00	18.75	0.30	6.53	5.40	0.08	2.40	0.98	0.08	0.23	1.13	0.08	0.23	0.60	0.38	0.38	0.00	0.00
				14:00	15:00	18.75	0.30	6.00	7.05	0.08	2.18	0.68	0.08	0.08	1.20	0.08	0.08	0.38	0.30	0.30	0.00	0.00
				15:00	16:00	18.75	0.38	6.75	7.13	0.08	1.95	0.68	0.08	0.08	0.90	0.15	0.08	0.23	0.15	0.15	0.00	0.00
				16:00	17:00	18.75	0.15	8.03	7.95	0.08	1.50	0.15	0.00	0.00	0.90	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	26.25	0.23	12.83	10.50	0.08	1.20	0.23	0.08	0.08	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	22.50	0.23	11.25	9.53	0.08	0.75	0.15	0.08	0.08	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	18.75	0.38	9.60	7.13	0.00	0.30	0.08	0.00	0.00	0.98	0.08	0.00	0.08	0.08	0.08	0.00	0.00
				20:00	21:00	18.75	0.38	7.80	8.63	0.00	0.30	0.00	0.00	0.00	1.58	0.00	0.00	0.08	0.00	0.00	0.00	0.00
				21:00	22:00	15.00	0.23	5.55	7.80	0.00	0.23	0.08	0.00	0.00	1.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	15.00	0.38	4.50	8.48	0.00	0.15	0.00	0.00	0.00	1.35	0.00	0.00	0.08	0.00	0.08	0.00	0.00
				23:00	0:00	11.25	0.23	3.38	6.30	0.00	0.15	0.00	0.00	0.00	1.05	0.00	0.00	0.08	0.00	0.08	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L149	Ko Shan Road (NB)	San Lau Street	Shan Si Street	0:00	1:00	18.75	0.60	4.20	11.85	0.00	0.30	0.00	0.00	0.00	1.58	0.08	0.00	0.08	0.08	0.00	0.00	0.00	
				1:00	2:00	11.25	0.38	2.63	7.65	0.00	0.23	0.00	0.00	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	11.25	0.30	2.40	8.03	0.00	0.15	0.00	0.00	0.00	0.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.50	0.23	1.80	5.25	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.50	0.30	1.65	5.33	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	7.50	0.23	1.58	5.33	0.00	0.23	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.08	0.00	0.00	0.00
				6:00	7:00	15.00	0.30	2.70	9.45	0.00	0.53	0.08	0.00	0.00	1.43	0.08	0.08	0.15	0.15	0.08	0.00	0.00	0.00
				7:00	8:00	37.50	0.45	7.73	20.93	0.08	1.35	0.23	0.08	0.08	4.20	0.38	0.23	0.75	0.68	0.38	0.00	0.00	0.00
				8:00	9:00	48.75	1.05	15.30	21.45	0.15	3.15	1.20	0.53	0.45	3.15	0.30	0.30	0.75	0.68	0.30	0.00	0.00	0.00
				9:00	10:00	45.00	0.75	11.55	21.53	0.15	3.00	1.20	0.53	0.45	2.78	0.45	0.38	0.98	0.90	0.38	0.00	0.00	0.00
				10:00	11:00	41.25	1.35	11.70	16.28	0.15	5.18	1.80	0.53	0.53	2.78	0.15	0.15	0.30	0.30	0.08	0.00	0.00	0.00
				11:00	12:00	37.50	0.83	10.50	12.75	0.23	6.00	0.60	0.23	0.15	4.58	0.38	0.23	0.45	0.45	0.15	0.00	0.00	0.00
				12:00	13:00	41.25	1.13	14.10	16.43	0.15	4.05	0.53	0.23	0.15	3.45	0.45	0.08	0.23	0.23	0.08	0.00	0.00	0.00
				13:00	14:00	37.50	0.83	9.98	11.25	0.15	5.25	2.18	0.68	0.68	3.53	0.15	0.53	0.98	0.98	0.38	0.00	0.00	0.00
				14:00	15:00	37.50	0.75	8.85	14.78	0.15	4.65	1.58	0.53	0.45	3.68	0.23	0.30	0.68	0.68	0.23	0.00	0.00	0.00
				15:00	16:00	45.00	1.20	12.00	18.23	0.15	5.25	1.95	0.60	0.53	3.30	0.38	0.23	0.53	0.53	0.15	0.00	0.00	0.00
				16:00	17:00	37.50	1.80	15.30	9.98	0.08	4.43	0.90	0.15	0.15	3.90	0.08	0.23	0.23	0.30	0.00	0.00	0.00	0.00
				17:00	18:00	48.75	2.70	23.03	12.53	0.08	3.45	1.65	0.30	0.30	4.13	0.15	0.15	0.15	0.15	0.00	0.00	0.00	0.00
				18:00	19:00	41.25	2.33	21.15	11.78	0.08	2.40	0.98	0.15	0.15	1.65	0.08	0.15	0.15	0.23	0.00	0.00	0.00	0.00
				19:00	20:00	41.25	1.28	16.80	17.70	0.00	0.75	0.23	0.08	0.08	3.60	0.15	0.08	0.23	0.23	0.08	0.00	0.00	0.00
				20:00	21:00	37.50	1.13	11.85	18.45	0.00	0.60	0.08	0.08	0.08	5.03	0.08	0.00	0.08	0.08	0.00	0.00	0.00	0.00
				21:00	22:00	33.75	0.75	9.23	18.53	0.00	0.68	0.15	0.08	0.08	4.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	30.00	0.90	6.68	17.55	0.00	0.30	0.00	0.00	0.00	4.20	0.08	0.08	0.08	0.08	0.08	0.00	0.00	0.00
L150	San Lau Street (WB)	Chatham Road North	Ko Shan Road	23:00	0:00	26.25	0.83	5.70	15.23	0.00	0.45	0.00	0.00	0.00	3.68	0.08	0.08	0.08	0.08	0.08	0.00	0.00	
				0:00	1:00	4.30	0.17	0.95	2.58	0.00	0.09	0.00	0.00	0.00	0.47	0.00	0.00	0.04	0.00	0.00	0.00	0.00	
				1:00	2:00	4.30	0.26	1.03	2.62	0.00	0.09	0.00	0.00	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.30	0.17	0.90	2.80	0.00	0.09	0.00	0.00	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.13	0.52	1.46	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.13	0.47	1.46	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.13	0.43	1.46	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.30	0.17	0.77	2.54	0.00	0.17	0.04	0.00	0.00	0.52	0.00	0.00	0.04	0.04	0.00	0.00	0.00	0.00
				7:00	8:00	8.60	0.17	1.89	4.21	0.04	0.39	0.09	0.04	0.04	1.20	0.09	0.09	0.17	0.17	0.00	0.00	0.00	0.00
				8:00	9:00	12.90	0.52	4.13	4.73	0.04	0.99	0.47	0.22	0.22	0.99	0.09	0.09	0.22	0.22	0.00	0.00	0.00	0.00
				9:00	10:00	10.75	0.34	2.84	4.39	0.04	0.90	0.47	0.17	0.17	0.77	0.13	0.09	0.22	0.22	0.00	0.00	0.00	0.00
				10:00	11:00	10.75	0.52	2.84	3.66	0.04	1.55	0.60	0.17	0.17	0.90	0.04	0.04	0.09	0.09	0.04	0.00	0.00	0.00
				11:00	12:00	10.75	0.34	2.75	3.27	0.09	1.94	0.26	0.04	0.04	1.59	0.09	0.04	0.17	0.09	0.04	0.00	0.00	0.00
				12:00	13:00	10.75	0.39	3.53	3.91	0.04	1.25	0.22	0.04	0.04	1.16	0.09	0.00	0.04	0.04	0.00	0.00	0.00	0.00
				13:00	14:00	10.75	0.34	2.62	2.75	0.04	1.72	0.82	0.22	0.22	1.25	0.04	0.13	0.26	0.22	0.13	0.00	0.00	0.00
				14:00	15:00	10.75	0.26	2.41	3.78	0.04	1.55	0.60	0.17	0.17	1.29	0.04	0.04	0.17	0.17	0.04	0.00	0.00	0.00
				15:00	16:00	10.75	0.43	2.71	3.78	0.04	1.42	0.65	0.17	0.17	0.99	0.09	0.04	0.13	0.09	0.04	0.00	0.00	0.00
				16:00	17:00	10.75	0.60	3.83	2.88	0.04	1.38	0.26	0.00	0.00	1.46	0.00	0.00	0.13	0.00	0.17	0.00	0.00	0.00
				17:00	18:00	12.90	0.86	5.55	3.35	0.04	1.03	0.47	0.04	0.04	1.38	0.00	0.00	0.04	0.00	0.09	0.00	0.00	0.00
				18:00	19:00	10.75	0.73	5.03	3.14	0.04	0.73	0.30	0.04	0.04	0.56	0.00	0.00	0.04	0.00	0.09	0.00	0.00	0.00
				19:00	20:00	10.75	0.47	4.34	4.26	0.00	0.26	0.09	0.00	0.00	1.20	0.04	0.00	0.04	0.04	0.00	0.00	0.00	0.00
				20:00	21:00	8.60	0.39	2.67	3.91	0.00	0.13	0.04	0.00	0.00	1.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	8.60	0.26	2.32	4.39	0.00	0.22	0.04	0.00	0.00	1.38	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	8.60	0.39	1.89	4.60	0.00	0.13	0.00	0.00	0.00	1.55	0.00	0.00	0.04	0.00	0.00	0.00	0.00	0.00				
23:00	0:00	6.45	0.30	1.38	3.48	0.00	0.09	0.00	0.00	0.00	1.16	0.00	0.00	0.04	0.00	0.00	0.00	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L151	San Lau Street (WB)	Gillies Avenue North	Chatham Road North	0:00	1:00	18.00	0.72	5.11	9.94	0.00	0.36	0.00	0.00	0.00	1.30	0.00	0.00	0.00	0.00	0.00	0.00	0.58
				1:00	2:00	10.80	0.43	3.24	6.62	0.00	0.22	0.00	0.00	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	10.80	0.43	2.95	6.91	0.00	0.14	0.00	0.00	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	7.20	0.36	2.16	4.46	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	7.20	0.29	2.02	4.68	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	7.20	0.22	2.02	4.68	0.00	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	14.40	0.43	3.31	7.99	0.07	0.50	0.07	0.00	0.00	1.30	0.00	0.00	0.07	0.00	0.00	0.00	0.65
				7:00	8:00	39.60	0.65	10.94	16.99	0.50	1.58	0.29	0.07	0.07	5.83	0.00	0.00	0.00	0.00	0.07	0.00	2.59
				8:00	9:00	54.00	1.66	21.60	17.21	0.79	3.74	1.58	0.36	0.36	4.39	0.00	0.00	0.00	0.00	0.07	0.00	2.23
				9:00	10:00	46.80	1.15	15.98	17.21	0.72	3.38	1.66	0.36	0.29	3.67	0.00	0.00	0.00	0.00	0.07	0.00	2.30
				10:00	11:00	43.20	1.66	13.90	13.61	1.01	5.47	1.87	0.50	0.50	2.38	0.00	0.00	0.07	0.00	0.07	0.00	2.16
				11:00	12:00	43.20	1.15	13.75	11.81	1.37	6.84	0.72	0.22	0.14	4.39	0.07	0.00	0.14	0.07	0.07	0.00	2.45
				12:00	13:00	46.80	1.44	18.22	15.05	1.01	4.61	0.72	0.22	0.14	3.31	0.07	0.00	0.07	0.00	0.00	0.00	1.94
				13:00	14:00	39.60	1.08	12.24	9.79	1.08	5.62	2.30	0.58	0.58	3.17	0.00	0.07	0.22	0.07	0.14	0.00	2.66
				14:00	15:00	39.60	0.79	10.94	13.10	1.01	5.18	1.73	0.43	0.43	3.31	0.00	0.07	0.14	0.07	0.07	0.00	2.30
				15:00	16:00	46.80	1.37	14.40	15.41	1.08	5.54	2.16	0.58	0.58	2.88	0.07	0.07	0.14	0.07	0.07	0.00	2.38
				16:00	17:00	43.20	1.94	18.07	10.66	0.79	4.75	0.79	0.22	0.22	2.30	0.00	0.14	0.36	0.14	0.07	0.00	2.74
				17:00	18:00	57.60	3.02	27.72	13.82	0.72	3.89	1.58	0.50	0.50	2.45	0.07	0.07	0.22	0.07	0.07	0.00	2.88
				18:00	19:00	50.40	2.81	25.92	13.25	0.43	2.81	0.94	0.29	0.29	1.01	0.07	0.07	0.29	0.07	0.07	0.00	2.09
				19:00	20:00	46.80	1.51	21.96	16.27	0.07	0.86	0.36	0.07	0.07	3.46	0.00	0.00	0.07	0.00	0.00	0.00	2.09
				20:00	21:00	39.60	1.30	14.69	16.13	0.07	0.65	0.07	0.07	0.07	4.61	0.00	0.00	0.00	0.00	0.00	0.00	1.94
				21:00	22:00	36.00	0.86	11.66	16.27	0.07	0.79	0.14	0.07	0.07	3.96	0.00	0.00	0.00	0.00	0.00	0.00	2.09
				22:00	23:00	32.40	1.15	8.50	15.70	0.07	0.36	0.00	0.00	0.00	3.89	0.00	0.00	0.00	0.00	0.00	0.00	2.74
L152	Gillies Avenue North (WB)	Ma Tau Wai Road	San Lau Street	23:00	0:00	25.20	0.94	6.41	12.17	0.07	0.36	0.00	0.00	0.00	3.10	0.00	0.00	0.00	0.00	0.00	2.16	
				0:00	1:00	4.50	0.18	1.39	2.29	0.00	0.11	0.00	0.00	0.00	0.32	0.00	0.00	0.02	0.02	0.00	0.00	
				1:00	2:00	2.70	0.11	0.88	1.53	0.00	0.09	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.70	0.09	0.81	1.64	0.00	0.07	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.80	0.09	0.59	1.03	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.80	0.07	0.58	1.10	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	1.80	0.07	0.54	1.06	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.02	0.02	0.00	0.00	
				6:00	7:00	3.60	0.09	0.86	1.84	0.02	0.16	0.04	0.00	0.00	0.27	0.02	0.02	0.04	0.04	0.02	0.00	
				7:00	8:00	9.90	0.14	3.01	3.73	0.13	0.41	0.11	0.04	0.02	1.13	0.07	0.05	0.13	0.13	0.05	0.00	
				8:00	9:00	14.40	0.43	6.12	3.85	0.23	0.99	0.49	0.14	0.13	0.86	0.07	0.07	0.14	0.14	0.05	0.00	
				9:00	10:00	12.60	0.29	4.52	3.80	0.20	0.92	0.50	0.14	0.13	0.74	0.11	0.09	0.20	0.20	0.07	0.00	
				10:00	11:00	10.80	0.38	3.47	2.92	0.27	1.58	0.58	0.16	0.16	0.50	0.02	0.04	0.07	0.05	0.02	0.00	
				11:00	12:00	10.80	0.23	3.46	2.52	0.36	1.98	0.23	0.07	0.05	0.94	0.07	0.04	0.09	0.09	0.02	0.00	
				12:00	13:00	10.80	0.31	4.32	3.01	0.23	1.26	0.20	0.07	0.05	0.67	0.09	0.02	0.04	0.04	0.02	0.00	
				13:00	14:00	10.80	0.23	3.15	2.20	0.31	1.71	0.74	0.22	0.22	0.70	0.02	0.09	0.20	0.20	0.05	0.00	
				14:00	15:00	9.90	0.18	2.72	2.79	0.27	1.46	0.52	0.14	0.14	0.68	0.04	0.05	0.13	0.11	0.04	0.00	
				15:00	16:00	12.60	0.36	3.80	3.51	0.32	1.75	0.72	0.22	0.20	0.65	0.07	0.07	0.11	0.11	0.04	0.00	
				16:00	17:00	9.90	0.41	4.03	2.14	0.20	1.44	0.23	0.05	0.05	0.54	0.00	0.04	0.05	0.04	0.00	0.00	
				17:00	18:00	13.50	0.67	6.37	2.84	0.20	1.22	0.50	0.14	0.14	0.59	0.02	0.02	0.04	0.02	0.00	0.00	
				18:00	19:00	10.80	0.54	5.62	2.54	0.11	0.79	0.27	0.09	0.07	0.22	0.02	0.02	0.04	0.02	0.00	0.00	
				19:00	20:00	10.80	0.36	5.33	3.38	0.02	0.23	0.09	0.02	0.02	0.70	0.02	0.02	0.04	0.04	0.02	0.00	
				20:00	21:00	9.00	0.31	3.55	3.37	0.02	0.18	0.04	0.02	0.02	0.95	0.02	0.00	0.02	0.02	0.00	0.00	
				21:00	22:00	8.10	0.18	2.84	3.42	0.02	0.20	0.05	0.02	0.02	0.81	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	7.20	0.23	2.05	3.22	0.02	0.11	0.00	0.00	0.00	0.79	0.02	0.02	0.02	0.02	0.00	0.00					
23:00	0:00	6.30	0.22	1.75	2.81	0.02	0.13	0.00	0.00	0.00	0.70	0.02	0.02	0.02	0.02	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L153	Gillies Avenue North (SB)	San Lau Street	San Wai Street	0:00	1:00	5.00	0.25	1.60	2.65	0.00	0.10	0.00	0.00	0.00	0.30	0.00	0.00	0.05	0.00	0.00	0.00	0.00
				1:00	2:00	2.50	0.10	0.85	1.50	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.50	0.10	0.75	1.60	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.50	0.15	0.80	1.50	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.50	0.10	0.75	1.55	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.50	0.10	0.75	1.45	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	0.00
				6:00	7:00	5.00	0.15	1.25	2.75	0.00	0.20	0.05	0.00	0.00	0.30	0.05	0.05	0.05	0.05	0.05	0.00	0.05
				7:00	8:00	10.00	0.10	3.20	3.80	0.10	0.40	0.15	0.05	0.05	0.80	0.15	0.15	0.30	0.35	0.10	0.00	0.30
				8:00	9:00	15.00	0.25	6.35	3.85	0.25	1.00	0.85	0.25	0.25	0.60	0.15	0.15	0.35	0.35	0.10	0.00	0.25
				9:00	10:00	15.00	0.25	5.05	4.25	0.25	1.00	1.00	0.30	0.30	0.60	0.30	0.25	0.50	0.50	0.20	0.00	0.25
				10:00	11:00	12.50	0.50	4.10	3.60	0.15	1.60	1.05	0.30	0.20	0.50	0.05	0.05	0.10	0.10	0.05	0.00	0.15
				11:00	12:00	12.50	0.35	4.20	3.20	0.25	2.10	0.40	0.10	0.05	0.95	0.20	0.05	0.20	0.20	0.05	0.00	0.20
				12:00	13:00	12.50	0.45	5.25	3.75	0.15	1.30	0.35	0.10	0.05	0.60	0.15	0.05	0.05	0.10	0.05	0.00	0.10
				13:00	14:00	12.50	0.35	3.55	2.65	0.25	1.70	1.35	0.35	0.30	0.65	0.05	0.20	0.35	0.35	0.20	0.00	0.20
				14:00	15:00	12.50	0.30	3.60	3.55	0.20	1.60	1.00	0.25	0.25	0.70	0.05	0.15	0.25	0.25	0.15	0.00	0.20
				15:00	16:00	15.00	0.45	4.70	4.35	0.25	1.75	1.30	0.35	0.30	0.65	0.10	0.10	0.20	0.20	0.10	0.00	0.20
				16:00	17:00	12.50	0.80	5.45	3.30	0.05	1.50	0.45	0.05	0.05	0.70	0.00	0.00	0.00	0.00	0.05	0.00	0.10
				17:00	18:00	15.00	1.05	7.35	3.70	0.05	1.05	0.75	0.15	0.10	0.65	0.00	0.00	0.00	0.00	0.05	0.00	0.10
				18:00	19:00	15.00	1.10	7.85	4.05	0.05	0.85	0.55	0.10	0.05	0.30	0.00	0.00	0.00	0.00	0.05	0.00	0.05
				19:00	20:00	12.50	0.50	6.40	4.10	0.00	0.25	0.15	0.00	0.05	0.65	0.05	0.05	0.05	0.10	0.05	0.00	0.10
				20:00	21:00	10.00	0.40	4.25	4.10	0.00	0.15	0.05	0.00	0.00	0.90	0.00	0.00	0.05	0.00	0.00	0.00	0.10
				21:00	22:00	10.00	0.30	3.75	4.60	0.00	0.25	0.15	0.00	0.00	0.80	0.00	0.00	0.00	0.00	0.00	0.00	0.15
				22:00	23:00	7.50	0.30	2.35	3.80	0.00	0.10	0.00	0.00	0.00	0.75	0.00	0.00	0.05	0.00	0.00	0.00	0.15
				L154	Gillies Avenue North (NB)	San Wai Street	San Lau Street	23:00	0:00	7.50	0.40	2.25	3.75	0.00	0.15	0.00	0.00	0.00	0.70	0.00	0.00	0.05
0:00	1:00	5.00	0.20					1.15	3.10	0.00	0.05	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00
1:00	2:00	5.00	0.30					1.20	3.40	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
2:00	3:00	2.50	0.10					0.55	1.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
3:00	4:00	2.50	0.15					0.60	1.70	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
4:00	5:00	2.50	0.15					0.55	1.80	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
5:00	6:00	2.50	0.15					0.50	1.80	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
6:00	7:00	5.00	0.20					0.90	3.20	0.00	0.05	0.00	0.00	0.00	0.55	0.00	0.00	0.05	0.00	0.05	0.00	0.00
7:00	8:00	7.50	0.05					1.80	3.55	0.05	0.30	0.10	0.00	0.00	1.60	0.00	0.00	0.00	0.00	0.05	0.00	0.00
8:00	9:00	10.00	0.15					3.60	3.55	0.10	0.65	0.60	0.05	0.05	1.20	0.00	0.00	0.00	0.00	0.05	0.00	0.00
9:00	10:00	10.00	0.15					3.05	4.00	0.15	0.65	0.70	0.05	0.05	1.15	0.00	0.00	0.00	0.00	0.05	0.00	0.00
10:00	11:00	10.00	0.60					3.05	4.10	0.10	0.65	0.50	0.05	0.05	0.80	0.00	0.00	0.05	0.00	0.05	0.00	0.00
11:00	12:00	10.00	0.40					3.00	3.70	0.15	0.85	0.20	0.00	0.00	1.50	0.00	0.00	0.10	0.00	0.10	0.00	0.00
12:00	13:00	10.00	0.40					3.55	4.20	0.10	0.50	0.15	0.00	0.00	1.00	0.00	0.00	0.05	0.00	0.05	0.00	0.00
13:00	14:00	10.00	0.35					2.95	3.40	0.10	0.75	0.70	0.05	0.10	1.15	0.00	0.00	0.20	0.00	0.25	0.00	0.00
14:00	15:00	10.00	0.30					2.55	4.40	0.10	0.65	0.45	0.05	0.05	1.20	0.00	0.00	0.10	0.00	0.15	0.00	0.00
15:00	16:00	12.50	0.55					3.60	5.25	0.15	0.75	0.65	0.05	0.05	1.15	0.00	0.00	0.15	0.00	0.15	0.00	0.00
16:00	17:00	12.50	0.95					4.65	4.75	0.05	0.35	0.15	0.00	0.00	1.10	0.00	0.00	0.30	0.00	0.20	0.00	0.00
17:00	18:00	17.50	1.45					7.45	6.25	0.05	0.30	0.30	0.05	0.05	1.20	0.00	0.00	0.20	0.00	0.20	0.00	0.00
18:00	19:00	15.00	1.30					6.75	5.75	0.05	0.15	0.15	0.05	0.05	0.45	0.00	0.00	0.15	0.00	0.15	0.00	0.00
19:00	20:00	12.50	0.55					5.10	5.30	0.00	0.15	0.05	0.00	0.00	1.25	0.00	0.00	0.05	0.00	0.05	0.00	0.00
20:00	21:00	10.00	0.45					3.15	4.80	0.00	0.05	0.00	0.00	0.00	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00
21:00	22:00	10.00	0.30					2.75	5.40	0.00	0.10	0.05	0.00	0.00	1.40	0.00	0.00	0.00	0.00	0.00	0.00	0.00
22:00	23:00	10.00	0.45					2.20	5.70	0.00	0.05	0.00	0.00	0.00	1.55	0.00	0.00	0.00	0.00	0.05	0.00	0.00
23:00	0:00	7.50	0.35	1.60	4.30	0.00	0.05	0.00	0.00	0.00	1.15	0.00	0.00	0.00	0.00	0.05	0.00	0.00				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L155	Gillies Avenue North (SB)	San Wai Street	Pak Kung Street	0:00	1:00	4.20	0.21	1.39	2.27	0.00	0.08	0.00	0.00	0.00	0.17	0.00	0.00	0.04	0.04	0.00	0.00	0.00	
				1:00	2:00	4.20	0.21	1.39	2.44	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.20	0.17	1.30	2.65	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.10	0.13	0.71	1.13	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.10	0.13	0.67	1.26	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.10	0.08	0.63	1.22	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.04	0.00	0.00	0.00
				6:00	7:00	4.20	0.17	1.09	2.18	0.00	0.21	0.04	0.00	0.00	0.17	0.04	0.04	0.08	0.08	0.04	0.00	0.04	0.00
				7:00	8:00	10.50	0.21	3.57	3.70	0.17	0.59	0.21	0.04	0.04	0.50	0.21	0.21	0.42	0.34	0.13	0.00	0.17	0.00
				8:00	9:00	14.70	0.42	6.38	3.44	0.21	1.26	0.80	0.25	0.25	0.34	0.21	0.21	0.38	0.34	0.08	0.00	0.13	0.00
				9:00	10:00	14.70	0.34	5.12	3.82	0.21	1.34	0.92	0.29	0.29	0.34	0.38	0.29	0.50	0.50	0.17	0.00	0.17	0.00
				10:00	11:00	12.60	0.50	4.33	3.49	0.25	1.76	0.88	0.29	0.29	0.29	0.04	0.08	0.13	0.13	0.04	0.00	0.08	0.00
				11:00	12:00	10.50	0.29	3.65	2.65	0.25	1.93	0.29	0.08	0.08	0.46	0.17	0.08	0.21	0.21	0.04	0.00	0.08	0.00
				12:00	13:00	12.60	0.42	5.42	3.65	0.21	1.47	0.29	0.08	0.08	0.38	0.25	0.04	0.08	0.08	0.04	0.00	0.08	0.00
				13:00	14:00	12.60	0.34	3.74	2.60	0.25	1.89	1.13	0.34	0.34	0.42	0.04	0.25	0.46	0.46	0.17	0.00	0.17	0.00
				14:00	15:00	10.50	0.21	3.11	3.07	0.21	1.51	0.76	0.21	0.21	0.34	0.08	0.13	0.25	0.25	0.08	0.00	0.08	0.00
				15:00	16:00	12.60	0.38	4.03	3.57	0.21	1.68	0.97	0.29	0.25	0.34	0.17	0.13	0.21	0.21	0.08	0.00	0.08	0.00
				16:00	17:00	10.50	0.59	4.79	2.94	0.08	1.13	0.29	0.08	0.04	0.42	0.00	0.00	0.00	0.00	0.04	0.00	0.08	0.00
				17:00	18:00	14.70	0.92	7.48	3.82	0.08	0.92	0.55	0.21	0.17	0.42	0.00	0.00	0.00	0.00	0.04	0.00	0.08	0.00
				18:00	19:00	12.60	0.84	6.76	3.53	0.04	0.63	0.34	0.08	0.08	0.21	0.00	0.00	0.00	0.00	0.04	0.00	0.04	0.00
				19:00	20:00	12.60	0.50	6.68	4.12	0.00	0.29	0.17	0.04	0.04	0.38	0.04	0.04	0.08	0.08	0.04	0.00	0.08	0.00
				20:00	21:00	10.50	0.42	4.62	4.28	0.00	0.21	0.04	0.04	0.04	0.59	0.04	0.00	0.04	0.04	0.00	0.00	0.13	0.00
				21:00	22:00	8.40	0.25	3.32	3.91	0.00	0.25	0.08	0.04	0.04	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00
				22:00	23:00	8.40	0.38	2.73	4.28	0.00	0.17	0.00	0.00	0.00	0.50	0.04	0.04	0.04	0.04	0.00	0.00	0.17	0.00
				L156	Gillies Avenue North (NB)	Pak Kung Street	San Wai Street	23:00	0:00	6.30	0.29	2.02	3.23	0.00	0.13	0.00	0.00	0.00	0.38	0.04	0.04	0.04	0.04
0:00	1:00	4.20	0.13					0.88	2.73	0.00	0.04	0.00	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	2.10	0.08					0.46	1.51	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.10	0.08					0.46	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.10	0.13					0.50	1.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.10	0.08					0.42	1.60	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.10	0.04					0.42	1.60	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	4.20	0.13					0.71	2.77	0.00	0.04	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
7:00	8:00	6.30	0.04					1.51	2.90	0.04	0.25	0.04	0.00	0.00	1.47	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
8:00	9:00	8.40	0.17					2.98	2.94	0.13	0.59	0.38	0.04	0.04	1.09	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
9:00	10:00	6.30	0.08					1.93	2.48	0.08	0.46	0.34	0.04	0.04	0.80	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
10:00	11:00	8.40	0.38					2.48	3.78	0.08	0.55	0.29	0.04	0.04	0.71	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
11:00	12:00	8.40	0.25					2.48	3.32	0.17	0.67	0.08	0.00	0.00	1.34	0.00	0.00	0.00	0.00	0.08	0.00	0.00	
12:00	13:00	8.40	0.29					2.86	3.74	0.08	0.42	0.08	0.00	0.00	0.88	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
13:00	14:00	6.30	0.21					1.85	2.31	0.08	0.46	0.29	0.04	0.08	0.80	0.00	0.00	0.00	0.00	0.17	0.00	0.00	
14:00	15:00	8.40	0.21					2.06	3.95	0.08	0.50	0.25	0.04	0.04	1.09	0.00	0.00	0.00	0.00	0.17	0.00	0.00	
15:00	16:00	8.40	0.29					2.35	3.91	0.08	0.50	0.25	0.04	0.04	0.84	0.00	0.00	0.00	0.00	0.08	0.00	0.00	
16:00	17:00	10.50	0.63					3.86	4.49	0.04	0.25	0.04	0.00	0.00	0.97	0.00	0.00	0.00	0.00	0.21	0.00	0.00	
17:00	18:00	12.60	0.88					5.17	5.12	0.04	0.17	0.08	0.04	0.04	0.92	0.00	0.00	0.00	0.00	0.13	0.00	0.00	
18:00	19:00	12.60	0.88					5.42	5.42	0.04	0.17	0.04	0.04	0.04	0.38	0.00	0.00	0.00	0.00	0.17	0.00	0.00	
19:00	20:00	8.40	0.34					3.23	3.78	0.00	0.08	0.04	0.00	0.00	0.88	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
20:00	21:00	8.40	0.29					2.48	4.24	0.00	0.04	0.00	0.00	0.00	1.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	8.40	0.21					2.14	4.75	0.00	0.04	0.04	0.00	0.00	1.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	6.30	0.21					1.30	3.70	0.00	0.04	0.00	0.00	0.00	1.01	0.00	0.00	0.00	0.00	0.04	0.00	0.00	
23:00	0:00	6.30	0.25	1.26	3.70	0.00	0.04	0.00	0.00	0.00	1.01	0.00	0.00	0.00	0.00	0.04	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L157	San Wai Street (EB)	Chatham Road North	Gillies Avenue North	0:00	1:00	4.20	0.25	1.18	2.69	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	4.20	0.17	1.18	2.69	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.20	0.25	1.09	2.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.20	0.25	1.09	2.60	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.20	0.34	1.01	2.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.20	0.25	0.84	2.86	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.20	0.17	0.92	2.77	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	8.40	0.34	2.77	3.86	0.08	0.50	0.25	0.00	0.00	0.00	0.08	0.08	0.34	0.08	0.00	0.00	0.00	0.00
				8:00	9:00	12.60	0.76	5.46	3.61	0.08	1.26	0.76	0.08	0.08	0.00	0.08	0.08	0.25	0.08	0.00	0.00	0.00	0.00
				9:00	10:00	12.60	0.67	4.45	4.12	0.08	1.34	0.92	0.08	0.08	0.00	0.25	0.08	0.42	0.08	0.00	0.00	0.00	0.00
				10:00	11:00	8.40	0.50	2.52	2.77	0.08	1.43	0.76	0.08	0.08	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00	0.00
				11:00	12:00	8.40	0.42	2.60	2.69	0.17	1.76	0.34	0.00	0.00	0.00	0.08	0.00	0.17	0.08	0.08	0.00	0.00	0.00
				12:00	13:00	8.40	0.42	3.19	3.11	0.08	1.18	0.25	0.00	0.00	0.00	0.08	0.00	0.08	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	8.40	0.34	2.35	2.18	0.08	1.51	1.01	0.08	0.17	0.00	0.00	0.08	0.34	0.08	0.17	0.00	0.00	0.00
				14:00	15:00	8.40	0.34	2.18	3.02	0.08	1.43	0.76	0.08	0.08	0.00	0.00	0.08	0.17	0.08	0.08	0.00	0.00	0.00
				15:00	16:00	12.60	0.67	3.53	4.03	0.17	1.85	1.09	0.17	0.17	0.00	0.17	0.17	0.25	0.17	0.17	0.00	0.00	0.00
				16:00	17:00	8.40	0.59	3.11	2.86	0.08	1.09	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.34	0.00	0.00	0.00
				17:00	18:00	8.40	0.59	3.61	2.77	0.08	0.59	0.42	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00
				18:00	19:00	8.40	0.59	3.61	2.94	0.08	0.50	0.34	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00
				19:00	20:00	8.40	0.50	4.03	3.53	0.00	0.17	0.08	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	8.40	0.50	3.36	4.28	0.00	0.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	8.40	0.42	2.86	4.70	0.00	0.25	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	8.40	0.59	2.44	5.21	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L158	San Wai Street (WB)	Gillies Avenue North	Chatham Road North	23:00	0:00	4.20	0.25	1.18	2.69	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				0:00	1:00	4.20	0.17	0.59	2.86	0.00	0.17	0.00	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	4.20	0.34	0.67	3.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.20	0.42	0.76	3.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.20	0.50	0.92	2.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.20	0.00	0.67	3.53	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	4.20	0.00	0.59	3.61	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	4.20	0.25	0.50	2.69	0.00	0.25	0.00	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	4.20	0.08	0.92	2.44	0.00	0.00	0.00	0.00	0.00	0.76	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	4.20	0.25	1.43	1.93	0.00	0.08	0.08	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	4.20	0.17	1.26	2.18	0.00	0.08	0.08	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	4.20	0.25	0.92	1.76	0.00	0.76	0.17	0.00	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	4.20	0.17	0.92	1.43	0.00	1.01	0.08	0.00	0.00	0.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	4.20	0.17	1.18	1.76	0.00	0.59	0.08	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	4.20	0.25	0.92	1.34	0.00	0.92	0.25	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	4.20	0.08	0.84	1.76	0.00	0.84	0.17	0.00	0.00	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	4.20	0.17	0.92	1.85	0.00	0.76	0.17	0.00	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	8.40	0.50	2.10	2.69	0.00	2.18	0.17	0.00	0.00	0.76	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	8.40	0.50	2.69	2.77	0.00	1.51	0.25	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	8.40	0.59	2.94	3.11	0.00	1.26	0.17	0.00	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	4.20	0.17	1.43	2.10	0.00	0.08	0.00	0.00	0.00	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	4.20	0.17	1.09	2.18	0.00	0.08	0.00	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	4.20	0.08	0.92	2.44	0.00	0.08	0.00	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	4.20	0.25	0.67	2.52	0.00	0.08	0.00	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	4.20	0.25	0.67	2.44	0.00	0.17	0.00	0.00	0.00	0.67	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L159	Chatham Road North (SB)	San Lau Street	San Wai Street	0:00	1:00	12.00	0.80	3.36	5.72	0.00	0.28	0.00	0.00	0.00	0.44	0.04	0.04	0.08	0.04	0.04	0.00	1.16	
				1:00	2:00	8.00	0.52	2.52	4.48	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.20	
				2:00	3:00	6.00	0.36	1.76	3.56	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.16
				3:00	4:00	6.00	0.44	1.88	3.16	0.00	0.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24
				4:00	5:00	4.00	0.28	1.12	2.32	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.16
				5:00	6:00	6.00	0.36	1.64	3.36	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.04	0.04	0.12	0.04	0.04	0.00	0.00
				6:00	7:00	10.00	0.44	2.16	4.52	0.04	0.48	0.12	0.00	0.00	0.44	0.04	0.08	0.16	0.12	0.04	0.00	1.36	
				7:00	8:00	30.00	0.96	7.04	9.64	0.20	1.72	0.52	0.08	0.08	1.04	0.20	0.20	0.52	0.44	0.20	0.00	7.16	
				8:00	9:00	44.00	2.72	14.40	10.24	0.36	4.12	2.44	0.48	0.44	0.84	0.28	0.28	0.52	0.44	0.16	0.00	6.28	
				9:00	10:00	40.00	1.92	10.88	10.20	0.28	3.80	2.52	0.48	0.44	0.68	0.36	0.28	0.64	0.60	0.20	0.00	6.72	
				10:00	11:00	34.00	1.76	9.52	8.00	0.36	4.84	2.28	0.56	0.52	0.84	0.08	0.12	0.32	0.24	0.08	0.00	4.48	
				11:00	12:00	32.00	1.12	8.80	6.64	0.52	5.92	0.84	0.24	0.16	1.52	0.28	0.16	0.40	0.40	0.12	0.00	4.88	
				12:00	13:00	32.00	1.36	11.52	8.16	0.32	3.84	0.76	0.24	0.16	1.12	0.32	0.08	0.16	0.16	0.04	0.00	3.76	
				13:00	14:00	32.00	1.04	7.84	5.48	0.40	4.72	2.64	0.68	0.64	1.08	0.08	0.32	0.72	0.72	0.24	0.00	5.40	
				14:00	15:00	30.00	0.84	7.04	7.28	0.36	4.32	2.00	0.48	0.44	1.16	0.12	0.20	0.48	0.44	0.16	0.00	4.68	
				15:00	16:00	36.00	1.48	9.60	8.76	0.40	4.84	2.52	0.64	0.56	1.04	0.24	0.20	0.40	0.36	0.12	0.00	4.84	
				16:00	17:00	28.00	1.32	11.28	5.48	0.28	2.76	0.60	0.16	0.16	1.40	0.04	0.12	0.32	0.36	0.12	0.00	3.60	
				17:00	18:00	36.00	2.00	16.92	6.88	0.24	2.20	1.20	0.40	0.36	1.48	0.08	0.08	0.20	0.20	0.08	0.00	3.68	
				18:00	19:00	30.00	1.76	15.24	6.36	0.12	1.52	0.68	0.24	0.24	0.56	0.04	0.08	0.24	0.24	0.08	0.00	2.60	
				19:00	20:00	30.00	1.52	13.36	8.52	0.04	0.68	0.32	0.04	0.08	1.12	0.08	0.08	0.16	0.16	0.04	0.00	3.80	
				20:00	21:00	26.00	1.32	9.40	8.92	0.04	0.52	0.12	0.04	0.04	1.56	0.04	0.04	0.08	0.04	0.04	0.00	3.80	
				21:00	22:00	24.00	0.92	7.52	9.12	0.04	0.64	0.20	0.04	0.04	1.32	0.00	0.00	0.04	0.00	0.00	0.00	4.12	
				22:00	23:00	22.00	1.08	5.32	8.48	0.04	0.32	0.00	0.00	0.00	1.28	0.04	0.04	0.08	0.04	0.04	0.00	5.24	
				23:00	0:00	18.00	0.92	4.20	6.92	0.04	0.32	0.00	0.00	0.00	1.08	0.04	0.04	0.08	0.04	0.04	0.00	4.28	
L160	Chatham Road North (SB)	San Wai Street	Pak Kung Street	0:00	1:00	12.90	0.82	3.44	6.15	0.00	0.30	0.00	0.00	0.00	0.56	0.04	0.00	0.09	0.04	0.04	0.00	1.42	
				1:00	2:00	6.45	0.43	1.94	3.70	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	
				2:00	3:00	6.45	0.39	1.81	3.91	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.22	
				3:00	4:00	4.30	0.34	1.29	2.32	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.17	
				4:00	5:00	4.30	0.30	1.16	2.49	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.22	
				5:00	6:00	4.30	0.26	1.16	2.54	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.04	0.04	0.00	0.00	
				6:00	7:00	10.75	0.47	2.24	4.95	0.04	0.47	0.09	0.00	0.00	0.60	0.04	0.04	0.13	0.09	0.04	0.00	1.55	
				7:00	8:00	30.10	0.90	6.54	9.98	0.17	1.55	0.47	0.09	0.09	1.33	0.17	0.17	0.34	0.34	0.17	0.00	7.78	
				8:00	9:00	43.00	2.62	13.50	10.49	0.30	3.74	2.32	0.52	0.43	1.03	0.17	0.17	0.34	0.34	0.17	0.00	6.84	
				9:00	10:00	38.70	1.76	9.89	10.36	0.26	3.40	2.41	0.47	0.43	0.90	0.26	0.22	0.43	0.47	0.22	0.00	7.22	
				10:00	11:00	32.25	1.68	8.64	7.83	0.34	4.47	2.06	0.52	0.47	0.99	0.09	0.09	0.22	0.17	0.09	0.00	4.60	
				11:00	12:00	32.25	1.12	8.64	6.88	0.47	5.72	0.82	0.22	0.22	1.85	0.22	0.13	0.34	0.26	0.13	0.00	5.25	
				12:00	13:00	32.25	1.38	11.14	8.47	0.34	3.70	0.73	0.22	0.22	1.38	0.26	0.04	0.13	0.13	0.04	0.00	4.09	
				13:00	14:00	32.25	1.03	7.61	5.68	0.43	4.64	2.58	0.65	0.65	1.33	0.09	0.26	0.60	0.56	0.26	0.00	5.89	
				14:00	15:00	30.10	0.86	6.88	7.40	0.34	4.17	1.94	0.47	0.43	1.38	0.13	0.17	0.34	0.34	0.17	0.00	5.07	
				15:00	16:00	36.55	1.51	9.42	9.16	0.39	4.77	2.49	0.60	0.60	1.25	0.22	0.13	0.34	0.26	0.13	0.00	5.29	
				16:00	17:00	30.10	1.46	11.65	5.89	0.26	3.10	0.60	0.17	0.17	1.76	0.04	0.17	0.34	0.26	0.17	0.00	4.04	
				17:00	18:00	36.55	2.06	16.81	7.10	0.22	2.32	1.12	0.39	0.34	1.72	0.09	0.09	0.22	0.13	0.09	0.00	3.87	
				18:00	19:00	32.25	1.94	16.08	6.97	0.13	1.72	0.69	0.22	0.22	0.73	0.04	0.09	0.26	0.22	0.09	0.00	2.88	
				19:00	20:00	32.25	1.63	13.76	9.37	0.04	0.69	0.34	0.04	0.09	1.42	0.09	0.04	0.13	0.13	0.04	0.00	4.43	
				20:00	21:00	25.80	1.29	8.94	8.86	0.04	0.47	0.09	0.04	0.04	1.81	0.04	0.00	0.04	0.04	0.04	0.00	4.04	
				21:00	22:00	23.65	0.82	7.01	9.07	0.04	0.56	0.22	0.04	0.04	1.55	0.00	0.00	0.00	0.00	0.00	0.00	4.30	
				22:00	23:00	23.65	1.12	5.33	9.12	0.04	0.30	0.00	0.00	0.00	1.59	0.04	0.04	0.09	0.04	0.04	0.00	5.89	
				23:00	0:00	19.35	0.95	4.21	7.44	0.04	0.30	0.00	0.00	0.00	1.38	0.04	0.04	0.09	0.04	0.04	0.00	4.77	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L161	Chatham Road North (NB)	Pak Kung Street	San Lau Street	0:00	1:00	24.75	1.88	4.65	11.88	0.00	0.79	0.00	0.00	0.00	2.18	0.10	0.10	0.40	0.20	0.10	0.00	2.48
				1:00	2:00	14.85	1.29	3.27	9.01	0.00	0.79	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.50
				2:00	3:00	14.85	1.09	2.97	9.60	0.00	0.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.59
				3:00	4:00	9.90	0.99	2.18	5.54	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.50
				4:00	5:00	9.90	0.79	1.88	6.14	0.00	0.40	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	0.59
				5:00	6:00	9.90	0.69	1.68	5.84	0.00	0.79	0.00	0.00	0.10	0.00	0.10	0.10	0.30	0.20	0.10	0.00	0.00
				6:00	7:00	24.75	1.19	3.27	10.40	0.10	1.49	0.20	0.00	0.10	2.18	0.40	0.40	0.79	0.69	0.40	0.00	3.17
				7:00	8:00	64.35	1.78	10.20	22.67	0.20	2.48	0.79	0.50	0.30	8.02	0.79	0.79	1.78	1.58	0.69	0.00	11.78
				8:00	9:00	84.15	4.85	20.59	22.87	0.40	5.74	3.56	1.88	1.88	6.14	0.89	0.89	1.98	1.68	0.79	0.00	10.00
				9:00	10:00	79.20	3.37	15.44	22.97	0.40	5.35	3.76	1.78	1.98	5.25	1.39	1.09	2.48	2.18	0.99	0.00	10.79
				10:00	11:00	74.25	4.06	12.47	16.43	0.79	14.06	4.85	2.28	2.18	3.86	0.50	0.59	1.29	1.29	0.50	0.00	9.11
				11:00	12:00	74.25	2.57	12.08	14.06	0.99	17.33	1.88	0.99	0.79	6.83	1.58	0.79	1.88	1.78	0.59	0.00	10.10
				12:00	13:00	64.35	3.07	14.65	16.34	0.59	10.69	1.49	0.89	0.69	4.75	1.58	0.40	0.69	0.79	0.30	0.00	7.43
				13:00	14:00	74.25	2.28	10.20	11.19	0.79	13.66	5.54	2.57	2.57	4.75	0.40	1.68	3.37	3.37	1.09	0.00	10.79
				14:00	15:00	69.30	1.88	9.41	15.35	0.69	12.57	4.26	1.98	1.88	4.95	0.69	1.09	2.18	2.08	0.79	0.00	9.50
				15:00	16:00	79.20	3.37	12.47	17.72	0.69	13.86	5.35	2.57	2.57	4.55	1.29	0.89	1.88	1.68	0.59	0.00	9.70
				16:00	17:00	69.30	3.96	14.06	10.10	0.69	15.74	2.08	0.79	0.79	4.06	0.20	1.19	2.28	2.67	0.69	0.00	10.00
				17:00	18:00	79.20	5.94	20.89	12.57	0.59	12.38	4.06	1.88	1.68	4.16	0.79	0.79	1.49	1.49	0.50	0.00	10.00
				18:00	19:00	64.35	5.35	19.40	11.88	0.30	8.61	2.28	1.09	0.99	1.68	0.59	0.99	1.68	1.88	0.50	0.00	7.13
				19:00	20:00	59.40	3.56	18.22	18.51	0.10	1.98	0.69	0.20	0.30	5.15	0.50	0.40	0.79	0.79	0.30	0.00	7.92
				20:00	21:00	49.50	2.87	11.88	17.82	0.10	1.39	0.20	0.20	0.20	6.53	0.20	0.10	0.30	0.20	0.10	0.00	7.43
				21:00	22:00	49.50	2.08	10.30	19.31	0.10	1.88	0.50	0.20	0.20	6.14	0.00	0.00	0.10	0.00	0.10	0.00	8.61
				22:00	23:00	49.50	2.67	7.62	19.01	0.10	0.99	0.00	0.00	0.00	6.24	0.20	0.20	0.50	0.20	0.20	0.00	11.58
				23:00	0:00	39.60	2.18	5.94	15.05	0.10	1.09	0.00	0.00	0.00	4.95	0.20	0.20	0.40	0.20	0.20	0.00	9.11
L162	Ko Shan Road (SB)	San Lau Street	Wai Yin Path	0:00	1:00	7.00	0.28	2.00	4.31	0.00	0.14	0.00	0.00	0.00	0.21	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				1:00	2:00	3.50	0.11	1.02	2.31	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	3.50	0.14	0.91	2.42	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.50	0.18	1.02	2.21	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.50	0.14	0.91	2.38	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.50	0.11	0.91	2.24	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.07	0.00	0.07	0.00	0.00
				6:00	7:00	5.25	0.14	1.23	3.29	0.00	0.18	0.04	0.00	0.00	0.21	0.04	0.04	0.04	0.04	0.04	0.00	0.00
				7:00	8:00	10.50	0.25	3.71	4.24	0.04	0.60	0.14	0.04	0.04	0.70	0.14	0.14	0.21	0.21	0.07	0.00	0.00
				8:00	9:00	15.75	0.70	7.28	4.31	0.04	1.33	0.56	0.18	0.18	0.49	0.11	0.11	0.25	0.18	0.07	0.00	0.00
				9:00	10:00	14.00	0.49	5.53	4.34	0.04	1.26	0.60	0.14	0.14	0.42	0.21	0.14	0.32	0.28	0.11	0.00	0.00
				10:00	11:00	14.00	0.53	4.83	5.18	0.04	1.89	0.56	0.14	0.14	0.35	0.04	0.04	0.14	0.07	0.07	0.00	0.00
				11:00	12:00	12.25	0.32	4.34	4.06	0.07	2.14	0.18	0.04	0.04	0.63	0.11	0.04	0.14	0.11	0.07	0.00	0.00
				12:00	13:00	14.00	0.42	5.85	5.22	0.04	1.47	0.21	0.04	0.04	0.46	0.11	0.04	0.07	0.04	0.04	0.00	0.00
				13:00	14:00	12.25	0.32	3.99	3.61	0.04	1.89	0.67	0.18	0.21	0.49	0.04	0.14	0.28	0.21	0.21	0.00	0.00
				14:00	15:00	12.25	0.28	3.64	4.73	0.04	1.72	0.49	0.14	0.14	0.49	0.04	0.07	0.21	0.14	0.14	0.00	0.00
				15:00	16:00	14.00	0.42	4.59	5.39	0.04	1.79	0.60	0.14	0.14	0.42	0.07	0.07	0.14	0.11	0.11	0.00	0.00
				16:00	17:00	14.00	0.49	5.85	5.29	0.04	1.40	0.21	0.04	0.04	0.46	0.00	0.00	0.07	0.00	0.14	0.00	0.00
				17:00	18:00	17.50	0.70	8.23	6.37	0.04	1.05	0.35	0.07	0.07	0.49	0.00	0.00	0.04	0.00	0.11	0.00	0.00
				18:00	19:00	15.75	0.63	7.74	6.06	0.04	0.74	0.18	0.04	0.04	0.18	0.00	0.00	0.04	0.00	0.11	0.00	0.00
				19:00	20:00	14.00	0.46	6.90	5.57	0.00	0.28	0.07	0.00	0.04	0.49	0.04	0.04	0.07	0.04	0.04	0.00	0.00
				20:00	21:00	12.25	0.42	4.90	5.92	0.00	0.21	0.04	0.00	0.00	0.70	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				21:00	22:00	10.50	0.28	3.71	5.67	0.00	0.25	0.04	0.00	0.00	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	8.75	0.32	2.56	5.18	0.00	0.11	0.00	0.00	0.00	0.53	0.00	0.00	0.04	0.00	0.04	0.00	0.00
				23:00	0:00	7.00	0.25	2.00	4.17	0.00	0.11	0.00	0.00	0.00	0.42	0.00	0.00	0.04	0.00	0.04	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L163	Ko Shan Road (NB)	Wai Yin Path	San Lau Street	0:00	1:00	7.00	0.21	1.68	4.62	0.00	0.11	0.00	0.00	0.00	0.32	0.00	0.00	0.04	0.04	0.00	0.00	0.00
				1:00	2:00	5.25	0.14	1.30	3.71	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	5.25	0.14	1.16	3.85	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	3.50	0.14	0.84	2.42	0.00	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	3.50	0.11	0.77	2.56	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	3.50	0.11	0.74	2.45	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.04	0.00	0.00
				6:00	7:00	5.25	0.11	0.98	3.43	0.00	0.18	0.04	0.00	0.00	0.28	0.04	0.04	0.07	0.07	0.04	0.00	0.00
				7:00	8:00	14.00	0.11	3.12	8.40	0.04	0.53	0.07	0.04	0.04	0.91	0.11	0.11	0.25	0.21	0.11	0.00	0.00
				8:00	9:00	19.25	0.28	6.44	8.93	0.07	1.26	0.32	0.21	0.21	0.74	0.11	0.11	0.28	0.21	0.11	0.00	0.00
				9:00	10:00	17.50	0.21	4.80	8.75	0.07	1.16	0.35	0.21	0.18	0.63	0.18	0.14	0.39	0.32	0.14	0.00	0.00
				10:00	11:00	15.75	0.53	4.69	6.48	0.07	1.96	0.63	0.21	0.18	0.60	0.04	0.07	0.18	0.11	0.04	0.00	0.00
				11:00	12:00	14.00	0.32	4.20	5.18	0.11	2.28	0.21	0.07	0.07	0.98	0.14	0.07	0.18	0.18	0.04	0.00	0.00
				12:00	13:00	15.75	0.42	5.64	6.58	0.07	1.54	0.21	0.07	0.07	0.77	0.18	0.04	0.07	0.07	0.04	0.00	0.00
				13:00	14:00	14.00	0.32	3.96	4.59	0.07	1.96	0.77	0.28	0.21	0.77	0.04	0.21	0.39	0.35	0.11	0.00	0.00
				14:00	15:00	14.00	0.25	3.54	6.02	0.07	1.75	0.56	0.18	0.14	0.81	0.07	0.11	0.25	0.21	0.07	0.00	0.00
				15:00	16:00	17.50	0.46	4.90	7.25	0.07	2.00	0.74	0.28	0.25	0.77	0.18	0.11	0.25	0.21	0.07	0.00	0.00
				16:00	17:00	14.00	0.74	5.99	3.99	0.04	1.61	0.32	0.07	0.04	0.84	0.04	0.11	0.11	0.14	0.00	0.00	0.00
				17:00	18:00	19.25	1.12	9.45	5.25	0.04	1.30	0.67	0.14	0.11	0.91	0.07	0.07	0.07	0.07	0.00	0.00	0.00
				18:00	19:00	15.75	0.95	8.19	4.62	0.04	0.84	0.35	0.07	0.07	0.35	0.04	0.07	0.07	0.11	0.00	0.00	0.00
				19:00	20:00	15.75	0.46	6.76	7.07	0.00	0.28	0.07	0.04	0.04	0.81	0.04	0.04	0.07	0.07	0.04	0.00	0.00
				20:00	21:00	14.00	0.39	4.73	7.35	0.00	0.21	0.04	0.04	0.04	1.12	0.04	0.00	0.04	0.04	0.00	0.00	0.00
				21:00	22:00	12.25	0.25	3.61	7.14	0.00	0.25	0.07	0.04	0.04	0.88	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	12.25	0.35	2.94	7.67	0.00	0.18	0.00	0.00	0.00	0.98	0.04	0.04	0.04	0.04	0.00	0.00	0.00
				L164	Wai Yin Path (EB)	Cul-de-sac	Ko Shan Road	23:00	0:00	8.75	0.28	2.03	5.46	0.00	0.14	0.00	0.00	0.00	0.70	0.04	0.04	0.04
0:00	1:00	2.60	0.00					1.61	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	2.60	0.00					1.56	1.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.60	0.00					1.56	1.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.60	0.00					1.72	0.88	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.60	0.00					1.72	0.88	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.60	0.00					1.30	1.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	2.60	0.00					1.30	1.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
7:00	8:00	2.60	0.00					1.98	0.31	0.16	0.00	0.00	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
8:00	9:00	2.60	0.00					1.92	0.16	0.05	0.05	0.16	0.21	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
9:00	10:00	2.60	0.00					1.72	0.16	0.10	0.10	0.16	0.26	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
10:00	11:00	2.60	0.00					1.25	0.47	0.10	0.21	0.21	0.26	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
11:00	12:00	2.60	0.00					1.46	0.47	0.10	0.36	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
12:00	13:00	2.60	0.00					1.61	0.47	0.10	0.21	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
13:00	14:00	2.60	0.00					1.14	0.31	0.10	0.21	0.31	0.42	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
14:00	15:00	2.60	0.00					1.14	0.52	0.10	0.21	0.21	0.31	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
15:00	16:00	2.60	0.00					1.20	0.47	0.10	0.16	0.26	0.31	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
16:00	17:00	2.60	0.00					1.35	0.73	0.00	0.31	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
17:00	18:00	2.60	0.00					1.46	0.68	0.00	0.16	0.16	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
18:00	19:00	2.60	0.00					1.61	0.68	0.00	0.10	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
19:00	20:00	2.60	0.00					2.03	0.57	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
20:00	21:00	2.60	0.00					1.82	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	2.60	0.00					1.66	0.94	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	2.60	0.00					1.51	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	2.60	0.00	1.51	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L165	Wai Yin Path (WB)	Ko Shan Road	Cul-de-sac	0:00	1:00	2.35	0.71	1.41	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.35	0.80	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.35	0.61	1.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.35	0.80	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.35	0.80	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.35	0.80	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.35	0.66	1.36	0.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.35	0.52	1.74	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.70	1.27	2.91	0.09	0.00	0.00	0.14	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.70	1.22	2.82	0.09	0.00	0.00	0.19	0.19	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.35	0.52	1.27	0.09	0.00	0.09	0.14	0.14	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.35	0.38	1.50	0.09	0.00	0.19	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.35	0.38	1.60	0.09	0.00	0.09	0.09	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.35	0.33	1.18	0.09	0.00	0.14	0.24	0.24	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.35	0.33	1.36	0.09	0.00	0.09	0.19	0.19	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.35	0.42	1.13	0.09	0.00	0.14	0.24	0.24	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.35	0.00	1.50	0.14	0.00	0.42	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.35	0.00	1.69	0.09	0.00	0.19	0.19	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.35	0.00	1.79	0.14	0.00	0.14	0.14	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.35	0.42	1.83	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.35	0.52	1.74	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.35	0.42	1.79	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.35	0.61	1.60	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
23:00	0:00	2.35	0.71	1.46	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00				
L166	Ko Shan Road (NB)	Pak Kung Street	Wai Yin Path	0:00	1:00	11.75	0.42	2.77	7.76	0.00	0.19	0.00	0.00	0.00	0.52	0.00	0.00	0.05	0.05	0.00	0.00	0.00	
				1:00	2:00	7.05	0.24	1.69	4.94	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	7.05	0.19	1.50	5.22	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.70	0.19	1.18	3.20	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.70	0.14	1.03	3.48	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	4.70	0.09	0.99	3.34	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.05	0.00	0.00	0.00	
				6:00	7:00	9.40	0.24	1.74	6.30	0.00	0.28	0.05	0.00	0.00	0.47	0.05	0.05	0.09	0.09	0.05	0.00	0.00	
				7:00	8:00	21.15	0.24	4.79	12.74	0.05	0.80	0.09	0.05	0.05	1.27	0.14	0.14	0.38	0.28	0.14	0.00	0.00	
				8:00	9:00	28.20	0.47	9.54	13.16	0.09	1.88	0.47	0.24	0.24	0.99	0.14	0.14	0.38	0.33	0.14	0.00	0.00	
				9:00	10:00	25.85	0.33	7.14	13.16	0.09	1.74	0.56	0.24	0.24	0.85	0.24	0.19	0.47	0.42	0.19	0.00	0.00	
				10:00	11:00	21.15	0.71	6.30	8.93	0.09	2.59	0.80	0.24	0.24	0.75	0.05	0.09	0.19	0.14	0.05	0.00	0.00	
				11:00	12:00	21.15	0.52	6.35	7.99	0.14	3.38	0.38	0.09	0.09	1.41	0.19	0.09	0.24	0.24	0.05	0.00	0.00	
				12:00	13:00	23.50	0.61	8.41	9.92	0.09	2.30	0.33	0.09	0.09	1.08	0.28	0.05	0.09	0.09	0.05	0.00	0.00	
				13:00	14:00	18.80	0.42	5.36	6.35	0.09	2.68	1.03	0.28	0.28	0.94	0.05	0.24	0.47	0.47	0.14	0.00	0.00	
				14:00	15:00	21.15	0.38	5.36	9.17	0.09	2.68	0.85	0.24	0.24	1.13	0.09	0.14	0.38	0.33	0.09	0.00	0.00	
				15:00	16:00	23.50	0.61	6.63	10.06	0.09	2.73	0.94	0.28	0.28	0.94	0.19	0.14	0.28	0.24	0.09	0.00	0.00	
				16:00	17:00	18.80	0.94	8.13	5.36	0.05	2.16	0.42	0.09	0.05	1.08	0.05	0.14	0.14	0.19	0.00	0.00	0.00	
				17:00	18:00	25.85	1.50	12.74	7.05	0.05	1.74	0.89	0.19	0.14	1.18	0.09	0.09	0.09	0.09	0.00	0.00	0.00	
				18:00	19:00	23.50	1.46	12.22	6.91	0.05	1.27	0.52	0.09	0.09	0.52	0.05	0.09	0.09	0.14	0.00	0.00	0.00	
				19:00	20:00	23.50	0.66	10.11	10.67	0.00	0.42	0.09	0.05	0.05	1.13	0.05	0.05	0.09	0.09	0.05	0.00	0.00	
				20:00	21:00	18.80	0.52	6.35	10.01	0.00	0.28	0.05	0.05	0.05	1.36	0.05	0.00	0.05	0.05	0.00	0.00	0.00	
				21:00	22:00	18.80	0.38	5.50	11.09	0.00	0.38	0.09	0.05	0.05	1.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	16.45	0.52	3.95	10.34	0.00	0.19	0.00	0.00	0.00	1.27	0.05	0.05	0.05	0.05	0.00	0.00	0.00	
				23:00	0:00	14.10	0.42	3.29	8.88	0.00	0.24	0.00	0.00	0.00	1.08	0.05	0.05	0.05	0.05	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L167	Ko Shan Road (SB)	Wai Yin Path	Pak Kung Street	0:00	1:00	9.40	0.38	2.73	5.73	0.00	0.19	0.00	0.00	0.00	0.28	0.00	0.00	0.05	0.00	0.05	0.00	0.00
				1:00	2:00	4.70	0.14	1.36	3.10	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.70	0.19	1.27	3.20	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.70	0.24	1.36	2.96	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.70	0.19	1.27	3.15	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.70	0.14	1.18	3.06	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.09	0.00
				6:00	7:00	7.05	0.19	1.65	4.37	0.00	0.24	0.05	0.00	0.00	0.28	0.05	0.05	0.09	0.05	0.05	0.00	0.00
				7:00	8:00	14.10	0.33	4.89	5.73	0.05	0.85	0.19	0.05	0.05	0.94	0.19	0.19	0.28	0.28	0.09	0.00	0.00
				8:00	9:00	18.80	0.80	8.60	5.08	0.05	1.69	0.71	0.19	0.19	0.61	0.14	0.14	0.28	0.24	0.09	0.00	0.00
				9:00	10:00	18.80	0.66	7.33	5.78	0.05	1.79	0.85	0.19	0.19	0.56	0.28	0.19	0.42	0.38	0.14	0.00	0.00
				10:00	11:00	18.80	0.66	6.49	7.05	0.05	2.49	0.80	0.19	0.19	0.47	0.05	0.05	0.14	0.09	0.09	0.00	0.00
				11:00	12:00	16.45	0.42	5.73	5.50	0.09	2.87	0.28	0.05	0.05	0.85	0.14	0.05	0.19	0.14	0.09	0.00	0.00
				12:00	13:00	18.80	0.56	7.80	7.05	0.05	1.97	0.28	0.05	0.05	0.61	0.14	0.05	0.09	0.05	0.05	0.00	0.00
				13:00	14:00	16.45	0.42	5.36	4.84	0.05	2.49	0.94	0.24	0.28	0.61	0.05	0.19	0.42	0.28	0.28	0.00	0.00
				14:00	15:00	16.45	0.38	4.94	6.53	0.05	2.26	0.66	0.14	0.14	0.66	0.05	0.09	0.28	0.14	0.14	0.00	0.00
				15:00	16:00	18.80	0.52	6.20	7.24	0.05	2.35	0.85	0.19	0.19	0.56	0.09	0.09	0.19	0.14	0.14	0.00	0.00
				16:00	17:00	18.80	0.66	7.94	7.19	0.05	1.69	0.24	0.05	0.05	0.61	0.00	0.00	0.14	0.00	0.19	0.00	0.00
				17:00	18:00	23.50	0.94	11.19	8.60	0.05	1.27	0.42	0.09	0.09	0.61	0.00	0.00	0.09	0.00	0.14	0.00	0.00
				18:00	19:00	21.15	0.85	10.43	8.13	0.05	0.89	0.24	0.05	0.05	0.24	0.00	0.00	0.09	0.00	0.14	0.00	0.00
				19:00	20:00	18.80	0.61	9.21	7.52	0.00	0.38	0.09	0.00	0.05	0.66	0.05	0.05	0.09	0.05	0.05	0.00	0.00
				20:00	21:00	16.45	0.56	6.63	7.90	0.00	0.28	0.05	0.00	0.00	0.94	0.00	0.00	0.05	0.00	0.05	0.00	0.00
				21:00	22:00	14.10	0.38	4.98	7.66	0.00	0.33	0.05	0.00	0.00	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	11.75	0.42	3.43	6.96	0.00	0.14	0.00	0.00	0.00	0.71	0.00	0.00	0.05	0.00	0.05	0.00	0.00
L168	Pak Kung Street (EB)	Shun Yung Street	Chatham Road North	23:00	0:00	9.40	0.33	2.68	5.59	0.00	0.14	0.00	0.00	0.00	0.56	0.00	0.00	0.05	0.00	0.05	0.00	0.00
				0:00	1:00	4.80	0.19	1.78	2.64	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00
				1:00	2:00	2.40	0.10	0.91	1.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.40	0.10	0.82	1.49	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.40	0.14	0.96	1.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.40	0.14	0.82	1.44	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.40	0.10	0.77	1.34	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00
				6:00	7:00	2.40	0.05	0.77	1.34	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.14	0.00	0.00
				7:00	8:00	7.20	0.14	2.64	3.50	0.05	0.14	0.05	0.05	0.05	0.00	0.00	0.00	0.19	0.00	0.38	0.00	0.00
				8:00	9:00	12.00	0.43	5.81	3.89	0.10	0.43	0.24	0.24	0.19	0.00	0.00	0.00	0.24	0.00	0.43	0.00	0.00
				9:00	10:00	9.60	0.29	4.03	3.50	0.10	0.38	0.19	0.19	0.14	0.00	0.00	0.00	0.24	0.00	0.53	0.00	0.00
				10:00	11:00	7.20	0.29	3.02	2.16	0.19	0.58	0.43	0.19	0.10	0.00	0.00	0.00	0.05	0.00	0.19	0.00	0.00
				11:00	12:00	7.20	0.19	3.22	2.06	0.19	0.86	0.19	0.05	0.05	0.00	0.00	0.00	0.14	0.00	0.24	0.00	0.00
				12:00	13:00	7.20	0.24	3.79	2.26	0.10	0.48	0.10	0.05	0.05	0.00	0.00	0.00	0.05	0.00	0.10	0.00	0.00
				13:00	14:00	7.20	0.19	2.69	1.68	0.19	0.72	0.58	0.24	0.14	0.00	0.00	0.00	0.24	0.00	0.53	0.00	0.00
				14:00	15:00	7.20	0.19	2.59	2.26	0.19	0.67	0.43	0.19	0.14	0.00	0.00	0.00	0.19	0.00	0.34	0.00	0.00
				15:00	16:00	7.20	0.24	2.88	2.26	0.14	0.58	0.43	0.24	0.10	0.00	0.00	0.00	0.10	0.00	0.24	0.00	0.00
				16:00	17:00	4.80	0.24	3.02	0.62	0.14	0.53	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	7.20	0.38	4.85	0.86	0.14	0.38	0.53	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.80	0.24	3.41	0.62	0.05	0.19	0.24	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	7.20	0.29	4.32	2.30	0.00	0.10	0.05	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.10	0.00	0.00
				20:00	21:00	4.80	0.19	2.50	2.02	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00
				21:00	22:00	4.80	0.14	2.26	2.30	0.00	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	4.80	0.19	1.92	2.59	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.00
				23:00	0:00	4.80	0.24	1.87	2.50	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L169	Pak Kung Street (WB)	Chatham Road North	Shun Yung Street	0:00	1:00	4.80	0.38	1.44	2.74	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.05	0.00	0.05	
				1:00	2:00	4.80	0.34	1.49	2.78	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.40	0.14	0.67	1.54	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.40	0.19	0.72	1.34	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.40	0.14	0.67	1.49	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.40	0.14	0.62	1.39	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.05	0.00	0.05	0.00	0.00
				6:00	7:00	4.80	0.29	1.15	2.83	0.00	0.19	0.05	0.00	0.00	0.00	0.05	0.05	0.05	0.05	0.05	0.05	0.00	0.05
				7:00	8:00	9.60	0.43	3.41	4.27	0.05	0.29	0.05	0.05	0.05	0.00	0.10	0.10	0.10	0.14	0.14	0.14	0.00	0.38
				8:00	9:00	14.40	1.20	6.67	4.13	0.05	0.67	0.24	0.24	0.24	0.00	0.10	0.10	0.14	0.14	0.14	0.00	0.34	
				9:00	10:00	12.00	0.82	4.70	3.98	0.05	0.62	0.24	0.24	0.24	0.00	0.14	0.10	0.19	0.19	0.19	0.00	0.29	
				10:00	11:00	12.00	0.77	3.89	3.60	0.05	1.82	0.72	0.24	0.24	0.05	0.05	0.05	0.14	0.10	0.10	0.00	0.19	
				11:00	12:00	12.00	0.53	3.94	3.36	0.10	2.45	0.29	0.10	0.10	0.05	0.19	0.05	0.24	0.19	0.19	0.00	0.24	
				12:00	13:00	12.00	0.62	4.90	3.79	0.05	1.54	0.29	0.10	0.10	0.05	0.14	0.05	0.10	0.10	0.05	0.00	0.14	
				13:00	14:00	12.00	0.53	3.46	2.74	0.05	2.02	0.91	0.34	0.34	0.05	0.05	0.19	0.38	0.34	0.34	0.00	0.29	
				14:00	15:00	12.00	0.38	3.41	3.79	0.05	1.92	0.67	0.29	0.24	0.05	0.05	0.14	0.29	0.24	0.24	0.00	0.24	
				15:00	16:00	14.40	0.67	4.46	4.42	0.05	2.11	0.91	0.34	0.34	0.05	0.10	0.10	0.24	0.19	0.19	0.00	0.24	
				16:00	17:00	12.00	0.58	4.46	3.02	0.05	2.21	0.48	0.10	0.05	0.10	0.00	0.10	0.29	0.24	0.19	0.00	0.14	
				17:00	18:00	16.80	0.96	7.25	4.22	0.05	1.92	0.96	0.24	0.19	0.10	0.05	0.05	0.24	0.19	0.19	0.00	0.19	
				18:00	19:00	14.40	0.82	6.77	3.89	0.05	1.30	0.53	0.10	0.10	0.05	0.05	0.05	0.24	0.24	0.14	0.00	0.10	
				19:00	20:00	12.00	0.67	6.00	4.22	0.00	0.29	0.10	0.05	0.05	0.05	0.05	0.05	0.10	0.10	0.05	0.00	0.24	
				20:00	21:00	9.60	0.58	4.08	4.32	0.00	0.19	0.05	0.05	0.05	0.05	0.00	0.00	0.05	0.00	0.05	0.00	0.14	
				21:00	22:00	9.60	0.43	3.60	4.85	0.00	0.29	0.05	0.05	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.24	
				22:00	23:00	7.20	0.43	2.26	4.03	0.00	0.10	0.00	0.00	0.00	0.00	0.05	0.00	0.00	0.05	0.00	0.05	0.00	0.24
L170	Pak Kung Street (EB)	Chatham Road North	Gillies Avenue North	23:00	0:00	7.20	0.48	2.16	3.98	0.00	0.19	0.00	0.00	0.00	0.00	0.05	0.00	0.00	0.05	0.00	0.05	0.00	0.24
				0:00	1:00	4.55	0.18	1.55	2.64	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	
				1:00	2:00	4.55	0.18	1.55	2.64	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	4.55	0.18	1.37	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	4.55	0.27	1.64	2.37	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	4.55	0.27	1.37	2.91	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	4.55	0.18	1.27	2.73	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	0.00	0.00	0.00	
				6:00	7:00	4.55	0.09	1.18	2.64	0.00	0.09	0.09	0.00	0.00	0.00	0.09	0.09	0.09	0.09	0.09	0.00	0.00	
				7:00	8:00	9.10	0.18	3.55	3.55	0.09	0.36	0.09	0.00	0.00	0.00	0.18	0.18	0.36	0.36	0.18	0.00	0.00	
				8:00	9:00	13.65	0.64	6.55	3.37	0.18	0.73	0.73	0.18	0.18	0.00	0.18	0.18	0.27	0.27	0.18	0.00	0.00	
				9:00	10:00	13.65	0.46	5.46	3.73	0.18	0.82	0.82	0.18	0.18	0.00	0.36	0.18	0.46	0.46	0.36	0.00	0.00	
				10:00	11:00	13.65	0.46	5.01	4.00	0.27	1.55	1.27	0.18	0.18	0.00	0.09	0.09	0.27	0.18	0.09	0.00	0.00	
				11:00	12:00	13.65	0.36	4.82	3.64	0.36	2.09	0.46	0.09	0.09	0.00	0.36	0.27	0.46	0.36	0.27	0.00	0.00	
				12:00	13:00	13.65	0.36	6.01	4.19	0.18	1.27	0.46	0.09	0.09	0.00	0.46	0.09	0.18	0.18	0.09	0.00	0.00	
				13:00	14:00	13.65	0.18	4.19	2.82	0.36	1.64	1.55	0.36	0.18	0.00	0.09	0.46	0.73	0.64	0.46	0.00	0.00	
				14:00	15:00	13.65	0.27	3.91	3.82	0.36	1.55	1.27	0.27	0.27	0.00	0.27	0.27	0.55	0.46	0.36	0.00	0.00	
				15:00	16:00	13.65	0.27	4.55	4.10	0.27	1.46	1.27	0.27	0.18	0.00	0.27	0.18	0.36	0.27	0.18	0.00	0.00	
				16:00	17:00	13.65	0.27	5.64	3.73	0.18	1.55	0.55	0.09	0.00	0.00	0.09	0.27	0.55	0.46	0.27	0.00	0.00	
				17:00	18:00	18.20	0.46	8.83	4.82	0.18	1.27	1.18	0.18	0.09	0.00	0.18	0.18	0.36	0.27	0.18	0.00	0.00	
				18:00	19:00	18.20	0.46	9.19	5.10	0.09	1.00	0.82	0.09	0.09	0.00	0.09	0.18	0.46	0.46	0.18	0.00	0.00	
				19:00	20:00	13.65	0.46	7.46	4.73	0.00	0.18	0.18	0.00	0.00	0.00	0.09	0.09	0.18	0.18	0.09	0.00	0.00	
				20:00	21:00	9.10	0.27	4.37	4.10	0.00	0.18	0.09	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00	
				21:00	22:00	9.10	0.18	3.82	4.82	0.00	0.18	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	9.10	0.36	3.28	5.10	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.09	0.00	0.00	
				23:00	0:00	9.10	0.27	3.00	5.10	0.00	0.18	0.00	0.00	0.00	0.00	0.18	0.00	0.18	0.00	0.18	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L171	Pak Kung Street (WB)	Gillies Avenue North	Chatham Road North	0:00	1:00	4.55	0.18	1.55	1.73	0.00	0.00	0.00	0.00	0.00	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	4.55	0.00	2.00	2.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	4.55	0.00	1.73	2.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.55	0.00	2.28	2.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.55	0.00	1.82	2.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.55	0.00	1.55	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	4.55	0.00	1.18	1.82	0.00	0.00	0.00	0.00	0.00	1.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	4.55	0.09	1.18	0.46	0.09	0.09	0.00	0.00	0.00	2.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.55	0.18	2.00	0.36	0.09	0.18	0.09	0.00	0.00	1.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.55	0.09	1.91	0.46	0.09	0.18	0.09	0.00	0.00	1.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	4.55	0.18	1.73	1.09	0.09	0.36	0.09	0.00	0.00	1.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	9.10	0.18	2.91	1.73	0.36	0.91	0.00	0.00	0.00	3.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	9.10	0.18	3.82	2.09	0.18	0.55	0.00	0.00	0.00	2.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	4.55	0.09	1.64	0.82	0.09	0.46	0.09	0.00	0.00	1.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	4.55	0.09	1.46	1.18	0.09	0.27	0.09	0.00	0.00	1.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	9.10	0.36	3.28	2.28	0.18	0.73	0.18	0.00	0.00	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	9.10	0.27	3.91	2.55	0.18	0.64	0.18	0.00	0.00	1.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	9.10	0.36	4.64	2.46	0.09	0.36	0.09	0.00	0.00	1.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	9.10	0.36	4.91	2.64	0.18	0.27	0.18	0.00	0.00	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	9.10	0.36	4.37	2.09	0.00	0.18	0.00	0.00	0.00	2.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	4.55	0.09	1.64	1.18	0.00	0.00	0.00	0.00	0.00	1.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	4.55	0.09	1.46	1.37	0.00	0.00	0.00	0.00	0.00	1.64	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	4.55	0.09	1.18	1.46	0.00	0.00	0.00	0.00	0.00	1.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	4.55	0.18	1.09	1.55	0.00	0.00	0.00	0.00	0.00	1.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L172	Pak Kung Street (EB)	Gillies Avenue North	Ma Tau Wai Road	0:00	1:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.95	0.00	1.95	1.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.95	0.00	1.12	1.12	0.24	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	
				8:00	9:00	2.95	0.12	1.53	0.65	0.12	0.30	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.00	
				9:00	10:00	2.95	0.18	1.18	0.77	0.18	0.30	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	
				10:00	11:00	2.95	0.18	1.36	0.53	0.18	0.53	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.95	0.00	1.30	0.53	0.35	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.95	0.18	1.71	0.53	0.18	0.35	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.95	0.00	1.18	0.41	0.24	0.65	0.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.24	0.00	
				14:00	15:00	2.95	0.00	1.24	0.59	0.18	0.59	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	
				15:00	16:00	2.95	0.18	1.12	0.65	0.18	0.47	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.18	0.00	
				16:00	17:00	2.95	0.18	1.53	0.35	0.18	0.53	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	2.95	0.12	2.01	0.30	0.12	0.30	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	2.95	0.18	1.95	0.30	0.18	0.18	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.95	0.18	2.01	0.77	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.95	0.24	1.71	1.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.95	0.00	1.77	1.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	2.95	0.00	1.48	1.48	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	2.95	0.00	1.71	1.24	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L173	Pak Kung Street (WB)	Ma Tau Wai Road	Gillies Avenue North	0:00	1:00	11.80	0.59	3.01	6.84	0.00	0.12	0.00	0.00	0.00	1.12	0.00	0.00	0.06	0.06	0.00	0.00	0.00	
				1:00	2:00	5.90	0.30	1.59	3.89	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	5.90	0.30	1.48	4.07	0.00	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	5.90	0.35	1.59	3.78	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	5.90	0.30	1.48	4.01	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	5.90	0.30	1.36	3.84	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.12	0.12	0.00	0.00	0.00
				6:00	7:00	8.85	0.30	1.77	5.13	0.00	0.18	0.06	0.00	0.00	1.00	0.06	0.06	0.12	0.12	0.06	0.00	0.00	0.00
				7:00	8:00	17.70	0.47	5.25	6.79	0.06	0.71	0.18	0.06	0.06	3.30	0.18	0.18	0.24	0.24	0.00	0.00	0.00	0.00
				8:00	9:00	26.55	1.36	11.15	7.32	0.12	1.77	0.89	0.30	0.24	2.60	0.18	0.18	0.24	0.24	0.00	0.00	0.00	0.00
				9:00	10:00	23.60	1.00	8.56	7.43	0.12	1.65	0.94	0.30	0.24	2.24	0.30	0.24	0.30	0.30	0.00	0.00	0.00	0.00
				10:00	11:00	23.60	1.18	7.32	8.61	0.18	2.24	0.83	0.35	0.35	1.89	0.06	0.12	0.24	0.18	0.06	0.00	0.00	0.00
				11:00	12:00	23.60	0.83	7.20	7.49	0.24	2.83	0.30	0.12	0.12	3.36	0.24	0.12	0.35	0.30	0.12	0.00	0.00	0.00
				12:00	13:00	26.55	1.06	9.97	9.50	0.18	2.01	0.30	0.12	0.12	2.66	0.30	0.06	0.12	0.12	0.06	0.00	0.00	0.00
				13:00	14:00	23.60	0.83	6.96	6.49	0.24	2.48	1.12	0.47	0.47	2.60	0.06	0.35	0.71	0.59	0.24	0.00	0.00	0.00
				14:00	15:00	23.60	0.65	6.20	8.73	0.18	2.24	0.83	0.35	0.35	2.71	0.12	0.18	0.47	0.41	0.18	0.00	0.00	0.00
				15:00	16:00	26.55	1.06	7.79	9.85	0.24	2.36	1.00	0.41	0.35	2.30	0.24	0.18	0.35	0.30	0.12	0.00	0.00	0.00
				16:00	17:00	26.55	1.36	9.62	9.15	0.18	1.59	0.30	0.12	0.12	2.48	0.06	0.18	0.71	0.41	0.30	0.00	0.00	0.00
				17:00	18:00	35.40	2.07	14.87	11.92	0.18	1.36	0.59	0.30	0.30	2.66	0.12	0.12	0.47	0.24	0.24	0.00	0.00	0.00
				18:00	19:00	29.50	1.83	12.98	10.74	0.12	0.94	0.30	0.18	0.18	1.06	0.06	0.12	0.47	0.30	0.24	0.00	0.00	0.00
				19:00	20:00	26.55	1.12	11.68	10.09	0.00	0.30	0.12	0.06	0.06	2.66	0.06	0.06	0.18	0.12	0.06	0.00	0.00	0.00
				20:00	21:00	23.60	1.00	7.91	10.44	0.00	0.24	0.06	0.06	0.06	3.66	0.06	0.00	0.06	0.06	0.00	0.00	0.00	0.00
				21:00	22:00	20.65	0.65	6.20	10.21	0.00	0.35	0.06	0.06	0.06	3.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	17.70	0.77	4.25	9.32	0.00	0.18	0.00	0.00	0.00	2.89	0.06	0.06	0.06	0.06	0.06	0.00	0.00	0.00
L174	Gillies Avenue North (SB)	Pak Kung Street	Hok Yeun Street	23:00	0:00	14.75	0.71	3.42	7.73	0.00	0.18	0.00	0.00	0.00	2.42	0.06	0.06	0.06	0.06	0.06	0.00	0.00	
				0:00	1:00	9.60	0.48	3.07	5.28	0.00	0.19	0.00	0.00	0.00	0.14	0.05	0.05	0.14	0.10	0.05	0.00	0.05	
				1:00	2:00	7.20	0.38	2.40	4.22	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	7.20	0.34	2.21	4.51	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.80	0.29	1.54	2.78	0.00	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.80	0.29	1.44	2.93	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	4.80	0.24	1.30	2.74	0.00	0.19	0.00	0.00	0.00	0.00	0.05	0.05	0.10	0.10	0.05	0.00	0.00	0.00
				6:00	7:00	9.60	0.34	2.35	5.18	0.05	0.43	0.10	0.00	0.00	0.19	0.10	0.10	0.34	0.29	0.10	0.00	0.05	0.05
				7:00	8:00	19.20	0.53	6.86	6.72	0.14	1.06	0.29	0.14	0.10	0.29	0.38	0.38	0.86	0.86	0.29	0.00	0.29	0.29
				8:00	9:00	31.20	1.49	13.92	6.91	0.29	2.45	1.39	0.62	0.58	0.24	0.43	0.43	0.96	0.96	0.29	0.00	0.24	0.24
				9:00	10:00	28.80	1.10	10.75	7.15	0.24	2.35	1.49	0.62	0.62	0.19	0.67	0.53	1.20	1.25	0.38	0.00	0.24	0.24
				10:00	11:00	26.40	1.20	8.69	7.49	0.29	3.50	1.92	0.67	0.67	0.24	0.19	0.24	0.48	0.48	0.19	0.00	0.14	0.14
				11:00	12:00	24.00	0.77	8.02	6.24	0.38	4.27	0.72	0.24	0.19	0.43	0.58	0.29	0.72	0.77	0.24	0.00	0.14	0.14
				12:00	13:00	26.40	0.96	10.94	8.02	0.29	2.93	0.67	0.29	0.19	0.38	0.62	0.14	0.34	0.38	0.10	0.00	0.14	0.14
				13:00	14:00	26.40	0.72	7.63	5.38	0.38	3.65	2.30	0.82	0.82	0.34	0.14	0.67	1.39	1.49	0.48	0.00	0.19	0.19
				14:00	15:00	24.00	0.62	6.72	6.91	0.29	3.26	1.68	0.62	0.58	0.34	0.24	0.43	0.91	0.96	0.29	0.00	0.14	0.14
				15:00	16:00	28.80	0.96	8.98	8.30	0.34	3.55	2.11	0.77	0.77	0.29	0.48	0.34	0.77	0.77	0.24	0.00	0.14	0.14
				16:00	17:00	26.40	1.15	10.66	7.68	0.14	2.83	0.86	0.19	0.19	0.48	0.05	0.34	0.72	0.72	0.29	0.00	0.10	0.10
				17:00	18:00	33.60	1.73	15.46	9.41	0.19	2.21	1.63	0.48	0.48	0.48	0.19	0.19	0.48	0.38	0.19	0.00	0.10	0.10
				18:00	19:00	28.80	1.49	13.92	8.59	0.10	1.49	0.91	0.24	0.29	0.19	0.14	0.24	0.48	0.48	0.19	0.00	0.05	0.05
				19:00	20:00	26.40	1.10	13.68	8.88	0.05	0.53	0.29	0.05	0.10	0.38	0.19	0.14	0.38	0.38	0.10	0.00	0.14	0.14
				20:00	21:00	19.20	0.86	8.59	8.26	0.05	0.34	0.10	0.05	0.05	0.43	0.05	0.05	0.10	0.10	0.05	0.00	0.14	0.14
				21:00	22:00	16.80	0.58	6.62	8.21	0.05	0.43	0.19	0.05	0.05	0.38	0.00	0.00	0.05	0.00	0.05	0.00	0.14	0.14
22:00	23:00	16.80	0.82	5.52	8.93	0.05	0.29	0.00	0.00	0.00	0.43	0.10	0.05	0.19	0.10	0.10	0.00	0.24	0.24				
23:00	0:00	14.40	0.72	4.56	7.73	0.05	0.29	0.00	0.00	0.00	0.34	0.10	0.05	0.19	0.10	0.10	0.00	0.19	0.19				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L175	Hok Yeun Street (EB)	Gillies Avenue North	Ma Tau Wai Road	0:00	1:00	12.90	0.43	3.96	7.65	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.34	0.09	0.09	0.00	0.00	
				1:00	2:00	8.60	0.26	2.67	5.42	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	8.60	0.26	2.49	5.68	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	4.30	0.17	1.29	2.67	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	4.30	0.09	1.20	2.92	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	8.60	0.17	2.15	4.90	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.17	0.17	0.34	0.17	0.17	0.00	0.00	0.00
				6:00	7:00	8.60	0.17	1.98	4.90	0.00	0.26	0.17	0.00	0.00	0.00	0.00	0.17	0.17	0.34	0.26	0.17	0.00	0.00	0.00
				7:00	8:00	25.80	0.52	8.94	9.55	0.09	1.12	0.52	0.09	0.09	0.00	0.69	0.69	1.46	1.38	0.69	0.00	0.00	0.00	0.00
				8:00	9:00	38.70	1.38	16.86	9.37	0.17	2.32	2.15	0.60	0.60	0.00	0.77	0.77	1.55	1.38	0.77	0.00	0.00	0.00	0.00
				9:00	10:00	34.40	0.95	12.38	9.12	0.17	2.06	2.15	0.60	0.60	0.00	1.12	0.86	1.81	1.72	0.86	0.00	0.00	0.00	0.00
				10:00	11:00	30.10	0.86	9.46	9.37	0.26	3.44	3.01	0.60	0.69	0.00	0.26	0.34	0.77	0.69	0.34	0.00	0.00	0.00	0.00
				11:00	12:00	25.80	0.52	8.77	7.40	0.26	3.96	1.03	0.26	0.17	0.00	0.77	0.43	0.95	0.86	0.43	0.00	0.00	0.00	0.00
				12:00	13:00	30.10	0.77	12.30	10.06	0.17	2.92	1.12	0.34	0.17	0.00	0.86	0.17	0.52	0.52	0.17	0.00	0.00	0.00	0.00
				13:00	14:00	30.10	0.60	8.00	6.62	0.26	3.53	3.61	0.77	0.77	0.00	0.26	0.95	1.98	1.81	0.95	0.00	0.00	0.00	0.00
				14:00	15:00	25.80	0.43	6.79	8.17	0.26	2.92	2.49	0.52	0.52	0.00	0.34	0.52	1.20	1.03	0.60	0.00	0.00	0.00	0.00
				15:00	16:00	34.40	0.77	10.15	10.66	0.26	3.61	3.44	0.77	0.77	0.00	0.77	0.52	1.12	0.95	0.60	0.00	0.00	0.00	0.00
				16:00	17:00	25.80	0.69	9.98	8.60	0.17	2.75	1.29	0.17	0.17	0.00	0.09	0.26	0.77	0.43	0.43	0.00	0.00	0.00	0.00
				17:00	18:00	34.40	1.03	15.31	11.01	0.17	2.24	2.58	0.34	0.34	0.00	0.17	0.17	0.52	0.26	0.26	0.00	0.00	0.00	0.00
				18:00	19:00	30.10	0.95	13.93	10.32	0.09	1.55	1.38	0.17	0.17	0.00	0.09	0.17	0.60	0.43	0.26	0.00	0.00	0.00	0.00
				19:00	20:00	30.10	0.95	14.96	11.27	0.00	0.52	0.52	0.09	0.09	0.00	0.34	0.17	0.52	0.52	0.17	0.00	0.00	0.00	0.00
				20:00	21:00	21.50	0.69	9.37	10.23	0.00	0.34	0.17	0.09	0.09	0.00	0.09	0.09	0.17	0.09	0.09	0.00	0.00	0.00	0.00
				21:00	22:00	21.50	0.52	8.17	11.61	0.00	0.52	0.34	0.09	0.09	0.00	0.00	0.00	0.09	0.00	0.09	0.00	0.00	0.00	0.00
				22:00	23:00	17.20	0.52	5.50	10.23	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.26	0.09	0.17	0.00	0.00	0.00
				L176	Hok Yeun Street (WB)	Gillies Avenue North	Hok Yuen Lane	23:00	0:00	17.20	0.69	5.25	10.06	0.00	0.34	0.00	0.00	0.00	0.00	0.09	0.09	0.34	0.09	0.26
0:00	1:00	4.70	0.42					1.65	2.21	0.00	0.14	0.00	0.00	0.00	0.19	0.00	0.00	0.05	0.00	0.00	0.00	0.05		
1:00	2:00	2.35	0.24					0.85	1.22	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	2.35	0.19					0.80	1.32	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	2.35	0.24					0.85	1.18	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	2.35	0.19					0.80	1.27	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	2.35	0.14					0.75	1.27	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	0.00	0.00		
6:00	7:00	4.70	0.33					1.27	2.16	0.00	0.24	0.00	0.00	0.00	0.24	0.09	0.09	0.09	0.09	0.00	0.00	0.09	0.09	
7:00	8:00	7.05	0.33					2.91	1.97	0.09	0.56	0.05	0.05	0.05	0.33	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.33	
8:00	9:00	11.75	0.99					5.41	2.02	0.24	1.27	0.33	0.28	0.28	0.28	0.09	0.09	0.09	0.09	0.00	0.00	0.00	0.28	
9:00	10:00	9.40	0.66					3.95	1.83	0.19	1.08	0.28	0.24	0.24	0.19	0.14	0.09	0.14	0.14	0.00	0.00	0.00	0.24	
10:00	11:00	11.75	0.80					4.04	2.87	0.24	1.74	0.47	0.33	0.33	0.28	0.05	0.05	0.19	0.14	0.05	0.00	0.00	0.19	
11:00	12:00	9.40	0.42					3.34	2.07	0.24	1.83	0.14	0.09	0.09	0.42	0.14	0.09	0.19	0.14	0.05	0.00	0.00	0.14	
12:00	13:00	11.75	0.61					4.98	2.96	0.24	1.41	0.19	0.14	0.14	0.42	0.24	0.05	0.14	0.05	0.00	0.00	0.00	0.19	
13:00	14:00	9.40	0.42					3.06	1.69	0.19	1.50	0.47	0.33	0.38	0.28	0.05	0.19	0.33	0.24	0.09	0.00	0.00	0.19	
14:00	15:00	9.40	0.38					2.91	2.44	0.19	1.50	0.38	0.24	0.24	0.38	0.09	0.09	0.24	0.14	0.05	0.00	0.00	0.14	
15:00	16:00	11.75	0.61					3.90	2.96	0.24	1.69	0.47	0.38	0.38	0.33	0.14	0.09	0.24	0.14	0.05	0.00	0.00	0.14	
16:00	17:00	11.75	0.71					4.84	2.96	0.09	1.18	0.19	0.09	0.09	0.47	0.05	0.14	0.42	0.28	0.14	0.00	0.00	0.09	
17:00	18:00	14.10	0.99					6.72	3.48	0.09	0.85	0.33	0.24	0.24	0.42	0.09	0.09	0.24	0.14	0.09	0.00	0.00	0.09	
18:00	19:00	14.10	1.03					7.00	3.71	0.05	0.71	0.24	0.14	0.14	0.24	0.05	0.09	0.33	0.24	0.09	0.00	0.00	0.05	
19:00	20:00	9.40	0.61					5.03	2.68	0.00	0.24	0.05	0.05	0.05	0.33	0.05	0.05	0.09	0.05	0.00	0.00	0.00	0.14	
20:00	21:00	9.40	0.66					4.32	3.38	0.00	0.19	0.00	0.05	0.05	0.52	0.00	0.00	0.05	0.00	0.00	0.00	0.00	0.19	
21:00	22:00	7.05	0.38					2.87	2.91	0.00	0.19	0.05	0.05	0.05	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.14	
22:00	23:00	7.05	0.52					2.44	3.20	0.00	0.09	0.00	0.00	0.00	0.47	0.05	0.00	0.05	0.00	0.00	0.00	0.00	0.24	
23:00	0:00	4.70	0.38	1.55	2.16	0.00	0.09	0.00	0.00	0.00	0.28	0.05	0.00	0.05	0.00	0.00	0.00	0.00	0.14					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L177	Hok Yuen Lane (SB)	Hok Yuen Street	Cul-de-sac	0:00	1:00	2.15	1.29	0.86	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	2.15	1.42	0.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.15	1.42	0.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.15	0.73	1.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.15	0.82	1.20	0.00	0.00	0.00	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.15	0.73	1.25	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.15	1.08	0.90	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.15	0.86	1.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.15	0.82	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.15	0.86	1.08	0.00	0.00	0.00	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.15	0.73	1.16	0.00	0.00	0.00	0.00	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.15	0.99	0.99	0.00	0.00	0.00	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.15	0.86	1.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.15	0.99	1.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.15	0.82	1.33	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.15	1.08	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L178	Hok Yuen Lane (NB)	Cul-de-sac	Hok Yuen Street	23:00	0:00	2.15	1.25	0.90	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				0:00	1:00	2.15	1.72	0.22	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.15	1.55	0.30	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.15	1.46	0.34	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.15	1.46	0.34	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.15	1.59	0.00	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.15	1.59	0.00	0.56	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.15	1.55	0.30	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.15	0.43	0.00	1.72	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	2.15	1.08	0.00	1.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.15	0.90	0.00	1.25	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.15	1.42	0.30	0.13	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.15	1.12	0.34	0.17	0.00	0.52	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.15	1.29	0.39	0.17	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.15	1.25	0.30	0.09	0.00	0.52	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.15	1.12	0.34	0.22	0.00	0.47	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.15	1.29	0.30	0.17	0.00	0.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	4.30	2.84	0.77	0.00	0.00	0.69	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	4.30	3.10	0.82	0.00	0.00	0.39	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	4.30	3.18	0.82	0.00	0.00	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.15	1.42	0.47	0.17	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.15	1.51	0.43	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.15	1.38	0.39	0.26	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	2.15	1.63	0.26	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	2.15	1.55	0.30	0.30	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L179	Hok Yuen Street (WB)	Hok Yuen Lane	Chatham Road North	0:00	1:00	4.30	0.39	1.46	2.11	0.00	0.09	0.00	0.00	0.00	0.17	0.00	0.00	0.04	0.00	0.00	0.00	0.04
				1:00	2:00	2.15	0.17	0.73	1.20	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.15	0.17	0.73	1.20	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.15	0.22	0.77	1.08	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.15	0.17	0.73	1.16	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.15	0.13	0.69	1.12	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	0.00
				6:00	7:00	4.30	0.26	1.16	1.94	0.00	0.22	0.00	0.00	0.00	0.22	0.09	0.09	0.09	0.09	0.09	0.00	0.09
				7:00	8:00	6.45	0.34	2.54	1.94	0.13	0.52	0.04	0.04	0.04	0.34	0.04	0.04	0.04	0.04	0.00	0.00	0.34
				8:00	9:00	10.75	0.82	4.99	1.98	0.22	1.29	0.30	0.26	0.22	0.26	0.04	0.04	0.04	0.04	0.00	0.00	0.26
				9:00	10:00	8.60	0.52	3.53	1.85	0.17	1.16	0.30	0.26	0.17	0.17	0.09	0.04	0.04	0.04	0.00	0.00	0.26
				10:00	11:00	10.75	0.82	3.70	2.71	0.22	1.59	0.39	0.30	0.30	0.26	0.04	0.04	0.09	0.09	0.04	0.00	0.17
				11:00	12:00	10.75	0.52	3.66	2.45	0.26	2.11	0.17	0.13	0.13	0.47	0.17	0.13	0.17	0.17	0.04	0.00	0.17
				12:00	13:00	10.75	0.65	4.52	2.84	0.22	1.29	0.17	0.09	0.09	0.34	0.22	0.04	0.04	0.04	0.04	0.00	0.17
				13:00	14:00	8.60	0.43	2.71	1.63	0.17	1.38	0.43	0.30	0.30	0.26	0.04	0.17	0.26	0.22	0.13	0.00	0.17
				14:00	15:00	8.60	0.34	2.71	2.32	0.17	1.33	0.30	0.22	0.22	0.30	0.09	0.09	0.17	0.13	0.09	0.00	0.13
				15:00	16:00	10.75	0.65	3.53	2.84	0.17	1.51	0.43	0.34	0.30	0.30	0.13	0.09	0.13	0.13	0.09	0.00	0.13
				16:00	17:00	12.90	0.90	5.29	3.27	0.09	1.16	0.22	0.09	0.09	0.47	0.04	0.26	0.39	0.34	0.22	0.00	0.09
				17:00	18:00	15.05	1.25	7.22	3.70	0.09	0.82	0.30	0.22	0.22	0.39	0.13	0.13	0.22	0.17	0.13	0.00	0.09
				18:00	19:00	12.90	1.08	6.36	3.35	0.04	0.56	0.17	0.13	0.13	0.17	0.09	0.17	0.26	0.22	0.13	0.00	0.04
				19:00	20:00	10.75	0.73	5.72	3.18	0.00	0.26	0.04	0.04	0.04	0.34	0.04	0.04	0.04	0.04	0.04	0.00	0.17
				20:00	21:00	8.60	0.65	3.83	3.18	0.00	0.17	0.00	0.04	0.04	0.47	0.00	0.00	0.04	0.00	0.00	0.00	0.17
				21:00	22:00	6.45	0.34	2.58	2.75	0.00	0.17	0.04	0.04	0.04	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.13
				22:00	23:00	6.45	0.47	2.15	3.05	0.00	0.09	0.00	0.00	0.00	0.39	0.04	0.00	0.04	0.00	0.00	0.00	0.22
				23:00	0:00	6.45	0.56	2.11	2.92	0.00	0.13	0.00	0.00	0.00	0.39	0.09	0.00	0.09	0.00	0.00	0.00	0.17
L180	Chatham Road North (SB)	Hok Yuen Street	Wo Chung Street	0:00	1:00	7.00	0.50	2.02	3.14	0.00	0.14	0.00	0.00	0.00	0.54	0.02	0.00	0.02	0.02	0.00	0.60	
				1:00	2:00	4.00	0.30	1.32	2.14	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	
				2:00	3:00	4.00	0.28	1.20	2.32	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	
				3:00	4:00	3.00	0.28	0.96	1.54	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.10	
				4:00	5:00	3.00	0.24	0.90	1.66	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.12	
				5:00	6:00	3.00	0.22	0.88	1.68	0.00	0.16	0.00	0.00	0.02	0.00	0.00	0.00	0.02	0.02	0.00	0.00	
				6:00	7:00	6.00	0.30	1.34	2.62	0.02	0.28	0.04	0.00	0.02	0.50	0.02	0.02	0.04	0.04	0.02	0.00	0.74
				7:00	8:00	16.00	0.52	3.88	4.66	0.12	0.94	0.20	0.06	0.06	1.48	0.08	0.06	0.12	0.12	0.04	0.00	3.66
				8:00	9:00	22.00	1.44	7.48	4.58	0.24	2.12	1.00	0.28	0.28	1.10	0.08	0.08	0.12	0.12	0.04	0.00	3.04
				9:00	10:00	20.00	1.00	5.68	4.62	0.22	1.98	1.06	0.28	0.30	0.96	0.12	0.10	0.16	0.16	0.06	0.00	3.30
				10:00	11:00	18.00	1.08	5.20	4.14	0.26	2.50	0.82	0.32	0.32	0.90	0.04	0.04	0.10	0.10	0.02	0.00	2.16
				11:00	12:00	18.00	0.74	5.14	3.56	0.34	3.18	0.34	0.12	0.10	1.62	0.10	0.06	0.12	0.12	0.04	0.00	2.42
				12:00	13:00	18.00	0.90	6.60	4.36	0.24	2.08	0.28	0.12	0.10	1.18	0.12	0.02	0.04	0.06	0.02	0.00	1.88
				13:00	14:00	17.00	0.64	4.38	2.88	0.28	2.54	1.00	0.40	0.40	1.14	0.04	0.12	0.22	0.22	0.08	0.00	2.66
				14:00	15:00	17.00	0.56	4.22	4.02	0.26	2.42	0.78	0.28	0.28	1.24	0.06	0.08	0.16	0.16	0.06	0.00	2.42
				15:00	16:00	20.00	0.94	5.62	4.76	0.28	2.66	0.98	0.40	0.38	1.10	0.10	0.06	0.12	0.12	0.04	0.00	2.44
				16:00	17:00	17.00	1.04	6.80	3.50	0.16	1.56	0.18	0.12	0.10	1.28	0.02	0.06	0.18	0.16	0.06	0.00	1.78
				17:00	18:00	22.00	1.56	10.30	4.44	0.14	1.24	0.34	0.26	0.24	1.34	0.04	0.04	0.12	0.08	0.04	0.00	1.82
				18:00	19:00	19.00	1.42	9.62	4.26	0.06	0.88	0.20	0.16	0.16	0.56	0.02	0.04	0.14	0.12	0.04	0.00	1.32
				19:00	20:00	18.00	1.00	8.10	4.80	0.02	0.38	0.12	0.04	0.04	1.26	0.04	0.02	0.08	0.08	0.02	0.00	2.00
				20:00	21:00	15.00	0.84	5.36	4.76	0.02	0.28	0.04	0.04	0.04	1.66	0.02	0.00	0.02	0.02	0.00	0.00	1.90
				21:00	22:00	13.00	0.54	4.06	4.60	0.02	0.32	0.08	0.04	0.04	1.36	0.00	0.00	0.00	0.00	0.00	0.00	1.94
				22:00	23:00	13.00	0.72	3.14	4.70	0.02	0.18	0.00	0.00	0.00	1.44	0.02	0.02	0.02	0.02	0.02	0.00	2.70
				23:00	0:00	11.00	0.64	2.56	3.98	0.02	0.20	0.00	0.00	0.00	1.22	0.02	0.02	0.02	0.02	0.02	0.00	2.28

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L181	Chatham Road North (SB)	Ha Heung Road	Wo Chung Street	0:00	1:00	765.00	62.22	198.90	462.06	0.00	13.26	0.00	0.00	0.00	0.00	2.04	2.04	8.16	3.06	2.04	0.00	11.22
				1:00	2:00	459.00	35.70	120.36	293.76	0.00	9.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	408.00	27.54	97.92	276.42	0.00	6.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	306.00	27.54	79.56	189.72	0.00	9.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	306.00	23.46	72.42	201.96	0.00	6.12	0.00	0.00	0.00	0.00	1.02	0.00	1.02	0.00	0.00	0.00	0.00
				5:00	6:00	306.00	20.40	67.32	191.76	0.00	10.20	0.00	0.00	2.04	0.00	2.04	2.04	5.10	3.06	2.04	0.00	0.00
				6:00	7:00	612.00	35.70	127.50	367.20	1.02	20.40	5.10	0.00	2.04	1.02	5.10	5.10	12.24	11.22	4.08	0.00	14.28
				7:00	8:00	1,530.00	76.50	388.62	782.34	2.04	55.08	20.40	12.24	11.22	5.10	17.34	15.30	33.66	30.60	12.24	0.00	67.32
				8:00	9:00	2,346.00	211.14	788.46	812.94	5.10	129.54	95.88	61.20	59.16	2.04	18.36	18.36	37.74	32.64	14.28	0.00	59.16
				9:00	10:00	2,142.00	149.94	595.68	820.08	4.08	122.40	100.98	60.18	62.22	2.04	27.54	23.46	46.92	44.88	17.34	0.00	64.26
				10:00	11:00	1,836.00	135.66	528.36	612.00	5.10	209.10	110.16	59.16	60.18	2.04	9.18	11.22	22.44	20.40	7.14	0.00	43.86
				11:00	12:00	1,632.00	87.72	507.96	525.30	8.16	259.08	40.80	24.48	19.38	2.04	24.48	14.28	29.58	29.58	10.20	0.00	48.96
				12:00	13:00	1,734.00	107.10	641.58	631.38	5.10	166.26	36.72	24.48	19.38	2.04	26.52	7.14	12.24	14.28	3.06	0.00	36.72
				13:00	14:00	1,683.00	82.62	447.78	434.52	7.14	212.16	131.58	70.38	74.46	2.04	7.14	28.56	55.08	55.08	20.40	0.00	54.06
				14:00	15:00	1,632.00	67.32	410.04	583.44	6.12	193.80	99.96	52.02	52.02	2.04	13.26	18.36	37.74	35.70	13.26	0.00	46.92
				15:00	16:00	1,989.00	113.22	544.68	688.50	8.16	213.18	123.42	69.36	67.32	2.04	21.42	16.32	31.62	29.58	12.24	0.00	47.94
				16:00	17:00	1,581.00	104.04	660.96	419.22	6.12	184.62	39.78	16.32	16.32	0.00	3.06	16.32	31.62	33.66	9.18	0.00	39.78
				17:00	18:00	2,091.00	157.08	992.46	527.34	4.08	146.88	79.56	39.78	37.74	0.00	10.20	10.20	21.42	18.36	6.12	0.00	39.78
				18:00	19:00	1,887.00	143.82	934.32	505.92	4.08	104.04	46.92	22.44	22.44	0.00	7.14	13.26	22.44	24.48	7.14	0.00	28.56
				19:00	20:00	1,734.00	121.38	773.16	686.46	2.04	31.62	15.30	7.14	9.18	2.04	9.18	7.14	14.28	14.28	3.06	0.00	37.74
				20:00	21:00	1,428.00	103.02	531.42	700.74	1.02	21.42	6.12	6.12	6.12	1.02	3.06	1.02	6.12	3.06	1.02	0.00	36.72
				21:00	22:00	1,275.00	67.32	416.16	693.60	1.02	26.52	10.20	6.12	6.12	3.06	0.00	1.02	3.06	0.00	1.02	0.00	39.78
				22:00	23:00	1,173.00	87.72	311.10	681.36	1.02	14.28	0.00	0.00	0.00	2.04	5.10	3.06	7.14	3.06	3.06	0.00	54.06
L182	Chatham Road North (NB)	Wo Chung Street	Ha Heung Road	23:00	0:00	969.00	76.50	246.84	562.02	1.02	15.30	0.00	0.00	0.00	2.04	4.08	3.06	8.16	3.06	3.06	0.00	43.86
				0:00	1:00	1,071.00	70.38	298.86	640.56	0.00	23.46	0.00	0.00	0.00	0.00	4.08	3.06	11.22	5.10	3.06	0.00	11.22
				1:00	2:00	663.00	42.84	185.64	416.16	0.00	18.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	612.00	33.66	157.08	408.00	0.00	13.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	459.00	33.66	128.52	279.48	0.00	17.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	408.00	25.50	104.04	265.20	0.00	11.22	0.00	0.00	0.00	0.00	1.02	0.00	1.02	0.00	0.00	0.00	0.00
				5:00	6:00	459.00	24.48	108.12	282.54	0.00	19.38	0.00	0.00	3.06	0.00	3.06	3.06	7.14	5.10	3.06	0.00	0.00
				6:00	7:00	867.00	41.82	193.80	510.00	1.02	37.74	8.16	0.00	3.06	0.00	8.16	9.18	18.36	17.34	8.16	0.00	10.20
				7:00	8:00	2,091.00	56.10	637.50	1,089.36	2.04	73.44	21.42	14.28	14.28	2.04	18.36	18.36	41.82	39.78	14.28	0.00	47.94
				8:00	9:00	3,213.00	157.08	1,297.44	1,125.06	6.12	173.40	103.02	77.52	77.52	3.06	22.44	22.44	44.88	44.88	16.32	0.00	41.82
				9:00	10:00	2,907.00	110.16	979.20	1,131.18	3.06	165.24	107.10	78.54	81.60	3.06	34.68	29.58	58.14	58.14	20.40	0.00	46.92
				10:00	11:00	2,754.00	162.18	826.20	885.36	11.22	390.66	157.08	87.72	90.78	0.00	14.28	16.32	32.64	32.64	11.22	0.00	35.70
				11:00	12:00	2,448.00	103.02	785.40	744.60	12.24	477.36	59.16	34.68	29.58	0.00	36.72	19.38	43.86	48.96	14.28	0.00	38.76
				12:00	13:00	2,601.00	127.50	1,010.82	904.74	10.20	309.06	52.02	33.66	28.56	0.00	38.76	10.20	18.36	20.40	8.16	0.00	28.56
				13:00	14:00	2,499.00	97.92	684.42	612.00	12.24	387.60	184.62	105.06	110.16	0.00	12.24	42.84	84.66	92.82	29.58	0.00	42.84
				14:00	15:00	2,448.00	80.58	638.52	837.42	10.20	360.06	139.74	78.54	80.58	0.00	19.38	26.52	59.16	59.16	19.38	0.00	38.76
				15:00	16:00	3,009.00	138.72	857.82	999.60	12.24	402.90	177.48	105.06	105.06	0.00	32.64	24.48	47.94	47.94	18.36	0.00	38.76
				16:00	17:00	2,601.00	187.68	977.16	632.40	10.20	426.36	72.42	31.62	31.62	0.00	5.10	31.62	65.28	72.42	20.40	0.00	36.72
				17:00	18:00	3,417.00	286.62	1,495.32	809.88	10.20	344.76	149.94	71.40	71.40	0.00	20.40	20.40	40.80	40.80	17.34	0.00	37.74
				18:00	19:00	3,009.00	256.02	1,389.24	767.04	6.12	240.72	84.66	38.76	41.82	0.00	15.30	24.48	47.94	54.06	15.30	0.00	27.54
				19:00	20:00	2,550.00	142.80	1,218.90	987.36	0.00	56.10	23.46	8.16	13.26	0.00	10.20	10.20	20.40	20.40	8.16	0.00	30.60
				20:00	21:00	2,040.00	120.36	813.96	977.16	2.04	40.80	8.16	8.16	10.20	2.04	4.08	4.08	8.16	6.12	4.08	0.00	30.60
				21:00	22:00	1,836.00	80.58	642.60	986.34	2.04	49.98	14.28	7.14	11.22	2.04	0.00	2.04	4.08	0.00	2.04	0.00	31.62
22:00	23:00	1,683.00	104.04	486.54	985.32	2.04	26.52	0.00	0.00	0.00	2.04	7.14	5.10	10.20	5.10	5.10	0.00	43.86				
23:00	0:00	1,377.00	90.78	384.54	803.76	1.02	27.54	0.00	0.00	0.00	1.02	7.14	5.10	11.22	5.10	5.10	0.00	34.68				

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L183	Chatham Road North (SB)	Pak Kung Street	Hok Yeun Street	0:00	1:00	17.00	1.16	4.62	7.55	0.00	0.41	0.00	0.00	0.00	1.22	0.00	0.00	0.00	0.00	0.00	0.00	2.04
				1:00	2:00	10.20	0.75	3.20	5.58	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.34
				2:00	3:00	10.20	0.61	2.92	6.05	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.34
				3:00	4:00	6.80	0.54	2.11	3.47	0.00	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.34
				4:00	5:00	6.80	0.54	1.97	3.74	0.00	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.34
				5:00	6:00	6.80	0.48	1.90	4.01	0.00	0.41	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	13.60	0.61	2.86	5.78	0.07	0.61	0.14	0.00	0.00	1.16	0.00	0.00	0.07	0.07	0.00	0.00	2.24
				7:00	8:00	44.20	1.36	9.38	13.06	0.27	2.31	0.68	0.14	0.14	3.94	0.14	0.14	0.27	0.27	0.14	0.00	11.97
				8:00	9:00	61.20	3.74	19.04	13.46	0.48	5.51	3.33	0.61	0.54	3.06	0.14	0.14	0.34	0.34	0.14	0.00	10.34
				9:00	10:00	57.80	2.72	14.96	14.01	0.41	5.37	3.60	0.61	0.61	2.72	0.20	0.14	0.34	0.34	0.20	0.00	11.56
				10:00	11:00	47.60	2.65	12.92	10.68	0.48	6.66	2.45	0.75	0.61	2.31	0.07	0.07	0.07	0.14	0.07	0.00	7.68
				11:00	12:00	47.60	1.77	12.51	9.25	0.75	8.50	0.95	0.27	0.27	4.22	0.14	0.07	0.14	0.14	0.07	0.00	8.57
				12:00	13:00	44.20	2.04	15.37	10.54	0.48	5.17	0.75	0.27	0.20	2.86	0.14	0.00	0.07	0.07	0.00	0.00	6.26
				13:00	14:00	44.20	1.56	10.68	7.34	0.54	6.66	2.79	0.95	0.75	2.86	0.07	0.14	0.27	0.27	0.14	0.00	9.18
				14:00	15:00	44.20	1.36	10.13	10.27	0.48	6.32	2.24	0.68	0.54	3.13	0.07	0.07	0.20	0.20	0.07	0.00	8.43
				15:00	16:00	51.00	2.24	13.26	11.90	0.54	6.87	2.79	0.95	0.75	2.79	0.14	0.07	0.14	0.14	0.07	0.00	8.36
				16:00	17:00	40.80	2.31	16.18	7.55	0.34	4.15	0.27	0.27	0.20	3.06	0.00	0.00	0.00	0.00	0.00	0.00	6.46
				17:00	18:00	51.00	3.33	23.60	9.32	0.27	3.20	0.54	0.61	0.54	3.13	0.00	0.00	0.00	0.00	0.00	0.00	6.46
				18:00	19:00	44.20	3.06	22.71	9.04	0.14	2.31	0.27	0.34	0.27	1.29	0.00	0.00	0.00	0.00	0.00	0.00	4.76
				19:00	20:00	44.20	2.31	18.77	11.70	0.07	0.95	0.34	0.07	0.07	3.06	0.07	0.00	0.07	0.07	0.00	0.00	6.66
				20:00	21:00	37.40	1.97	12.38	11.63	0.07	0.68	0.14	0.07	0.07	4.01	0.00	0.00	0.00	0.00	0.00	0.00	6.39
				21:00	22:00	34.00	1.29	9.79	11.63	0.07	0.82	0.20	0.07	0.07	3.40	0.00	0.00	0.00	0.00	0.00	0.00	6.66
				22:00	23:00	34.00	1.70	7.55	11.56	0.07	0.48	0.00	0.00	0.00	3.54	0.00	0.00	0.00	0.00	0.00	0.00	9.11
				23:00	0:00	27.20	1.36	5.78	9.38	0.07	0.48	0.00	0.00	0.00	2.86	0.00	0.00	0.00	0.00	0.00	0.00	7.28
L184	Chatham Road North (NB)	Wo Chung Street	Pak Kung Street	0:00	1:00	30.90	2.37	6.08	15.04	0.00	1.13	0.00	0.00	0.00	2.37	0.21	0.21	0.41	0.21	0.10	0.00	2.78
				1:00	2:00	15.45	1.34	3.50	9.37	0.00	0.82	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.41
				2:00	3:00	15.45	1.13	3.19	9.99	0.00	0.62	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.52
				3:00	4:00	10.30	0.93	2.27	5.97	0.00	0.72	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.41
				4:00	5:00	10.30	0.82	2.16	6.18	0.00	0.52	0.00	0.00	0.00	0.00	0.00	0.00	0.10	0.00	0.00	0.00	0.52
				5:00	6:00	15.45	1.03	2.88	8.65	0.00	1.24	0.00	0.00	0.21	0.00	0.21	0.31	0.41	0.31	0.21	0.00	0.00
				6:00	7:00	25.75	1.24	3.61	11.33	0.10	1.65	0.21	0.00	0.10	2.06	0.31	0.41	0.72	0.72	0.31	0.00	2.99
				7:00	8:00	66.95	2.06	12.15	22.66	0.21	2.58	0.62	0.41	0.41	7.93	0.82	0.82	1.65	1.65	0.62	0.00	12.36
				8:00	9:00	92.70	6.08	25.34	24.00	0.31	6.28	3.30	2.27	2.16	6.18	0.93	0.93	1.75	1.75	0.62	0.00	10.82
				9:00	10:00	87.55	4.43	18.95	24.41	0.31	5.97	3.61	2.16	2.16	5.46	1.44	1.13	2.16	2.47	0.93	0.00	11.95
				10:00	11:00	87.55	4.94	15.86	19.98	0.62	17.00	5.46	2.68	2.58	4.02	0.62	0.72	1.44	1.44	0.52	0.00	9.68
				11:00	12:00	87.55	3.30	15.35	17.20	0.93	21.32	2.06	1.03	0.93	7.21	1.75	0.93	2.06	2.06	0.62	0.00	10.82
				12:00	13:00	82.40	4.12	20.09	21.22	0.62	14.11	1.85	1.03	0.93	5.36	1.96	0.41	0.93	1.03	0.31	0.00	8.45
				13:00	14:00	87.55	2.88	13.08	13.70	0.72	16.89	6.28	3.19	2.99	4.94	0.52	1.85	3.81	3.81	1.24	0.00	11.64
				14:00	15:00	82.40	2.47	12.15	18.75	0.62	15.66	4.74	2.27	2.27	5.25	0.82	1.24	2.47	2.47	0.82	0.00	10.40
				15:00	16:00	97.85	4.33	16.69	22.45	0.82	17.72	6.28	3.19	2.88	4.94	1.55	1.03	2.16	2.16	0.82	0.00	10.82
				16:00	17:00	92.70	5.15	19.67	15.97	0.72	21.12	2.78	1.03	0.82	4.53	0.41	1.65	3.30	3.40	1.03	0.00	11.12
				17:00	18:00	103.00	7.42	28.33	19.26	0.62	15.97	5.25	2.16	1.96	4.53	1.03	1.03	2.06	1.85	0.72	0.00	10.82
				18:00	19:00	87.55	6.80	26.68	18.85	0.31	11.43	3.09	1.34	1.34	1.96	0.82	1.34	2.37	2.47	0.82	0.00	7.93
				19:00	20:00	72.10	4.53	23.69	22.56	0.10	2.58	0.72	0.21	0.41	5.46	0.52	0.41	0.93	0.93	0.31	0.00	8.76
				20:00	21:00	61.80	3.81	15.97	22.76	0.10	1.75	0.21	0.21	0.31	7.31	0.21	0.21	0.31	0.21	0.10	0.00	8.34
				21:00	22:00	56.65	2.58	12.67	22.76	0.10	2.27	0.52	0.21	0.31	6.08	0.00	0.10	0.10	0.00	0.10	0.00	8.86
				22:00	23:00	56.65	3.30	9.58	22.66	0.10	1.24	0.00	0.00	0.00	6.39	0.31	0.21	0.41	0.21	0.21	0.00	12.05
				23:00	0:00	46.35	2.78	7.42	18.44	0.10	1.34	0.00	0.00	0.00	5.15	0.31	0.21	0.41	0.21	0.21	0.00	9.79

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L185	Shun Yung Street (SB)	Pak Kung Street	Fat Kwong Street	0:00	1:00	22.50	1.08	6.57	13.14	0.00	0.45	0.00	0.00	0.00	0.63	0.09	0.09	0.27	0.09	0.00	0.00	0.09	
				1:00	2:00	13.50	0.72	4.05	8.37	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	13.50	0.63	3.69	8.91	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	9.00	0.54	2.70	5.40	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	9.00	0.54	2.43	5.76	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	9.00	0.45	2.25	5.58	0.00	0.36	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.09	0.00	0.00	0.00
				6:00	7:00	18.00	0.63	4.14	10.62	0.00	0.81	0.09	0.00	0.00	0.63	0.18	0.18	0.36	0.18	0.09	0.00	0.09	
				7:00	8:00	36.00	1.08	12.96	14.58	0.09	1.62	0.36	0.18	0.18	1.98	0.45	0.45	0.72	0.72	0.18	0.00	0.45	
				8:00	9:00	58.50	3.24	27.36	15.66	0.18	3.96	1.62	0.81	0.81	1.62	0.54	0.54	0.81	0.81	0.18	0.00	0.36	
				9:00	10:00	54.00	2.43	21.42	16.56	0.18	3.87	1.80	0.90	0.90	1.44	0.90	0.72	1.08	1.17	0.27	0.00	0.36	
				10:00	11:00	45.00	2.25	14.85	15.03	0.27	6.30	1.98	0.72	0.72	1.08	0.27	0.27	0.45	0.45	0.09	0.00	0.27	
				11:00	12:00	45.00	1.44	15.03	13.50	0.27	8.28	0.81	0.27	0.27	2.07	0.81	0.45	0.72	0.72	0.09	0.00	0.27	
				12:00	13:00	45.00	1.71	18.18	15.48	0.18	5.04	0.63	0.27	0.27	1.44	0.72	0.18	0.27	0.27	0.09	0.00	0.27	
				13:00	14:00	40.50	1.26	12.69	10.53	0.27	6.30	2.34	0.81	0.90	1.35	0.18	0.81	1.17	1.26	0.27	0.00	0.36	
				14:00	15:00	40.50	1.08	11.61	14.40	0.27	5.85	1.71	0.63	0.63	1.44	0.36	0.54	0.72	0.81	0.18	0.00	0.27	
				15:00	16:00	49.50	1.80	15.66	17.19	0.27	6.57	2.25	0.81	0.81	1.35	0.63	0.45	0.63	0.63	0.18	0.00	0.27	
				16:00	17:00	45.00	1.98	16.83	14.76	0.18	6.12	0.81	0.18	0.18	1.53	0.09	0.54	0.63	0.72	0.09	0.00	0.36	
				17:00	18:00	54.00	2.79	23.94	17.37	0.18	4.50	1.44	0.36	0.36	1.44	0.27	0.27	0.36	0.36	0.09	0.00	0.27	
				18:00	19:00	49.50	2.61	23.04	17.01	0.09	3.24	0.81	0.18	0.18	0.63	0.18	0.36	0.36	0.54	0.09	0.00	0.18	
				19:00	20:00	45.00	1.89	22.23	16.83	0.00	0.90	0.27	0.09	0.09	1.44	0.18	0.18	0.27	0.27	0.09	0.00	0.27	
				20:00	21:00	40.50	1.80	16.29	18.63	0.00	0.72	0.09	0.09	0.09	2.16	0.09	0.09	0.09	0.09	0.00	0.00	0.27	
				21:00	22:00	36.00	1.17	12.87	18.63	0.00	0.90	0.18	0.09	0.09	1.80	0.00	0.00	0.00	0.00	0.00	0.00	0.27	
				22:00	23:00	31.50	1.44	9.36	17.64	0.00	0.45	0.00	0.00	0.00	1.80	0.09	0.09	0.18	0.09	0.00	0.00	0.36	
L186	Shun Yung Street (NB)	Fat Kwong Street	Pak Kung Street	23:00	0:00	27.00	1.35	7.74	15.03	0.00	0.54	0.00	0.00	0.00	1.62	0.09	0.09	0.18	0.09	0.00	0.00	0.27	
				0:00	1:00	22.50	0.63	5.40	14.85	0.00	0.36	0.00	0.00	0.00	0.90	0.00	0.00	0.18	0.09	0.09	0.00	0.00	
				1:00	2:00	13.50	0.36	3.33	9.63	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	13.50	0.36	2.97	9.90	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	9.00	0.36	2.25	6.21	0.00	0.18	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	9.00	0.27	1.98	6.66	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	9.00	0.18	1.89	6.39	0.00	0.27	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.09	0.09	0.00	0.00	
				6:00	7:00	18.00	0.36	3.51	11.79	0.00	0.54	0.09	0.00	0.00	0.81	0.09	0.09	0.36	0.18	0.18	0.00	0.00	
				7:00	8:00	49.50	0.18	11.88	30.24	0.18	1.53	0.27	0.18	0.18	2.25	0.36	0.27	0.90	0.72	0.36	0.00	0.00	
				8:00	9:00	67.50	0.63	24.21	31.32	0.27	3.60	1.26	0.81	0.72	1.80	0.36	0.36	0.99	0.81	0.36	0.00	0.00	
				9:00	10:00	63.00	0.45	18.54	31.95	0.27	3.51	1.35	0.81	0.72	1.62	0.54	0.45	1.26	1.08	0.45	0.00	0.00	
				10:00	11:00	45.00	1.26	13.77	18.72	0.45	5.04	1.62	0.72	0.63	1.53	0.09	0.09	0.54	0.27	0.27	0.00	0.00	
				11:00	12:00	40.50	0.81	12.51	15.39	0.45	5.85	0.63	0.27	0.18	2.52	0.27	0.18	0.63	0.45	0.36	0.00	0.00	
				12:00	13:00	45.00	1.08	16.47	19.08	0.36	3.96	0.54	0.27	0.18	1.98	0.36	0.09	0.27	0.18	0.18	0.00	0.00	
				13:00	14:00	40.50	0.81	11.70	13.23	0.45	5.04	2.07	0.81	0.81	1.98	0.09	0.45	1.26	0.99	0.81	0.00	0.00	
				14:00	15:00	40.50	0.63	10.53	17.73	0.36	4.50	1.53	0.54	0.45	2.07	0.18	0.18	0.81	0.54	0.45	0.00	0.00	
				15:00	16:00	49.50	1.08	14.22	21.33	0.36	5.13	1.89	0.81	0.72	1.89	0.27	0.18	0.72	0.54	0.36	0.00	0.00	
				16:00	17:00	31.50	1.98	14.40	6.84	0.36	3.78	0.72	0.09	0.09	2.34	0.00	0.00	0.36	0.00	0.54	0.00	0.00	
				17:00	18:00	40.50	2.88	21.33	8.37	0.27	2.88	1.35	0.27	0.18	2.34	0.00	0.00	0.18	0.00	0.45	0.00	0.00	
				18:00	19:00	36.00	2.70	20.43	8.10	0.18	1.98	0.81	0.18	0.09	0.99	0.00	0.00	0.18	0.00	0.36	0.00	0.00	
				19:00	20:00	45.00	1.08	19.53	20.43	0.00	0.72	0.18	0.09	0.09	2.07	0.09	0.09	0.27	0.18	0.18	0.00	0.00	
				20:00	21:00	40.50	0.99	13.86	21.78	0.00	0.45	0.09	0.09	0.09	2.88	0.00	0.00	0.09	0.09	0.09	0.00	0.00	
				21:00	22:00	36.00	0.72	10.71	21.15	0.00	0.72	0.18	0.09	0.09	2.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	31.50	0.81	7.65	19.98	0.00	0.36	0.00	0.00	0.00	2.25	0.09	0.00	0.18	0.09	0.09	0.00	0.00	
				23:00	0:00	27.00	0.72	6.39	17.19	0.00	0.36	0.00	0.00	0.00	1.89	0.09	0.00	0.18	0.09	0.09	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																	
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD		
L187	Sheung Lok Street (SB)	Sheung Wo Street	Fat Kwong Street	0:00	1:00	12.40	0.50	6.20	4.46	0.00	0.50	0.00	0.00	0.00	0.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	12.40	0.74	6.45	4.46	0.00	0.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	12.40	0.74	5.70	5.21	0.00	0.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	12.40	0.99	5.95	4.46	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	12.40	0.00	6.20	5.21	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	12.40	0.00	6.20	5.21	0.00	0.99	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	12.40	0.50	4.46	3.97	0.00	0.74	0.00	0.00	0.00	0.74	0.50	0.50	0.50	0.50	0.00	0.00	0.00	0.00	
				7:00	8:00	24.80	0.25	11.66	5.46	0.00	1.98	0.50	0.00	0.00	2.23	0.50	0.50	0.74	0.74	0.25	0.00	0.00	0.00	
				8:00	9:00	49.60	0.99	27.78	6.70	0.00	5.21	2.48	0.74	0.00	1.98	0.74	0.74	0.99	0.99	0.25	0.00	0.00	0.00	
				9:00	10:00	37.20	0.50	18.35	5.95	0.00	4.22	2.23	0.50	0.00	1.49	0.74	0.50	1.24	1.24	0.25	0.00	0.00	0.00	
				10:00	11:00	37.20	0.99	16.12	5.70	0.00	6.94	2.98	0.74	0.00	1.49	0.50	0.50	0.50	0.74	0.00	0.00	0.00	0.00	
				11:00	12:00	37.20	0.74	15.62	4.71	0.00	8.68	0.99	0.50	0.00	2.98	0.99	0.50	0.74	0.74	0.00	0.00	0.00	0.00	
				12:00	13:00	37.20	0.74	20.09	5.95	0.00	5.46	0.99	0.25	0.00	1.98	0.99	0.25	0.25	0.25	0.00	0.00	0.00	0.00	
				13:00	14:00	37.20	0.74	13.64	3.97	0.00	7.19	3.72	0.74	0.00	2.23	0.50	0.99	1.49	1.49	0.50	0.00	0.00	0.00	
				14:00	15:00	24.80	0.25	9.67	3.97	0.00	4.71	1.74	0.50	0.00	1.49	0.25	0.50	0.74	0.74	0.25	0.00	0.00	0.00	
				15:00	16:00	37.20	0.74	15.13	5.95	0.00	6.70	2.98	0.74	0.00	1.74	0.74	0.74	0.74	0.74	0.25	0.00	0.00	0.00	
				16:00	17:00	24.80	0.74	13.64	2.98	0.00	3.47	0.74	0.00	0.00	1.74	0.00	0.50	0.50	0.50	0.00	0.00	0.00	0.00	
				17:00	18:00	37.20	1.49	23.31	4.46	0.00	3.22	1.49	0.25	0.00	1.98	0.25	0.25	0.25	0.25	0.00	0.00	0.00	0.00	
				18:00	19:00	37.20	1.49	24.06	4.71	0.00	2.48	1.24	0.50	0.00	0.74	0.50	0.50	0.50	0.50	0.00	0.00	0.00	0.00	
				19:00	20:00	37.20	0.99	24.06	6.45	0.00	0.99	0.50	0.00	0.00	2.23	0.50	0.50	0.50	0.50	0.00	0.00	0.00	0.00	
				20:00	21:00	24.80	0.74	14.63	5.95	0.00	0.74	0.00	0.00	0.00	2.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	24.80	0.50	13.14	6.70	0.00	1.24	0.50	0.00	0.00	2.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	24.80	0.99	11.90	8.18	0.00	0.50	0.00	0.00	0.00	3.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				L188	Sheung Lok Street (NB)	Fat Kwong Street	Sheung Wo Street	23:00	0:00	12.40	0.50	5.70	4.22	0.00	0.25	0.00	0.00	0.00	1.74	0.00	0.00	0.00	0.00	0.00
0:00	1:00	12.40	0.00					6.70	3.97	0.00	0.50	0.00	0.00	0.00	0.74	0.00	0.00	0.50	0.00	0.00	0.00	0.00		
1:00	2:00	12.40	0.00					7.69	4.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
2:00	3:00	12.40	0.00					7.44	4.96	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
3:00	4:00	12.40	0.00					7.94	4.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
4:00	5:00	12.40	0.00					7.44	4.96	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
5:00	6:00	12.40	0.00					6.70	4.46	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	1.24	0.00	0.00	0.00	0.00		
6:00	7:00	12.40	0.00					4.71	3.72	0.00	0.50	0.50	0.00	0.00	0.99	0.50	0.50	0.50	0.50	0.00	0.00	0.00		
7:00	8:00	24.80	0.00					12.15	5.95	0.00	0.74	0.50	0.00	0.00	2.23	0.50	0.50	1.24	0.74	0.25	0.00	0.00		
8:00	9:00	49.60	0.00					29.76	7.44	0.00	2.23	2.98	0.74	0.00	1.98	0.74	0.74	1.74	0.99	0.25	0.00	0.00		
9:00	10:00	37.20	0.00					19.84	6.45	0.00	1.98	2.48	0.50	0.00	1.49	0.74	0.50	1.74	1.24	0.25	0.00	0.00		
10:00	11:00	24.80	0.00					12.40	3.72	0.00	2.73	2.98	0.50	0.00	0.99	0.25	0.25	0.50	0.50	0.00	0.00	0.00		
11:00	12:00	24.80	0.00					12.65	3.22	0.00	3.22	0.99	0.25	0.00	2.23	0.74	0.25	0.74	0.50	0.00	0.00	0.00		
12:00	13:00	24.80	0.00					14.88	3.72	0.00	1.98	0.99	0.25	0.00	1.49	0.74	0.25	0.25	0.25	0.00	0.00	0.00		
13:00	14:00	24.80	0.00					10.42	2.48	0.00	2.48	3.47	0.74	0.00	1.49	0.25	0.74	1.49	0.99	0.25	0.00	0.00		
14:00	15:00	24.80	0.00					10.66	3.72	0.00	2.48	2.73	0.50	0.00	1.74	0.25	0.50	1.24	0.74	0.25	0.00	0.00		
15:00	16:00	37.20	0.00					17.36	5.70	0.00	3.47	4.46	0.99	0.00	1.74	0.74	0.74	0.99	0.74	0.25	0.00	0.00		
16:00	17:00	24.80	0.00					13.89	1.98	0.00	2.73	1.74	0.25	0.00	1.98	0.00	0.74	0.74	0.74	0.00	0.00	0.00		
17:00	18:00	37.20	0.00					23.81	2.98	0.00	2.23	3.22	0.74	0.00	2.23	0.50	0.50	0.50	0.50	0.00	0.00	0.00		
18:00	19:00	24.80	0.00					18.10	2.23	0.00	1.24	1.49	0.25	0.00	0.50	0.25	0.25	0.25	0.25	0.00	0.00	0.00		
19:00	20:00	24.80	0.00					17.61	3.97	0.00	0.25	0.50	0.00	0.00	1.49	0.25	0.25	0.25	0.25	0.00	0.00	0.00		
20:00	21:00	24.80	0.00					15.87	5.46	0.00	0.25	0.25	0.00	0.00	2.73	0.00	0.00	0.25	0.00	0.00	0.00	0.00		
21:00	22:00	24.80	0.00					14.63	6.45	0.00	0.50	0.50	0.00	0.00	2.73	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
22:00	23:00	24.80	0.00					13.14	7.44	0.00	0.50	0.00	0.00	0.00	3.22	0.00	0.00	0.50	0.00	0.00	0.00	0.00		
23:00	0:00	12.40	0.00	6.20	3.97	0.00	0.25	0.00	0.00	0.00	1.74	0.00	0.00	0.25	0.00	0.00	0.00	0.00						

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L189	Kiang Su Street (EB)	Maidstone Road	Ma Tau Wai Road	0:00	1:00	2.10	0.21	1.47	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.10	0.34	1.43	0.34	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.10	0.42	1.26	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.10	0.42	1.26	0.42	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.10	0.00	1.39	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.10	0.00	1.39	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.10	0.29	1.22	0.59	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.10	0.21	1.34	0.42	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	2.10	0.25	1.34	0.21	0.00	0.17	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	2.10	0.25	1.26	0.25	0.00	0.21	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	2.10	0.21	1.22	0.21	0.00	0.34	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	2.10	0.13	1.26	0.21	0.00	0.42	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	2.10	0.21	1.34	0.21	0.00	0.25	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.10	0.17	1.18	0.17	0.00	0.42	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	2.10	0.17	1.09	0.25	0.00	0.42	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	2.10	0.21	1.18	0.25	0.00	0.34	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	2.10	0.21	1.34	0.13	0.00	0.34	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	2.10	0.17	1.60	0.13	0.00	0.17	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	2.10	0.21	1.51	0.17	0.00	0.17	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.10	0.17	1.64	0.25	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.10	0.17	1.51	0.34	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.10	0.21	1.39	0.42	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.10	0.25	1.30	0.55	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L190	Kiang Su Street (WB)	Ma Tau Wai Road	Maidstone Road	23:00	0:00	2.10	0.34	1.26	0.50	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				0:00	1:00	2.55	0.26	1.58	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.55	0.00	1.53	1.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.55	0.00	1.89	0.66	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.55	0.00	1.68	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.55	0.00	1.28	1.28	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.55	0.00	1.28	0.97	0.00	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.55	0.00	1.38	0.77	0.20	0.10	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	2.55	0.00	1.68	0.41	0.15	0.15	0.15	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.55	0.00	1.43	0.51	0.20	0.20	0.20	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.55	0.05	1.33	0.36	0.15	0.46	0.15	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.55	0.05	1.38	0.31	0.20	0.56	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.55	0.05	1.58	0.36	0.15	0.36	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.55	0.10	1.12	0.26	0.15	0.56	0.26	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.55	0.10	1.17	0.41	0.15	0.46	0.15	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.55	0.05	1.22	0.41	0.15	0.46	0.20	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.55	0.20	1.58	0.15	0.00	0.56	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	2.55	0.20	1.79	0.15	0.00	0.31	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	2.55	0.26	1.79	0.15	0.00	0.26	0.05	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.55	0.05	2.04	0.41	0.00	0.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.55	0.10	1.79	0.56	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.55	0.10	1.68	0.66	0.00	0.10	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	2.55	0.15	1.53	0.87	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					
23:00	0:00	2.55	0.20	1.43	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)															
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
L191	Access Road (NB)	Hung Fook Street	Ngan Hon Street	0:00	1:00	2.05	0.00	1.31	0.66	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				1:00	2:00	2.05	0.00	1.31	0.74	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				2:00	3:00	2.05	0.00	1.27	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.05	0.00	1.35	0.70	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.05	0.00	1.27	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.05	0.00	1.27	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.05	0.00	1.15	0.78	0.00	0.12	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.05	0.00	1.44	0.45	0.04	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				8:00	9:00	4.10	0.00	2.95	0.49	0.08	0.25	0.29	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	4.10	0.00	2.67	0.62	0.12	0.29	0.37	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	4.10	0.00	2.34	0.62	0.16	0.49	0.45	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	4.10	0.00	2.46	0.53	0.25	0.66	0.21	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	4.10	0.00	2.79	0.62	0.16	0.37	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	4.10	0.00	2.13	0.49	0.25	0.53	0.62	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	4.10	0.00	2.13	0.66	0.21	0.53	0.49	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	4.10	0.00	2.26	0.62	0.21	0.49	0.49	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	4.10	0.00	2.79	0.53	0.16	0.45	0.16	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.10	0.00	2.95	0.49	0.12	0.25	0.25	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.10	0.00	3.12	0.53	0.08	0.21	0.12	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	4.10	0.00	3.36	0.66	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	4.10	0.00	3.12	0.90	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.05	0.00	1.44	0.53	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.05	0.00	1.35	0.66	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				L192	Access Road (NB)	Kai Ming Street	Hung Fook Street	23:00	0:00	2.05	0.00	1.27	0.70	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00
0:00	1:00	1.85	0.00					1.07	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
1:00	2:00	1.85	0.00					1.07	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
2:00	3:00	1.85	0.00					1.00	0.85	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
3:00	4:00	1.85	0.00					1.04	0.81	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
4:00	5:00	1.85	0.00					1.07	0.78	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
5:00	6:00	1.85	0.00					0.93	0.93	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
6:00	7:00	1.85	0.00					0.81	0.81	0.11	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
7:00	8:00	1.85	0.00					1.07	0.52	0.11	0.11	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
8:00	9:00	3.70	0.00					2.55	0.59	0.19	0.19	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
9:00	10:00	3.70	0.00					2.22	0.70	0.26	0.26	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
10:00	11:00	3.70	0.00					1.89	0.70	0.30	0.41	0.33	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
11:00	12:00	3.70	0.00					2.00	0.63	0.44	0.52	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
12:00	13:00	3.70	0.00					2.29	0.70	0.30	0.30	0.11	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
13:00	14:00	3.70	0.00					1.74	0.52	0.41	0.48	0.48	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
14:00	15:00	3.70	0.00					1.78	0.78	0.33	0.41	0.33	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
15:00	16:00	3.70	0.00					1.89	0.70	0.33	0.37	0.33	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
16:00	17:00	3.70	0.00					2.26	0.63	0.30	0.33	0.19	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
17:00	18:00	3.70	0.00					2.48	0.59	0.15	0.22	0.22	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
18:00	19:00	3.70	0.00					2.63	0.63	0.11	0.15	0.15	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
19:00	20:00	3.70	0.00					2.74	0.74	0.07	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
20:00	21:00	1.85	0.00					1.26	0.52	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
21:00	22:00	1.85	0.00					1.15	0.59	0.04	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
22:00	23:00	1.85	0.00					1.04	0.74	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
23:00	0:00	1.85	0.00	1.04	0.74	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00					

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L193	Yuk Shing Street (NB)	Kai Ming Street	Hung Fook Street	0:00	1:00	2.10	0.00	1.18	0.80	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.10	0.00	1.26	0.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.10	0.00	1.18	0.92	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				3:00	4:00	2.10	0.00	1.39	0.71	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				4:00	5:00	2.10	0.00	1.26	0.84	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				5:00	6:00	2.10	0.00	1.05	1.05	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				6:00	7:00	2.10	0.00	1.05	0.88	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				7:00	8:00	2.10	0.00	1.22	0.59	0.00	0.08	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.13	0.00	0.00
				8:00	9:00	4.20	0.00	2.69	0.63	0.00	0.21	0.34	0.13	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.13	0.00	0.00
				9:00	10:00	4.20	0.00	2.35	0.71	0.00	0.25	0.38	0.17	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.25	0.00	0.00
				10:00	11:00	4.20	0.00	2.10	0.63	0.00	0.71	0.42	0.17	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				11:00	12:00	2.10	0.00	1.09	0.34	0.00	0.50	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				12:00	13:00	2.10	0.00	1.34	0.34	0.00	0.29	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	2.10	0.00	0.97	0.25	0.00	0.42	0.25	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				14:00	15:00	2.10	0.00	0.97	0.38	0.00	0.38	0.21	0.08	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				15:00	16:00	4.20	0.00	2.06	0.67	0.00	0.67	0.46	0.17	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.08	0.00	0.00
				16:00	17:00	4.20	0.00	2.52	0.59	0.00	0.92	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	4.20	0.00	2.90	0.55	0.00	0.55	0.17	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	4.20	0.00	3.02	0.55	0.00	0.42	0.13	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	2.10	0.00	1.64	0.38	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	2.10	0.00	1.51	0.55	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	2.10	0.00	1.39	0.63	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	2.10	0.00	1.26	0.76	0.00	0.08	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L194	Access Road (NB)	Wing Kwong Street	Kai Ming Street	23:00	0:00	2.10	0.00	1.22	0.76	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		
				C:00	1:00	1.10	0.00	0.75	0.31	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	1.10	0.00	0.70	0.33	0.00	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	1.10	0.00	0.73	0.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	1.10	0.00	0.79	0.31	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	1.10	0.00	0.73	0.37	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	1.10	0.00	0.64	0.37	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	1.10	0.00	0.64	0.33	0.00	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	2.20	0.00	1.56	0.40	0.04	0.11	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	3.30	0.00	2.44	0.33	0.02	0.20	0.24	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				9:00	10:00	2.20	0.00	1.50	0.26	0.02	0.18	0.20	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				10:00	11:00	2.20	0.00	1.32	0.26	0.04	0.26	0.26	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				11:00	12:00	2.20	0.00	1.34	0.26	0.09	0.37	0.11	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				12:00	13:00	2.20	0.00	1.54	0.29	0.04	0.22	0.09	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				13:00	14:00	2.20	0.00	1.17	0.22	0.07	0.31	0.35	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				14:00	15:00	2.20	0.00	1.17	0.33	0.07	0.29	0.29	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				15:00	16:00	2.20	0.00	1.21	0.31	0.04	0.29	0.29	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				16:00	17:00	2.20	0.00	1.50	0.26	0.04	0.24	0.13	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				17:00	18:00	3.30	0.00	2.42	0.35	0.04	0.20	0.24	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				18:00	19:00	3.30	0.00	2.55	0.37	0.02	0.18	0.15	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				19:00	20:00	2.20	0.00	1.80	0.31	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				20:00	21:00	2.20	0.00	1.69	0.42	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				21:00	22:00	2.20	0.00	1.61	0.51	0.00	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				22:00	23:00	1.10	0.00	0.77	0.31	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				23:00	0:00	1.10	0.00	0.75	0.33	0.00	0.02	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

TABLE B – VEHICLE-KILOMETRE-TRAVELLED (VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date:

01 June 2016

Link ID	Road Section	From Road	To Road	1-hour interval		Total VKT (km)	VKT (km)																
				Start	End		MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
L195	Wing Kwong Street (WB)	Kai Ming Street	Wan On Street	0:00	1:00	4.30	0.43	2.41	1.33	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	2.15	0.22	1.16	0.69	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	2.15	0.17	1.12	0.82	0.00	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	2.15	0.22	1.12	0.69	0.00	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	2.15	0.17	1.16	0.73	0.00	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	2.15	0.17	1.08	0.73	0.00	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	2.15	0.17	1.03	0.73	0.04	0.13	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	6.45	0.34	3.74	1.51	0.22	0.43	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00	
				8:00	9:00	12.90	1.12	7.87	1.63	0.47	1.03	0.60	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00
				9:00	10:00	10.75	0.77	5.98	1.68	0.43	1.03	0.65	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.13	0.00	0.00
				10:00	11:00	10.75	0.82	5.20	1.51	0.56	1.81	0.73	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				11:00	12:00	8.60	0.43	4.26	1.12	0.65	1.89	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				12:00	13:00	10.75	0.69	6.32	1.55	0.52	1.42	0.26	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	8.60	0.43	4.00	0.95	0.56	1.63	0.82	0.04	0.09	0.00	0.00	0.00	0.00	0.00	0.00	0.09	0.00	0.00
				14:00	15:00	8.60	0.39	3.87	1.38	0.56	1.63	0.65	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				15:00	16:00	10.75	0.65	5.20	1.63	0.60	1.76	0.77	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.04	0.00	0.00
				16:00	17:00	8.60	0.60	5.07	0.99	0.34	1.38	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	12.90	0.95	8.47	1.38	0.34	1.20	0.47	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	10.75	0.82	7.31	1.25	0.17	0.82	0.30	0.04	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	10.75	0.73	7.87	1.72	0.04	0.26	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	8.60	0.69	5.72	1.89	0.04	0.22	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	6.45	0.39	4.04	1.68	0.04	0.26	0.04	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	6.45	0.60	3.66	2.02	0.04	0.13	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	4.30	0.43	2.32	1.33	0.04	0.17	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
L196	Wing Kwong Street (WB)	Wan On Street	Ma Tau Wai Road	0:00	1:00	7.20	0.29	3.96	2.81	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				1:00	2:00	3.60	0.14	1.87	1.44	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				2:00	3:00	3.60	0.14	1.73	1.58	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				3:00	4:00	3.60	0.14	1.87	1.44	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				4:00	5:00	3.60	0.22	1.73	1.44	0.00	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				5:00	6:00	3.60	0.14	1.87	1.44	0.00	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				6:00	7:00	3.60	0.07	1.73	1.44	0.07	0.22	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				7:00	8:00	10.80	0.14	6.34	3.02	0.50	0.50	0.29	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	
				8:00	9:00	18.00	0.36	11.59	2.81	0.94	1.15	1.01	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				9:00	10:00	14.40	0.29	8.42	2.74	0.79	1.01	1.01	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				10:00	11:00	14.40	0.43	7.13	2.45	1.08	1.94	1.22	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				11:00	12:00	14.40	0.29	7.27	2.16	1.58	2.66	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				12:00	13:00	14.40	0.36	8.50	2.52	1.01	1.58	0.43	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				13:00	14:00	10.80	0.22	5.04	1.44	1.08	1.66	1.15	0.07	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				14:00	15:00	10.80	0.22	4.90	2.02	1.01	1.66	0.86	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				15:00	16:00	14.40	0.29	6.91	2.59	1.22	1.94	1.30	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				16:00	17:00	10.80	0.43	6.48	1.44	0.65	1.44	0.36	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				17:00	18:00	18.00	0.79	11.81	2.30	0.72	1.44	0.79	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				18:00	19:00	14.40	0.65	9.86	2.02	0.43	0.86	0.43	0.07	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				19:00	20:00	14.40	0.43	10.73	2.74	0.07	0.29	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				20:00	21:00	10.80	0.36	7.34	2.81	0.07	0.14	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				21:00	22:00	10.80	0.29	6.84	3.24	0.07	0.29	0.07	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				22:00	23:00	7.20	0.22	4.10	2.66	0.07	0.14	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
				23:00	0:00	7.20	0.29	3.96	2.66	0.07	0.22	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : PD (Primary Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
0:00	1:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	20	25	24.1%	22.1%	21.4%	20.3%	0.0%	26.8%	0.0%	0.0%	0.0%	100.0%	28.2%	30.0%	23.0%	27.4%	26.5%	0.0%	68.1%
0:00	1:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	30	35	75.9%	77.9%	78.6%	79.7%	0.0%	73.2%	0.0%	0.0%	0.0%	0.0%	71.8%	70.0%	77.0%	72.6%	73.5%	0.0%	31.9%
0:00	1:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
1:00	2:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	20	25	22.4%	22.9%	22.4%	21.2%	0.0%	29.5%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%
1:00	2:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	30	35	77.6%	77.1%	77.6%	78.8%	0.0%	70.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	20	25	22.9%	23.6%	22.8%	21.8%	0.0%	29.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%
2:00	3:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	30	35	77.1%	76.4%	77.2%	78.2%	0.0%	71.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : PD (Primary Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
3:00	4:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	20	25	23.6%	24.1%	23.4%	22.1%	0.0%	29.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%
3:00	4:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	30	35	76.4%	75.9%	76.6%	77.9%	0.0%	70.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%
4:00	5:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	20	25	21.8%	22.8%	21.6%	20.5%	0.0%	27.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	35.0%	0.0%	0.0%	0.0%	100.0%
4:00	5:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	30	35	78.2%	77.2%	78.4%	79.5%	0.0%	72.9%	0.0%	0.0%	0.0%	0.0%	98.0%	0.0%	65.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%
5:00	6:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	20	25	24.0%	25.3%	24.2%	22.9%	0.0%	30.4%	0.0%	0.0%	15.7%	0.0%	24.5%	33.2%	30.9%	29.6%	30.1%	0.0%	0.0%
5:00	6:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	30	35	76.0%	74.7%	75.8%	77.1%	0.0%	69.6%	0.0%	0.0%	84.3%	0.0%	75.5%	66.8%	69.1%	70.4%	69.9%	0.0%	0.0%
5:00	6:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : PD (Primary Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
6:00	7:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	20	25	25.2%	21.6%	21.2%	20.3%	53.5%	26.9%	24.5%	0.0%	14.3%	98.0%	26.3%	26.9%	26.0%	24.7%	24.3%	0.0%	69.1%
6:00	7:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	30	35	74.8%	78.4%	78.8%	79.7%	46.5%	73.1%	75.5%	0.0%	85.7%	2.0%	73.7%	73.1%	74.0%	75.3%	75.7%	0.0%	30.9%
6:00	7:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
7:00	8:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	20	25	28.1%	21.2%	22.5%	20.1%	68.8%	32.0%	26.9%	16.4%	15.8%	96.4%	24.4%	24.9%	23.8%	23.6%	24.0%	0.0%	67.8%
7:00	8:00	25	30	41.5%	33.3%	48.2%	46.5%	15.6%	38.9%	37.5%	45.0%	47.1%	1.0%	38.9%	41.0%	42.2%	43.2%	40.9%	0.0%	13.4%
7:00	8:00	30	35	30.4%	45.5%	29.3%	33.4%	15.6%	29.1%	35.6%	38.6%	37.1%	2.6%	36.7%	34.1%	34.0%	33.2%	35.1%	0.0%	18.8%
7:00	8:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
8:00	9:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	15	20	42.9%	33.6%	48.2%	46.3%	21.9%	38.7%	37.3%	47.3%	48.0%	2.0%	41.8%	41.8%	41.3%	44.3%	41.0%	0.0%	13.4%
8:00	9:00	20	25	25.8%	21.3%	22.6%	20.3%	59.9%	32.4%	27.9%	15.3%	15.3%	96.7%	24.1%	24.1%	23.9%	23.4%	23.2%	0.0%	67.7%
8:00	9:00	25	30	31.3%	45.1%	29.2%	33.4%	18.2%	28.9%	34.8%	37.4%	36.7%	1.3%	34.1%	34.1%	34.8%	32.3%	35.8%	0.0%	18.9%
8:00	9:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : PD (Primary Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
9:00	10:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	20	25	68.7%	54.6%	70.8%	66.5%	81.6%	71.2%	65.1%	63.2%	63.3%	98.5%	66.8%	66.1%	65.6%	66.7%	64.8%	0.0%	81.2%
9:00	10:00	25	30	31.3%	45.4%	29.2%	33.5%	18.4%	28.8%	34.9%	36.8%	36.7%	1.5%	33.2%	33.9%	34.4%	33.3%	35.2%	0.0%	18.8%
9:00	10:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
10:00	11:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	20	25	70.0%	64.4%	69.2%	67.4%	86.5%	74.4%	68.8%	66.3%	66.4%	97.8%	70.9%	69.7%	69.7%	71.5%	71.4%	0.0%	82.7%
10:00	11:00	25	30	30.0%	35.6%	30.8%	32.6%	13.5%	25.6%	31.2%	33.7%	33.6%	2.2%	29.1%	30.3%	30.3%	28.5%	28.6%	0.0%	17.3%
10:00	11:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
11:00	12:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	20	25	27.4%	22.2%	21.2%	20.2%	58.2%	27.0%	24.6%	16.5%	16.1%	98.8%	27.7%	28.0%	27.1%	26.4%	26.0%	0.0%	68.7%
11:00	12:00	25	30	43.5%	42.0%	47.9%	46.8%	25.1%	47.3%	44.6%	48.9%	50.7%	0.0%	43.4%	41.4%	43.5%	45.9%	43.2%	0.0%	13.8%
11:00	12:00	30	35	29.1%	35.8%	30.9%	33.0%	16.7%	25.7%	30.8%	34.6%	33.2%	1.2%	28.9%	30.6%	29.4%	27.7%	30.8%	0.0%	17.5%
11:00	12:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : PD (Primary Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
12:00	13:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	20	25	25.4%	22.0%	21.3%	20.3%	56.1%	27.0%	24.3%	16.4%	16.2%	98.3%	27.9%	24.4%	27.0%	26.7%	24.8%	0.0%	69.4%
12:00	13:00	25	30	74.6%	78.0%	78.7%	79.7%	43.9%	73.0%	75.7%	83.6%	83.8%	1.7%	72.1%	75.6%	73.0%	73.3%	75.2%	0.0%	30.6%
12:00	13:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
13:00	14:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	20	25	26.7%	21.7%	21.3%	20.2%	55.6%	26.9%	24.5%	16.4%	15.7%	98.3%	27.0%	27.3%	26.5%	26.4%	25.8%	0.0%	68.8%
13:00	14:00	25	30	43.8%	42.5%	47.6%	46.7%	28.0%	47.2%	44.1%	50.1%	50.3%	0.0%	46.1%	43.6%	44.5%	46.2%	43.9%	0.0%	13.8%
13:00	14:00	30	35	29.5%	35.8%	31.1%	33.1%	16.4%	25.9%	31.4%	33.5%	34.0%	1.7%	26.9%	29.1%	29.0%	27.4%	30.3%	0.0%	17.4%
13:00	14:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
14:00	15:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	20	25	26.5%	22.0%	21.3%	20.3%	57.0%	27.0%	24.6%	16.5%	16.0%	98.4%	25.5%	27.4%	25.7%	26.4%	25.8%	0.0%	69.0%
14:00	15:00	25	30	44.1%	42.5%	47.9%	47.0%	26.9%	47.4%	44.0%	50.3%	51.0%	0.0%	44.2%	42.9%	45.4%	45.9%	44.1%	0.0%	14.0%
14:00	15:00	30	35	29.4%	35.5%	30.8%	32.7%	16.1%	25.6%	31.4%	33.2%	33.0%	1.6%	30.3%	29.7%	28.9%	27.7%	30.1%	0.0%	17.0%
14:00	15:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : PD (Primary Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
15:00	16:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	20	25	70.3%	64.9%	69.4%	67.5%	81.5%	74.7%	69.1%	66.8%	67.2%	98.2%	71.5%	70.3%	70.7%	72.0%	69.4%	0.0%	82.9%
15:00	16:00	25	30	29.7%	35.1%	30.6%	32.5%	18.5%	25.3%	30.9%	33.2%	32.8%	1.8%	28.5%	29.7%	29.3%	28.0%	30.6%	0.0%	17.1%
15:00	16:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
16:00	17:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	20	25	25.6%	22.7%	20.4%	20.3%	53.1%	23.5%	22.1%	17.2%	16.6%	100.0%	33.7%	30.7%	29.3%	30.1%	30.7%	0.0%	70.2%
16:00	17:00	25	30	46.3%	49.7%	47.5%	47.9%	29.3%	53.4%	50.3%	54.6%	55.0%	0.0%	41.4%	45.7%	47.6%	47.7%	47.8%	0.0%	14.3%
16:00	17:00	30	35	28.1%	27.6%	32.1%	31.8%	17.6%	23.1%	27.6%	28.2%	28.4%	0.0%	24.9%	23.6%	23.1%	22.2%	21.5%	0.0%	15.5%
16:00	17:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
17:00	18:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	10	15	47.2%	50.1%	48.1%	48.4%	33.3%	54.1%	51.3%	53.3%	54.9%	0.0%	45.9%	45.9%	46.2%	48.1%	51.9%	0.0%	14.5%
17:00	18:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	20	25	23.9%	22.5%	20.0%	20.0%	53.4%	22.9%	21.4%	17.0%	16.1%	100.0%	31.1%	31.1%	29.5%	30.3%	29.8%	0.0%	70.2%
17:00	18:00	25	30	28.9%	27.4%	31.9%	31.6%	13.3%	23.0%	27.3%	29.7%	29.0%	0.0%	23.0%	23.0%	24.3%	21.6%	18.3%	0.0%	15.3%
17:00	18:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : PD (Primary Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
18:00	19:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	20	25	70.3%	72.1%	67.8%	68.2%	78.6%	76.8%	72.0%	69.7%	70.9%	100.0%	78.0%	75.7%	77.5%	78.0%	78.0%	0.0%	84.8%
18:00	19:00	25	30	29.7%	27.9%	32.2%	31.8%	21.4%	23.2%	28.0%	30.3%	29.1%	0.0%	22.0%	24.3%	22.5%	22.0%	22.0%	0.0%	15.2%
18:00	19:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
19:00	20:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	20	25	25.0%	22.1%	21.4%	20.4%	54.1%	26.9%	24.3%	15.3%	16.0%	98.4%	27.6%	25.4%	25.3%	26.0%	25.1%	0.0%	69.6%
19:00	20:00	25	30	75.0%	77.9%	78.6%	79.6%	45.9%	73.1%	75.7%	84.7%	84.0%	1.6%	72.4%	74.6%	74.7%	74.0%	74.9%	0.0%	30.4%
19:00	20:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
20:00	21:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	20	25	26.1%	22.0%	21.4%	20.4%	43.4%	27.0%	23.0%	15.7%	16.0%	98.2%	28.3%	30.0%	24.2%	25.1%	26.5%	0.0%	69.0%
20:00	21:00	25	30	43.5%	42.0%	47.5%	46.4%	37.7%	47.9%	44.0%	48.2%	52.5%	1.2%	41.0%	56.0%	43.3%	49.9%	58.8%	0.0%	14.1%
20:00	21:00	30	35	30.4%	36.0%	31.1%	33.2%	18.9%	25.1%	33.0%	36.1%	31.5%	0.6%	30.7%	14.0%	32.5%	25.0%	14.7%	0.0%	16.9%
20:00	21:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : PD (Primary Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
21:00	22:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	20	25	26.5%	22.3%	21.5%	20.6%	43.4%	27.1%	24.2%	16.7%	15.2%	96.6%	0.0%	17.1%	17.8%	0.0%	26.4%	0.0%	69.3%
21:00	22:00	25	30	43.4%	42.3%	47.7%	46.6%	37.7%	47.6%	44.2%	44.8%	54.9%	1.4%	0.0%	55.3%	47.0%	0.0%	49.1%	0.0%	13.6%
21:00	22:00	30	35	30.1%	35.4%	30.8%	32.8%	18.9%	25.3%	31.6%	38.5%	29.9%	2.0%	0.0%	27.6%	35.2%	0.0%	24.5%	0.0%	17.1%
21:00	22:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	0.0%	100.0%	0.0%	100.0%
22:00	23:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	20	25	28.0%	22.3%	21.6%	20.6%	43.4%	27.2%	0.0%	0.0%	0.0%	97.3%	22.8%	25.7%	26.1%	27.4%	26.6%	0.0%	69.1%
22:00	23:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	30	35	72.0%	77.7%	78.4%	79.4%	56.6%	72.8%	0.0%	0.0%	0.0%	2.7%	77.2%	74.3%	73.9%	72.6%	73.4%	0.0%	30.9%
22:00	23:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
23:00	0:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	15	20	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	20	25	28.1%	22.2%	21.7%	20.6%	53.5%	27.9%	0.0%	0.0%	0.0%	97.6%	25.6%	25.7%	24.8%	28.9%	28.2%	0.0%	69.5%
23:00	0:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	30	35	71.9%	77.8%	78.3%	79.4%	46.5%	72.1%	0.0%	0.0%	0.0%	2.4%	74.4%	74.3%	75.2%	71.1%	71.8%	0.0%	30.5%
23:00	0:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : DD (District Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
0:00	1:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
0:00	1:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
0:00	1:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
1:00	2:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
1:00	2:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : DD (District Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
3:00	4:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	15	20	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%
5:00	6:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : DD (District Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
6:00	7:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
6:00	7:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
7:00	8:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
7:00	8:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
8:00	9:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	10	15	4.9%	6.5%	5.9%	6.7%	3.5%	4.3%	2.6%	6.0%	5.9%	2.3%	3.0%	3.0%	3.1%	2.9%	2.2%	0.0%	0.1%
8:00	9:00	15	20	95.1%	93.5%	94.1%	93.3%	96.5%	95.7%	97.4%	94.0%	94.1%	97.7%	97.0%	97.0%	96.9%	97.1%	97.8%	0.0%	99.9%
8:00	9:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : DD (District Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
9:00	10:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	10	15	2.6%	1.1%	2.8%	4.6%	2.4%	2.1%	3.0%	2.8%	1.3%	1.1%	1.2%	1.7%	1.4%	1.4%	0.0%	0.0%	0.0%
9:00	10:00	15	20	97.4%	98.9%	97.2%	95.4%	97.6%	97.9%	98.8%	97.0%	97.2%	98.7%	98.9%	98.8%	98.3%	98.6%	98.6%	0.0%	100.0%
9:00	10:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
10:00	11:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
10:00	11:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
11:00	12:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
11:00	12:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : DD (District Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
12:00	13:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
12:00	13:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
13:00	14:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
13:00	14:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
14:00	15:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
14:00	15:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

ROAD GROUP : DD (District Distributor)

Date: 01 June 2016

Prepared by CKM Asia Limited

Date: 01 June 2016

[illegible]

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : DD (District Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																	
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
21:00	22:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	0.0%	100.0%	0.0%	
21:00	22:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
21:00	22:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	0.0%	100.0%	0.0%	100.0%
22:00	23:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	
22:00	23:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
22:00	23:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	
23:00	0:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	10	15	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	15	20	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	
23:00	0:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
23:00	0:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																	
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD	
0:00	1:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	10	15	62.1%	52.9%	54.1%	68.0%	0.0%	57.7%	0.0%	0.0%	0.0%	73.0%	65.0%	63.4%	62.9%	65.7%	55.3%	0.0%	95.0%	
0:00	1:00	15	20	37.9%	47.1%	45.9%	32.0%	0.0%	42.3%	0.0%	0.0%	0.0%	27.0%	35.0%	36.6%	37.1%	34.3%	44.7%	0.0%	5.0%	
0:00	1:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
0:00	1:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	
1:00	2:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	10	15	18.7%	16.7%	15.1%	21.8%	0.0%	15.9%	0.0%	0.0%	0.0%	14.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	15	20	81.3%	83.3%	84.9%	78.2%	0.0%	84.1%	0.0%	0.0%	0.0%	85.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
1:00	2:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
2:00	3:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	10	15	11.6%	10.0%	8.1%	14.1%	0.0%	12.5%	0.0%	0.0%	0.0%	13.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	15	20	88.4%	90.0%	91.9%	85.9%	0.0%	87.5%	0.0%	0.0%	0.0%	86.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
2:00	3:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
3:00	4:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	10	15	3.3%	2.6%	2.3%	4.2%	0.0%	2.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	15	20	96.7%	97.4%	97.7%	95.8%	0.0%	97.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
3:00	4:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	10	15	3.3%	3.1%	2.2%	4.2%	0.0%	2.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	15	20	96.7%	96.9%	97.8%	95.8%	0.0%	97.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
4:00	5:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	100.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	10	15	4.4%	3.7%	2.8%	5.1%	0.0%	4.3%	0.0%	0.0%	0.0%	0.0%	8.6%	10.2%	7.9%	9.7%	6.5%	0.0%	0.0%
5:00	6:00	15	20	95.6%	96.3%	97.2%	94.9%	0.0%	95.7%	0.0%	0.0%	100.0%	0.0%	91.4%	89.8%	92.1%	90.3%	93.5%	0.0%	0.0%
5:00	6:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
5:00	6:00	Total		100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	0.0%	0.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
6:00	7:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	10	15	45.9%	40.9%	38.1%	50.3%	55.9%	44.9%	48.7%	0.0%	100.0%	51.5%	48.8%	51.1%	48.4%	46.3%	42.6%	0.0%	36.4%
6:00	7:00	15	20	54.1%	59.1%	61.9%	49.7%	44.1%	55.1%	51.3%	0.0%	0.0%	48.5%	51.2%	48.9%	51.6%	53.7%	57.4%	0.0%	63.6%
6:00	7:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
6:00	7:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
7:00	8:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	10	15	90.3%	80.9%	85.4%	93.5%	80.2%	85.8%	87.7%	90.8%	94.9%	97.3%	95.9%	95.2%	95.2%	96.0%	90.2%	0.0%	100.0%
7:00	8:00	15	20	9.7%	19.1%	14.6%	6.5%	19.8%	14.2%	12.3%	9.2%	5.1%	2.7%	4.1%	4.1%	4.8%	4.0%	9.8%	0.0%	0.0%
7:00	8:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
7:00	8:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
8:00	9:00	0	5	1.7%	1.1%	1.2%	3.2%	1.0%	1.4%	1.0%	1.6%	1.7%	3.2%	0.9%	0.9%	1.1%	1.1%	1.0%	0.0%	0.0%
8:00	9:00	5	10	10.6%	9.0%	9.6%	12.1%	7.7%	10.2%	10.6%	8.5%	9.1%	16.0%	10.9%	10.9%	11.9%	11.9%	11.7%	0.0%	16.4%
8:00	9:00	10	15	84.1%	84.4%	84.9%	82.0%	86.3%	84.0%	84.0%	86.5%	87.6%	78.7%	86.8%	86.8%	85.6%	85.7%	85.7%	0.0%	83.6%
8:00	9:00	15	20	3.6%	5.5%	4.3%	2.7%	5.0%	4.4%	4.4%	3.4%	1.6%	2.1%	1.4%	1.4%	1.4%	1.3%	1.6%	0.0%	0.0%
8:00	9:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
8:00	9:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
9:00	10:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	5	10	8.0%	5.3%	6.5%	10.4%	4.8%	6.9%	7.6%	7.9%	8.5%	7.4%	9.6%	9.9%	9.9%	10.3%	8.5%	0.0%	0.0%
9:00	10:00	10	15	87.6%	87.9%	87.8%	86.5%	88.0%	87.3%	87.0%	87.9%	89.4%	90.0%	88.5%	88.7%	88.3%	88.5%	88.2%	0.0%	100.0%
9:00	10:00	15	20	4.4%	6.8%	5.7%	3.1%	7.2%	5.8%	5.4%	4.2%	2.1%	2.6%	1.9%	1.4%	1.8%	1.2%	3.3%	0.0%	0.0%
9:00	10:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
9:00	10:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
10:00	11:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	5	10	1.8%	1.6%	1.4%	2.7%	0.8%	1.5%	1.2%	1.4%	1.7%	3.8%	0.9%	0.7%	0.7%	0.8%	0.4%	0.0%	0.0%
10:00	11:00	10	15	92.3%	87.4%	91.3%	93.3%	90.6%	91.4%	92.3%	93.8%	95.7%	94.1%	96.1%	96.3%	96.3%	96.8%	95.3%	0.0%	100.0%
10:00	11:00	15	20	5.9%	11.0%	7.3%	4.0%	8.6%	7.1%	6.5%	4.8%	2.6%	2.1%	3.0%	3.0%	3.0%	2.4%	4.3%	0.0%	0.0%
10:00	11:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
10:00	11:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
11:00	12:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	10	15	94.2%	89.2%	92.6%	96.0%	91.1%	92.8%	92.9%	94.9%	96.9%	99.4%	96.7%	97.3%	96.4%	97.1%	95.3%	0.0%	100.0%
11:00	12:00	15	20	5.8%	10.8%	7.4%	4.0%	8.9%	7.2%	7.1%	5.1%	3.1%	0.6%	3.3%	2.7%	3.6%	2.9%	4.7%	0.0%	0.0%
11:00	12:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
11:00	12:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : LD (Local Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
12:00	13:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	5	10	1.8%	1.7%	1.4%	2.6%	0.8%	1.5%	1.0%	1.6%	1.6%	3.6%	0.9%	0.7%	0.9%	0.9%	0.8%	0.0%	0.0%
12:00	13:00	10	15	91.7%	86.8%	90.6%	93.3%	91.3%	89.7%	90.8%	94.0%	95.8%	95.8%	94.5%	96.1%	95.1%	95.2%	92.4%	0.0%	100.0%
12:00	13:00	15	20	6.5%	11.5%	8.0%	4.1%	7.9%	8.8%	8.2%	4.4%	2.6%	0.6%	4.6%	3.2%	4.0%	3.9%	6.8%	0.0%	0.0%
12:00	13:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
12:00	13:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
13:00	14:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	10	15	94.0%	88.0%	92.4%	95.7%	90.4%	92.7%	93.1%	94.2%	97.1%	97.8%	96.2%	96.7%	96.6%	97.2%	94.7%	0.0%	100.0%
13:00	14:00	15	20	6.0%	12.0%	7.6%	4.3%	9.6%	7.3%	6.9%	5.8%	2.9%	2.2%	3.8%	3.3%	3.4%	2.8%	5.3%	0.0%	0.0%
13:00	14:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
13:00	14:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
14:00	15:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	10	15	93.7%	88.0%	91.7%	95.5%	89.5%	92.3%	92.5%	93.2%	96.5%	98.0%	97.2%	96.7%	96.6%	97.3%	94.5%	0.0%	100.0%
14:00	15:00	15	20	6.3%	12.0%	8.3%	4.5%	10.5%	7.7%	7.5%	6.8%	3.5%	2.0%	2.8%	3.3%	3.4%	2.7%	5.5%	0.0%	0.0%
14:00	15:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
14:00	15:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
15:00	16:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	5	10	2.9%	2.8%	2.4%	3.9%	1.8%	2.5%	2.7%	2.1%	2.5%	4.0%	2.3%	2.2%	2.1%	2.3%	1.8%	0.0%	0.0%
15:00	16:00	10	15	93.0%	89.3%	92.3%	93.5%	93.6%	92.0%	91.9%	94.1%	95.3%	95.6%	96.0%	96.2%	95.8%	96.2%	93.7%	0.0%	100.0%
15:00	16:00	15	20	4.1%	7.9%	5.3%	2.6%	4.6%	5.5%	5.4%	3.8%	2.2%	0.4%	1.7%	1.6%	2.1%	1.5%	4.5%	0.0%	0.0%
15:00	16:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
15:00	16:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
16:00	17:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	10	15	94.7%	92.9%	93.4%	96.7%	91.7%	93.0%	92.0%	96.3%	98.7%	99.5%	96.5%	96.9%	96.6%	97.3%	94.9%	0.0%	100.0%
16:00	17:00	15	20	5.3%	7.1%	6.6%	3.3%	8.3%	7.0%	8.0%	3.7%	1.3%	0.5%	3.5%	3.1%	3.4%	2.7%	5.1%	0.0%	0.0%
16:00	17:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
16:00	17:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
17:00	18:00	0	5	3.0%	3.9%	2.7%	3.9%	1.9%	2.2%	2.0%	2.1%	2.5%	6.6%	1.2%	1.2%	1.4%	1.0%	1.1%	0.0%	0.0%
17:00	18:00	5	10	5.5%	5.8%	5.4%	6.4%	3.3%	4.5%	4.9%	6.4%	7.4%	7.2%	3.0%	3.0%	3.0%	2.9%	3.4%	0.0%	0.0%
17:00	18:00	10	15	88.5%	86.5%	88.2%	88.4%	91.2%	88.9%	88.3%	89.3%	89.2%	86.2%	95.2%	95.2%	94.6%	95.3%	92.5%	0.0%	100.0%
17:00	18:00	15	20	3.0%	3.8%	3.7%	1.3%	3.6%	4.4%	4.8%	2.2%	0.9%	0.0%	0.6%	0.6%	1.0%	0.8%	3.0%	0.0%	0.0%
17:00	18:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
17:00	18:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

ROAD GROUP : LD (Local Distributor)

Job No.: J6518

Date: 01 June 2016

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV ≤2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV ≤15t	HGV >15t	PLB	PrLB ≤3.5t	PrLB >3.5t	NFB ≤6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
18:00	19:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	5	10	2.8%	3.6%	2.6%	3.7%	2.0%	2.1%	1.9%	1.9%	2.1%	6.6%	0.8%	1.0%	1.2%	1.0%	1.1%	0.0%	0.0%
18:00	19:00	10	15	93.8%	92.1%	93.2%	94.7%	92.5%	92.9%	92.2%	95.6%	97.1%	93.4%	98.2%	97.9%	97.5%	97.7%	96.0%	0.0%	100.0%
18:00	19:00	15	20	3.4%	4.3%	4.2%	1.6%	5.5%	5.0%	5.9%	2.5%	0.8%	0.0%	1.0%	1.1%	1.3%	1.3%	2.9%	0.0%	0.0%
18:00	19:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
18:00	19:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
19:00	20:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	5	10	1.8%	1.7%	1.4%	2.6%	0.0%	1.6%	1.1%	3.4%	2.2%	3.7%	1.0%	0.6%	0.8%	0.8%	0.8%	0.0%	0.0%
19:00	20:00	10	15	91.9%	88.1%	90.8%	93.1%	100.0%	91.2%	91.6%	96.6%	97.8%	95.7%	93.8%	96.4%	95.2%	95.2%	92.4%	0.0%	100.0%
19:00	20:00	15	20	6.3%	10.2%	7.8%	4.3%	0.0%	7.2%	7.3%	0.0%	0.0%	0.6%	5.2%	3.0%	4.0%	4.0%	6.8%	0.0%	0.0%
19:00	20:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
19:00	20:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
20:00	21:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	10	15	91.0%	85.5%	88.1%	93.7%	94.2%	87.7%	94.0%	100.0%	100.0%	98.1%	94.3%	93.5%	95.5%	95.8%	90.0%	0.0%	100.0%
20:00	21:00	15	20	9.0%	14.5%	11.9%	6.3%	5.8%	12.3%	6.0%	0.0%	0.0%	1.9%	5.7%	6.5%	4.5%	4.2%	10.0%	0.0%	0.0%
20:00	21:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
20:00	21:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

TABLE C – SPEED FRACTION (PERCENTAGE IN TERMS OF VKT)

YEAR 2041 TRAFFIC FORECAST

Job No.: J6518

Date: 01 June 2016

ROAD GROUP : LD (Local Distributor)

1-hour interval		Speed (mph)		Percentage of Speed Distribution (in terms of VKT)																
Start	End	From	To	Total	MC	PC	Taxi	LGV <=2.5t	LGV 2.5-3.5t	LGV >3.5t	HGV <=15t	HGV >15t	PLB	PrLB <=3.5t	PrLB >3.5t	NFB <=6.4t	NFB 6.4-15t	NFB >15t	FBSD	FBDD
21:00	22:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	10	15	88.6%	79.6%	83.6%	92.4%	95.3%	85.5%	88.4%	100.0%	100.0%	97.8%	0.0%	100.0%	94.9%	0.0%	100.0%	0.0%	100.0%
21:00	22:00	15	20	11.4%	20.4%	16.4%	7.6%	4.7%	14.5%	11.6%	0.0%	0.0%	2.2%	0.0%	0.0%	5.1%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
21:00	22:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%	100.0%	0.0%	100.0%	0.0%	100.0%
22:00	23:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	10	15	84.3%	75.2%	78.4%	88.1%	89.1%	80.5%	0.0%	0.0%	0.0%	92.0%	91.7%	90.2%	90.4%	92.2%	77.0%	0.0%	100.0%
22:00	23:00	15	20	15.7%	24.8%	21.6%	11.9%	10.9%	19.5%	0.0%	0.0%	0.0%	8.0%	8.3%	9.8%	9.6%	7.8%	23.0%	0.0%	0.0%
22:00	23:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
22:00	23:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%
23:00	0:00	0	5	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	5	10	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	10	15	73.5%	65.1%	65.3%	79.0%	91.0%	69.4%	0.0%	0.0%	0.0%	81.4%	79.9%	74.9%	76.8%	77.2%	64.4%	0.0%	96.0%
23:00	0:00	15	20	26.5%	34.9%	34.7%	21.0%	9.0%	30.6%	0.0%	0.0%	0.0%	18.6%	20.1%	25.1%	23.2%	22.8%	35.6%	0.0%	4.0%
23:00	0:00	20	25	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	25	30	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	30	35	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	35	40	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	40	45	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	45	50	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	50	55	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	55	60	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	60	65	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	65	70	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
23:00	0:00	Total		100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	0.0%	0.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	0.0%	100.0%

APPENDIX 3.2: EMFAC INFORMATION

Hour	Annual Hourly Average	
	RH(%)	Temp.(°C)
1	78	23
2	78	23
3	79	23
4	79	23
5	79	23
6	80	23
7	80	23
8	79	23
9	78	23
10	70	24
11	66	25
12	63	26
13	60	27
14	60	27
15	60	27
16	61	27
17	63	26
18	65	26
19	69	25
20	72	25
21	74	24
22	75	24
23	76	24
24	77	24
min	60	23
max	80	27

[illegible]

Table 2: Running emission for combination of speed and vehicle class (g/km) & starting emission for each vehicle class (g/trip) for Year 2026

Emission Type	pollutant	Sf/time	PC	Taxi	LGV3	LGV4	LGV6	HGV7	HGV8	PLB	PV4	PV5	NFB6	NFB7	NFB8	FBSD	FBDD	MC
Running Exhaust	Nox	4	0.059	0.6472	2.511	2.3357	2.7877	4.9311	8.0191	2.2163	0.6962	2.4733	5.3702	4.0963	7.0842	9.514	11.9559	0.584
Running Exhaust	Nox	12	0.0395	0.4805	1.654	1.605	2.1105	3.9105	5.578	1.8005	0.5726	2.1195	4.6622	3.5562	4.7732	6.7517	8.4852	0.5123
Running Exhaust	Nox	20	0.0321	0.4181	1.1762	1.1353	1.4595	2.818	3.5246	1.3197	0.4864	1.5898	3.5128	2.6795	2.8783	4.3554	5.4741	0.4567
Running Exhaust	Nox	28	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	3.2631	4.1014	0.4138
Running Exhaust	Nox	36	0.0246	0.3563	0.6615	0.6113	0.9665	1.7463	2.0967	0.897	0.3873	1.0785	2.3777	1.8137	1.6077	2.63	3.3059	0.381
Running Exhaust	Nox	44	0.0222	0.3373	0.5464	0.4963	0.8458	1.4909	1.7617	0.799	0.363	0.9709	2.1447	1.6359	1.3196	2.214	2.783	0.3564
Running Exhaust	Nox	52	0.0203	0.3223	0.475	0.4243	0.7729	1.3363	1.5267	0.7229	0.3515	0.8829	1.9517	1.4887	1.1211	1.982	2.4902	0.3385
Running Exhaust	Nox	60	0.0195	0.3062	0.4414	0.3863	0.7543	1.2891	1.3527	0.6634	0.3518	0.8134	1.7987	1.372	0.9765	1.9754	2.4777	0.3264
Running Exhaust	Nox	68	0.0189	0.3248	0.4172	0.3588	0.7412	1.2547	1.265	0.6176	0.364	0.7615	1.6856	1.2858	0.9047	1.9721	2.4713	0.3193
Running Exhaust	Nox	76	0.0183	0.3436	0.3992	0.3381	0.7315	1.2288	1.265	0.5838	0.3895	0.727	1.6125	1.23	0.9047	1.9721	2.4713	0.3169
Running Exhaust	Nox	84	0.0178	0.3625	0.3855	0.3222	0.7242	1.2087	1.265	0.5607	0.4312	0.7096	1.5794	1.2048	0.9047	1.9721	2.4713	0.319
Running Exhaust	Nox	92	0.0174	0.3816	0.3797	0.3149	0.7185	1.1992	1.265	0.5477	0.4942	0.709	1.5863	1.21	0.9047	1.9721	2.4713	0.3257
Running Exhaust	Nox	100	0.017	0.4007	0.3807	0.3154	0.714	1.1992	1.265	0.5441	0.5865	0.7252	1.6331	1.2457	0.9047	1.9721	2.4713	0.3371
Running Exhaust	PM10-total	4	0.0169	0	0.054	0.0694	0.0573	0.1966	0.2114	0.0755	0.0348	0.1475	0.1828	0.0947	0.1574	0.1711	0.3767	0.0157
Running Exhaust	PM10-total	12	0.0107	0	0.0419	0.0537	0.0477	0.1308	0.1646	0.0644	0.0249	0.1259	0.156	0.0747	0.1228	0.1639	0.3074	0.0103
Running Exhaust	PM10-total	20	0.0071	0	0.0332	0.0426	0.0377	0.0811	0.1194	0.0469	0.0186	0.0916	0.1135	0.0557	0.089	0.1496	0.2371	0.0071
Running Exhaust	PM10-total	28	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0.1359	0.1998	0.0051
Running Exhaust	PM10-total	36	0.0037	0	0.0224	0.0287	0.0294	0.0505	0.0824	0.0274	0.0116	0.0536	0.0664	0.0402	0.0614	0.1282	0.1759	0.0039
Running Exhaust	PM10-total	44	0.0029	0	0.019	0.0244	0.0276	0.044	0.0727	0.0234	0.0096	0.0457	0.0567	0.036	0.0542	0.1272	0.1588	0.0031
Running Exhaust	PM10-total	52	0.0024	0	0.0165	0.0211	0.0266	0.0398	0.0671	0.0218	0.0082	0.0425	0.0527	0.0329	0.05	0.1313	0.1487	0.0026
Running Exhaust	PM10-total	60	0.002	0	0.0146	0.0187	0.0263	0.0368	0.0669	0.0214	0.0072	0.0419	0.0519	0.0304	0.0495	0.1406	0.1483	0.0023
Running Exhaust	PM10-total	68	0.0019	0	0.0132	0.0169	0.0266	0.0347	0.0668	0.0213	0.0065	0.0417	0.0517	0.0284	0.0494	0.149	0.148	0.0022
Running Exhaust	PM10-total	76	0.0018	0	0.0122	0.0157	0.0274	0.0332	0.0668	0.0212	0.0061	0.0415	0.0514	0.0268	0.0494	0.149	0.148	0.0021
Running Exhaust	PM10-total	84	0.0018	0	0.0116	0.0148	0.0289	0.0321	0.0668	0.0212	0.0059	0.0415	0.0514	0.0254	0.0494	0.149	0.148	0.0022
Running Exhaust	PM10-total	92	0.0019	0	0.0112	0.0143	0.0309	0.0314	0.0668	0.0212	0.0059	0.0415	0.0514	0.0242	0.0494	0.149	0.148	0.0024
Running Exhaust	PM10-total	100	0.0022	0	0.0111	0.0142	0.0336	0.0309	0.0668	0.0212	0.006	0.0415	0.0514	0.0231	0.0494	0.149	0.148	0.0027
Running Exhaust	PM2.5-total	4	0.0156	0.0000	0.0497	0.0638	0.0527	0.1808	0.1945	0.0695	0.0321	0.1357	0.1681	0.0872	0.1448	0.1574	0.3465	0.0137
Running Exhaust	PM2.5-total	12	0.0099	0.0000	0.0385	0.0494	0.0439	0.1204	0.1514	0.0593	0.0230	0.1158	0.1435	0.0687	0.1130	0.1508	0.2828	0.0089
Running Exhaust	PM2.5-total	20	0.0066	0.0000	0.0306	0.0392	0.0347	0.0746	0.1098	0.0431	0.0171	0.0843	0.1044	0.0513	0.0819	0.1377	0.2181	0.0061
Running Exhaust	PM2.5-total	28	0.0046	0.0000	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0.1250	0.1838	0.0044
Running Exhaust	PM2.5-total	36	0.0034	0.0000	0.0206	0.0264	0.0271	0.0465	0.0759	0.0252	0.0107	0.0493	0.0611	0.0370	0.0565	0.1180	0.1618	0.0033
Running Exhaust	PM2.5-total	44	0.0027	0.0000	0.0175	0.0224	0.0254	0.0405	0.0669	0.0215	0.0088	0.0421	0.0521	0.0331	0.0499	0.1170	0.1461	0.0027
Running Exhaust	PM2.5-total	52	0.0022	0.0000	0.0152	0.0194	0.0244	0.0366	0.0617	0.0200	0.0076	0.0391	0.0485	0.0303	0.0460	0.1208	0.1368	0.0022
Running Exhaust	PM2.5-total	60	0.0019	0.0000	0.0134	0.0172	0.0242	0.0338	0.0615	0.0197	0.0067	0.0385	0.0477	0.0280	0.0456	0.1293	0.1364	0.0020
Running Exhaust	PM2.5-total	68	0.0017	0.0000	0.0122	0.0156	0.0244	0.0319	0.0614	0.0196	0.0060	0.0384	0.0475	0.0262	0.0454	0.1371	0.1362	0.0018
Running Exhaust	PM2.5-total	76	0.0017	0.0000	0.0112	0.0144	0.0252	0.0305	0.0614	0.0195	0.0056	0.0382	0.0473	0.0246	0.0454	0.1371	0.1362	0.0018
Running Exhaust	PM2.5-total	84	0.0017	0.0000	0.0106	0.0136	0.0265	0.0295	0.0614	0.0195	0.0054	0.0382	0.0473	0.0233	0.0454	0.1371	0.1362	0.0019
Running Exhaust	PM2.5-total	92	0.0018	0.0000	0.0103	0.0132	0.0284	0.0288	0.0614	0.0195	0.0054	0.0382	0.0473	0.0222	0.0454	0.1371	0.1362	0.0020
Running Exhaust	PM2.5-total	100	0.0020	0.0000	0.0102	0.0131	0.0309	0.0284	0.0614	0.0195	0.0056	0.0382	0.0473	0.0213	0.0454	0.1371	0.1362	0.0023
Starting emission	Nox	all	0.0581	0.9441	0.0247	0.0078	0	0	0	3.8563	0.1111	0.9392	0	0	0	0	0	0.2693
Starting emission	PM10-total	all	0.0044	0	0.0002	0.0003	0	0	0	0	0.0035	0	0	0	0	0	0	0.0066
Starting emission	PM2.5-total	all	0.0041	0	0.0001	0.0002	0	0	0	0	0.0032	0	0	0	0	0	0	0.0055

Note: running & starting emission based on highest value within temperature range of 20-30°C and RH of 60-80% based on annual meteorological data; starting emission based on highest value among different durations before cold start

Emission factor generated using EMFAC mode for Year 2025 (EMFAC V3.1.1)

Table 3.1: Hourly running emission factor (g/km) for road group with speed limit of Local Distributor

Pollutant	hour of day	PC	Taxi	LGV3	LGV4	LGV6	HGV7	HGV8	PLB	PV4	PV5	NFB6	NFB7	NFB8	FBSD	FBDD	MC
Nox	00:00 ~ 01:00	0.03008	0.406388	0	1.012842	0	0	0	1.245477	0.46561	1.463859	3.227019	2.477953	2.51328	0	5.405465	0.436494
Nox	01:00 ~ 02:00	0.028364	0.389479	0	0.891831	0	0	0	1.08521	0	0	0	0	0	0	0	0.420964
Nox	02:00 ~ 03:00	0.028056	0.386661	0	0.881988	0	0	0	1.082461	0	0	0	0	0	0	0	0.41809
Nox	03:00 ~ 04:00	0.027801	0.383037	0	0.852169	0	0	0	0	0	0	0	0	0	0	0	0.414915
Nox	04:00 ~ 05:00	0.027797	0.383037	0	0.853906	0	0	0	0	0	0	2.7425	0	0	0	0	0.41513
Nox	05:00 ~ 06:00	0.027823	0.383367	0	0.858249	0	0	2.6143	0	0.432108	1.280798	2.803354	2.148897	2.114779	0	0	0.415387
Nox	06:00 ~ 07:00	0.029376	0.39991	1.049413	0.975786	1.300111	0	3.5246	1.186374	0.455987	1.421535	3.115325	2.363959	2.409572	0	4.601063	0.431346
Nox	07:00 ~ 08:00	0.031458	0.415721	1.119275	1.094191	1.421284	2.761089	3.478175	1.312278	0.483965	1.575692	3.475826	2.655996	2.798273	0	5.4741	0.448506
Nox	08:00 ~ 09:00	0.032236	0.431993	1.211964	1.187277	1.528117	2.92364	3.773301	1.419546	0.496852	1.650671	3.659226	2.791773	3.128997	0	5.96792	0.460745
Nox	09:00 ~ 10:00	0.03233	0.423455	1.178434	1.150918	1.492198	2.878326	3.680023	1.348132	0.493547	1.637423	3.612725	2.762749	3.012419	0	5.4741	0.45673
Nox	10:00 ~ 11:00	0.031882	0.418321	1.155297	1.121791	1.447117	2.803602	3.53584	1.332198	0.485394	1.583185	3.497737	2.672411	2.850766	0	5.4741	0.452871
Nox	11:00 ~ 12:00	0.031774	0.416636	1.150613	1.114456	1.43744	2.786451	3.496381	1.318051	0.48444	1.580509	3.485069	2.66246	2.83992	0	5.4741	0.452067
Nox	12:00 ~ 13:00	0.031852	0.418222	1.15731	1.11687	1.440533	2.808262	3.533787	1.335359	0.484443	1.582497	3.492333	2.664474	2.83793	0	5.4741	0.452712
Nox	13:00 ~ 14:00	0.031766	0.416526	1.1486	1.114167	1.438062	2.782121	3.498201	1.313652	0.484143	1.578445	3.48661	2.663047	2.83502	0	5.4741	0.451552
Nox	14:00 ~ 15:00	0.031735	0.416453	1.146013	1.113009	1.436198	2.775935	3.49274	1.314202	0.484737	1.578445	3.48661	2.663635	2.833387	0	5.4741	0.451552
Nox	15:00 ~ 16:00	0.032044	0.419582	1.171575	1.13112	1.460299	2.817436	3.555908	1.337832	0.487373	1.595948	3.520761	2.69085	2.875661	0	5.4741	0.454868
Nox	16:00 ~ 17:00	0.03181	0.416892	1.152338	1.115035	1.434644	2.795112	3.512766	1.318326	0.484321	1.579133	3.48661	2.663635	2.836653	0	5.4741	0.453654
Nox	17:00 ~ 18:00	0.030363	0.430553	1.206979	1.170107	1.503049	2.918686	3.780721	1.413493	0.491147	1.614228	3.565583	2.714392	2.964494	0	5.4741	0.463259
Nox	18:00 ~ 19:00	0.032108	0.419823	1.169944	1.130689	1.453538	2.823293	3.560439	1.351433	0.486496	1.591312	3.516579	2.680628	2.875463	0	5.4741	0.456857
Nox	19:00 ~ 20:00	0.03186	0.418149	1.1762	1.121971	1.44398	2.855145	3.569775	1.33584	0.484173	1.582655	3.491183	2.66301	2.83793	0	5.4741	0.453269
Nox	20:00 ~ 21:00	0.031576	0.415794	1.159525	1.099692	1.440858	2.818	3.5246	1.314477	0.483014	1.567434	3.478137	2.654821	2.79664	0	5.4741	0.45048
Nox	21:00 ~ 22:00	0.031378	0.415318	1.162688	1.093323	1.423459	2.818	3.5246	1.313652	0	1.5898	3.473515	0	2.8783	0	5.4741	0.447948
Nox	22:00 ~ 23:00	0.03115	0.413745	1.144863	1.078848	0	0	1.297708	0.48147	1.556078	3.438851	2.633667	2.690482	0	0	5.4741	0.446061
Nox	23:00 ~ 24:00	0.030573	0.410414	1.150325	1.046713	0	0	1.268569	0.474461	1.503431	3.33409	2.545527	2.58759	0	5.419192	0.441728	
PM10-total	00:00 ~ 01:00	0.006136	0	0	0.039216	0	0	0	0.04366	0.01713	0.083036	0.102741	0.052442	0.081401	0	0.235235	0.006158
PM10-total	01:00 ~ 02:00	0.005317	0	0	0.035872	0	0	0	0.036664	0	0	0	0	0	0	0	0.005434
PM10-total	02:00 ~ 03:00	0.00517	0	0	0.0356	0	0	0	0.036544	0	0	0	0	0	0	0	0.0053
PM10-total	03:00 ~ 04:00	0.005048	0	0	0.034776	0	0	0	0	0	0	0	0	0	0	0	0.005152
PM10-total	04:00 ~ 05:00	0.005046	0	0	0.034824	0	0	0	0	0	0	0.0845	0	0	0	0	0.005162
PM10-total	05:00 ~ 06:00	0.005059	0	0	0.034944	0	0	0.0966	0	0.014761	0.070587	0.086791	0.047122	0.073105	0	0	0.005174
PM10-total	06:00 ~ 07:00	0.0058	0	0.030466	0.038192	0.035032	0	0.1194	0.04108	0.01645	0.080157	0.098536	0.050599	0.079242	0	0.213377	0.005918
PM10-total	07:00 ~ 08:00	0.006793	0	0.031972	0.041464	0.03706	0.07926	0.118237	0.046576	0.018428	0.090641	0.112108	0.05532	0.087334	0	0.2371	0.006718
PM10-total	08:00 ~ 09:00	0.00727	0	0.033768	0.043755	0.038727	0.086493	0.124712	0.050363	0.019374	0.095514	0.118914	0.058267	0.093367	0	0.248629	0.007373
PM10-total	09:00 ~ 10:00	0.007214	0	0.033171	0.042902	0.038179	0.084186	0.122763	0.047883	0.019125	0.094668	0.117186	0.057543	0.091312	0	0.2371	0.007134
PM10-total	10:00 ~ 11:00	0.006997	0	0.032736	0.042199	0.037482	0.080836	0.119576	0.047313	0.018531	0.091138	0.112928	0.055624	0.088404	0	0.2371	0.006931
PM10-total	11:00 ~ 12:00	0.006945	0	0.032648	0.042024	0.037331	0.08008	0.118693	0.046828	0.018461	0.090968	0.112456	0.055425	0.088201	0	0.2371	0.006884
PM10-total	12:00 ~ 13:00	0.006982	0	0.03278	0.042063	0.037374	0.081015	0.11953	0.047458	0.018464	0.091091	0.112723	0.055501	0.088114	0	0.2371	0.006924
PM10-total	13:00 ~ 14:00	0.00694	0	0.032605	0.042016	0.037341	0.07994	0.118739	0.046636	0.01844	0.090828	0.112514	0.055434	0.088099	0	0.2371	0.00686
PM10-total	14:00 ~ 15:00	0.006926	0	0.032549	0.041984	0.03731	0.07974	0.118602	0.04666	0.018482	0.090828	0.112514	0.055444	0.088065	0	0.2371	0.00686
PM10-total	15:00 ~ 16:00	0.007075	0	0.033071	0.042438	0.037689	0.081384	0.120028	0.047552	0.018674	0.09198	0.113784	0.055995	0.088443	0	0.2371	0.007032
PM10-total	16:00 ~ 17:00	0.006961	0	0.032685	0.04204	0.037284	0.08036	0.119104	0.046838	0.018453	0.090875	0.112514	0.055444	0.088133	0	0.2371	0.006958
PM10-total	17:00 ~ 18:00	0.007481	0	0.033659	0.043337	0.038332	0.086266	0.12484	0.050048	0.018958	0.093159	0.115455	0.056565	0.090392	0	0.2371	0.007545
PM10-total	18:00 ~ 19:00	0.007105	0	0.033093	0.042433	0.037583	0.081544	0.120167	0.048055	0.018608	0.091686	0.113633	0.055767	0.088879	0	0.2371	0.007129
PM10-total	19:00 ~ 20:00	0.006987	0	0.0332	0.042202	0.03743	0.08279	0.118602	0.047476	0.018445	0.091104	0.11268	0.055472	0.088114	0	0.2371	0.00695
PM10-total	20:00 ~ 21:00	0.00685	0	0.03284	0.041616	0.037388	0.0811	0.1194	0.046672	0.018361	0.090079	0.112195	0.055301	0.0873	0	0.2371	0.00681
PM10-total	21:00 ~ 22:00	0.006756	0	0.032909	0.04144	0.037097	0.0811	0.1194	0.046636	0	0.0916	0.112021	0	0.089	0	0.2371	0.006692
PM10-total	22:00 ~ 23:00	0.006646	0	0.032524	0.04104	0	0	0	0.04594	0.018251	0.089307	0.110716	0.054959	0.08509	0	0.2371	0.006604
PM10-total	23:00 ~ 24:00	0.006371	0	0.032642	0.040152	0	0	0	0.044668	0.017756	0.085727	0.106772	0.053534	0.082948	0	0.235608	0.006402
PM2.5-total	00:00 ~ 01:00	0.005682	0	0	0.03607	0	0	0	0.04013	0.01577	0.076431	0.094531	0.048282	0.074882	0	0.216385	0.005299
PM2.5-total	01:00 ~ 02:00	0.004902	0	0	0.032977	0	0	0	0.033717	0	0	0	0	0	0	0	0.004684
PM2.5-total	02:00 ~ 03:00	0.004762	0	0	0.032725	0	0	0	0.033607	0	0	0	0	0	0	0	0.00457
PM2.5-total	03:00 ~ 04:00	0.004646	0	0	0.031963	0	0	0	0	0	0	0	0	0	0	0	0.004444
PM2.5-total	04:00 ~ 05:00	0.004644	0	0	0.032007	0	0	0	0	0	0	0.0778	0	0	0	0	0.004453
PM2.5-total	05:00 ~ 06:00	0.004656	0	0	0.032118	0	0	0.0888	0	0.013627	0.064993	0.079901	0.043354	0.067221	0	0	0.004463
PM2.5-total	06:00 ~ 07:00	0.005362	0	0.028042	0.035123	0.032238	0	0.1098	0.037765	0.015154	0.073787	0.090674	0.046574	0.072888	0	0.196285	0.005095
PM2.5-total	07:00 ~ 08:00	0.006308	0	0.029452	0.038149	0.03411	0.072907	0.108729	0.042803	0.016944	0.083419	0.103123	0.050948	0.080361	0	0.2181	0.005775
PM2.5-total	08:00 ~ 09:00	0.006752	0	0.031109	0.040259	0.035644	0.079567	0.11469	0.046306	0.017825	0.087895	0.109381	0.053651	0.085917	0	0.228711	0.006342
PM2.5-total	09:00 ~ 10:00	0.006701	0	0.030562	0.039475	0.035514	0.077445	0.112895	0.044013	0.017594	0.087118	0.107792	0.052987	0.084025	0	0.2181	0.006133
PM2.5-total	10:00 ~ 11:00	0.0065	0	0.030164	0.038828	0.034498	0.074358	0.109961	0.043485	0.017039	0.083876	0.103876	0.051228	0.081349	0	0.2181	0.005958
PM2.5-total	11:00 ~ 12:00	0.006452	0	0.030084	0.038667	0.034359	0.073662	0.									

Table 3.2: Hourly running emission factor (g/km) for road group with speed limit of District Distributor

Pollutant	hour of day	PC	Taxi	LGV3	LGV4	LGV6	HGV7	HGV8	PLB	PV4	PV5	NFB6	NFB7	NFB8	FBS0	FBD0	MC
Nox	00:00 ~ 01:00	0.0277	0.3815	0	0.8458	0	0	0	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	01:00 ~ 02:00	0.0277	0.3815	0	0.8458	0	0	0	1.0448	0	0	0	0	0	0	0	0.4138
Nox	02:00 ~ 03:00	0.0277	0.3815	0	0.8458	0	0	0	1.0448	0	0	0	0	0	0	0	0.4138
Nox	03:00 ~ 04:00	0.0277	0.3815	0	0.8458	0	0	0	0	0	0	0	0	0	0	0	0.4138
Nox	04:00 ~ 05:00	0.0277	0.3815	0	0.8458	0	0	0	0	0.427	0	2.7425	0	0	0	0	0.4138
Nox	05:00 ~ 06:00	0.0277	0.3815	0	0.8458	0	0	2.6143	0	0.427	1.2457	2.7425	2.0919	2.0617	0	0	0.4138
Nox	06:00 ~ 07:00	0.0277	0.3815	0.8887	0.8458	1.1488	0	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	07:00 ~ 08:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	08:00 ~ 09:00	0.02796	0.383952	0.898763	0.858249	1.156878	2.236516	2.668008	1.051123	0.428782	1.256023	2.766379	2.10894	2.079665	0	4.102773	0.416589
Nox	09:00 ~ 10:00	0.027823	0.383184	0.8956	0.85188	1.152528	2.217958	2.639788	1.048374	0.427653	1.249829	2.755595	2.100126	2.073132	0	4.1014	0.414272
Nox	10:00 ~ 11:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	11:00 ~ 12:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	12:00 ~ 13:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	13:00 ~ 14:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	14:00 ~ 15:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	15:00 ~ 16:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	16:00 ~ 17:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	17:00 ~ 18:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	18:00 ~ 19:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	19:00 ~ 20:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	20:00 ~ 21:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	21:00 ~ 22:00	0.0277	0.3815	0.8887	0.8458	1.1488	2.1994	2.6143	1.0448	0	1.2457	2.7425	0	2.0617	0	4.1014	0.4138
Nox	22:00 ~ 23:00	0.0277	0.3815	0.8887	0.8458	0	0	0	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
Nox	00:00 ~ 01:00	0.0277	0.3815	0.8887	0.8458	0	0	0	1.0448	0.427	1.2457	2.7425	2.0919	2.0617	0	4.1014	0.4138
PM10-total	00:00 ~ 01:00	0.005	0	0	0.0346	0	0	0	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	01:00 ~ 02:00	0.005	0	0	0.0346	0	0	0	0.0349	0	0	0	0	0	0	0	0.0051
PM10-total	02:00 ~ 03:00	0.005	0	0	0.0346	0	0	0	0.0349	0	0	0	0	0	0	0	0.0051
PM10-total	03:00 ~ 04:00	0.005	0	0	0.0346	0	0	0	0	0	0	0	0	0	0	0	0.0051
PM10-total	04:00 ~ 05:00	0.005	0	0	0.0346	0	0	0	0	0.0144	0	0.0845	0	0	0	0	0.0051
PM10-total	05:00 ~ 06:00	0.005	0	0	0.0346	0	0	0.0966	0	0.0144	0.0682	0.0845	0.0462	0.072	0	0	0.0051
PM10-total	06:00 ~ 07:00	0.005	0	0.027	0.0346	0.0325	0	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	07:00 ~ 08:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	08:00 ~ 09:00	0.005124	0	0.027121	0.034944	0.032635	0.0623	0.097945	0.035176	0.014526	0.068902	0.085399	0.046476	0.072374	0	0.199837	0.00523
PM10-total	09:00 ~ 10:00	0.005059	0	0.027149	0.034768	0.032562	0.0617	0.097238	0.035056	0.014446	0.068481	0.084993	0.046333	0.072238	0	0.1998	0.005122
PM10-total	10:00 ~ 11:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	11:00 ~ 12:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	12:00 ~ 13:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	13:00 ~ 14:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	14:00 ~ 15:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	15:00 ~ 16:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	16:00 ~ 17:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	17:00 ~ 18:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	18:00 ~ 19:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	19:00 ~ 20:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	20:00 ~ 21:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	21:00 ~ 22:00	0.005	0	0.027	0.0346	0.0325	0.0611	0.0966	0.0349	0	0.0682	0.0845	0	0.072	0	0.1998	0.0051
PM10-total	22:00 ~ 23:00	0.005	0	0.027	0.0346	0	0	0	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM10-total	00:00 ~ 01:00	0.005	0	0.027	0.0346	0	0	0	0.0349	0.0144	0.0682	0.0845	0.0462	0.072	0	0.1998	0.0051
PM2.5-total	00:00 ~ 01:00	0.0046	0	0	0.0318	0	0	0	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	01:00 ~ 02:00	0.0046	0	0	0.0318	0	0	0	0.0321	0	0	0	0	0	0	0	0.0044
PM2.5-total	02:00 ~ 03:00	0.0046	0	0	0.0318	0	0	0	0.0321	0	0	0	0	0	0	0	0.0044
PM2.5-total	03:00 ~ 04:00	0.0046	0	0	0.0318	0	0	0	0	0	0	0	0	0	0	0	0.0044
PM2.5-total	04:00 ~ 05:00	0.0046	0	0	0.0318	0	0	0	0	0.0133	0	0.0778	0	0	0	0	0.0044
PM2.5-total	05:00 ~ 06:00	0.0046	0	0	0.0318	0	0	0.0888	0	0.0133	0.0628	0.0778	0.0425	0.0662	0	0	0.0044
PM2.5-total	06:00 ~ 07:00	0.0046	0	0.0248	0.0318	0.0299	0	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	07:00 ~ 08:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	08:00 ~ 09:00	0.004718	0	0.025003	0.032118	0.030025	0.057304	0.090039	0.032353	0.013414	0.063445	0.078625	0.042755	0.066545	0	0.183834	0.004511
PM2.5-total	09:00 ~ 10:00	0.004656	0	0.024939	0.031955	0.029958	0.056752	0.089388	0.032243	0.013342	0.063058	0.078252	0.042623	0.06642	0	0.1838	0.004419
PM2.5-total	10:00 ~ 11:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	11:00 ~ 12:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	12:00 ~ 13:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	13:00 ~ 14:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	14:00 ~ 15:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	15:00 ~ 16:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662	0	0.1838	0.0044
PM2.5-total	16:00 ~ 17:00	0.0046	0	0.0248	0.0318	0.0299	0.0562	0.0888	0.0321	0.0133	0.0628	0.0778	0.0425	0.0662			

Table 3.3: Hourly running emission factor (g/km) for road group with speed limit of Primary Distributor

Pollutant	hour of day	PC	Taxi	LGV3	LGV4	LGV6	HGV7	HGV8	PLB	PV4	PV5	NFB6	NFB7	NFB8	FBSD	FBDD	MC
Nox	00:00 ~ 01:00	0.02122	0.329202	0	0.474416	0	0	0	0.897	0.361596	0.94158	2.04968	1.57775	1.250049	0	3.045692	0.347893
Nox	01:00 ~ 02:00	0.021263	0.329508	0	0.479465	0	0	0	0.897	0	0	0	0	0	0	3.3059	0.348233
Nox	02:00 ~ 03:00	0.02128	0.329712	0	0.47853	0	0	0	0.897	0	0	0	0	0	0	3.3059	0.34853
Nox	03:00 ~ 04:00	0.021306	0.329814	0	0.479839	0	0	0	0	0	0	0	0	0	0	3.3059	0.348743
Nox	04:00 ~ 05:00	0.021229	0.32927	0	0.474977	0	0	0	0	0.352216	0	2.1008	0	0	0	3.3059	0.34819
Nox	05:00 ~ 06:00	0.021341	0.330086	0	0.481148	0	0	1.61619	0	0.360271	0.947839	2.083334	1.5849	1.267567	0	0	0.349253
Nox	06:00 ~ 07:00	0.021212	0.329202	0.574778	0.474603	0.820332	0	1.60821	0.893518	0.360915	0.935516	2.06246	1.568975	1.239344	0	3.053849	0.34768
Nox	07:00 ~ 08:00	0.022183	0.336109	0.61445	0.512148	0.852316	1.47311	1.727445	0.891493	0.364709	0.967684	2.134534	1.62899	1.319071	0	3.08248	0.353471
Nox	08:00 ~ 09:00	0.025393	0.361622	0.690309	0.668817	0.992494	1.865097	2.222203	0.898682	0.395608	1.111698	2.447278	1.879513	1.6907	0	3.313669	0.380926
Nox	09:00 ~ 10:00	0.023899	0.349935	0.640322	0.57818	0.924376	1.652313	1.973755	0.89553	0.379232	1.042024	2.297548	1.754493	1.506289	0	3.207595	0.369832
Nox	10:00 ~ 11:00	0.023861	0.350106	0.645962	0.58186	0.928842	1.66023	1.98414	0.894844	0.380229	1.045897	2.307101	1.763027	1.525303	0	3.215438	0.372242
Nox	11:00 ~ 12:00	0.022122	0.336188	0.601464	0.508846	0.853039	1.479549	1.737615	0.894911	0.366408	0.9741	2.151101	1.642065	1.333368	0	3.09092	0.355453
Nox	12:00 ~ 13:00	0.022711	0.341157	0.610971	0.52735	0.87513	1.532786	1.81597	0.895334	0.36978	0.997154	2.20761	1.683373	1.391049	0	3.145893	0.361812
Nox	13:00 ~ 14:00	0.02212	0.336173	0.598686	0.508587	0.852481	1.480995	1.734395	0.89404	0.366468	0.974667	2.150475	1.642506	1.333784	0	3.091808	0.35533
Nox	14:00 ~ 15:00	0.022126	0.336252	0.600512	0.508918	0.852602	1.481714	1.73775	0.894214	0.365712	0.974246	2.148804	1.642065	1.334181	0	3.094025	0.355458
Nox	15:00 ~ 16:00	0.023866	0.350125	0.640207	0.582205	0.929204	1.661507	1.98682	0.895236	0.380375	1.046543	2.309431	1.763916	1.519541	0	3.216484	0.372365
Nox	16:00 ~ 17:00	0.02208	0.336387	0.594952	0.506693	0.852354	1.491232	1.75057	0.897	0.368376	0.983165	2.168386	1.656739	1.365369	0	3.104692	0.357044
Nox	17:00 ~ 18:00	0.027442	0.380207	0.817587	0.868334	1.186458	2.241662	2.783467	0.897	0.427198	1.288439	2.845497	2.191745	2.214419	0	3.540285	0.412185
Nox	18:00 ~ 19:00	0.023827	0.350258	0.636869	0.58462	0.932704	1.668914	1.999215	0.897	0.381954	1.052353	2.325275	1.774584	1.544318	0	3.226419	0.374137
Nox	19:00 ~ 20:00	0.022714	0.341176	0.608669	0.527235	0.87513	1.529976	1.8153	0.895432	0.369707	0.99823	2.203649	1.682128	1.391913	0	3.146938	0.361837
Nox	20:00 ~ 21:00	0.022123	0.336196	0.582859	0.509278	0.849504	1.475187	1.741275	0.894779	0.366346	0.99086	2.138361	1.643728	1.366767	0	3.094318	0.355368
Nox	21:00 ~ 22:00	0.022131	0.336294	0.582859	0.509249	0.851973	1.474031	1.742325	0.892146	0	0.965012	2.118238	0	1.347026	0	3.095301	0.355453
Nox	22:00 ~ 23:00	0.021229	0.329304	0.555941	0.475164	0	0	0.892299	0.359662	0.933169	2.062886	1.57775	1.250536	0	3.053849	0.347978	
Nox	00:00 ~ 01:00	0.021233	0.329304	0.574778	0.476473	0	0	0.892822	0.360665	0.933169	2.057348	1.582625	1.258321	0	3.057112	0.347935	
PM10-total	00:00 ~ 01:00	0.002678	0	0.023137	0	0	0	0.0274	0.009159	0.04583	0.055851	0.0349	0.053021	0	0.167223	0.002887	
PM10-total	01:00 ~ 02:00	0.002691	0	0.023342	0	0	0	0.0274	0	0	0	0	0	0	0.1759	0.002898	
PM10-total	02:00 ~ 03:00	0.002696	0	0.023304	0	0	0	0.0274	0	0	0	0	0	0	0.1759	0.002907	
PM10-total	03:00 ~ 04:00	0.002704	0	0.023357	0	0	0	0	0	0	0	0	0	0	0.1759	0.002913	
PM10-total	04:00 ~ 05:00	0.002681	0	0.02316	0	0	0	0.008268	0	0.057495	0	0	0	0	0.1759	0.002896	
PM10-total	05:00 ~ 06:00	0.002715	0	0.02341	0	0	0.069502	0	0.009033	0.046185	0.056933	0.035061	0.053431	0	0	0.002929	
PM10-total	06:00 ~ 07:00	0.002676	0	0.019657	0.023144	0.027286	0	0.069288	0.027288	0.009094	0.045486	0.056262	0.034703	0.052777	0	0.167495	0.002881
PM10-total	07:00 ~ 08:00	0.002947	0	0.020949	0.024816	0.027728	0.043445	0.072155	0.027214	0.005974	0.046576	0.057649	0.035962	0.054454	0	0.168495	0.003042
PM10-total	08:00 ~ 09:00	0.004093	0	0.022789	0.029741	0.02993	0.053083	0.085656	0.027498	0.012088	0.057009	0.0705	0.041501	0.063168	0	0.175871	0.003942
PM10-total	09:00 ~ 10:00	0.003466	0	0.021774	0.027462	0.028772	0.048108	0.07884	0.02734	0.010936	0.050922	0.063063	0.038801	0.058866	0	0.172685	0.003537
PM10-total	10:00 ~ 11:00	0.003454	0	0.021941	0.027599	0.028838	0.04831	0.079141	0.027312	0.011018	0.051206	0.063461	0.039003	0.059341	0	0.172942	0.003615
PM10-total	11:00 ~ 12:00	0.002915	0	0.020561	0.024713	0.027735	0.043619	0.072403	0.027333	0.009749	0.046933	0.058153	0.03625	0.054778	0	0.16878	0.003099
PM10-total	12:00 ~ 13:00	0.00307	0	0.020907	0.025561	0.028037	0.045066	0.074271	0.027332	0.010158	0.047628	0.059319	0.037121	0.055986	0	0.170667	0.003276
PM10-total	13:00 ~ 14:00	0.002915	0	0.02048	0.024702	0.027727	0.043659	0.072319	0.027305	0.009763	0.046926	0.058111	0.036259	0.054785	0	0.168807	0.003095
PM10-total	14:00 ~ 15:00	0.002916	0	0.020536	0.024716	0.027729	0.043678	0.072404	0.02731	0.009686	0.046914	0.058037	0.03625	0.054793	0	0.168882	0.003099
PM10-total	15:00 ~ 16:00	0.003455	0	0.021771	0.027612	0.028844	0.048342	0.079218	0.027328	0.01103	0.051254	0.063558	0.039024	0.059197	0	0.172976	0.003619
PM10-total	16:00 ~ 17:00	0.002903	0	0.020365	0.024648	0.027722	0.043934	0.07272	0.0274	0.009925	0.04737	0.058618	0.036576	0.055057	0	0.169239	0.003144
PM10-total	17:00 ~ 18:00	0.00508	0	0.025544	0.035231	0.033167	0.064879	0.0999	0.0274	0.014353	0.069225	0.085803	0.046748	0.074077	0	0.182158	0.005284
PM10-total	18:00 ~ 19:00	0.003442	0	0.021672	0.027702	0.028896	0.048531	0.079577	0.0274	0.01116	0.05168	0.064218	0.039276	0.059816	0	0.173301	0.003677
PM10-total	19:00 ~ 20:00	0.003071	0	0.020839	0.025557	0.028037	0.044995	0.074252	0.027336	0.010152	0.047707	0.059154	0.037092	0.056007	0	0.170702	0.003277
PM10-total	20:00 ~ 21:00	0.002916	0	0.020003	0.024733	0.027684	0.043504	0.072488	0.027318	0.009736	0.047622	0.057747	0.036279	0.055491	0	0.168892	0.003096
PM10-total	21:00 ~ 22:00	0.002918	0	0.020003	0.02473	0.02772	0.043469	0.0725	0.027232	0	0.046168	0.057019	0	0.055072	0	0.168923	0.003101
PM10-total	22:00 ~ 23:00	0.002681	0	0.019061	0.023167	0	0	0.027249	0.008975	0.045353	0.056276	0.0349	0.053032	0	0.167495	0.00289	
PM10-total	00:00 ~ 01:00	0.002682	0	0.019657	0.02322	0	0	0.027266	0.00907	0.045353	0.056098	0.03501	0.053215	0	0.167604	0.002889	
PM2.5-total	00:00 ~ 01:00	0.002457	0	0.021276	0	0	0	0.0252	0.008474	0.04216	0.051398	0.032136	0.048783	0	0.153825	0.002443	
PM2.5-total	01:00 ~ 02:00	0.002469	0	0.021465	0	0	0	0.0252	0	0	0	0	0	0	0.1618	0.002452	
PM2.5-total	02:00 ~ 03:00	0.002474	0	0.02143	0	0	0	0.0252	0	0	0	0	0	0	0.1618	0.00246	
PM2.5-total	03:00 ~ 04:00	0.002481	0	0.021479	0	0	0	0	0	0	0	0	0	0	0.1618	0.002465	
PM2.5-total	04:00 ~ 05:00	0.002459	0	0.021297	0	0	0	0.007662	0	0.05291	0	0	0	0	0.1618	0.002451	
PM2.5-total	05:00 ~ 06:00	0.00249	0	0.021528	0	0	0.063929	0	0.00836	0.042486	0.052393	0.032283	0.049161	0	0	0.002478	
PM2.5-total	06:00 ~ 07:00	0.002454	0	0.018089	0.021283	0.025062	0	0.063731	0.025096	0.008415	0.041844	0.051776	0.031955	0.048552	0	0.154075	0.002438
PM2.5-total	07:00 ~ 08:00	0.002711	0	0.019274	0.022807	0.025501	0.039979	0.066393	0.025028	0.008823	0.04287	0.053018	0.033091	0.050115	0	0.154996	0.0026
PM2.5-total	08:00 ~ 09:00	0.003774	0	0.020956	0.027334	0.027553	0.048844	0.078789	0.02529	0.011139	0.052488	0.064865	0.038177	0.058114	0	0.161781	0.003399
PM2.5-total	09:00 ~ 10:00	0.003196	0	0.02003	0.025248	0.026507	0.044292	0.072597	0.025145	0.010069	0.046859	0.058004	0.035701	0.054177	0	0.158848	0.003028
PM2.5-total	10:00 ~ 11:00	0.003184	0	0.020182	0.025376	0.02657	0.044478	0.072876	0.025119	0.010147	0.047118	0.058373	0.035889	0.054612	0	0.159084	0.003086
PM2.5-total	11:00 ~ 12:00	0.002694</															

Table 4: Summary of trip and VKT per day for vehicle class with starting emission in Year 2026

Model Year 2026	PC	Taxi	LGV3	LGV4	PLB	PV4	PV5	MC
Trip per day (HK total)	1059695	73794	4116	218922	17557	2923	8481	379446
VKT per day (HK total)	19875414	7772840	73046	3565618	1314480	100388	231263	1192615
VKT for rural & local road per day	2583804	1010469.2	9495.98	463530.34	170882.4	13050.44	30064.19	155039.95
Trip/VKT for rural & local road	0.41013	0.07302944	0.433446574	0.472292709	0.102743173	0.223977123	0.282096408	2.44740791

Note: VKT for rural & local road per day = 13% of VKT per day (HK total) based on Liantang EIAR

Table 5: Hourly composite emission rate for each road link

Road No.	Road Group	Speed Limit	Hour	Total Vehicle	Nox-running (g/km)	Nox-starting (g/km)	Nox emission (g/km)	Nox emission (g/mile)	RSP-running (g/km)	RSP-starting (g/km)	RSP emission (g/km)	RSP emission (g/mile)	FSP-running (g/km)	FSP-starting (g/km)	FSP emission (g/km)	FSP emission (g/mile)
L001	LD	50	00:00 ~ 01:00	100	0.411070217	0.075022032	0.486092249	0.782287699	0.007178408	0.001020002	0.008198481	0.01319403	0.00605398	0.000901468	0.007506865	0.012081099
L002	LD	50	00:00 ~ 01:00	100	0.538399992	0.097033462	0.635433454	1.022628475	0.009192115	0.00038875	0.009580866	0.01541887	0.008455007	0.000345448	0.008000455	0.014162924
L003	LD	50	00:00 ~ 01:00	50	0.197160974	0.043680705	0.24084168	0.387596149	0.003411672	0.00101056	0.004422331	0.007116874	0.003159192	0.000941658	0.00410085	0.006599692
L004	DD	50	00:00 ~ 01:00	400	0.6619703	0.081184477	0.743154777	1.195988709	0.0204479	0.001185131	0.021633031	0.034814901	0.0187986	0.001031825	0.019830425	0.031913897
L005	DD	50	00:00 ~ 01:00	350	0.8311716	0.08151573	0.91268733	1.468824228	0.0274102	0.00059341	0.028269541	0.04595303	0.0252085	0.000749951	0.025958451	0.041775974
L006	LD	50	00:00 ~ 01:00	100	0.620394934	0.103236691	0.723631625	1.16456932	0.01253096	0.000586371	0.01311733	0.021110244	0.011523324	0.000513686	0.01203701	0.019371642
L007	LD	50	00:00 ~ 01:00	200	0.521677022	0.088681092	0.610358114	0.982273727	0.010070363	0.00708296	0.010778659	0.017346527	0.009264234	0.000627041	0.009891276	0.015918426
L008	LD	50	00:00 ~ 01:00	100	0.543582591	0.065736206	0.609318798	0.980601114	0.010771112	0.000741409	0.011512521	0.018527561	0.009912271	0.000657265	0.010509536	0.017009977
L009	LD	50	00:00 ~ 01:00	100	0.368187595	0.077361387	0.445548982	0.717039799	0.009366942	0.0016693	0.011036242	0.017761066	0.008618155	0.001474303	0.010092458	0.016242196
L010	LD	50	00:00 ~ 01:00	50	0.281077569	0.053606787	0.334684356	0.538620922	0.002043321	0.000613554	0.002656876	0.004275816	0.001892106	0.000571721	0.002463827	0.003965135
L011	LD	50	00:00 ~ 01:00	50	0.56120822	0.064833835	0.626042056	1.007514522	0.012126904	0.000777501	0.012904404	0.020767574	0.011600019	0.000690895	0.011850915	0.019072151
L012	LD	50	00:00 ~ 01:00	50	0.35878352	0.034078883	0.392862403	0.63224918	0.011310051	0.001118834	0.012428885	0.020002301	0.010428023	0.00104255	0.011470573	0.018460052
L013	LD	50	00:00 ~ 01:00	100	0.356481003	0.061871299	0.418352302	0.673271094	0.005378463	0.000924234	0.006302697	0.01143182	0.004953479	0.000827899	0.005781378	0.009304203
L014	DD	50	00:00 ~ 01:00	450	0.6889929	0.090826764	0.779819664	1.254994979	0.0215519	0.000934244	0.022486144	0.036187851	0.0198172	0.000820157	0.020637357	0.033212525
L015	DD	50	00:00 ~ 01:00	150	1.2002799	0.061247124	1.261527024	2.030275901	0.0463796	0.000557808	0.046937408	0.075538248	0.0426607	0.000487617	0.043148317	0.069440312
L016	LD	50	00:00 ~ 01:00	50	0.331699773	0.068616175	0.400312594	0.644244468	0.002388701	0.000758989	0.00314769	0.005065704	0.002198339	0.000674672	0.002873011	0.004623651
L017	LD	50	00:00 ~ 01:00	50	0.56120822	0.064833835	0.626042056	1.007514522	0.012126904	0.000777501	0.012904404	0.020767574	0.011600019	0.000690895	0.011850915	0.019072151
L018	LD	50	00:00 ~ 01:00	100	0.368187595	0.077361387	0.445548982	0.717039799	0.009366942	0.0016693	0.011036242	0.017761066	0.008618155	0.001474303	0.010092458	0.016242196
L019	LD	50	00:00 ~ 01:00	100	0.519638346	0.105998251	0.629234527	1.012652407	0.013859484	0.023485318	0.016344812	0.023630436	0.012728776	0.002138722	0.014867498	0.023926859
L020	LD	50	00:00 ~ 01:00	150	0.431309292	0.083534614	0.514843905	0.826558891	0.007998484	0.01422949	0.009421434	0.01516229	0.007350274	0.001240244	0.008590517	0.013825063
L021	DD	50	00:00 ~ 01:00	400	0.7241212	0.091966331	0.816087531	1.313362308	0.0228679	0.000910682	0.032778582	0.038267824	0.0210277	0.000796699	0.021823769	0.035121864
L022	DD	50	00:00 ~ 01:00	200	1.077454	0.17075458	1.187051628	1.896052978	0.0405453	0.000527388	0.041067638	0.07375627	0.00467077	0.000765277	0.060762687	0.076732847
L023	LD	50	00:00 ~ 01:00	150	0.553847741	0.067841171	0.621688911	1.000508839	0.01181388	0.000920334	0.012734214	0.020493679	0.010870993	0.000813635	0.011684628	0.018804539
L024	LD	50	00:00 ~ 01:00	100	0.226862492	0.069052203	0.295914695	0.476227355	0.005202191	0.00164288	0.006845072	0.011016048	0.00479166	0.000625766	0.010070874	0.018040708
L025	DD	50	00:00 ~ 01:00	50	0.72756	0.030954745	0.758514745	1.22070812	0.02682	0.001066096	0.027886096	0.04487821	0.02468	0.000990329	0.025670329	0.041312288
L026	DD	50	00:00 ~ 01:00	50	0.7221301	0.014665514	0.736795614	1.185754653	0.0247743	0.001096912	0.025871212	0.041635576	0.022780925	0.001018365	0.023809265	0.038317203
L027	DD	50	00:00 ~ 01:00	150	0.4846698	0.097922132	0.582591932	0.937584999	0.0105998	0.000922856	0.011522656	0.018543872	0.0097442	0.000815918	0.010560118	0.016994821
L028	DD	50	00:00 ~ 01:00	100	0.5194496	0.104654271	0.624103871	1.004395323	0.011816	0.000769662	0.012585662	0.020254609	0.0108646	0.000683728	0.011548328	0.018585187
L029	LD	50	00:00 ~ 01:00	50	0.119641609	0.034656995	0.154298604	0.248318915	0.004675708	0.001371474	0.006047182	0.009731972	0.004329684	0.001277965	0.005607649	0.009024563
L030	DD	50	00:00 ~ 01:00	500	0.6451489	0.097268167	0.742417067	1.194801482	0.019835	0.001089075	0.020924075	0.03367395	0.018235	0.000950306	0.019185306	0.030875618
L031	DD	50	00:00 ~ 01:00	400	0.7482613	0.095981784	0.844243084	1.554004164	0.0017608	0.000847389	0.002608189	0.004197462	0.0016106	0.000741162	0.002351762	0.003784784
L032	DD	50	00:00 ~ 01:00	150	1.1025215	0.130265625	1.232787125	1.983973632	0.0424776	0.001314806	0.043612406	0.07018719	0.0390625	0.000982352	0.040044852	0.06445782
L033	LD	50	00:00 ~ 01:00	100	0.232435646	0.124140942	0.356576588	0.573852967	0.005898784	0.003259587	0.009158371	0.014738933	0.005394127	0.002813125	0.008207252	0.01320826
L034	LD	50	00:00 ~ 01:00	100	0.21626854	0.094835396	0.311103936	0.500672008	0.005109248	0.002468571	0.012195287	0.004688601	0.002155959	0.000684599	0.011015223	0.01320826
L035	LD	50	00:00 ~ 01:00	50	0.0300804	0.023828543	0.053908943	0.086757818	0.0061361	0.001804571	0.007940671	0.01277924	0.005682	0.001681532	0.007363532	0.011850427
L036	LD	50	00:00 ~ 01:00	50	0.0300804	0.023828543	0.053908943	0.086757818	0.0061361	0.001804571	0.007940671	0.01277924	0.005682	0.001681532	0.007363532	0.011850427
L037	LD	50	00:00 ~ 01:00	100	0.217382428	0.100736795	0.318119223	0.51196199	0.005170872	0.0026301	0.007800971	0.01255441	0.004740828	0.002290566	0.007031394	0.011315904
L038	LD	50	00:00 ~ 01:00	100	0.214926471	0.083483785	0.48024356	0.804841628	0.002127467	0.006696905	0.011215643	0.004447403	0.001869929	0.000631732	0.010166735	0.01315904
L039	LD	50	00:00 ~ 01:00	50	0.1860344	0.041875963	0.222479403	0.358045003	0.00368166	0.001082743	0.004764403	0.007667544	0.0034092	0.001008919	0.004418119	0.007110256
L040	LD	50	00:00 ~ 01:00	50	0.167056366	0.040071221	0.207127588	0.333338712	0.003902556	0.001154926	0.005057485	0.008139213	0.003613752	0.001076181	0.004689933	0.007547696
L041	LD	50	00:00 ~ 01:00	50	0.2312468	0.076978046	0.308224846	0.496038574	0.005643555	0.001916052	0.007559607	0.012165978	0.005193555	0.001687709	0.006881264	0.011074293
L042	LD	50	00:00 ~ 01:00	50	0.205676373	0.040167806	0.245844179	0.39564687	0.005089699	0.001052319	0.006142018	0.009884955	0.004704009	0.000979067	0.005683076	0.009146001
L043	LD	50	00:00 ~ 01:00	100	0.5808946	0.075080448	0.656065407	1.055832302	0.01199375	0.01091303	0.013090678	0.021067352	0.011033584	0.000951999	0.011987383	0.019291775
L044	LD	50	00:00 ~ 01:00	150	0.470903672	0.102978564	0.573882326	0.923571638	0.011341892	0.01160988	0.020121385	0.010431331	0.01027709	0.01145904	0.018441492	0.023926859
L045	LD	50	00:00 ~ 01:00	50	0.262069921	0.065504465	0.327574385	0.527178561	0.005923087	0.001623418	0.007546505	0.012144892	0.005453451	0.001448346	0.006091798	0.011017339
L046	LD	50	00:00 ~ 01:00	100	0.204920515	0.042375441	0.247295956	0.397983274	0.004218137	0.00113394	0.00523153	0.008419311	0.003901338	0.000943547	0.004844885	0.007797068
L047	DD	50	00:00 ~ 01:00	450	0.6591564	0.096087157	0.755243557	1.215443666	0.0206983	0.001067944	0.021766244	0.035029287	0.0190296	0.00093425	0.011996385	0.032128623
L048	DD	50	00:00 ~ 01:00	150	1.2265421	0.135016848	1.361558948	2.191211277	0.0491034	0.000990084	0.080617448	0.0501595	0.000858353	0.000617853	0.074058372	0.074058372
L049	DD	50	00:00 ~ 01:00	250	0.3786968	0.072390917	0.451087717	0.725953507	0.005575	0.000950227	0.006525227	0.010501309	0.0051213	0.000839789	0.005961089	0.009594942
L050	DD	50	00:00 ~ 01:00	200	0.4928081	0.094390703	0.587198803	0.945002522	0.0137628	0.01258109	0.0213749	0.0126493	0.0107688	0.013756988	0.022139671	0.022139671
L051	DD	50	00:00 ~ 01:00	250	0.3849064	0.074407124	0.459313524	0.739191627	0.0054428	0.000920787	0.006363587	0.010241176	0.0049995</			

Table 2: Hourly composite emission rate for each road link

L082	LD	50	00:00:00 ~ 01:00	50	0.203934735	0.139005453	0.342490188	0.551907363	0.004298555	0.003558931	0.007857486	0.012645367	0.003919995	0.003061746	0.006981741	0.011235996
L083	LD	50	00:00:00 ~ 01:00	50	0.241172665	0.101888905	0.34306157	0.552102707	0.005682694	0.002559334	0.006242028	0.013264226	0.005212152	0.002224429	0.007436401	0.011967698
L084	LD	50	00:00:00 ~ 01:00	50	0.0300804	0.023828543	0.053908943	0.086757818	0.0061361	0.001804571	0.001797294	0.005682	0.001681532	0.007363532	0.010186532	0.01585858
L085	LD	50	00:00:00 ~ 01:00	50	0.0300804	0.023828543	0.053908943	0.086757818	0.0061361	0.001804571	0.001797294	0.005682	0.001681532	0.007363532	0.010186532	0.01585858
L086	LD	50	00:00:00 ~ 01:00	50	0.191975099	0.133685867	0.356069666	0.540192619	0.006764653	0.004068114	0.010832767	0.017433605	0.006183426	0.003502893	0.009686318	0.01558858
L087	LD	250	00:00:00 ~ 01:00	250	0.440553224	0.108181126	0.548734349	0.883100137	0.009838651	0.001531871	0.011370521	0.018299035	0.00904194	0.001337383	0.010379322	0.016703585
L088	LD	50	00:00:00 ~ 01:00	50	0.259869071	0.133519254	0.393388325	0.633095567	0.003273454	0.002983233	0.006256687	0.002977607	0.002557117	0.005534724	0.008907253	0.016703585
L089	LD	50	00:00:00 ~ 01:00	100	0.340638501	0.070603057	0.411318858	0.661951891	0.004206969	0.001036631	0.0052436	0.008438735	0.003869282	0.000917338	0.00478662	0.0077033
L090	LD	50	00:00:00 ~ 01:00	200	0.591175147	0.140407762	0.731582908	1.177365637	0.015393088	0.000677348	0.016166436	0.026017292	0.014155605	0.000687348	0.014843213	0.023887733
L091	LD	50	00:00:00 ~ 01:00	50	0.34899566	0.06372263	0.41271829	0.664204052	0.009115372	0.00159016	0.010705532	0.017228841	0.00839066	0.001416605	0.009807265	0.015783224
L092	LD	50	00:00:00 ~ 01:00	150	0.658929644	0.158745885	0.817675529	1.315917936	0.017727318	0.000704062	0.018431381	0.029662358	0.01629862	0.000623399	0.016922019	0.027233282
L093	LD	50	00:00:00 ~ 01:00	50	0.363800199	0.08452623	0.448326429	0.721509654	0.006824065	0.001657744	0.00848181	0.013650115	0.006270618	0.001448516	0.007719134	0.012422721
L094	LD	50	00:00:00 ~ 01:00	150	0.609308151	0.175048416	0.784356567	1.262296397	0.017359042	0.000687394	0.018046436	0.029042851	0.015963183	0.000618564	0.016581747	0.026685669
L095	LD	50	00:00:00 ~ 01:00	200	0.373480652	0.087043554	0.460524206	0.741140026	0.007217174	0.000811883	0.008029057	0.012921482	0.006646006	0.000740431	0.007386437	0.011887288
L096	LD	50	00:00:00 ~ 01:00	150	0.69034793	0.163885163	0.854233094	1.374751487	0.020166014	0.000560873	0.020726887	0.033356608	0.018547404	0.000511184	0.019058587	0.030671747
L097	LD	50	00:00:00 ~ 01:00	150	0.631565218	0.162966889	0.794532088	1.27867227	0.018730623	0.000991261	0.0197271884	0.031739217	0.017224882	0.000880661	0.018105149	0.02913734
L098	LD	50	00:00:00 ~ 01:00	50	0.211785584	0.117469744	0.329255328	0.52988377	0.007610442	0.003427666	0.011038108	0.017764068	0.006972282	0.002968241	0.009940523	0.015997681
L099	LD	250	00:00:00 ~ 01:00	250	0.433862721	0.097879522	0.531742243	0.855754061	0.010100052	0.001272077	0.011372129	0.018301622	0.009290795	0.001127569	0.010418364	0.016766691
L100	LD	50	00:00:00 ~ 01:00	50	0.2182342	0.046387819	0.264622019	0.425866799	0.00306805	0.000902286	0.003970396	0.00638962	0.000840766	0.000361866	0.005925214	0.02367333
L101	LD	50	00:00:00 ~ 01:00	50	0.0470853	0.077755969	0.502464489	0.80836201	0.009905099	0.001748852	0.011729451	0.018876674	0.009156418	0.001551297	0.010707715	0.017232544
L102	LD	50	00:00:00 ~ 01:00	50	0.207270068	0.086476623	0.293746691	0.47272883	0.003280642	0.001328582	0.013073605	0.00275936	0.002089557	0.000736549	0.011853583	0.01853583
L103	LD	50	00:00:00 ~ 01:00	100	0.297344626	0.059115894	0.356460519	0.573666172	0.004865248	0.001083812	0.00594906	0.00977348	0.005461891	0.000979021	0.008790201	0.01585858
L104	PD	400	00:00:00 ~ 01:00	400	0.558467447	0.115064771	0.673532218	1.08394234	0.018046968	0.001086675	0.019133642	0.030792536	0.016590804	0.000947943	0.017538746	0.028258086
L105	PD	300	00:00:00 ~ 01:00	300	0.645322419	0.155016966	0.80039384	1.288018185	0.021014389	0.001435967	0.022450356	0.036130255	0.019314633	0.002054079	0.03057113	0.03057113
L106	LD	50	00:00:00 ~ 01:00	50	0.434010902	0.157670391	0.591681293	0.952216372	0.013069418	0.003166524	0.016235942	0.026129151	0.01198404	0.00277159	0.014711204	0.02367333
L107	LD	50	00:00:00 ~ 01:00	200	0.406294865	0.066184772	0.472479457	0.76038009	0.006890961	0.000979016	0.007869977	0.00634256	0.000871674	0.007214234	0.011610156	0.01610156
L108	LD	50	00:00:00 ~ 01:00	150	0.475754552	0.082966827	0.558721379	0.899172663	0.007495346	0.000680946	0.008176292	0.015158434	0.000849951	0.000601608	0.007496559	0.012064512
L109	LD	50	00:00:00 ~ 01:00	100	0.399859364	0.067295227	0.46715459	0.751810568	0.006886612	0.001092185	0.007978797	0.012840597	0.006337593	0.000968729	0.007306322	0.011758357
L110	LD	50	00:00:00 ~ 01:00	100	0.32241776	0.132676628	0.455094388	0.732401603	0.005236544	0.002915301	0.008141845	0.01302996	0.00477108	0.002492689	0.010689794	0.01689794
L111	LD	50	00:00:00 ~ 01:00	100	0.381472953	0.161627883	0.443100836	0.713099899	0.007666737	0.01226344	0.008893081	0.014311991	0.007057219	0.001093604	0.008150822	0.013117444
L112	LD	50	00:00:00 ~ 01:00	200	0.490577649	0.091637538	0.582215186	0.936982188	0.011494411	0.000870915	0.012620327	0.020301037	0.010813061	0.000787004	0.011601032	0.018668508
L113	LD	50	00:00:00 ~ 01:00	200	0.452645559	0.10145124	0.554096799	0.891730143	0.01124541	0.001356652	0.020281003	0.010343371	0.0011997	0.011543071	0.018576726	0.018576726
L114	LD	50	00:00:00 ~ 01:00	100	0.337184845	0.075238801	0.412423646	0.66372987	0.006624656	0.001594283	0.008218939	0.013227067	0.006091074	0.001405152	0.007496226	0.012063977
L115	LD	50	00:00:00 ~ 01:00	50	0.210836537	0.027324888	0.238161425	0.383282707	0.009719625	0.001319128	0.01138753	0.017765107	0.008961871	0.001223927	0.010191799	0.016402077
L116	LD	50	00:00:00 ~ 01:00	50	0.111649571	0.02221697	0.133866541	0.215436779	0.008881732	0.001671541	0.010553272	0.016883083	0.008204281	0.001554566	0.009758754	0.015705153
L117	LD	50	00:00:00 ~ 01:00	50	0.239458808	0.046768466	0.324227274	0.521791922	0.007349524	0.002350218	0.00969742	0.015610183	0.006751923	0.002059705	0.008811628	0.014180906
L118	PD	450	00:00:00 ~ 01:00	450	0.525378167	0.105680002	0.63105817	1.015587155	0.016667273	0.001329288	0.017996561	0.028962586	0.015319103	0.001152916	0.016472019	0.026509509
L119	PD	300	00:00:00 ~ 01:00	300	0.667275786	0.161863393	0.829139179	1.334366846	0.022397848	0.001743592	0.038851785	0.020582888	0.001480945	0.002063833	0.035580209	0.035580209
L120	LD	50	00:00:00 ~ 01:00	50	0.403802044	0.124256564	0.528058608	0.849825841	0.01105839	0.003731612	0.012432002	0.020007318	0.010171181	0.001214821	0.011386002	0.018329449
L121	LD	50	00:00:00 ~ 01:00	100	0.400919083	0.072365211	0.473284294	0.761675346	0.006371447	0.001017168	0.017388616	0.00586147	0.000899579	0.006761049	0.010880826	0.010880826
L122	LD	50	00:00:00 ~ 01:00	200	0.445283124	0.07747574	0.522758864	0.841296749	0.006983655	0.000736548	0.007720113	0.012424827	0.006425802	0.000653505	0.007079307	0.011393011
L123	LD	50	00:00:00 ~ 01:00	50	0.450067962	0.104030119	0.554098082	0.891732206	0.01250136	0.001119903	0.021999741	0.01154717	0.01010978	0.012558148	0.020210329	0.020210329
L124	LD	50	00:00:00 ~ 01:00	50	0.18060344	0.041875963	0.222479403	0.358045003	0.00368166	0.001082743	0.004764403	0.0034092	0.001008919	0.004418119	0.007110256	0.007110256
L125	LD	50	00:00:00 ~ 01:00	50	0.18060344	0.041875963	0.222479403	0.358045003	0.00368166	0.001082743	0.004764403	0.0034092	0.001008919	0.004418119	0.007110256	0.007110256
L126	PD	300	00:00:00 ~ 01:00	300	0.665653209	0.157778732	0.823431941	1.32518196	0.02352872	0.001641921	0.024000193	0.03862447	0.020547957	0.001366811	0.021944768	0.035316593
L127	PD	50	00:00:00 ~ 01:00	50	0.1847842	0.038363064	0.359119818	0.59477558	0.005880302	0.001124502	0.0040378	0.005880302	0.00463405	0.001046328	0.005420128	0.008722829
L128	LD	50	00:00:00 ~ 01:00	100	0.457255646	0.081033594	0.538289241	0.866290406	0.009388279	0.000875102	0.010263381	0.01651727	0.008639933	0.000782731	0.009422664	0.01516427
L129	LD	50	00:00:00 ~ 01:00	150	0.458476585	0.080804729	0.539281314	0.867886989	0.006915019	0.000680002	0.00759502	0.01222297	0.006359904	0.000600978	0.009608083	0.012024227
L130	LD	50	00:00:00 ~ 01:00	150	0.497309122	0.086852163	0.583891284	0.93967796	0.007193972	0.000668327	0.007862299	0.012653112	0.006612545	0.000579244	0.007191789	0.01574034
L131	LD	50	00:00:00 ~ 01:00	100	0.457255646	0.081033594	0.538289241	0.866290406	0.009388279	0.000875102	0.010263381	0.01651727	0.008639933	0.000782731	0.009422664	0.01516427
L132	PD	500	00:00:00 ~ 01:00	500	0.541544155	0.147600007	0.689144162	1.10906								

Table 5: Hourly composite emission rate for each road link

L164	LD	50	00:00 ~ 01:00	50	0.17119575	0.040973592	0.212169342	0.341452609	0.003835063	0.001118834	0.004953897	0.007972504	0.00355125	0.00104255	0.0045938	0.00739286
L165	LD	50	00:00 ~ 01:00	50	0.18963527	0.21891792	0.40855319	0.657500991	0.00552906	0.00592861	0.01145767	0.018439287	0.00498989	0.005047142	0.010046132	0.01616763
L166	LD	50	00:00 ~ 01:00	250	0.387258862	0.092347865	0.479606726	0.771850289	0.004865944	0.00100965	0.005875594	0.009455829	0.004471442	0.00088294	0.005354382	0.008617021
L167	LD	50	00:00 ~ 01:00	200	0.365878646	0.087291416	0.453170062	0.729304707	0.005210753	0.001172275	0.010272462	0.004789309	0.001027963	0.005817272	0.009361968	
L168	DD	50	00:00 ~ 01:00	100	0.2936823	0.073174618	0.366856918	0.590397513	0.00419599	0.001316641	0.005512541	0.008871552	0.0038474	0.001162486	0.005009886	0.00806621
L169	DD	50	00:00 ~ 01:00	100	0.3719863	0.09924904	0.47123534	0.758377883	0.0065881	0.001836436	0.013557944	0.0060382	0.001583208	0.007621408	0.012265437	
L170	LD	50	00:00 ~ 01:00	50	0.345585297	0.074528175	0.420113472	0.676105415	0.005220845	0.001262504	0.006483348	0.010433912	0.004800949	0.001111204	0.005912989	0.00951601
L171	LD	50	00:00 ~ 01:00	50	0.482225844	0.155755118	0.637980962	1.026728281	0.012729985	0.00125967	0.012729985	0.002514112	0.011697412	0.001110151	0.012807563	0.020611724
L172	LD	50	00:00 ~ 01:00	50	0.2182342	0.046387819	0.264622019	0.425866799	0.00306805	0.000902286	0.003970336	0.00638962	0.002841	0.000840766	0.003681766	0.00595214
L173	LD	50	00:00 ~ 01:00	200	0.428917989	0.116669587	0.878079361	0.545614575	0.007426458	0.01269227	0.013994315	0.006819618	0.001102772	0.007922391	0.01274982	
L174	LD	50	00:00 ~ 01:00	200	0.415576086	0.089966332	0.501542419	0.807152276	0.007872482	0.001391861	0.009264343	0.014909478	0.007236043	0.0012166	0.008452643	0.013603176
L175	LD	50	00:00 ~ 01:00	150	0.411960866	0.072166905	0.484127777	0.779126186	0.007040007	0.001098947	0.013098345	0.006477058	0.000970399	0.007447457	0.01198549	
L176	LD	50	00:00 ~ 01:00	100	0.428204493	0.116021806	0.544226299	0.875845152	0.009749695	0.002089611	0.011839305	0.019053468	0.008947577	0.010750414	0.017301071	
L177	LD	50	00:00 ~ 01:00	50	0.27392862	0.040983587	0.678912207	1.092600571	0.00614924	0.010413564	0.016562804	0.026655183	0.00545238	0.008749059	0.014201439	0.022854944
L178	LD	50	00:00 ~ 01:00	50	0.396770798	0.536547124	0.933317922	1.502025865	0.005595629	0.013102771	0.0186984	0.030092083	0.004851889	0.010936748	0.015788637	0.025409286
L179	LD	50	00:00 ~ 01:00	100	0.420063796	0.117125624	0.53718942	0.864520422	0.009228156	0.002070148	0.011298304	0.018182813	0.008469485	0.001785077	0.010254562	0.016503076
L180	PD	50	00:00 ~ 01:00	350	0.52761162	0.115591003	0.643202623	1.035131709	0.018058086	0.001679599	0.019737685	0.013764646	0.016594177	0.001450661	0.018048438	0.02904028
L181	PD	70	00:00 ~ 01:00	750	0.320752474	0	0.320752474	0.516195986	0.004854017	0	0.004854017	0.007811764	0.004446499	0	0.004446499	0.007155929
L182	PD	70	00:00 ~ 01:00	1050	0.303045025	0	0.303045025	0.487702481	0.00418522	0	0.00418522	0.006735443	0.003834397	0	0.003834397	0.006170849
L183	PD	70	00:00 ~ 01:00	250	0.620966202	0.110527211	0.731493413	1.17722161	0.023711828	0.001592641	0.025304469	0.04072494	0.021794959	0.001374974	0.023169933	0.0372883
L184	PD	50	00:00 ~ 01:00	300	0.600842809	0.121213785	0.722085694	1.162034559	0.020135485	0.001603799	0.021739194	0.034985755	0.018505218	0.001370933	0.019876152	0.031987486
L185	PD	50	00:00 ~ 01:00	250	0.36434943	0.091186042	0.454808342	0.731735262	0.0058624	0.001308743	0.007170643	0.011540093	0.0053793	0.001141879	0.006521179	0.010494795
L186	DD	50	00:00 ~ 01:00	250	0.3697437	0.085585649	0.455329349	0.732779735	0.00466246	0.00087645	0.005492245	0.00838889	0.0042276	0.00078198	0.00500958	0.008062117
L187	LD	50	00:00 ~ 01:00	50	0.282870803	0.087018569	0.369889372	0.595277761	0.007184572	0.005154069	0.00873864	0.014063443	0.006615233	0.001382974	0.00799607	0.012871835
L188	LD	50	00:00 ~ 01:00	50	0.364378915	0.058850348	0.423229263	0.681119782	0.010743012	0.000980136	0.017232343	0.018866684	0.009901424	0.000911806	0.01081323	0.017402164
L189	LD	50	00:00 ~ 01:00	50	0.145983229	0.096378094	0.242361384	0.390041869	0.00491107	0.002878489	0.007789559	0.012536049	0.004057083	0.002523147	0.007030477	0.013144428
L190	LD	50	00:00 ~ 01:00	50	0.169796022	0.09987578	0.269783599	0.434173538	0.004462938	0.002734123	0.007197061	0.011582518	0.004095988	0.002388624	0.004848413	0.010435947
L191	LD	50	00:00 ~ 01:00	50	0.189809276	0.037460693	0.22769969	0.365754652	0.005495744	0.001160593	0.006656337	0.01071231	0.005079272	0.001079959	0.006159231	0.009912297
L192	LD	50	00:00 ~ 01:00	50	0.19151636	0.042778334	0.234294695	0.377059824	0.003503713	0.001046651	0.005503664	0.007323083	0.003244422	0.000975289	0.004219711	0.006790949
L193	LD	50	00:00 ~ 01:00	50	0.233109699	0.039764913	0.272874612	0.439148028	0.005919096	0.001019061	0.006938157	0.011165854	0.005465681	0.000947326	0.006413007	0.0103020709
L194	LD	50	00:00 ~ 01:00	50	0.173083773	0.035655951	0.208739724	0.335933187	0.005572052	0.001232776	0.006804828	0.010951281	0.005150909	0.00114722	0.006298129	0.010135831
L195	LD	50	00:00 ~ 01:00	100	0.219614537	0.100736795	0.320351331	0.515554211	0.005210175	0.0026301	0.007840275	0.012617668	0.0044775367	0.002290566	0.007065933	0.013171489
L196	LD	50	00:00 ~ 01:00	100	0.210173824	0.066432221	0.276606045	0.445158173	0.004378486	0.001641464	0.006019949	0.009688145	0.00403491	0.001465162	0.005500072	0.008851486
L001	LD	50	01:00 ~ 02:00	50	0.290345429	0.065006691	0.355325212	0.57188238	0.002639645	0.000903354	0.003542999	0.005701891	0.002423472	0.000809194	0.003232668	0.005025249
L002	LD	50	01:00 ~ 02:00	100	0.360415945	0.071822766	0.432238711	0.695619047	0.002184191	0.000581365	0.002765556	0.00450572	0.002003875	0.000508408	0.002512283	0.004043117
L003	LD	50	01:00 ~ 02:00	50	0.2089216	0.046387819	0.255309419	0.41087966	0.00265855	0.000902286	0.003560836	0.005730595	0.002451	0.000840766	0.003291766	0.005297521
L004	DD	50	01:00 ~ 02:00	200	0.2983474	0.080912297	0.610357801	0.0027123	0.001236411	0.003948711	0.006354818	0.0024806	0.001079396	0.003559996	0.005729944	
L005	DD	50	01:00 ~ 02:00	150	0.3137603	0.076350409	0.390110709	0.627820768	0.0022576	0.000963275	0.003220875	0.005183482	0.0020662	0.000843568	0.002909768	0.004682806
L006	LD	50	01:00 ~ 02:00	50	0.350351671	0.070018024	0.420369694	0.676517764	0.002560517	0.00063548	0.005712524	0.002349696	0.000575669	0.002953565	0.004707906	
L007	LD	50	01:00 ~ 02:00	100	0.325623916	0.067762096	0.393386012	0.633091844	0.002369424	0.000743777	0.003383201	0.00544472	0.002422129	0.000659746	0.003081875	0.004959784
L008	LD	50	01:00 ~ 02:00	50	0.322955772	0.079113708	0.40206948	0.647066497	0.002601982	0.00108488	0.003686863	0.005933415	0.002384963	0.000945776	0.003330738	0.00536029
L009	LD	50	01:00 ~ 02:00	50	0.239948475	0.08008052	0.320036527	0.515047585	0.00439054	0.001841035	0.006231575	0.010028723	0.004023259	0.001618558	0.005641817	0.009079602
L010	LD	50	01:00 ~ 02:00	50	0.2089216	0.046387819	0.255309419	0.41087966	0.00265855	0.000902286	0.003560836	0.005730595	0.002451	0.000840766	0.003291766	0.005297521
L011	LD	50	01:00 ~ 02:00	50	0.31262484	0.078211337	0.390836177	0.628988293	0.002999928	0.001120972	0.0041209	0.006631929	0.002749737	0.000979406	0.003729143	0.00600146
L012	LD	50	01:00 ~ 02:00	50	0.148615495	0.03916885	0.187784335	0.302208859	0.003546506	0.001191017	0.004737523	0.007624285	0.003269634	0.001109811	0.004379445	0.007048016
L013	LD	50	01:00 ~ 02:00	50	0.287959013	0.065006691	0.352965704	0.568041826	0.002920614	0.000903354	0.003823969	0.006154066	0.002680403	0.000809194	0.003489597	0.005615948
L014	DD	50	01:00 ~ 02:00	250	0.315126	0.084737248	0.399863248	0.643515919	0.0033487	0.001141493	0.004490193	0.007226247	0.0030685	0.000998829	0.004067329	0.00645716
L015	DD	50	01:00 ~ 02:00	100	0.3347373	0.078127058	0.412864358	0.664439125	0.0014917	0.000776152	0.002267852	0.003649745	0.0013628	0.000674757	0.002037551	0.003279121
L016	LD	50	01:00 ~ 02:00	50	0.2992002	0.057216271	0.356416471	0.573595283	0.001329275	0.000469189	0.001798464	0.002894339	0.0012255	0.000437198	0.001662698	0.002675847
L017	LD	50	01:00 ~ 02:00	50	0.31262484	0.078211337	0.390836177	0.628988293	0.002999928	0.001120972	0.0041209	0.006631929	0.002749737	0.000979406	0.003729143	0.00600146
L018	LD	50	01:00 ~ 02:00	50	0.239948475	0.08008052	0.320036527	0.515047585	0.00439054	0.001841035	0.006231575	0.010028723	0.004023259	0.001618558	0.005641817	0.009079602
L019	LD	50	01:00 ~ 02:00	50	0.256311799	0.115995922	0.37230772	0.599169707	0.00599412	0.00274086	0.007840271	0.012617662	0.004658747	0.002360831	0.007019578</	

Table 5: Hourly composite emission rate for each road link

L050	DD	50	01:00 ~ 02:00	100	0.2674799	0.079802327	0.347282227	0.558895179	0.0032609	0.001479587	0.004740487	0.007629055	0.0029858	0.001298038	0.004283838	0.006894152
L051	DD	50	01:00 ~ 02:00	150	0.3042262	0.070477103	0.374070303	0.603025014	0.0025893	0.000916686	0.003505986	0.005642323	0.0023738	0.00081051	0.00318431	0.005124638
L052	DD	50	01:00 ~ 02:00	100	0.2772246	0.068249269	0.345473869	0.555984916	0.0033113	0.001121854	0.004433154	0.007134452	0.0030372	0.000996137	0.004033337	0.00649101
L053	DD	50	01:00 ~ 02:00	150	0.2865003	0.06807078	0.35457108	0.570625422	0.0029388	0.00101293	0.00395173	0.006359676	0.0026944	0.000900192	0.003594592	0.005784921
L054	DD	50	01:00 ~ 02:00	100	0.2739086	0.069353086	0.343261686	0.552424762	0.0027819	0.001102392	0.003884292	0.006251146	0.0025056	0.000978377	0.003528977	0.005679324
L055	LD	50	01:00 ~ 02:00	50	0.226240154	0.065578142	0.291818297	0.469634858	0.004763531	0.001626252	0.006389783	0.010283333	0.00437145	0.001450236	0.005821686	0.009369071
L056	LD	50	01:00 ~ 02:00	50	0.229945677	0.076978046	0.306923723	0.505494625	0.0052498	0.001916052	0.007165852	0.011532292	0.004815128	0.006502836	0.010465275	0.00692671
L057	DD	200	01:00 ~ 02:00	200	0.2956122	0.06778009	0.36339229	0.584821747	0.0029224	0.000941838	0.003864238	0.006218873	0.0026798	0.000836349	0.003516149	0.005586679
L058	DD	50	01:00 ~ 02:00	100	0.2854625	0.06915164	0.3570694719	0.570694719	0.003107	0.001085763	0.004192763	0.006747581	0.0028496	0.000962506	0.003812106	0.006134975
L059	LD	50	01:00 ~ 02:00	50	0.196969597	0.037960171	0.234929767	0.378081872	0.00525758	0.001091244	0.006348824	0.010217417	0.004841812	0.001014587	0.005856398	0.009429436
L060	LD	50	01:00 ~ 02:00	50	0.208906663	0.0637734	0.272680063	0.438834933	0.005018752	0.001698435	0.006717187	0.010810237	0.004606746	0.001517497	0.006124243	0.009855989
L061	LD	50	01:00 ~ 02:00	50	0.0283644	0.023828543	0.052192943	0.08399619	0.0053171	0.001804571	0.007121671	0.01146619	0.004902	0.001681532	0.00658352	0.010595142
L062	LD	50	01:00 ~ 02:00	50	0.236796193	0.097208491	0.334004684	0.537527098	0.00266685	0.002505151	0.004717001	0.007591259	0.002435515	0.001783103	0.004218618	0.006789191
L063	LD	50	01:00 ~ 02:00	0	0	0	0	0	0	0	0	0	0	0	0	0
L064	LD	50	01:00 ~ 02:00	50	0.195688975	0.173511474	0.369200448	0.594162656	0.004162656	0.004564196	0.008726852	0.014044472	0.003765338	0.0039303022	0.007668359	0.012340998
L065	DD	50	01:00 ~ 02:00	150	0.3022564	0.078345635	0.380602035	0.612518078	0.0025777	0.001132058	0.003709058	0.005969135	0.0023582	0.000898987	0.003348187	0.005388371
L066	DD	50	01:00 ~ 02:00	100	0.308472	0.07976634	0.38823834	0.62480749	0.0023134	0.001083463	0.003396863	0.005466708	0.0021164	0.000944831	0.003061231	0.004926562
L067	DD	50	01:00 ~ 02:00	150	0.2989294	0.078479932	0.377409332	0.607379935	0.0025773	0.001143143	0.003720443	0.005987459	0.0023588	0.001005067	0.003359367	0.005406364
L068	DD	50	01:00 ~ 02:00	200	0.3041533	0.082366577	0.386519877	0.622041899	0.0022954	0.001190588	0.003485988	0.00561014	0.0020976	0.001036886	0.003134486	0.005044453
L069	LD	50	01:00 ~ 02:00	100	0.297317545	0.077059227	0.374376772	0.602499514	0.002946024	0.001191738	0.004137762	0.006590666	0.002699402	0.001045723	0.003745126	0.00602718
L070	LD	50	01:00 ~ 02:00	100	0.297050591	0.078163044	0.375213636	0.603846312	0.002562522	0.001172275	0.003734797	0.006010559	0.002348884	0.001027963	0.003736844	0.005447489
L071	LD	50	01:00 ~ 02:00	50	0.313488088	0.068616175	0.382104263	0.614935674	0.002102224	0.000758989	0.002861213	0.004046464	0.00192969	0.000674672	0.002604362	0.00491304
L072	LD	50	01:00 ~ 02:00	50	0.29449041	0.065909062	0.360399472	0.580005286	0.00262778	0.000867263	0.003495043	0.005624712	0.002412113	0.000775564	0.003187676	0.005130055
L073	LD	50	01:00 ~ 02:00	50	0.32900338	0.070420917	0.399421255	0.642804603	0.001972152	0.000686806	0.002658958	0.00479167	0.001808931	0.00060741	0.002416341	0.003888715
L074	LD	50	01:00 ~ 02:00	50	0.298996506	0.068811433	0.365807938	0.588709348	0.002610598	0.000831172	0.003441769	0.005358977	0.002395851	0.000741933	0.003137784	0.005049761
L075	DD	50	01:00 ~ 02:00	200	0.3014181	0.069560686	0.370978786	0.597030999	0.0025025	0.000895307	0.003400807	0.005473055	0.0022968	0.000793366	0.003090166	0.004971128
L076	DD	50	01:00 ~ 02:00	100	0.2780981	0.063902873	0.342000973	0.550395846	0.0027795	0.00092817	0.003702317	0.005958297	0.0023518	0.000836954	0.003378754	0.005437564
L077	LD	50	01:00 ~ 02:00	100	0.288431296	0.076156856	0.364588151	0.586746396	0.003002124	0.001227829	0.004229954	0.006807433	0.002750863	0.001079354	0.003830015	0.006163796
L078	DD	50	01:00 ~ 02:00	100	0.304199	0.07796257	0.37716157	0.606981202	0.0025407	0.000958026	0.003498726	0.00563064	0.0023274	0.000843854	0.0031711254	0.005103627
L079	DD	50	01:00 ~ 02:00	200	0.3053388	0.08259217	0.38793097	0.624312827	0.0023443	0.001181565	0.003525865	0.005674316	0.0021434	0.001028478	0.003171878	0.00510463
L080	DD	50	01:00 ~ 02:00	100	0.2988571	0.073615202	0.372472302	0.599434575	0.0020566	0.000956609	0.003013209	0.004849278	0.0018816	0.00084291	0.00272451	0.004384661
L081	DD	50	01:00 ~ 02:00	200	0.3068801	0.085542869	0.392522969	0.631702915	0.00236	0.00116233	0.00362233	0.00582956	0.0021558	0.001095782	0.003251282	0.005232901
L082	LD	50	01:00 ~ 02:00	50	0.184207183	0.137200711	0.321407895	0.517254581	0.004007347	0.003631114	0.007638461	0.012292881	0.003640077	0.003129008	0.006769085	0.01089379
L083	LD	50	01:00 ~ 02:00	50	0.195536633	0.103597062	0.299133695	0.48140782	0.002589758	0.003392314	0.00582071	0.009627186	0.002254102	0.005351927	0.0061307	0.00861307
L084	LD	50	01:00 ~ 02:00	50	0.0283644	0.023828543	0.052192943	0.08399619	0.0053171	0.001804571	0.007121671	0.01146619	0.004902	0.001681532	0.006583532	0.010595142
L085	LD	50	01:00 ~ 02:00	50	0.0283644	0.023828543	0.052192943	0.08399619	0.0053171	0.001804571	0.007121671	0.01146619	0.004902	0.001681532	0.006583532	0.010595142
L086	LD	50	01:00 ~ 02:00	50	0.154234688	0.132688856	0.286923544	0.461757537	0.004488667	0.003811571	0.008260238	0.013293531	0.004046943	0.003297161	0.007344104	0.011819161
L087	LD	50	01:00 ~ 02:00	150	0.272464665	0.085335786	0.357800451	0.575822578	0.003492861	0.001647603	0.008272754	0.003198144	0.001438565	0.004636709	0.007462402	0.010469162
L088	LD	50	01:00 ~ 02:00	50	0.257996688	0.122618827	0.380615415	0.612539612	0.002673163	0.002624084	0.005297247	0.008525071	0.002423738	0.002254271	0.004678009	0.005728507
L089	LD	50	01:00 ~ 02:00	100	0.303612649	0.07685778	0.380470429	0.61230628	0.003034292	0.001175109	0.002782226	0.003812112	0.001029852	0.000812122	0.006134985	0.0106134985
L090	LD	50	01:00 ~ 02:00	100	0.344624544	0.086215634	0.430840178	0.693368332	0.00502969	0.001033043	0.006353601	0.010518665	0.005055547	0.000913005	0.005968552	0.009605429
L091	LD	50	01:00 ~ 02:00	50	0.242914458	0.077880417	0.320794875	0.516268024	0.005255511	0.00187996	0.007135471	0.011483399	0.00481871	0.001654078	0.006472788	0.010416916
L092	LD	50	01:00 ~ 02:00	100	0.355967164	0.094968498	0.450935661	0.725708797	0.00540174	0.000994117	0.006395857	0.010293108	0.004959882	0.000877485	0.005837367	0.009394308
L093	LD	50	01:00 ~ 02:00	50	0.2717249	0.082392272	0.354117172	0.56989493	0.00419214	0.001699503	0.005891643	0.009481657	0.003843825	0.00532975	0.0057387	0.00857738
L094	LD	50	01:00 ~ 02:00	50	0.336084048	0.090285232	0.42636928	0.686173137	0.005948851	0.000939446	0.006888297	0.011086111	0.005467806	0.000842825	0.006310631	0.010155951
L095	LD	50	01:00 ~ 02:00	150	0.254532062	0.059000432	0.309532494	0.502429472	0.00086398	0.002429472	0.00323587	0.005207615	0.002236019	0.00074056	0.002976579	0.004790328
L096	LD	50	01:00 ~ 02:00	50	0.32744685	0.076677693	0.404124543	0.650373792	0.005946279	0.000688571	0.00663485	0.010677729	0.005472604	0.000640871	0.006113476	0.009838661
L097	LD	50	01:00 ~ 02:00	100	0.309675857	0.094351852	0.40402771	0.650217954	0.006275488	0.001391657	0.007667145	0.012339044	0.005764407	0.001231636	0.006996043	0.011259012
L098	LD	50	01:00 ~ 02:00	50	0.23051195	0.103056419	0.333568369	0.536824918	0.008486348	0.003110275	0.011596713	0.018663054	0.007771313	0.002702805	0.010474117	0.016856416
L099	LD	50	01:00 ~ 02:00	150	0.238472765	0.07256228	0.31038993	0.500567493	0.002836961	0.001429041	0.004266003	0.006865449	0.002602246	0.001267723	0.003869699	0.006228096
L100	LD	50	01:00 ~ 02:00	50	0.2089216	0.046387819	0.255309419	0.41087966	0.00265855	0.000902286	0.003560836	0.005730595	0.002451	0.000840766	0.003291766	0.005295771
L101	LD	50	01:00 ~ 02:00	50	0.244157695	0.092793221	0.336950916	0.542268587	0.004238054	0.002128002	0.006366056	0.010245148	0.003878495			

Table 5: Hourly composite emission rate for each road link

L132	PD	50	01:00 ~ 02:00	250	0.330646789	0.127622127	0.458268916	0.737510497	0.004989795	0.002182357	0.007172152	0.011542431	0.00456447	0.001854036	0.006418506	0.010329559
L133	DD	50	01:00 ~ 02:00	200	0.2860337	0.068825324	0.354859024	0.571088821	0.0032858	0.001086471	0.004372271	0.007036471	0.0030128	0.003997579	0.006399388	0.006399388
L134	DD	50	01:00 ~ 02:00	150	0.3016245	0.068403766	0.370028266	0.595501289	0.003219	0.000966697	0.004185697	0.006736209	0.0029518	0.000856611	0.003808411	0.006129028
L135	LD	50	01:00 ~ 02:00	50	0.318104231	0.079113708	0.397217939	0.639258718	0.002525918	0.00108488	0.003610798	0.005811002	0.002316099	0.000945776	0.003261875	0.005249466
L136	PD	50	01:00 ~ 02:00	200	0.314119289	0.091710335	0.405829624	0.653117847	0.006329474	0.00170481	0.008034284	0.012928895	0.005805505	0.001475713	0.007280767	0.01171723
L137	PD	50	01:00 ~ 02:00	300	0.296738724	0.088538442	0.385277166	0.620041954	0.004615156	0.001383993	0.005999149	0.00965467	0.004231705	0.001203409	0.005435114	0.008746947
L138	LD	50	01:00 ~ 02:00	100	0.301023075	0.073164017	0.374187092	0.602194255	0.002183457	0.000974655	0.003158112	0.005082476	0.00200105	0.000859725	0.002860776	0.004603961
L139	LD	50	01:00 ~ 02:00	100	0.359414004	0.086284384	0.445698388	0.717280244	0.001488892	0.000847452	0.002336345	0.003759973	0.00135847	0.000752287	0.002083577	0.003353474
L140	LD	50	01:00 ~ 02:00	50	0.333770187	0.094123347	0.427893534	0.68862618	0.007023931	0.00104772	0.008071651	0.01299003	0.006455748	0.000943717	0.007399465	0.011908255
L141	PD	50	01:00 ~ 02:00	250	0.300903024	0.088811085	0.389714109	0.627182504	0.004578747	0.001457899	0.006036646	0.009715016	0.004196678	0.001261696	0.005458374	0.008784379
L142	LD	50	01:00 ~ 02:00	100	0.28841414	0.065457876	0.353872017	0.569500391	0.002323918	0.000885309	0.003209226	0.005164736	0.002135339	0.000792379	0.002927718	0.004711693
L143	LD	50	01:00 ~ 02:00	100	0.348764319	0.08059215	0.436823534	0.702997586	0.003809972	0.000902472	0.004712444	0.007583925	0.003495964	0.000792464	0.004288428	0.006901538
L144	LD	50	01:00 ~ 02:00	50	0.296174444	0.092624662	0.388799107	0.625709954	0.001686399	0.001438362	0.003124761	0.005028803	0.001537473	0.001244843	0.002782316	0.004477692
L145	LD	50	01:00 ~ 02:00	50	0.263863061	0.088112807	0.351975868	0.566448844	0.002197542	0.001618819	0.003816361	0.006141823	0.002006756	0.001412996	0.003419752	0.005503544
L146	PD	50	01:00 ~ 02:00	200	0.315673233	0.089085952	0.404759185	0.651395146	0.006385353	0.001623337	0.00800869	0.012889706	0.005857111	0.001407937	0.007265048	0.011691933
L147	PD	50	01:00 ~ 02:00	250	0.314203985	0.09037375	0.404577795	0.651103132	0.005448844	0.001523644	0.006972489	0.011221106	0.004996568	0.001316294	0.006312863	0.010159542
L148	LD	50	01:00 ~ 02:00	100	0.286608568	0.065457876	0.352066445	0.566594612	0.002350503	0.000885309	0.003235812	0.005207521	0.000792379	0.000792379	0.0029518	0.004751138
L149	LD	50	01:00 ~ 02:00	150	0.337987515	0.08769421	0.425681725	0.680566628	0.003337515	0.000962335	0.004299845	0.006919912	0.003062459	0.000842938	0.003905227	0.006285112
L150	LD	50	01:00 ~ 02:00	100	0.365818234	0.115130067	0.480948302	0.77409394	0.004875874	0.001405104	0.006280978	0.01108229	0.004469406	0.001213102	0.005682508	0.009145087
L151	LD	50	01:00 ~ 02:00	150	0.313223956	0.08643983	0.399662785	0.643193307	0.00364536	0.001190321	0.004835681	0.007782255	0.003423209	0.001044779	0.004387088	0.007060316
L152	LD	50	01:00 ~ 02:00	150	0.309401791	0.086547235	0.395949025	0.637216604	0.004204551	0.002140332	0.005444883	0.008762667	0.00385805	0.001090879	0.004948909	0.00764477
L153	LD	50	01:00 ~ 02:00	50	0.278006358	0.075907117	0.353913475	0.569567112	0.002742614	0.001262504	0.004005118	0.006445596	0.002513568	0.00111204	0.003625608	0.005834636
L154	LD	50	01:00 ~ 02:00	100	0.316521395	0.092217169	0.408743364	0.657807045	0.002287565	0.001405104	0.003692669	0.00594276	0.002087852	0.001213102	0.003300954	0.005312357
L155	LD	50	01:00 ~ 02:00	100	0.288478746	0.080954437	0.369433183	0.594543598	0.003314116	0.001408821	0.004722937	0.007600811	0.003034794	0.001231721	0.004266515	0.006866273
L156	LD	50	01:00 ~ 02:00	50	0.321340086	0.081321343	0.402661429	0.648019144	0.002104562	0.00150517	0.005070253	0.001925328	0.000910256	0.002835584	0.004563419	0.006399388
L157	LD	50	01:00 ~ 02:00	50	0.314917243	0.077308966	0.392226209	0.631225326	0.003310338	0.001157063	0.004674001	0.007189568	0.003032969	0.001013037	0.004040606	0.00651139
L158	LD	50	01:00 ~ 02:00	50	0.331785992	0.108939315	0.440725306	0.709276865	0.00138978	0.001580963	0.00291994	0.004699177	0.001207398	0.001345905	0.002553302	0.004109132
L159	PD	50	01:00 ~ 02:00	200	0.315673233	0.089085952	0.404759185	0.651395146	0.006385353	0.001623337	0.00800869	0.012889706	0.005857111	0.001407937	0.007265048	0.011691933
L160	PD	50	01:00 ~ 02:00	150	0.32583933	0.090740156	0.416579487	0.670418031	0.006734203	0.001622954	0.003575157	0.013449507	0.006177438	0.001404991	0.007582429	0.012202706
L161	PD	50	01:00 ~ 02:00	150	0.372130862	0.104837526	0.476518388	0.766880102	0.008009411	0.00180448	0.00981389	0.015793886	0.003747463	0.001541573	0.008889035	0.01430548
L162	LD	50	01:00 ~ 02:00	100	0.297207497	0.072261646	0.369469143	0.594601471	0.002505148	0.001010746	0.003515894	0.005685269	0.002296732	0.000893356	0.003190088	0.005133936
L163	LD	50	01:00 ~ 02:00	150	0.310898211	0.07224965	0.383147861	0.616615179	0.002095104	0.000878705	0.002973809	0.004785869	0.001921034	0.00077562	0.002696654	0.004398384
L164	LD	50	01:00 ~ 02:00	50	0.17281016	0.041875963	0.214686123	0.345502966	0.00319026	0.001082743	0.006876714	0.0029412	0.001008919	0.003950119	0.006357085	0.006357085
L165	LD	50	01:00 ~ 02:00	50	0.159100167	0.239816401	0.398916568	0.641992389	0.005356028	0.006683	0.012039028	0.019374889	0.004829373	0.005664644	0.010515837	0.016923557
L166	LD	50	01:00 ~ 02:00	150	0.316409158	0.076049618	0.392458776	0.631599607	0.002352226	0.000975305	0.003327531	0.005355129	0.002155464	0.000854778	0.003010242	0.004844503
L167	LD	50	01:00 ~ 02:00	100	0.29576304	0.072261646	0.368024686	0.592276848	0.002526416	0.001010746	0.003537162	0.005692497	0.00231634	0.000893356	0.003209696	0.005165492
L168	DD	50	01:00 ~ 02:00	50	0.248508	0.075407639	0.323915639	0.521290394	0.0020684	0.001331853	0.003400259	0.005472163	0.001893	0.001177412	0.004941337	0.004941337
L169	DD	50	01:00 ~ 02:00	100	0.2912485	0.093659605	0.384908105	0.619448009	0.0031629	0.001695787	0.004858687	0.007819279	0.0028874	0.001467305	0.004354705	0.007008202
L170	LD	50	01:00 ~ 02:00	50	0.283044797	0.074601853	0.357646649	0.003288004	0.004504141	0.007248694	0.002971647	0.004085576	0.00113929	0.004085576	0.006575081	0.006575081
L171	LD	50	01:00 ~ 02:00	50	0.229144006	0.04904932	0.278238938	0.447781053	0.002360792	0.000794011	0.003154804	0.005077152	0.002176488	0.000739874	0.002916362	0.004694318
L172	LD	50	01:00 ~ 02:00	50	0.148615495	0.03916885	0.187784345	0.302208859	0.003546506	0.001191017	0.004737523	0.007624285	0.003269634	0.001109811	0.004379445	0.007048016
L173	LD	50	01:00 ~ 02:00	100	0.303803429	0.084966814	0.388770243	0.625663503	0.002476814	0.001297713	0.003774527	0.006074497	0.002264513	0.00112894	0.003393453	0.00546122
L174	LD	50	01:00 ~ 02:00	150	0.283957581	0.083641351	0.367598931	0.591591664	0.002991268	0.00146679	0.004458058	0.007174531	0.002738004	0.001280936	0.00401894	0.006467841
L175	LD	50	01:00 ~ 02:00	100	0.292480778	0.070706643	0.36318742	0.584492043	0.002804869	0.001048254	0.003853124	0.006200986	0.002574114	0.000927931	0.003502045	0.005356931
L176	LD	50	01:00 ~ 02:00	50	0.27452428	0.110413137	0.382865565	0.616160869	0.003254974	0.002267769	0.008887971	0.005227443	0.001953315	0.00492234	0.007921718	0.007921718
L177	LD	50	01:00 ~ 02:00	50	0.290228533	0.443099092	0.733327625	1.18017348	0.005395072	0.011274463	0.01669535	0.02682695	0.004756527	0.009455812	0.014212339	0.022872486
L178	LD	50	01:00 ~ 02:00	50	0.360320088	0.487531193	0.847851281	1.364480981	0.004640221	0.011882722	0.016522944	0.026591034	0.004045291	0.00992715	0.01397244	0.022486407
L179	LD	50	01:00 ~ 02:00	50	0.277601965	0.099512711	0.377114676	0.606905732	0.003112293	0.001908619	0.005020912	0.008080355	0.002838881	0.00165047	0.00448935	0.007224891
L180	PD	50	01:00 ~ 02:00	200	0.306200426	0.094310572	0.400510998	0.644558369	0.0061442	0.001811934	0.007956134	0.012804125	0.005633055	0.001567767	0.007200822	0.011588571
L181	PD	70	01:00 ~ 02:00	450	0.253049226	0	0.253049226	0.400724221	0.001423988	0	0.001423988	0.002291681	0.001291308	0	0.001291308	0.002078153
L182	PD	70	01:00 ~ 02:00	650	0.24859662	0	0.24859662	0.400076484	0.001592565	0	0.001592565	0.002562978	0.001449206	0	0.001449206	0.002332265
L183																

Table 5: Hourly composite emission rate for each road link

L018	LD		50	02:00 ~ 03:00	50	0.232779348	0.06959052	0.302369867	0.486615922	0.003471997	0.001515144	0.00498714	0.008026004	0.003181795	0.001347454	0.004529249	0.007289102
L019	LD		50	02:00 ~ 03:00	50	0.271650341	0.105095496	0.376745836	0.606312144	0.004994372	0.00238171	0.007376082	0.011870624	0.004565693	0.002057985	0.006623678	0.010659751
L020	LD		100	02:00 ~ 03:00	100	0.288282171	0.083613257	0.371895428	0.598506188	0.00231836	0.00135185	0.00367021	0.005906615	0.002120363	0.001179386	0.003299749	0.005310418
L021	DD		200	02:00 ~ 03:00	200	0.3299232	0.084915872	0.414839072	0.667617112	0.0027974	0.000965191	0.003762591	0.006055288	0.0025628	0.000842952	0.003405572	0.005481013
L022	DD		100	02:00 ~ 03:00	100	0.3165184	0.07587113	0.39238953	0.631488166	0.0016969	0.00086638	0.00412519	0.0015522	0.000758833	0.002311033	0.003071928	
L023	LD		100	02:00 ~ 03:00	100	0.311691826	0.07296257	0.384654397	0.619039707	0.002729656	0.000958026	0.003687682	0.005934734	0.002501388	0.000843854	0.003345242	0.005383632
L024	LD		50	02:00 ~ 03:00	50	0.206732296	0.055080609	0.261812905	0.421345981	0.00130036	0.005058304	0.008140531	0.003452274	0.001179131	0.004631405	0.004735306	
L025	DD		50	02:00 ~ 03:00	50	0.0277	0.023828543	0.051528543	0.082926945	0.005	0.001804571	0.006804571	0.010950869	0.0046	0.001681532	0.006281532	0.010109121
L026	DD		50	02:00 ~ 03:00	50	0.0277	0.023828543	0.051528543	0.082926945	0.005	0.001804571	0.006804571	0.010950869	0.0046	0.001681532	0.006281532	0.010109121
L027	DD		100	02:00 ~ 03:00	100	0.2981908	0.07685778	0.37504858	0.603580682	0.0028592	0.001175109	0.004034309	0.006492575	0.0026188	0.001029852	0.003648652	0.005871922
L028	DD		50	02:00 ~ 03:00	50	0.311866	0.06640854	0.37827454	0.608772348	0.0030932	0.000797914	0.003891114	0.006262125	0.0028372	0.000710192	0.003547392	0.005708959
L029	LD		50	02:00 ~ 03:00	50	0.125955347	0.036461737	0.162417084	0.26138431	0.003758663	0.001299291	0.005057954	0.008139968	0.003461974	0.001210703	0.004672677	0.007519926
L030	LD		250	02:00 ~ 03:00	250	0.3159807	0.038801165	0.399781865	0.643384947	0.0028229	0.001039657	0.003862557	0.006216167	0.002586	0.0009106	0.0034966	0.005627218
L031	DD		200	02:00 ~ 03:00	200	0.3101667	0.079465659	0.389632359	0.62705094	0.0019093	0.000785616	0.002694916	0.004337036	0.001748	0.00069153	0.00243953	0.003926032
L032	DD		50	02:00 ~ 03:00	50	0.286584	0.089514656	0.376098656	0.605270611	0.002498	0.001513379	0.004011379	0.006455672	0.00228	0.001313993	0.003593993	0.005783957
L033	LD		50	02:00 ~ 03:00	50	0.232480069	0.10058364	0.333063709	0.53601275	0.005152231	0.002562168	0.007714399	0.012145091	0.004708378	0.002226139	0.006934517	0.011159995
L034	LD		50	02:00 ~ 03:00	50	0.218257469	0.091391372	0.309649021	0.498330555	0.004047931	0.002233442	0.006281374	0.011098866	0.003701413	0.001953145	0.005645558	0.009100707
L035	LD		50	02:00 ~ 03:00	50	0.0280564	0.023828543	0.051884943	0.083500514	0.0051701	0.001804571	0.006974671	0.011224617	0.004762	0.001681532	0.006445332	0.010369834
L036	LD		50	02:00 ~ 03:00	50	0.0280564	0.023828543	0.051884943	0.083500514	0.0051701	0.001804571	0.006974671	0.011224617	0.004762	0.001681532	0.006445332	0.010369834
L037	LD		50	02:00 ~ 03:00	50	0.218257469	0.091391372	0.309649021	0.498330555	0.004047931	0.002233442	0.006281374	0.011098866	0.003701413	0.001953145	0.005645558	0.009100707
L038	LD		50	02:00 ~ 03:00	50	0.216647826	0.092293743	0.308941569	0.497719205	0.003836891	0.002197351	0.006034242	0.009711147	0.003510719	0.001891515	0.005430234	0.008739092
L039	LD		50	02:00 ~ 03:00	50	0.17149808	0.041874043	0.343391383	0.571688934	0.00310206	0.001082748	0.004184803	0.00673477	0.0028572	0.001008919	0.00386116	0.006212901
L040	LD		50	02:00 ~ 03:00	50	0.181897602	0.042778334	0.224675936	0.361579971	0.002952127	0.001046651	0.003998778	0.006435394	0.002719102	0.000975289	0.003694391	0.005945531
L041	LD		50	02:00 ~ 03:00	50	0.187276665	0.043680705	0.23095737	0.371688934	0.00101056	0.003885135	0.006252504	0.002647672	0.000941658	0.00338993	0.005776452	
L042	LD		50	02:00 ~ 03:00	50	0.181897602	0.042778334	0.224675936	0.361579971	0.002952127	0.001046651	0.003998778	0.006435394	0.002719102	0.000975289	0.003694391	0.005945531
L043	LD		50	02:00 ~ 03:00	50	0.3137422	0.068616175	0.615344626	0.382358374	0.002360634	0.003758989	0.003119623	0.005020534	0.002164131	0.000674672	0.002383803	0.004568599
L044	LD		100	02:00 ~ 03:00	100	0.277602127	0.073699482	0.351301609	0.565368731	0.003318126	0.001301429	0.004619555	0.007434434	0.003042654	0.00114756	0.004190214	0.006734479
L045	LD		50	02:00 ~ 03:00	50	0.227016635	0.067382884	0.29439952	0.473788923	0.00414442	0.001554069	0.005698489	0.009170806	0.002389072	0.001382974	0.005185946	0.008345491
L046	LD		50	02:00 ~ 03:00	50	0.217744739	0.042874919	0.260619658	0.419425641	0.003888512	0.000944045	0.004832556	0.007777226	0.003579366	0.000878175	0.004457541	0.007173699
L047	DD		250	02:00 ~ 03:00	250	0.3132811	0.083259743	0.396540843	0.63816904	0.0029576	0.001061311	0.004018911	0.006467795	0.0027093	0.000930778	0.003640078	0.005858124
L048	DD		50	02:00 ~ 03:00	50	0.2834829	0.088612285	0.372095185	0.598827665	0.0027499	0.00154947	0.00429937	0.006919148	0.0025136	0.001347624	0.003861224	0.006214022
L049	DD		150	02:00 ~ 03:00	150	0.3059616	0.071513772	0.607486215	0.907475372	0.0022822	0.00089168	0.00317388	0.005107852	0.0020912	0.00078746	0.002873763	
L050	DD		100	02:00 ~ 03:00	100	0.2718878	0.074803299	0.346691099	0.557943854	0.0027837	0.001281966	0.004065666	0.006543039	0.0025494	0.0011298	0.0036792	0.005921083
L051	DD		150	02:00 ~ 03:00	150	0.310561	0.072115353	0.615856361	0.907475372	0.0022822	0.00089168	0.00317388	0.005107852	0.0020912	0.00078746	0.002873763	
L052	DD		100	02:00 ~ 03:00	100	0.2812702	0.070255457	0.351525657	0.565724301	0.0027713	0.0010663	0.0038376	0.006176003	0.0025402	0.000944746	0.003484946	0.005608464
L053	DD		150	02:00 ~ 03:00	150	0.2942775	0.065640466	0.359917966	0.57923038	0.0025953	0.000845091	0.003440391	0.005536759	0.00238	0.000754403	0.003134403	0.005044432
L054	DD		100	02:00 ~ 03:00	100	0.2851741	0.064805244	0.349979344	0.563235758	0.0026795	0.000886726	0.003566226	0.005739269	0.0024598	0.000793324	0.003253124	0.005235382
L055	LD		50	02:00 ~ 03:00	50	0.209118546	0.08179621	0.290914756	0.468180754	0.00288185	0.001871459	0.004753309	0.007649691	0.002636991	0.001648411	0.004285331	0.006895654
L056	LD		50	02:00 ~ 03:00	50	0.19373154	0.044583076	0.238314617	0.383529246	0.002781514	0.000974468	0.003755982	0.006044652	0.002561956	0.000908027	0.003469983	0.005584383
L057	DD		150	02:00 ~ 03:00	150	0.2992989	0.066677135	0.365976035	0.588979872	0.0024309	0.000820086	0.003250986	0.005231941	0.0022294	0.000731353	0.002960753	0.004764858
L058	DD		100	02:00 ~ 03:00	100	0.2892769	0.059355031	0.348631931	0.561067312	0.0025748	0.000707151	0.003281951	0.005281775	0.0023638	0.000641901	0.003005701	0.004837195
L059	LD		50	02:00 ~ 03:00	50	0.216330634	0.039764913	0.256095547	0.412144808	0.005148396	0.001019061	0.006167457	0.009252536	0.004737919	0.000947326	0.005685245	0.009149492
L060	LD		50	02:00 ~ 03:00	50	0.217133622	0.076075675	0.293209297	0.47187345	0.005159806	0.001952143	0.007111949	0.011445544	0.00473143	0.00172134	0.00645277	0.0103847
L061	LD		50	02:00 ~ 03:00	50	0.0280564	0.023828543	0.051884943	0.083500514	0.0051701	0.001804571	0.006974671	0.011224617	0.004762	0.001681532	0.006445332	0.010369834
L062	LD		50	02:00 ~ 03:00	50	0.237122649	0.049997303	0.287119951	0.462073622	0.002155932	0.00075792	0.002913852	0.004689378	0.001985754	0.000706244	0.002691998	0.004332339
L063	LD		0	02:00 ~ 03:00	0	0	0	0	0	0	0	0	0	0	0	0	0
L064	LD		50	02:00 ~ 03:00	50	0.21521585	0.020728552	0.415942402	0.669392745	0.00391005	0.005065946	0.008975996	0.01444543	0.0035235	0.004306929	0.007803429	0.012601822
L065	DD		100	02:00 ~ 03:00	100	0.3055596	0.074066388	0.379425988	0.610625419	0.0021594	0.000938563	0.003097963	0.004985676	0.0019766	0.000826095	0.002802695	0.005104485
L066	DD		50	02:00 ~ 03:00	50	0.317426	0.074767312	0.392139233	0.631712385	0.0021669	0.000895843	0.003052743	0.004912902	0.0019848	0.00076593	0.002761393	0.00444402
L067	DD		100	02:00 ~ 03:00	100	0.3056029	0.074066388	0.379425988	0.610625419	0.0021594	0.000938563	0.003097963	0.004985676	0.0019766	0.000826095	0.002802695	0.005104485
L068	DD		200	02:00 ~ 03:00	200	0.3108504	0.077919458	0.388769858	0.625662884	0.0019861	0.000938236	0.003299336	0.004779672	0.0018158	0.000859768	0.002675568	0.004305998
L069	LD		100	02:00 ~ 03:00	100	0.301222772	0.073164017	0.374386789	0.602515634	0.002410437	0.000774655	0.00					

Table 5: Hourly composite emission rate for each road link

L100	LD	50	02:00 ~ 03:00	50	0.2073585	0.046387819	0.253746319	0.4083641	0.00258505	0.000902286	0.003487336	0.005612309	0.002381	0.000840766	0.003221766	0.005184917
L101	LD	50	02:00 ~ 03:00	50	0.253318294	0.070089997	0.323408291	0.5204739	0.004007694	0.001445795	0.005453488	0.008776516	0.003675155	0.001282082	0.004957237	0.00797788
L102	LD	50	02:00 ~ 03:00	50	0.244399593	0.089280321	0.333679914	0.537004433	0.00549412	0.002169761	0.007663881	0.01233379	0.00503370	0.001891552	0.006925259	0.011455096
L103	LD	50	02:00 ~ 03:00	50	0.26851347	0.072797111	0.341310581	0.54928477	0.003206688	0.00133752	0.004544208	0.007313175	0.002941895	0.00118119	0.004123085	0.006635446
L104	PD	200	02:00 ~ 03:00	200	0.323151263	0.077241015	0.400392278	0.644367308	0.00562292	0.001127889	0.006750579	0.010684316	0.005161274	0.000986397	0.006147672	0.009893694
L105	PD	150	02:00 ~ 03:00	150	0.387022304	0.111241398	0.498263702	0.801875706	0.007613762	0.001535318	0.00914908	0.014723981	0.006986390	0.001313477	0.008299786	0.01335718
L106	LD	50	02:00 ~ 03:00	50	0.217426816	0.124592128	0.342018944	0.550424767	0.005145675	0.003241541	0.008387215	0.013497881	0.004694197	0.00279631	0.007495057	0.012054772
L107	LD	100	02:00 ~ 03:00	100	0.294231721	0.066360247	0.360591968	0.580315078	0.002496439	0.003345657	0.005849217	0.003538429	0.002293046	0.000758748	0.003049094	0.00490703
L108	LD	100	02:00 ~ 03:00	100	0.328915076	0.070872103	0.399787178	0.643393498	0.001656939	0.00066876	0.003235699	0.00374284	0.00151947	0.000590595	0.002110342	0.003396258
L109	LD	50	02:00 ~ 03:00	50	0.291676891	0.065909062	0.357585953	0.575477378	0.002727251	0.000862673	0.003594514	0.005784794	0.002501432	0.000775664	0.003276996	0.0052327
L110	LD	50	02:00 ~ 03:00	50	0.286426083	0.124020676	0.410446759	0.660548387	0.003232783	0.002518644	0.005751426	0.009256	0.002937025	0.002155269	0.005092294	0.008195322
L111	LD	50	02:00 ~ 03:00	50	0.264949728	0.062299578	0.327249306	0.526555398	0.003161078	0.001011629	0.004172707	0.006715304	0.002900639	0.000910086	0.003810725	0.006132753
L112	LD	100	02:00 ~ 03:00	100	0.267676461	0.056849365	0.324525826	0.522272393	0.002810257	0.000832054	0.003642311	0.005861716	0.002583132	0.000758664	0.003341796	0.005378085
L113	LD	50	02:00 ~ 03:00	50	0.25065958	0.06684742	0.317506999	0.510976714	0.003139421	0.001227295	0.004366716	0.007027531	0.002879523	0.00109514	0.003974663	0.006396584
L114	LD	50	02:00 ~ 03:00	50	0.248844548	0.027297633	0.321142181	0.516826958	0.003126132	0.001406869	0.004533001	0.00729514	0.002864919	0.001246562	0.004111481	0.006616772
L115	LD	50	02:00 ~ 03:00	50	0.201448948	0.027324888	0.228773835	0.368174884	0.008782251	0.001319128	0.010101379	0.016256553	0.008079743	0.001223927	0.00930367	0.014972769
L116	LD	50	02:00 ~ 03:00	50	0.0280564	0.023828543	0.051884945	0.083500514	0.0051701	0.001804571	0.006974671	0.011224617	0.004762	0.001681532	0.006443532	0.010369834
L117	LD	50	02:00 ~ 03:00	50	0.147471599	0.03916885	0.186640449	0.30036794	0.003446457	0.001191017	0.004639474	0.007466491	0.003176254	0.001109811	0.004286065	0.006897736
L118	PD	250	02:00 ~ 03:00	250	0.31060176	0.082058302	0.392658062	0.631920326	0.004971061	0.001269583	0.006240644	0.010433318	0.004560142	0.001105759	0.005665902	0.009118365
L119	PD	150	02:00 ~ 03:00	150	0.394615431	0.127144953	0.521760385	0.839689857	0.007889189	0.001942946	0.009832135	0.015823426	0.007233939	0.001650639	0.008884578	0.014298307
L120	LD	50	02:00 ~ 03:00	50	0.221656813	0.083600952	0.305257765	0.491263531	0.002764367	0.001799276	0.004563643	0.007344453	0.002525282	0.001106431	0.006068644	0.010296756
L121	LD	100	02:00 ~ 03:00	100	0.29917673	0.07861423	0.37779096	0.607994103	0.002340478	0.001154229	0.003494707	0.005624172	0.00214228	0.001011148	0.003153428	0.00507948
L122	LD	100	02:00 ~ 03:00	100	0.324884981	0.064068333	0.388953314	0.625958126	0.003196971	0.000543323	0.002480293	0.003991635	0.001779155	0.000489618	0.0023661228	0.003651228
L123	LD	50	02:00 ~ 03:00	50	0.219970961	0.082698581	0.302669542	0.4870982	0.002742647	0.001833568	0.004578015	0.007367582	0.002050777	0.00161478	0.00412255	0.006634585
L124	LD	50	02:00 ~ 03:00	50	0.2073585	0.046387819	0.253746319	0.4083641	0.00258505	0.000902286	0.003487336	0.005612309	0.002381	0.000840766	0.003221766	0.005184917
L125	LD	50	02:00 ~ 03:00	50	0.2073585	0.046387819	0.253746319	0.4083641	0.00258505	0.000902286	0.003487336	0.005612309	0.002381	0.000840766	0.003221766	0.005184917
L126	PD	150	02:00 ~ 03:00	150	0.392287048	0.122609107	0.514896155	0.828642978	0.007881903	0.001589321	0.009741224	0.015676941	0.007229118	0.001583321	0.008812439	0.014182211
L127	DD	50	02:00 ~ 03:00	50	0.1564832	0.040071221	0.196554421	0.316322892	0.00318	0.001154926	0.004334926	0.006976369	0.0029256	0.001076181	0.004001781	0.006440226
L128	LD	50	02:00 ~ 03:00	50	0.289614466	0.065909062	0.355253528	0.572158234	0.00236903	0.000867263	0.005208296	0.0021745	0.000775564	0.000957064	0.004747556	0.00746256
L129	LD	100	02:00 ~ 03:00	100	0.337768681	0.076572054	0.414340735	0.666615119	0.002014238	0.00081366	0.002827898	0.004551049	0.001845077	0.000709332	0.002554409	0.004110912
L130	LD	100	02:00 ~ 03:00	100	0.359116781	0.079279168	0.438395948	0.705528136	0.001657962	0.000705386	0.002363347	0.003080343	0.0015173	0.00060844	0.00212574	0.003421038
L131	LD	50	02:00 ~ 03:00	50	0.289614466	0.065909062	0.355253528	0.572158234	0.00236903	0.000867263	0.003236293	0.005208296	0.0021745	0.000775564	0.000957064	0.004747556
L132	PD	250	02:00 ~ 03:00	250	0.340660724	0.117916116	0.45857684	0.738006051	0.005071256	0.000907767	0.011116946	0.004643337	0.001563887	0.006207724	0.009989584	0.014972769
L133	DD	50	02:00 ~ 03:00	50	0.2880606	0.069408239	0.357468839	0.575288902	0.0025819	0.000975893	0.003557793	0.005725699	0.0023666	0.000865932	0.003232532	0.005022243
L134	DD	100	02:00 ~ 03:00	100	0.3027823	0.06609986	0.369392286	0.594477782	0.0024996	0.000814543	0.003314143	0.0022928	0.000726062	0.003018862	0.004858376	0.00746256
L135	LD	50	02:00 ~ 03:00	50	0.322472252	0.080016079	0.402488331	0.647740571	0.002477243	0.001048789	0.003526031	0.005674583	0.002269652	0.000912145	0.003181797	0.005120593
L136	PD	150	02:00 ~ 03:00	150	0.32893372	0.087541769	0.416475489	0.670250664	0.0053989	0.001502293	0.008042182	0.012942606	0.006000279	0.001303413	0.007303692	0.011758412
L137	PD	300	02:00 ~ 03:00	300	0.30601348	0.087020227	0.393033707	0.632524866	0.004526967	0.001238799	0.005765766	0.009279077	0.004152256	0.001079096	0.005231351	0.008419023
L138	LD	100	02:00 ~ 03:00	100	0.305964407	0.068164989	0.374129397	0.602101403	0.001987825	0.002764859	0.004449599	0.001824515	0.000691487	0.002516002	0.004049103	0.006405128
L139	LD	50	02:00 ~ 03:00	50	0.331365306	0.074932772	0.436268078	0.702103669	0.00133501	0.000506349	0.001841359	0.002963372	0.0012221	0.000439257	0.001661357	0.002673689
L140	LD	50	02:00 ~ 03:00	50	0.333851174	0.10287621	0.436727385	0.702842849	0.006237823	0.001008795	0.007246618	0.011662272	0.00573043	0.000908197	0.006386227	0.010683808
L141	PD	250	02:00 ~ 03:00	250	0.31807787	0.085238654	0.397046442	0.63898272	0.004618176	0.001291451	0.005909627	0.009510599	0.004234822	0.001119624	0.005354445	0.008617123
L142	LD	100	02:00 ~ 03:00	100	0.298243267	0.067262618	0.365505885	0.588223241	0.002714637	0.000813126	0.004764875	0.001971366	0.000725118	0.002696454	0.004395111	0.00746256
L143	LD	100	02:00 ~ 03:00	100	0.35653035	0.086332802	0.442863152	0.712717385	0.003738819	0.000704852	0.00443637	0.007151377	0.003432572	0.000624226	0.004056798	0.006528767
L144	LD	50	02:00 ~ 03:00	50	0.297889573	0.069019068	0.366908641	0.590480753	0.001440925	0.000792246	0.002333171	0.003593932	0.00131846	0.000706413	0.002024873	0.003252761
L145	LD	50	02:00 ~ 03:00	50	0.276397538	0.089917549	0.366315087	0.589525522	0.001952141	0.001546636	0.003498778	0.005630723	0.001778416	0.001345735	0.003124151	0.005027821
L146	PD	150	02:00 ~ 03:00	150	0.331039341	0.087541769	0.416475489	0.673639324	0.006732649	0.001502293	0.008234942	0.013252821	0.006178632	0.001303413	0.007482045	0.012041154
L147	PD	200	02:00 ~ 03:00	200	0.321177201	0.08678831	0.40796032	0.656546861	0.005265092	0.001343803	0.006068895	0.010635958	0.004829845	0.001163558	0.005993403	0.009645423
L148	LD	100	02:00 ~ 03:00	100	0.291788391	0.066360247	0.358148639	0.57638293	0.002240699	0.000849217	0.004972726	0.002057052	0.000758748	0.0028158	0.004531528	0.00746256
L149	LD	100	02:00 ~ 03:00	100	0.342298876	0.085097404	0.42739628	0.687825929	0.002951929	0.000817608	0.003769537	0.006066467	0.002708724	0.000718939	0.003427663	0.005516276
L150	LD	100	02:00 ~ 03:00	100	0.34818692	0.107953441	0.482772133	0.776944504	0.004877749	0.002107909	0.005815659	0.009359372	0.004392718	0.000893441	0.005286159	0.008507227
L151	LD	100	02:00													

Table 5 : Hourly composite emission rate for each road link

L182	PD		70:02:00 ~ 03:00	600	0.254605247		0	0.254605247	0.409746408	0.001342233	0	0.001342233	0.002160109	0.001221023	0	0.001221023	0.001965041
L183	PD		50:02:00 ~ 03:00	150	0.349759014	0.087382912	0.437141926	0.703509987	0.007718978	0.001490262	0.009209241	0.014820799	0.007085086	0.001292203	0.008377289	0.013481906	
L184	PD		50:02:00 ~ 03:00	150	0.364632786	0.07990752	0.744518563	0.00723537	0.001563158	0.00712878	0.0008691937	0.001338416	0.006540724	0.001338416	0.007879141	0.012680126	
L185	DD		50:02:00 ~ 03:00	150	0.2947235	0.082489286	0.377572786	0.607642987	0.0022173	0.001249885	0.003467185	0.005579879	0.0020262	0.001089676	0.003115876	0.005014504	
L186	DD		50:02:00 ~ 03:00	150	0.3130722	0.073452812	0.386525012	0.622050162	0.0018057	0.000830583	0.004242676	0.0016534	0.000730779	0.002384179	0.003836955		
L187	LD		50:02:00 ~ 03:00	50	0.249195248	0.079685159	0.328880407	0.529280394	0.004843047	0.001807778	0.006650825	0.010703438	0.004438545	0.001586817	0.006025362	0.009696856	
L188	LD		50:02:00 ~ 03:00	50	0.17448808	0.041875963	0.213374043	0.343391383	0.00310206	0.004184803	0.00673477	0.004184803	0.0028572	0.00108919	0.003866119	0.006221901	
L189	LD		50:02:00 ~ 03:00	50	0.17778399	0.159903935	0.37687895	0.543454636	0.00416206	0.004313321	0.008475381	0.01363977	0.0037712	0.0037712	0.007472268	0.013025242	
L190	LD		50:02:00 ~ 03:00	50	0.17448808	0.041875963	0.213374043	0.343391383	0.00310206	0.004184803	0.00673477	0.004184803	0.0028572	0.00108919	0.003866119	0.006221901	
L191	LD		50:02:00 ~ 03:00	50	0.166119017	0.040973592	0.33328242	0.207092609	0.003179612	0.001118834	0.004298446	0.006917661	0.00104255	0.00397118	0.006390979		
L192	LD		50:02:00 ~ 03:00	50	0.191221311	0.044583076	0.235804387	0.379489433	0.002817705	0.000974468	0.003792173	0.006102896	0.00259529	0.000980827	0.003503317	0.005638029	
L193	LD		50:02:00 ~ 03:00	50	0.187276665	0.043680705	0.23095737	0.371688934	0.00284576	0.00101056	0.003885135	0.006252504	0.002647672	0.000941658	0.003589933	0.005776452	
L194	LD		50:02:00 ~ 03:00	50	0.147471599	0.03916885	0.186640449	0.30036794	0.003448457	0.001191017	0.004639474	0.007466491	0.003176254	0.001109811	0.004286065	0.006897736	
L195	LD		50:02:00 ~ 03:00	50	0.21940545	0.091391372	0.310796821	0.500177757	0.004145631	0.002233442	0.010266098	0.003789713	0.001953145	0.005742858	0.009242212		
L196	LD		50:02:00 ~ 03:00	50	0.224382854	0.068285255	0.29268109	0.471002495	0.00393992	0.001517977	0.005457897	0.008738613	0.003616497	0.001349344	0.004965841	0.007991726	
L001	LD		50:03:00 ~ 04:00	50	0.28994838	0.075504224	0.588137493	0.002922525	0.00129246	0.004517771	0.006681611	0.002678098	0.001080298	0.003758396	0.006048538		
L002	LD		50:03:00 ~ 04:00	50	0.360823366	0.070517502	0.431340868	0.694174113	0.002793426	0.000584199	0.003377625	0.005435747	0.002560148	0.000510297	0.003070445	0.00494139	
L003	LD		50:03:00 ~ 04:00	50	0.2054192	0.046387819	0.251807019	0.405243107	0.00252415	0.000902286	0.003426436	0.0055143	0.002323	0.000840766	0.003163766	0.005091575	
L004	DD		50:03:00 ~ 04:00	150	0.3050414	0.084742409	0.389783809	0.627294675	0.0032273	0.00137338	0.004006068	0.007404058	0.0029522	0.001193143	0.004145343	0.006671267	
L005	DD		50:03:00 ~ 04:00	100	0.315347	0.080217525	0.635697813	0.002392	0.001065418	0.003454718	0.005564161	0.002188	0.000928016	0.003116016	0.005014729		
L006	LD		50:03:00 ~ 04:00	50	0.341876524	0.082723192	0.424599716	0.683325307	0.002090472	0.000940515	0.003030987	0.004877888	0.001913303	0.000811253	0.002724283	0.004384298	
L007	LD		50:03:00 ~ 04:00	100	0.328023114	0.078912261	0.406935376	0.654897377	0.00317446	0.00168251	0.004242712	0.006827966	0.002908867	0.000929905	0.003838678	0.006177884	
L008	LD		50:03:00 ~ 04:00	50	0.322539512	0.079113708	0.40165322	0.646396593	0.003118039	0.00108488	0.00420292	0.006763927	0.002853284	0.000945776	0.00379906	0.006113979	
L009	LD		50:03:00 ~ 04:00	50	0.241664586	0.078782788	0.320447374	0.515708777	0.004878985	0.001843869	0.006722854	0.010819358	0.004469491	0.001620448	0.006089939	0.00980782	
L010	LD		50:03:00 ~ 04:00	50	0.3830372	0.068947094	0.451984294	0.727396404	0	0	0	0	0	0	0	0	
L011	LD		50:03:00 ~ 04:00	50	0.312338152	0.087806499	0.400144651	0.643968792	0.003639395	0.001482955	0.00512235	0.008243603	0.00333038	0.001284141	0.004614521	0.007426333	
L012	LD		50:03:00 ~ 04:00	50	0.1166102	0.035559366	0.152169566	0.24489257	0.003786225	0.001335383	0.005121608	0.008242408	0.0034845	0.001244334	0.004728834	0.007610302	
L013	LD		50:03:00 ~ 04:00	50	0.291183083	0.051898629	0.343081712	0.552135123	0.003170279	0.00058313	0.006040512	0.002915754	0.000541869	0.003457623	0.00556449		
L014	DD		50:03:00 ~ 04:00	150	0.3062664	0.077607596	0.383876156	0.617787253	0.0030721	0.001145033	0.004217133	0.0067868	0.0028124	0.001001827	0.003814227	0.006138388	
L015	DD		50:03:00 ~ 04:00	50	0.3373782	0.084028456	0.421406656	0.678186588	0.0018203	0.000937681	0.002757981	0.004438529	0.000809364	0.002470164	0.003975334		
L016	LD		50:03:00 ~ 04:00	50	0.289610132	0.057126271	0.346826403	0.558161603	0.001327703	0.00046189	0.001796891	0.002891809	0.001221898	0.000437198	0.001659096	0.00267005	
L017	LD		50:03:00 ~ 04:00	50	0.312338152	0.087806499	0.400144651	0.643968792	0.003639395	0.001482955	0.00512235	0.008243603	0.00333038	0.001284141	0.004614521	0.007426333	
L018	LD		50:03:00 ~ 04:00	50	0.241664586	0.078782788	0.320447374	0.515708777	0.004878985	0.001843869	0.006722854	0.010819358	0.004469491	0.001620448	0.006089939	0.00980782	
L019	LD		50:03:00 ~ 04:00	50	0.26290659	0.113788286	0.606230133	0.606230133	0.005847578	0.002779785	0.008627363	0.01388436	0.005340951	0.002366351	0.007737302	0.01245195	
L020	LD		50:03:00 ~ 04:00	50	0.290039644	0.087307021	0.377346665	0.607279081	0.003315616	0.001552304	0.00486792	0.007831438	0.003031884	0.001349513	0.004381397	0.007051158	
L021	DD		50:03:00 ~ 04:00	150	0.318513	0.082312095	0.400825095	0.645063859	0.0030859	0.001205541	0.004291441	0.006906388	0.0028238	0.001047354	0.003871154	0.006230003	
L022	LD		50:03:00 ~ 04:00	50	0.3128591	0.081321343	0.394180443	0.634370354	0.0020211	0.001045955	0.003067055	0.004935934	0.0018468	0.000910256	0.002757056	0.004437041	
L023	LD		50:03:00 ~ 04:00	50	0.319393253	0.076003702	0.395396955	0.636328135	0.003974794	0.00159897	0.005134691	0.008263463	0.003641817	0.001014926	0.004656744	0.007494284	
L024	LD		50:03:00 ~ 04:00	50	0.224950412	0.065175249	0.290125661	0.466910831	0.005031247	0.001592994	0.006624241	0.010606506	0.004618453	0.001418494	0.006063997	0.00971155	
L025	LD		50:03:00 ~ 04:00	50	0.1643227	0.020605397	0.297612184	0.0099432	0.00153851	0.0099432	0.01148171	0.018477975	0.0091424	0.0014276	0.017010725		
L026	DD		50:03:00 ~ 04:00	50	0.0277	0.023828543	0.051528543	0.082926945	0.005	0.001804571	0.006804571	0.01050869	0.0046	0.001681532	0.006281532	0.010109121	
L027	DD		50:03:00 ~ 04:00	50	0.3131312	0.073796066	0.387109266	0.627990427	0.0042407	0.00198822	0.005439522	0.00875404	0.0038848	0.001050446	0.004935246	0.007942489	
L028	DD		50:03:00 ~ 04:00	50	0.3267277	0.073993173	0.400120873	0.643930526	0.0048155	0.001165564	0.005981064	0.009625566	0.0044182	0.001018705	0.005436905	0.008749828	
L029	LD		50:03:00 ~ 04:00	50	0.1166102	0.035559366	0.152169566	0.24489257	0.003786225	0.001335383	0.005121608	0.008242408	0.0034845	0.001244334	0.004728834	0.007610302	
L030	DD		50:03:00 ~ 04:00	200	0.3053988	0.083109641	0.388508441	0.625242174	0.0031177	0.001310278	0.004427978	0.007126122	0.0028526	0.001139709	0.003992309	0.006424983	
L031	DD		50:03:00 ~ 04:00	50	0.3172278	0.079414498	0.396642298	0.638332316	0.0026836	0.00107285	0.00375645	0.006045405	0.0024566	0.000934166	0.005457538		
L032	LD		50:03:00 ~ 04:00	50	0.2968588	0.097804554	0.394663354	0.635147522	0.0038167	0.001878195	0.005694895	0.009165023	0.0034866	0.001620617	0.005017217	0.008219249	
L033	LD		50:03:00 ~ 04:00	50	0.236148474	0.124189235	0.360338708	0.579907497	0.005720491	0.003208283	0.008928774	0.01469434	0.005220713	0.002764568	0.007985283	0.012851036	
L034	LD		50:03:00 ~ 04:00	50	0.234678501	0.100583644	0.335262141	0.539550774	0.005545219	0.002562168	0.008107387	0.013047542	0.005069814	0.002226139	0.007295952	0.011741668	
L035	LD		50:03:00 ~ 04:00	50	0.0278012	0.023828543	0.051629743	0.08308981	0.0050483	0.001804571	0.006852871	0.0110286	0.0044646	0.001681532	0.006327532	0.010183151	
L036	LD		50:03:00 ~ 04:00	50	0.0278012	0.023828543	0.051629743	0.08308981	0.0050483	0.001804571	0.006852871	0.0110286	0.0044646	0.001681532	0.006327532	0.010183151	
L037	LD		50:03:00 ~ 04:00	50	0.234678501	0.100583644	0.335262141	0.539550774	0.005545219	0.002562168	0.008107387	0.013047					

Table 5: Hourly composite emission rate for each road link

L068	DD	50	03:00~04:00	150	0.3090683	0.086815747	0.395884047	0.637112032	0.0027065	0.001323369	0.004029869	0.006485429	0.002474	0.001147043	0.003621043	0.00582749
L069	LD	50	03:00~04:00	100	0.289633995	0.08250944	0.372143435	0.598905315	0.002869924	0.001371312	0.004241237	0.006825592	0.002626248	0.001197146	0.003823394	0.006153141
L070	LD	50	03:00~04:00	100	0.295987392	0.071609014	0.367596406	0.515876	0.002836704	0.001012163	0.003848867	0.006194136	0.002599116	0.0008943	0.003493416	0.005622095
L071	LD	50	03:00~04:00	50	0.31408603	0.068616175	0.382702205	0.65189766	0.002344263	0.000758989	0.003103251	0.004994186	0.002147613	0.002822285	0.003768943	0.004542016
L072	LD	50	03:00~04:00	50	0.296424714	0.076406595	0.372831309	0.600012338	0.002971633	0.001193155	0.004164787	0.006702559	0.002722275	0.001046668	0.003768943	0.006065551
L073	LD	50	03:00~04:00	50	0.32852333	0.08091845	0.40944178	0.658931034	0.002327284	0.001012697	0.003339981	0.005375166	0.002130631	0.000878515	0.003009146	0.004842738
L074	LD	50	03:00~04:00	50	0.296424714	0.076406595	0.372831309	0.600012338	0.002971633	0.001193155	0.004164787	0.006702559	0.002722275	0.001046668	0.003768943	0.006065551
L075	DD	50	03:00~04:00	150	0.3042326	0.07367549	0.3779809	0.608182606	0.0028228	0.000373747	0.003860147	0.006212289	0.000912089	0.003499089	0.005631223	0.005631223
L076	DD	50	03:00~04:00	50	0.2848244	0.062799056	0.347623456	0.559444332	0.0032996	0.00094228	0.00424188	0.006826626	0.0030278	0.000844714	0.003872514	0.006232192
L077	LD	50	03:00~04:00	50	0.288773694	0.075504224	0.364277918	0.586247024	0.00336265	0.001229246	0.004591896	0.007389922	0.003800097	0.001080298	0.004160395	0.006695491
L078	DD	50	03:00~04:00	50	0.3088858	0.078211337	0.387097137	0.622970906	0.0030818	0.001120972	0.004202772	0.006763689	0.002821	0.000979406	0.003800406	0.006161446
L079	DD	50	03:00~04:00	150	0.3066227	0.087250835	0.393873535	0.633876434	0.0024696	0.001322424	0.003792024	0.006102656	0.0022554	0.001146413	0.003401813	0.005474674
L080	DD	50	03:00~04:00	50	0.3013622	0.067713804	0.369076004	0.59396876	0.0023472	0.00079508	0.00314228	0.005056997	0.002151	0.000708302	0.002859302	0.00460159
L081	DD	50	03:00~04:00	150	0.3078592	0.087250835	0.3938110035	0.63566683	0.0025044	0.001322424	0.003826824	0.006158661	0.0022868	0.001146413	0.003433213	0.00525208
L082	LD	50	03:00~04:00	50	0.21338875	0.200724552	0.414115302	0.66645532	0.00381215	0.005065946	0.008878096	0.014287875	0.003434025	0.004306929	0.007470979	0.012457867
L083	LD	50	03:00~04:00	50	0.209403975	0.117204601	0.326608576	0.525624246	0.00316815	0.002840633	0.006008783	0.009670174	0.002878525	0.002456055	0.00334568	0.008585154
L084	LD	50	03:00~04:00	50	0.0278012	0.023828543	0.051629743	0.08308981	0.0050483	0.001804571	0.006852871	0.0110286	0.004646	0.001681532	0.006327532	0.010183151
L085	LD	50	03:00~04:00	50	0.0278012	0.023828543	0.051629743	0.08308981	0.0050483	0.001804571	0.006852871	0.0110286	0.004646	0.001681532	0.006327532	0.010183151
L086	LD	50	03:00~04:00	50	0.153171748	0.168999619	0.322171367	0.518483268	0.00451096	0.004744653	0.009255613	0.014895429	0.004085494	0.004071175	0.008156669	0.013126854
L087	LD	50	03:00~04:00	100	0.271316743	0.090952492	0.362269235	0.583014371	0.00355831	0.001804061	0.005398992	0.008593702	0.003230514	0.001568197	0.004798712	0.007722599
L088	LD	50	03:00~04:00	50	0.270067271	0.148029164	0.418096435	0.672859317	0.002541468	0.003198017	0.002289299	0.007272544	0.000514739	0.000702421	0.008070421	0.010070421
L089	LD	50	03:00~04:00	50	0.306927526	0.087806499	0.394734025	0.635261255	0.003575143	0.001482955	0.005058097	0.008140199	0.003272265	0.001284141	0.004556406	0.007332807
L090	LD	50	03:00~04:00	50	0.296129632	0.062396162	0.358525794	0.57689902	0.00387517	0.000909022	0.004784192	0.007699392	0.003557857	0.000812973	0.004570803	0.007034151
L091	LD	50	03:00~04:00	50	0.246647794	0.08837795	0.335025744	0.539170331	0.005843222	0.002205852	0.008049074	0.012953697	0.005349427	0.001925182	0.007274609	0.01170732
L092	LD	50	03:00~04:00	50	0.306927526	0.087806499	0.394734025	0.635261255	0.003575143	0.001482955	0.005058097	0.008140199	0.003272265	0.001284141	0.004556406	0.007332807
L093	LD	50	03:00~04:00	50	0.291789658	0.094694547	0.386484205	0.621984491	0.004637151	0.001953212	0.006590343	0.010606103	0.004243519	0.001689768	0.005933287	0.009458674
L094	LD	50	03:00~04:00	50	0.260318197	0.048289145	0.308607342	0.49665414	0.003235918	0.000277496	0.003963414	0.006378481	0.002976539	0.000676391	0.00365293	0.005878806
L095	LD	50	03:00~04:00	100	0.251864536	0.056599626	0.308464162	0.496423714	0.002522781	0.000866729	0.003389509	0.005454878	0.002316992	0.00079135	0.003108342	0.005002237
L096	LD	50	03:00~04:00	50	0.260318197	0.048289145	0.308607342	0.49665414	0.003235918	0.000277496	0.003963414	0.006378481	0.002976539	0.000676391	0.00365293	0.005878806
L097	LD	50	03:00~04:00	50	0.254981368	0.056981936	0.311963304	0.502055024	0.004427492	0.00112557	0.005553063	0.008936766	0.004063573	0.001014757	0.005078833	0.008172759
L098	LD	50	03:00~04:00	50	0.251841874	0.368102939	0.116261065	0.592402783	0.008592283	0.003327893	0.011920176	0.010183616	0.007859067	0.002873017	0.010732084	0.017271572
L099	LD	50	03:00~04:00	100	0.237837214	0.072499079	0.310336293	0.49943661	0.002879065	0.001423498	0.004302563	0.006924288	0.002636562	0.001262433	0.003898995	0.006274808
L100	LD	50	03:00~04:00	50	0.146094788	0.03916885	0.298152184	0.503367216	0.001191017	0.004558233	0.007335747	0.003098882	0.001109811	0.004206693	0.006773218	0.006773218
L101	LD	50	03:00~04:00	50	0.24563587	0.079685159	0.325321029	0.523552145	0.004728453	0.001807778	0.006536231	0.00519017	0.004331633	0.001586817	0.00591845	0.009524798
L102	LD	50	03:00~04:00	50	0.23502549	0.097973112	0.332998602	0.53590797	0.006384772	0.002567835	0.00852607	0.014407788	0.005845107	0.002229917	0.008075024	0.012995459
L103	LD	50	03:00~04:00	50	0.255946208	0.070992368	0.326938576	0.526155329	0.003782117	0.001409703	0.00519182	0.008355404	0.003466476	0.001248452	0.004714928	0.007587922
L104	PD	50	03:00~04:00	150	0.344459702	0.082143382	0.426603084	0.68649407	0.0076919	0.00138541	0.00907731	0.014608478	0.007061108	0.001204354	0.008265462	0.013301938
L105	PD	50	03:00~04:00	100	0.40503837	0.113080933	0.51811927	0.833830066	0.009579883	0.0019985	0.011578383	0.018633554	0.008786672	0.00170203	0.010490071	0.016883105
L106	LD	50	03:00~04:00	50	0.228051459	0.375846288	0.147794829	0.37846466	0.005634135	0.003854399	0.009488534	0.015270277	0.005130364	0.003302998	0.008433363	0.013572148
L107	LD	50	03:00~04:00	100	0.285685548	0.070255457	0.355941005	0.572830097	0.002983105	0.0010663	0.004049405	0.006516869	0.00273385	0.000944746	0.003678596	0.005992012
L108	LD	50	03:00~04:00	50	0.331575269	0.069115653	0.400690922	0.644847928	0.002808063	0.003497702	0.005628992	0.002575482	0.0006093	0.003184782	0.005125397	0.005125397
L109	LD	50	03:00~04:00	50	0.283280982	0.074601853	0.357882835	0.575955161	0.00315842	0.001265337	0.004423757	0.00711933	0.002894177	0.00113929	0.004008106	0.006405405
L110	LD	50	03:00~04:00	50	0.285548621	0.145418635	0.430967256	0.693572844	0.003689076	0.001092795	0.003344072	0.002792218	0.00607329	0.000773989	0.009773989	0.009773989
L111	LD	50	03:00~04:00	50	0.28360586	0.070186582	0.353792442	0.569372329	0.004817124	0.001343188	0.006160311	0.009914035	0.004418725	0.001184699	0.005603694	0.009018249
L112	LD	50	03:00~04:00	100	0.256966079	0.060744575	0.317710653	0.511304463	0.003254512	0.001049137	0.004303649	0.006926034	0.002988724	0.000944662	0.003933035	0.006301544
L113	LD	50	03:00~04:00	100	0.249634046	0.075338764	0.32497281	0.522997142	0.004012964	0.00160874	0.005621704	0.009047233	0.003677909	0.001417634	0.005095543	0.008200462
L114	LD	50	03:00~04:00	50	0.256629788	0.08058753	0.337217318	0.542697319	0.004336978	0.001771686	0.006108664	0.009830917	0.00397305	0.001553186	0.005526236	0.008895593
L115	LD	50	03:00~04:00	50	0.224795035	0.027824365	0.2526194	0.406550505	0.00916976	0.001249778	0.010419538	0.01676858	0.008432024	0.001158555	0.009590579	0.015434502
L116	LD	50	03:00~04:00	50	0.165470623	0.020605397	0.18607602	0.299459582	0.001012826	0.00153851	0.011551336	0.019590027	0.009207906	0.0014276	0.010635506	0.01161145
L117	LD	50	03:00~04:00	50	0.26327196	0.02823321	0.292545281	0.470804823	0.00988418	0.00111108	0.01109526	0.017856046	0.00918016	0.001027811	0.010207971	0.016428096
L118	PD	50	03:00~04:00	200	0.326475492	0.089561283	0.416034775	0.669541405	0.006616005	0.00157964	0.008195645	0.013189579	0.006069536	0.001366843	0.007436379	0.011967663
L119	PD	50	03:00~04:00	100	0.408168897	0.13666527	0									

Table 5: Hourly composite emission rate for each road link

L150	LD		50	03:00 ~ 04:00	50	0.315042376	0.092221769	0.407264145	0.655426479	0.002498823	0.001405104	0.003903927	0.006282746	0.002280939	0.001213102	0.003494041	0.005623099
L151	LD		100	03:00 ~ 04:00	100	0.292248222	0.082960625	0.375208854	0.603838616	0.002960499	0.001353267	0.004313766	0.006942316	0.002207143	0.001180331	0.003887474	0.006256267
L152	LD		100	03:00 ~ 04:00	100	0.288792092	0.083001805	0.369093897	0.593997572	0.003597628	0.001410238	0.005007865	0.005095938	0.003294654	0.001232666	0.004527372	0.007285997
L153	LD		50	03:00 ~ 04:00	50	0.315042376	0.086112285	0.372038657	0.598736692	0.002948121	0.001354947	0.004497591	0.007238154	0.002694433	0.001347624	0.004040257	0.006055044
L154	LD		50	03:00 ~ 04:00	50	0.315042376	0.092217469	0.407264145	0.655426479	0.002498823	0.001405104	0.003903927	0.006282746	0.002280939	0.001213102	0.003494041	0.005623099
L155	LD		50	03:00 ~ 04:00	50	0.290295122	0.085093886	0.375389458	0.604137397	0.003717412	0.001591229	0.005308641	0.005340487	0.003403875	0.001385033	0.004788903	0.007706881
L156	LD		50	03:00 ~ 04:00	50	0.301437554	0.093527033	0.394964587	0.635632309	0.004023071	0.001446863	0.005469935	0.008082986	0.003876751	0.001250511	0.004932687	0.007934467
L157	LD		50	03:00 ~ 04:00	50	0.321758658	0.08870887	0.41065728	0.660578914	0.001692595	0.002335933	0.004027947	0.006482337	0.001524718	0.001985226	0.003509945	0.005648644
L158	LD		50	03:00 ~ 04:00	50	0.307713288	0.129837796	0.437551084	0.704168461	0.00283371	0.001713525	0.008841492	0.010575917	0.006114324	0.001518409	0.009632734	0.015023494
L159	PD		150	03:00 ~ 04:00	150	0.355430155	0.092283371	0.447713525	0.720523285	0.00841492	0.001713525	0.010575917	0.006114324	0.001518409	0.009632734	0.015023494	0.015023494
L160	PD		100	03:00 ~ 04:00	100	0.36382038	0.097254305	0.461074685	0.742025934	0.009014612	0.00183927	0.010853882	0.017467587	0.008272608	0.001580598	0.009857706	0.01586444
L161	PD		100	03:00 ~ 04:00	100	0.418388871	0.110019219	0.528358091	0.850307809	0.010991407	0.002022213	0.01301362	0.020943339	0.01087871	0.001722624	0.011810495	0.019007102
L162	LD		100	03:00 ~ 04:00	100	0.296561131	0.083411811	0.379972942	0.611505655	0.002284135	0.001335221	0.004159356	0.006693818	0.002583471	0.001163515	0.003746986	0.006030175
L163	LD		100	03:00 ~ 04:00	100	0.311722247	0.07976634	0.391488587	0.630038242	0.002356624	0.001083469	0.003440087	0.002536277	0.002155804	0.000944831	0.004989976	0.006030175
L164	LD		50	03:00 ~ 04:00	50	0.146094788	0.03916885	0.185263638	0.298152184	0.003367216	0.001191017	0.005458233	0.007335477	0.003098882	0.001109811	0.004208693	0.006773218
L165	LD		50	03:00 ~ 04:00	50	0.156710229	0.239816401	0.39652663	0.638146166	0.005082832	0.006683	0.011765832	0.018935225	0.004578801	0.005686464	0.010265265	0.016520301
L166	LD		100	03:00 ~ 04:00	100	0.308201696	0.079315154	0.387516851	0.623646369	0.002306954	0.001101509	0.003408463	0.005485376	0.002111067	0.000961647	0.003072714	0.004945041
L167	LD		100	03:00 ~ 04:00	100	0.296561131	0.083411811	0.379972942	0.611505655	0.002284135	0.001335221	0.004159356	0.006693818	0.002583471	0.001163515	0.003746986	0.006030175
L168	DD		50	03:00 ~ 04:00	50	0.241595	0.086308065	0.327903065	0.527707519	0.0020255	0.001691002	0.003946002	0.006350459	0.00206	0.001480258	0.003540258	0.005697478
L169	DD		50	03:00 ~ 04:00	50	0.3041099	0.098706925	0.402816825	0.648269229	0.0037665	0.001842104	0.005060804	0.009026151	0.0034398	0.005026787	0.008083909	0.008083909
L170	LD		50	03:00 ~ 04:00	50	0.2837551	0.084197014	0.367952114	0.592160056	0.003763305	0.00162732	0.005390625	0.008675349	0.00344645	0.001418664	0.004865114	0.007829622
L171	LD		50	03:00 ~ 04:00	50	0.2054192	0.046387819	0.405243107	0.60052415	0.00252415	0.000902286	0.003426436	0.0055143	0.002323	0.000840766	0.003163766	0.005091573
L172	LD		50	03:00 ~ 04:00	50	0.2054192	0.046387819	0.405243107	0.60052415	0.00252415	0.000902286	0.003426436	0.0055143	0.002323	0.000840766	0.003163766	0.005091573
L173	LD		100	03:00 ~ 04:00	100	0.302076296	0.09021558	0.392291877	0.631331009	0.002735738	0.001460658	0.006753428	0.002498338	0.001264492	0.00376283	0.006055673	0.006055673
L174	LD		100	03:00 ~ 04:00	100	0.288873657	0.087307021	0.376180678	0.605402612	0.00327172	0.001552304	0.004824024	0.007763495	0.002990381	0.001349513	0.004339894	0.006984366
L175	LD		50	03:00 ~ 04:00	50	0.294876394	0.076406595	0.37128299	0.597520565	0.003181828	0.001193155	0.004374982	0.007040034	0.002914363	0.001046668	0.003961031	0.006374455
L176	LD		50	03:00 ~ 04:00	50	0.271334757	0.109107873	0.38044263	0.612261543	0.003515534	0.002270602	0.005786136	0.009311861	0.003024074	0.001955204	0.005159479	0.008303355
L177	LD		50	03:00 ~ 04:00	50	0.2213583	0.341457746	0.562816046	0.905762376	0.00510015	0.008978732	0.014078882	0.022657707	0.0045451	0.007571138	0.012116238	0.019499146
L178	LD		50	03:00 ~ 04:00	50	0.344943669	0.463023228	0.807966897	1.300293446	0.004274298	0.011272698	0.015546996	0.025020403	0.003735719	0.009422351	0.01315807	0.021175808
L179	LD		50	03:00 ~ 04:00	50	0.275032823	0.384140696	0.618212988	0.903404723	0.002270602	0.005675325	0.009133527	0.005058431	0.001935204	0.005058431	0.008140736	0.008140736
L180	PD		150	03:00 ~ 04:00	150	0.339722813	0.104680113	0.444402926	0.715195404	0.008103292	0.002090734	0.010194025	0.016405653	0.007433139	0.001798205	0.009229595	0.014853556
L181	PD		300	03:00 ~ 04:00	300	0.255956312	0.411920731	0.01689362	0.012599531	0.001689362	0.002718758	0.001532716	0.002718758	0.001532716	0.002718758	0.001532716	0.002718758
L182	PD		450	03:00 ~ 04:00	450	0.250841983	0.012599531	0.01689362	0.012599531	0.001689362	0.002718758	0.001532716	0.002718758	0.001532716	0.002718758	0.001532716	0.002718758
L183	PD		100	03:00 ~ 04:00	100	0.379491968	0.095461017	0.474952985	0.764360836	0.010237197	0.010237197	0.01858733	0.019466464	0.00939709	0.001602857	0.010999947	0.017702655
L184	PD		100	03:00 ~ 04:00	100	0.395120976	0.104807292	0.499928267	0.804554558	0.009744026	0.018606884	0.01160471	0.018675924	0.008941297	0.001580816	0.010529314	0.016945245
L185	PD		100	03:00 ~ 04:00	100	0.2959569	0.088209392	0.384166292	0.61825418	0.0030764	0.005459212	0.002811	0.009391075	0.001315883	0.004126883	0.006641557	0.006641557
L186	DD		100	03:00 ~ 04:00	100	0.3063851	0.079967787	0.386352887	0.621773154	0.0022439	0.001100092	0.003434992	0.00538162	0.00202534	0.000960702	0.003041402	0.004805701
L187	LD		50	03:00 ~ 04:00	50	0.240635674	0.089280321	0.329915995	0.503947008	0.002169761	0.002169761	0.007353847	0.012124521	0.004912543	0.001891552	0.006804095	0.010950102
L188	LD		50	03:00 ~ 04:00	50	0.157107104	0.040071221	0.191718325	0.317326966	0.003210719	0.001154926	0.004365644	0.007025806	0.002954856	0.001076181	0.004031037	0.006487309
L189	LD		50	03:00 ~ 04:00	50	0.17627124	0.159903935	0.336175175	0.541020155	0.00405938	0.004313321	0.008372701	0.013474523	0.0036764	0.003701068	0.007377508	0.011872919
L190	LD		50	03:00 ~ 04:00	50	0.1166102	0.035559366	0.152169566	0.24489257	0.003786225	0.001335383	0.005121608	0.008242408	0.0034845	0.001244334	0.004728834	0.007106302
L191	LD		50	03:00 ~ 04:00	50	0.146094788	0.03916885	0.185263638	0.298152184	0.003367216	0.001191017	0.005458233	0.007335477	0.003098882	0.001109811	0.004208693	0.006773218
L192	LD		50	03:00 ~ 04:00	50	0.185525984	0.043680705	0.229206689	0.368871494	0.002806855	0.00101056	0.003817415	0.006143518	0.002583176	0.000941658	0.003524834	0.005672656
L193	LD		50	03:00 ~ 04:00	50	0.146094788	0.03916885	0.185263638	0.298152184	0.003367216	0.001191017	0.005458233	0.007335477	0.003098882	0.001109811	0.004208693	0.006773218
L194	LD		50	03:00 ~ 04:00	50	0.124780628	0.036461737	0.161242365	0.259493788	0.003670114	0.001299291	0.004964005	0.007997463	0.003377642	0.001210703	0.004588345	0.007384208
L195	LD		50	03:00 ~ 04:00	50	0.234678501	0.10058364	0.335262141	0.539550774	0.005545219	0.002562168	0.008107387	0.013047542	0.005069814	0.002226139	0.002759592	0.011741668
L196	LD		50	03:00 ~ 04:00	50	0.218792202	0.066480513	0.285272715	0.459100792	0.004357165	0.00159016	0.005947325	0.009571268	0.003995359	0.001416605	0.005411964	0.008706969
L001	LD		50	04:00 ~ 05:00	50	0.306590069	0.078211337	0.384801406	0.619276295	0.008121097	0.00120972	0.009397868	0.006433928	0.002634712	0.000979406	0.003614119	0.005816346
L002	LD		50	04:00 ~ 05:00	50	0.354223957	0.084527934	0.438751891	0.706100968	0.001870341	0.000688332	0.002738673	0.004047456	0.001710757	0.000743992	0.002454749	0.003905526
L003	LD		50	04:00 ~ 05:00	50	0.205417	0.046387819	0.405239567	0.60052415	0.0025231	0.000902286	0.003426436	0.0055143	0.002323	0.000840766	0.003163766	0.005091573
L004	DD		150	04:00 ~ 05:00	150	0.3260464</											

Table 5: Hourly composite emission rate for each road link

L036	LD	50	04:00~05:00	50	0.0277968	0.023828543	0.051625343	0.083082729	0.0050462	0.001804571	0.006850771	0.01102522	0.004644	0.001681532	0.006325532	0.010179932
L037	LD	50	04:00~05:00	50	0.21971436	0.090086108	0.498574284	0.004429944	0.00236276	0.010728214	0.00666622	0.004051384	0.001955035	0.006006419	0.00966637	
L038	LD	50	04:00~05:00	50	0.217246608	0.07828331	0.295529919	0.475608119	0.004154126	0.001913218	0.006067344	0.009764419	0.003801073	0.00168582	0.005486893	0.008830766
L039	LD	50	04:00~05:00	50	0.205417	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00551261	0.002322	0.000840766	0.003162766	0.005089966	
L040	LD	50	04:00~05:00	50	0.205417	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00551261	0.002322	0.000840766	0.003162766	0.005089966	
L041	LD	50	04:00~05:00	50	0.180194932	0.042778334	0.358839796	0.00288138	0.001046651	0.003928032	0.006321538	0.002651724	0.000975289	0.003627013	0.005837097	
L042	LD	50	04:00~05:00	50	0.185523538	0.043680705	0.229204243	0.368867557	0.002805687	0.001010556	0.003816247	0.006141639	0.002582064	0.000941658	0.003523722	0.005670867
L043	LD	50	04:00~05:00	50	0.409748791	0.076355824	0.486104615	0.782307601	0.00589614	0.00108488	0.00698102	0.011234836	0.00541464	0.000945776	0.006360416	0.012036072
L044	LD	50	04:00~05:00	50	0.268878338	0.063201949	0.332080286	0.534430088	0.002941286	0.000975537	0.003916823	0.006303501	0.002697361	0.000876456	0.003573817	0.005751486
L045	LD	50	04:00~05:00	50	0.207794081	0.081179621	0.28959029	0.466049238	0.00284326	0.001871459	0.004714719	0.007587586	0.002597876	0.001648411	0.004246287	0.006839719
L046	LD	50	04:00~05:00	50	0.22896044	0.043374397	0.272334837	0.438279346	0.0042643	0.000874695	0.005138995	0.008270391	0.00392236	0.000812803	0.004735163	0.007620487
L047	DD	150	04:00~05:00	150	0.3198635	0.082556131	0.402419631	0.647630009	0.0031343	0.001226768	0.004361068	0.007018442	0.0028702	0.001067885	0.003938085	0.006337718
L048	DD	50	04:00~05:00	50	0.2926601	0.100012189	0.392672289	0.631943221	0.0032475	0.00183927	0.00508677	0.008186343	0.002964	0.001585098	0.004549098	0.007321045
L049	DD	100	04:00~05:00	100	0.3366546	0.072724285	0.409378885	0.658829815	0.0033807	0.00093998	0.00432068	0.006953443	0.0031026	0.000827039	0.003929639	0.006324125
L050	DD	100	04:00~05:00	100	0.2800788	0.074601853	0.354680653	0.570801761	0.0032086	0.001265337	0.004473937	0.007200086	0.0029396	0.00113929	0.004053529	0.006523506
L051	DD	100	04:00~05:00	100	0.3465882	0.073626656	0.420214856	0.676268576	0.0034049	0.000903889	0.004308789	0.006934906	0.0031246	0.000793408	0.003918008	0.006305408
L052	DD	50	04:00~05:00	50	0.277583	0.065006691	0.342589691	0.551345293	0.0025431	0.000903354	0.003446454	0.005546517	0.0023322	0.000809194	0.003141394	0.005055572
L053	DD	100	04:00~05:00	100	0.3238696	0.064965512	0.388235112	0.624802295	0.0037973	0.000852051	0.004649351	0.007482387	0.00348664	0.000760638	0.004247038	0.006384927
L054	DD	50	04:00~05:00	50	0.277583	0.065006691	0.342589691	0.551345293	0.0025431	0.000903354	0.003446454	0.005546517	0.0023322	0.000809194	0.003141394	0.005055572
L055	LD	50	04:00~05:00	50	0.208080694	0.093599007	0.301679701	0.48550621	0.002851546	0.002194517	0.005146063	0.008281765	0.002691574	0.001917626	0.00460692	0.007417769
L056	LD	50	04:00~05:00	50	0.185523538	0.043680705	0.229204243	0.368867557	0.002805687	0.001010556	0.003816247	0.006141639	0.002582064	0.000941658	0.003523722	0.005670867
L057	LD	100	04:00~05:00	100	0.3303128	0.072723099	0.402585899	0.647897591	0.0035149	0.000958026	0.004472926	0.007198459	0.003224	0.000843854	0.004067854	0.005465651
L058	DD	50	04:00~05:00	50	0.2967108	0.064603798	0.361314598	0.581478034	0.0030849	0.000870097	0.003954997	0.006364934	0.0028306	0.00077453	0.003608053	0.005806584
L059	LD	50	04:00~05:00	50	0.232280243	0.038056755	0.270336998	0.435064145	0.005919163	0.000988637	0.00609078	0.01116999	0.005443635	0.000917473	0.006361108	0.010237186
L060	LD	50	04:00~05:00	50	0.2306459	0.086573208	0.317219108	0.5105134	0.005793778	0.002278035	0.008071812	0.012909291	0.005305884	0.001992444	0.007298328	0.011745491
L061	LD	50	04:00~05:00	50	0.0277968	0.023828543	0.051625343	0.083082729	0.0050462	0.001804571	0.006850771	0.01102522	0.004644	0.001681532	0.006325532	0.010179932
L062	LD	50	04:00~05:00	50	0.205417	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00551261	0.002322	0.000840766	0.003162766	0.005089966	
L063	LD	50	04:00~05:00	0	0	0	0	0	0	0	0	0	0	0	0	0
L064	LD	50	04:00~05:00	50	0.1763115	0.159903935	0.336215435	0.541084947	0.00460612	0.004313321	0.008373441	0.013475714	0.003367694	0.003701068	0.007378008	0.011873723
L065	DD	100	04:00~05:00	100	0.310853	0.073413756	0.384266756	0.618415861	0.0025401	0.00093998	0.004308008	0.005600632	0.0023262	0.000827039	0.003153239	0.005074634
L066	DD	50	04:00~05:00	50	0.3115867	0.081321343	0.392908043	0.63232263	0.0020858	0.001045955	0.003131755	0.005040059	0.001906	0.000910256	0.002816256	0.004532313
L067	DD	50	04:00~05:00	50	0.310853	0.073413756	0.384266756	0.618415861	0.0025401	0.00093998	0.004308008	0.005600632	0.0023262	0.000827039	0.003153239	0.005074634
L068	DD	150	04:00~05:00	150	0.33813	0.084495171	0.417625171	0.672100893	0.0027876	0.001165672	0.003953272	0.006362158	0.0025508	0.001011205	0.003562005	0.005732791
L069	LD	100	04:00~05:00	100	0.292813531	0.07861423	0.37142776	0.597753552	0.002252484	0.001154229	0.003379714	0.005439108	0.002036938	0.001011148	0.003048086	0.004905407
L070	LD	100	04:00~05:00	100	0.297643562	0.079065415	0.376708978	0.606252826	0.00221524	0.001136184	0.003351423	0.005939358	0.002026575	0.000994333	0.003020906	0.004861668
L071	LD	50	04:00~05:00	50	0.320831359	0.080016079	0.400847438	0.645098815	0.002344684	0.001048789	0.003393473	0.005461251	0.002147293	0.000912145	0.003059438	0.004923676
L072	LD	50	04:00~05:00	50	0.291699442	0.080821865	0.372521307	0.59951344	0.002654328	0.001115304	0.003524328	0.004271717	0.00140355	0.000975628	0.002379178	0.003829870
L073	LD	50	04:00~05:00	50	0.318505156	0.084431349	0.402936505	0.648461835	0.001124541	0.000970939	0.00209548	0.003372339	0.00102389	0.000841106	0.001864995	0.003001412
L074	LD	50	04:00~05:00	50	0.291699442	0.080821865	0.372521307	0.59951344	0.002654328	0.001115304	0.003524328	0.004271717	0.00140355	0.000975628	0.002379178	0.003829870
L075	DD	150	04:00~05:00	150	0.3075878	0.071814562	0.379402362	0.610587398	0.0022125	0.00087965	0.00309215	0.00497632	0.0020274	0.00077625	0.00280365	0.004512028
L076	DD	50	04:00~05:00	50	0.287775	0.065909062	0.353684062	0.569197908	0.0024925	0.000867263	0.003359763	0.005407001	0.002285	0.000775564	0.003060564	0.004925486
L077	LD	50	04:00~05:00	50	0.291825156	0.077711859	0.369537015	0.5947107	0.002567251	0.001190321	0.003757571	0.00604721	0.002347047	0.001044779	0.003391826	0.005458601
L078	DD	50	04:00~05:00	50	0.3126838	0.068616175	0.381299975	0.613641302	0.0021872	0.000758899	0.002946189	0.004741419	0.0020038	0.000674672	0.002678472	0.003410572
L079	DD	100	04:00~05:00	100	0.3371444	0.080631872	0.417776272	0.67234066	0.002851	0.001045955	0.003896955	0.006271526	0.0026112	0.000910256	0.003521456	0.00566722
L080	DD	50	04:00~05:00	50	0.3148812	0.069518546	0.384399746	0.618629887	0.0022963	0.000722897	0.003019197	0.004858915	0.0021032	0.000641041	0.002744241	0.004416417
L081	DD	50	04:00~05:00	50	0.3367921	0.086533271	0.423325371	0.681274452	0.0029171	0.001207484	0.004124584	0.006637858	0.0026688	0.001044863	0.003713663	0.005976547
L082	LD	50	04:00~05:00	50	0.184784188	0.125397914	0.310182102	0.499188464	0.003619546	0.003380856	0.006927602	0.011148868	0.00328846	0.002859793	0.006148253	0.009894629
L083	LD	50	04:00~05:00	50	0.209428588	0.117204601	0.326633189	0.525663856	0.00316835	0.002840633	0.006008983	0.009670496	0.002878588	0.002456055	0.005334643	0.008585254
L084	LD	50	04:00~05:00	50	0.0277968	0.023828543	0.051625343	0.083082729	0.0050462	0.001804571	0.006850771	0.01102522	0.004644	0.001681532	0.006325532	0.010179932
L085	LD	50	04:00~05:00	50	0.0277968	0.023828543	0.051625343	0.083082729	0.0050462	0.001804571	0.006850771	0.01102522	0.004644	0.001681532	0.006325532	0.010179932
L086	LD	50	04:00~05:00	50	0.133984811	0.119081317	0.253066127	0.407269442	0.004341153	0.003560696	0.007901849	0.012176762	0.003952552	0.003095207	0.007047599	0.011342241
L087	LD	100	04:00~05:00	100	0.272974097	0.086606096	0.359580193	0.578686788	0.003040406	0.001605024	0.00464543	0.004746077	0.002780241	0.001399015	0.004179255	0.006725843
L088	LD	50	04:00~05:00	50	0.270101628	0.148029164	0.418130791	0.672914608	0.002542439	0.001198017	0.005740456	0.00923				

Table 5: Hourly composite emission rate for each road link

L118	PD		50	04:00 ~ 05:00	150	0.351703845	0.088285283	0.439989128	0.708092104	0.007056493	0.001454171	0.008510664	0.013696552	0.006476468	0.001258572	0.00773504	0.01244831
L119	PD		50	04:00 ~ 05:00	100	0.451379747	0.119195232	0.570574979	0.918249136	0.011368687	0.002123937	0.013260084	0.021341142	0.010218694	0.001803007	0.012021701	0.019347004
L120	LD		50	04:00 ~ 05:00	50	0.205417	0.046387819	0.005231	0.009092386	0.00052531	0.003425386	0.00051261	0.002322	0.000840766	0.003162766	0.000508966	0.005089964
L121	LD		50	04:00 ~ 05:00	50	0.298806454	0.077308966	0.37611542	0.60529759	0.002657548	0.00157063	0.003814611	0.006139007	0.002435221	0.001013037	0.003448258	0.00554942
L122	LD		100	04:00 ~ 05:00	100	0.325139552	0.068664467	0.393804019	0.633764561	0.002376504	0.000707685	0.00308419	0.000496351	0.002180285	0.000626115	0.0028064	0.004516451
L123	LD		50	04:00 ~ 05:00	50	0.205417	0.046387819	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00051261	0.002322	0.000840766	0.003162766	0.000508966
L124	LD		50	04:00 ~ 05:00	50	0.205417	0.046387819	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00051261	0.002322	0.000840766	0.003162766	0.000508966
L125	LD		50	04:00 ~ 05:00	50	0.205417	0.046387819	0.251804819	0.405239567	0.0025231	0.000902286	0.003425386	0.00051261	0.002322	0.000840766	0.003162766	0.000508966
L126	PD		100	04:00 ~ 05:00	100	0.451133787	0.113293833	0.56442762	0.908355946	0.011099213	0.001962408	0.013061622	0.02102059	0.010186834	0.001668399	0.011855233	0.019079101
L127	OD		50	04:00 ~ 05:00	50	0.1794802	0.042778334	0.222258534	0.35768955	0.002855	0.001046651	0.003901651	0.006279084	0.0026266	0.000975289	0.003601889	0.005796644
L128	LD		50	04:00 ~ 05:00	50	0.290488204	0.076406595	0.366894799	0.590458475	0.002728579	0.001193155	0.003921733	0.006311402	0.002500929	0.001046668	0.003547597	0.005709289
L129	LD		50	04:00 ~ 05:00	50	0.332290523	0.071323288	0.403613811	0.649951851	0.001757731	0.000650714	0.002408445	0.003876007	0.001610174	0.00005378	0.002189594	0.003514724
L130	LD		100	04:00 ~ 05:00	100	0.360124577	0.072725137	0.432848707	0.696602347	0.002056828	0.000545274	0.002602102	0.004187666	0.001885327	0.000474777	0.002360104	0.00379821
L131	LD		50	04:00 ~ 05:00	50	0.290488204	0.076406595	0.366894799	0.590458475	0.002728579	0.001193155	0.003921733	0.006311402	0.002500929	0.001046668	0.003547597	0.005709289
L132	PD		150	04:00 ~ 05:00	150	0.371267074	0.118705784	0.489972859	0.78853292	0.006810973	0.002069778	0.00888075	0.014292146	0.006241267	0.00175625	0.007998893	0.012872938
L133	OD		100	04:00 ~ 05:00	100	0.2957514	0.070505196	0.366256596	0.58943139	0.0030717	0.001031626	0.004103326	0.006063646	0.0028156	0.00091206	0.00372766	0.005999073
L134	OD		100	04:00 ~ 05:00	100	0.3111635	0.071657306	0.382820806	0.61608836	0.0030597	0.000908086	0.00402056	0.006470448	0.00288044	0.000845744	0.003650144	0.005874322
L135	LD		50	04:00 ~ 05:00	50	0.333037522	0.05510522	0.388142742	0.624653641	0.00291042	0.000405507	0.0033315927	0.002676266	0.000357605	0.00305187	0.004911497	0.009746665
L136	PD		100	04:00 ~ 05:00	100	0.37362696	0.09290791	0.46653487	0.750813227	0.00907049	0.001640233	0.010710729	0.017237194	0.008325751	0.001415915	0.009744166	0.015677652
L137	PD		200	04:00 ~ 05:00	200	0.33580887	0.085095745	0.420905632	0.67738027	0.006241077	0.00137158	0.007612657	0.012251353	0.003189253	0.001189253	0.006915888	0.01131625
L138	LD		50	04:00 ~ 05:00	50	0.311922224	0.068616175	0.380538399	0.612415667	0.002735585	0.000759889	0.002994574	0.004819288	0.002048585	0.000674672	0.00273257	0.004382647
L139	LD		50	04:00 ~ 05:00	50	0.365799169	0.085430305	0.451229474	0.726181641	0.001658784	0.00083224	0.002491025	0.004008905	0.001516401	0.0007710361	0.002726762	0.003583618
L140	LD		50	04:00 ~ 05:00	50	0.242545675	0.086308065	0.32885374	0.529237478	0.00227658	0.001691002	0.003967582	0.006385188	0.002080235	0.001480258	0.0035660493	0.005730043
L141	PD		150	04:00 ~ 05:00	150	0.353582988	0.089053357	0.442636444	0.712352374	0.006761729	0.001406994	0.013146252	0.006205242	0.002124361	0.0007419603	0.011946064	0.01946664
L142	LD		50	04:00 ~ 05:00	50	0.298363291	0.066811433	0.365174724	0.58769029	0.002522452	0.000831172	0.003353623	0.00539712	0.002311909	0.000741933	0.003053842	0.004914671
L143	LD		100	04:00 ~ 05:00	100	0.312918899	0.080870158	0.393789057	0.637404081	0.001988253	0.001640001	0.004928208	0.001826883	0.000927071	0.002753955	0.00443205	0.00758297
L144	LD		50	04:00 ~ 05:00	50	0.291699442	0.080821865	0.372521307	0.59951344	0.001539024	0.001153004	0.002654328	0.004271717	0.00140355	0.000975628	0.002379178	0.003828807
L145	LD		50	04:00 ~ 05:00	50	0.27646508	0.055411529	0.331876609	0.534102302	0.00151386	0.000541371	0.002055231	0.003707566	0.0013932	0.00050446	0.00189766	0.00305398
L146	PD		100	04:00 ~ 05:00	100	0.376127561	0.09290791	0.469035471	0.754837545	0.009274899	0.001640233	0.010915132	0.017566159	0.008515457	0.001415915	0.009931371	0.015982953
L147	PD		150	04:00 ~ 05:00	150	0.357631432	0.092552534	0.450183966	0.077383946	0.007738946	0.001515624	0.014322433	0.006776539	0.001304729	0.000801368	0.013005508	0.020530508
L148	LD		50	04:00 ~ 05:00	50	0.298363291	0.066811433	0.365174724	0.58769029	0.002522452	0.000831172	0.003353623	0.00539712	0.002311909	0.000741933	0.003053842	0.004914671
L149	LD		100	04:00 ~ 05:00	100	0.319056904	0.080668711	0.399725615	0.643294422	0.002201652	0.001047372	0.003249024	0.005228784	0.002013782	0.000911201	0.002924982	0.004707291
L150	LD		50	04:00 ~ 05:00	50	0.319906697	0.091818876	0.411725573	0.662606433	0.002590339	0.001371847	0.003962186	0.006376504	0.0023632	0.00118136	0.003545518	0.005704402
L151	LD		100	04:00 ~ 05:00	100	0.295108076	0.077961598	0.373069674	0.002494715	0.001155646	0.000563061	0.005874672	0.002283826	0.001012093	0.003295918	0.005304253	0.009746665
L152	LD		50	04:00 ~ 05:00	50	0.285165732	0.076156856	0.361322588	0.581490894	0.002741443	0.001227829	0.003969272	0.006387908	0.002509615	0.001079354	0.003588869	0.005775871
L153	LD		100	04:00 ~ 05:00	100	0.29306281	0.076406595	0.369469404	0.594601891	0.00193155	0.000426553	0.006480092	0.002596377	0.001046668	0.003643044	0.005682897	0.01131625
L154	LD		50	04:00 ~ 05:00	50	0.304838895	0.094249404	0.3992683	0.642558445	0.001475971	0.001366179	0.00284215	0.004573986	0.00133897	0.00177582	0.002516551	0.004049897
L155	LD		50	04:00 ~ 05:00	50	0.286976194	0.088612285	0.375588479	0.604449562	0.002886637	0.00154947	0.004438107	0.007142423	0.002640229	0.001347624	0.003987853	0.006417811
L156	LD		50	04:00 ~ 05:00	50	0.310593589	0.083528978	0.394122567	0.634277213	0.001220071	0.00100703	0.002227101	0.003584163	0.0011127	0.000874736	0.001987436	0.00319846
L157	LD		50	04:00 ~ 05:00	50	0.303447806	0.10532983	0.408777636	0.57682201	0.001563146	0.001725328	0.003288475	0.005292274	0.001415622	0.001480427	0.002896049	0.004606728
L158	LD		50	04:00 ~ 05:00	50	0.323712053	0.061728126	0.385440179	0.620304298	0.000842715	0.000288731	0.001131447	0.001820883	0.000775548	0.000269045	0.001044593	0.001681106
L159	PD		100	04:00 ~ 05:00	100	0.376127561	0.09290791	0.469035471	0.754837545	0.009274899	0.001640233	0.010915132	0.017566159	0.008515457	0.001415915	0.009931371	0.015982953
L160	PD		100	04:00 ~ 05:00	100	0.3853637	0.09269624	0.478033325	0.769318151	0.00963174	0.001622187	0.011253927	0.018111395	0.008843114	0.0013991	0.010242214	0.016483204
L161	PD		100	04:00 ~ 05:00	100	0.466167587	0.100148933	0.911395828	0.156631652	0.011395828	0.001640767	0.013821959	0.022244232	0.01186049	0.001400129	0.012586178	0.02025544
L162	LD		100	04:00 ~ 05:00	100	0.301163874	0.079516601	0.380680475	0.612644315	0.002159616	0.00118138	0.003277754	0.00527502	0.001975683	0.000977517	0.0029532	0.004752703
L163	LD		100	04:00 ~ 05:00	100	0.311489163	0.075419945	0.386909108	0.62268304	0.001807569	0.000884426	0.001653945	0.002775649	0.002429594	0.000775649	0.00249594	0.003910042
L164	LD		50	04:00 ~ 05:00	50	0.146091853	0.03916885	0.185260703	0.298147461	0.003365815	0.001191017	0.004556832	0.007333493	0.003097548	0.001109811	0.004207359	0.006771072
L165	LD		50	04:00 ~ 05:00	50	0.156778722	0.239816401	0.369595124	0.638256396	0.005084761	0.006683	0.011776762	0.01893833	0.004580297	0.005686464	0.010266761	0.016522709
L166	LD		100	04:00 ~ 05:00	100	0.31166459	0.076072577	0.387737167	0.624000932	0.001752328	0.000883009	0.002635338	0.004241154	0.001603744	0.000774704	0.002378448	0.003827732
L167	LD		100	04:00 ~ 05:00	100	0.295687546	0.079065415	0.374752962	0.603104932	0.002190277	0.001136184	0.00332646	0.005353406	0.002004239	0.000994333	0.00298571	0.004825721
L168	OD		50	04:00 ~ 05:00	50	0.2654934	0.089015178	0.35450857									

Table 5: Hourly composite emission rate for each road link

L004	DD	50	05:00 ~ 06:00	150	0.4800446	0.072775257	0.552819857	0.889675108	0.0091707	0.00096453	0.01013523	0.016311031	0.0084273	0.000843803	0.009271103	0.014920357
L005	DD	50	05:00 ~ 06:00	150	0.5692041	0.072138803	0.641342903	1.032138788	0.0114797	0.000763937	0.012243637	0.019704176	0.0105561	0.000667902	0.011224002	0.018063235
L006	LD	50	05:00 ~ 06:00	50	0.639683818	0.064805972	0.704489789	1.133763597	0.0136585	0.000563786	0.013729636	0.022095652	0.01210824	0.000491001	0.012599241	0.020276462
L007	LD	50	05:00 ~ 06:00	100	0.566178191	0.067920539	0.63409873	1.02048045	0.01499308	0.000652598	0.012151905	0.019556547	0.010578163	0.000574133	0.011152296	0.017947836
L008	LD	50	05:00 ~ 06:00	50	0.619999072	0.056921331	0.676920403	1.089395081	0.013901788	0.000325756	0.014227544	0.022899555	0.0127935	0.000301493	0.013094993	0.021074295
L009	LD	50	05:00 ~ 06:00	50	0.417845201	0.083575452	0.501420653	0.806956313	0.010343658	0.00167909	0.012022748	0.019348689	0.009502908	0.001466629	0.010969537	0.017653715
L010	LD	50	05:00 ~ 06:00	50	0.3833666	0.068947094	0.452313694	0.727926521	0	0	0	0	0	0	0	0
L011	LD	50	05:00 ~ 06:00	50	0.648867044	0.05601896	0.704886004	1.134401242	0.015649629	0.000361847	0.016011476	0.025767909	0.014402	0.000335123	0.014737123	0.023717041
L012	LD	50	05:00 ~ 06:00	50	0.494740771	0.035356283	0.530097054	0.853106393	0.014737935	0.000902286	0.015640221	0.025170433	0.013553824	0.000840766	0.01439459	0.023165789
L013	LD	50	05:00 ~ 06:00	50	0.435943943	0.04923733	0.485181274	0.780821631	0.008092488	0.000480524	0.008573011	0.01379689	0.00744526	0.000444755	0.007890015	0.012697136
L014	DD	50	05:00 ~ 06:00	200	0.4702724	0.074536522	0.544808922	0.87678279	0.0086769	0.000980492	0.009657392	0.015542027	0.0079735	0.000856065	0.008829565	0.014209772
L015	DD	50	05:00 ~ 06:00	50	0.485574	0.073813533	0.559387533	0.900244732	0.0071626	0.00054244	0.00770504	0.012400029	0.0065843	0.000472888	0.007057188	0.011357415
L016	LD	50	05:00 ~ 06:00	50	0.304435965	0.059021013	0.363456978	0.584925853	0.001123054	0.000397006	0.001520059	0.002446292	0.001033632	0.000369937	0.001403569	0.00225882
L017	LD	50	05:00 ~ 06:00	50	0.648867044	0.05601896	0.704886004	1.134401242	0.015649629	0.000361847	0.016011476	0.025767909	0.014402	0.000335123	0.014737123	0.023717041
L018	LD	50	05:00 ~ 06:00	50	0.417845201	0.083575452	0.501420653	0.806956313	0.010343658	0.00167909	0.012022748	0.019348689	0.009502908	0.001466629	0.010969537	0.017653715
L019	LD	50	05:00 ~ 06:00	50	0.680512755	0.10555571	0.786068326	1.2650512	0.018577375	0.002196519	0.020773893	0.033432257	0.017065824	0.001884871	0.018950695	0.030498111
L020	LD	50	05:00 ~ 06:00	100	0.509212581	0.07435532	0.5835679	0.939159165	0.010674949	0.00113665	0.012011599	0.019330747	0.009996109	0.000993741	0.01098985	0.017686406
L021	DD	50	05:00 ~ 06:00	150	0.47193709	0.070369502	0.549740402	0.884719218	0.0084792	0.000797637	0.009276837	0.014929584	0.0077934	0.000698644	0.008492044	0.013666585
L022	DD	50	05:00 ~ 06:00	100	0.4661758	0.069371076	0.535546876	0.861877009	0.0076834	0.000651181	0.008333581	0.013411565	0.0070634	0.000573188	0.007636588	0.012289867
L023	LD	50	05:00 ~ 06:00	100	0.708231775	0.063048032	0.771279807	1.24125444	0.01392484	0.000708152	0.018100996	0.029130656	0.016002523	0.000625523	0.016625795	0.027656525
L024	LD	50	05:00 ~ 06:00	50	0.23846867	0.06607762	0.304546291	0.490118527	0.00504139	0.001556903	0.006598293	0.01618896	0.004627195	0.00138463	0.006012058	0.009675446
L025	DD	50	05:00 ~ 06:00	50	1.0543984	0.036976402	1.091374802	1.756393124	0.0374779	0.000670024	0.038147924	0.06139279	0.0344844	0.000619209	0.005103609	0.056493642
L026	DD	50	05:00 ~ 06:00	50	1.165471	0.0112390842	1.177861842	1.895580177	0.0363459	0.000938377	0.037284277	0.06003078	0.0334509	0.000874397	0.034315297	0.05522498
L027	DD	50	05:00 ~ 06:00	50	0.5650817	0.061746736	0.626828436	1.008780075	0.0128599	0.000747077	0.013606977	0.02189252	0.0118261	0.000661043	0.012487143	0.020096059
L028	DD	50	05:00 ~ 06:00	50	0.5813085	0.061746736	0.643055236	1.034894514	0.0136203	0.000747077	0.014367377	0.023121994	0.0125254	0.000661043	0.013186443	0.02212147
L029	DD	50	05:00 ~ 06:00	50	0.129508612	0.036461737	0.16597035	0.267102722	0.003611983	0.001299291	0.004911274	0.00790391	0.003324384	0.001210703	0.004535087	0.007298497
L030	LD	50	05:00 ~ 06:00	200	0.4188076	0.075044064	0.493851664	0.794775237	0.0069665	0.000999721	0.007966221	0.012820358	0.0063988	0.00087412	0.00727292	0.01704602
L031	DD	50	05:00 ~ 06:00	150	0.3087688	0.072040811	0.380809611	0.612852139	0.0023635	0.000789577	0.003153077	0.005074372	0.0021648	0.000691976	0.002856776	0.004957542
L032	DD	50	05:00 ~ 06:00	50	0.5581777	0.07435532	0.581796069	1.01796069	0.0124738	0.00113665	0.01361045	0.021903842	0.0114654	0.000993741	0.012459141	0.020050994
L033	LD	50	05:00 ~ 06:00	50	0.245843187	0.111983544	0.357826731	0.575864871	0.005815098	0.002851968	0.008667066	0.013948256	0.005514335	0.002463612	0.007777947	0.012517361
L034	LD	50	05:00 ~ 06:00	50	0.249505474	0.090182692	0.339688166	0.546673753	0.005407074	0.002133669	0.007540743	0.012135619	0.004951193	0.006809114	0.010958114	0.010958114
L035	LD	50	05:00 ~ 06:00	50	0.0278232	0.023828543	0.051651743	0.083125216	0.0050588	0.001804571	0.006863371	0.011045498	0.004656	0.001681532	0.006337532	0.010199244
L036	LD	50	05:00 ~ 06:00	50	0.0278232	0.023828543	0.051651743	0.083125216	0.0050588	0.001804571	0.006863371	0.011045498	0.004656	0.001681532	0.006337532	0.010199244
L037	LD	50	05:00 ~ 06:00	50	0.249505474	0.090182692	0.339688166	0.546673753	0.005407074	0.002133669	0.007540743	0.012135619	0.004951193	0.006809114	0.010958114	0.010958114
L038	LD	50	05:00 ~ 06:00	50	0.237885307	0.077880417	0.317657224	0.508174411	0.005051775	0.00187996	0.006931736	0.01155519	0.004627953	0.001654078	0.006282032	0.010109925
L039	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L040	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L041	LD	50	05:00 ~ 06:00	50	0.264955138	0.038556233	0.303511371	0.488452989	0.0068974	0.000919288	0.007816688	0.012579709	0.006342775	0.000852101	0.007194876	0.011579002
L042	LD	50	05:00 ~ 06:00	50	0.245010098	0.041166762	0.286176859	0.46055867	0.005429752	0.000913621	0.006343373	0.010208643	0.004993811	0.000848323	0.005842133	0.009401979
L043	LD	50	05:00 ~ 06:00	50	0.730266295	0.063330445	0.793596741	1.277166979	0.017066684	0.000666393	0.017373077	0.02853855	0.015698862	0.000588114	0.016286977	0.026211283
L044	LD	50	05:00 ~ 06:00	50	0.505844433	0.060821458	0.566665891	0.911958085	0.01190167	0.00088609	0.012790279	0.020583907	0.010945606	0.000793677	0.011739282	0.018892497
L045	LD	50	05:00 ~ 06:00	50	0.33528782	0.077353076	0.412640896	0.664079499	0.00793761	0.001735595	0.009673205	0.015567475	0.00728528	0.001519556	0.008804836	0.014169974
L046	LD	50	05:00 ~ 06:00	50	0.220211733	0.041972548	0.262184281	0.421943651	0.004323064	0.000980136	0.0053032	0.008534652	0.003976762	0.000911806	0.004888567	0.007863767
L047	DD	50	05:00 ~ 06:00	200	0.3991092	0.076563588	0.475672788	0.76519245	0.0060749	0.001098531	0.007173431	0.01154449	0.0055779	0.000958239	0.006536139	0.010518871
L048	DD	50	05:00 ~ 06:00	50	0.5916641	0.085755224	0.109018014	0.14326	0.0142645	0.00112645	0.007157525	0.025351048	0.0131662	0.014397414	0.023170335	0.010958114
L049	DD	50	05:00 ~ 06:00	100	0.4688345	0.065595802	0.534430302	0.860080063	0.0082401	0.00073357	0.008973467	0.014441686	0.0075757	0.000650098	0.008225798	0.013238105
L050	DD	50	05:00 ~ 06:00	100	0.3695122	0.070112266	0.439624466	0.707055239	0.0062124	0.007690534	0.012376684	0.007029236	0.00060826	0.000946636	0.007029236	0.01131243
L051	DD	50	05:00 ~ 06:00	50	0.4720187	0.066046988	0.538065688	0.865930634	0.0081951	0.000715525	0.008910625	0.014340224	0.0075343	0.000633282	0.008167582	0.013144417
L052	DD	50	05:00 ~ 06:00	50	0.385292	0.06054065	0.44583265	0.717496316	0.006292	0.00087293	0.00716493	0.01530809	0.005782	0.000779342	0.006561342	0.010559543
L053	DD	50	05:00 ~ 06:00	150	0.4390344	0.063862421	0.502896821	0.80933197	0.0079766	0.000809311	0.008785911	0.014139518	0.0073337	0.000720378	0.008054078	0.012961749
L054	DD	50	05:00 ~ 06:00	100	0.4699991	0.063029093	0.533028193	0.857823593	0.0097088	0.000849684	0.010558484	0.01699219	0.0089288	0.000758157	0.009686957	0.015589607
L055	LD	50	05:00 ~ 06:00	50	0.241229441	0.089280321	0.330509762	0.531902581	0.005382837	0.002169761	0.007552597	0.012154697	0.004929914	0.001891552	0.006821466	0.010978058
L056	LD	50</														

Table 5: Hourly composite emission rate for each road link

L086	LD	50	05:00~06:00	50	0.134087573	0.119081317	0.253168889	0.407434821	0.004351865	0.003560696	0.007912561	0.0121734002	0.003962579	0.003095207	0.007057786	0.011358377
L087	LD	50	05:00~06:00	100	0.447260388	0.079037882	0.52629827	0.846992858	0.009198882	0.00136394	0.010562822	0.016999171	0.008449798	0.001189387	0.009639185	0.015512726
L088	LD	50	05:00~06:00	50	0.270318105	0.14839164	0.673262993	0.418347268	0.002548638	0.003198017	0.005746655	0.009248322	0.002295752	0.00272544	0.005021192	0.008080086
L089	LD	50	05:00~06:00	50	0.388817387	0.064626705	0.453444092	0.729745714	0.005993919	0.000764656	0.006363976	0.0102418	0.005143627	0.00067845	0.005822077	0.009369701
L090	LD	50	05:00~06:00	100	0.500063153	0.062854833	0.562917986	0.905926432	0.010831111	0.000802165	0.018721895	0.009959573	0.000713025	0.010672598	0.017175839	0.015804964
L091	LD	50	05:00~06:00	50	0.485382975	0.035673901	0.521056876	0.838557674	0.01337458	0.00080818	0.01418276	0.022824883	0.012307608	0.00074932	0.013056928	0.021013036
L092	LD	50	05:00~06:00	50	0.493899246	0.067115148	0.561014394	0.902862905	0.00996614	0.000741409	0.010707549	0.01732088	0.009163509	0.000657265	0.009820773	0.015804964
L093	LD	50	05:00~06:00	50	0.292427691	0.094694547	0.387122238	0.623011303	0.00465437	0.01953212	0.006607582	0.010633845	0.00425945	0.001689768	0.005949218	0.009574314
L094	LD	50	05:00~06:00	50	0.269386172	0.049191516	0.318577669	0.512699817	0.003164684	0.006991405	0.006064761	0.002911093	0.003553854	0.000642761	0.005719359	0.005719359
L095	LD	50	05:00~06:00	100	0.362842118	0.057238326	0.420080444	0.676052262	0.005816941	0.000758454	0.006575396	0.010582047	0.005346866	0.000690458	0.006037324	0.009716106
L096	LD	50	05:00~06:00	50	0.56423803	0.050250248	0.614488278	0.988920565	0.013582993	0.005598884	0.014142876	0.022760696	0.012501098	0.000520683	0.013021781	0.020956473
L097	LD	50	05:00~06:00	50	0.528962531	0.057637774	0.586600306	0.944039336	0.013372526	0.000960792	0.014333318	0.023067182	0.012298524	0.000860938	0.013159462	0.0221178049
L098	LD	50	05:00~06:00	50	0.290603178	0.129465711	0.420068888	0.676033665	0.009224047	0.00354551	0.012769557	0.020550559	0.008432388	0.003043229	0.011475617	0.018468169
L099	LD	50	05:00~06:00	100	0.469670912	0.065521273	0.535192184	0.86130619	0.010290132	0.001125443	0.014115575	0.018371541	0.0094615	0.00100047	0.010461971	0.016836868
L100	LD	50	05:00~06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L101	LD	50	05:00~06:00	50	0.593345305	0.071786537	0.665131843	1.07042328	0.014954761	0.00162732	0.016582082	0.026686207	0.013742752	0.001418664	0.015161416	0.0243999872
L102	LD	50	05:00~06:00	50	0.25628516	0.098875483	0.355180643	0.571574229	0.0065412	0.002551744	0.009072944	0.014601451	0.00598611	0.000818296	0.003168258	0.013168258
L103	LD	50	05:00~06:00	50	0.342702186	0.046433693	0.389135818	0.636251838	0.006435108	0.000691405	0.007126513	0.011468982	0.005918225	0.000642761	0.005650986	0.010558857
L104	PD	50	05:00~06:00	150	0.348935184	0.07527846	0.424213645	0.682703987	0.005416747	0.01104026	0.006520773	0.010494141	0.004973811	0.000936024	0.005936834	0.009554385
L105	PD	50	05:00~06:00	100	0.3823125	0.103010382	0.485322882	0.781049527	0.005342623	0.0164719	0.006988812	0.01124895	0.004989782	0.00094462	0.010147103	0.010147103
L106	LD	50	05:00~06:00	50	0.350211008	0.110967275	0.461167733	0.74217568	0.009363059	0.002776951	0.01214001	0.019537404	0.008575926	0.000299462	0.010970387	0.017655083
L107	LD	50	05:00~06:00	100	0.481118201	0.069631416	0.550749617	0.886343369	0.009468085	0.009598492	0.010365798	0.016681288	0.008647085	0.000843263	0.009490347	0.015273196
L108	LD	50	05:00~06:00	50	0.477788888	0.065881198	0.538680086	0.866919409	0.007051036	0.000653548	0.007704584	0.013999296	0.006479871	0.000575669	0.00709554	0.011354761
L109	LD	50	05:00~06:00	50	0.503646933	0.069035314	0.572682248	0.921640449	0.009798836	0.001157063	0.010955899	0.017631766	0.001013037	0.010180818	0.016122396	0.016122396
L110	LD	50	05:00~06:00	50	0.351460284	0.108154731	0.459615016	0.739976829	0.005774816	0.00219842	0.007973236	0.012831467	0.005280555	0.001887943	0.007168498	0.01153655
L111	LD	50	05:00~06:00	50	0.517116012	0.064620044	0.581736056	0.936211105	0.011743793	0.001234914	0.012978706	0.020887151	0.010792751	0.001084077	0.019113853	0.019113853
L112	LD	50	05:00~06:00	100	0.514080594	0.060259756	0.57506835	0.925480498	0.011501944	0.000902404	0.012404348	0.019962814	0.010579901	0.000807658	0.011387559	0.018326454
L113	LD	50	05:00~06:00	100	0.470532162	0.063801662	0.534333824	0.859924796	0.010775508	0.001147739	0.011923247	0.009188559	0.009074778	0.00102012	0.010927597	0.017586219
L114	LD	50	05:00~06:00	50	0.487968128	0.078681247	0.566649374	0.911931504	0.011366918	0.00162732	0.012994238	0.020912147	0.010440543	0.001418664	0.011859206	0.019085495
L115	LD	50	05:00~06:00	50	0.225879973	0.027824365	0.253704338	0.40829654	0.009204809	0.001249778	0.010454587	0.016824985	0.008464635	0.001158555	0.009623191	0.015486986
L116	LD	50	05:00~06:00	50	0.19390826	0.019799611	0.213707871	0.343928625	0.01103584	0.001471995	0.012507835	0.020129358	0.01014844	0.001364118	0.011512558	0.018527619
L117	LD	50	05:00~06:00	50	0.324315375	0.030321755	0.570731718	0.35463731	0.0012654	0.000903033	0.012168433	0.019583146	0.01035755	0.000831695	0.01189245	0.018007299
L118	PD	50	05:00~06:00	200	0.358743241	0.082374972	0.441118213	0.709099185	0.005837756	0.001260769	0.007089525	0.01142394	0.005358822	0.001093319	0.006452141	0.010383689
L119	PD	50	05:00~06:00	100	0.391728999	0.117224343	0.508953342	0.819078972	0.005901951	0.001952202	0.007854153	0.012640003	0.005408179	0.001658751	0.00706693	0.011373093
L120	LD	50	05:00~06:00	50	0.265164149	0.041666239	0.306830389	0.493794418	0.005476599	0.000844272	0.006320871	0.01017243	0.00503658	0.000782951	0.005819331	0.009365604
L121	LD	50	05:00~06:00	50	0.447839511	0.068869893	0.516736494	0.83160471	0.00774667	0.000774667	0.008687091	0.013980483	0.007275389	0.000689006	0.007964396	0.012817421
L122	LD	50	05:00~06:00	100	0.470486995	0.061737431	0.532224426	0.856530057	0.007910947	0.000517904	0.008428851	0.013564887	0.007275434	0.000465044	0.007740479	0.012457062
L123	LD	50	05:00~06:00	50	0.256026999	0.042971504	0.298998503	0.48119025	0.005022795	0.000841438	0.005864233	0.009437544	0.0004619517	0.0005400579	0.008691368	0.008691368
L124	LD	50	05:00~06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L125	LD	50	05:00~06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L126	PD	50	05:00~06:00	100	0.391518167	0.111322945	0.502841112	0.809242315	0.005869733	0.001790673	0.007660404	0.012328198	0.005380917	0.001524144	0.006095056	0.011112591
L127	PD	50	05:00~06:00	50	0.160375	0.040973592	0.201348592	0.324038344	0.003125	0.00118834	0.004243834	0.006829772	0.002875	0.00104255	0.003917651	0.00630467
L128	LD	50	05:00~06:00	50	0.525174066	0.065236729	0.590410795	0.950171708	0.011387994	0.000810759	0.012198752	0.01963194	0.010473739	0.000722637	0.011196376	0.018018776
L129	LD	50	05:00~06:00	100	0.511544329	0.068931344	0.580475673	0.934182719	0.009089759	0.000616506	0.009706265	0.015620681	0.008359209	0.000540502	0.008899712	0.014322662
L130	LD	50	05:00~06:00	100	0.515737395	0.070736086	0.586473481	0.943835231	0.008388127	0.000544323	0.00893245	0.01437535	0.007173843	0.000473241	0.008187084	0.013175802
L131	LD	50	05:00~06:00	50	0.525174066	0.065236729	0.590410795	0.950171708	0.011387994	0.000810759	0.012198752	0.01963194	0.010473739	0.000722637	0.011196376	0.018018776
L132	PD	50	05:00~06:00	200	0.404171574	0.10647309	0.512644664	0.825019564	0.00662969	0.001776169	0.008405859	0.013527886	0.006079664	0.00150969	0.007589354	0.012212835
L133	DD	50	05:00~06:00	150	0.4216168	0.06577168	0.48738848	0.784373776	0.00077554	0.000937721	0.008693121	0.013990187	0.00712627	0.000829018	0.007955718	0.012803455
L134	DD	50	05:00~06:00	100	0.4419159	0.070668507	0.512584407	0.82492259	0.008477	0.000934024	0.009411024	0.015145538	0.0077902	0.000820225	0.008610425	0.013857101
L135	LD	50	05:00~06:00	50	0.347528408	0.053799556	0.401328364	0.645873789	0.004137888	0.000408341	0.004546228	0.003716427	0.003804635	0.000377494	0.004182129	0.006730468
L136	PD	50	05:00~06:00	150	0.314615349	0.091492094	0.406107443	0.653564952	0.004515298	0.001570861	0.006086159	0.0097947	0.004138785	0.001355607	0.005494393	0.008428346
L137	PD	50	05:00~06:00	200	0.317368732	0.078980798	0.39634953									

Table 5: Hourly composite emission rate for each road link

L168	DD	50	05:00 ~ 06:00	50	0.3669476	0.07274634	0.43969394	0.707617045	0.0066165	0.001229246	0.007845746	0.012626473	0.0060708	0.001080298	0.007151098	0.011508549
L169	DD	50	05:00 ~ 06:00	50	0.4198416	0.085950986	0.505792586	0.81399224	0.0074881	0.001446863	0.008934963	0.014379394	0.0068724	0.001250511	0.008122911	0.013072525
L170	LD	50	05:00 ~ 06:00	50	0.422171893	0.074551082	0.496722975	0.799396153	0.007538449	0.001157063	0.006895512	0.013994035	0.006922776	0.001013037	0.007935813	0.012771421
L171	LD	50	05:00 ~ 06:00	50	0.264970648	0.053606787	0.318577435	0.512699409	0.00168458	0.000613554	0.002298135	0.003698488	0.001550448	0.000571721	0.002122169	0.003415291
L172	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L173	LD	50	05:00 ~ 06:00	100	0.380213131	0.08336104	0.463574171	0.746048457	0.004498538	0.001226947	0.005725485	0.009214252	0.004125319	0.001062623	0.005187942	0.008349163
L174	LD	50	05:00 ~ 06:00	100	0.440363795	0.081733541	0.522097337	0.840232128	0.007962254	0.001308386	0.009270664	0.014919611	0.007312518	0.001137996	0.008450514	0.013595971
L175	LD	50	05:00 ~ 06:00	100	0.491291094	0.06438265	0.555673744	0.894267983	0.009869558	0.000795547	0.011637779	0.009076944	0.000077711	0.009759028	0.007584655	0.015746636
L176	LD	50	05:00 ~ 06:00	50	0.37873812	0.08459137	0.458332257	0.737612435	0.006064224	0.001552304	0.007616528	0.012257583	0.005559967	0.001349513	0.006090948	0.011119702
L177	LD	50	05:00 ~ 06:00	50	0.22160525	0.341547746	0.563062996	0.906159803	0.0051164	0.008978732	0.014095132	0.022683859	0.004559945	0.007571138	0.012130588	0.019522244
L178	LD	50	05:00 ~ 06:00	50	0.407382125	0.505050588	0.913032713	1.469380066	0.0038805	0.01195314	0.01583364	0.025481711	0.003347175	0.00996095	0.013008125	0.021417298
L179	LD	50	05:00 ~ 06:00	50	0.37903598	0.083243873	0.462279852	0.743965458	0.006801359	0.001555138	0.008356497	0.013448445	0.006238881	0.001351402	0.007590283	0.012215346
L180	PD	50	05:00 ~ 06:00	150	0.287946945	0.094129596	0.38207654	0.614891059	0.003694832	0.001721443	0.005416275	0.008716628	0.003381594	0.001485408	0.004867003	0.007832662
L181	PD	70	05:00 ~ 06:00	300	0.331055458	0	0.331055458	0.53278079	0.004157867	0	0.004157867	0.006691421	0.003809435	0	0.003809435	0.006130676
L182	PD	70	05:00 ~ 06:00	450	0.32779291	0	0.32779291	0.527530241	0.004357838	0	0.004357838	0.007013244	0.003996504	0	0.003996504	0.006431733
L183	PD	50	05:00 ~ 06:00	100	0.253186471	0.093707897	0.346894369	0.558270983	0.00243913	0.001644484	0.004083613	0.008571922	0.002225404	0.001418749	0.003644152	0.00586468
L184	PD	50	05:00 ~ 06:00	150	0.395323669	0.092922892	0.488246561	0.785754721	0.007039068	0.0014355	0.008474568	0.013638461	0.006460467	0.001228382	0.007688849	0.012373972
L185	DD	50	05:00 ~ 06:00	100	0.370027	0.084704325	0.454731325	0.731817911	0.005315	0.00127294	0.006587294	0.010601196	0.0048766	0.001104366	0.005980966	0.009625408
L186	DD	50	05:00 ~ 06:00	100	0.3890601	0.067248687	0.455308787	0.732746649	0.0045584	0.000706268	0.005264668	0.008472641	0.0041861	0.00062517	0.00481127	0.007749797
L187	LD	50	05:00 ~ 06:00	50	0.24501098	0.041166762	0.286176859	0.460555867	0.005423752	0.00913621	0.006343373	0.010208643	0.000993811	0.000848323	0.005842133	0.009401897
L188	LD	50	05:00 ~ 06:00	50	0.409814273	0.037688367	0.447502664	0.720183899	0.010655027	0.00974468	0.011629495	0.018715812	0.009808547	0.000808027	0.010714675	0.01746613
L189	LD	50	05:00 ~ 06:00	50	0.146219152	0.03916885	0.185388002	0.298352328	0.00337422	0.001191017	0.004565237	0.007547018	0.003105552	0.001109811	0.004215363	0.006739393
L190	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L191	LD	50	05:00 ~ 06:00	50	0.161151975	0.040973592	0.202125567	0.325288761	0.00316175	0.001118834	0.004280584	0.00688915	0.00291	0.00104255	0.003955255	0.006360997
L192	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L193	LD	50	05:00 ~ 06:00	50	0.2055949	0.046387819	0.251982719	0.405525868	0.0025294	0.000902286	0.003431686	0.005522749	0.002328	0.000840766	0.003168766	0.005099622
L194	LD	50	05:00 ~ 06:00	50	0.215144452	0.037572728	0.25270173	0.406683002	0.005854691	0.001124848	0.006912678	0.011124848	0.005384915	0.00082845	0.00636776	0.010247891
L195	LD	50	05:00 ~ 06:00	50	0.2446264	0.08837795	0.33300435	0.535917221	0.005618486	0.002205852	0.00782438	0.01259202	0.005144745	0.001925182	0.007069927	0.011377916
L196	LD	50	05:00 ~ 06:00	50	0.22804974	0.066480513	0.294530487	0.473999694	0.004339769	0.00159016	0.005929929	0.009543272	0.003978806	0.001416605	0.00539541	0.00868305
L001	LD	50	06:00 ~ 07:00	100	0.572347141	0.071628802	0.643975943	1.036376245	0.012144178	0.000742826	0.012887005	0.020739572	0.011173642	0.000658209	0.011831852	0.019041472
L002	LD	50	06:00 ~ 07:00	100	0.681308739	0.090990214	0.772298954	1.242891598	0.014480474	0.000328657	0.014809132	0.023832928	0.01319893	0.000288187	0.013608081	0.021900029
L003	LD	50	06:00 ~ 07:00	50	0.795942726	0.037059484	0.83300221	1.340583777	0.018485363	0.000613554	0.019098917	0.030736651	0.017020163	0.000571721	0.017591884	0.028311322
L004	DD	50	06:00 ~ 07:00	350	0.8918899	0.70427079	0.962316979	1.548695207	0.0298019	0.000865472	0.030667372	0.049354228	0.000754552	0.028163052	0.045323926	0.054323926
L005	DD	50	06:00 ~ 07:00	300	1.0934103	0.071313964	1.164724264	1.874437347	0.0379599	0.000586209	0.038582109	0.062091732	0.0349516	0.000512977	0.035464577	0.057074562
L006	LD	50	06:00 ~ 07:00	100	0.822478067	0.089876264	0.912354331	1.46828832	0.019282299	0.000363332	0.019645631	0.0316165	0.017739107	0.000320874	0.018059981	0.029064649
L007	LD	50	06:00 ~ 07:00	150	0.759403635	0.081481057	0.840884693	1.353269371	0.018114707	0.000502333	0.01861704	0.029961147	0.016665666	0.000448444	0.01711051	0.027536628
L008	LD	50	06:00 ~ 07:00	50	0.769333439	0.066444616	0.835778055	1.345051055	0.018275624	0.000598977	0.018875502	0.0303771	0.016814178	0.000524631	0.017338809	0.027904309
L009	LD	50	06:00 ~ 07:00	100	0.555501315	0.07311726	0.628618576	1.011661018	0.014866607	0.001363872	0.016230478	0.026120358	0.01367436	0.001204581	0.014878941	0.023945275
L010	LD	50	06:00 ~ 07:00	50	0.2146431	0.046387819	0.261030919	0.420087498	0.00290005	0.000902286	0.003802336	0.006119251	0.002681	0.000840766	0.003521766	0.005667719
L011	LD	50	06:00 ~ 07:00	50	0.79695644	0.064639874	0.861596314	1.386601412	0.020116343	0.00067206	0.020788403	0.033455609	0.018508187	0.000591893	0.01910008	0.030738522
L012	LD	50	06:00 ~ 07:00	50	0.47319685	0.03296788	0.618458938	0.91464345	0.00916454	0.015559904	0.025041177	0.01438208	0.000850212	0.014332292	0.013026531	0.023065531
L013	LD	50	06:00 ~ 07:00	50	0.495937937	0.064907513	0.56084545	0.902591017	0.010175926	0.000780335	0.01095626	0.017632348	0.009361984	0.000692785	0.010054769	0.016181542
L014	DD	50	06:00 ~ 07:00	350	0.903291	0.086769756	0.990060756	1.59344377	0.0303079	0.000698189	0.031006089	0.04989934	0.0278755	0.00612565	0.028488065	0.045846982
L015	DD	50	06:00 ~ 07:00	150	1.480532	0.052406307	1.532938307	2.467018936	0.00596589	0.000390914	0.060049814	0.096640567	0.0548786	0.000342457	0.05521057	0.088869459
L016	LD	50	06:00 ~ 07:00	50	0.476453002	0.055860236	0.532313238	0.856672986	0.007172379	0.000363748	0.007536127	0.012128191	0.006603414	0.000338196	0.006941609	0.011714709
L017	LD	50	06:00 ~ 07:00	50	0.79695644	0.064639874	0.861596314	1.386601412	0.020116343	0.00067206	0.020788403	0.033455609	0.018508187	0.000591893	0.01910008	0.030738522
L018	LD	50	06:00 ~ 07:00	100	0.555501315	0.07311726	0.628618576	1.011661018	0.014866607	0.001363872	0.016230478	0.026120358	0.01367436	0.001204581	0.014878941	0.023945275
L019	LD	50	06:00 ~ 07:00	100	0.921431147	0.079723898	1.001155044	1.611198859	0.026577572	0.001670835	0.028428407	0.045461292	0.024439906	0.00144214	0.025882046	0.041653011
L020	LD	50	06:00 ~ 07:00	150	0.630126796	0.073719892	0.703846688	1.132728629	0.014152349	0.001088528	0.015240877	0.024527754	0.013015112	0.0009489	0.013964012	0.022472843
L021	DD	50	06:00 ~ 07:00	300	0.9455684	0.08619676	1.03176516	1.660460943	0.0321374	0.000664743	0.032802143	0.0527888	0.0295589	0.000581252	0.030140152	0.048505752
L022	DD	50	06:00 ~ 07:00	200	1.3488334	0.097489335	1.446322735	2.32762503	0.0530428	0.00044755	0.05349035	0.086084159	0.0487922	0.000392353	0.049184553	0.079154668
L023	LD	50	06:00 ~ 07:00	100	0.855449104	0.060976166	0.91642527	1.474839843	0.021							

Table 5: Hourly composite emission rate for each road link

L054	DD	00:00 ~ 07:00	150	0.7747846	0.091525274	0.866309874	1.394187133	0.0236235	0.000729379	0.024352879	0.039192062	0.0217293	0.000646055	0.022375355	0.036009553
L055	LD	00:00 ~ 07:00	50	0.282354292	0.065527372	0.347881664	0.559859876	0.005949744	0.001517977	0.007467721	0.012018103	0.00547198	0.001349344	0.006821324	0.010977829
L056	LD	00:00 ~ 07:00	50	0.256835038	0.036520197	0.079685159	0.051575414	0.005081277	0.001807778	0.006689055	0.01086832	0.004667775	0.0005186817	0.002545952	0.010067565
L057	DD	00:00 ~ 07:00	250	0.5231983	0.084038081	0.607236381	0.977249797	0.0108566	0.000722182	0.015877882	0.018648681	0.0099917	0.000639008	0.010630708	0.017108423
L058	LD	00:00 ~ 07:00	150	0.7948859	0.085869513	0.880755413	1.417434917	0.0240592	0.000593351	0.024652551	0.039674336	0.022131	0.000529747	0.022660747	0.036468847
L059	LD	00:00 ~ 07:00	50	0.97750016	0.039978456	1.017478616	1.637469035	0.029890665	0.000705182	0.030595847	0.049239121	0.027516287	0.000654023	0.028170309	0.045335606
L060	LD	00:00 ~ 07:00	50	0.761420368	0.064682183	0.826102551	1.32947988	0.022845803	0.01335619	0.024181422	0.03891613	0.021021933	0.022200051	0.035727243	0.045335606
L061	LD	00:00 ~ 07:00	50	0.0293764	0.023828543	0.053204943	0.085624842	0.0058001	0.001804571	0.007604671	0.012238502	0.005362	0.001681532	0.007043532	0.011335438
L062	LD	00:00 ~ 07:00	50	0.53456193	0.075923363	0.610485293	0.982478402	0.01169226	0.00162732	0.01331958	0.021435734	0.010750035	0.001418669	0.012168699	0.019583574
L063	LD	00:00 ~ 07:00	50	0.3999098	0.068947094	0.468856894	0.754550155	0	0	0	0	0	0	0	0
L064	LD	00:00 ~ 07:00	50	0.289813732	0.148793785	0.438607516	0.70586862	0.007185793	0.003715701	0.010901494	0.017544211	0.006557849	0.003172254	0.009730103	0.015659044
L065	DD	00:00 ~ 07:00	200	1.1490366	0.090823276	1.239859876	1.995356093	0.0439751	0.000612079	0.044587179	0.07175593	0.0404474	0.000536959	0.040984359	0.065957768
L066	DD	00:00 ~ 07:00	200	1.146142	0.098327935	1.244669935	2.002775245	0.041972	0.000574095	0.042546095	0.068471132	0.0386068	0.000501615	0.039108415	0.062938737
L067	DD	00:00 ~ 07:00	200	1.1356879	0.089858021	1.225545921	1.972320072	0.0433212	0.00061137	0.04393257	0.070702442	0.0398459	0.000536486	0.040382386	0.06498899
L068	DD	00:00 ~ 07:00	300	0.8502577	0.106151848	0.956409548	1.539188142	0.027337	0.000725366	0.028062366	0.045161889	0.0251413	0.000632224	0.025773524	0.041478363
L069	LD	00:00 ~ 07:00	150	0.570163088	0.075275594	0.645438681	1.038730287	0.011833074	0.000963589	0.01279666	0.020594177	0.0108834173	0.000843173	0.011726317	0.018871631
L070	LD	00:00 ~ 07:00	150	0.591514738	0.071942909	0.663457646	1.067728928	0.012514508	0.000831386	0.013946347	0.02147881	0.011512047	0.000731015	0.0112243062	0.019703249
L071	LD	00:00 ~ 07:00	100	0.475779586	0.067742395	0.543521981	0.874711665	0.007759988	0.000685855	0.008445845	0.013592233	0.007140068	0.000605874	0.007745942	0.012465855
L072	LD	00:00 ~ 07:00	100	0.580667797	0.062978323	0.643646119	1.035845446	0.011910565	0.000741409	0.0126591329	0.020361329	0.010959202	0.000657265	0.011618467	0.018694845
L073	LD	00:00 ~ 07:00	100	0.47650764	0.070926079	0.547483719	0.881008982	0.007059706	0.000613673	0.007678378	0.012349074	0.006494439	0.00038613	0.007039052	0.011318571
L074	LD	00:00 ~ 07:00	50	0.31438246	0.067713804	0.38205205	0.614851646	0.002519047	0.00079508	0.003314127	0.005333557	0.002316448	0.000708302	0.00302475	0.004067852
L075	DD	00:00 ~ 07:00	300	0.4640912	0.08015694	0.54424814	0.867580302	0.0063575	0.000653814	0.014502269	0.007684278	0.000580878	0.008264278	0.013300304	0.013300304
L076	DD	00:00 ~ 07:00	150	0.8131978	0.086114717	0.899312517	1.447799606	0.0250669	0.000501032	0.025567932	0.041147496	0.0230599	0.000454738	0.023514638	0.037840308
L077	LD	00:00 ~ 07:00	150	0.682261733	0.07490191	0.754751924	1.214652462	0.016544602	0.000878382	0.028039506	0.015219804	0.000774202	0.015994006	0.007774202	0.025739749
L078	DD	00:00 ~ 07:00	150	1.4328914	0.080986226	1.513877626	2.436343818	0.0595367	0.000476072	0.060012772	0.096580954	0.0547647	0.000421558	0.055186258	0.088813453
L079	DD	00:00 ~ 07:00	300	0.860506	0.107232897	0.967738897	1.557420916	0.02797	0.000725839	0.028695839	0.046181361	0.0257238	0.000632539	0.026356339	0.04241631
L080	DD	00:00 ~ 07:00	150	1.4961192	0.082114376	1.578235576	2.539914423	0.0627324	0.000464986	0.063197386	0.101706081	0.0577044	0.000410978	0.058115378	0.093527402
L081	DD	00:00 ~ 07:00	300	0.8663893	0.109350425	0.975739725	1.570236969	0.02825	0.000773666	0.046708947	0.0259802	0.000671803	0.026652003	0.042892134	0.042892134
L082	LD	00:00 ~ 07:00	50	0.260025666	0.123786341	0.383812007	0.617684015	0.006457956	0.003175026	0.009632982	0.015502743	0.005903333	0.002732827	0.008636159	0.013898517
L083	LD	00:00 ~ 07:00	50	0.257391719	0.089280321	0.34667204	0.55791318	0.00603766	0.002169761	0.008207421	0.01320853	0.005541833	0.001891552	0.007433385	0.011962843
L084	LD	00:00 ~ 07:00	50	0.0293764	0.023828543	0.053204943	0.085624842	0.0058001	0.001804571	0.007604671	0.012238502	0.005362	0.001681532	0.007043532	0.011335438
L085	LD	00:00 ~ 07:00	50	0.0293764	0.023828543	0.053204943	0.085624842	0.0058001	0.001804571	0.007604671	0.012238502	0.005362	0.001681532	0.007043532	0.011335438
L086	LD	00:00 ~ 07:00	50	0.22035446	0.12057975	0.34093421	0.548679062	0.006826018	0.003852649	0.010178667	0.016830936	0.006248089	0.002899091	0.00914718	0.014720923
L087	LD	00:00 ~ 07:00	200	0.595357965	0.101117071	0.696475036	1.120865134	0.014366956	0.001137834	0.01550479	0.024952479	0.013209471	0.000994981	0.014204552	0.022859793
L088	LD	00:00 ~ 07:00	50	0.727446146	0.101538467	0.828984612	1.334118096	0.015693205	0.002120569	0.017813774	0.028668419	0.014417139	0.001816903	0.016234042	0.026126094
L089	LD	00:00 ~ 07:00	100	0.474601016	0.070430222	0.545031238	0.887140573	0.008823572	0.000915979	0.009739551	0.015674248	0.008115412	0.00080341	0.008918821	0.014353415
L090	LD	00:00 ~ 07:00	150	0.728064631	0.13851757	0.866582201	1.3946254	0.019610689	0.000565953	0.020176641	0.032471076	0.01803528	0.000530808	0.018539088	0.029835696
L091	LD	00:00 ~ 07:00	50	0.613637166	0.068796102	0.682432367	1.098267155	0.01816206	0.00144106	0.01960312	0.031548085	0.016707709	0.01277121	0.01798483	0.028943706
L092	LD	00:00 ~ 07:00	150	0.769246577	0.165812505	0.935059081	1.504827982	0.02132444	0.000611597	0.021936037	0.030325242	0.019609354	0.000536236	0.02014559	0.032421104
L093	LD	00:00 ~ 07:00	50	0.510089826	0.070658481	0.586155667	0.94323761	0.012296486	0.001260603	0.013557089	0.021817965	0.011311876	0.001108967	0.012420843	0.01989936
L094	LD	00:00 ~ 07:00	100	0.709399191	0.188128339	0.89752753	1.444426955	0.020606838	0.000516487	0.021123325	0.033994613	0.018949773	0.0004641	0.019413873	0.031243522
L095	LD	00:00 ~ 07:00	150	0.430202888	0.092439648	0.522642536	0.881109538	0.008500926	0.00150928	0.014726955	0.0078248	0.000594828	0.000417667	0.000594828	0.008417667
L096	LD	00:00 ~ 07:00	100	0.830898674	0.167171637	0.998070311	1.606234474	0.024450647	0.000361381	0.024812028	0.039930989	0.022492524	0.000335715	0.022828239	0.03673897
L097	LD	00:00 ~ 07:00	150	0.780372024	0.943936743	1.519115158	2.033185474	0.02751551	0.023937025	0.038522812	0.021323479	0.006667216	0.002199069	0.005390504	0.03673897
L098	LD	00:00 ~ 07:00	50	0.23832096	0.094363628	0.33268458	0.535402615	0.00789106	0.002712201	0.010603261	0.017064252	0.00723899	0.00236444	0.00960343	0.015455183
L099	LD	00:00 ~ 07:00	200	0.634424629	0.18789474	0.730614104	1.175806502	0.016380648	0.000955867	0.017336514	0.027900346	0.015068436	0.000850035	0.015918471	0.025618232
L100	LD	00:00 ~ 07:00	50	0.25169644	0.050899674	0.302596114	0.468698003	0.00232004	0.000721828	0.003041868	0.004895401	0.0021448	0.000672613	0.002817413	0.004534175
L101	LD	00:00 ~ 07:00	50	0.681669869	0.07670365	0.758373518	1.220480838	0.018550699	0.00157365	0.020124348	0.032386919	0.017059049	0.001367626	0.018426675	0.029654755
L102	LD	00:00 ~ 07:00	50	0.39329265	0.072937805	0.466230455	0.75032332	0.011267087	0.001813445	0.013080532	0.021051024	0.010357225	0.001590595	0.01194782	0.019228104
L103	LD	00:00 ~ 07:00	50	0.453084687	0.05866223	0.511746917	0.823574783	0.00964806	0.00094228	0.010607086	0.017070407	0.008891631	0.009736345	0.01566909	0.01566909
L104	PD	00:00 ~ 07:00	350	0.699892043	0.108575789	0.808467832	1.301099621	0.02431861	0.000750149	0.025068758	0.040544156	0.022364005	0.000656222	0.023020227	0.037047372
L105	PD	00:00 ~ 07:00	250	0.789255296	0.143667914	0.93292321	1.501390638	0.027798093	0.00094043	0.009918523	0.046539736	0.025726495	0.000805098	0.026531592	0.042698353
L106	LD	00:00 ~ 07:00	50	0.52313699	0.159957005	0.683083995	1.099314397	0.015996396	0.002704768	0.018601164	0.029935997	0.014592124	0.0023272	0.016919324	0.027228945
L107	LD	00:00 ~ 07:00	150	0.609186492	0.068529919	0.677									

Table 5: Hourly composite emission rate for each road link

L136	PD	50	06:00 ~ 07:00	250	0.710455024	0.084053866	0.79450889	1.278634937	0.027352998	0.001120585	0.028473583	0.045823677	0.02515057	0.000972273	0.026122843	0.042040536
L137	PD	50	06:00 ~ 07:00	450	0.61833034	0.098189015	0.716519355	0.020768489	0.000970782	0.021676361	0.034884635	0.019095955	0.00079085	0.019886805	0.032004045	0.032004045
L138	LD	50	06:00 ~ 07:00	100	0.610806465	0.084509259	0.695315724	1.118999407	0.013159352	0.000676599	0.013835952	0.02226675	0.012104449	0.000597762	0.012702211	0.020442177
L139	LD	50	06:00 ~ 07:00	100	0.65443466	0.096033702	0.750468362	1.207758754	0.012084746	0.000641042	0.020480119	0.011111387	0.000548346	0.011659733	0.018764475	0.034853453
L140	LD	50	06:00 ~ 07:00	50	0.475500024	0.086148406	0.56164843	0.903883284	0.010614487	0.000939446	0.011553932	0.018594200	0.009765026	0.000842825	0.010607851	0.0107071638
L141	PD	50	06:00 ~ 07:00	400	0.652528514	0.100293916	0.75282243	1.211547249	0.022632515	0.000979824	0.023612339	0.038000281	0.020809769	0.000847217	0.021656986	0.034853453
L142	LD	50	06:00 ~ 07:00	100	0.470520653	0.078976784	0.549497437	0.884328205	0.008465588	0.000600294	0.009065882	0.014590086	0.00778803	0.000541954	0.008329983	0.013405775
L143	LD	50	06:00 ~ 07:00	150	0.522298693	0.096206473	0.618505103	0.995385103	0.009768373	0.007076646	0.010534833	0.016954128	0.008981625	0.000670185	0.009653144	0.015533044
L144	LD	50	06:00 ~ 07:00	100	0.500298064	0.133657782	0.633955846	1.020250502	0.009931001	0.000991818	0.010292189	0.01757853	0.009124266	0.00085981	0.009984076	0.016067773
L145	LD	50	06:00 ~ 07:00	50	0.366592271	0.074551082	0.441143535	0.709949644	0.005144835	0.001157063	0.006301898	0.010418977	0.004724324	0.001013037	0.005737361	0.009233364
L146	PD	50	06:00 ~ 07:00	250	0.726987701	0.085182798	0.812170498	1.30705847	0.028242767	0.01110798	0.029353565	0.047239866	0.025969273	0.000963058	0.026932331	0.043343278
L147	PD	50	06:00 ~ 07:00	350	0.690096349	0.099529227	0.789625575	1.270776024	0.024701237	0.000975533	0.025676769	0.041322652	0.022712853	0.000843016	0.023555868	0.037909401
L148	LD	50	06:00 ~ 07:00	100	0.461848271	0.082450844	0.544299116	0.875962339	0.008141645	0.000616922	0.008758568	0.014095513	0.007490347	0.000557824	0.008048171	0.012952243
L149	LD	50	06:00 ~ 07:00	200	0.501495308	0.100125433	0.601620741	0.968212324	0.009213438	0.00056759	0.009870197	0.015884504	0.008471073	0.00057878	0.009049854	0.014564292
L150	LD	50	06:00 ~ 07:00	100	0.533659602	0.119023777	0.652683379	1.050389468	0.010419424	0.000976606	0.01139603	0.018340087	0.009572786	0.000844884	0.010417669	0.016765572
L151	LD	50	06:00 ~ 07:00	200	0.625538758	0.099360042	0.7248988	1.166608635	0.017391612	0.000950531	0.018296643	0.029445519	0.015991526	0.000794097	0.016785624	0.027013776
L152	LD	50	06:00 ~ 07:00	200	0.70810905	0.088743129	0.796852933	1.2824073	0.021200174	0.000847648	0.022047822	0.035482443	0.019497869	0.000748137	0.020246007	0.023582708
L153	LD	50	06:00 ~ 07:00	100	0.555057841	0.090468799	0.645526641	1.038871844	0.013570977	0.000949236	0.014520213	0.023387996	0.012481015	0.000835151	0.013316166	0.021430239
L154	LD	50	06:00 ~ 07:00	100	0.496506251	0.11839853	0.614904781	0.988959086	0.008975594	0.000973355	0.00994795	0.016009633	0.0084205	0.009088337	0.014632025	0.014632025
L155	LD	50	06:00 ~ 07:00	100	0.574105228	0.08734221	0.661447438	1.06449382	0.013765491	0.01330228	0.014895719	0.023972776	0.012657705	0.000987518	0.013645223	0.021959803
L156	LD	50	06:00 ~ 07:00	100	0.463775985	0.118104002	0.58086357	0.934521782	0.007590195	0.00972781	0.008382976	0.013491059	0.006973643	0.000690627	0.010766427	0.012334417
L157	LD	50	06:00 ~ 07:00	50	0.440592299	0.072758195	0.517850494	0.833397514	0.006565524	0.001048789	0.007614313	0.012254019	0.006033379	0.000912146	0.006945524	0.011177771
L158	LD	50	06:00 ~ 07:00	50	0.484820983	0.134296835	0.619117819	0.99637107	0.008134342	0.001194223	0.009328565	0.015012833	0.007461842	0.001015096	0.008476938	0.013642726
L159	PD	50	06:00 ~ 07:00	250	0.726987701	0.085182798	0.812170498	1.30705847	0.028242767	0.01110798	0.029353565	0.047239866	0.025969273	0.000963058	0.026932331	0.043343278
L160	PD	50	06:00 ~ 07:00	250	0.738513536	0.089223733	0.827737269	1.332110696	0.028945008	0.01095795	0.030040803	0.048345866	0.026614776	0.000949228	0.027564004	0.044359855
L161	PD	50	06:00 ~ 07:00	250	0.820476059	0.103506787	0.923982847	1.487002554	0.030619765	0.01034933	0.031654698	0.050943172	0.028156771	0.000858386	0.029042157	0.046738705
L162	LD	50	06:00 ~ 07:00	150	0.435567848	0.084245819	0.519813667	0.836556927	0.007414989	0.008671759	0.013320102	0.006818275	0.007592377	0.000757512	0.012184794	0.012184794
L163	LD	50	06:00 ~ 07:00	150	0.480681251	0.085861277	0.566542528	0.911759551	0.008055473	0.00066986	0.008725333	0.014042028	0.007407892	0.000591028	0.00799892	0.012872982
L164	LD	50	06:00 ~ 07:00	50	0.2146431	0.046387819	0.261030919	0.420087498	0.00290005	0.00902286	0.006119251	0.002681	0.000840766	0.003521766	0.005667719	0.005667719
L165	LD	50	06:00 ~ 07:00	50	0.1973261	0.208017494	0.405343504	0.652335515	0.005004405	0.005569461	0.010573866	0.017016946	0.004518958	0.004744297	0.009263255	0.014907726
L166	LD	50	06:00 ~ 07:00	200	0.469030793	0.088450093	0.557480886	0.897176289	0.007519623	0.007545838	0.008265461	0.01301937	0.006913151	0.000654019	0.00756717	0.01217815
L167	LD	50	06:00 ~ 07:00	150	0.457781499	0.083786172	0.54156767	0.871566515	0.008095004	0.000861759	0.008956764	0.014414478	0.007443973	0.000759237	0.00820321	0.013201754
L168	DD	50	06:00 ~ 07:00	50	0.4330474	0.059490923	0.492538323	0.792661625	0.0086252	0.00903354	0.015334684	0.0079262	0.000809194	0.008735394	0.01405822	0.01405822
L169	DD	50	06:00 ~ 07:00	100	0.4743302	0.088988496	0.563318696	0.906571311	0.0101802	0.00145777	0.01159977	0.01866187	0.0093496	0.001222158	0.010571758	0.017013553
L170	LD	50	06:00 ~ 07:00	50	0.546186142	0.065236729	0.61142287	0.983987282	0.011992099	0.00810759	0.012802858	0.020604151	0.01103254	0.00072637	0.01755177	0.018918077
L171	LD	50	06:00 ~ 07:00	50	0.562869794	0.168485148	0.731354943	1.176998763	0.015228267	0.000469189	0.015697455	0.025262543	0.014007399	0.000437198	0.014444597	0.023246268
L172	LD	50	06:00 ~ 07:00	50	0.2146431	0.046387819	0.261030919	0.420087498	0.00290005	0.00902286	0.006119251	0.002681	0.000840766	0.003521766	0.005667719	0.005667719
L173	LD	50	06:00 ~ 07:00	150	0.523102345	0.113634088	0.636736433	1.024725411	0.010507345	0.00097404	0.011414749	0.018370212	0.009656815	0.000791665	0.01044848	0.016815157
L174	LD	50	06:00 ~ 07:00	200	0.587567269	0.077179231	0.6647465	1.069803132	0.013282387	0.01022212	0.014304507	0.023020815	0.012214271	0.000894736	0.013109007	0.021096849
L175	LD	50	06:00 ~ 07:00	100	0.579140446	0.06386924	0.643009686	1.034821209	0.012286766	0.000758038	0.013044804	0.020993526	0.01130511	0.000673136	0.011978246	0.01927707
L176	LD	50	06:00 ~ 07:00	100	0.540492026	0.110076652	0.650568678	1.031707652	0.013713751	0.001640699	0.01535445	0.024710531	0.012599492	0.010415323	0.010414816	0.022554603
L177	LD	50	06:00 ~ 07:00	50	0.29749019	0.443099092	0.740589282	1.191599554	0.005878739	0.011274463	0.011753202	0.027605335	0.005184111	0.009455812	0.014639923	0.023560611
L178	LD	50	06:00 ~ 07:00	50	0.369369042	0.487531193	0.856900235	1.379043825	0.005054866	0.016937589	0.013882722	0.027258339	0.00440481	0.00992715	0.01433196	0.023064997
L179	LD	50	06:00 ~ 07:00	100	0.58846065	0.102796311	0.661642376	1.064807541	0.014255216	0.010479171	0.015734886	0.025321977	0.013098823	0.001280716	0.014379538	0.023141566
L180	PD	50	06:00 ~ 07:00	300	0.68761976	0.102574026	0.790193786	1.271690468	0.02626293	0.00122018	0.02748311	0.044229668	0.02414632	0.001055521	0.025201841	0.040558331
L181	PD	70	06:00 ~ 07:00	600	0.414091009	0	0.414091009	0.666413224	0.008592914	0	0.008592914	0.01382892	0.007891427	0	0.007891427	0.012699989
L182	PD	70	06:00 ~ 07:00	850	0.386015986	0	0.386015986	0.621230966	0.007192818	0	0.007192818	0.01157569	0.006605786	0	0.006605786	0.010630956
L183	PD	50	06:00 ~ 07:00	200	0.790436469	0.09786245	0.888298919	1.429574982	0.032515166	0.00111265	0.033627816	0.054118589	0.029898101	0.000963323	0.030861423	0.049666523
L184	PD	50	06:00 ~ 07:00	250	0.770603212	0.101821889	0.872425102	1.404028613	0.028023309	0.001046801	0.029070109	0.04678369	0.025767786	0.00089635	0.026664136	0.042911661
L185	DD	50	06:00 ~ 07:00	200	0.4591327	0.086158754	0.545291454	0.877559349	0.0090947	0.000994618	0.010089318	0.016237143	0.0083567	0.000869296	0.009225996	0.014847765
L186	DD	50	06:00 ~ 07:00	200	0.451685	0.082377695	0.534062695	0.859488457	0.0073305	0.000633119	0.008013619	0.012896638	0.0067387	0.000603531	0.007342231	

Table 5: Hourly composite emission rate for each road link

L022	DD		50	07:00 ~ 08:00	450	1.89835	0.08824453	1.986594453	3.197105917	0.0830066	0.000414419	0.083421019	0.134252783	0.0763563	0.000368062	0.076724362	0.123475585
L023	LD		300	07:00 ~ 08:00		0.937119408	0.049834709	0.986954117	1.588344738	0.025835176	0.000642677	0.026478393	0.042612738	0.023783084	0.000579521	0.024362605	0.039207714
L024	LD		200	07:00 ~ 08:00		0.25289476	0.03789222	0.290786919	0.467975117	0.006943592	0.00106205	0.007410376	0.0128838	0.006422429	0.000987947	0.011925815	
L025	DD		100	07:00 ~ 08:00		0.8918729	0.02695596	0.91882886	1.478708038	0.0304456	0.000980407	0.031426007	0.05057513	0.0280141	0.000912122	0.028926222	0.046552127
L026	DD		50	07:00 ~ 08:00		0.8543554	0.030257141	0.884612541	1.423642347	0.0295417	0.001021504	0.030563204	0.049186586	0.0271872	0.000951031	0.028138231	0.045283981
L027	DD		300	07:00 ~ 08:00		0.6997886	0.097680774	0.797469374	1.283399362	0.0197568	0.000773862	0.020530662	0.033040815	0.0181724	0.000691259	0.018863659	0.03058041
L028	DD		250	07:00 ~ 08:00		0.7332764	0.109595674	0.842872074	1.356467743	0.0209572	0.000708169	0.021665369	0.034866946	0.0192765	0.000630797	0.019907297	0.032037609
L029	DD		50	07:00 ~ 08:00		0.173530661	0.030667525	0.204198186	0.328624308	0.007570249	0.001377142	0.008947391	0.014399394	0.007008849	0.001281743	0.008290592	0.013342381
L030	DD		1100	07:00 ~ 08:00		1.1120721	0.083537921	1.195610021	1.924143031	0.0432433	0.000626028	0.034369328	0.070606065	0.0397772	0.000562551	0.040339751	0.064920374
L031	DD		950	07:00 ~ 08:00		0.1726029	0.087705766	0.260308666	0.418925148	0.0022268	0.000501622	0.002728422	0.004390959	0.0020452	0.000452481	0.002497681	0.004019618
L032	DD		400	07:00 ~ 08:00		1.9478756	0.099098804	2.046974404	3.294277787	0.0835231	0.000424363	0.0083947463	0.13510001	0.0768332	0.000374236	0.077207436	0.124253015
L033	LD		150	07:00 ~ 08:00		0.365448915	0.084834479	0.450283394	0.724659078	0.011767828	0.002442124	0.014191952	0.022839676	0.010824429	0.002116968	0.012941397	0.020827108
L034	LD		150	07:00 ~ 08:00		0.328487299	0.061402029	0.390127528	0.627847836	0.01031248	0.001801336	0.012113816	0.019495248	0.009503181	0.001600278	0.011103458	0.01786924
L035	LD		50	07:00 ~ 08:00		0.094158871	0.022619863	0.116778734	0.187935687	0.008838965	0.001704798	0.010543764	0.016988051	0.008186631	0.001586308	0.009772939	0.015727981
L036	LD		50	07:00 ~ 08:00		0.08459427	0.021619863	0.107214133	0.172543993	0.010175337	0.001908022	0.010966288	0.017648487	0.008539455	0.001521592	0.010061047	0.016191646
L037	LD		150	07:00 ~ 08:00		0.32761081	0.065574495	0.388335575	0.624963975	0.009260844	0.001705444	0.009588779	0.015430014	0.008945436	0.000414431	0.008867647	0.01427106
L038	LD		200	07:00 ~ 08:00		0.296051021	0.058433239	0.354484261	0.5704857	0.008495912	0.001091878	0.01016227	0.013922851	0.022406601	0.011908844	0.00944734	0.02366155
L039	LD		50	07:00 ~ 08:00		0.280666436	0.034177913	0.314844349	0.506691604	0.008495912	0.001091878	0.01016227	0.013922851	0.022406601	0.011908844	0.00944734	0.02366155
L040	LD		50	07:00 ~ 08:00		0.426092889	0.032796526	0.458889515	0.738509252	0.012906624	0.001016227	0.013922851	0.022406601	0.011908844	0.00944734	0.02366155	
L041	LD		50	07:00 ~ 08:00		0.28022286	0.045791939	0.326014799	0.524668657	0.0084232	0.00138412	0.010225732	0.016456679	0.008161963	0.001257772	0.0100944734	0.02366155
L042	LD		100	07:00 ~ 08:00		0.641168917	0.031655869	0.492824786	0.793122642	0.014221793	0.01034273	0.015256066	0.04552197	0.013119018	0.000962252	0.014081269	0.02366155
L043	LD		200	07:00 ~ 08:00		1.134104428	0.057274132	1.18682856	1.910010674	0.031222555	0.000524043	0.031746598	0.05109107	0.028735719	0.000469652	0.029205731	0.047001371
L044	LD		300	07:00 ~ 08:00		0.814960893	0.097642004	0.912602897	1.468888346	0.023909223	0.000838864	0.024748087	0.039828086	0.022004657	0.000752786	0.022757442	0.03662442
L045	LD		150	07:00 ~ 08:00		0.445566115	0.04797257	0.493536885	0.794271547	0.012760293	0.001267892	0.014028184	0.022576118	0.011763627	0.001147627	0.020778598	
L046	LD		150	07:00 ~ 08:00		0.345272253	0.033788824	0.379061078	0.610038155	0.010573802	0.001067239	0.011641041	0.018734393	0.009764065	0.000991722	0.010755787	0.017809719
L047	DD		1050	07:00 ~ 08:00		1.1187416	0.083998886	1.202740486	1.935618374	0.04005791	0.000559791	0.044717591	0.01765808	0.0405262	0.000593142	0.041119342	0.066175002
L048	DD		350	07:00 ~ 08:00		2.1456574	0.105340256	2.250997656	3.622620568	0.0941342	0.000376426	0.094510626	0.15209973	0.0865941	0.000331486	0.086925586	0.139892822
L049	DD		450	07:00 ~ 08:00		0.5132665	0.059071847	0.572383847	0.921086995	0.0113524	0.000690805	0.012043205	0.019381612	0.010442	0.00062376	0.011106576	0.017808569
L050	DD		400	07:00 ~ 08:00		0.7530475	0.086525115	0.839572615	1.351157792	0.0254622	0.000788582	0.026250782	0.042246434	0.0234192	0.000709126	0.024128326	0.038830679
L051	DD		450	07:00 ~ 08:00		0.5149025	0.05841522	0.573266314	0.92266314	0.0114477	0.000645251	0.012092951	0.01946167	0.01053	0.000584649	0.01114649	0.017887249
L052	DD		350	07:00 ~ 08:00		0.8279973	0.087120852	0.915118152	1.472736247	0.0284902	0.000578605	0.029068805	0.04678159	0.0262077	0.000582528	0.026735958	0.043027247
L053	DD		550	07:00 ~ 08:00		0.5333581	0.077040217	0.610398317	0.982338428	0.0130169	0.000679631	0.013696531	0.022042375	0.0119727	0.000616539	0.021589239	0.020260365
L054	DD		350	07:00 ~ 08:00		0.8882515	0.086841348	0.975092848	1.569255923	0.030614	0.000529133	0.031143133	0.050119889	0.0281636	0.000486772	0.028650372	0.04610819
L055	LD		100	07:00 ~ 08:00		0.510394861	0.045634197	0.556929058	0.89628821	0.016664773	0.0180392	0.029031206	0.015361921	0.001246746	0.016068667	0.026728992	
L056	LD		50	07:00 ~ 08:00		0.173822844	0.040336364	0.214159208	0.34465498	0.00806086	0.001741958	0.010548045	0.01697539	0.008138748	0.001588367	0.009727115	0.015642355
L057	DD		600	07:00 ~ 08:00		0.5372671	0.07619676	0.61346386	0.987271928	0.01244	0.000609499	0.0114422	0.01001081	0.0119447	0.0005525	0.0119947	0.01930355
L058	DD		350	07:00 ~ 08:00		0.9254555	0.083944804	1.009400304	1.624468284	0.0310132	0.000428101	0.031441301	0.050599744	0.0285324	0.000397243	0.028929643	0.046557631
L059	LD		150	07:00 ~ 08:00		0.90372103	0.034155971	1.509362972	2.027983045	0.000828643	0.000816888	0.025774946	0.000769433	0.026544379	0.000769433	0.042718931	
L060	LD		150	07:00 ~ 08:00		0.99495643	0.043839997	1.038396427	1.671132905	0.029914456	0.000744153	0.030658609	0.049340125	0.027542343	0.000679413	0.028221756	0.0454184
L061	LD		50	07:00 ~ 08:00		0.098169262	0.022875401	0.121044663	0.098482018	0.008246216	0.001732388	0.009978604	0.016058967	0.007642477	0.001614271	0.009256748	0.014897254
L062	LD		100	07:00 ~ 08:00		0.62544579	0.050650992	0.676096782	1.080695956	0.016705809	0.000875034	0.017580483	0.028293554	0.015390327	0.000797925	0.016188252	0.026052401
L063	LD		50	07:00 ~ 08:00		0.3932068	0.030321755	0.423528555	0.681601444	0.0137627	0.000903033	0.014665733	0.032602151	0.0126913	0.000831695	0.013522995	0.021763096
L064	LD		50	07:00 ~ 08:00		0.250718268	0.13095528	0.381673548	0.614242508	0.008619619	0.03643083	0.012262702	0.019734857	0.007904835	0.0003136409	0.011041244	0.017769115
L065	DD		600	07:00 ~ 08:00		1.5510461	0.077583684	1.628629784	2.621019057	0.0658343	0.000500565	0.066334865	0.07565351	0.0605606	0.000451641	0.061012241	0.098189441
L066	DD		450	07:00 ~ 08:00		1.6679815	0.084890531	1.752872031	2.820967074	0.0695987	0.00035468	0.06995338	0.112578772	0.0640254	0.000381396	0.064343796	0.103551044
L067	DD		600	07:00 ~ 08:00		1.5398174	0.077548201	1.617365601	2.602891156	0.0653736	0.00050658	0.06588018	0.106023609	0.0601368	0.000457246	0.060594496	0.097516423
L068	DD		750	07:00 ~ 08:00		1.1618758	0.105576768	1.287452598	2.071948916	0.0453892	0.000594953	0.045984153	0.074004137	0.0417513	0.000529502	0.042280802	0.068044187
L069	LD		350	07:00 ~ 08:00		0.790128874	0.063323081	0.853451954	1.373494384	0.022030717	0.000936882	0.022967598	0.036962675	0.020281304	0.000838931	0.021120235	0.03398964
L070	LD		350	07:00 ~ 08:00		0.820648241	0.062093149	0.882741384	1.420631019	0.02284977	0.000870669	0.023714646	0.038164928	0.021030208	0.00078167	0.021811878	0.035102728
L071	LD		150	07:00 ~ 08:00		0.625740932	0.053892257	0.679633189	0.879637687	0.015851611	0.000828678	0.01668029	0.028844257	0.014599462	0.000748438	0.01535479	0.024699899
L072	LD		150	07:00 ~ 08:00		0.897208948	0.052624292	1.028604627	1.528604627	0.023114321	0.000545687	0.023660008	0.038076997	0.0217915	0.000496666	0.021775816	0.035044692
L073	LD		150	07:00 ~ 08:00		0.630720335	0.060250007	0.690970942	1.11206261	0.010768222	0.0016571						

Table 5: Hourly composite emission rate for each road link

L104	PD	50	07:00 ~ 08:00	1000	0.873623912	0.109560734	0.983184646	1.582278378	0.035835764	0.000766555	0.036602319	0.058890557	0.032962183	0.000680121	0.033642303	0.054141905
L105	PD	50	07:00 ~ 08:00	750	0.906664259	0.105214838	1.056879097	1.700877806	0.035400147	0.000610496	0.036010643	0.057953369	0.032560014	0.000533892	0.033093905	0.053259346
L106	LD	50	07:00 ~ 08:00	150	0.510268543	0.127230032	0.637498575	1.025951957	0.017567842	0.002152651	0.019720493	0.031736978	0.016156954	0.001887171	0.018044671	0.029040011
L107	LD	50	07:00 ~ 08:00	350	0.634005118	0.053890675	0.687895793	1.107058216	0.016529666	0.000935512	0.017465178	0.028107409	0.015223446	0.00084182	0.016065284	0.025845498
L108	LD	50	07:00 ~ 08:00	250	0.661184329	0.081923371	0.7431077	1.195912945	0.016008664	0.000574792	0.016583457	0.02668842	0.01473615	0.000520703	0.015256852	0.024553462
L109	LD	50	07:00 ~ 08:00	150	0.641894911	0.051603308	0.693498219	1.116074424	0.017122562	0.00089916	0.018022479	0.029004296	0.015770992	0.00081507	0.016586062	0.026692163
L110	LD	50	07:00 ~ 08:00	150	0.453166013	0.092195403	0.545361416	0.877671941	0.011726338	0.002167283	0.013893621	0.02235956	0.010778939	0.001879139	0.012658077	0.02037115
L111	LD	50	07:00 ~ 08:00	150	0.562176555	0.052327117	0.614503672	0.98894534	0.016324943	0.001143716	0.017468659	0.028113012	0.015037847	0.00103064	0.016068487	0.025859568
L112	LD	50	07:00 ~ 08:00	400	0.7160752	0.094660309	0.810735509	1.304749084	0.02116424	0.000847406	0.022011646	0.035424222	0.019484202	0.000772259	0.020256461	0.032599534
L113	LD	50	07:00 ~ 08:00	450	0.642831728	0.094794525	0.737626253	1.187091434	0.019571481	0.00106975	0.020668456	0.03262573	0.018018344	0.00098868	0.019007023	0.030588763
L114	LD	50	07:00 ~ 08:00	200	0.5242763	0.057495381	0.581771682	0.936268438	0.015160656	0.001369349	0.016530005	0.026602398	0.013964242	0.001226176	0.015190418	0.024464568
L115	LD	50	07:00 ~ 08:00	50	0.280281117	0.025213837	0.305494954	0.491645249	0.012765429	0.001255446	0.014020875	0.022564355	0.011785005	0.001162334	0.010294738	0.02083667
L116	LD	50	07:00 ~ 08:00	50	0.218913597	0.020384364	0.239297961	0.38511178	0.011959257	0.001530009	0.013489266	0.021708815	0.011052574	0.001421933	0.012474507	0.020075722
L117	LD	50	07:00 ~ 08:00	50	0.821499895	0.040510675	0.862010569	1.38726809	0.026778678	0.01221933	0.028000611	0.045062503	0.024655295	0.00110202	0.025757315	0.041452278
L118	PD	50	07:00 ~ 08:00	1100	0.865312976	0.091547762	0.956860738	1.53991426	0.036126691	0.000927629	0.03705432	0.059632999	0.033292255	0.000818695	0.034047951	0.054794729
L119	PD	50	07:00 ~ 08:00	750	0.906664259	0.150214838	1.056879097	1.700877806	0.035400147	0.000610496	0.036010643	0.057953369	0.032560014	0.000533892	0.033093905	0.053259346
L120	LD	50	07:00 ~ 08:00	100	0.744049869	0.124536489	0.868586358	1.397850769	0.024179327	0.000878818	0.025058145	0.040327075	0.022252635	0.000801351	0.023053985	0.037101701
L121	LD	50	07:00 ~ 08:00	200	0.61731796	0.0679253126	0.679253126	1.093149226	0.015761478	0.001035469	0.016796947	0.027031999	0.015435695	0.00092269	0.015435695	0.024841282
L122	LD	50	07:00 ~ 08:00	350	0.634876692	0.072650225	0.707526917	1.138651369	0.015605996	0.000693398	0.016299394	0.026231267	0.014367658	0.000625597	0.014993256	0.024129246
L123	LD	50	07:00 ~ 08:00	100	0.650627688	0.114506969	0.765134657	1.231361809	0.020974515	0.000952735	0.02192725	0.0352884	0.019308984	0.000869479	0.020178463	0.032474008
L124	LD	50	07:00 ~ 08:00	50	0.234727482	0.031337265	0.266064747	0.42818864	0.009133702	0.00120802	0.010341721	0.016643346	0.008441997	0.001121146	0.009563144	0.015390349
L125	LD	50	07:00 ~ 08:00	50	0.233070248	0.034447271	0.267517519	0.430526644	0.008093826	0.001133003	0.009226829	0.014849365	0.007483465	0.001051996	0.008553461	0.013736459
L126	PD	50	07:00 ~ 08:00	750	0.902310493	0.14734829	1.049748784	1.689402707	0.035110573	0.000567537	0.03567811	0.05741821	0.032294197	0.000498054	0.032792251	0.052773881
L127	PD	50	07:00 ~ 08:00	50	0.185087325	0.033874116	0.218956616	0.35237564	0.0089175	0.001198518	0.007117018	0.020145370	0.00504425	0.001154479	0.006557979	0.010554018
L128	LD	50	07:00 ~ 08:00	200	0.713908346	0.080195492	0.794103739	1.277992911	0.019707722	0.000770502	0.020478225	0.032956426	0.018143793	0.000899907	0.0188437	0.030321592
L129	LD	50	07:00 ~ 08:00	300	0.666894358	0.080548526	0.747442884	1.2028973	0.015766027	0.000693147	0.016459173	0.026485406	0.014510195	0.000617016	0.015127221	0.023444825
L130	LD	50	07:00 ~ 08:00	300	0.712732441	0.083354486	0.796086927	1.281174535	0.016500963	0.000597375	0.017098339	0.02751704	0.015182575	0.000527649	0.015710223	0.025238091
L131	LD	50	07:00 ~ 08:00	200	0.713908246	0.080195492	0.794103739	1.277992911	0.019707722	0.000770502	0.020478225	0.032956426	0.018143793	0.000899907	0.0188437	0.030321592
L132	PD	50	07:00 ~ 08:00	1200	0.740974351	0.121795923	1.388490713	2.025975473	0.025975473	0.000637973	0.026613446	0.042830083	0.023890393	0.000559642	0.024450576	0.039349289
L133	DD	50	07:00 ~ 08:00	700	0.7493813	0.067953436	0.817334736	1.31359484	0.0260909	0.000914095	0.027004995	0.043460219	0.02399362	0.00081936	0.024816036	0.039937439
L134	DD	50	07:00 ~ 08:00	500	0.8798677	0.071221842	0.951089542	1.530626444	0.0322167	0.000756974	0.029273674	0.053058552	0.0296322	0.000680652	0.030312852	0.048783686
L135	LD	50	07:00 ~ 08:00	150	0.462238526	0.053178096	0.515416622	0.829480587	0.01155793	0.001079338	0.012637268	0.02033766	0.010649572	0.000971175	0.011620747	0.018701733
L136	PD	50	07:00 ~ 08:00	750	1.008509059	0.06619888	1.07470794	1.729570476	0.045655387	0.000981761	0.046637147	0.075055027	0.04199315	0.000859282	0.042852431	0.068964132
L137	PD	50	07:00 ~ 08:00	1250	0.706142331	0.098748265	0.804809596	1.295342632	0.026331197	0.000661134	0.026974331	0.043410669	0.024203987	0.000591371	0.024795357	0.039904161
L138	LD	50	07:00 ~ 08:00	200	0.667861317	0.082763957	0.750625274	1.208011278	0.018403935	0.000868604	0.019272539	0.030116068	0.016942609	0.000783554	0.017726163	0.028527423
L139	LD	50	07:00 ~ 08:00	200	0.777268372	0.100366171	0.877634543	1.412412375	0.018323465	0.000489644	0.018813109	0.030276689	0.016857186	0.000430432	0.017287619	0.027821656
L140	LD	50	07:00 ~ 08:00	200	0.59117674	0.079501076	0.670677816	1.079348637	0.014327346	0.000538627	0.014866063	0.023924549	0.013194248	0.000500757	0.013695005	0.022039919
L141	PD	50	07:00 ~ 08:00	1100	0.761875736	0.100531612	0.862407348	1.387966642	0.029579457	0.000727278	0.030306734	0.047873839	0.027207159	0.000644058	0.027851118	0.048422079
L142	LD	50	07:00 ~ 08:00	200	0.630408691	0.094652336	0.72503426	1.16682626	0.019934751	0.001213458	0.032081792	0.017231212	0.001080607	0.001886219	0.029469942	0.029469942
L143	LD	50	07:00 ~ 08:00	350	0.640936615	0.092428553	0.733365168	1.1802339	0.014509133	0.000552321	0.015003234	0.024145305	0.01329894	0.000494676	0.013793616	0.022198617
L144	LD	50	07:00 ~ 08:00	200	0.54151297	0.11072365	0.652236621	1.049670483	0.012040453	0.000678933	0.0121719386	0.020469816	0.011079045	0.000670784	0.011686885	0.018808172
L145	LD	50	07:00 ~ 08:00	100	0.410164851	0.055207604	0.465372454	0.748942506	0.00438887	0.000365165	0.004754035	0.007650858	0.004049477	0.00033914	0.004388617	0.007062778
L146	PD	50	07:00 ~ 08:00	750	1.023538024	0.06478755	1.088325574	1.75148588	0.046399737	0.000954293	0.04735403	0.076208734	0.042678031	0.000835826	0.043513857	0.070028591
L147	PD	50	07:00 ~ 08:00	950	0.826974678	0.101437663	0.928412341	1.494131117	0.033485691	0.000771471	0.034257163	0.055131422	0.030799724	0.000680008	0.031479732	0.050661591
L148	LD	50	07:00 ~ 08:00	200	0.635718671	0.094726059	0.73044473	1.175539322	0.018766978	0.00121772	0.01998875	0.032168695	0.017272712	0.001088542	0.018361254	0.02949545
L149	LD	50	07:00 ~ 08:00	500	0.604365868	0.097658094	0.702023962	1.129795243	0.013383472	0.00057869	0.013962162	0.022469865	0.012316032	0.000518579	0.012834611	0.020655523
L150	LD	50	07:00 ~ 08:00	200	0.65476101	0.11079478	0.765555888	1.232039713	0.016269481	0.000734712	0.017004193	0.027365527	0.014966626	0.000650787	0.015617412	0.025133727
L151	LD	50	07:00 ~ 08:00	550	0.829984032	0.105589295	0.935573326	1.505655577	0.027149013	0.000769809	0.027918822	0.043940876	0.024978213	0.000689131	0.025667124	0.041307773
L152	LD	50	07:00 ~ 08:00	550	0.94690031	0.090071434	1.034761465	1.665283016	0.02795614	0.000795614	0.03262258	0.05304748	0.029598223	0.000716081	0.030314304	0.048786022
L153	LD	50	07:00 ~ 08:00	200	0.800604415	0.076714428	0.877318842	1.411904306	0.024259086	0.000577285	0.025016371	0.040259847	0.022329951	0.00068766	0.023017612	0.037043163
L154	LD	50	07:00 ~ 08:00	150	0.585332968	0.12749093	0.712823968	1.147176013	0.014950554	0.000547028	0.015497582	0.024940879				

Table 5: Hourly composite emission rate for each road link

L186	DD	00:07:00~08:00	550	0.4498768	0.070024339	0.519901139	0.836697699	0.0080602	0.000502231	0.008562431	0.013779862	0.0074138	0.000460806	0.007874606	0.012672918
L187	LD	00:07:00~08:00	100	0.620339092	0.074708697	0.695047789	1.118568209	0.020197703	0.001036691	0.021234394	0.034173359	0.018602384	0.000946819	0.019549203	0.031461315
L188	LD	00:07:00~08:00	100	0.641606588	0.069789146	0.711995729	1.144877602	0.020606382	0.000904169	0.021510551	0.034617791	0.01893643	0.000841119	0.019824762	0.031904783
L189	LD	00:07:00~08:00	50	0.2031518	0.095169414	0.298321214	0.480100263	0.00716071	0.002778716	0.009939426	0.015999196	0.00658519	0.002427923	0.009013113	0.014505162
L190	LD	00:07:00~08:00	50	0.316442763	0.034555387	0.35099815	0.564875363	0.009484941	0.000987071	0.010035562	0.016150631	0.008360242	0.000915273	0.009275515	0.014927458
L191	LD	00:07:00~08:00	50	0.210657545	0.032709819	0.242867364	0.380856169	0.001270601	0.009776994	0.014601095	0.007719491	0.001181718	0.008401119	0.013520257	0.014832027
L192	LD	00:07:00~08:00	50	0.279403233	0.033989142	0.313392374	0.504354884	0.00867437	0.001060354	0.009734724	0.015666481	0.00801758	0.009883557	0.009001137	0.01448589
L193	LD	00:07:00~08:00	50	0.389774349	0.033273097	0.423047445	0.680827176	0.011971578	0.013023897	0.020959878	0.011049743	0.000979067	0.01020881	0.019358445	0.019358445
L194	LD	00:07:00~08:00	100	0.227242812	0.029727059	0.256698971	0.413551893	0.008992428	0.001290064	0.010282492	0.016548025	0.008315735	0.001199478	0.009515213	0.015313213
L195	LD	00:07:00~08:00	150	0.321102073	0.065661977	0.386764051	0.622434857	0.010183253	0.001920474	0.012103727	0.019479013	0.009381603	0.001700937	0.01108254	0.017835575
L196	LD	00:07:00~08:00	150	0.281537871	0.042743958	0.324281829	0.521879719	0.008502817	0.001284711	0.009787528	0.015751461	0.007852362	0.001172406	0.009020768	0.01452392
L001	LD	00:08:00~09:00	250	0.686757243	0.055028299	0.741785543	1.193785145	0.019526472	0.001177246	0.020703718	0.03319321	0.017979539	0.01048911	0.019028455	0.030623246
L002	LD	00:08:00~09:00	300	0.809061452	0.087638033	0.896699485	1.44309435	0.022724074	0.00088851	0.023712584	0.038161609	0.020908816	0.000869606	0.021778422	0.035048886
L003	LD	00:08:00~09:00	50	0.527530642	0.035860489	0.56339113	0.906687882	0.019060925	0.001019994	0.020080919	0.032317026	0.017575255	0.000946142	0.018521398	0.029807226
L004	DD	00:08:00~09:00	1200	0.869961225	0.058888186	0.928849411	1.494834512	0.033773854	0.001210753	0.034984607	0.056302128	0.031060236	0.001073859	0.032134095	0.051714684
L005	DD	00:08:00~09:00	900	1.227980535	0.056613737	1.284594272	2.067348946	0.047993477	0.001050883	0.04904436	0.07892905	0.000922538	0.004506503	0.072524955	0.072524955
L006	LD	00:08:00~09:00	300	0.948080439	0.084060143	1.032140583	1.661065126	0.026846966	0.000883326	0.027730292	0.044627467	0.024702536	0.00077685	0.025479386	0.041004996
L007	LD	00:08:00~09:00	550	0.807115667	0.06996334	0.877112002	1.411571429	0.023379793	0.00100294	0.024382733	0.039240107	0.021521139	0.000893037	0.022414176	0.03607203
L008	LD	00:08:00~09:00	250	0.845311102	0.049619627	0.894930729	1.440247819	0.024212111	0.000927765	0.025139877	0.040458609	0.022289373	0.000828155	0.023117529	0.037203964
L009	LD	00:08:00~09:00	350	0.590981414	0.048287468	0.639268882	0.92800983	0.019225086	0.00132799	0.020553076	0.03076887	0.017712083	0.001197335	0.018909418	0.030431683
L010	LD	00:08:00~09:00	50	0.279508472	0.0134686768	0.414195239	0.666580966	0.00841586	0.003534175	0.011950035	0.019231669	0.007709003	0.003035672	0.010744675	0.017291835
L011	LD	00:08:00~09:00	200	0.890911798	0.04065134	0.931563138	1.499201821	0.02622676	0.000811684	0.027038444	0.04351405	0.024149294	0.000735582	0.024884876	0.040048226
L012	LD	00:08:00~09:00	100	0.511898828	0.036421414	0.548320241	0.882433697	0.018579468	0.001493569	0.020073037	0.032304342	0.017127979	0.001355786	0.018483765	0.029746662
L013	LD	00:08:00~09:00	250	0.468059938	0.045311903	0.513371841	0.826189839	0.013997225	0.001159864	0.015157089	0.02439291	0.01051313	0.013953679	0.02456213	0.02456213
L014	DD	00:08:00~09:00	1250	0.874571059	0.076028897	0.950599555	1.529838532	0.033514861	0.00107716	0.034592021	0.055670323	0.030823286	0.000958001	0.031781287	0.051146897
L015	DD	00:08:00~09:00	450	1.762156823	0.05426696	1.81671952	2.923719392	0.075918934	0.000997594	0.076916527	0.123784844	0.069828045	0.00861899	0.030768994	0.113764155
L016	LD	00:08:00~09:00	200	0.432549664	0.050422174	0.482971837	0.777265897	0.01049081	0.01060726	0.011590807	0.018523193	0.009634748	0.000962851	0.0105976	0.017055141
L017	LD	00:08:00~09:00	200	0.890911798	0.04065134	0.931563138	1.499201821	0.02622676	0.000811684	0.027038444	0.04351405	0.024149294	0.000735582	0.024884876	0.040048226
L018	LD	00:08:00~09:00	350	0.590981414	0.048287468	0.639268882	0.92800983	0.019225086	0.00132799	0.020553076	0.03076887	0.017712083	0.001197335	0.018909418	0.030431683
L019	LD	00:08:00~09:00	300	0.820282869	0.082581018	0.920851018	1.452994257	0.026240472	0.00223024	0.028463496	0.045807443	0.024135255	0.001928755	0.026064008	0.04194585
L020	LD	00:08:00~09:00	450	0.612892983	0.068830075	0.681723058	1.097124186	0.01810424	0.001715376	0.019819616	0.031896501	0.0166629	0.001510888	0.018173788	0.029247804
L021	DD	00:08:00~09:00	1150	0.962529349	0.073518093	1.036047441	1.66735259	0.037541867	0.001020857	0.038562724	0.06206534	0.034527706	0.000905854	0.035433559	0.057024644
L022	DD	00:08:00~09:00	500	1.553170378	0.080487222	1.6336576	2.629110522	0.065931823	0.000888736	0.066820559	0.10736999	0.060643547	0.000779658	0.061423204	0.098885072
L023	LD	00:08:00~09:00	450	0.914973883	0.047729896	0.962703778	1.549317699	0.027076473	0.000951641	0.028028114	0.045106765	0.024926642	0.000850119	0.025776761	0.041483572
L024	LD	00:08:00~09:00	300	0.338231607	0.029804439	0.368036045	0.592259129	0.01864494	0.001205061	0.013070005	0.021034062	0.010958252	0.001120769	0.012079021	0.019439252
L025	DD	00:08:00~09:00	150	0.732509056	0.022282988	0.754792044	1.214717028	0.02591089	0.001119456	0.026710545	0.042868349	0.023546926	0.001041761	0.024588688	0.039571558
L026	DD	00:08:00~09:00	100	0.68065359	0.025963955	0.706617545	1.13718788	0.024138159	0.00116933	0.025255092	0.040644029	0.02221223	0.001039478	0.023251708	0.037419903
L027	DD	00:08:00~09:00	450	0.661061425	0.071416445	0.73247788	1.178805935	0.020267631	0.00106328	0.021330911	0.034328688	0.01863779	0.000947145	0.019584935	0.03151882
L028	DD	00:08:00~09:00	350	0.70693508	0.079415277	0.786350357	1.265505084	0.021807172	0.001011903	0.022819075	0.03672365	0.020053385	0.000896684	0.020950068	0.033715782
L029	LD	00:08:00~09:00	100	0.721142347	0.024774249	0.75916596	0.476230415	0.01179913	0.001398021	0.013197151	0.021238703	0.01090503	0.001300447	0.012205477	0.019642763
L030	DD	00:08:00~09:00	1500	0.877120248	0.070544424	0.947574671	1.524969822	0.03320776	0.001041387	0.034362163	0.055300403	0.03064539	0.00092685	0.031572239	0.050810468
L031	DD	00:08:00~09:00	1200	0.1763838	0.071576677	0.247960477	0.399052713	0.0037966	0.000862128	0.004658728	0.007497478	0.0034856	0.00077154	0.00425714	0.006851186
L032	DD	00:08:00~09:00	450	0.161937901	0.087849364	1.707282373	2.74751091	0.067741187	0.000804094	0.068621681	0.110495616	0.06230891	0.000768805	0.063077715	0.101513489
L033	LD	00:08:00~09:00	300	0.417560143	0.02933923	0.520494066	0.83765192	0.014426647	0.003043475	0.017470121	0.028115365	0.013262554	0.002635009	0.015891265	0.025574445
L034	LD	00:08:00~09:00	300	0.392860919	0.070307706	0.463163625	0.745387748	0.013737514	0.002241223	0.015978737	0.025715221	0.012684024	0.001973202	0.014620325	0.023529074
L035	LD	00:08:00~09:00	50	0.477838768	0.020163331	0.498002099	0.801454698	0.019971241	0.001521507	0.021492748	0.034589139	0.018422939	0.001983925	0.019182806	0.023598013
L036	LD	00:08:00~09:00	50	0.422138423	0.020163331	0.443301753	0.718139933	0.013835988	0.001521507	0.019905106	0.032034083	0.016944022	0.001416265	0.018380287	0.023598013
L037	LD	00:08:00~09:00	300	0.380471336	0.074715802	0.455187137	0.732550868	0.013316847	0.00235369	0.015670538	0.025219223	0.012258713	0.002066871	0.014325584	0.023054736
L038	LD	00:08:00~09:00	350	0.372053776	0.062671921	0.435577297	0.700991968	0.01206141	0.001562135	0.015632006	0.021623682	0.012623682	0.00162556	0.013987917	0.022511315
L039	LD	00:08:00~09:00	50	0.321185845	0.039687019	0.360872864	0.580767136	0.011676732	0.001562135	0.013238862	0.02130583	0.010777995	0.001420058	0.012198051	0.019630812
L040	LD	00:08:00~09:00	100	0.496566864	0.025974436	0.522544836	0.840952306	0.016967439	0.001178639	0.018146078	0.029203209	0.015652429	0.001096774	0.016749209	0.026955162
L041	LD	00:08:00~09:00	100	0.339448004	0.051751009	0.391199013	0.629572219	0.012158452	0.001797798	0.01395625	0.027460352	0.01121127	0.00160708		

Table 5: Hourly composite emission rate for each road link

L072	LD	50	08:00 ~ 09:00	200	0.935981448	0.049795669	0.985777177	1.586450546	0.026431609	0.000848941	0.02728055	0.043903681	0.02433263	0.000765282	0.025097911	0.040391072
L073	LD	50	08:00 ~ 09:00	200	0.731881491	0.051728432	0.783609923	1.261094793	0.015345651	0.000247957	0.015593608	0.025095417	0.014127282	0.000228027	0.014355309	0.023102573
L074	LD	50	08:00 ~ 09:00	200	0.505197662	0.051383071	0.556580733	0.895727637	0.012023483	0.000827515	0.012850998	0.020681626	0.011080133	0.000752794	0.011832926	0.019043021
L075	DD	50	08:00 ~ 09:00	1050	0.488254728	0.06091411	0.548825478	0.883246794	0.012891176	0.00096438	0.013855556	0.02298301	0.011853633	0.000866396	0.012720029	0.020470852
L076	DD	50	08:00 ~ 09:00	450	0.761670048	0.067511705	0.829181753	1.334435363	0.026461504	0.000868673	0.027358377	0.040402893	0.024341873	0.00081583	0.025157703	0.040487298
L077	LD	50	08:00 ~ 09:00	400	1.04610205	0.06219179	1.108293841	1.78362161	0.030552236	0.001047871	0.013610016	0.050855315	0.028116421	0.00092606	0.02904247	0.046739225
L078	DD	50	08:00 ~ 09:00	550	1.414472765	0.064733199	1.479205964	2.380545326	0.061366027	0.0007415	0.062107528	0.099952129	0.0564474	0.000668394	0.057115794	0.091518731
L079	DD	50	08:00 ~ 09:00	900	0.975916586	0.09756961	1.066673547	1.716640406	0.03687507	0.00104133	0.037916399	0.061020378	0.033913564	0.000918265	0.034831829	0.056056256
L080	DD	50	08:00 ~ 09:00	550	1.434662191	0.068587956	1.503250147	2.419240591	0.062253397	0.000813459	0.063066856	0.101496014	0.057261834	0.000726701	0.057988535	0.093332369
L081	DD	50	08:00 ~ 09:00	900	0.960250703	0.09550561	1.055801264	1.699143206	0.036358141	0.001182689	0.03754083	0.06041596	0.033435735	0.001037697	0.034473431	0.055479472
L082	LD	50	08:00 ~ 09:00	100	0.308527657	0.096376963	0.40490462	0.651629201	0.011657349	0.00295249	0.014652598	0.023581013	0.010717832	0.002613409	0.013331241	0.0214545
L083	LD	50	08:00 ~ 09:00	100	0.420912283	0.065386166	0.486298449	0.782619546	0.014717287	0.002068369	0.016785655	0.027013827	0.013553558	0.001827388	0.015380946	0.024753171
L084	LD	50	08:00 ~ 09:00	50	0.17200856	0.012651048	0.724659608	1.166223693	0.02400429	0.000930623	0.024943913	0.040128753	0.02212084	0.000859658	0.022980948	0.036983434
L085	LD	50	08:00 ~ 09:00	50	0.308575947	0.019283867	0.327859814	0.527637913	0.013898409	0.001452158	0.015350568	0.024704283	0.012839298	0.001350893	0.014190192	0.022836843
L086	LD	50	08:00 ~ 09:00	150	0.431631569	0.102882284	0.534513853	0.860214525	0.016388166	0.003260416	0.019468581	0.031621248	0.015062481	0.002833606	0.017896087	0.028800888
L087	LD	50	08:00 ~ 09:00	750	0.559981853	0.089821559	0.649803412	1.045754622	0.017648969	0.001831736	0.019480705	0.031351077	0.016242817	0.001613074	0.017855891	0.028736199
L088	LD	50	08:00 ~ 09:00	50	0.564994411	0.091027632	0.656022493	1.055762515	0.016657912	0.002459561	0.019117473	0.030766513	0.015315243	0.002129025	0.017444268	0.028073758
L089	LD	50	08:00 ~ 09:00	350	0.507435168	0.06711802	0.56914697	0.915950985	0.014752071	0.001483159	0.01623523	0.026128006	0.013584204	0.001314597	0.014898801	0.029772326
L090	LD	50	08:00 ~ 09:00	600	0.7228917	0.099385248	0.822276948	1.323323184	0.022931147	0.001035202	0.02394835	0.038541037	0.021093007	0.000928643	0.02202165	0.035440322
L091	LD	50	08:00 ~ 09:00	100	0.617471253	0.03872336	0.656194614	1.06040239	0.021339869	0.001395418	0.022735286	0.036588806	0.019658961	0.0020922401	0.023671256	0.033672126
L092	LD	50	08:00 ~ 09:00	400	0.804928223	0.130314005	0.935242228	1.505122727	0.025452529	0.001104569	0.0268557097	0.042739399	0.023418881	0.000978109	0.024396989	0.039263051
L093	LD	50	08:00 ~ 09:00	150	0.633970821	0.069155466	0.703126287	1.131569259	0.020512987	0.001831957	0.022344945	0.035960613	0.018877232	0.001606819	0.020484051	0.023965802
L094	LD	50	08:00 ~ 09:00	300	0.788839968	0.140098455	0.928938423	1.494977761	0.026554124	0.000869939	0.027424062	0.04413464	0.024441609	0.000792542	0.025234151	0.040610328
L095	LD	50	08:00 ~ 09:00	600	0.473909735	0.067079756	0.54089332	0.870366771	0.015069091	0.001215701	0.016276603	0.031388352	0.001113815	0.0014997167	0.024135541	0.034135541
L096	LD	50	08:00 ~ 09:00	350	0.965366407	0.13482251	1.099748659	1.769869506	0.031970333	0.000721083	0.032691372	0.052611533	0.029423886	0.000660135	0.029038402	0.048415419
L097	LD	50	08:00 ~ 09:00	400	0.892632698	0.128396321	1.021029019	1.643182841	0.02998277	0.001085551	0.031068321	0.049999492	0.027592739	0.000972788	0.028565527	0.049751675
L098	LD	50	08:00 ~ 09:00	100	0.351081785	0.084077714	0.435789499	0.701335472	0.014318547	0.002089514	0.01712806	0.027564873	0.013172949	0.002455329	0.015626188	0.025147849
L099	LD	50	08:00 ~ 09:00	700	0.631186634	0.074333511	0.705520146	1.135421791	0.02054898	0.001493613	0.022042593	0.035474027	0.018923102	0.001340676	0.020263777	0.032611307
L100	LD	50	08:00 ~ 09:00	50	0.488198577	0.0280959	0.516294477	0.803093354	0.017667227	0.00165161	0.027667227	0.028432575	0.015232332	0.001082715	0.016306407	0.026241974
L101	LD	50	08:00 ~ 09:00	200	0.657251065	0.056105645	0.71335671	1.148033487	0.021591254	0.001659777	0.023251031	0.037418814	0.019877932	0.001469747	0.021347679	0.034355673
L102	LD	50	08:00 ~ 09:00	100	0.393609549	0.068841644	0.462451193	0.744241202	0.014420451	0.002250777	0.016671228	0.026829675	0.013276259	0.0019807	0.015256959	0.024553634
L103	LD	50	08:00 ~ 09:00	250	0.432507385	0.046146413	0.478653799	0.770316704	0.01391683	0.001458519	0.015375349	0.024744164	0.012831863	0.001318191	0.014150054	0.022722248
L104	PD	50	08:00 ~ 09:00	1400	0.746296714	0.08930347	0.835600184	1.344764801	0.029350491	0.001277755	0.030628246	0.049291262	0.02699249	0.00112441	0.0281166	0.045249652
L105	PD	50	08:00 ~ 09:00	900	0.800296722	0.129749607	0.930046329	1.496760759	0.030114614	0.00124566	0.03133918	0.050435396	0.027692407	0.001063099	0.028755506	0.04627386
L106	LD	50	08:00 ~ 09:00	250	0.424280564	0.123980707	0.548261271	0.882338794	0.015535087	0.003050112	0.018585199	0.029090904	0.014276865	0.002649299	0.016292164	0.027239953
L107	LD	50	08:00 ~ 09:00	500	0.604985876	0.053717277	0.658757152	1.060164236	0.017920513	0.001317108	0.019237622	0.030959874	0.01605502	0.001175858	0.017680877	0.028454543
L108	LD	50	08:00 ~ 09:00	350	0.645327542	0.071368046	0.713368046	1.148051731	0.017362491	0.000967466	0.018329957	0.029499133	0.015987202	0.000866416	0.016853618	0.027123202
L109	LD	50	08:00 ~ 09:00	250	0.644650309	0.051333182	0.695983491	1.120074072	0.019534983	0.001317342	0.020852325	0.033558481	0.017991878	0.001179811	0.019171689	0.030853765
L110	LD	50	08:00 ~ 09:00	250	0.498689817	0.06737056	0.597746573	0.914983034	0.0029468	0.017929834	0.028855198	0.01375952	0.002538801	0.00262954	0.02258952	0.03622954
L111	LD	50	08:00 ~ 09:00	250	0.582650131	0.054057603	0.616707733	0.992492424	0.018173839	0.001543237	0.019171076	0.031731478	0.016738101	0.001376826	0.018114927	0.029150376
L112	LD	50	08:00 ~ 09:00	600	0.627362269	0.069103543	0.696465811	1.120850289	0.020154048	0.001198441	0.021352489	0.034363415	0.018565639	0.001080208	0.019653667	0.031629433
L113	LD	50	08:00 ~ 09:00	700	0.573723999	0.075441528	0.649165526	1.044728048	0.019050262	0.001544544	0.020594805	0.031140444	0.017544856	0.0013823	0.018927156	0.030460229
L114	LD	50	08:00 ~ 09:00	350	0.614600564	0.060438315	0.675038879	1.086367069	0.019999651	0.001790378	0.021790029	0.035067565	0.01841235	0.001588453	0.020000803	0.032188092
L115	LD	50	08:00 ~ 09:00	100	0.32334721	0.021902384	0.345249594	0.555623982	0.015371319	0.001313834	0.016685153	0.026852084	0.014187533	0.001215613	0.015403146	0.024788899
L116	LD	50	08:00 ~ 09:00	100	0.326488364	0.01899149	0.345479855	0.555945459	0.014811706	0.001423151	0.016234858	0.026127406	0.013678	0.001321986	0.014999986	0.024140077
L117	LD	50	08:00 ~ 09:00	100	0.779957473	0.043313521	0.823270993	1.32492294	0.026574437	0.001405758	0.027980195	0.045029647	0.024470537	0.001256276	0.0025726813	0.041403189
L118	PD	50	08:00 ~ 09:00	1600	0.737860722	0.083002348	0.82086307	1.321047773	0.029176102	0.03068817	0.049387699	0.062829587	0.00132291	0.028152497	0.04530694	0.05430694
L119	PD	50	08:00 ~ 09:00	900	0.800296722	0.129749607	0.930046329	1.496760759	0.030114614	0.00124566	0.03133918	0.050435396	0.027692407	0.001063099	0.028755506	0.04627386
L120	LD	50	08:00 ~ 09:00	150	0.703393015	0.090483546	0.793876561	1.277617305	0.023743809	0.001444301	0.025188111	0.040536234	0.021857984	0.001289953	0.023147936	0.0372529
L121	LD	50	08:00 ~ 09:00	300	0.597235854	0.060748152	0.657984006	1.05891998	0.017488307	0.001412642	0.018900949	0.030418053	0.016102608	0.001254841	0.017357449	0.027934037
L122	LD	50	08:00 ~ 09:00	500	0.650435171	0.062110267	0.712545438	1.146727875	0.018215541	0.001044601	0.019260141	0.030996116	0.016773669	0.00093491	0.017708578	0

Table 5: Hourly composite emission rate for each road link

L154	LD	50	08:00 ~ 09:00	200	0.5817154	0.090832332	0.672547732	1.082357966	0.016093874	0.000902016	0.01699589	0.027352165	0.014819427	0.000813836	0.015633263	0.025159235
L155	LD	50	08:00 ~ 09:00	350	0.743942876	0.058988083	0.802940959	1.292205003	0.023485089	0.001269793	0.024754881	0.09839021	0.021621689	0.001133813	0.022755501	0.036621339
L156	LD	50	08:00 ~ 09:00	200	0.5811258844	0.097697923	0.678956767	1.092672284	0.016315345	0.000974899	0.017290244	0.027825882	0.015021807	0.000873421	0.015895228	0.025580026
L157	LD	50	08:00 ~ 09:00	150	0.57147569	0.072007707	0.643483397	1.03558357	0.016442654	0.001771127	0.018213782	0.029312167	0.015126882	0.001550822	0.0166677703	0.026840095
L158	LD	50	08:00 ~ 09:00	50	0.441476136	0.119057112	0.560533249	0.902088579	0.010014314	0.001585562	0.011599876	0.018668144	0.00920382	0.001381255	0.010585075	0.017034985
L159	PD	50	08:00 ~ 09:00	1100	0.836794121	0.07428745	0.911222866	1.466467407	0.035247621	0.001608094	0.036855716	0.059313377	0.032411952	0.001396199	0.03380815	0.054408808
L160	PD	50	08:00 ~ 09:00	1000	0.876510856	0.075573317	0.952084174	1.532227144	0.037430875	0.001568031	0.038989806	0.062762499	0.034420363	0.001360495	0.035780855	0.057583565
L161	PD	50	08:00 ~ 09:00	8000	0.89281732	0.094833238	0.589465549	0.934393134	0.001391132	0.035784266	0.057589051	0.031626815	0.001201691	0.032828506	0.052832228	0.057583565
L162	LD	50	08:00 ~ 09:00	450	0.542249284	0.07345928	0.615995212	0.991345735	0.016303105	0.001569403	0.017872507	0.028762941	0.015008173	0.001388348	0.016396521	0.026387577
L163	LD	50	08:00 ~ 09:00	550	0.565950865	0.066513734	0.632464598	0.97631242	0.013970116	0.000852528	0.014822644	0.023854673	0.012866598	0.000768591	0.013635189	0.021943655
L164	LD	50	08:00 ~ 09:00	50	0.519097414	0.022057747	0.541155162	0.870902648	0.019501641	0.00133995	0.020841591	0.033541206	0.017988331	0.00124709	0.019235241	0.030956332
L165	LD	50	08:00 ~ 09:00	100	0.399065763	0.194106115	0.593171878	0.954615231	0.014032685	0.005480115	0.01951528	0.03140273	0.012827892	0.004676951	0.017504842	0.028171243
L166	LD	50	08:00 ~ 09:00	600	0.548249399	0.066819804	0.615069204	0.989855472	0.01330624	0.000893416	0.014199566	0.022852075	0.012255332	0.000803289	0.013058621	0.021015761
L167	LD	50	08:00 ~ 09:00	400	0.55786547	0.072937276	0.630802746	1.015176091	0.016822443	0.001530937	0.01835338	0.029536829	0.01548649	0.001355368	0.016841858	0.027104276
L168	LD	50	08:00 ~ 09:00	250	0.423842182	0.057817272	0.481659454	0.775153826	0.011702151	0.001460711	0.013162862	0.02118352	0.010753795	0.001302196	0.012055991	0.019402188
L169	DD	50	08:00 ~ 09:00	300	0.488762144	0.087869098	0.576631242	0.927995723	0.015225946	0.00219432	0.017420265	0.02803513	0.01398223	0.001910169	0.015892399	0.025576274
L170	LD	50	08:00 ~ 09:00	150	0.620810984	0.063405648	0.684216632	1.101137195	0.01865887	0.001639161	0.020298031	0.032666433	0.01717861	0.001450476	0.018629085	0.029980532
L171	LD	50	08:00 ~ 09:00	50	0.680502468	0.185360342	0.86586281	1.393467654	0.024824618	0.001447528	0.026272146	0.02280816	0.02283991	0.001282849	0.024122859	0.038821882
L172	LD	50	08:00 ~ 09:00	50	0.507129184	0.054719314	0.561848499	0.904205263	0.015426875	0.001602129	0.017029004	0.027405458	0.014208426	0.001424006	0.015632432	0.025157898
L173	LD	50	08:00 ~ 09:00	450	0.57272987	0.10359163	0.676389033	1.088539926	0.01724454	0.001598569	0.018843023	0.030324831	0.015866416	0.001405505	0.017271921	0.022796393
L174	LD	50	08:00 ~ 09:00	650	0.745697619	0.064787648	0.810485266	1.304346358	0.02331137	0.001598257	0.024090626	0.040080858	0.021454679	0.001409931	0.02286461	0.036796931
L175	LD	50	08:00 ~ 09:00	450	0.750699678	0.056578567	0.807278245	1.299185171	0.02256644	0.001348481	0.023951321	0.038545819	0.02077532	0.0012312	0.02200652	0.035415973
L176	LD	50	08:00 ~ 09:00	250	0.718516975	0.09062295	0.809139925	1.302181248	0.02470047	0.002210253	0.026910724	0.043308504	0.022716909	0.00192101	0.024637918	0.039650788
L177	LD	50	08:00 ~ 09:00	50	0.375085233	0.263797025	0.638882258	1.028178773	0.012299549	0.001748659	0.019448208	0.031298778	0.01118538	0.006056741	0.017242123	0.027748438
L178	LD	50	08:00 ~ 09:00	50	0.4463691	0.364017022	0.810386122	1.304186802	0.0036863	0.008076446	0.011762744	0.018930258	0.00317105	0.006730372	0.009901422	0.015934757
L179	LD	50	08:00 ~ 09:00	250	0.701360774	0.085157825	0.786518599	1.265775842	0.024016047	0.002068813	0.02610286	0.024008377	0.022089545	0.001818316	0.023907587	0.038475877
L180	PD	50	08:00 ~ 09:00	1100	0.811498555	0.086931607	0.898430162	1.445879597	0.034515981	0.001688284	0.036204265	0.058264972	0.031737708	0.001464969	0.033202677	0.053434397
L181	PD	70	08:00 ~ 09:00	2300	0.522616311	0	0.522616311	0.841067334	0.015225125	0.015225125	0.024502402	0.013988404	0	0.013988404	0.022512097	0.022512097
L182	PD	70	08:00 ~ 09:00	3150	0.446399701	0	0.446399701	0.718408895	0.012467165	0.012467165	0.02063908	0.011461051	0	0.011461051	0.018447261	0.018447261
L183	PD	50	08:00 ~ 09:00	900	0.90167694	0.083728529	0.985405469	1.585852437	0.039220091	0.001563712	0.040783802	0.065635005	0.001356175	0.037421981	0.060224991	0.060224991
L184	PD	50	08:00 ~ 09:00	900	0.861889321	0.097167143	0.959056464	1.54344793	0.033466186	0.001569893	0.035036078	0.056384962	0.030772361	0.001355759	0.03212812	0.051705069
L185	DD	50	08:00 ~ 09:00	650	0.443663422	0.08033893	0.523697315	0.842807038	0.013263689	0.0017557	0.015019389	0.021815866	0.001545101	0.013730967	0.022097794	0.022097794
L186	DD	50	08:00 ~ 09:00	750	0.433551428	0.059040046	0.492591473	0.792747162	0.009854845	0.000810084	0.010664929	0.017163498	0.009063448	0.000737777	0.009801225	0.015773503
L187	LD	50	08:00 ~ 09:00	200	0.562818957	0.05641616	0.619235118	0.996559844	0.019613392	0.001360254	0.020973645	0.037744215	0.018071997	0.001231542	0.019303483	0.031065813
L188	LD	50	08:00 ~ 09:00	200	0.548920244	0.045000736	0.593920981	0.955820791	0.019035766	0.001100877	0.020136643	0.032406705	0.01755181	0.001023921	0.01857573	0.028984666
L189	LD	50	08:00 ~ 09:00	50	0.287197475	0.101530121	0.388727596	0.62559487	0.010865155	0.003104608	0.013969763	0.022482098	0.009982943	0.002699027	0.012681969	0.0204096
L190	LD	50	08:00 ~ 09:00	50	0.340326423	0.02761774	0.367948197	0.592153752	0.012005726	0.00120472	0.013210446	0.021260098	0.011089818	0.001118079	0.012207897	0.019646658
L191	LD	50	08:00 ~ 09:00	100	0.310180058	0.02865358	0.336045416	0.540811329	0.012164385	0.001309526	0.013473912	0.021684105	0.011238533	0.001217238	0.012545577	0.02004557
L192	LD	50	08:00 ~ 09:00	100	0.303851788	0.02819273	0.332044518	0.534372525	0.01192115	0.001256573	0.012448688	0.020034171	0.010342757	0.001167147	0.011509904	0.018523349
L193	LD	50	08:00 ~ 09:00	100	0.518287471	0.025776526	0.544063996	0.875583952	0.017684537	0.00116201	0.018846537	0.030330501	0.01631163	0.001080904	0.017392534	0.027990501
L194	LD	50	08:00 ~ 09:00	150	0.313050188	0.024820238	0.337870427	0.543748392	0.012754142	0.001344462	0.014098603	0.022689447	0.011782172	0.00125029	0.013032462	0.020973662
L195	LD	50	08:00 ~ 09:00	300	0.37129496	0.081076847	0.452371808	0.728020045	0.013074603	0.002515219	0.015589822	0.025089325	0.012032191	0.002201478	0.0142336613	0.022936613
L196	LD	50	08:00 ~ 09:00	250	0.435338871	0.040705555	0.385409426	0.620254805	0.012304878	0.001498778	0.013803655	0.022214775	0.011355066	0.001360421	0.012715487	0.020463542
L197	LD	50	08:00 ~ 10:00	250	0.799676003	0.050217531	0.851693534	1.370664472	0.022345233	0.000920282	0.023265516	0.037442125	0.020572146	0.000822209	0.021394175	0.034435052
L198	LD	50	08:00 ~ 10:00	280	0.880368778	0.08172899	0.972097768	1.564435622	0.024569666	0.000801488	0.025365453	0.040821638	0.022601726	0.000704615	0.023306341	0.037507828
L199	LD	50	08:00 ~ 10:00	50	0.592304709	0.036805072	0.620109781	1.012451535	0.020621043	0.000943077	0.012564119	0.0304704	0.000873919	0.0019879743	0.031979265	0.031979265
L200	LD	50	08:00 ~ 10:00	1100	0.002094888	0.055842455	0.058676844	1.700552311	0.038467286	0.000868714	0.039610388	0.063746581	0.035546102	0.000855633	0.036401735	0.058582768
L201	DD	50	09:00 ~ 10:00	900	1.370561572	0.050575869	1.421137395	2.287093255	0.035223932	0.000689374	0.030408971	0.054032646	0.030117124	0.000683873	0.029778712	0.039778712
L202	DD	50	09:00 ~ 10:00	300	1.050652223	0.076555239	1.12707462	1.814060576	0.029427764	0.000658937	0.027997612	0.040505767	0.025097928	0.000653163	0.025751991	0.041442262
L203	DD	50	09:00 ~ 10:00	500	0.919230422	0.06528584	0.984516261	1.5844214	0.026111157	0.000765629	0.027997612	0.040505767	0.025097928	0.000653163	0.025751991	0.041442262
L204	DD	50	09:00 ~ 10:00	200	0.962652603	0.047879299	1.010531902	1.626289411	0.027265823	0.000731789	0.027997612	0.040505767	0.025097928	0.000653163	0.025751991	0.041442262
L205	DD															

Table 5: Hourly composite emission rate for each road link

L040	LD		50	09:00 ~ 10:00	0.623623622	0.026854958	0.650478579	1.046841197	0.020126291	0.00108841	0.021214701	0.034141667	0.018554422	0.001012698	0.01956712	0.031490148
L041	LD		100	09:00 ~ 10:00	0.38564393	0.046181025	0.431824955	0.694953173	0.01296081	0.001548324	0.014509134	0.02235013	0.011950933	0.00138978	0.013340713	0.021469743
L042	LD		100	09:00 ~ 10:00	0.635571662	0.029975494	0.661547156	1.064645	0.020476159	0.001019061	0.02149522	0.034599118	0.018875331	0.000947326	0.019822657	0.031901394
L043	LD		250	09:00 ~ 10:00	1.269504596	0.049139321	1.318643917	2.122146401	0.036576005	0.000655856	0.037231861	0.059918723	0.033660153	0.000581856	0.034242009	0.055107304
L044	LD		400	09:00 ~ 10:00	0.888247363	0.071175761	0.959423124	1.54403801	0.026937175	0.000970735	0.027907911	0.044913317	0.024795923	0.000869246	0.02566517	0.041303984
L045	LD		200	09:00 ~ 10:00	0.568318946	0.044970522	0.613289468	0.986991272	0.017631708	0.001361182	0.01899289	0.02566018	0.016246599	0.001225602	0.017472201	0.028118712
L046	LD		200	09:00 ~ 10:00	0.470011159	0.027944002	0.497985651	0.801428082	0.015392801	0.00109357	0.016486371	0.026532176	0.014201744	0.001015632	0.015217375	0.024489931
L047	DD		1350	09:00 ~ 10:00	0.992855946	0.064445876	1.057301822	1.701558114	0.038054909	0.000836764	0.038891673	0.062589924	0.035002537	0.000749338	0.035751875	0.057536923
L048	DD		400	09:00 ~ 10:00	1.979736099	0.08062271	2.060358369	3.315817138	0.085023951	0.000587646	0.085611597	0.137778167	0.078211937	0.000513823	0.07872576	0.126695114
L049	DD		600	09:00 ~ 10:00	0.568851099	0.04808194	0.618659293	0.995633147	0.014809807	0.001749802	0.013578889	0.025071729	0.013621703	0.000694327	0.01431603	0.02303961
L050	DD		500	09:00 ~ 10:00	0.720074039	0.068727617	0.788801655	1.269450056	0.024211408	0.01024205	0.025235613	0.040612681	0.022266649	0.000913119	0.037303871	0.07303871
L051	DD		600	09:00 ~ 10:00	0.573596128	0.050267475	0.623863603	1.00408652	0.01502579	0.000757385	0.015783155	0.025400463	0.013820501	0.000683176	0.014503676	0.023341347
L052	DD		400	09:00 ~ 10:00	0.779934034	0.064411703	0.846345738	1.36205805	0.026708024	0.000792245	0.027500268	0.044257282	0.024566191	0.000715413	0.025281605	0.040686698
L053	DD		750	09:00 ~ 10:00	0.578564763	0.061004561	0.639569324	1.029284496	0.015844404	0.000848587	0.016692991	0.026864698	0.0145717	0.000738785	0.015335485	0.024680009
L054	DD		450	09:00 ~ 10:00	0.836115585	0.064184784	0.900300369	1.448889395	0.000706727	0.00929281	0.07142092	0.026297435	0.000645232	0.026942667	0.043599912	0.073599912
L055	DD		150	09:00 ~ 10:00	0.645029052	0.037935083	0.682964136	1.099121502	0.020901262	0.001335626	0.022237788	0.035788162	0.019259827	0.001209714	0.020469541	0.032942451
L056	LD		100	09:00 ~ 10:00	0.22109711	0.043840134	0.264937244	0.426374104	0.011163318	0.001873063	0.013036382	0.020979771	0.010308536	0.001693122	0.019314749	0.019314749
L057	DD		800	09:00 ~ 10:00	0.599124502	0.060406717	0.659531218	1.061409971	0.015908919	0.000775781	0.0166847	0.026851355	0.014631411	0.000697486	0.015328897	0.024669407
L058	DD		500	09:00 ~ 10:00	0.900571757	0.059116506	0.959688262	1.544464708	0.029800601	0.000548123	0.030348924	0.048841738	0.027416941	0.000504828	0.027921777	0.044935621
L059	LD		200	09:00 ~ 10:00	0.99866074	0.029915372	1.028576113	1.655328681	0.031620987	0.000823285	0.032444273	0.052213866	0.029123641	0.000763341	0.029886892	0.048089311
L060	LD		200	09:00 ~ 10:00	1.110178023	0.044673923	1.154851946	1.856549431	0.0343853	0.000682425	0.035353545	0.056895874	0.031652299	0.000865523	0.032517822	0.052323235
L061	LD		50	09:00 ~ 10:00	0.360229061	0.019136512	0.379365573	0.610528191	0.014912044	0.001446491	0.016358535	0.026326445	0.013771677	0.001347115	0.015118792	0.024331277
L062	LD		150	09:00 ~ 10:00	0.775992604	0.036356996	0.8123496	1.307346706	0.000977336	0.000977336	0.024649674	0.021806365	0.000897974	0.022704339	0.036539001	0.036539001
L063	LD		50	09:00 ~ 10:00	0.408177243	0.024504635	0.432681878	0.696332254	0.016389309	0.001086324	0.017475633	0.028124235	0.015115721	0.001001737	0.016117458	0.02593847
L064	LD		100	09:00 ~ 10:00	0.430277321	0.037991526	0.568265247	0.914392111	0.03862117	0.029377259	0.013202222	0.028377259	0.030307566	0.016509787	0.026569861	0.026569861
L065	DD		700	09:00 ~ 10:00	1.347730358	0.059917871	1.407648229	2.2653846	0.05825736	0.000682514	0.056508249	0.090940986	0.051352845	0.000612895	0.05196574	0.083630543
L066	DD		500	09:00 ~ 10:00	1.504679785	0.066271024	1.570950809	2.528193974	0.06105803	0.0052212	0.06158015	0.09103399	0.056167802	0.056632547	0.091141024	0.091141024
L067	DD		700	09:00 ~ 10:00	1.336296139	0.061751816	1.398047955	2.249934496	0.055323888	0.000731367	0.056055254	0.090211963	0.050809539	0.000653818	0.051543537	0.082952395
L068	DD		900	09:00 ~ 10:00	1.070808742	0.081846604	1.152675346	1.855017574	0.040000158	0.000833631	0.06571545	0.03679135	0.00736993	0.00736993	0.060395864	0.060395864
L069	LD		550	09:00 ~ 10:00	0.909067784	0.055382143	0.964449927	1.552127845	0.026837203	0.001060206	0.027897409	0.044896416	0.024704467	0.000944126	0.025648593	0.041277307
L070	LD		500	09:00 ~ 10:00	0.93024578	0.05465182	0.984903962	1.585045341	0.027326789	0.001005469	0.028242258	0.045451396	0.025071738	0.000895358	0.025967097	0.041789887
L071	LD		250	09:00 ~ 10:00	0.772462426	0.049594896	0.820257322	1.32296973	0.022023852	0.000967675	0.022991527	0.037001184	0.020277969	0.000866245	0.021144214	0.03402823
L072	LD		200	09:00 ~ 10:00	1.086499489	0.047100548	1.133600037	1.824347883	0.030489647	0.000644593	0.031134239	0.051005577	0.028064735	0.000582614	0.028647349	0.046103324
L073	LD		200	09:00 ~ 10:00	0.766256331	0.052913455	0.819178786	1.318337188	0.016390292	0.00020095	0.016591243	0.026700951	0.015086724	0.000184276	0.015217	0.024576231
L074	LD		200	09:00 ~ 10:00	0.575762548	0.049836022	0.62559857	1.006800802	0.01325266	0.000637293	0.01388953	0.022353656	0.012209736	0.000583358	0.012792736	0.020587631
L075	DD		950	09:00 ~ 10:00	0.546083801	0.059894541	0.605978341	0.975225184	0.014001577	0.000781009	0.014782586	0.023790206	0.012876097	0.000702347	0.013578444	0.021852333
L076	DD		450	09:00 ~ 10:00	0.886964727	0.067262224	0.95423095	1.535682038	0.030327942	0.000761217	0.03108916	0.050033028	0.02789958	0.000689271	0.012588885	0.04600981
L077	LD		400	09:00 ~ 10:00	1.196010332	0.055648985	1.251660227	2.01434687	0.034737592	0.000779857	0.035517449	0.057159652	0.031967476	0.000692047	0.032659523	0.052560276
L078	DD		500	09:00 ~ 10:00	1.583719662	0.060500033	1.644219695	2.646108523	0.068972253	0.000589628	0.069561882	0.111948719	0.063445027	0.000531108	0.063976135	0.102959535
L079	DD		850	09:00 ~ 10:00	1.096820197	0.048509021	1.180879218	1.900436161	0.041414076	0.000825984	0.04224006	0.067978619	0.038091989	0.000729591	0.038821581	0.062477123
L080	DD		500	09:00 ~ 10:00	1.606357155	0.062717621	1.669074776	2.6861088	0.070023948	0.000643413	0.070667361	0.11372781	0.064411391	0.000574862	0.064986172	0.104584846
L081	DD		850	09:00 ~ 10:00	1.08112762	0.087907178	1.169034797	1.881374461	0.040782684	0.000950453	0.041733137	0.067162806	0.037509238	0.000834389	0.038343627	0.061707932
L082	LD		100	09:00 ~ 10:00	0.355628908	0.090938203	0.446567111	0.718678314	0.012612933	0.002762954	0.015375887	0.02745053	0.01601227	0.002412485	0.0122552828	0.0122552828
L083	LD		100	09:00 ~ 10:00	0.476768103	0.05868698	0.535455083	0.861729283	0.015898867	0.001784221	0.017683087	0.028458099	0.014640404	0.001577396	0.016217858	0.026100040
L084	LD		50	09:00 ~ 10:00	0.766792802	0.011295013	0.778087515	1.252207845	0.025448259	0.000825183	0.026273942	0.023446704	0.000760655	0.024207339	0.038957871	0.038957871
L085	LD		50	09:00 ~ 10:00	0.368970968	0.018404403	0.387375371	0.62341868	0.015347509	0.001382809	0.016730318	0.02692477	0.014170147	0.002185521	0.015455668	0.034712359
L086	LD		100	09:00 ~ 10:00	0.506775575	0.09154019	0.598316164	0.962891364	0.01853171	0.002891775	0.017040024	0.034477672	0.021429774	0.002515862	0.015455668	0.034712359
L087	LD		650	09:00 ~ 10:00	0.657248796	0.08397351	0.741146146	1.192756139	0.019894519	0.001535255	0.021429774	0.034477672	0.021429774	0.002515862	0.015455668	0.034712359
L088	LD		50	09:00 ~ 10:00	0.636217171	0.079247742	0.715464912	1.151426302	0.01812135	0.002031062	0.020152413	0.034320987	0.016668187	0.017680807	0.018428995	0.029658518
L089	LD		350	09:00 ~ 10:00	0.583142545	0.058798561	0.641941106	1.0331015	0.016419535	0.001218181	0.017637716	0.028385082	0.015118215	0.001080988	0.026070025	0.026070025
L090	LD		500	09:00 ~ 10:00	0.796965477	0.096424006	0.893389483	1.43776743	0.024463383	0.000854814	0.025317697	0.040744783	0.022516495	0.00076609	0.023285284	0.037469594
L091	LD		100	0												

Table 5: Hourly composite emission rate for each road link

L122	LD	50	09:00 ~ 10:00	450	0.733478203	0.058346113	0.791824316	1.274314544	0.020073151	0.0008369	0.020910051	0.033651381	0.01848115	0.000751396	0.019232545	0.030951705
L123	LD	50	09:00 ~ 10:00	150	0.661426473	0.075726291	0.737152764	1.186329429	0.021553438	0.001204658	0.022758006	0.036625369	0.019851175	0.001086928	0.020990381	0.036369526
L124	LD	50	09:00 ~ 10:00	50	0.264825987	0.027824365	0.470973918	0.001599188	0.001249778	0.002678356	0.010715834	0.001158555	0.001174839	0.001158555	0.001174839	0.019109993
L125	LD	50	09:00 ~ 10:00	50	0.29803967	0.028323843	0.326365314	0.525229857	0.012225601	0.001180429	0.01340603	0.021574861	0.001289454	0.001093183	0.012382637	0.019927637
L126	PD	50	09:00 ~ 10:00	850	0.848623916	0.116239201	0.964683118	1.55279281	0.032395084	0.000867738	0.03262822	0.05353119	0.029794552	0.000575825	0.030552378	0.049169163
L127	DD	50	09:00 ~ 10:00	100	0.314096509	0.027302823	0.341399331	0.5494276	0.011095211	0.001167677	0.012262888	0.019735157	0.01020456	0.001084682	0.011289242	0.011682292
L128	LD	50	09:00 ~ 10:00	300	0.789883419	0.064946393	0.854629812	1.375389942	0.02304065	0.001007212	0.024047867	0.038701195	0.021213485	0.000903652	0.022117137	0.035593993
L129	LD	50	09:00 ~ 10:00	400	0.763816062	0.066291233	0.830108195	1.335926322	0.020046703	0.000859316	0.020906019	0.033648893	0.0184511	0.000761217	0.019212318	0.030919151
L130	LD	50	09:00 ~ 10:00	400	0.82335249	0.069882436	0.893234926	1.437518696	0.021133776	0.000795343	0.021929119	0.035291409	0.019446325	0.000697561	0.020148366	0.032418361
L131	LD	50	09:00 ~ 10:00	300	0.789883419	0.064946393	0.854629812	1.375389942	0.02304065	0.001007212	0.024047867	0.038701195	0.021213485	0.000903652	0.022117137	0.035593993
L132	PD	50	09:00 ~ 10:00	1450	0.724692679	0.098321462	0.823014141	1.324509577	0.025034658	0.000972362	0.02600702	0.041854137	0.023024004	0.000846002	0.023870041	0.038415013
L133	DD	50	09:00 ~ 10:00	1000	0.749550284	0.055827193	0.805377477	1.296126189	0.025737097	0.001064604	0.026801701	0.043133049	0.023667854	0.00095019	0.024618044	0.039618803
L134	DD	50	09:00 ~ 10:00	700	0.836806486	0.056422929	0.893229415	1.437509826	0.029869119	0.000898989	0.030769019	0.049517812	0.027470242	0.00080514	0.028275382	0.045504703
L135	LD	50	09:00 ~ 10:00	200	0.629370094	0.051922758	0.681292672	1.096431549	0.018616964	0.001258801	0.019875764	0.031986862	0.017141352	0.001120929	0.018622821	0.029390219
L136	PD	50	09:00 ~ 10:00	1000	0.900110439	0.06026145	0.966136584	1.554842725	0.038558427	0.001303646	0.039862073	0.064151629	0.03546247	0.001132716	0.036595186	0.058894096
L137	PD	50	09:00 ~ 10:00	1600	0.673806965	0.076430296	0.750237261	1.207386834	0.024694404	0.00088953	0.041173247	0.022714873	0.000790982	0.023505855	0.037829912	0.037829912
L138	LD	50	09:00 ~ 10:00	300	0.729567662	0.06698696	0.796536558	1.281930009	0.021529018	0.001097152	0.02262617	0.036413201	0.019821374	0.000982152	0.020803525	0.033479945
L139	LD	50	09:00 ~ 10:00	200	0.85729809	0.082079263	0.939377353	1.511777549	0.0221573504	0.00072195	0.022295299	0.035880716	0.019847692	0.000629838	0.02047753	0.032955308
L140	LD	50	09:00 ~ 10:00	250	0.740414106	0.057500179	0.787974285	1.284115376	0.020407794	0.000639599	0.021047344	0.039872332	0.000594071	0.019388807	0.031201896	0.031201896
L141	PD	50	09:00 ~ 10:00	1400	0.712741502	0.081816387	0.794557889	1.278713793	0.026982116	0.001455131	0.031607793	0.034773481	0.028166573	0.000903889	0.025720561	0.041393128
L142	LD	50	09:00 ~ 10:00	250	0.652996512	0.074275884	0.727272352	1.170428488	0.020152162	0.000696599	0.021869315	0.018966599	0.018966599	0.00053706	0.013346386	0.021478872
L143	LD	50	09:00 ~ 10:00	400	0.72733967	0.072436002	0.799770174	1.287102131	0.018269315	0.001126641	0.011689994	0.018813175	0.010380697	0.00039242	0.01077316	0.01737607
L144	LD	50	09:00 ~ 10:00	200	0.563812152	0.086196999	0.65000885	1.046085243	0.013635943	0.000888142	0.014524086	0.023374192	0.015252118	0.000794268	0.01737607	0.01737607
L145	LD	50	09:00 ~ 10:00	100	0.578885434	0.046380384	0.62565818	1.006265291	0.01126641	0.000423553	0.011689994	0.018813175	0.010380697	0.00039242	0.01077316	0.01737607
L146	PD	50	09:00 ~ 10:00	1000	0.908086797	0.064938072	0.973024869	1.565927843	0.038947192	0.001287305	0.040234496	0.064750984	0.035820381	0.00111922	0.03699601	0.059448377
L147	PD	50	09:00 ~ 10:00	1200	0.766885401	0.08301116	0.850186517	1.368239169	0.030047	0.001075939	0.031122939	0.027635252	0.000942195	0.028577421	0.045990787	0.045990787
L148	LD	50	09:00 ~ 10:00	250	0.666705541	0.074522157	0.741227688	1.192887383	0.020539045	0.001453998	0.021993042	0.035394283	0.018906664	0.001287909	0.020194573	0.032499944
L149	LD	50	09:00 ~ 10:00	600	0.66952991	0.077251318	0.746781229	1.201824903	0.016496772	0.000749962	0.017246734	0.027755859	0.015184693	0.000669548	0.025514864	0.025514864
L150	LD	50	09:00 ~ 10:00	250	0.711678897	0.08609381	0.798488277	1.285039124	0.018803969	0.001014955	0.018918924	0.031895387	0.017300504	0.000891377	0.018193181	0.029276921
L151	LD	50	09:00 ~ 10:00	650	0.758000063	0.08123157	0.839231633	1.350609036	0.024637831	0.001025519	0.02566335	0.041301056	0.022674318	0.000913146	0.037960249	0.037960249
L152	LD	50	09:00 ~ 10:00	700	0.902260427	0.070140592	0.972401019	1.564923856	0.030323818	0.001034681	0.031358499	0.050466488	0.027908304	0.00092433	0.028832694	0.046401511
L153	LD	50	09:00 ~ 10:00	300	0.882699074	0.059727863	0.942426937	1.516685367	0.026991185	0.000903623	0.027894508	0.024884768	0.025585894	0.0011816	0.025585894	0.041294029
L154	LD	50	09:00 ~ 10:00	200	0.595564722	0.090696869	0.686261591	1.104428228	0.015615551	0.000803198	0.016418748	0.026423348	0.014377161	0.000721568	0.01509873	0.024298989
L155	LD	50	09:00 ~ 10:00	350	0.852276439	0.056781759	0.909058198	1.462983721	0.026142123	0.001032582	0.027174705	0.04273334	0.024065214	0.000921494	0.024986708	0.040212108
L156	LD	50	09:00 ~ 10:00	150	0.596745996	0.093813746	0.690559741	1.111345414	0.015936478	0.00078032	0.016716798	0.026903012	0.014671227	0.000702651	0.015373921	0.024741866
L157	LD	50	09:00 ~ 10:00	150	0.642974897	0.068821738	0.711796635	1.145522797	0.018049844	0.001530472	0.019580316	0.01511386	0.01660662	0.001336747	0.017943362	0.028878674
L158	LD	50	09:00 ~ 10:00	50	0.444213171	0.106059057	0.553272229	0.890403129	0.009101817	0.001190321	0.010292138	0.016565439	0.008367759	0.001044779	0.009412537	0.015147772
L159	PD	50	09:00 ~ 10:00	1000	0.908086797	0.064938072	0.973024869	1.565927843	0.038947192	0.001287305	0.040234496	0.064750984	0.035820381	0.00111922	0.03699601	0.059448377
L160	PD	50	09:00 ~ 10:00	900	0.955440496	0.065844499	1.02129495	1.643610888	0.041659597	0.001215263	0.04287486	0.069000227	0.0383158	0.001056295	0.039372095	0.063360362
L161	PD	50	09:00 ~ 10:00	800	0.948549723	0.083281885	1.031831608	1.66056788	0.037114957	0.001062105	0.038177062	0.034136923	0.000919116	0.030505609	0.056417086	0.056417086
L162	LD	50	09:00 ~ 10:00	400	0.628225334	0.069121194	0.697346528	1.122267662	0.018163951	0.001302884	0.019466835	0.031328757	0.016720399	0.001154692	0.01787506	0.02876705
L163	LD	50	09:00 ~ 10:00	500	0.620279576	0.065829476	0.686109052	1.104182742	0.014783926	0.000705825	0.015489751	0.024928276	0.013612965	0.000635844	0.014248839	0.022931226
L164	LD	50	09:00 ~ 10:00	50	0.617902667	0.020439264	0.638341932	1.027309204	0.021895462	0.001200152	0.023094714	0.037167247	0.020186341	0.001115323	0.021301664	0.03428162
L165	LD	50	09:00 ~ 10:00	100	0.45856325	0.187038675	0.645601925	1.038993002	0.015765382	0.005282495	0.021047876	0.03873189	0.014431504	0.004508713	0.018940216	0.030481248
L166	LD	50	09:00 ~ 10:00	550	0.602888045	0.06548073	0.668368775	1.075632605	0.014202398	0.000721274	0.014923672	0.024017262	0.01307801	0.000649061	0.013727071	0.022091525
L167	LD	50	09:00 ~ 10:00	400	0.640874689	0.068848103	0.709722792	1.142185278	0.018602747	0.00129457	0.019897317	0.032021548	0.01712466	0.001146756	0.018271417	0.029404121
L168	DD	50	09:00 ~ 10:00	200	0.504778554	0.055200703	0.559979257	0.901197017	0.013585838	0.001249041	0.014834879	0.023874364	0.012486701	0.001144278	0.013600978	0.021888598
L169	DD	50	09:00 ~ 10:00	250	0.572874902	0.079701689	0.652576591	1.05021761	0.017356285	0.00182291	0.019179195	0.00658646	0.015946635	0.001588177	0.017534812	0.028219474
L170	LD	50	09:00 ~ 10:00	150	0.740024216	0.054906471	0.794930632	1.279313663	0.021508687	0.00129082	0.022799506	0.03692158	0.019801794	0.001146663	0.020948456	0.033713188
L171	LD	50	09:00 ~ 10:00	50	0.697228613	0.181005144	0.878233756	1.413376714	0.025100907	0.001088379	0.022147466	0.026189286	0.023094685	0.000980104	0.024074789	0.038744521
L172	LD	50	09:00 ~ 10:00	50	0.571202158	0.068013635	0.639215793	1.028715544	0.016533894	0.001710372	0.018244266	0.023961226	0.015219612	0.001492304	0.016711	

Table 5: Hourly composite emission rate for each road link

L008	LD		50	10:00 ~ 11:00	150	0.925394902	0.047177595	0.972572497	1.565199822	0.026451681	0.000745512	0.027197192	0.043769529	0.024344783	0.000655419	0.025000202	0.040233825
L009	LD		50	10:00 ~ 11:00	250	0.626527482	0.059328977	0.685856459	1.103776234	0.020444789	0.001546648	0.021991436	0.035391698	0.018820222	0.001364272	0.020184844	0.032483714
L010	LD		50	10:00 ~ 11:00	50	0.358462025	0.122577662	0.481039687	0.774156409	0.010020306	0.003075253	0.013095558	0.021075206	0.009186398	0.002637602	0.011824	0.019028837
L011	LD		50	10:00 ~ 11:00	150	0.93349621	0.077632054	0.044135844	1.57334237	0.007797151	0.000793634	0.027990784	0.045046688	0.025032446	0.00070026	0.025732706	0.041412673
L012	LD		50	10:00 ~ 11:00	50	0.681662513	0.034106438	0.695768951	1.119728803	0.022524943	0.001253681	0.023778624	0.038267891	0.020746443	0.001128873	0.021875315	0.035204801
L013	LD		50	10:00 ~ 11:00	150	0.624598588	0.065103642	0.707037895	1.070377895	0.018265379	0.000840896	0.019106275	0.0307748493	0.016822166	0.000753995	0.017576181	0.028286052
L014	DD		50	10:00 ~ 11:00	1050	0.9680716	0.06834282	1.03641442	1.667943182	0.0363103	0.000978275	0.027288575	0.000900995	0.0333902	0.000857527	0.034247277	0.055116237
L015	DD		50	10:00 ~ 11:00	400	1.7001739	0.045644619	1.745818519	2.809615575	0.0715033	0.000652117	0.072155417	0.116122598	0.0657769	0.000561565	0.066333086	0.106752488
L016	LD		50	10:00 ~ 11:00	100	0.508391832	0.0490211	0.557412932	0.89706928	0.011581147	0.000751744	0.012332891	0.019847814	0.010671638	0.000681199	0.011352837	0.018270574
L017	LD		50	10:00 ~ 11:00	150	0.93349621	0.044135844	0.077632054	1.57334237	0.007797151	0.000793634	0.027990784	0.045046688	0.025032446	0.00070026	0.025732706	0.041412673
L018	LD		50	10:00 ~ 11:00	250	0.626527482	0.059328977	0.685856459	1.103776234	0.020444789	0.001546648	0.021991436	0.035391698	0.018820222	0.001364272	0.020184844	0.032483714
L019	LD		50	10:00 ~ 11:00	250	0.910691643	0.073769452	0.984461095	1.584332619	0.028754563	0.001837276	0.030591839	0.04923267	0.026447854	0.001582566	0.028030419	0.045110475
L020	LD		50	10:00 ~ 11:00	350	0.695133565	0.067463053	0.762596618	1.227277241	0.019880579	0.001459956	0.021340535	0.034344177	0.018293213	0.001273331	0.019566544	0.031489221
L021	DD		50	10:00 ~ 11:00	950	1.0239505	0.068267438	1.092217938	1.757750016	0.0388747	0.000931965	0.029806665	0.064062458	0.0357493	0.000814065	0.036563365	0.058842886
L022	DD		50	10:00 ~ 11:00	450	1.4962586	0.070102702	1.566361302	2.520807898	0.0615593	0.000598489	0.062157789	0.100033016	0.0566215	0.000526336	0.057147836	0.091970298
L023	LD		50	10:00 ~ 11:00	350	0.95498049	0.046934942	1.001915431	1.61242258	0.0279673	0.000893861	0.028861161	0.04644742	0.025738797	0.000784582	0.026523379	0.042685135
L024	LD		50	10:00 ~ 11:00	250	0.46074938	0.04978613	0.51053551	0.821625218	0.015899731	0.001507946	0.017407677	0.028014871	0.01464867	0.001347914	0.015996584	0.025743943
L025	DD		50	10:00 ~ 11:00	100	0.970897	0.020776581	0.991673581	1.595939961	0.0330706	0.000725502	0.033796102	0.054389419	0.030422	0.000669375	0.031091375	0.050036593
L026	DD		50	10:00 ~ 11:00	100	0.9700156	0.02030001	0.99031561	1.593754525	0.033126	0.000689411	0.038815411	0.054420493	0.030473	0.000635744	0.031108944	0.050064868
L027	DD		50	10:00 ~ 11:00	400	0.7711042	0.062713106	0.833817306	1.341895543	0.0238122	0.000834702	0.024646902	0.039665245	0.021894	0.000733783	0.022627783	0.036415786
L028	DD		50	10:00 ~ 11:00	300	0.7917598	0.065848333	0.857608133	1.380183072	0.0246068	0.00074582	0.02535262	0.040800985	0.0226244	0.000654078	0.023278478	0.037462985
L029	LD		50	10:00 ~ 11:00	100	0.423326096	0.02303828	0.446364325	0.718351962	0.015428409	0.001153882	0.016582291	0.026868544	0.014236094	0.0010707	0.015306795	0.024633837
L030	DD		50	10:00 ~ 11:00	1200	0.9268802	0.070443101	0.997323301	1.605032282	0.0347371	0.001050603	0.035793163	0.057603369	0.0319411	0.000972268	0.032863778	0.052888992
L031	DD		50	10:00 ~ 11:00	1000	0.2370551	0.071736683	0.308791783	0.496950968	0.0052268	0.000871652	0.006098452	0.009814482	0.0047968	0.000763139	0.005599399	0.008947833
L032	DD		50	10:00 ~ 11:00	450	1.4225016	0.089413486	1.511815086	2.433185425	0.0599076	0.001028679	0.060079679	0.091688683	0.0543323	0.000870739	0.038840458	0.059230339
L033	LD		50	10:00 ~ 11:00	250	0.554524108	0.082988887	0.637519289	1.028975163	0.018792404	0.002404416	0.02119282	0.034106453	0.01728092	0.002074421	0.019355341	0.031149324
L034	LD		50	10:00 ~ 11:00	250	0.542872407	0.064291383	0.607163789	0.977132973	0.01806119	0.00190903	0.019970219	0.032138873	0.01662344	0.001668048	0.029139488	0.029437324
L035	LD		50	10:00 ~ 11:00	50	0.674815879	0.017524939	0.692340818	1.114211773	0.025580989	0.00131346	0.026902549	0.043295348	0.023582795	0.001220149	0.024802944	0.03991637
L036	LD		50	10:00 ~ 11:00	50	0.585074066	0.01830725	0.603404792	0.971083467	0.022879621	0.001379975	0.024259596	0.039041938	0.022109379	0.001283632	0.023377429	0.036012892
L037	LD		50	10:00 ~ 11:00	250	0.517179144	0.067169466	0.58434861	0.940415592	0.017145931	0.00193596	0.019139527	0.030802006	0.015780128	0.001740936	0.017521064	0.028197349
L038	LD		50	10:00 ~ 11:00	300	0.513403444	0.058104171	0.571507614	0.919750064	0.018823532	0.001768056	0.020293462	0.015704098	0.01554281	0.001725879	0.017277499	0.027774599
L039	LD		50	10:00 ~ 11:00	50	0.472060282	0.050467396	0.522527678	0.840924693	0.015281219	0.001573439	0.016854658	0.027124875	0.014081633	0.001395021	0.015476654	0.024907198
L040	LD		50	10:00 ~ 11:00	50	0.640021506	0.064436198	0.644436198	1.069303751	0.022133688	0.000994305	0.022133688	0.035520631	0.019481706	0.000921252	0.020400297	0.032835295
L041	LD		50	10:00 ~ 11:00	100	0.506509924	0.043190511	0.549700434	0.884654897	0.016896464	0.001415294	0.018311757	0.029469844	0.01556599	0.001262814	0.016829319	0.027084232
L042	LD		50	10:00 ~ 11:00	100	0.556008311	0.025464418	0.581472729	0.935787321	0.018675872	0.000963881	0.019639753	0.03160704	0.017215428	0.000891399	0.018106027	0.029140041
L043	LD		50	10:00 ~ 11:00	250	0.961262905	0.05898787	1.020250775	1.641930383	0.02757957	0.001059116	0.028638687	0.046089384	0.025378631	0.000924346	0.026302977	0.042330433
L044	LD		50	10:00 ~ 11:00	350	0.782911979	0.074052782	0.866964761	1.395241068	0.024819151	0.001159464	0.025978615	0.041808424	0.022841405	0.001023815	0.02386522	0.038407253
L045	LD		50	10:00 ~ 11:00	150	0.548494768	0.074142369	0.595637137	0.95858267	0.017732541	0.001462497	0.019195038	0.030891343	0.016333027	0.001303497	0.017636524	0.028983164
L046	LD		50	10:00 ~ 11:00	200	0.536568683	0.026076235	0.562644918	0.905486972	0.01739652	0.000951833	0.018348353	0.029528739	0.016040724	0.000880818	0.016920904	0.027231488
L047	DD		50	10:00 ~ 11:00	1150	0.9344554	0.070311177	1.004766577	1.617011042	0.0354616	0.001058737	0.036520337	0.068773639	0.0326709	0.000927153	0.033535053	0.059693902
L048	DD		50	10:00 ~ 11:00	350	1.5641834	0.09670068	1.66088408	2.672927185	0.0663723	0.000965737	0.067338037	0.108369797	0.061045	0.000803707	0.061882037	0.09589238
L049	DD		50	10:00 ~ 11:00	550	0.5657088	0.052577527	0.618286327	0.995032918	0.0157861	0.000938692	0.016724792	0.026915877	0.0145117	0.000830204	0.015341904	0.024690393
L050	DD		50	10:00 ~ 11:00	450	0.6709908	0.071041133	0.742031933	1.194181671	0.0236289	0.00127075	0.02489965	0.040072003	0.0217211	0.001117606	0.022838706	0.036755243
L051	DD		50	10:00 ~ 11:00	500	0.5811779	0.053810366	0.634988266	1.021912017	0.0161535	0.000945394	0.017098894	0.027517934	0.0148494	0.000833555	0.015682955	0.025239207
L052	DD		50	10:00 ~ 11:00	400	0.7133736	0.067664372	0.781037972	1.256955649	0.0255092	0.001398888	0.026549088	0.04726551	0.0234547	0.000922949	0.024377649	0.02931926
L053	DD		50	10:00 ~ 11:00	700	0.5681372	0.063539817	0.631677017	1.01658309	0.0167952	0.001012023	0.017807223	0.028657876	0.0153485	0.000895275	0.016333775	0.022686597
L054	DD		50	10:00 ~ 11:00	400	0.7681561	0.066645105	0.834801205	1.343478971	0.0268885	0.000928767	0.028717267	0.044767441	0.02555041	0.000828041	0.01412675	0.02555041
L055	LD		50	10:00 ~ 11:00	150	0.588675812	0.049783545	0.638459358	1.027498183	0.018777647	0.001531202	0.020308849	0.032683843	0.017292279	0.00135816	0.018650439	0.030014898
L056	LD		50	10:00 ~ 11:00	100	0.343915015	0.047844235	0.391759249	0.63047383	0.012570018	0.001540113	0.014110131	0.022707998	0.011589302	0.001296954	0.012969544	0.027827407
L057	DD		50	10:00 ~ 11:00	700	0.5707532	0.062574363	0.633327563	1.01923938	0.0165175	0.000968666	0.017484366	0.028138289	0.0151834	0.00085545	0.01603885	0.025811962
L058	DD		50	10:00 ~ 11:00	400	0.8027708	0.062579133	0.865349933	1.392642261	0.0276675	0.0						

Table 5: Hourly composite emission rate for each road link

L090	LD		50	10:00 ~ 11:00	450	0.811118235	0.095935217	0.907053453	1.459757404	0.025224066	0.000787491	0.026011557	0.041861439	0.023211826	0.000698027	0.023909853	0.038479083
L091	LD		50	10:00 ~ 11:00	100	0.65534406	0.051701174	0.707045234	1.137876177	0.021852115	0.001542269	0.023394384	0.037649518	0.020120085	0.001367335	0.021487419	0.034580563
L092	LD		50	10:00 ~ 11:00	350	0.876472411	0.11978888	0.996271249	1.603339172	0.02718162	0.000860072	0.028041693	0.045128617	0.025004785	0.000754329	0.025759114	0.041455172
L093	LD		50	10:00 ~ 11:00	100	0.675130079	0.070595054	0.745725133	1.200125285	0.02080602	0.00168542	0.02254562	0.036283568	0.019191707	0.001468908	0.020660614	0.033249953
L094	LD		50	10:00 ~ 11:00	300	0.884500108	0.126056133	1.010556241	1.626328581	0.028645922	0.000655226	0.029301147	0.047155508	0.026357739	0.000595767	0.026953506	0.043377355
L095	LD		50	10:00 ~ 11:00	450	0.641137408	0.068735769	0.709873178	1.1424273	0.018892707	0.000973764	0.019866471	0.031971906	0.017398483	0.000885526	0.018287009	0.029430015
L096	LD		50	10:00 ~ 11:00	300	0.9810392	0.118953714	1.099632914	1.769683233	0.031430365	0.000634318	0.032064683	0.051602978	0.028921391	0.000576695	0.029498086	0.04747245
L097	LD		50	10:00 ~ 11:00	350	0.917047629	0.115101361	1.03214899	1.661078655	0.029759437	0.000964271	0.030759708	0.049502829	0.027415062	0.000857753	0.028272814	0.054005571
L098	LD		50	10:00 ~ 11:00	100	0.528777234	0.075759231	0.604536465	0.972904715	0.019509261	0.02344415	0.021853676	0.035169995	0.017941443	0.002030125	0.019971567	0.032141042
L099	LD		50	10:00 ~ 11:00	550	0.785931288	0.074337412	0.8602687	1.384464829	0.024440003	0.001268013	0.025512016	0.041057507	0.022314749	0.001130033	0.023444782	0.037730625
L100	LD		50	10:00 ~ 11:00	50	0.577317287	0.02644786	0.603562072	0.971336586	0.018450733	0.000892332	0.019343064	0.031129567	0.017008243	0.000823983	0.017832226	0.028698115
L101	LD		50	10:00 ~ 11:00	150	0.712996144	0.060822479	0.773818623	1.245337263	0.022801884	0.001593397	0.024395281	0.039260301	0.020984823	0.001392058	0.02237688	0.036012008
L102	LD		50	10:00 ~ 11:00	100	0.505939493	0.061266449	0.567205924	0.91282721	0.01735626	0.001900664	0.019256924	0.030999938	0.015975478	0.001667736	0.017643214	0.02839393
L103	LD		50	10:00 ~ 11:00	200	0.568645822	0.042773594	0.641141916	0.983981723	0.017174539	0.001120594	0.018295132	0.029443088	0.015821864	0.001006365	0.016828229	0.027082342
L104	PD		50	10:00 ~ 11:00	1000	0.776962102	0.079887102	0.856848224	1.378961973	0.030495605	0.001000646	0.014346625	0.030604502	0.057913485	0.001216005	0.03015046	0.053132434
L105	PD		50	10:00 ~ 11:00	700	0.876385155	0.12325432	0.999639475	1.608759792	0.034582853	0.001421649	0.034582853	0.052272762	0.017596356	0.002367072	0.019963427	0.032127942
L106	LD		50	10:00 ~ 11:00	150	0.561110689	0.121146465	0.682257154	1.097983728	0.0191476	0.002738863	0.021886464	0.035227262	0.020480361	0.000967412	0.018790439	0.030240205
L107	LD		50	10:00 ~ 11:00	400	0.678470018	0.052162056	0.730632074	1.175835422	0.01935892	0.001091442	0.020480361	0.039291584	0.017832027	0.000967412	0.018790439	0.030240205
L108	LD		50	10:00 ~ 11:00	300	0.735133998	0.059180363	0.794314361	1.278321873	0.019916522	0.000714813	0.020631335	0.039320283	0.018332767	0.000632708	0.018965457	0.030521898
L109	LD		50	10:00 ~ 11:00	200	0.763493349	0.045327957	0.808816306	1.301660434	0.022364435	0.00109127	0.023373561	0.020589691	0.021492801	0.0009311	0.034589225	0.045892522
L110	LD		50	10:00 ~ 11:00	200	0.598035862	0.104924576	0.702960438	1.131320351	0.017476479	0.002619866	0.020096165	0.032341562	0.016052483	0.002244162	0.018296644	0.029445522
L111	LD		50	10:00 ~ 11:00	200	0.735017248	0.047326983	0.78234423	1.0734423	0.022906263	0.001179865	0.03876277	0.021085928	0.001043857	0.0022129785	0.035614349	0.035151227
L112	LD		50	10:00 ~ 11:00	450	0.745975539	0.067500401	0.813475939	1.309159368	0.02290145	0.00095672	0.023858171	0.038395908	0.021085327	0.000862114	0.02194375	0.03520749
L113	LD		50	10:00 ~ 11:00	500	0.708156019	0.074361808	0.7259137239	1.259337239	0.022409246	0.001296348	0.03815036	0.020627227	0.02175881	0.00093044	0.035044797	0.045892522
L114	LD		50	10:00 ~ 11:00	300	0.799088866	0.055096954	0.85500582	1.375995066	0.024910625	0.001451737	0.026362362	0.042426004	0.022924236	0.001278719	0.024202955	0.038950784
L115	LD		50	10:00 ~ 11:00	100	0.550768897	0.056817927	0.567586824	0.913440179	0.022762489	0.000949416	0.038160518	0.020974639	0.000867402	0.001842041	0.035151227	0.045892522
L116	LD		50	10:00 ~ 11:00	100	0.57214765	0.104228116	0.586375765	0.943677974	0.02140161	0.001044565	0.022446175	0.036123528	0.019729106	0.000964328	0.020693435	0.033027702
L117	LD		50	10:00 ~ 11:00	100	0.919704488	0.033638728	0.95343216	1.534253371	0.030451681	0.000939243	0.031390923	0.028836785	0.020803768	0.000832217	0.020836335	0.046455185
L118	PD		50	10:00 ~ 11:00	1200	0.765835638	0.074537003	0.840372642	1.352445307	0.030027876	0.001204575	0.031232451	0.025023633	0.027616844	0.001046501	0.028663345	0.046129068
L119	PD		50	10:00 ~ 11:00	700	0.904153602	0.130354198	1.0345078	1.664874784	0.036070507	0.001704706	0.037775212	0.06079316	0.038162958	0.001450199	0.034613156	0.055704337
L120	LD		50	10:00 ~ 11:00	100	0.769871018	0.083821628	0.853692645	1.373881722	0.02489595	0.001216045	0.026111995	0.042023078	0.022912567	0.001080011	0.023992578	0.038612215
L121	LD		50	10:00 ~ 11:00	250	0.640685228	0.061713831	0.702399058	0.917769926	0.001224384	0.001899431	0.030568303	0.016356967	0.001078906	0.017435873	0.028060428	0.046455185
L122	LD		50	10:00 ~ 11:00	450	0.769186899	0.055822824	0.825009113	1.327720166	0.021236945	0.000760553	0.021997498	0.035401453	0.019547554	0.000673919	0.020221473	0.032543226
L123	LD		50	10:00 ~ 11:00	150	0.752516677	0.07357319	0.826088867	1.329459467	0.024108623	0.001189143	0.025297766	0.020712706	0.001060045	0.023252386	0.037420995	0.046455185
L124	LD		50	10:00 ~ 11:00	50	0.398856951	0.024504635	0.423361586	0.681392735	0.016064115	0.001086324	0.017150439	0.027600887	0.014816308	0.001001737	0.015818045	0.025456612
L125	LD		50	10:00 ~ 11:00	50	0.446218271	0.023295956	0.469514227	0.755608026	0.017788552	0.00086551	0.018775103	0.020215524	0.016400698	0.000906513	0.01730721	0.027853186
L126	PD		50	10:00 ~ 11:00	700	0.906434786	0.127920375	1.034355161	1.664629135	0.036056835	0.001658678	0.037715513	0.060697084	0.033151184	0.001411801	0.034562982	0.055623595
L127	DD		50	10:00 ~ 11:00	100	0.3826883	0.024476916	0.407165216	0.65267269	0.0135339	0.001049859	0.014583759	0.023470226	0.012445	0.000972642	0.013417642	0.021593548
L128	LD		50	10:00 ~ 11:00	250	0.711614879	0.065649935	0.77264815	1.250883357	0.020371039	0.00104015	0.021411189	0.034457883	0.018753605	0.000926829	0.019680343	0.031672509
L129	LD		50	10:00 ~ 11:00	350	0.778336854	0.062087747	0.840424601	1.352528927	0.02104442	0.000780168	0.021824589	0.035123183	0.019365186	0.002049066	0.032265764	0.046455185
L130	LD		50	10:00 ~ 11:00	350	0.808095715	0.06572222	0.873817936	1.406270157	0.021071513	0.000730597	0.021802109	0.035087006	0.019385736	0.000633104	0.02001884	0.032217121
L131	LD		50	10:00 ~ 11:00	250	0.711614879	0.065649935	0.77264815	1.250883357	0.020371039	0.00104015	0.021411189	0.034457883	0.018753605	0.000926829	0.019680343	0.031672509
L132	PD		50	10:00 ~ 11:00	1300	0.771670751	0.109488922	0.881159673	1.418085508	0.028458757	0.001701245	0.030160003	0.048537699	0.026162536	0.001446609	0.027609144	0.044432501
L133	DD		50	10:00 ~ 11:00	900	0.814278	0.051997401	0.866275401	1.394131654	0.0279648	0.000958882	0.028923682	0.046548039	0.0257114	0.000846138	0.026557538	0.042740108
L134	DD		50	10:00 ~ 11:00	600	0.8897705	0.05099016	0.94076066	1.51400376	0.0317369	0.000779323	0.032516223	0.052323659	0.029183	0.000689778	0.029872778	0.048075457
L135	LD		50	10:00 ~ 11:00	150	0.78422095	0.044825152	0.832247248	1.339368785	0.022727159	0.000810443	0.037147753	0.020497518	0.000715354	0.0021212872	0.03413827	0.046455185
L136	PD		50	10:00 ~ 11:00	850	0.795849039	0.069144374	0.864993413	1.392068499	0.033418843	0.001382498	0.03480134	0.056007189	0.030733466	0.001198136	0.031931602	0.051388804
L137	PD		50	10:00 ~ 11:00	1450	0.702074512	0.075244114	0.777318626	1.250969958	0.027035658	0.001828157	0.045413188	0.024864203	0.001028782	0.025892895	0.046170616	0.055623595
L138	LD		50	10:00 ~ 11:00	300	0.686476158	0.066997889	0.753474047	1.212595922	0.018819238	0.000962489	0.019781727	0.031835525	0.017323845	0.000855648	0.018179492	0.029259864
L139	LD		50	10:00 ~ 11:00	200	0.776105708	0.08342233	0.859447941	1.38314395	0.018656195	0.000833535	0.01489873	0.031365602	0.01751761	0.00071638	0.017873999	0.028765327
L140	LD		50														

Table 5: Hourly composite emission rate for each road link

L172	LD	50	10:00 ~ 11:00	50	0.495892786	0.06422229	0.560115076	0.901415597	0.015837481	0.001829981	0.017667463	0.028432955	0.014576274	0.001600753	0.016177026	0.026034335
L173	LD	50	10:00 ~ 11:00	400	0.627464527	0.099020764	0.726485291	1.169161839	0.017291663	0.018674795	0.018674795	0.030054095	0.015903969	0.001205403	0.017109372	0.027534797
L174	LD	50	10:00 ~ 11:00	550	0.776107968	0.064153663	0.840261331	1.352266171	0.022972862	0.001355543	0.024326405	0.039149457	0.0221138001	0.001183452	0.022324513	0.035922807
L175	LD	50	10:00 ~ 11:00	350	0.770422382	0.051546136	0.821968518	1.322826815	0.02180507	0.01052317	0.022857388	0.036785308	0.020071283	0.000930384	0.021001668	0.033788824
L176	LD	50	10:00 ~ 11:00	250	0.780725169	0.081265679	0.861990848	1.387236351	0.024620701	0.001745008	0.026365709	0.04243139	0.022644264	0.001511491	0.024155575	0.038874823
L177	LD	50	10:00 ~ 11:00	50	0.439168606	0.339551463	0.778720069	1.253225356	0.012206698	0.008834366	0.021041064	0.033862225	0.011046904	0.007343615	0.008483519	0.029746267
L178	LD	50	10:00 ~ 11:00	50	0.483530746	0.442985952	0.926516698	1.491080383	0.011166125	0.010933385	0.02209951	0.035565626	0.01000245	0.009132729	0.019135179	0.03079501
L179	LD	50	10:00 ~ 11:00	250	0.754499334	0.087089951	0.841589285	1.3544033	0.023632624	0.001874232	0.025506855	0.041049202	0.02173224	0.001619172	0.037580369	0.037580369
L180	PD	50	10:00 ~ 11:00	900	0.763348925	0.083407633	0.846755659	1.3627192	0.031797304	0.001513167	0.033310471	0.053607873	0.029239488	0.001308759	0.030548246	0.049162514
L181	PD	70	10:00 ~ 11:00	1800	0.533029165	0	0.533029165	0.857825157	0.016330315	0	0.016330315	0.02628103	0.015011567	0	0.024158714	0.024158714
L182	PD	70	10:00 ~ 11:00	2700	0.503180465	0	0.503180465	0.80978845	0.015135066	0	0.015135066	0.024357468	0.013915331	0	0.013915331	0.022394499
L183	PD	50	10:00 ~ 11:00	700	0.864292925	0.078933413	0.943226339	1.517971876	0.03796104	0.001411582	0.039372622	0.063369395	0.03491012	0.0012221053	0.036313173	0.058147342
L184	PD	50	10:00 ~ 11:00	850	0.873734835	0.078599567	0.952334402	1.532629846	0.034869934	0.001272759	0.036142693	0.058165881	0.032067552	0.001088491	0.033156043	0.053359347
L185	DD	50	10:00 ~ 11:00	500	0.5188888	0.075674053	0.594562853	0.956853782	0.0148246	0.001428213	0.016252813	0.026156302	0.0136191	0.001245727	0.014864827	0.023922561
L186	DD	50	10:00 ~ 11:00	500	0.4852014	0.068998363	0.554199763	0.891895846	0.0120842	0.001022784	0.013106984	0.021093593	0.0111046	0.000903896	0.012008496	0.019325753
L187	LD	50	10:00 ~ 11:00	150	0.623172147	0.058873624	0.682045771	1.097643541	0.020680735	0.001249625	0.02193036	0.035293406	0.019043456	0.001114806	0.020158261	0.032441496
L188	LD	50	10:00 ~ 11:00	100	0.634981088	0.041408191	0.676389279	1.088540322	0.020834934	0.00092571	0.021760645	0.035020276	0.019200733	0.000858324	0.020099056	0.032281842
L189	LD	50	10:00 ~ 11:00	50	0.389460481	0.08721338	0.476673861	0.767130312	0.014211922	0.002684611	0.016896533	0.027192266	0.013070495	0.002336476	0.024795056	0.024795056
L190	LD	50	10:00 ~ 11:00	50	0.513334477	0.036530641	0.549865118	0.884919929	0.017733529	0.00129214	0.019025669	0.030618769	0.016163215	0.0017503052	0.028168361	0.028168361
L191	LD	50	10:00 ~ 11:00	100	0.468331999	0.024794645	0.489627844	0.787977675	0.015712702	0.001049076	0.016716778	0.0269754	0.014494983	0.000971542	0.015466526	0.024890898
L192	LD	50	10:00 ~ 11:00	100	0.488338749	0.026514222	0.514852971	0.82857348	0.015573236	0.000942852	0.016516115	0.026580045	0.014365212	0.000871439	0.015236652	0.024520953
L193	LD	50	10:00 ~ 11:00	100	0.641269906	0.064152502	0.664152502	1.068847188	0.021427536	0.000926373	0.022353909	0.039975939	0.019746125	0.000856824	0.020620949	0.03315715
L194	LD	50	10:00 ~ 11:00	100	0.476131422	0.023226966	0.489158380	0.803637427	0.016465277	0.001101479	0.017666755	0.028270883	0.015188034	0.001021121	0.0161209155	0.026086041
L195	LD	50	10:00 ~ 11:00	250	0.499842649	0.072451827	0.572314476	0.921048378	0.016438585	0.002128344	0.018773199	0.031314505	0.001855001	0.00321346	0.0171469506	0.027631173
L196	LD	50	10:00 ~ 11:00	200	0.50082847	0.044589027	0.545417497	0.877762195	0.016209688	0.001403479	0.016713167	0.028345575	0.014936327	0.001252184	0.01618851	0.026058217
L001	LD	50	11:00 ~ 12:00	200	0.742481113	0.051725714	0.794206828	1.278148816	0.022569341	0.000956454	0.023525794	0.037861002	0.020776486	0.000851243	0.021627729	0.03408637
L002	LD	50	11:00 ~ 12:00	250	0.903034153	0.086165712	0.989199865	1.591958911	0.026699872	0.002713739	0.043673848	0.024566524	0.00386171	0.002495565	0.04015737	0.04015737
L003	LD	50	11:00 ~ 12:00	50	0.798752035	0.03931688	0.832068916	1.393081789	0.026429884	0.000794367	0.027224251	0.043813076	0.024345027	0.000732619	0.025077646	0.040358458
L004	DD	50	11:00 ~ 12:00	950	1.0163211	0.050287389	1.066608489	1.716535705	0.0395404	0.000905459	0.040445859	0.065091138	0.0363643	0.000787536	0.037161836	0.05980603
L005	DD	50	11:00 ~ 12:00	800	1.2763607	0.057101808	1.333462508	2.145994553	0.0496846	0.000672174	0.050356774	0.081041171	0.0450719	0.000593123	0.046295023	0.074504433
L006	LD	50	11:00 ~ 12:00	250	1.03362055	0.086995376	1.120615926	1.803452035	0.030720199	0.000521474	0.031241673	0.050278475	0.028265152	0.00459312	0.028724464	0.046227429
L007	LD	50	11:00 ~ 12:00	450	0.940451527	0.067987297	1.008438824	1.622920937	0.028553137	0.00061939	0.029172528	0.046948516	0.026277687	0.000550889	0.026828576	0.043176301
L008	LD	50	11:00 ~ 12:00	150	0.945520407	0.043253122	0.988773528	1.59127279	0.028521497	0.000670568	0.029192065	0.046979959	0.026251756	0.00059398	0.026845736	0.034203916
L009	LD	50	11:00 ~ 12:00	250	0.662029806	0.050285434	0.71231524	1.146357408	0.022520829	0.001296024	0.023816854	0.038329415	0.020736635	0.001154112	0.021890746	0.035229654
L010	LD	50	11:00 ~ 12:00	50	0.45119675	0.106552764	0.557749514	0.897608602	0.013970725	0.002624832	0.016595557	0.027670893	0.01282585	0.0022452	0.024254444	0.024254444
L011	LD	50	11:00 ~ 12:00	150	0.990111093	0.039140255	1.029251348	1.656415364	0.030277418	0.000699856	0.030977274	0.049852966	0.027869361	0.000621179	0.02849054	0.0345850965
L012	LD	50	11:00 ~ 12:00	50	0.648495275	0.063817039	0.668812314	1.07634641	0.023723102	0.00097805	0.024701152	0.039752551	0.021859869	0.000900845	0.022760714	0.036679277
L013	LD	50	11:00 ~ 12:00	150	0.637492453	0.034620512	0.672112965	1.081658279	0.020018375	0.000765717	0.020784692	0.033448671	0.018437653	0.00069161	0.019129263	0.030785487
L014	DD	50	11:00 ~ 12:00	1050	1.0278042	0.073210228	1.101014428	1.771906559	0.0397367	0.000783044	0.040519744	0.065210044	0.00692317	0.000692317	0.037238517	0.059929435
L015	DD	50	11:00 ~ 12:00	350	1.8432641	0.042134729	1.88598829	3.034247751	0.0802045	0.000545875	0.080750375	0.129954809	0.0737761	0.000477608	0.074253708	0.119499463
L016	LD	50	11:00 ~ 12:00	100	0.543680698	0.046887142	0.59056784	0.950424448	0.00793503	0.0014619553	0.023527832	0.012736876	0.000718608	0.013455484	0.021654484	0.021654484
L017	LD	50	11:00 ~ 12:00	150	0.990111093	0.039140255	1.029251348	1.656415364	0.030277418	0.000699856	0.030977274	0.049852966	0.027869361	0.000621179	0.02849054	0.0345850965
L018	LD	50	11:00 ~ 12:00	250	0.662029806	0.050285434	0.71231524	1.146357408	0.022520829	0.001296024	0.023816854	0.038329415	0.020736635	0.001154112	0.021890746	0.035229654
L019	LD	50	11:00 ~ 12:00	250	0.989972239	0.055530998	1.045503237	1.682570179	0.032115751	0.001385219	0.03350279	0.053917381	0.029552691	0.001203536	0.030756227	0.049497226
L020	LD	50	11:00 ~ 12:00	350	0.770141209	0.054781587	0.824922796	1.327581253	0.023231098	0.001199892	0.02443099	0.039317769	0.0213831	0.001056277	0.022439377	0.036112587
L021	DD	50	11:00 ~ 12:00	1000	1.0908478	0.072129174	1.162976974	1.871625363	0.0426362	0.000737686	0.043373886	0.06980333	0.0392136	0.000649913	0.039863513	0.064153946
L022	DD	50	11:00 ~ 12:00	500	1.5586478	0.090229453	1.648877253	2.635604119	0.0656374	0.000502186	0.066139586	0.104641081	0.0603761	0.00044473	0.06082083	0.097818395
L023	LD	50	11:00 ~ 12:00	350	1.00408325	0.041854126	1.045937376	1.683268856	0.030745595	0.000773946	0.031519541	0.050725658	0.028299016	0.000684077	0.028983093	0.046643651
L024	LD	50	11:00 ~ 12:00	250	0.434077723	0.04601158	0.478679303	0.77035775	0.01335916	0.0017402898	0.016012898	0.02800718	0.014808078	0.001204749	0.016012828	0.025770084
L025	DD	50	11:00 ~ 12:00	100	0.9728977	0.024076437	0.996974137	1.604470358	0.033584	0.000704605	0.034288605	0.055182024	0.0308984	0.000647951	0.031546351	0.050768804
L026	DD	50	11:00 ~ 12:00	100	0.983116	0.021178149	1.004294149	1.616250746	0.0332162	0.000696766	0.039212966	0.054577493	0.0305597	0.000640784	0.050212186	0.050212186
L027	DD	50	11:00 ~ 12:00	400	0.7736319	0.074078548	0									

Table 5: Hourly composite emission rate for each road link

L058	DD	50	11:00 ~ 12:00	450	0.8500366	0.072066071	0.922102671	1.483976713	0.0302451	0.000723826	0.030968926	0.049839532	0.0278177	0.000649192	0.028466892	0.045812908
L059	LD	50	11:00 ~ 12:00	150	1.007406926	0.031763107	1.039170033	1.672377901	0.033815465	0.000854684	0.034670149	0.055796057	0.03113528	0.000776122	0.031911402	0.051356296
L060	LD	50	11:00 ~ 12:00	150	0.821805993	0.037268781	0.859092733	1.382572364	0.028089485	0.001175627	0.047098095	0.025863405	0.0001053996	0.026917401	0.04331925	0.0331925
L061	LD	50	11:00 ~ 12:00	50	0.294669395	0.044586021	0.339282416	0.546020763	0.014128409	0.002004168	0.016132577	0.025962802	0.013034982	0.001797877	0.014832859	0.023871113
L062	LD	50	11:00 ~ 12:00	100	0.585325492	0.04579871	0.631124202	1.015693423	0.018466925	0.001193681	0.019660606	0.0316406	0.017014811	0.001075557	0.018090367	0.029113551
L063	LD	50	11:00 ~ 12:00	50	0.637387057	0.020975181	0.658362268	1.059528733	0.023324552	0.000684398	0.02400895	0.038638563	0.021482116	0.00061895	0.022101066	0.03556813
L064	LD	50	11:00 ~ 12:00	50	0.458921687	0.097973295	0.556894952	0.896233321	0.016498068	0.000614867	0.019375074	0.031181081	0.015167384	0.002479437	0.017646821	0.028399734
L065	DD	600	11:00 ~ 12:00	600	1.3304926	0.077862679	1.408355279	2.266522484	0.0599424	0.000681918	0.056624318	0.091127781	0.0514549	0.000603156	0.052058056	0.08379112
L066	DD	500	11:00 ~ 12:00	500	1.3066516	0.089122258	1.395773858	2.246274701	0.0530546	0.000614867	0.056624318	0.0837242	0.0487994	0.000541181	0.049340581	0.07940577
L067	DD	600	11:00 ~ 12:00	600	1.3163501	0.078378479	1.394728579	2.244592492	0.0552835	0.000716556	0.056000056	0.09012313	0.0508481	0.000632804	0.051480904	0.082850278
L068	DD	900	11:00 ~ 12:00	900	0.9852685	0.097690484	1.082958984	1.742849211	0.0369025	0.000818748	0.037721248	0.060706314	0.0339386	0.000720232	0.034568832	0.055777844
L069	LD	400	11:00 ~ 12:00	400	0.719534569	0.057113386	0.776647954	1.249890619	0.021439167	0.01028722	0.022467889	0.036158472	0.019736116	0.000911391	0.020647506	0.033228858
L070	LD	400	11:00 ~ 12:00	400	0.736272031	0.057371468	0.793643499	1.277242229	0.02197038	0.000970923	0.022941303	0.036920356	0.02022505	0.000861415	0.021086466	0.03935293
L071	LD	200	11:00 ~ 12:00	200	0.636063039	0.046166784	0.682228223	1.097939744	0.017628706	0.000830817	0.018459523	0.029707649	0.016232637	0.000472965	0.016975602	0.027319515
L072	LD	200	11:00 ~ 12:00	200	0.744818662	0.048932723	0.793751385	1.277415854	0.021793981	0.000954039	0.02274802	0.036609299	0.020066056	0.000851128	0.020917183	0.03366286
L073	LD	150	11:00 ~ 12:00	150	0.580499715	0.053416015	0.63391573	1.020185941	0.01484631	0.000855234	0.015701543	0.02569122	0.013668702	0.000758571	0.014427272	0.023218387
L074	LD	150	11:00 ~ 12:00	150	0.467939177	0.056030532	0.523969708	0.84324541	0.012890024	0.001193002	0.014083026	0.022664377	0.011874534	0.001064297	0.012938831	0.020822978
L075	DD	800	11:00 ~ 12:00	800	0.5441849	0.066421992	0.610606892	0.982674095	0.0160104	0.000660499	0.016870899	0.027151012	0.0147189	0.000765895	0.015484795	0.0249203
L076	DD	350	11:00 ~ 12:00	350	0.668577	0.079622194	0.948199894	1.525976017	0.0313325	0.000735448	0.032067948	0.051608231	0.0288188	0.000660505	0.029479305	0.047442225
L077	LD	350	11:00 ~ 12:00	350	0.822468633	0.057524336	0.879892999	1.416207933	0.025227962	0.000998278	0.02622624	0.042206937	0.023222425	0.000887038	0.024109463	0.038800323
L078	DD	400	11:00 ~ 12:00	400	1.6398397	0.072070277	1.711939977	2.755045203	0.0721929	0.00567545	0.072760445	0.117096295	0.000504166	0.0064004	0.06908866	0.107679115
L079	DD	850	11:00 ~ 12:00	850	0.9990588	0.10011377	1.1001377	1.770551724	0.0377547	0.000814904	0.038569604	0.062071607	0.034722	0.00071644	0.03543844	0.057032499
L080	DD	400	11:00 ~ 12:00	400	1.7197133	0.037087588	1.792800888	2.885226182	0.0763245	0.000548928	0.076875428	0.123718702	0.0702073	0.000487007	0.070694307	0.113771175
L081	DD	850	11:00 ~ 12:00	850	1.0006408	0.102940429	1.102940429	1.775979478	0.0378294	0.000850788	0.038680188	0.062449574	0.0347901	0.000746134	0.035536234	0.057189882
L082	LD	100	11:00 ~ 12:00	100	0.394457259	0.081824136	0.476281395	0.766498701	0.014138513	0.002438838	0.016577351	0.026678593	0.013004812	0.002121493	0.015126305	0.023431367
L083	LD	100	11:00 ~ 12:00	100	0.399726338	0.06899357	0.755778102	1.06692119	0.014590135	0.001201985	0.016692119	0.028683295	0.013427977	0.001838297	0.015246523	0.024568624
L084	LD	50	11:00 ~ 12:00	50	0.41911501	0.01668468	0.435799717	0.701349917	0.018236505	0.001227856	0.019446436	0.031324774	0.016822098	0.001134371	0.017956468	0.028898602
L085	LD	50	11:00 ~ 12:00	50	0.542431567	0.014852042	0.557283603	0.896858794	0.021322535	0.001086324	0.030603473	0.019656277	0.001001737	0.020658014	0.033245768	0.033245768
L086	LD	100	11:00 ~ 12:00	100	0.429588184	0.08525484	0.514853669	0.828574603	0.01673705	0.002688861	0.019425911	0.01762895	0.015395436	0.00233931	0.017734477	0.028541237
L087	LD	600	11:00 ~ 12:00	600	0.715748067	0.087117241	0.802865307	1.292083253	0.022932743	0.001226731	0.024159474	0.038880808	0.021104452	0.0010833	0.022181752	0.035707637
L088	LD	50	11:00 ~ 12:00	50	0.688014544	0.078037261	0.766051805	1.232837812	0.020268064	0.002016317	0.022284341	0.035861346	0.018641402	0.00174529	0.020386691	0.032809118
L089	LD	250	11:00 ~ 12:00	250	0.632122889	0.050021221	0.68234411	1.09812367	0.019235191	0.001016505	0.017709922	0.020251695	0.000898924	0.000898924	0.018609474	0.029949409
L090	LD	450	11:00 ~ 12:00	450	0.890791451	0.126348601	1.017140053	1.636924172	0.0293945	0.000598829	0.029993329	0.048269646	0.027704395	0.000535161	0.027579556	0.044384883
L091	LD	100	11:00 ~ 12:00	100	0.646863733	0.044161339	0.691025073	1.11209429	0.022312261	0.001358939	0.038095009	0.020549421	0.001209373	0.001209373	0.027579556	0.035017297
L092	LD	400	11:00 ~ 12:00	400	0.941723273	0.15858348	1.100306753	1.7707767	0.030927512	0.000634954	0.031562467	0.05079474	0.028447153	0.000560764	0.029007319	0.0466836
L093	LD	100	11:00 ~ 12:00	100	0.661905666	0.05691679	0.718822656	1.156830054	0.02137901	0.01444267	0.019673786	0.036730414	0.019677865	0.001274474	0.020952936	0.033719438
L094	LD	300	11:00 ~ 12:00	300	0.885652555	0.180413381	1.060605936	1.715662553	0.030151628	0.000535843	0.030687471	0.049386574	0.02773519	0.00048936	0.02822455	0.045422897
L095	LD	400	11:00 ~ 12:00	400	0.553539399	0.090122326	0.643661724	1.035870559	0.017030732	0.000947757	0.017978488	0.028933501	0.015686305	0.000869703	0.016556008	0.026644246
L096	LD	350	11:00 ~ 12:00	350	0.995057288	0.163644434	1.158701722	1.864745029	0.033174497	0.000511559	0.033686056	0.054212317	0.030518922	0.000467664	0.030986586	0.049867952
L097	LD	400	11:00 ~ 12:00	400	0.923987786	0.155450969	1.079528755	1.737328807	0.033181494	0.00078045	0.031961944	0.051437636	0.028685579	0.000698242	0.029383821	0.047288559
L098	LD	50	11:00 ~ 12:00	50	0.540032052	0.055140677	0.595172729	0.957835279	0.0212088	0.001889209	0.023098009	0.03717255	0.019519848	0.001650674	0.021170522	0.034070568
L099	LD	500	11:00 ~ 12:00	500	0.744529923	0.08725458	1.338628703	0.023766972	0.031126278	0.002489325	0.004661703	0.021879021	0.001011276	0.022890297	0.03638827	0.03638827
L100	LD	50	11:00 ~ 12:00	50	0.736126079	0.023109361	0.759235439	1.221867962	0.024335234	0.000829284	0.025164518	0.040498265	0.02242024	0.000762233	0.023182474	0.037308483
L101	LD	150	11:00 ~ 12:00	150	0.751166677	0.042039466	0.793206142	1.276538873	0.024958101	0.00117538	0.026133481	0.022977771	0.01042185	0.024019956	0.038656277	0.038656277
L102	LD	100	11:00 ~ 12:00	100	0.504625478	0.054182978	0.558808456	0.899312801	0.018257496	0.001731324	0.01998882	0.032168808	0.016808914	0.001522848	0.018331761	0.029502037
L103	LD	150	11:00 ~ 12:00	150	0.56939835	0.037498829	0.607438664	0.91775734	0.018309447	0.01061626	0.019371074	0.031174644	0.016869198	0.00096027	0.017829469	0.028693677
L104	PD	1050	11:00 ~ 12:00	1050	0.77611676	0.094512599	0.870629359	1.401138652	0.03145254	0.000802953	0.032254593	0.051910055	0.028926974	0.000705854	0.029632828	0.047689296
L105	PD	750	11:00 ~ 12:00	750	0.896200458	0.137561883	1.033762341	1.663675085	0.00959007	0.000959007	0.036937736	0.054945375	0.033087809	0.000827096	0.039914905	0.05480641
L106	LD	150	11:00 ~ 12:00	150	0.596215476	0.124041499	0.720256975	1.15913836	0.021557638	0.00218204	0.023739679	0.038205214	0.01982509	0.001899738	0.021724828	0.034962635
L107	LD	400	11:00 ~ 12:00	400	0.745288291	0.046884257	0.792172548	1.274874968	0.022500651	0.00097126	0.027471911	0.03774285	0.020717178	0.00086545	0.021582628	0.034733787
L108	LD	300	11:00 ~ 12:00	300	0.788100159	0.065809724	0.853909883	1.374231331	0.02294937	0.00061245	0.023547387	0.037895751	0.021109877	0.000545869	0.021655745	0.034851457
L109	LD	200														

Table 5: Hourly composite emission rate for each road link

L140	LD	50	11:00 ~ 12:00	150	0.631055239	0.075313852	0.706369091	1.136788033	0.020071662	0.001152776	0.021224438	0.034157337	0.018481959	0.001038672	0.0191520631	0.031415332
L141	PD	50	11:00 ~ 12:00	1250	0.728148154	0.082162089	0.810310243	1.304064686	0.030054396	0.000979145	0.03103354	0.049943518	0.02763854	0.000852942	0.028491482	0.045852481
L142	LD	50	11:00 ~ 12:00	250	0.563887779	0.074015662	0.637903441	1.026063523	0.016775181	0.000936027	0.017711207	0.028503354	0.01544824	0.000840784	0.016289024	0.026214578
L143	LD	50	11:00 ~ 12:00	350	0.676511081	0.080792016	0.757303097	1.218758166	0.01866619	0.000855743	0.020521933	0.033026768	0.018099719	0.000758487	0.018858206	0.030349265
L144	LD	50	11:00 ~ 12:00	150	0.626278583	0.131979176	0.758257759	1.220294542	0.01865535	0.001161642	0.019771491	0.031819052	0.017158765	0.000983758	0.018142523	0.023919748
L145	LD	50	11:00 ~ 12:00	100	0.50144107	0.051139277	0.552580347	0.889289656	0.014448486	0.00121529	0.015663776	0.025208341	0.013305885	0.001077567	0.014383452	0.023147865
L146	PD	50	11:00 ~ 12:00	800	0.804390812	0.06514527	0.869536082	1.399379198	0.035818841	0.001096089	0.03691493	0.059408673	0.03294158	0.000957998	0.033899578	0.054555947
L147	PD	50	11:00 ~ 12:00	1100	0.764866892	0.080017338	0.84488423	1.359705987	0.030262364	0.000944927	0.030007291	0.053119954	0.029485478	0.000822312	0.030307758	0.048775538
L148	LD	50	11:00 ~ 12:00	250	0.556967284	0.077765846	0.634733129	1.021501451	0.016610934	0.00100729	0.017618223	0.028353711	0.015295885	0.000909075	0.01616986	0.026066255
L149	LD	50	11:00 ~ 12:00	500	0.659161551	0.09544352	0.754605072	1.214416126	0.019325247	0.000891673	0.02021692	0.025335899	0.017783508	0.000789506	0.018573014	0.029890295
L150	LD	50	11:00 ~ 12:00	250	0.718945427	0.108797262	0.827742689	1.33211942	0.021870217	0.001011331	0.022881548	0.036824191	0.020117627	0.000884299	0.021001927	0.03379924
L151	LD	50	11:00 ~ 12:00	600	0.866218707	0.08525159	0.951470297	1.531239208	0.030425078	0.001031685	0.031456763	0.050624626	0.027995965	0.000911764	0.028907729	0.046522364
L152	LD	50	11:00 ~ 12:00	600	0.938684536	0.074412368	1.013096904	1.630417371	0.033450279	0.000961534	0.034411813	0.055380307	0.03078196	0.00085328	0.03163524	0.050911858
L153	LD	50	11:00 ~ 12:00	250	0.720063261	0.076514063	0.76517324	1.281963751	0.023116499	0.001096697	0.024213196	0.038967264	0.021275397	0.000970099	0.022245496	0.035800567
L154	LD	50	11:00 ~ 12:00	250	0.591034753	0.118927463	0.709962216	1.142570592	0.016447572	0.001200831	0.017648403	0.028402281	0.015128794	0.001051569	0.016180362	0.026039704
L155	LD	50	11:00 ~ 12:00	250	0.708698574	0.065007092	0.737705666	1.245155477	0.022500636	0.001120966	0.023621602	0.038015189	0.020711431	0.000991962	0.021703393	0.034928139
L156	LD	50	11:00 ~ 12:00	200	0.559600978	0.117938324	0.677539302	1.090391101	0.015227018	0.001030004	0.016257022	0.026163075	0.014009344	0.000908298	0.014917642	0.024007558
L157	LD	50	11:00 ~ 12:00	100	0.639962508	0.063640843	0.69762335	1.122713162	0.018233957	0.001406389	0.019640346	0.031670995	0.016777722	0.001222183	0.017999905	0.028697967
L158	LD	50	11:00 ~ 12:00	50	0.64106513	0.111401091	0.752466221	1.210973988	0.018755644	0.001077126	0.01983277	0.03191677	0.017246272	0.000931037	0.0181177309	0.029253471
L159	PD	50	11:00 ~ 12:00	800	0.804390812	0.06514527	0.869536082	1.399379198	0.035818841	0.001096089	0.03691493	0.059408673	0.03294158	0.000957998	0.033899578	0.054555947
L160	PD	50	11:00 ~ 12:00	750	0.831904027	0.068605935	0.900595462	1.44936443	0.03746045	0.001075216	0.038481666	0.034502007	0.034592781	0.000939454	0.035341462	0.056876428
L161	PD	50	11:00 ~ 12:00	750	0.890377493	0.080588978	0.970966371	1.56236102	0.037616455	0.00090445	0.038520905	0.045927933	0.034592781	0.000939454	0.035341462	0.056876428
L162	LD	50	11:00 ~ 12:00	350	0.562721488	0.070291038	0.630312526	1.018732379	0.016527633	0.001006603	0.017614235	0.028347293	0.015215374	0.00096473	0.016180104	0.026039704
L163	LD	50	11:00 ~ 12:00	400	0.609939115	0.077475259	0.687414644	1.106288883	0.017282851	0.000936325	0.018291876	0.029320848	0.015907904	0.000830168	0.0161738073	0.026977725
L164	LD	50	11:00 ~ 12:00	50	0.64693709	0.02669845	0.493192159	0.793713869	0.015853245	0.016887109	0.02717771	0.046235218	0.00956616	0.015580134	0.025073733	0.037505733
L165	LD	50	11:00 ~ 12:00	50	0.367443892	0.123756774	0.491200666	0.790508879	0.013604336	0.003750723	0.017355059	0.027930191	0.012490112	0.003237456	0.015727268	0.025311005
L166	LD	50	11:00 ~ 12:00	450	0.59827194	0.077779676	0.67051616	1.087996907	0.016781041	0.000966436	0.017747477	0.028561725	0.015455994	0.000855273	0.016301268	0.026234292
L167	LD	50	11:00 ~ 12:00	350	0.565542845	0.070351867	0.635894712	1.023370796	0.016534743	0.001076291	0.017611034	0.02842141	0.015221761	0.000955121	0.016176882	0.026034104
L168	DD	50	11:00 ~ 12:00	150	0.4541686	0.048711464	0.502880064	0.809505003	0.0135347	0.0012561	0.0147908	0.023803426	0.01242381	0.00122528	0.013560628	0.021823662
L169	DD	50	11:00 ~ 12:00	250	0.588646	0.06000691	0.648646691	1.043893066	0.019136	0.001344767	0.020480767	0.032960518	0.0175864	0.001174899	0.018761299	0.030193309
L170	LD	50	11:00 ~ 12:00	150	0.703375015	0.121435776	0.754568804	1.21435776	0.021423835	0.001113301	0.022537136	0.036269914	0.019725588	0.000987847	0.020713434	0.033334958
L171	LD	50	11:00 ~ 12:00	100	0.680718982	0.164552259	0.84617124	1.361777224	0.022996316	0.000918157	0.023914473	0.038485619	0.021156021	0.000818485	0.021974506	0.035644552
L172	LD	50	11:00 ~ 12:00	50	0.514653543	0.025137581	0.539791124	0.868707448	0.017621815	0.000841253	0.018463068	0.029713354	0.016246799	0.000769635	0.017016434	0.027858228
L173	LD	50	11:00 ~ 12:00	400	0.65796499	0.110808853	0.768737843	1.237218497	0.019506452	0.001141454	0.020647906	0.0332295	0.01794571	0.001002929	0.018948639	0.030494803
L174	LD	50	11:00 ~ 12:00	500	0.744627896	0.058711094	0.80333899	1.29284557	0.023018199	0.001165041	0.02418324	0.038919055	0.021187302	0.001027084	0.022214386	0.0357055
L175	LD	50	11:00 ~ 12:00	300	0.747561436	0.046882465	0.794443902	1.278530349	0.022186505	0.000982722	0.023169227	0.037287164	0.020426816	0.000873555	0.02130417	0.034285653
L176	LD	50	11:00 ~ 12:00	200	0.743448204	0.075124506	0.81857271	1.317361805	0.024742939	0.001409058	0.026151997	0.042087455	0.022766481	0.001232931	0.023999412	0.038623213
L177	LD	50	11:00 ~ 12:00	50	0.19989136	0.277931906	0.477823266	0.768980094	0.00692036	0.0075439	0.01446426	0.023277912	0.00623776	0.006393217	0.012630977	0.020327536
L178	LD	50	11:00 ~ 12:00	50	0.54095896	0.35293768	0.89389664	1.438583619	0.014776576	0.00872224	0.023488816	0.037817585	0.013388976	0.007291302	0.020680278	0.032815598
L179	LD	50	11:00 ~ 12:00	250	0.730078154	0.077447459	0.8087525613	1.29958327	0.024159411	0.001431287	0.025590698	0.041184135	0.022228156	0.001248858	0.023477014	0.037724898
L180	PD	50	11:00 ~ 12:00	900	0.754979785	0.085069577	0.840049362	1.35192504	0.0303266	0.001210393	0.034243053	0.055108715	0.030377431	0.001055045	0.031432476	0.050585541
L181	PD	70	11:00 ~ 12:00	1600	0.466776165	0.751201553	0.015009863	0	0.015009863	0	0.015009863	0.024155974	0.013797672	0	0.013797672	0.022205146
L182	PD	70	11:00 ~ 12:00	2400	0.430807656	0.693315993	0.013455453	0	0.013455453	0	0.013455453	0.021654953	0.012369389	0	0.012369389	0.021560628
L183	PD	50	11:00 ~ 12:00	700	0.872019001	0.080507728	0.952526729	1.532939367	0.040117792	0.001103212	0.041221004	0.06633861	0.036895307	0.00096157	0.037856876	0.060924585
L184	PD	50	11:00 ~ 12:00	850	0.841975543	0.079478169	0.921453713	1.482932318	0.035248788	0.000975541	0.036224329	0.032414193	0.03325351	0.000839317	0.03525351	0.053516204
L185	DD	50	11:00 ~ 12:00	450	0.5132149	0.071798665	0.585013565	0.941485731	0.0156227	0.001160321	0.016783021	0.027009586	0.0143586	0.001022917	0.015381517	0.024754091
L186	DD	50	11:00 ~ 12:00	500	0.500384	0.073389479	0.923396611	0.0135354	0.000907125	0.014442525	0.023242934	0.0124419	0.000807525	0.013249425	0.021322823	0.034294161
L187	LD	50	11:00 ~ 12:00	150	0.65336822	0.068675447	0.722043667	1.162013756	0.023065446	0.001134943	0.024200389	0.038946654	0.021237725	0.001016611	0.022254337	0.035814794
L188	LD	50	11:00 ~ 12:00	100	0.634641	0.060649314	0.695290313	1.118958513	0.022162363	0.000962268	0.023124631	0.037215394	0.020418094	0.000891363	0.021309457	0.034294161
L189	LD	50	11:00 ~ 12:00	50	0.372323052	0.061473829	0.433796881	0.698126672	0.014519691	0.002080254	0.016599945	0.026714956	0.013367879	0.001835456	0.015203334	0.024467334
L190	LD	50	11:00 ~ 12:00	50	0.472755576	0.035989748	0.508745324	0.8187442	0.01727605	0.001135633	0.029952526	0.018611683	0.029952526	0.015919864	0.017121355	0.027554081
L191	LD	50	11:00 ~ 12:00	100	0.391290888	0.0244										

Table 5: Hourly composite emission rate for each road link

L026	DD	50	12:00 ~ 13:00	50	0.6089496	0.021244119	0.630193719	1.01419596	0.0220742	0.00107553	0.02314973	0.037255787	0.0203109	0.000994813	0.021305713	0.034288137
L027	DD	50	12:00 ~ 13:00	350	0.5761997	0.070794216	0.64693916	1.041233189	0.0717176	0.000911883	0.018629483	0.029981172	0.0162908	0.000814911	0.017105711	0.027528905
L028	DD	50	12:00 ~ 13:00	300	0.6185962	0.079578103	0.698174303	1.123599833	0.0192135	0.00088321	0.020101821	0.03250665	0.0176661	0.000787749	0.018453849	0.029698518
L029	DD	50	12:00 ~ 13:00	100	0.246830634	0.026275161	0.273105795	0.439520081	0.01022353	0.001349551	0.012371905	0.019910601	0.010187801	0.00125378	0.014141581	0.018413394
L030	LD	50	12:00 ~ 13:00	1200	0.7697571	0.075442789	0.845199889	1.36021399	0.0289089	0.00108336	0.02999226	0.048267744	0.0265826	0.000958226	0.027540826	0.044322554
L031	DD	50	12:00 ~ 13:00	950	0.2256799	0.079126473	0.304824373	0.490566057	0.0047647	0.000905114	0.005669814	0.009124659	0.0043738	0.000802639	0.005176439	0.00830605
L032	DD	50	12:00 ~ 13:00	400	1.2114388	0.101332771	1.312771571	2.1126958	0.0504382	0.00099499	0.05143319	0.082773489	0.0463893	0.000875444	0.047264744	0.076065044
L033	LD	50	12:00 ~ 13:00	250	0.047289082	0.075187747	0.482476829	0.77646926	0.014935279	0.002316413	0.017251691	0.027763837	0.013744853	0.00020325	0.015768103	0.025376239
L034	LD	50	12:00 ~ 13:00	250	0.394765717	0.05880273	0.45364599	0.730070638	0.014225931	0.001890771	0.016116702	0.025937253	0.013105754	0.001671476	0.014777231	0.023781588
L035	LD	50	12:00 ~ 13:00	50	0.358219657	0.019760438	0.377980095	0.608298486	0.016492843	0.00148825	0.017981092	0.028937691	0.015224271	0.001384524	0.016608795	0.026729198
L036	LD	50	12:00 ~ 13:00	50	0.31879141	0.021190151	0.339981561	0.547145925	0.015344314	0.001596524	0.016940838	0.027263568	0.014169016	0.001485416	0.015654432	0.025193303
L037	LD	50	12:00 ~ 13:00	250	0.360718787	0.061810847	0.422529633	0.67999384	0.013096745	0.001982209	0.015078954	0.024267164	0.012066287	0.001750917	0.013817204	0.022236578
L038	LD	50	12:00 ~ 13:00	300	0.369852465	0.05419732	0.424049784	0.682440279	0.013331501	0.001781972	0.015113473	0.024322717	0.012286966	0.001584534	0.0138715	0.022323999
L039	LD	50	12:00 ~ 13:00	50	0.334100527	0.053605266	0.387705793	0.623950441	0.011532804	0.001712137	0.013244941	0.021315613	0.010638066	0.001525765	0.012163831	0.01957574
L040	LD	50	12:00 ~ 13:00	50	0.266687474	0.029958323	0.296645797	0.477403946	0.010372659	0.00120802	0.011580679	0.018637249	0.009585535	0.001121146	0.010706497	0.017230393
L041	LD	50	12:00 ~ 13:00	100	0.346694048	0.045664295	0.392358343	0.631437975	0.012809566	0.001608129	0.014417695	0.023202974	0.011812185	0.001444004	0.012526188	0.021333714
L042	LD	50	12:00 ~ 13:00	100	0.353638489	0.028176488	0.381814977	0.614470114	0.013291158	0.001174762	0.01446592	0.023280584	0.012285505	0.001089405	0.01335791	0.021497419
L043	LD	50	12:00 ~ 13:00	200	0.703263628	0.055646168	0.75809796	1.22134389	0.019863139	0.001127257	0.021025876	0.033837783	0.018313991	0.000993043	0.019313033	0.031061237
L044	LD	50	12:00 ~ 13:00	350	0.592928317	0.07383474	0.671313057	1.080370956	0.019294563	0.001168529	0.020463092	0.032932073	0.017764284	0.001046765	0.018811048	0.030273574
L045	LD	50	12:00 ~ 13:00	150	0.354039427	0.04543827	0.399477697	0.642895436	0.01265019	0.001567003	0.014223032	0.022889679	0.0011671865	0.001412735	0.013084599	0.021057569
L046	LD	50	12:00 ~ 13:00	200	0.363121752	0.028658197	0.391779948	0.630507142	0.012882262	0.001128315	0.014010976	0.022548424	0.011893417	0.001045755	0.012393972	0.020823527
L047	DD	50	12:00 ~ 13:00	1150	0.7772247	0.074997178	1.371514758	0.852221878	0.0295849	0.00168155	0.049931187	0.0272051	0.000947778	0.028152878	0.045307553	0.054053757
L048	DD	50	12:00 ~ 13:00	350	1.3551031	0.110029659	1.465132759	2.357896754	0.057604	0.000928184	0.058532184	0.094198185	0.052982	0.000816049	0.053798049	0.086579352
L049	DD	50	12:00 ~ 13:00	550	0.4333505	0.053971397	0.487321897	0.784266621	0.0116761	0.001060382	0.012736482	0.020497329	0.0107033	0.0009474	0.0116804	0.018779734
L050	DD	50	12:00 ~ 13:00	450	0.5368386	0.072572648	0.609411248	0.980749998	0.0190009	0.001267501	0.020268401	0.032618749	0.0174679	0.001129819	0.018597719	0.029930053
L051	DD	50	12:00 ~ 13:00	550	0.4446044	0.055388666	0.499930666	0.804658841	0.011892	0.001027341	0.012919341	0.010932	0.000916561	0.011848561	0.019068363	0.029930053
L052	DD	50	12:00 ~ 13:00	400	0.5809496	0.072775498	0.653725098	1.052065949	0.0206299	0.001085772	0.021778672	0.035049288	0.0190275	0.000974802	0.020002302	0.021190505
L053	DD	50	12:00 ~ 13:00	700	0.4424126	0.065701387	0.508113987	0.817728165	0.0129	0.001068767	0.022480495	0.0118585	0.000958366	0.012816866	0.020626695	0.029930053
L054	DD	50	12:00 ~ 13:00	400	0.6161125	0.07172934	0.68784184	1.06971387	0.0216439	0.000976728	0.022620628	0.036404281	0.019905	0.000881538	0.020786538	0.03452607
L055	LD	50	12:00 ~ 13:00	150	0.342794431	0.044866054	0.387660485	0.623877525	0.01266043	0.001563113	0.013629156	0.011128564	0.001409925	0.012538488	0.020178691	0.029930053
L056	LD	50	12:00 ~ 13:00	100	0.271213675	0.047985935	0.31919961	0.513700701	0.010782745	0.001660766	0.01244351	0.020025839	0.009950115	0.001494172	0.014444286	0.018417748
L057	DD	50	12:00 ~ 13:00	700	0.442401	0.064732801	0.501733801	0.816150711	0.0125314	0.001016359	0.013547759	0.021802951	0.0115198	0.000911677	0.012431477	0.020006473
L058	DD	50	12:00 ~ 13:00	400	0.6315296	0.06858597	0.700118197	1.126728219	0.0218646	0.000944584	0.022809184	0.036707732	0.020108	0.000850988	0.020958988	0.033730138
L059	LD	50	12:00 ~ 13:00	150	0.633939737	0.064326476	0.691217171	0.022202182	0.00111419	0.023334372	0.037552938	0.020473121	0.01020598	0.001493719	0.034590702	0.034590702
L060	LD	50	12:00 ~ 13:00	150	0.532297341	0.044007019	0.57630436	0.927469658	0.019314767	0.001457661	0.020772428	0.0334299	0.017795308	0.001309109	0.019104417	0.030745503
L061	LD	50	12:00 ~ 13:00	50	0.198183072	0.033492425	0.231675497	0.372844645	0.011230211	0.001850233	0.013082244	0.021053778	0.010367437	0.001689259	0.012065695	0.019417806
L062	LD	50	12:00 ~ 13:00	100	0.386561172	0.049900265	0.436551437	0.70259569	0.011968784	0.001475862	0.013444646	0.021637007	0.011034391	0.001324093	0.012358484	0.019889002
L063	LD	50	12:00 ~ 13:00	50	0.457686384	0.02198327	0.481884711	0.77551634	0.017498013	0.000950459	0.018448472	0.026989864	0.016130799	0.000872882	0.017003681	0.027364705
L064	LD	50	12:00 ~ 13:00	50	0.352081263	0.100749687	0.45283005	0.728758961	0.012945091	0.003008302	0.015953393	0.025674434	0.011896993	0.002605536	0.014502529	0.0233395
L065	DD	50	12:00 ~ 13:00	550	1.0719612	0.07571503	1.14767623	1.847001264	0.0440972	0.00088934	0.04498654	0.072398638	0.0405575	0.000790039	0.041347539	0.066542248
L066	DD	50	12:00 ~ 13:00	450	1.0598912	0.085050967	1.144942167	1.842601227	0.0421874	0.000798033	0.042985433	0.069178176	0.0388017	0.00070738	0.03950908	0.063583543
L067	DD	50	12:00 ~ 13:00	550	1.0608575	0.075676321	1.136533821	1.829069339	0.0436206	0.000895902	0.044516502	0.071642187	0.0401191	0.000796154	0.040915254	0.065846554
L068	DD	50	12:00 ~ 13:00	850	0.7713876	0.091703935	0.863091535	1.389007731	0.0278566	0.01041286	0.028897886	0.046506523	0.025616	0.000919642	0.026535642	0.042704871
L069	LD	50	12:00 ~ 13:00	450	0.511629577	0.059942373	0.571571195	0.919853602	0.014405437	0.01223495	0.015628933	0.025152266	0.001089215	0.014355857	0.023103455	0.023103455
L070	LD	50	12:00 ~ 13:00	400	0.518805337	0.05861389	0.577456726	0.929324208	0.014527453	0.001143339	0.015670791	0.025219631	0.013802053	0.001020694	0.014400947	0.023176019
L071	LD	50	12:00 ~ 13:00	200	0.470035607	0.053908741	0.483204597	0.71184551	0.01784551	0.001000481	0.012785032	0.020575464	0.010856309	0.000895925	0.011752233	0.018913339
L072	LD	50	12:00 ~ 13:00	200	0.523483374	0.05011485	0.573598224	0.923114565	0.014303213	0.001085509	0.015388722	0.024765686	0.01317768	0.000975631	0.014153311	0.02277749
L073	LD	50	12:00 ~ 13:00	200	0.46634762	0.057042443	0.523390063	0.842312564	0.010574924	0.00092107	0.011495994	0.018500963	0.009741637	0.000822422	0.01056406	0.017001164
L074	LD	50	12:00 ~ 13:00	150	0.362714547	0.052624433	0.41533898	0.668421634	0.008819874	0.001161973	0.009981847	0.016064186	0.00813405	0.001074993	0.009182043	0.014777028
L075	DD	50	12:00 ~ 13:00	850	0.4124729	0.063907976	0.476380876	0.766658799	0.0108869	0.00978526	0.011865426	0.019950504	0.0100007	0.000878603	0.010885603	0.017518637
L076	DD	50	12:00 ~ 13:00	350	0.64919	0.073618521	0.722808521	1.163244665	0.0227509	0.000936137	0.023687037	0.038120497	0.0209239	0.000844966	0.021768866	0.035033507
L077	LD	50	12:00 ~ 13:00	350	0.560861144	0.										

Table 5: Hourly composite emission rate for each road link

L108	LD	50	12:00 ~ 13:00	300	0.568478748	0.064725729	0.633204477	0.1019041293	0.014941112	0.000778059	0.01571917	0.02529749	0.013758667	0.000697843	0.01445651	0.02326544
L109	LD	50	12:00 ~ 13:00	200	0.531833366	0.044518023	0.576351388	0.927455343	0.015632795	0.001088784	0.016721579	0.026910706	0.014403058	0.00095886	0.013588943	0.024766042
L110	LD	50	12:00 ~ 13:00	200	0.410716453	0.092236244	0.502952697	0.809421894	0.012311748	0.002401801	0.014713549	0.023679103	0.013136486	0.002081843	0.013398328	0.021562466
L111	LD	50	12:00 ~ 13:00	200	0.495798034	0.04685578	0.542653814	0.87331449	0.015985373	0.001290986	0.017276359	0.027803535	0.014725432	0.001156208	0.015881639	0.025558957
L112	LD	50	12:00 ~ 13:00	450	0.547160249	0.074157053	0.621317302	0.999910786	0.017404189	0.001070813	0.018475002	0.029732559	0.016031889	0.000972658	0.017004548	0.027366099
L113	LD	50	12:00 ~ 13:00	550	0.527270403	0.076434997	0.6037054	0.971567249	0.01742523	0.001320472	0.018745703	0.030168209	0.016048142	0.001185661	0.017233803	0.027735048
L114	LD	50	12:00 ~ 13:00	300	0.496290465	0.053998351	0.550288816	0.885601803	0.016010253	0.001558328	0.017568582	0.028273821	0.014745924	0.001389299	0.016163522	0.025967056
L115	LD	50	12:00 ~ 13:00	100	0.442475202	0.018832393	0.018832393	0.742400764	0.019304179	0.001115705	0.020419883	0.032862535	0.017798208	0.001026106	0.018823417	0.030294726
L116	LD	50	12:00 ~ 13:00	100	0.342571058	0.018094774	0.360665832	0.58043395	0.015891879	0.001344258	0.017236137	0.027738804	0.014668363	0.001254566	0.015913829	0.025610762
L117	LD	50	12:00 ~ 13:00	50	0.61344503	0.033467313	0.646912343	1.04110191	0.021786522	0.001257448	0.02304397	0.037085582	0.020067604	0.001129579	0.021197183	0.034113474
L118	PD	50	12:00 ~ 13:00	1150	0.611454789	0.079771099	0.6911225888	1.11241747	0.023980536	0.001234032	0.025214568	0.040578813	0.022057488	0.001085413	0.023142901	0.037244796
L119	PD	50	12:00 ~ 13:00	700	0.774594781	0.135359738	0.909954519	1.464426206	0.030479541	0.001516023	0.031995565	0.051491742	0.028028116	0.001302114	0.02933023	0.047202313
L120	LD	50	12:00 ~ 13:00	100	0.59890248	0.095103039	0.694005519	1.116890841	0.020213903	0.001363999	0.021577902	0.034726181	0.018607404	0.001218867	0.019826271	0.031970212
L121	LD	50	12:00 ~ 13:00	250	0.466267354	0.056042048	0.522309402	0.840573414	0.012805222	0.001236421	0.014041642	0.022597776	0.011796725	0.001103366	0.01290009	0.020760631
L122	LD	50	12:00 ~ 13:00	400	0.579044327	0.060020065	0.639064392	1.028471889	0.015728638	0.000847341	0.016575979	0.026676387	0.014483596	0.000760069	0.015243665	0.02453224
L123	LD	50	12:00 ~ 13:00	150	0.535088955	0.087424571	0.622513526	1.001835918	0.018176111	0.001364661	0.019540721	0.031447664	0.016737239	0.001224367	0.017961605	0.02890633
L124	LD	50	12:00 ~ 13:00	50	0.300242417	0.026116208	0.326358265	0.525221989	0.013069761	0.001219355	0.014289115	0.022996045	0.012066655	0.001128703	0.013195358	0.021235818
L125	LD	50	12:00 ~ 13:00	50	0.372938616	0.024975258	0.397846144	0.640269714	0.015420128	0.001119582	0.016539709	0.026618015	0.014225101	0.001033479	0.015258579	0.024556242
L126	PD	50	12:00 ~ 13:00	700	0.774515822	0.133867467	0.908383289	1.461897563	0.030364302	0.001493072	0.031857734	0.051269346	0.027922247	0.001128246	0.029205193	0.047001085
L127	PD	50	12:00 ~ 13:00	100	0.2141007	0.02746411	0.24156481	0.388759911	0.0086446	0.001280202	0.009268602	0.0159755	0.014876325	0.001031328	0.014876325	0.023841064
L128	LD	50	12:00 ~ 13:00	250	0.517384244	0.067358445	0.587472689	0.941049799	0.015050403	0.00112553	0.016175933	0.026032576	0.014876325	0.000757949	0.015280545	0.032485517
L129	LD	50	12:00 ~ 13:00	350	0.594810703	0.066241798	0.661052502	1.063858233	0.015569406	0.000853922	0.016423327	0.026430717	0.014332596	0.000757949	0.015280545	0.032485517
L130	LD	50	12:00 ~ 13:00	350	0.564776262	0.07024733	0.616009738	1.153947446	0.016009738	0.000773819	0.016783758	0.027010773	0.014732404	0.000678616	0.015411021	0.024805157
L131	LD	50	12:00 ~ 13:00	250	0.517384244	0.067358445	0.587472689	0.941049799	0.015050403	0.00112553	0.016175933	0.026032576	0.014876325	0.000757949	0.015280545	0.032485517
L132	PD	50	12:00 ~ 13:00	1250	0.634559926	0.114433609	0.748993534	1.205385255	0.023138433	0.001603116	0.024741548	0.039817563	0.021273544	0.001376026	0.02264957	0.03645086
L133	DD	50	12:00 ~ 13:00	800	0.6084487	0.055932301	0.664381001	1.06921492	0.0211234	0.001067294	0.022190694	0.035712371	0.0194218	0.000954767	0.020376567	0.032792825
L134	DD	50	12:00 ~ 13:00	550	0.7002579	0.05829618	0.75855408	1.220771423	0.0252593	0.000950458	0.026164758	0.042107991	0.0232271	0.000811276	0.024038376	0.03868592
L135	LD	50	12:00 ~ 13:00	150	0.566850926	0.045156026	0.612006952	0.984927269	0.015657548	0.000855375	0.016512923	0.026574807	0.014420262	0.00076883	0.015189092	0.024444413
L136	PD	50	12:00 ~ 13:00	800	0.636965298	0.069927089	0.706892388	1.137630195	0.027047938	0.001387889	0.028435827	0.05762914	0.024878606	0.001218531	0.026097137	0.041999166
L137	PD	50	12:00 ~ 13:00	1400	0.56465897	0.080714602	0.645400522	1.038668877	0.021629374	0.001195193	0.022824566	0.036732488	0.019894592	0.001052438	0.020947029	0.033710892
L138	LD	50	12:00 ~ 13:00	300	0.51100197	0.068403991	0.58084236	0.934772844	0.013863424	0.001031906	0.01489533	0.02397165	0.012768936	0.00092619	0.013695124	0.022040114
L139	LD	50	12:00 ~ 13:00	200	0.633700571	0.068686565	0.720567136	1.159637515	0.014660913	0.000760953	0.015421866	0.024819026	0.013486875	0.000665062	0.01451937	0.022775279
L140	LD	50	12:00 ~ 13:00	200	0.476919231	0.069639281	0.546558512	0.879598475	0.014946231	0.001357831	0.016304062	0.02623887	0.013769938	0.001223278	0.014993216	0.024129182
L141	PD	50	12:00 ~ 13:00	1250	0.600525443	0.082462405	0.682987847	1.099159662	0.023478203	0.001218769	0.024696972	0.039745825	0.021598328	0.001067528	0.022661356	0.036468927
L142	LD	50	12:00 ~ 13:00	250	0.407657793	0.067671295	0.475329089	0.764966115	0.011094342	0.001057674	0.012152016	0.019556725	0.010225391	0.000956391	0.01181783	0.01799529
L143	LD	50	12:00 ~ 13:00	400	0.513526382	0.076987724	0.590514106	0.950337971	0.013719134	0.001000853	0.014719986	0.023689463	0.012631703	0.000892362	0.013524065	0.021764819
L144	LD	50	12:00 ~ 13:00	200	0.49137219	0.110333319	0.608405508	0.979131321	0.013462128	0.001320891	0.01478302	0.023790905	0.012386358	0.00116532	0.013551678	0.021809257
L145	LD	50	12:00 ~ 13:00	100	0.416381864	0.060244091	0.476625954	0.767053213	0.011287082	0.001473758	0.012760841	0.020536531	0.010395396	0.00130551	0.011700906	0.018830736
L146	PD	50	12:00 ~ 13:00	800	0.650997693	0.069498605	0.720496299	1.159523513	0.027752726	0.009119128	0.026181852	0.046862577	0.025531354	0.001196369	0.026727723	0.043013993
L147	PD	50	12:00 ~ 13:00	1050	0.635918551	0.08185405	0.717772601	1.155140158	0.025412034	0.001217028	0.026629063	0.042855216	0.023372087	0.01064211	0.024436308	0.039326313
L148	LD	50	12:00 ~ 13:00	250	0.400594595	0.070012538	0.470607133	0.757366883	0.01098924	0.001142807	0.012132047	0.019524588	0.010127651	0.001029657	0.011157308	0.017955902
L149	LD	50	12:00 ~ 13:00	550	0.515775295	0.087864845	0.60364014	0.971462223	0.013828591	0.001080147	0.014908738	0.023932228	0.012729335	0.00095914	0.013668475	0.022029441
L150	LD	50	12:00 ~ 13:00	250	0.548487027	0.100099378	0.648586405	1.043796045	0.01196457	0.001654368	0.01654368	0.026624405	0.014120639	0.001052994	0.015173633	0.024419535
L151	LD	50	12:00 ~ 13:00	600	0.668579087	0.080394511	0.748973598	1.205353171	0.022460693	0.001216431	0.023677123	0.038104541	0.020672569	0.001080018	0.021752586	0.035007307
L152	LD	50	12:00 ~ 13:00	650	0.722193962	0.073139372	0.795333335	1.279961749	0.025082439	0.001204435	0.026268873	0.042304517	0.020308595	0.001071933	0.024157927	0.038878319
L153	LD	50	12:00 ~ 13:00	250	0.538909966	0.075307238	0.614217204	0.988484315	0.016453458	0.001364607	0.017818064	0.028675324	0.015147749	0.001209775	0.016357524	0.026324818
L154	LD	50	12:00 ~ 13:00	200	0.461075432	0.103692496	0.564767928	0.90803617	0.01655107	0.00129469	0.012949797	0.020840626	0.010724227	0.00114053	0.011864728	0.019494428
L155	LD	50	12:00 ~ 13:00	300	0.541597623	0.0660858	0.607683422	0.977969239	0.016770221	0.001348049	0.018182629	0.029158456	0.015440594	0.001197827	0.016638421	0.026776877
L156	LD	50	12:00 ~ 13:00	200	0.450502062	0.103744353	0.554264414	0.891970924	0.010953449	0.001186857	0.012140306	0.01953788	0.010079278	0.001048003	0.011127282	0.017907579
L157	LD	50	12:00 ~ 13:00	100	0.446446865	0.068391262	0.514838127	0.828549591	0.011857768	0.001521924	0.013379692	0.021532474	0.010915016	0.001332844	0.01224786	0.019710972
L158	LD	50	12:00 ~ 13:00	50	0.538117453	0.102129843	0.635947296	1.023455422	0.014044658	0.001171232	0.01521589	0.02448754	0.012916766	0.001022483	0.013939249</	

Table 5: Hourly composite emission rate for each road link

L190	LD	50	12:00 ~ 13:00	50	0.340661351	0.03876614	0.379427492	0.610627839	0.012987646	0.00146693	0.014454575	0.023262326	0.011979693	0.00132759	0.013307283	0.021415942
L191	LD	50	12:00 ~ 13:00	100	0.290920256	0.02730528	0.318225524	0.512133065	0.01465981	0.001243328	0.012709308	0.020453598	0.010594019	0.001153677	0.011747696	0.018906038
L192	LD	50	12:00 ~ 13:00	100	0.320575452	0.029024845	0.349600298	0.562625743	0.01371004	0.0012497385	0.012497385	0.020112542	0.010494956	0.001053574	0.011548551	0.018585511
L193	LD	50	12:00 ~ 13:00	50	0.363271623	0.026797546	0.390069169	0.627753917	0.013587414	0.001174762	0.01476218	0.023757367	0.01254092	0.001089405	0.013630325	0.021935828
L194	LD	50	12:00 ~ 13:00	100	0.304285356	0.026225613	0.330510969	0.531904523	0.012201263	0.001279102	0.013480365	0.021694491	0.011270568	0.001187385	0.012457953	0.020049083
L195	LD	50	12:00 ~ 13:00	250	0.345333588	0.067121296	0.412454884	0.663780143	0.012613638	0.0022117737	0.014731375	0.023707791	0.011618629	0.001864778	0.013483407	0.021699386
L196	LD	50	12:00 ~ 13:00	200	0.346233768	0.043756412	0.389990179	0.627626795	0.012932877	0.001490173	0.01378346	0.022182274	0.01134157	0.001342047	0.012683167	0.020412253
L001	LD	50	13:00 ~ 14:00	200	0.131445873	0.048124506	0.107957039	1.737395793	0.031512427	0.00077927	0.032309698	0.051997729	0.029006428	0.000712525	0.029718953	0.0478279
L002	LD	50	13:00 ~ 14:00	250	1.204574891	0.073671452	1.278246344	2.057132971	0.036139544	0.000819399	0.03652114	0.058774932	0.033253054	0.000335624	0.033588678	0.054055604
L003	LD	50	13:00 ~ 14:00	50	1.020872171	0.034333399	1.05520557	1.698184533	0.033583617	0.000703672	0.034287289	0.055179906	0.030927165	0.000649134	0.031576299	0.050817001
L004	DD	50	13:00 ~ 14:00	950	1.2584029	0.046753851	1.305156751	2.100440966	0.0480654	0.000811939	0.048877339	0.078660256	0.0442082	0.000713707	0.044921907	0.072294623
L005	DD	50	13:00 ~ 14:00	900	1.547334	0.047354052	1.594688502	2.566395993	0.0584926	0.000524548	0.059017148	0.094978656	0.0538065	0.000462051	0.054268551	0.087336655
L006	LD	50	13:00 ~ 14:00	250	1.398505233	0.073147676	1.471652908	2.368389892	0.041697166	0.000446845	0.042144011	0.067824043	0.038367488	0.000391864	0.038759352	0.062376975
L007	LD	50	13:00 ~ 14:00	450	1.270582187	0.059691005	1.350273192	2.140861858	0.038249009	0.000532247	0.038781256	0.062412227	0.035199659	0.000471869	0.035671527	0.057407616
L008	LD	50	13:00 ~ 14:00	250	1.337403587	0.042045281	1.379445888	2.220050492	0.040175501	0.000586058	0.040761559	0.065599208	0.038973928	0.00051527	0.037489198	0.060332866
L009	LD	50	13:00 ~ 14:00	250	0.943003866	0.048275307	0.991279173	1.595305224	0.030702018	0.00109831	0.031800328	0.05117754	0.028263807	0.000978224	0.029242031	0.047066037
L010	LD	50	13:00 ~ 14:00	50	0.451029275	0.106552764	0.557582098	0.89739078	0.01396415	0.002624832	0.016588982	0.026897312	0.01281995	0.0022452	0.01506515	0.024244949
L011	LD	50	13:00 ~ 14:00	200	1.37408798	0.036896033	1.410984013	2.270753011	0.041428159	0.000527734	0.041955893	0.06751296	0.038128555	0.000469133	0.038597688	0.062116803
L012	LD	50	13:00 ~ 14:00	50	0.963146492	0.016377103	0.979883595	1.576965865	0.032500324	0.000822249	0.033322673	0.05362751	0.02993154	0.000758766	0.030690306	0.049391321
L013	LD	50	13:00 ~ 14:00	150	0.8187704	0.034184726	0.852955126	1.372694802	0.025429697	0.000699952	0.026129649	0.04025149	0.02044793	0.000632516	0.02404793	0.038701295
L014	DD	50	13:00 ~ 14:00	1050	1.2565245	0.06232314	1.31884764	2.122474261	0.0478444	0.000700636	0.048545036	0.078125468	0.0440061	0.000613885	0.044624485	0.071815968
L015	DD	50	13:00 ~ 14:00	400	2.0542777	0.034036365	2.088314065	3.360807357	0.0876054	0.000404737	0.080401337	0.141638234	0.0805858	0.000354652	0.080940525	0.130260716
L016	LD	50	13:00 ~ 14:00	100	0.747487654	0.041510597	0.788998251	1.269766446	0.020482503	0.000754577	0.02123708	0.034717682	0.018589816	0.000668308	0.019542904	0.03415177
L017	LD	50	13:00 ~ 14:00	200	1.37408798	0.036896033	1.410984013	2.270753011	0.041428159	0.000527734	0.041955893	0.06751296	0.038128555	0.000469133	0.038597688	0.062116803
L018	LD	50	13:00 ~ 14:00	250	0.943003866	0.048275307	0.991279173	1.595305224	0.030702018	0.00109831	0.031800328	0.05117754	0.028263807	0.000978224	0.029242031	0.047066037
L019	LD	50	13:00 ~ 14:00	300	1.37880688	0.051603501	1.43041038	2.302016642	0.034028564	0.001110859	0.04139424	0.07103534	0.039593672	0.00096404	0.040055713	0.065271149
L020	LD	50	13:00 ~ 14:00	350	1.096832579	0.050060046	1.146892843	1.845740529	0.033238144	0.001003906	0.034242051	0.055107102	0.03059103	0.000884522	0.031475552	0.050654865
L021	DD	50	13:00 ~ 14:00	950	1.3191579	0.061267439	1.380425339	2.221573716	0.0508821	0.000658868	0.062946942	0.0468008	0.000579659	0.000579659	0.047380459	0.076251267
L022	DD	50	13:00 ~ 14:00	500	1.7829901	0.070095388	1.853085488	2.9822446	0.0740177	0.000446005	0.074463705	0.11983472	0.0680857	0.000393521	0.068479221	0.11020635
L023	LD	50	13:00 ~ 14:00	400	1.411662698	0.03851007	1.450172768	2.33821042	0.042655116	0.000565211	0.032023027	0.0695562	0.039257057	0.000500137	0.039577913	0.063982841
L024	LD	50	13:00 ~ 14:00	200	0.542986399	0.040024759	0.583011159	0.938263178	0.018983587	0.001298087	0.020281674	0.0372640109	0.01748931	0.001170439	0.018659749	0.03002988
L025	DD	50	13:00 ~ 14:00	150	1.341842	0.025929032	1.367771032	2.201208633	0.0437781	0.000453748	0.044231848	0.071184082	0.0402777	0.000418027	0.040695727	0.065493162
L026	DD	50	13:00 ~ 14:00	100	1.3737782	0.024516169	1.398294369	2.25033106	0.0444628	0.000423264	0.044850064	0.072179002	0.0408748	0.000389384	0.041264184	0.066408102
L027	DD	50	13:00 ~ 14:00	400	1.037438	0.058673992	1.096111992	1.764016874	0.0325312	0.000571796	0.033102996	0.053273976	0.0299212	0.000505274	0.030426474	0.048966542
L028	LD	50	13:00 ~ 14:00	350	1.0581176	0.065648544	1.123766054	1.808521662	0.0330506	0.000556816	0.034057416	0.054089961	0.0308119	0.00048785	0.03129975	0.050371939
L029	LD	50	13:00 ~ 14:00	100	0.49609686	0.020541537	0.516638396	0.831446837	0.017499476	0.001066488	0.029878948	0.016139782	0.000988513	0.001728295	0.027565129	0.040040897
L030	DD	50	13:00 ~ 14:00	1200	1.1753775	0.064338841	1.239716341	1.995125096	0.0452748	0.000776227	0.046051027	0.07411176	0.0416399	0.000662894	0.042322794	0.068111765
L031	DD	50	13:00 ~ 14:00	950	1.197066	0.067708155	1.264774155	2.06711638	0.0050799	0.000635013	0.009197239	0.0405714913	0.0046648	0.000560305	0.005225105	0.008400897
L032	DD	50	13:00 ~ 14:00	450	1.6938553	0.08904844	1.777760144	2.861020509	0.06934	0.000627559	0.069967559	0.112601592	0.0637818	0.00054894	0.06433074	0.103530033
L033	LD	50	13:00 ~ 14:00	250	0.666640621	0.059669659	0.72631028	1.168880186	0.022652002	0.001830438	0.02484244	0.03940057	0.020844774	0.00159286	0.022437634	0.036109782
L034	LD	50	13:00 ~ 14:00	200	0.634014013	0.046008792	0.680022806	1.094387902	0.021197035	0.001476383	0.022673418	0.036489238	0.019517175	0.001301391	0.020818566	0.033504152
L035	LD	50	13:00 ~ 14:00	50	0.928430895	0.014665514	0.943096409	1.571762775	0.032718663	0.001096912	0.033815575	0.054420757	0.030133921	0.001018365	0.031152286	0.050134621
L036	LD	50	13:00 ~ 14:00	50	0.649585269	0.01649812	0.666353389	1.077389163	0.024709882	0.001238443	0.025948326	0.041759679	0.022775188	0.001150999	0.023926187	0.038050537
L037	LD	50	13:00 ~ 14:00	200	0.602690367	0.049879182	0.65256955	1.05026279	0.020186553	0.001580949	0.021767502	0.035031312	0.018586404	0.001391812	0.019978215	0.032151741
L038	LD	50	13:00 ~ 14:00	250	0.601077803	0.044177623	0.645255426	1.038435367	0.020034276	0.001444266	0.021478542	0.034566276	0.018450588	0.001278766	0.019729624	0.031751674
L039	LD	50	13:00 ~ 14:00	50	0.53968307	0.029239099	0.562622169	0.905450361	0.017502894	0.000290557	0.018431951	0.029663275	0.016139366	0.000857458	0.016996824	0.027353668
L040	LD	50	13:00 ~ 14:00	50	0.770510667	0.01982402	0.790334869	1.271917519	0.025605214	0.000852773	0.026457987	0.042579897	0.02358095	0.000788618	0.024377669	0.039231957
L041	LD	50	13:00 ~ 14:00	100	0.594261121	0.03345154	0.627712661	1.010203094	0.019984557	0.001205612	0.021190169	0.018412856	0.034102187	0.001081461	0.019494317	0.031372984
L042	LD	50	13:00 ~ 14:00	100	0.780744513	0.020385904	0.801130417	1.289291225	0.025897058	0.000838978	0.026736036	0.043027371	0.02385573	0.000774637	0.024630366	0.039638633
L043	LD	50	13:00 ~ 14:00	250	1.516021965	0.045372618	1.561394583	2.512814758	0.045548121	0.000597591	0.074264141	0.049197607	0.000526816	0.000526816	0.042444422	0.068307507
L044	LD	50	13:00 ~ 14:00	350	1.121321013	0.070734947	1.19205596	1.918423339	0.035677253	0.000807477	0.03648473	0.058716336	0.032833531	0.000718701	0.033552231	0.035996948
L045	LD	50	13:00 ~ 14:00	150	0.725484193	0										

Table 5: Hourly composite emission rate for each road link

L076	DD	50	13:00 ~ 14:00	350	1.0942551	0.06576704	1.16002214	1.866870032	0.0388035	0.000618168	0.039421668	0.063442867	0.0356943	0.000557115	0.036251415	0.058340852
L077	LD	50	13:00 ~ 14:00	350	1.2230022	0.052186257	2.052211792	0.037348005	0.000829172	0.038177178	0.061440059	0.034372051	0.000735942	0.035107993	0.056506098	
L078	DD	50	13:00 ~ 14:00	400	1.859277	0.058348004	1.911320804	0.075965023	0.0816528	0.000520326	0.082173126	0.132244499	0.0751079	0.000460987	0.075568887	0.121616033
L079	DD	50	13:00 ~ 14:00	800	1.1973208	0.084012616	1.281333416	2.062101119	0.040748392	0.045786992	0.047366838	0.04142444	0.000657184	0.00042081584	0.067723577	
L080	DD	50	13:00 ~ 14:00	400	1.9406751	0.057926917	1.998602017	3.216430171	0.080758	0.00046513	0.08654093	0.139273781	0.0791773	0.000413907	0.079591207	0.128089313
L081	DD	50	13:00 ~ 14:00	800	1.2053245	0.085246983	1.290571483	2.07696831	0.04671993	0.000761993	0.046191693	0.04338139	0.0417837	0.000667823	0.068318934	
L082	LD	50	13:00 ~ 14:00	100	0.57675479	0.084769525	0.661524315	1.064617541	0.019162923	0.002486425	0.021649348	0.034841161	0.017622546	0.00215143	0.019773976	0.031823051
L083	LD	50	13:00 ~ 14:00	100	0.570448935	0.060207468	0.630656403	1.014940575	0.019572017	0.001865439	0.021437456	0.018013463	0.001634539	0.0019648902	0.031621764	
L084	LD	50	13:00 ~ 14:00	50	0.631594537	0.013456834	0.645051372	1.038106975	0.0221705489	0.000997139	0.022702627	0.036536246	0.020008224	0.000923141	0.020931364	0.033685682
L085	LD	50	13:00 ~ 14:00	50	0.762618007	0.011147658	0.773756564	1.245252034	0.024934913	0.000819515	0.025754428	0.04147631	0.022974118	0.000756877	0.023730995	0.03819124
L086	LD	50	13:00 ~ 14:00	100	0.735753484	0.06939286	0.805146343	1.295754216	0.025735454	0.002271324	0.028006778	0.045072428	0.023674726	0.001983185	0.025657911	0.041292302
L087	LD	50	13:00 ~ 14:00	550	0.973275026	0.07491271	1.048187736	1.68689045	0.030351538	0.001095015	0.031446553	0.050608196	0.027931005	0.000965577	0.028896582	0.046504425
L088	LD	50	13:00 ~ 14:00	50	0.972686755	0.065872041	1.038558796	1.671394212	0.029153252	0.001602564	0.030755816	0.049496566	0.026826438	0.00139259	0.028219028	0.04541401
L089	LD	50	13:00 ~ 14:00	250	0.847610605	0.048902805	0.896513409	1.44279489	0.025945941	0.000954858	0.026900799	0.043292531	0.02388234	0.000844252	0.024726593	0.039793495
L090	LD	50	13:00 ~ 14:00	450	1.059965597	0.102356071	1.162321668	1.870570753	0.033779237	0.000555686	0.034348084	0.055277745	0.031094122	0.000496168	0.031590289	0.050839516
L091	LD	50	13:00 ~ 14:00	100	0.973500956	0.048156201	1.021657157	1.644193729	0.032350624	0.001218329	0.033568952	0.054023858	0.029781313	0.001081389	0.030862702	0.049668581
L092	LD	50	13:00 ~ 14:00	350	1.13030016	0.129006621	1.259066221	2.026651873	0.036025044	0.000587413	0.036612456	0.05892189	0.033141027	0.000518547	0.033659574	0.054169699
L093	LD	50	13:00 ~ 14:00	100	0.96050262	0.053919112	1.014421732	1.63254947	0.031047323	0.001146696	0.032194019	0.051811123	0.028575332	0.001014639	0.029589971	0.047620324
L094	LD	50	13:00 ~ 14:00	300	1.05054965	0.144952883	1.195020533	1.923970047	0.034809023	0.000511154	0.035320177	0.056842174	0.030202465	0.000466832	0.032488472	0.052286606
L095	LD	50	13:00 ~ 14:00	400	0.839302562	0.072472358	0.911774921	1.467355851	0.026149372	0.000883888	0.027033261	0.043505708	0.024072126	0.000810377	0.024882503	0.040044408
L096	LD	50	13:00 ~ 14:00	350	1.277502332	0.129585736	1.407088068	2.64483112	0.041214755	0.000459352	0.041674108	0.067068708	0.037919998	0.000420003	0.038340001	0.061702098
L097	LD	50	13:00 ~ 14:00	350	1.204744644	0.122303521	1.327048166	2.135671695	0.039261665	0.000665427	0.039927092	0.064256266	0.036122974	0.00059768	0.036720653	0.050960166
L098	LD	50	13:00 ~ 14:00	50	0.592260328	0.054516751	0.646777709	1.040884224	0.00184745	0.0021785006	0.00184745	0.023632456	0.020047025	0.001613265	0.021660209	0.034858771
L099	LD	50	13:00 ~ 14:00	550	1.152870629	0.070150562	1.223021191	1.968256924	0.036281787	0.000941032	0.037222819	0.059904171	0.033398007	0.000845593	0.0342364	0.050598008
L100	LD	50	13:00 ~ 14:00	50	0.893406058	0.019798784	0.913204841	1.469657079	0.002797439	0.000714708	0.029512147	0.047495079	0.026523889	0.000657719	0.027181607	0.037844448
L101	LD	50	13:00 ~ 14:00	150	1.072288875	0.043438835	1.11572771	1.795585233	0.034112036	0.001054379	0.035166414	0.056594717	0.031397705	0.000932213	0.032329917	0.052029329
L102	LD	50	13:00 ~ 14:00	100	0.631636261	0.04521051	0.676846772	1.089276583	0.021684098	0.00151109	0.023195188	0.037328944	0.021996492	0.001335038	0.034278878	
L103	LD	50	13:00 ~ 14:00	150	0.82791444	0.032887647	0.860802086	1.385323229	0.025974446	0.000960666	0.026995105	0.043347743	0.023917562	0.00086744	0.039887495	
L104	PD	50	13:00 ~ 14:00	1050	0.916708431	0.076636808	0.99345239	1.598630226	0.036951673	0.000714588	0.037666261	0.06061782	0.033987059	0.000627976	0.034615035	0.050757361
L105	PD	50	13:00 ~ 14:00	750	1.038601988	0.115680111	1.154282099	1.857632353	0.042176531	0.002925636	0.043102167	0.069366041	0.038790134	0.000796816	0.03985685	0.063708862
L106	LD	50	13:00 ~ 14:00	150	0.784665519	0.01462882	0.889294339	1.351176951	0.026892652	0.002010424	0.028903075	0.046514875	0.024734456	0.00175143	0.026485885	0.042624795
L107	LD	50	13:00 ~ 14:00	400	1.035699161	0.042980922	1.078680083	1.735963005	0.031066841	0.000786122	0.031852963	0.051262247	0.02859936	0.000702701	0.029302061	0.047156977
L108	LD	50	13:00 ~ 14:00	300	1.037285874	0.058557853	1.095843727	1.763585143	0.030321333	0.000601762	0.030923095	0.049765774	0.027906113	0.000532418	0.028438531	0.054757265
L109	LD	50	13:00 ~ 14:00	200	1.138932726	0.040558044	1.17949077	1.88201675	0.034753213	0.000784705	0.035537918	0.057192593	0.031988488	0.000701756	0.032690244	0.052609718
L110	LD	50	13:00 ~ 14:00	200	0.762651391	0.078250297	0.840901688	1.353296722	0.023441413	0.002012762	0.025454175	0.040964422	0.021555257	0.001733359	0.023288615	0.0374793
L111	LD	50	13:00 ~ 14:00	200	1.050247321	0.040242881	1.090490202	1.754969501	0.033348058	0.000919085	0.034267143	0.055147483	0.030694862	0.000816766	0.035111628	0.050712923
L112	LD	50	13:00 ~ 14:00	450	1.112484556	0.067983734	1.18046829	1.899774838	0.034933256	0.00072409	0.035657346	0.057384793	0.032154676	0.000656117	0.032810793	0.052803721
L113	LD	50	13:00 ~ 14:00	500	1.036345919	0.070298107	1.06644026	1.780966496	0.032991768	0.000945929	0.033937697	0.054617293	0.030365885	0.000844426	0.031210311	0.050228001
L114	LD	50	13:00 ~ 14:00	300	1.111832401	0.043282914	1.155115315	1.858973281	0.035219302	0.001080735	0.036300037	0.058419101	0.032412853	0.000959401	0.033372255	0.050737304
L115	LD	50	13:00 ~ 14:00	100	0.587202192	0.015560955	0.602763147	0.970050843	0.024221405	0.00090947	0.025122352	0.040430406	0.022315324	0.000820734	0.023136058	0.037237884
L116	LD	50	13:00 ~ 14:00	100	0.65393985	0.012835242	0.666775092	1.073067827	0.02303095	0.000937708	0.024247208	0.039022001	0.02148115	0.00064831	0.022345531	0.039615556
L117	LD	50	13:00 ~ 14:00	100	1.307095445	0.031513494	1.338608938	2.154276909	0.04248079	0.000790626	0.043271425	0.069638435	0.039097658	0.000694861	0.039792519	0.064039692
L118	PD	50	13:00 ~ 14:00	1200	0.91233013	0.06619752	1.074783688	0.9852765	0.036815278	0.000849562	0.03766479	0.060615453	0.033860968	0.000743388	0.034604357	0.05690175
L119	PD	50	13:00 ~ 14:00	750	1.085446802	0.117346851	1.202793653	1.935703937	0.044366627	0.001102439	0.045469066	0.073175186	0.040802546	0.000942525	0.047145071	0.067182012
L120	LD	50	13:00 ~ 14:00	100	1.096926685	0.086412824	1.18333951	1.904395607	0.035541837	0.00091015	0.036451987	0.058663641	0.03270615	0.000810881	0.033517031	0.059340299
L121	LD	50	13:00 ~ 14:00	250	1.019120055	0.054329706	1.073449761	1.727545639	0.030635779	0.000929493	0.031565272	0.050799254	0.028199701	0.000823211	0.029022912	0.046707733
L122	LD	50	13:00 ~ 14:00	400	1.075136868	0.050825571	1.125962439	1.812056391	0.032038434	0.000575617	0.032614051	0.052487097	0.029487497	0.000513692	0.030001188	0.048282113
L123	LD	50	13:00 ~ 14:00	150	0.999294044	0.08054334	1.079837385	1.737825497	0.032130252	0.000951011	0.033081264	0.053239001	0.029572178	0.000848254	0.030420432	0.048956862
L124	LD	50	13:00 ~ 14:00	50	0.42439821	0.025004113	0.449402323	0.723241134	0.016674863	0.001016975	0.01769138	0.028472183	0.015376242	0.000936365	0.033126077	0.026252532
L125	LD	50	13:00 ~ 14:00	50	0.507743079	0.02339254	0.531135619	0.854777797	0.019375684	0.000839944	0.020259629	0.032604631	0.017856551	0.000809399	0.01866595	0.030039986
L126	PD	50	13:00 ~ 14:00	750	1.082428182	0.11462981	1.197057992	1.926473309	0.044143592	0.001035652	0.045179244	0.07270865	0.040597895	0.000886556	0.04148445	0.066762586
L127	DD	50	13:00 ~ 14:00	100	0.4717394	0.021767										

Table 5: Hourly composite emission rate for each road link

L158	LD	50	13:00 ~ 14:00	50	0.651892948	0.115206041	0.767098988	1.234523086	0.01905475	0.001397351	0.020452101	0.032914383	0.017519745	0.001198363	0.018718108	0.030123799
L159	PD	50	13:00 ~ 14:00	800	0.952060059	0.058264404	1.007886463	1.622032001	0.041866919	0.000991031	0.04285795	0.068973013	0.038506454	0.000865716	0.03997217	0.063363207
L160	PD	50	13:00 ~ 14:00	750	0.977599716	0.058047685	1.036044685	1.667348153	0.043667534	0.000966421	0.044633954	0.071831208	0.040162948	0.000848677	0.041006624	0.065993601
L161	PD	50	13:00 ~ 14:00	750	1.061320746	0.066159992	1.127480738	1.814499851	0.043805102	0.007774359	0.044579461	0.07174351	0.040028351	0.00065392	0.040953743	0.065908497
L162	LD	50	13:00 ~ 14:00	350	0.72693764	0.06454573	0.79148337	1.273765846	0.021668395	0.001027483	0.022695878	0.036525384	0.019944954	0.000910578	0.020855532	0.033563641
L163	LD	50	13:00 ~ 14:00	400	0.780924419	0.051462737	0.707053819	1.127480738	0.022555779	0.000895461	0.023451224	0.03714019	0.020759515	0.000793132	0.021552647	0.034685537
L164	LD	50	13:00 ~ 14:00	50	0.918801016	0.019481166	0.938282182	1.510015047	0.028904692	0.000808814	0.029613506	0.0476582	0.026525991	0.000749165	0.027728756	0.043990793
L165	LD	50	13:00 ~ 14:00	50	0.844782834	0.017165361	0.951948195	1.532008308	0.027477878	0.003172192	0.03065007	0.049326384	0.02525005	0.002730938	0.045030923	0.038664779
L166	LD	50	13:00 ~ 14:00	400	0.76622605	0.068653166	0.834877926	1.343604517	0.021996612	0.000900327	0.02289638	0.036848959	0.020245199	0.000797572	0.021042772	0.033864974
L167	LD	50	13:00 ~ 14:00	350	0.738074372	0.063403181	0.801477551	1.289489885	0.021966472	0.001027078	0.0229393549	0.037004439	0.020219026	0.000910308	0.021129333	0.034004281
L168	DD	50	13:00 ~ 14:00	150	0.6737897	0.043213182	0.717002882	1.153901418	0.0206697	0.001120931	0.021790631	0.035068534	0.0190016	0.000997327	0.019998927	0.032185073
L169	DD	50	13:00 ~ 14:00	250	0.8008026	0.058167589	0.858970189	1.382375084	0.0262283	0.00125773	0.027486603	0.044234367	0.0241112	0.001095466	0.025206692	0.040566692
L170	LD	50	13:00 ~ 14:00	150	1.016770878	0.040069265	1.056840143	1.700815116	0.030879469	0.000793314	0.031672783	0.050972276	0.02842693	0.000712416	0.029139345	0.046895114
L171	LD	50	13:00 ~ 14:00	50	0.658811197	0.015361552	0.812426749	1.307470864	0.022187238	0.000888606	0.023175844	0.037297813	0.020412361	0.000884879	0.021297241	0.034274501
L172	LD	50	13:00 ~ 14:00	50	0.696593501	0.020850955	0.717444456	1.15461206	0.023196991	0.000759935	0.023956926	0.038554839	0.02137491	0.000696861	0.022071771	0.035520985
L173	LD	50	13:00 ~ 14:00	400	0.841656919	0.097171102	0.938828021	1.510893487	0.025184056	0.001115404	0.026299459	0.042324772	0.023170193	0.000979321	0.024149514	0.038864779
L174	LD	50	13:00 ~ 14:00	550	1.088889985	0.051491057	1.140381043	1.835260872	0.033514137	0.000987334	0.034501471	0.055524598	0.030842492	0.000870819	0.031713311	0.051037501
L175	LD	50	13:00 ~ 14:00	350	1.107583859	0.043745123	1.151328981	1.852879783	0.033046778	0.000826618	0.033873397	0.054513812	0.030416828	0.000733602	0.030733602	0.050131633
L176	LD	50	13:00 ~ 14:00	200	1.042711272	0.067926783	1.110638055	1.787394248	0.034415214	0.001341689	0.035756903	0.057545015	0.031661555	0.001171795	0.03283335	0.052840024
L177	LD	50	13:00 ~ 14:00	50	0.47471572	0.027549051	0.750264771	1.207431107	0.0142082	0.002736442	0.021571642	0.034716107	0.01293586	0.006225064	0.019160744	0.030836151
L178	LD	50	13:00 ~ 14:00	50	0.547543557	0.038924843	0.936792	1.507616838	0.014909345	0.009565323	0.024564668	0.039532902	0.013489712	0.0008065316	0.021555028	0.034689369
L179	LD	50	13:00 ~ 14:00	200	1.001257706	0.071673403	1.072931109	1.726710951	0.032857293	0.01404408	0.034261701	0.05138726	0.03022768	0.001222284	0.031449963	0.050613684
L180	PD	50	13:00 ~ 14:00	850	0.893144133	0.071857265	0.965001398	1.553015351	0.039268657	0.001097494	0.04036615	0.064962861	0.036115457	0.000956513	0.03707197	0.059661404
L181	PD	70	13:00 ~ 14:00	1650	0.640576193	0	0.640576193	0.030904891	0.021340258	0	0.021340258	0.034343731	0	0.01962317	0	0.031580353
L182	PD	70	13:00 ~ 14:00	2450	0.60656108	0	0.60656108	0.076163008	0.019609143	0	0.019609143	0.031557778	0.018032139	0	0.018032139	0.029019843
L183	PD	50	13:00 ~ 14:00	650	1.004068574	0.067647719	1.071743293	1.724799351	0.046477739	0.01031072	0.047508812	0.042746995	0.000898336	0.00364533	0.024017076	0.046895114
L184	PD	50	13:00 ~ 14:00	850	1.012662871	0.064989599	1.0765247	1.734309226	0.041407611	0.000834382	0.042241993	0.067981728	0.038082457	0.00071451	0.038799909	0.062442245
L185	DD	50	13:00 ~ 14:00	450	0.6887913	0.065158315	0.753949615	1.213361274	0.0214498	0.001094069	0.025438669	0.03628075	0.0197196	0.000963827	0.020668327	0.033286666
L186	DD	50	13:00 ~ 14:00	450	0.6458064	0.065534977	0.711341377	1.144790132	0.018273	0.000864716	0.019137716	0.030799092	0.0168009	0.00076882	0.01756972	0.028275653
L187	LD	50	13:00 ~ 14:00	150	0.868682519	0.061154939	0.929837458	1.496424615	0.02907072	0.001022579	0.030029651	0.048327919	0.026704256	0.000913595	0.027617851	0.044446513
L188	LD	50	13:00 ~ 14:00	100	0.948892631	0.049240783	0.998133413	1.606336027	0.030949343	0.000779928	0.03172927	0.051063184	0.028500329	0.000722857	0.029223185	0.047030041
L189	LD	50	13:00 ~ 14:00	50	0.409821492	0.072323484	0.482144976	1.05475175	0.02331129	0.017806304	0.028656397	0.014243567	0.002037409	0.0016280976	0.02601627	0.034685537
L190	LD	50	13:00 ~ 14:00	50	0.587451909	0.045195568	0.632647477	1.018144891	0.020264428	0.0014765	0.021740928	0.034988545	0.018666218	0.001301685	0.019967039	0.032135145
L191	LD	50	13:00 ~ 14:00	100	0.540397349	0.021785766	0.562183115	0.904743774	0.0179792	0.000961998	0.018941198	0.030408287	0.016579252	0.000889277	0.017468529	0.028112803
L192	LD	50	13:00 ~ 14:00	100	0.566881172	0.022508587	0.58938976	0.948528516	0.021812556	0.000876104	0.01906866	0.030687957	0.016774724	0.000807368	0.017582091	0.028295563
L193	LD	50	13:00 ~ 14:00	50	0.77939818	0.019971558	0.799331375	1.286399556	0.02647778	0.0005844	0.02647778	0.042611751	0.023599877	0.000792397	0.024392274	0.039254662
L194	LD	50	13:00 ~ 14:00	100	0.571754815	0.020360765	0.59211558	0.952915287	0.019230305	0.00097886	0.020209165	0.032523417	0.017729798	0.000905737	0.018635355	0.029909912
L195	LD	50	13:00 ~ 14:00	200	0.589694751	0.053014636	0.642709386	1.034337926	0.019916181	0.001679326	0.021595507	0.034754513	0.018336671	0.001475714	0.019812384	0.031884864
L196	LD	50	13:00 ~ 14:00	150	0.583415102	0.035130147	0.618545248	0.99544961	0.019165988	0.001195585	0.020361573	0.032768694	0.01766008	0.001072748	0.018732848	0.030147489
L001	LD	50	14:00 ~ 15:00	200	0.866706791	0.051552695	0.918259486	1.477791721	0.024988602	0.00075395	0.025742552	0.041428518	0.023002041	0.000672659	0.017367465	0.038100633
L002	LD	50	14:00 ~ 15:00	250	0.990568572	0.0805641	1.071132672	1.723816655	0.027863107	0.000371731	0.028234837	0.045439453	0.025636362	0.000326772	0.025963135	0.041783511
L003	LD	50	14:00 ~ 15:00	50	0.857154567	0.035026839	0.892181405	1.435823222	0.027646057	0.000739764	0.027646057	0.044491906	0.024781529	0.000682765	0.025464293	0.040980706
L004	DD	50	14:00 ~ 15:00	900	1.109172	0.04831178	1.15748378	1.862784947	0.041619	0.000731844	0.042350844	0.068156907	0.0382786	0.000646027	0.038924627	0.062642959
L005	DD	50	14:00 ~ 15:00	850	1.373071	0.090624378	1.423695378	2.29120992	0.0516037	0.000480065	0.052091765	0.083833362	0.0474689	0.000342267	0.047091167	0.070826434
L006	LD	50	14:00 ~ 15:00	250	1.160895948	0.078101711	1.238997659	1.939368493	0.033061802	0.000448467	0.03510269	0.053929416	0.03041947	0.000393361	0.030812831	0.049588321
L007	LD	50	14:00 ~ 15:00	400	1.063267297	0.063145261	1.126412557	1.812780785	0.030795402	0.000511462	0.031306863	0.028340795	0.028795004	0.000454209	0.028795004	0.046340952
L008	LD	50	14:00 ~ 15:00	150	1.100140015	0.040191265	1.14033128	1.835180744	0.031777438	0.000462238	0.032239677	0.051884601	0.029248305	0.00041283	0.029661135	0.0497734851
L009	LD	50	14:00 ~ 15:00	200	0.802285181	0.0495314	0.851816581	1.370862496	0.025620107	0.001107717	0.026727824	0.043014156	0.02358777	0.000986824	0.024574594	0.037548877
L010	LD	50	14:00 ~ 15:00	50	0.356420204	0.122577662	0.478997866	0.770870425	0.009948892	0.003075253	0.013024441	0.020960276	0.009120999	0.002637602	0.011758601	0.018923587
L011	LD	50	14:00 ~ 15:00	150	1.163006725	0.036648385	1.19965511	1.930652954	0.034060705	0.000473324	0.034534029	0.055576995	0.031350826	0.00042341	0.031774236	0.051135549
L012	LD	50	14:00 ~ 15:00	50	0.771097428	0.021424777	0.792521605	1.27543672	0.025958902	0.000861274	0.026820176	0.043162781	0.023913161	0.000794286	0.024707446	0.039762682
L013	LD	50	14:00 ~ 15:00	150	0.723469981	0.038168943	0.761689924									

Table 5: Hourly composite emission rate for each road link

L044	LD	50	14:00 ~ 15:00	350	0.958870001	0.076146881	1.035016882	1.665694069	0.029784482	0.000793573	0.030578055	0.049210487	0.027410865	0.000705626	0.028116491	0.045248994
L045	LD	50	14:00 ~ 15:00	150	0.634367993	0.042952475	0.677320468	1.090038923	0.020016567	0.001260089	0.021276656	0.034241374	0.018436053	0.001124996	0.019561049	0.031480378
L046	LD	50	14:00 ~ 15:00	150	0.595827661	0.026415122	0.622242783	1.0014002	0.018741469	0.000854774	0.019596242	0.031537017	0.017274889	0.000788989	0.018063878	0.029070921
L047	DD	50	14:00 ~ 15:00	1100	1.0649974	0.068904842	1.133902242	1.824834234	0.0405608	0.00171488	0.041272888	0.06343008	0.0373044	0.00063415	0.03793585	0.061056277
L048	DD	50	14:00 ~ 15:00	350	1.7115797	0.100165	1.8117447	2.915713215	0.0719953	0.000560805	0.072556105	0.116767442	0.0662245	0.00049192	0.06671642	0.107369404
L049	DD	50	14:00 ~ 15:00	550	0.6854321	0.048437456	0.733869567	1.181045631	0.0192905	0.000707693	0.019998193	0.01283892	0.0177401	0.000629274	0.018369974	0.029562568
L050	DD	50	14:00 ~ 15:00	450	0.7776909	0.069998542	0.847689442	1.364220527	0.0272632	0.000885583	0.028148783	0.045300963	0.0250702	0.000786784	0.025856984	0.041612679
L051	DD	50	14:00 ~ 15:00	500	0.6962178	0.049497503	0.745715303	1.200109466	0.0194906	0.000673055	0.020136355	0.032450176	0.0179246	0.000598609	0.01852269	0.029809305
L052	DD	50	14:00 ~ 15:00	350	0.83379	0.07131475	0.90510475	1.456621278	0.0295873	0.000765515	0.030352815	0.048848	0.0272102	0.00068436	0.02789456	0.044891831
L053	DD	50	14:00 ~ 15:00	650	0.6664172	0.06211515	0.728548715	1.17248259	0.0196273	0.000717956	0.020345256	0.032742434	0.0180489	0.000642574	0.018691474	0.030080936
L054	DD	50	14:00 ~ 15:00	350	0.930067	0.070655204	1.000722204	1.610502271	0.0322037	0.000668798	0.032872498	0.052903027	0.0296208	0.00059967	0.03022047	0.04863501
L055	LD	50	14:00 ~ 15:00	100	0.650775905	0.039755378	0.690531283	1.111299615	0.020781532	0.001174414	0.021955946	0.019140114	0.01056888	0.002019703	0.032503844	0.020197003
L056	LD	50	14:00 ~ 15:00	100	0.385265776	0.041551396	0.426817173	0.686893948	0.013630288	0.001292606	0.014922894	0.024016011	0.01256694	0.001164393	0.013731333	0.022098383
L057	DD	50	14:00 ~ 15:00	700	0.6711648	0.062005175	0.733169975	1.179919768	0.0195477	0.000672317	0.020220017	0.032540882	0.017976	0.000601378	0.018577378	0.029897317
L058	DD	50	14:00 ~ 15:00	400	0.935876	0.066290794	1.019878394	1.641331094	0.0326041	0.000609317	0.033213417	0.053451681	0.0299894	0.000546024	0.030353542	0.049141879
L059	LD	50	14:00 ~ 15:00	150	1.230115375	0.032636117	1.262751491	2.032196485	0.039385809	0.000725697	0.040111506	0.064553051	0.036256993	0.000658142	0.036915135	0.059409003
L060	LD	50	14:00 ~ 15:00	150	1.015163409	0.039647006	1.054810415	1.697548593	0.033058167	0.001009915	0.034068082	0.054827126	0.030433755	0.000901361	0.031335116	0.050428855
L061	LD	50	14:00 ~ 15:00	50	0.43106829	0.042935275	0.474003565	0.762832898	0.016873346	0.001887393	0.018760739	0.030192407	0.015555552	0.001691318	0.01724687	0.030152717
L062	LD	50	14:00 ~ 15:00	100	0.873217387	0.042378483	0.91559587	1.473505058	0.025751	0.000948414	0.026699414	0.042968434	0.023710695	0.000847901	0.024558596	0.039525131
L063	LD	50	14:00 ~ 15:00	50	0.54118792	0.024794389	0.56982309	0.910857969	0.01956388	0.000778504	0.020342384	0.032737812	0.01802568	0.000710396	0.018736076	0.030152717
L064	LD	50	14:00 ~ 15:00	50	0.546682632	0.086260172	0.632942804	1.018620172	0.018017569	0.002444573	0.020462143	0.029390545	0.016567597	0.002108308	0.018675905	0.030055881
L065	DD	50	14:00 ~ 15:00	550	1.4034908	0.06850613	1.471199693	2.36894354	0.0578282	0.000553692	0.058381892	0.099396314	0.0551915	0.000491316	0.053682816	0.086399303
L066	DD	50	14:00 ~ 15:00	500	1.3835206	0.075506978	1.459027578	2.448071442	0.0546834	0.000490333	0.055173733	0.088793296	0.0503001	0.000433066	0.050733166	0.081646913
L067	DD	50	14:00 ~ 15:00	500	1.3898281	0.06797739	1.4578002	2.346090971	0.0571206	0.00055713	0.057767713	0.092697919	0.052632	0.000494452	0.053117652	0.085484369
L068	DD	50	14:00 ~ 15:00	850	1.0594517	0.063596983	1.444848683	2.842450779	0.0384667	0.000652303	0.039139003	0.062878964	0.035398	0.000575456	0.057893522	0.07893522
L069	LD	50	14:00 ~ 15:00	400	0.861392082	0.052335861	0.913727943	1.470498928	0.024428061	0.000785461	0.025213522	0.04057713	0.02248583	0.000698637	0.023184467	0.037311691
L070	LD	50	14:00 ~ 15:00	400	0.864005767	0.053599492	0.917605259	1.476738848	0.024280436	0.000765355	0.025045791	0.040307193	0.02234963	0.000679774	0.03029404	0.037062141
L071	LD	50	14:00 ~ 15:00	200	0.753183253	0.049041007	0.80222426	1.29105159	0.019989253	0.000707894	0.020697147	0.033306747	0.018400179	0.000630439	0.01903617	0.030626734
L072	LD	50	14:00 ~ 15:00	200	0.945186361	0.043789942	0.988976303	1.591599124	0.026804863	0.000661707	0.02746657	0.04420305	0.024674595	0.000596275	0.025270871	0.040669423
L073	LD	50	14:00 ~ 15:00	150	0.720206696	0.051612874	0.77181957	1.242120107	0.01791203	0.000622209	0.018534239	0.029827893	0.016488476	0.000553976	0.017042452	0.0274271
L074	LD	50	14:00 ~ 15:00	150	0.554443159	0.052842594	0.607285753	0.977329254	0.014020898	0.000943976	0.014964875	0.024083572	0.012913447	0.00084395	0.013757597	0.0240329
L075	DD	50	14:00 ~ 15:00	800	0.6133349	0.061569134	0.674904034	1.086150058	0.0169443	0.000668398	0.017628278	0.028368993	0.0155801	0.000611204	0.016191304	0.026057313
L076	DD	50	14:00 ~ 15:00	350	0.9613369	0.070753295	1.032090195	1.660884034	0.0334479	0.000601619	0.034049519	0.05477255	0.0307663	0.000541723	0.031308023	0.050385254
L077	LD	50	14:00 ~ 15:00	350	1.0222112842	0.054695165	1.076808007	1.732950198	0.03001924	0.000820424	0.030839664	0.049631505	0.027628511	0.000727672	0.028356183	0.04563474
L078	DD	50	14:00 ~ 15:00	400	1.7121792	0.06263472	1.77481392	2.856279034	0.0744015	0.000465347	0.074866847	0.120486212	0.0684379	0.000414015	0.068851915	0.110806141
L079	DD	50	14:00 ~ 15:00	800	1.0687269	0.087710202	1.156437102	1.861100485	0.0391915	0.000645046	0.039836546	0.064110547	0.0306461	0.000568975	0.036615075	0.058926105
L080	DD	50	14:00 ~ 15:00	400	1.7842491	0.063539235	1.847788335	2.973719679	0.0782335	0.000451242	0.078674742	0.126144049	0.0719536	0.000401059	0.072354659	0.116443247
L081	DD	50	14:00 ~ 15:00	800	1.0676039	0.090151992	1.157755892	1.863222868	0.0392277	0.000703364	0.039931064	0.064262658	0.0360786	0.000617351	0.036695951	0.059056262
L082	LD	50	14:00 ~ 15:00	50	0.550931188	0.025527826	0.625551982	1.006725826	0.018193196	0.002182829	0.016736128	0.032791953	0.016736128	0.001899975	0.018636103	0.029991826
L083	LD	50	14:00 ~ 15:00	50	0.551459792	0.063794045	0.615253837	0.99015261	0.018010962	0.001826514	0.019837476	0.031925243	0.016575332	0.001599019	0.018174351	0.02924871
L084	LD	50	14:00 ~ 15:00	50	0.563423183	0.014409976	0.577833159	0.929930016	0.00276511	0.001069321	0.021445832	0.035413636	0.018787265	0.000990402	0.01777667	0.031828991
L085	LD	50	14:00 ~ 15:00	50	0.714083536	0.011845262	0.725928798	1.168266252	0.024382532	0.000864108	0.02524664	0.040630427	0.0246637	0.000796175	0.023262545	0.037437344
L086	LD	50	14:00 ~ 15:00	100	0.669762626	0.077362671	0.747125297	1.202378625	0.023362701	0.002432853	0.025795554	0.041513816	0.021488221	0.002117792	0.023606013	0.037990102
L087	LD	50	14:00 ~ 15:00	550	0.825769661	0.078674306	0.904439666	1.455557852	0.024825432	0.00150625	0.025881456	0.041652063	0.022845783	0.000932268	0.02377805	0.038266967
L088	LD	50	14:00 ~ 15:00	50	0.830905209	0.071314131	0.90221934	1.451977673	0.02440085	0.00159973	0.02604058	0.041908147	0.022488146	0.001390701	0.023878846	0.038429183
L089	LD	50	14:00 ~ 15:00	250	0.715767801	0.049935359	0.765703161	1.232276725	0.020336878	0.000859073	0.02119595	0.034111491	0.018722106	0.000761552	0.019483856	0.03135583
L090	LD	50	14:00 ~ 15:00	450	0.945616005	0.110322577	1.055938582	1.69364198	0.029504218	0.000508243	0.030012461	0.048300254	0.027147446	0.000455798	0.027603244	0.044323005
L091	LD	50	14:00 ~ 15:00	100	0.866444285	0.036248938	0.902693222	1.452740311	0.028251125	0.000931362	0.029182487	0.046964544	0.026013332	0.000845805	0.02689137	0.043225483
L092	LD	50	14:00 ~ 15:00	350	1.006933678	0.137702247	1.144635925	1.842108379	0.03148082	0.000526819	0.032007639	0.051511173	0.028958929	0.000466913	0.029425842	0.047356185
L093	LD	50	14:00 ~ 15:00	100	0.78063857	0.053933633	0.83479749	1.343472992	0.024256527	0.001172581	0.025429107	0.04092408	0.022325849	0.001038622	0.023366471	0.037601378
L094	LD	50	14:00 ~ 15:00	300	0.966035592	0.159419186	1.125454778	1.811239393	0.031463772	0.000480133	0.031943905	0.051408604	0.028943107	0.000438166	0.029381723	0.047284458
L095	LD	50	14:00 ~ 15:00	400	0.714220799	0.07804857	0.79226937	1.275030787	0.020870886							

Table 5: Hourly composite emission rate for each road link

L126	PD		50	14:00 ~ 15:00	700	0.985042636	0.119530444	1.10457308	1.77763364	0.039494567	0.000944185	0.040438752	0.065079701	0.036322219	0.000809885	0.037132104	0.059758181
L127	DD		50	14:00 ~ 15:00	100	0.4139295	0.025629026	0.439558526	0.707399118	0.0141055	0.000979093	0.015084593	0.024276239	0.0129703	0.000906325	0.013876625	0.022332208
L128	LD		150	14:00 ~ 15:00	250	0.917093499	0.066151651	0.983245149	1.58237549	0.026576772	0.000719521	0.027296293	0.024636868	0.000647097	0.025110785	0.04041179	0.04041179
L129	LD		350	14:00 ~ 15:00	350	0.907803603	0.062423641	0.970227244	1.561425512	0.025118453	0.000578358	0.025696811	0.041354905	0.02311528	0.000509601	0.023624881	0.038020466
L130	LD		50	14:00 ~ 15:00	350	0.942990831	0.063601474	1.006592304	1.619949259	0.025285426	0.000485147	0.025770572	0.041473613	0.023265563	0.000422853	0.023688416	0.038122715
L131	LD		250	14:00 ~ 15:00	250	0.917093499	0.066151651	0.983245149	1.58237549	0.026576772	0.000719521	0.027296293	0.024636868	0.000647097	0.025110785	0.04041179	0.04041179
L132	PD		1250	14:00 ~ 15:00	1250	0.835877832	0.096807288	0.932685121	1.501007472	0.031188661	0.00100668	0.032195341	0.05181325	0.028680251	0.00861723	0.029541974	0.047543081
L133	DD		800	14:00 ~ 15:00	800	0.9112041	0.049341719	0.960545819	1.545844806	0.031291	0.000692554	0.01383554	0.051472413	0.0287758	0.000616644	0.029392444	0.047302435
L134	DD		550	14:00 ~ 15:00	550	0.9394527	0.059678902	1.04131602	1.680362752	0.0355241	0.00055797	0.036068207	0.058068319	0.0326707	0.00049822	0.03316892	0.053380069
L135	LD		150	14:00 ~ 15:00	150	0.855411043	0.035751807	0.93116205	1.434184021	0.024248669	0.000536808	0.032478637	0.039898967	0.022319963	0.000481086	0.022800949	0.0366948
L136	PD		800	14:00 ~ 15:00	800	0.862623188	0.058853649	0.921476837	1.482969533	0.037071	0.000916451	0.037987451	0.061134724	0.034095293	0.000802198	0.034897492	0.056161929
L137	PD		1350	14:00 ~ 15:00	1350	0.751117809	0.072431207	0.823549017	1.325370375	0.029457013	0.000796137	0.030275314	0.048687588	0.02709212	0.000698753	0.027780873	0.044724963
L138	LD		250	14:00 ~ 15:00	250	0.839255453	0.066820606	0.906076059	1.458184446	0.023382575	0.000679728	0.024062302	0.038724425	0.0215329	0.000610221	0.022133511	0.0335620344
L139	LD		200	14:00 ~ 15:00	200	0.95147735	0.077831252	1.029308602	1.656507505	0.024525877	0.000468574	0.024994452	0.040224571	0.022564863	0.000480238	0.02029731	0.036971529
L140	LD		150	14:00 ~ 15:00	150	0.842730102	0.061886584	0.904616686	1.455835818	0.025152638	0.00087659	0.026020297	0.041875505	0.023154798	0.000784693	0.023939491	0.038526781
L141	PD		1200	14:00 ~ 15:00	1200	0.795042161	0.07378251	0.868420412	1.397583705	0.031888318	0.00089239	0.032697557	0.052621487	0.029327757	0.000706264	0.030034022	0.048334952
L142	LD		250	14:00 ~ 15:00	250	0.633564991	0.066184435	0.699749426	1.126134741	0.017342187	0.000717234	0.018054921	0.029056507	0.015969495	0.000646699	0.016616194	0.026741106
L143	LD		250	14:00 ~ 15:00	250	0.755116327	0.072244172	0.827360499	1.331504366	0.020449255	0.000672791	0.021122047	0.033992554	0.018820531	0.000598749	0.019419279	0.031252223
L144	LD		150	14:00 ~ 15:00	150	0.660147831	0.118273316	0.78421147	1.252744289	0.018189732	0.000922353	0.019112085	0.030757843	0.016731164	0.000814289	0.017545453	0.028236599
L145	LD		100	14:00 ~ 15:00	100	0.656845766	0.050189456	0.707035223	1.137860065	0.017273531	0.001045794	0.018319326	0.029482023	0.015900465	0.000922156	0.016821971	0.027072721
L146	PD		750	14:00 ~ 15:00	750	0.784324852	0.058622211	0.932947063	1.501429026	0.037686604	0.000900333	0.038586936	0.0620995	0.034661751	0.000788489	0.03545024	0.05705149
L147	PD		1050	14:00 ~ 15:00	1050	0.831693288	0.071051199	0.902744487	1.452822813	0.033908555	0.000777821	0.034686376	0.055822172	0.03118604	0.000678294	0.031864333	0.051280546
L148	LD		250	14:00 ~ 15:00	250	0.626857837	0.071081446	0.697939284	1.123211607	0.017231493	0.000855827	0.018087321	0.029108649	0.015865839	0.00076746	0.016633298	0.026766832
L149	LD		500	14:00 ~ 15:00	500	0.724642845	0.087567352	0.8071220196	1.307122357	0.019756484	0.000771556	0.02052804	0.03036595	0.018179837	0.0006882343	0.01886208	0.0303555
L150	LD		250	14:00 ~ 15:00	250	0.776921097	0.094702696	0.871623799	1.402739035	0.021973021	0.000815779	0.022788799	0.036674927	0.020214646	0.000716363	0.020931009	0.0368511
L151	LD		550	14:00 ~ 15:00	550	0.92098317	0.076956232	0.997939402	1.606023797	0.030919388	0.000842531	0.031761918	0.05115726	0.028450869	0.000747398	0.029198267	0.04698939
L152	LD		550	14:00 ~ 15:00	550	1.036059262	0.06700529	1.103759791	1.776324782	0.035412966	0.000815207	0.036228173	0.058303448	0.03258662	0.000724097	0.033310717	0.053608291
L153	LD		250	14:00 ~ 15:00	250	0.852353234	0.068371068	0.902724301	1.481758447	0.025859173	0.000930045	0.026789217	0.043112959	0.0237967	0.000822299	0.02462059	0.036922969
L154	LD		200	14:00 ~ 15:00	200	0.640315019	0.104077142	0.744392161	1.19798008	0.016532898	0.000954829	0.017487727	0.028143699	0.015209703	0.000839186	0.01604889	0.02582812
L155	LD		250	14:00 ~ 15:00	250	0.844957383	0.057169223	0.902126606	1.451828432	0.02532267	0.000885619	0.026208289	0.042178048	0.02307247	0.000787151	0.024094398	0.038776078
L156	LD		200	14:00 ~ 15:00	200	0.601102042	0.1065555	0.707657542	1.138861589	0.014961441	0.00085531	0.015816751	0.025454531	0.013764679	0.000754595	0.014519274	0.032366448
L157	LD		100	14:00 ~ 15:00	100	0.742634068	0.060762624	0.803396692	1.292938433	0.020660477	0.001140258	0.021800735	0.035084795	0.019010945	0.00099212	0.020003675	0.032191732
L158	LD		50	14:00 ~ 15:00	50	0.63232433	0.095187024	0.727511354	1.170813122	0.016800903	0.00071231	0.017513213	0.028184714	0.015454742	0.000624413	0.016079155	0.025876827
L159	PD		750	14:00 ~ 15:00	750	0.784324852	0.058622211	0.932947063	1.501429026	0.037686604	0.000900333	0.038586936	0.0620995	0.034661751	0.000788489	0.03545024	0.05705149
L160	PD		700	14:00 ~ 15:00	700	0.898930716	0.061584707	0.960515423	1.545795892	0.039277684	0.000897969	0.040175653	0.064656286	0.036125412	0.000785599	0.036911011	0.059402367
L161	PD		700	14:00 ~ 15:00	700	0.968142622	0.069878522	1.038021144	1.670528949	0.039625179	0.000717754	0.040342933	0.064925497	0.036443751	0.000618309	0.03706206	0.059645455
L162	LD		350	14:00 ~ 15:00	350	0.638200992	0.066718945	0.704919937	1.134455852	0.017452099	0.000927748	0.018379847	0.029579423	0.016064845	0.000822725	0.016887571	0.027177843
L163	LD		400	14:00 ~ 15:00	400	0.675785928	0.07260549	0.748391419	1.204416246	0.017795485	0.000760394	0.029862717	0.016379058	0.00065758	0.017054816	0.027446997	0.031274697
L164	LD		50	14:00 ~ 15:00	50	0.819852696	0.024996934	0.84484963	1.359650303	0.025005668	0.000808814	0.025814482	0.041544278	0.023032672	0.000749165	0.023781837	0.038273061
L165	LD		50	14:00 ~ 15:00	50	0.65910558	0.108997967	0.768103546	1.236139761	0.02190722	0.003313724	0.025220944	0.040589073	0.020134377	0.002863571	0.022997948	0.037011518
L166	LD		450	14:00 ~ 15:00	450	0.665227163	0.071512962	0.736380125	1.18508599	0.017401013	0.000766137	0.01816715	0.029237121	0.016016719	0.00080633	0.016697352	0.026871717
L167	LD		350	14:00 ~ 15:00	350	0.625553842	0.067564468	0.69311831	1.115463021	0.017047057	0.000932499	0.017979556	0.028935219	0.01569234	0.00082726	0.016519634	0.026585708
L168	DD		150	14:00 ~ 15:00	150	0.5871013	0.048386676	0.635487976	1.02271622	0.0167432	0.01059525	0.017839125	0.028709218	0.0153901	0.000974277	0.016364377	0.026335846
L169	DD		250	14:00 ~ 15:00	250	0.6924281	0.055141325	0.747569425	1.203093378	0.0213329	0.01055543	0.022388443	0.036030617	0.0196108	0.000926453	0.020537253	0.033051422
L170	LD		150	14:00 ~ 15:00	150	0.671481828	0.045817354	0.791299182	1.476246265	0.025071648	0.000874416	0.025946065	0.041756039	0.023080732	0.00077745	0.023858182	0.038395926
L171	LD		50	14:00 ~ 15:00	50	0.64735561	0.158030822	0.805386433	1.296140602	0.020586522	0.000910756	0.021497277	0.034596428	0.018937687	0.00081384	0.019751526	0.031786921
L172	LD		50	14:00 ~ 15:00	50	0.681399484	0.025176551	0.706576035	1.137121077	0.021741062	0.000791459	0.022532521	0.036262488	0.020032829	0.000727736	0.020760564	0.033410807
L173	LD		400	14:00 ~ 15:00	400	0.732963168	0.097996057	0.830959225	1.337295919	0.020483115	0.000935935	0.02141905	0.034470534	0.018845211	0.000824455	0.019696666	0.031655518
L174	LD		500	14:00 ~ 15:00	500	0.90711301	0.054895264	0.961972274	1.548140459	0.026699001	0.000953404	0.027642405	0.044486028	0.024563007	0.000841342	0.025404349	0.040848236
L175	LD		300	14:00 ~ 15:00	300	0.9324074	0.04524813	0.968655531	1.26818891	0.000771796	0.002693687	0.043377647	0.024100108	0.000687844	0.024787952	0.039892243	0.048482436
L176	LD	</															

Table 5: Hourly composite emission rate for each road link

L012	ID		50	15:00 ~ 16:00	0.698083822	0.034953152	0.733018954	1.179676723	0.023348263	0.001214756	0.024563019	0.039530249	0.021504346	0.001093353	0.022597699	0.03636738
L013	LD		200	15:00 ~ 16:00	0.661008524	0.039278198	0.700286721	1.126999432	0.019249902	0.000775602	0.020025503	0.032227844	0.01772817	0.000698973	0.018427143	0.02965559
L014	DD		1150	15:00 ~ 16:00	1.00372	0.06553309	1.06880509	1.720070784	0.0372675	0.000825961	0.038093461	0.06130533	0.0342741	0.000728345	0.035002445	0.056330835
L015	DD		450	15:00 ~ 16:00	1.7167223	0.0434875	1.7602098	2.832776039	0.0717725	0.000552125	0.07279625	0.11635146	0.0660191	0.00045855	0.06647765	0.106985142
L016	LD		150	15:00 ~ 16:00	0.553171464	0.050428416	0.603599879	0.97139743	0.0127699	0.000781053	0.013550954	0.021808092	0.011764642	0.000703333	0.012467975	0.02006521
L017	LD		200	15:00 ~ 16:00	1.023026762	0.08159336	1.061186097	1.707809234	0.02966519	0.000601469	0.027305713	0.030266661	0.048709348	0.000537497	0.02784321	0.044809192
L018	LD		300	15:00 ~ 16:00	0.712703237	0.053850638	0.766553875	1.233645813	0.022990175	0.00132264	0.024312815	0.039127586	0.021165887	0.001172805	0.023238692	0.035950551
L019	LD		300	15:00 ~ 16:00	1.01390146	0.061812574	1.075714033	1.731189622	0.031675536	0.001498167	0.033173703	0.053387768	0.029141098	0.00129781	0.030438908	0.046985652
L020	LD		400	15:00 ~ 16:00	0.783792768	0.05896724	0.842789492	1.356334841	0.022510609	0.001201071	0.023711679	0.038160154	0.020717018	0.001054531	0.021771349	0.030375033
L021	DD		1100	15:00 ~ 16:00	1.0600725	0.064567333	1.124639833	1.80927869	0.0398125	0.000768128	0.040580628	0.065308028	0.0366158	0.000675473	0.037291373	0.060014498
L022	DD		550	15:00 ~ 16:00	1.5101292	0.070904956	1.581034156	2.544421509	0.0615848	0.000530898	0.062115698	0.099965277	0.056647	0.000468586	0.057115586	0.091918397
L023	LD		400	15:00 ~ 16:00	1.045825935	0.043068583	1.088894517	1.752401503	0.03068927	0.000730339	0.031419609	0.050564833	0.028245503	0.000645768	0.028891271	0.046495878
L024	LD		250	15:00 ~ 16:00	0.49002853	0.047856944	0.537885474	0.865640609	0.016588541	0.001413895	0.018002436	0.02897204	0.015284441	0.001266789	0.01655123	0.026636556
L025	DD		100	15:00 ~ 16:00	1.009042	0.020745116	1.029787116	1.657277598	0.034077	0.000681843	0.034758843	0.055938796	0.031351	0.000628893	0.031979893	0.051466522
L026	DD		100	15:00 ~ 16:00	1.0615117	0.022404585	1.083916285	1.744389834	0.0354738	0.000608243	0.036082043	0.058068275	0.0326381	0.000560687	0.033198787	0.053428137
L027	DD		450	15:00 ~ 16:00	0.8181227	0.061125905	0.879248605	1.415009949	0.0251823	0.000696448	0.025878748	0.041647705	0.0231586	0.000616588	0.023775188	0.03826236
L028	DD		350	15:00 ~ 16:00	0.8463717	0.06455871	0.91093041	1.465996745	0.026063	0.00062616	0.02668916	0.042951932	0.023968	0.000552445	0.024520445	0.039461733
L029	LD		100	15:00 ~ 16:00	0.429160133	0.023489414	0.452649547	0.728467022	0.01537658	0.001135837	0.016512417	0.026574093	0.014188917	0.001053885	0.015242802	0.024530851
L030	DD		1350	15:00 ~ 16:00	0.9500097	0.068111983	1.018121683	1.63850395	0.0352954	0.00089846	0.03619386	0.058248226	0.0325479	0.000789369	0.033247269	0.059506199
L031	DD		1100	15:00 ~ 16:00	0.2265123	0.070211217	0.296723517	0.477529025	0.0048047	0.000755203	0.005540203	0.00891607	0.0044108	0.000647786	0.005058586	0.008140984
L032	DD		500	15:00 ~ 16:00	1.4432198	0.087535898	1.530755698	2.463506374	0.0590683	0.000823302	0.059891602	0.096385951	0.0543291	0.000718416	0.055047516	0.08859017
L033	LD		250	15:00 ~ 16:00	0.57895707	0.027298419	0.61941988	1.049136319	0.019427335	0.001220315	0.021547649	0.034677494	0.017872496	0.001838871	0.019711367	0.031722291
L034	LD		250	15:00 ~ 16:00	0.570290957	0.05669798	0.626981755	1.009026818	0.018679275	0.001693887	0.020373162	0.032787344	0.017197197	0.001486514	0.018683711	0.030668443
L035	LD		50	15:00 ~ 16:00	0.836173699	0.014655514	0.850839213	1.369289578	0.030119794	0.001099612	0.031216706	0.050238293	0.027747595	0.001018365	0.0287659	0.046294114
L036	LD		50	15:00 ~ 16:00	0.571076128	0.018330725	0.589406853	0.948556025	0.022522642	0.001379975	0.023902618	0.038474739	0.020746985	0.001283632	0.022050617	0.03548684
L037	LD		250	15:00 ~ 16:00	0.52480326	0.059882424	0.584685684	0.940958059	0.017199099	0.001784758	0.018938857	0.03055148	0.015834609	0.001565577	0.017400185	0.028002814
L038	LD		300	15:00 ~ 16:00	0.540231838	0.049937515	0.590169353	0.949783146	0.017673081	0.001546013	0.019219094	0.030930357	0.01368714	0.001368714	0.017639262	0.028392826
L039	LD		50	15:00 ~ 16:00	0.490579312	0.051296089	0.541875401	0.872067158	0.015355058	0.001534514	0.016889571	0.027813063	0.014150526	0.001359501	0.015510027	0.024960906
L040	LD		50	15:00 ~ 16:00	0.627180226	0.025719956	0.652900182	1.050738379	0.020483082	0.000991471	0.021474553	0.034559857	0.018878451	0.000919362	0.019797814	0.031861414
L041	LD		100	15:00 ~ 16:00	0.534407722	0.042821002	0.577228723	0.9328957274	0.017503329	0.001380069	0.018883398	0.030889807	0.016125716	0.001229616	0.017355332	0.02793063
L042	LD		100	15:00 ~ 16:00	0.604926382	0.024951008	0.629877739	1.013686879	0.009939481	0.002656373	0.020865853	0.033580252	0.018378151	0.000856824	0.019234975	0.030595515
L043	LD		250	15:00 ~ 16:00	1.065310458	0.052522102	1.11783256	1.798972653	0.030746077	0.000827218	0.031573295	0.050812167	0.028294827	0.000727565	0.029022933	0.046706897
L044	LD		400	15:00 ~ 16:00	0.871208377	0.070435304	0.941643682	1.534248443	0.027070257	0.000960476	0.028030734	0.045110981	0.024914459	0.000852842	0.0257673	0.041468347
L045	LD		200	15:00 ~ 16:00	0.597565464	0.041969165	0.639534629	1.029228659	0.019090375	0.001299784	0.020390159	0.032814699	0.017586757	0.001165386	0.018751063	0.030176836
L046	LD		200	15:00 ~ 16:00	0.571061377	0.026270715	0.597332092	0.961310429	0.018123628	0.000915033	0.019038661	0.030639678	0.016709464	0.000846077	0.017555542	0.028252836
L047	DD		1300	15:00 ~ 16:00	0.9570312	0.06836808	1.02539928	1.650216078	0.0359749	0.000906828	0.036881728	0.05935524	0.0330833	0.000798143	0.033881443	0.054526761
L048	DD		400	15:00 ~ 16:00	1.5758862	0.094926382	1.670812238	2.688904967	0.0660913	0.000787943	0.066879243	0.107631441	0.0607902	0.00068691	0.06147711	0.098937573
L049	DD		650	15:00 ~ 16:00	0.6120971	0.050609287	0.662166387	1.06550854	0.0170671	0.000825591	0.017892691	0.028795423	0.0156935	0.000733359	0.016426859	0.026346401
L050	DD		500	15:00 ~ 16:00	0.7111539	0.067675386	0.778829286	1.253401124	0.0247541	0.00107425	0.02582835	0.041566596	0.0227601	0.000950315	0.023710415	0.038158119
L051	DD		600	15:00 ~ 16:00	0.621385	0.050725256	0.672110256	1.08165392	0.0171581	0.000802314	0.017960414	0.028904413	0.0157772	0.00071195	0.01648915	0.026536649
L052	DD		400	15:00 ~ 16:00	0.7551434	0.067952037	0.823095437	1.32464041	0.0266647	0.000922629	0.027587329	0.044397392	0.0245202	0.000822006	0.025342206	0.047844226
L053	DD		750	15:00 ~ 16:00	0.6021244	0.060998873	0.663124273	1.067192418	0.0175944	0.000882061	0.018476461	0.029734907	0.016177	0.000785726	0.016962726	0.027298794
L054	DD		450	15:00 ~ 16:00	0.8203345	0.06640873	0.886975373	1.427444947	0.028419	0.000816483	0.029058383	0.047674818	0.0259749	0.000730837	0.026705737	0.042978611
L055	LD		150	15:00 ~ 16:00	0.606728705	0.044006063	0.650734768	1.047253491	0.019456511	0.001333086	0.020789597	0.03345753	0.017921286	0.001194672	0.019115958	0.030764075
L056	LD		100	15:00 ~ 16:00	0.349851618	0.047844235	0.397695852	0.640027842	0.012749771	0.001540113	0.014289884	0.0229797282	0.011755916	0.001380243	0.01136159	0.021140546
L057	DD		800	15:00 ~ 16:00	0.6065149	0.060314267	0.666829167	1.073154852	0.0174568	0.00083187	0.01828867	0.029432688	0.0160506	0.000740733	0.016791333	0.027022964
L058	DD		150	15:00 ~ 16:00	0.8396659	0.062101246	0.901767146	1.45124994	0.0285931	0.000743995	0.029337095	0.047213361	0.0262985	0.000666295	0.026964795	0.043395524
L059	LD		150	15:00 ~ 16:00	1.081796492	0.030664075	1.142460567	1.790327288	0.035105842	0.0008237	0.035929542	0.057822848	0.032321428	0.000749688	0.033071116	0.05322267
L060	LD		150	15:00 ~ 16:00	0.902591334	0.03956149	0.942152824	1.516244226	0.029870716	0.001132612	0.031003328	0.049894897	0.02750191	0.001016352	0.028518262	0.045895579
L061	LD		50	15:00 ~ 16:00	0.448614006	0.030230107	0.478844113	0.770622984	0.017362891	0.001600426	0.018963318	0.030518426	0.016009736	0.001455733	0.01746547	0.028107879
L062	LD		100	15:00 ~ 16:00	0.729644151	0.048163062	0.777807213	1.25175626	0.021595496	0.001241268	0.022836763	0.036752117	0.019885677	0.001105494	0.020991171	0.033781931
L063	LD		50	15:00 ~ 16:00	0.54918216	0.024794389	0.573976549	0.92372342	0.01980504	0.000778504	0.020583544	0.03312592	0.01824848	0.000710396	0.018958876	0.030511278
L064	LD		100	15:00 ~ 16:												

Table 5: Hourly composite emission rate for each road link

L094	LD		50	15:00 ~ 16:00	300	0.936985175	0.133112294	1.070097469	1.722150661	0.030295242	0.00062033	0.030915573	0.049753668	0.027873532	0.000563581	0.028437113	0.045764984
L095	LD		50	15:00 ~ 16:00	500	0.688275527	0.070638489	0.758914016	1.221350683	0.020171099	0.000901651	0.02107275	0.03391322	0.018574503	0.000823215	0.019397718	0.032117524
L096	LD		350	15:00 ~ 16:00	350	1.045780485	1.169354491	1.681888956	0.03393122	0.000578573	0.030725485	0.054672007	0.030725485	0.000526354	0.031251839	0.005294835	
L097	LD		400	15:00 ~ 16:00	400	0.985799868	0.116597362	1.102397229	1.774131957	0.031882785	0.000806666	0.032689541	0.052608585	0.029335812	0.000723644	0.030094596	0.048375884
L098	LD		100	15:00 ~ 16:00	100	0.543189504	0.062815778	0.606005281	0.97526854	0.019990057	0.020209463	0.02202946	0.03452891	0.018393276	0.001777725	0.020171001	0.032461999
L099	LD		600	15:00 ~ 16:00	600	0.864864265	0.070901664	0.935765929	1.50596554	0.02667516	0.001140663	0.027779223	0.047406215	0.02455272	0.000989326	0.025542118	0.041105953
L100	LD		50	15:00 ~ 16:00	50	0.723889415	0.024338502	0.748227917	1.204153116	0.022833086	0.000674966	0.023581052	0.03794993	0.021038126	0.00068946	0.021727586	0.034967073
L101	LD		200	15:00 ~ 16:00	200	0.793691069	0.053782682	0.847473751	1.363873407	0.025093807	0.001351028	0.026444834	0.04255873	0.023097463	0.001187799	0.024285262	0.0309083243
L102	LD		100	15:00 ~ 16:00	100	0.540345639	0.061479349	0.601824988	0.968541026	0.018123556	0.01864572	0.019988128	0.032167694	0.016683015	0.001634105	0.021831737	0.029478501
L103	LD		200	15:00 ~ 16:00	200	0.64435832	0.042830513	0.687188833	1.105920476	0.019266665	0.001074062	0.020340727	0.032735146	0.017746	0.000963382	0.018709382	0.030109757
L104	PD		1150	15:00 ~ 16:00	1150	0.804820236	0.078228769	0.883048999	1.421126075	0.031147347	0.000848819	0.031996167	0.051492711	0.028650805	0.000745098	0.029395903	0.047308003
L105	PD		800	15:00 ~ 16:00	800	0.901434504	0.118187864	1.019627368	1.640919061	0.035071192	0.00112815	0.036199342	0.058257048	0.032252296	0.000969718	0.033222014	0.053465516
L106	LD		50	15:00 ~ 16:00	50	0.747687132	0.048729966	0.796416792	1.2817054	0.021308727	0.000927245	0.022235971	0.035785239	0.01916873	0.000826612	0.020445342	0.032920307
L107	LD		450	15:00 ~ 16:00	450	0.747687132	0.048729966	0.796416792	1.2817054	0.021308727	0.000927245	0.022235971	0.035785239	0.01916873	0.000826612	0.020445342	0.032920307
L108	LD		300	15:00 ~ 16:00	300	0.78532093	0.060231095	0.845552025	1.360780696	0.021166203	0.000630828	0.021793051	0.035078834	0.019482361	0.000560117	0.020304278	0.032255161
L109	LD		250	15:00 ~ 16:00	250	0.852621522	0.046099489	0.898771011	1.446347672	0.024928867	0.000923381	0.025852248	0.041605056	0.023948111	0.000823105	0.02377149	0.038256312
L110	LD		250	15:00 ~ 16:00	250	0.641889509	0.087619891	0.729509399	1.174028657	0.018697645	0.002150597	0.020848241	0.033551909	0.017186278	0.001851749	0.019038028	0.030638659
L111	LD		250	15:00 ~ 16:00	250	0.808085729	0.044730303	0.852816032	1.372470953	0.02495685	0.001032018	0.025988868	0.041824925	0.02297483	0.000916082	0.03890911	0.038448599
L112	LD		500	15:00 ~ 16:00	500	0.825277746	0.067884255	0.893162001	1.437401335	0.025314423	0.000845178	0.026159601	0.042099692	0.023305325	0.0007764	0.024069325	0.038735728
L113	LD		550	15:00 ~ 16:00	550	0.786710876	0.069671354	0.856382229	1.378210177	0.024631343	0.00109361	0.021409451	0.022673935	0.000981841	0.023655776	0.038070186	
L114	LD		350	15:00 ~ 16:00	350	0.872022851	0.052229521	0.924252371	1.487436311	0.026925061	0.001300923	0.028225983	0.045425204	0.024779125	0.001148285	0.02592741	0.041726018
L115	LD		100	15:00 ~ 16:00	100	0.54448835	0.017672006	0.562120841	0.904643554	0.022330274	0.000964628	0.023294903	0.037489418	0.0205779	0.000882328	0.021460227	0.034536803
L116	LD		100	15:00 ~ 16:00	100	0.573886078	0.01493274	0.588278802	0.946740607	0.021333396	0.001059777	0.022393173	0.036038229	0.019667567	0.000979254	0.020646821	0.033227756
L117	LD		100	15:00 ~ 16:00	100	0.983350812	0.03356415	1.016914962	1.636561925	0.03241132	0.000978923	0.03320055	0.053623297	0.02977502	0.000870182	0.030642683	0.049314496
L118	PD		1350	15:00 ~ 16:00	1350	0.79572464	0.071268214	0.866692855	1.395286281	0.030793168	0.001029603	0.031822772	0.051213659	0.028323387	0.000895868	0.029221955	0.04702806
L119	PD		800	15:00 ~ 16:00	800	0.939529901	0.121567698	1.061097599	1.70766681	0.036952334	0.00135777	0.038310104	0.051653983	0.033979187	0.001159721	0.035138908	0.05655045
L120	LD		150	15:00 ~ 16:00	150	0.860439802	0.089528174	0.949967976	1.528821463	0.027884066	0.001101984	0.028986051	0.046648411	0.025660008	0.000979394	0.026639302	0.042871855
L121	LD		250	15:00 ~ 16:00	250	0.710042386	0.055198194	0.765240553	1.231532195	0.01980785	0.000998082	0.020805932	0.033483818	0.018236228	0.000887354	0.019123582	0.030776346
L122	LD		500	15:00 ~ 16:00	500	0.826724353	0.05504132	0.881765673	1.419060769	0.022920057	0.000675208	0.023595265	0.037972804	0.021096652	0.000600609	0.021697342	0.0349184
L123	LD		150	15:00 ~ 16:00	150	0.87449345	0.080717146	0.913723669	1.413723669	0.02532672	0.001115382	0.026442102	0.042554333	0.023312542	0.00099113	0.024030672	0.039112871
L124	LD		50	15:00 ~ 16:00	50	0.403005102	0.027614642	0.430619747	0.693013583	0.015101841	0.001011307	0.016113148	0.025931534	0.013929332	0.000932587	0.014861918	0.023917788
L125	LD		50	15:00 ~ 16:00	50	0.448912135	0.025035991	0.474415723	0.763496199	0.016813412	0.000947626	0.017761038	0.028583549	0.015502051	0.000870993	0.016173044	0.026349794
L126	PD		800	15:00 ~ 16:00	800	0.935380272	0.119689029	1.055069302	1.69796523	0.036793668	0.00131528	0.038074648	0.061275054	0.038302529	0.001124012	0.034926541	0.056208679
L127	DD		100	15:00 ~ 16:00	100	0.3798936	0.024928101	0.404821701	0.651495757	0.0132809	0.000318133	0.020340422	0.01212214	0.000955827	0.001124012	0.034926541	0.056208679
L128	LD		300	15:00 ~ 16:00	300	0.797011547	0.062623665	0.859635212	1.383445332	0.022972689	0.000881568	0.023854256	0.038396069	0.021148539	0.000790824	0.021939363	0.03507895
L129	LD		400	15:00 ~ 16:00	400	0.832701884	0.058627378	0.891329262	1.434451834	0.02263003	0.00062883	0.02325886	0.037431414	0.020826344	0.000555783	0.023812127	0.034411112
L130	LD		400	15:00 ~ 16:00	400	0.87041616	0.064552005	0.93468165	1.504681667	0.022883025	0.00061842	0.023501445	0.037821816	0.021053808	0.000538153	0.021591961	0.034748806
L131	LD		300	15:00 ~ 16:00	300	0.797011547	0.062623665	0.859635212	1.383445332	0.022972689	0.000881568	0.023854256	0.038396069	0.021148539	0.000790824	0.021939363	0.03507895
L132	PD		1450	15:00 ~ 16:00	1450	0.806409549	0.100882887	0.907292436	1.460142009	0.029382983	0.001380536	0.030763518	0.049508961	0.0270178	0.001178196	0.028195996	0.045376945
L133	DD		950	15:00 ~ 16:00	950	0.8435368	0.050310779	0.938847579	1.438504663	0.0287374	0.000850754	0.029588154	0.0476174	0.0264249	0.000754565	0.027179465	0.043741
L134	DD		650	15:00 ~ 16:00	650	0.9202098	0.049974173	0.970183973	1.561355875	0.0324175	0.000683178	0.033100678	0.053270246	0.0298115	0.000607684	0.030419184	0.04895481
L135	LD		200	15:00 ~ 16:00	200	0.839157912	0.041181305	0.880339217	1.416765115	0.023724706	0.000704436	0.024429143	0.03314796	0.021836818	0.000624843	0.022461484	0.036148449
L136	PD		900	15:00 ~ 16:00	900	0.823310583	0.063602745	0.886913328	1.427345095	0.034277221	0.00117073	0.035447951	0.057047805	0.031526292	0.00101793	0.032546086	0.052377717
L137	PD		1600	15:00 ~ 16:00	1600	0.730327827	0.072580551	0.802908377	1.292152567	0.027843374	0.001015177	0.028858551	0.04644322	0.02560967	0.000887065	0.026496735	0.042642256
L138	LD		300	15:00 ~ 16:00	300	0.799380224	0.065903607	0.805301631	1.296004127	0.020337013	0.000821732	0.021158745	0.034051615	0.018721484	0.000735285	0.019456769	0.031312556
L139	LD		250	15:00 ~ 16:00	250	0.856164521	0.083289303	0.939453824	1.511900618	0.021456442	0.000799944	0.022166386	0.035673251	0.019736964	0.000612609	0.020349573	0.032749382
L140	LD		200	15:00 ~ 16:00	200	0.804489483	0.059373122	0.863862605	1.390248644	0.023991131	0.001025844	0.025016975	0.040260818	0.022085914	0.000921504	0.023007418	0.037026758
L141	PD		1400	15:00 ~ 16:00	1400	0.770582144	0.073377078	0.843959222	1.358217335	0.030072418	0.001022139	0.031094557	0.050041714	0.027658776	0.000889201	0.028547977	0.045943402
L142	LD		300	15:00 ~ 16:00	300	0.554476072	0.066344221	0.620820293	0.999110931	0.015007063	0.0009159	0.015922963	0.025625462	0.013821514	0.00062259	0.014644104	0.023567342
L143	LD		400	15:00 ~ 16:00	400	0.702480002	0.069862835	0.772310838	1.242910723	0.018730893	0.000852141	0.019583033	0.031515799	0.017239126	0.		

Table 5: Hourly composite emission rate for each road link

L176	LD	50	15:00 ~ 16:00	250	0.816531688	0.073814871	0.890346559	1.432870331	0.025683773	0.001470612	0.027154385	0.043700638	0.023627457	0.001281297	0.024208754	0.040086655
L177	LD	50	15:00 ~ 16:00	50	0.441441071	0.314141127	0.755582198	1.215988655	0.012776809	0.008260433	0.021037242	0.038566075	0.011587569	0.0018553016	0.029585811	0.029585811
L178	LD	50	15:00 ~ 16:00	50	0.512947845	0.404967033	0.917914878	1.477237129	0.012876233	0.009969879	0.022846112	0.036767162	0.011598328	0.008328863	0.019927191	0.032069626
L179	LD	50	15:00 ~ 16:00	250	0.792624174	0.872386231	1.403966057	0.872386231	0.024643441	0.001591703	0.026235144	0.042212266	0.022667374	0.001381706	0.02404908	0.038703146
L180	PD	50	15:00 ~ 16:00	1000	0.784613725	0.077432879	0.862046604	1.387326082	0.032552449	0.001290248	0.038842697	0.054464406	0.029937587	0.001121919	0.031059506	0.049985305
L181	PD	50	15:00 ~ 16:00	1950	0.560253475	0	0.560253475	0.901638328	0.017035886	0	0.027416532	0.015665118	0	0	0.025210501	0
L182	PD	70	15:00 ~ 16:00	2950	0.531924326	0	0.531924326	0.856047095	0.015881256	0	0.015881256	0.025558341	0.014605384	0	0.023505029	0
L183	PD	750	15:00 ~ 16:00	750	0.965777128	0.073972212	0.965777128	1.554263764	0.038938925	0.001202012	0.040140937	0.064600415	0.035812585	0.001044565	0.036857151	0.059315687
L184	PD	50	15:00 ~ 16:00	950	0.904896879	0.072981405	0.977878284	1.573738637	0.035694582	0.001060615	0.036755197	0.059151609	0.032829784	0.000910636	0.033740419	0.054299806
L185	DD	50	15:00 ~ 16:00	550	0.5471718	0.069527057	0.616698857	0.992478139	0.0156104	0.001187534	0.016797934	0.020733588	0.0143463	0.001043353	0.015389653	0.024767184
L186	DD	50	15:00 ~ 16:00	550	0.5159866	0.067622073	0.583606673	0.939224782	0.0128607	0.000890421	0.013751121	0.022130229	0.0118216	0.000790761	0.012612361	0.020297578
L187	LD	50	15:00 ~ 16:00	150	0.690256305	0.058852953	0.749109258	1.205571494	0.023050465	0.001098099	0.024148564	0.038863251	0.021227086	0.000984375	0.022211461	0.035745793
L188	LD	50	15:00 ~ 16:00	150	0.721073348	0.046322009	0.767395358	1.235000045	0.0235662	0.000871036	0.024437236	0.039327821	0.021711623	0.000807866	0.022519489	0.036241515
L189	LD	50	15:00 ~ 16:00	50	0.381667836	0.088115752	0.469783588	0.756041519	0.013631846	0.002648519	0.016280365	0.026200643	0.012539191	0.002302846	0.014842037	0.023885884
L190	LD	50	15:00 ~ 16:00	50	0.526814295	0.036956441	0.563707736	0.907298797	0.017654755	0.001219957	0.0188874712	0.030375829	0.016268128	0.001095954	0.017364081	0.02794471
L191	LD	50	15:00 ~ 16:00	100	0.476694354	0.024425135	0.501119489	0.806471639	0.015864655	0.001013851	0.016878506	0.027163255	0.014635644	0.000938345	0.015539898	0.025063843
L192	LD	50	15:00 ~ 16:00	100	0.494412148	0.026584445	0.520996593	0.838460657	0.015589785	0.000942302	0.016532087	0.026065749	0.014381357	0.000870928	0.015252286	0.024546114
L193	LD	50	15:00 ~ 16:00	100	0.646417479	0.023296942	0.669714422	1.077798207	0.021312523	0.000906911	0.022138162	0.03562783	0.019565853	0.000839064	0.020404917	0.032838449
L194	LD	50	15:00 ~ 16:00	100	0.510555557	0.023453119	0.534006876	0.859398626	0.017216513	0.001012667	0.01822918	0.029336949	0.015878586	0.000937889	0.016816575	0.027063587
L195	LD	50	15:00 ~ 16:00	250	0.509980616	0.06276189	0.572742506	0.921737424	0.016848723	0.001870677	0.0187194	0.030125879	0.015510703	0.001639425	0.017150128	0.027603087
L196	LD	50	15:00 ~ 16:00	200	0.516056542	0.038437262	0.554493804	0.892369059	0.016523894	0.001215748	0.017739642	0.028549116	0.015225945	0.001092787	0.016323237	0.026268029
L001	LD	50	16:00 ~ 17:00	200	0.653834138	0.077330953	0.727143641	1.170221347	0.020624179	0.001418494	0.022042673	0.035447155	0.018986293	0.001252818	0.020239111	0.032571611
L002	LD	50	16:00 ~ 17:00	250	0.975031555	0.079026681	1.054058196	1.696338017	0.027780745	0.000324583	0.028105328	0.045231029	0.025564609	0.000289122	0.025853731	0.041607443
L003	LD	50	16:00 ~ 17:00	50	0.821519778	0.046212464	0.867734045	1.396479108	0.026434589	0.001170163	0.027604752	0.044425431	0.024345794	0.001054055	0.025399848	0.040876992
L004	DD	50	16:00 ~ 17:00	900	0.9937436	0.060001855	1.053745455	1.69583471	0.0373283	0.001280842	0.038609142	0.062135237	0.0343263	0.001120483	0.035446783	0.057045925
L005	DD	50	16:00 ~ 17:00	900	1.2216104	0.062852039	1.28462439	2.067136782	0.0454002	0.000731015	0.046131215	0.07424081	0.0417611	0.000647166	0.068249319	0.050636852
L006	LD	50	16:00 ~ 17:00	250	1.155482459	0.08547754	1.240959999	1.997126505	0.033550542	0.000727981	0.034233323	0.050593057	0.030828661	0.000635689	0.031464359	0.04610693
L007	LD	50	16:00 ~ 17:00	400	1.030659106	0.085659825	1.096318931	1.764349908	0.030579036	0.000709311	0.031288347	0.050535388	0.028144819	0.000631383	0.028776202	0.036110693
L008	LD	50	16:00 ~ 17:00	150	1.085853935	0.049416954	1.179921184	1.79921184	0.031580144	0.000808112	0.032388256	0.052123716	0.029068778	0.000715095	0.029783872	0.047932377
L009	LD	50	16:00 ~ 17:00	250	0.554147224	0.06922917	0.623763394	1.00324566	0.019782663	0.001940411	0.021723804	0.034959378	0.018213173	0.003712858	0.019926075	0.032067757
L010	LD	50	16:00 ~ 17:00	50	0.357168219	0.122577662	0.479745881	0.727074236	0.009986193	0.003075253	0.013061446	0.021020307	0.00915487	0.002637602	0.011792472	0.018978098
L011	LD	50	16:00 ~ 17:00	150	1.084678839	0.051059967	1.135738807	1.827789891	0.032471506	0.00098478	0.033456286	0.053624539	0.0298897	0.000869864	0.030757834	0.049499812
L012	LD	50	16:00 ~ 17:00	50	0.712970114	0.033056712	0.746026826	1.200610811	0.02482624	0.001284105	0.02482624	0.028265312	0.022804023	0.001158725	0.024006283	0.038662843
L013	LD	50	16:00 ~ 17:00	100	0.694055713	0.038293117	0.732348883	1.178598266	0.022227821	0.000812789	0.02304061	0.037080175	0.020471311	0.000731321	0.021202632	0.034122243
L014	DD	50	16:00 ~ 17:00	1050	1.0264312	0.06976124	1.096193249	1.764146341	0.0389377	0.001105639	0.040043399	0.064443346	0.0358096	0.00087176	0.03678136	0.059193715
L015	DD	50	16:00 ~ 17:00	400	1.7003841	0.042725339	1.743109439	2.805255745	0.0718302	0.000524325	0.072354525	0.116443031	0.0660739	0.000464994	0.066538894	0.107083073
L016	LD	50	16:00 ~ 17:00	50	0.587667987	0.052062231	0.639730218	1.02954343	0.015638952	0.005953615	0.016592566	0.02670308	0.0144400813	0.000852271	0.015253084	0.024547398
L017	LD	50	16:00 ~ 17:00	150	1.084678839	0.051059967	1.135738807	1.827789891	0.032471506	0.00098478	0.033456286	0.053624539	0.0298897	0.000869864	0.030757834	0.049499812
L018	LD	50	16:00 ~ 17:00	250	0.554147224	0.06922917	0.623763394	1.00324566	0.019782663	0.001940411	0.021723804	0.034959378	0.018213173	0.003712858	0.019926075	0.032067757
L019	LD	50	16:00 ~ 17:00	300	1.107874622	0.069047375	1.176921977	1.894067646	0.035116249	0.001666173	0.036782422	0.059195422	0.032309437	0.001445215	0.033754652	0.054322712
L020	LD	50	16:00 ~ 17:00	300	0.843698102	0.061954974	0.905653076	1.457033722	0.025319799	0.001382484	0.026702283	0.042973052	0.023303253	0.001214359	0.024517612	0.039457174
L021	DD	50	16:00 ~ 17:00	950	1.0807276	0.070779632	1.151507232	1.853166649	0.0410098	0.001019941	0.042029741	0.067640143	0.0377162	0.000894437	0.038610637	0.062137643
L022	DD	50	16:00 ~ 17:00	500	1.4395217	0.079114397	1.518636097	2.444001816	0.0500988	0.000534917	0.059597027	0.054362	0.000478604	0.005480634	0.088257177	0.088257177
L023	LD	50	16:00 ~ 17:00	250	1.091174057	0.055789348	1.146963404	1.845854085	0.032940851	0.00110509	0.034045941	0.054791495	0.030316991	0.000969541	0.031286532	0.050350667
L024	LD	50	16:00 ~ 17:00	350	0.35546207	0.068874163	0.424366234	0.682901274	0.014648524	0.002057252	0.016705776	0.026885273	0.013492395	0.001820836	0.015313231	0.024644195
L025	DD	50	16:00 ~ 17:00	150	1.2290046	0.02714529	1.25614989	2.021572264	0.0410401	0.000543125	0.041583225	0.066921548	0.0377584	0.000498647	0.038257047	0.061568597
L026	DD	50	16:00 ~ 17:00	100	1.2528876	0.02130076	1.279017676	2.058374306	0.0413246	0.000538623	0.041863223	0.067372159	0.0380204	0.000494999	0.038515399	0.061984372
L027	DD	50	16:00 ~ 17:00	400	0.8283961	0.070515935	0.898912035	1.446655094	0.0257617	0.000836483	0.026598183	0.042805521	0.0236924	0.000740428	0.024432828	0.039320727
L028	DD	50	16:00 ~ 17:00	300	0.843855	0.075709518	1.479891962	2.0262929	0.02709052	0.002708352	0.045871927	0.024877227	0.024877227	0.000697227	0.024877227	0.040035916
L029	LD	50	16:00 ~ 17:00	100	0.260332163	0.025384244	0.285716407	0.459814842	0.011520804	0.001332923	0.01285372	0.020686016	0.010647274	0.001237909	0.011885183	0.0191273
L030	DD	50	16:00 ~ 17:00	1150	0.8929859	0.081984592	0.974970492	1.569059012	0.0347242	0.00138649	0.036062849	0.058037385	0.0319271	0.001168975	0.033096075	0.053262838
L031	DD	50	16:00 ~ 17:00	900	1.2221078	0.085811214	0.306909014	0.493920952								

Table 5: Hourly composite emission rate for each road link

L062	LD		50	16:00 ~ 17:00	50	0.328925653	0.08255417	0.411479823	0.662210939	0.01964958	0.0028594	0.016824358	0.027076112	0.01285559	0.002500851	0.015356442	0.024713736
L063	LD		50	16:00 ~ 17:00	50	0.520756164	0.032319666	0.55307583	0.89087056	0.016324428	0.000625637	0.016950064	0.027278416	0.015041418	0.000570206	0.015611624	0.025124111
L064	LD		50	16:00 ~ 17:00	50	0.394358214	0.100846272	0.495204485	0.796952387	0.014231251	0.002905695	0.017136947	0.027597174	0.013083181	0.002508422	0.015591603	0.025092499
L065	DD		500	16:00 ~ 17:00	500	1.2417651	0.085199986	1.326965086	2.135557992	0.0527823	0.001104622	0.053886922	0.086722379	0.048542	0.00096495	0.040950695	0.079673515
L066	DD		500	16:00 ~ 17:00	500	1.1281589	0.090145959	1.218304659	1.960666742	0.0447238	0.000957889	0.045681789	0.073517531	0.0411317	0.000840198	0.041971888	0.067547038
L067	DD		500	16:00 ~ 17:00	500	1.2234038	0.083280024	1.306683824	2.120898545	0.0519382	0.001086753	0.053024953	0.085335178	0.0477658	0.000951481	0.048717281	0.078402668
L068	DD		950	16:00 ~ 17:00	950	0.8662328	0.094838079	0.961070879	1.54689809	0.0314179	0.001200231	0.032618131	0.052493663	0.0288984	0.001051138	0.029940538	0.048184506
L069	LD		50	16:00 ~ 17:00	400	0.470043574	0.066368872	0.536412446	0.863270006	0.013016545	0.001346277	0.014362822	0.023114664	0.011985711	0.001189759	0.013175471	0.021203812
L070	LD		350	16:00 ~ 17:00	350	0.473303526	0.065994111	0.539297637	0.86791326	0.01264228	0.001264228	0.014221368	0.022887016	0.011931854	0.00111823	0.013050084	0.021002022
L071	LD		200	16:00 ~ 17:00	200	0.48573328	0.060048734	0.545782014	0.878348827	0.011685951	0.00111416	0.012800111	0.020599731	0.010760503	0.00098615	0.011746654	0.018904359
L072	LD		200	16:00 ~ 17:00	200	0.484520211	0.051148654	0.535668866	0.862073332	0.014866066	0.001377948	0.016244014	0.026142142	0.013695832	0.001231389	0.014927222	0.024022975
L073	LD		150	16:00 ~ 17:00	150	0.351548849	0.065783143	0.417331992	0.671629069	0.011171026	0.001874764	0.013045789	0.020995111	0.010291558	0.001658089	0.011949647	0.019231045
L074	LD		150	16:00 ~ 17:00	150	0.30087016	0.0677622	0.36863236	0.593254802	0.009440959	0.001929615	0.011370574	0.01829912	0.008700483	0.001710702	0.010411135	0.016755056
L075	DD		750	16:00 ~ 17:00	750	0.4802997	0.071456214	0.551755914	0.887962862	0.0143231	0.001248268	0.015571368	0.025059625	0.013164	0.001108846	0.014272846	0.022969862
L076	DD		350	16:00 ~ 17:00	350	0.8197678	0.073743486	0.893511286	1.437963453	0.0301352	0.000995837	0.031131037	0.050100423	0.0277147	0.000890502	0.028605202	0.046035496
L077	LD		350	16:00 ~ 17:00	350	0.470652218	0.065015057	0.535667275	0.862070773	0.015272896	0.001569511	0.016842407	0.027105159	0.014066127	0.001393782	0.015459909	0.02488025
L078	DD		350	16:00 ~ 17:00	350	1.68593	0.067096824	1.753026824	2.821216188	0.0752058	0.000880097	0.076085897	0.122448078	0.0691735	0.000771527	0.069945027	0.11256533
L079	DD		850	16:00 ~ 17:00	850	0.8698694	0.097088392	0.966957792	1.556163852	0.0319478	0.001199962	0.033147762	0.053346019	0.0293763	0.001050947	0.030427247	0.048967785
L080	DD		300	16:00 ~ 17:00	300	1.8516994	0.065528208	1.917227608	3.085471079	0.08379	0.000765412	0.084555412	0.136078407	0.0770721	0.000673477	0.077745757	0.125119067
L081	DD		850	16:00 ~ 17:00	850	0.8853875	0.099535247	0.984922747	1.585075573	0.0325797	0.001242611	0.033822311	0.054431597	0.0299567	0.001084941	0.031041641	0.049956554
L082	LD		100	16:00 ~ 17:00	100	0.37586284	0.119628933	0.495491773	0.79741473	0.01307788	0.00340061	0.01647849	0.026519492	0.012004884	0.002924492	0.014929377	0.024026443
L083	LD		100	16:00 ~ 17:00	100	0.272959927	0.095940686	0.368896613	0.593680075	0.010547325	0.002963958	0.013511283	0.021744249	0.009697787	0.002583124	0.011280911	0.019764161
L084	LD		50	16:00 ~ 17:00	50	0.236518158	0.02037009	0.256755167	0.41320636	0.01524341	0.001352878	0.011772407	0.01352878	0.0011099382	0.001418155	0.025157537	0.020144972
L085	LD		50	16:00 ~ 17:00	50	0.896460711	0.006292001	0.90483361	1.456345856	0.030647304	0.000567623	0.031214927	0.050255431	0.02822012	0.000512391	0.028732511	0.046240379
L086	LD		100	16:00 ~ 17:00	100	0.332977044	0.105876517	0.43883556	0.706264589	0.01327361	0.003038645	0.016577284	0.026678349	0.01201276	0.002868209	0.015069485	0.024251925
L087	LD		600	16:00 ~ 17:00	600	0.730912798	0.085629563	0.816542361	1.314094283	0.023049857	0.001469558	0.024519414	0.034660075	0.021212101	0.001292845	0.022504946	0.026218109
L088	LD		100	16:00 ~ 17:00	100	0.738531087	0.094898658	0.828429745	1.333225126	0.022131779	0.002239644	0.024371423	0.039211906	0.020955826	0.001941138	0.022296694	0.035883396
L089	LD		200	16:00 ~ 17:00	200	0.611817803	0.059300327	0.67111813	1.080057252	0.018231185	0.001285717	0.019516903	0.01409332	0.016794414	0.001136485	0.017920899	0.028840819
L090	LD		400	16:00 ~ 17:00	400	0.894363102	0.121064479	1.015427581	1.634168223	0.029020358	0.000723586	0.029743943	0.047868118	0.026709023	0.000648039	0.022735106	0.044017157
L091	LD		100	16:00 ~ 17:00	100	0.66806819	0.050678674	0.718775493	1.156754151	0.023066196	0.001515188	0.024581384	0.039559804	0.021247411	0.001360638	0.022668049	0.026384037
L092	LD		350	16:00 ~ 17:00	350	0.942500516	0.142997712	1.085498227	1.746935717	0.030256695	0.000782668	0.031039363	0.049952889	0.027833323	0.000692297	0.02852562	0.045907422
L093	LD		150	16:00 ~ 17:00	150	0.610051854	0.078832034	0.688433888	1.107924193	0.019550328	0.001840843	0.021791171	0.035069404	0.018361298	0.001616534	0.019977833	0.032151126
L094	LD		300	16:00 ~ 17:00	300	0.798832368	0.150715246	0.949547614	1.528144957	0.026952825	0.00073059	0.027683414	0.044552026	0.024800496	0.000666745	0.025467241	0.040985449
L095	LD		400	16:00 ~ 17:00	400	0.515770977	0.077204974	0.592975951	0.954299916	0.016061255	0.001076951	0.017138206	0.0275812	0.014798479	0.000986487	0.015784966	0.025403377
L096	LD		350	16:00 ~ 17:00	350	0.982066616	0.132299717	1.114366332	1.793394314	0.031821581	0.000682994	0.032504574	0.052310911	0.029284336	0.000624896	0.029909232	0.048134213
L097	LD		400	16:00 ~ 17:00	400	0.888548041	0.125885761	0.914433802	1.632568896	0.029322647	0.001005158	0.030327804	0.048807749	0.026983756	0.000901922	0.027885674	0.044877537
L098	LD		50	16:00 ~ 17:00	50	0.451943291	0.069860311	0.521803602	0.839759408	0.01789288	0.002342464	0.020235343	0.032565548	0.016461385	0.002024966	0.018506351	0.029783011
L099	LD		550	16:00 ~ 17:00	550	0.781594992	0.077982084	0.859577076	1.38351772	0.024791957	0.001241646	0.018186918	0.022825021	0.001114464	0.0023939485	0.03852767	0.03852767
L100	LD		50	16:00 ~ 17:00	50	0.572627375	0.023371809	0.595999184	0.959165327	0.01979382	0.000863642	0.020657462	0.033244879	0.018247095	0.000794997	0.019042092	0.030645201
L101	LD		200	16:00 ~ 17:00	200	0.75254046	0.059622568	0.814876614	1.31141353	0.02475488	0.001581881	0.02363676	0.022789878	0.001396735	0.0024186613	0.038924484	0.038924484
L102	LD		100	16:00 ~ 17:00	100	0.421268853	0.069128222	0.490397075	0.789215628	0.015420031	0.00221752	0.01763755	0.028384815	0.014198576	0.001948958	0.016147534	0.025986873
L103	LD		150	16:00 ~ 17:00	150	0.581028991	0.041524444	0.622553434	1.001900144	0.01864765	0.00122275	0.019869925	0.031977465	0.01718223	0.001101363	0.018279585	0.029418068
L104	PD		850	16:00 ~ 17:00	850	0.760351051	0.089600752	0.849951803	1.367861435	0.029986786	0.001025829	0.031012615	0.049909842	0.02757933	0.000902731	0.028482061	0.045837321
L105	PD		750	16:00 ~ 17:00	750	0.960668244	0.130723689	1.091391933	1.756420693	0.03914108	0.001663007	0.040804087	0.065667649	0.035990933	0.001422638	0.037413571	0.060211156
L106	LD		150	16:00 ~ 17:00	150	0.593653291	0.126092158	0.719745549	1.158315141	0.021139161	0.002536467	0.023768428	0.038251481	0.019519769	0.002199435	0.021719204	0.034953583
L107	LD		400	16:00 ~ 17:00	400	0.777915771	0.052563944	0.830479715	1.336524225	0.022947066	0.001126973	0.024074039	0.038743314	0.021129639	0.001006162	0.022135802	0.035624031
L108	LD		300	16:00 ~ 17:00	300	0.822131955	0.063332806	0.885464761	1.425013859	0.023728628	0.000760794	0.024489422	0.039411807	0.021842296	0.000675475	0.022517771	0.03623875
L109	LD		200	16:00 ~ 17:00	200	0.821496899	0.051437881	0.872934781	1.40484886	0.024835669	0.001119229	0.025954895	0.041770251	0.022868178	0.000998572	0.03286675	0.038409715
L110	LD		200	16:00 ~ 17:00	200	0.485959295	0.106030872	0.591990167	0.952713455	0.01470899	0.002759801	0.01746879	0.028113223	0.013512268	0.002375851	0.015888118	0.025569385
L111	LD		200	16:00 ~ 17:00	200	0.786377614	0.051010344	0.837387958	1.347641936	0.025612312	0.001220352	0.026832664	0.043182879	0.023580584	0.00108184	0.024662423	0.039690225
L112	LD		450	16:00													

Table 5: Hourly composite emission rate for each road link

L144	LD	50	16:00 ~ 17:00	150	0.50920524	0.16433878	0.673543958	1.083961234	0.016822032	0.002092855	0.018914887	0.030404084	0.015461133	0.001821142	0.017282275	0.027813056
L145	LD	50	16:00 ~ 17:00	100	0.377386934	0.081748384	0.459135318	0.738904832	0.015419221	0.002792885	0.018212106	0.029309471	0.01418916	0.002437368	0.016626528	0.026757736
L146	PD	50	16:00 ~ 17:00	700	0.685160629	0.07561631	0.760776939	1.224348759	0.029904057	0.001504431	0.031408488	0.050546937	0.027501612	0.001322763	0.028824375	0.04638822
L147	PD	50	16:00 ~ 17:00	1100	0.786719666	0.076382376	0.863102042	1.38902464	0.033141857	0.001379101	0.034520958	0.055555959	0.030475832	0.00119517	0.023671002	0.050969411
L148	LD	50	16:00 ~ 17:00	250	0.36092486	0.064060423	0.424985283	0.683945816	0.009059321	0.000913261	0.009752582	0.016049276	0.008356946	0.000835112	0.009192058	0.014793146
L149	LD	50	16:00 ~ 17:00	500	0.525673853	0.102999378	0.628673231	1.011748977	0.016406706	0.001530064	0.01793677	0.028866361	0.015096815	0.001344847	0.026460224	0.046460224
L150	LD	50	16:00 ~ 17:00	250	0.586829623	0.118268369	0.705097992	1.134742402	0.018131179	0.001565472	0.019696652	0.031698609	0.016675777	0.001364691	0.018040468	0.029033247
L151	LD	50	16:00 ~ 17:00	600	0.804517949	0.079249912	0.883767861	1.422282969	0.029214817	0.001498967	0.030713784	0.049428921	0.026882165	0.001320386	0.028202524	0.04538745
L152	LD	50	16:00 ~ 17:00	550	0.855913904	0.075508947	0.93142285	1.498976049	0.031663207	0.00143278	0.033095987	0.053262696	0.029135784	0.001262352	0.030398136	0.048920936
L153	LD	50	16:00 ~ 17:00	250	0.50937923	0.093445209	0.602824639	0.970149804	0.016081014	0.001837927	0.017918941	0.028837669	0.014793335	0.001606144	0.016399479	0.026392337
L154	LD	50	16:00 ~ 17:00	250	0.495084967	0.120167043	0.61525201	0.99014967	0.012910385	0.001903234	0.01481362	0.02384015	0.011866661	0.001651365	0.013518026	0.021755099
L155	LD	50	16:00 ~ 17:00	250	0.476355546	0.083411719	0.559767265	0.900855851	0.014873804	0.001743442	0.016617246	0.026742799	0.013688153	0.001531129	0.015219282	0.024492999
L156	LD	50	16:00 ~ 17:00	250	0.432789712	0.114405897	0.547195609	0.880623781	0.010312766	0.001637003	0.011949769	0.019231242	0.009481977	0.001428889	0.010910866	0.017559292
L157	LD	50	16:00 ~ 17:00	100	0.521476135	0.078980676	0.60045676	0.966339082	0.014106083	0.001817668	0.015923764	0.02562675	0.012974024	0.001577132	0.014551156	0.023417757
L158	LD	50	16:00 ~ 17:00	100	0.613024105	0.104181997	0.717206102	1.154228469	0.018312065	0.001457155	0.01976922	0.031815397	0.016835762	0.001252587	0.01808849	0.029110304
L159	PD	50	16:00 ~ 17:00	700	0.685160629	0.07561631	0.760776939	1.224348759	0.029904057	0.001504431	0.031408488	0.050546937	0.027501612	0.001322763	0.028824375	0.04638822
L160	PD	50	16:00 ~ 17:00	700	0.705822254	0.079958474	0.785780728	1.264588356	0.03015124	0.001499632	0.032550873	0.052385421	0.028556188	0.001315912	0.0298721	0.048074366
L161	PD	50	16:00 ~ 17:00	700	0.624644492	0.081305815	0.100595307	0.39497372	0.001324383	0.0040821754	0.065696082	0.036319131	0.00134233	0.037453364	0.060275196	0.019195104
L162	LD	50	16:00 ~ 17:00	400	0.434279339	0.072315918	0.506952527	0.815284011	0.011667762	0.001333145	0.031000907	0.02092288	0.010744595	0.00118272	0.011927314	0.019195104
L163	LD	50	16:00 ~ 17:00	400	0.491751634	0.090170907	0.582462542	0.93780267	0.014881934	0.001637952	0.016519886	0.026586113	0.013695098	0.001438907	0.015133405	0.024354794
L164	LD	50	16:00 ~ 17:00	50	0.43626524	0.032138095	0.468403395	0.753820223	0.013370488	0.00095938	0.014325868	0.023055192	0.012334068	0.000885732	0.013320204	0.021276022
L165	LD	50	16:00 ~ 17:00	50	0.490979203	0.02050192	0.511029395	0.822420347	0.01180429	0.000224491	0.020226215	0.017378705	0.001091383	0.018471888	0.039727548	0.018471888
L166	LD	50	16:00 ~ 17:00	400	0.488756758	0.08191811	0.576948569	0.918506411	0.01480218	0.001640873	0.016408773	0.013622433	0.001413063	0.015035496	0.024197225	0.024197225
L167	LD	50	16:00 ~ 17:00	400	0.432426621	0.072742957	0.50489378	0.812962702	0.011535951	0.001340751	0.012876452	0.020722412	0.010623729	0.001190183	0.011813912	0.019012601
L168	DD	50	16:00 ~ 17:00	100	0.625697	0.057655663	0.320272563	0.515351807	0.00948228	0.001962711	0.011445511	0.018419718	0.0087048	0.001744093	0.010448393	0.016815017
L169	DD	50	16:00 ~ 17:00	250	0.5751957	0.063884946	0.639080646	1.028498047	0.0184131	0.001473057	0.019886157	0.032003587	0.0169211	0.001289199	0.018210299	0.029306563
L170	LD	50	16:00 ~ 17:00	50	0.692845259	0.047901456	0.740746715	1.192113318	0.02081445	0.001091387	0.021905837	0.03525394	0.019170013	0.00098031	0.002150323	0.03242872
L171	LD	50	16:00 ~ 17:00	100	0.46315949	0.109227337	0.572386828	0.9211165017	0.01427606	0.001272204	0.015548264	0.025022444	0.01341793	0.00113436	0.014276153	0.022975185
L172	LD	50	16:00 ~ 17:00	50	0.441664218	0.061515177	0.503179396	0.809786729	0.015620337	0.001938256	0.028257746	0.014384024	0.001701645	0.001701645	0.02588731	0.02588731
L173	LD	50	16:00 ~ 17:00	450	0.605127316	0.105159802	0.710287118	1.14309347	0.016984207	0.001490069	0.018474277	0.029731392	0.015623753	0.001304631	0.016928204	0.027243235
L174	LD	50	16:00 ~ 17:00	550	0.643859375	0.069519901	0.713369574	1.14805419	0.019125964	0.001450341	0.020576305	0.033114271	0.017604169	0.001277778	0.018881947	0.030387473
L175	LD	50	16:00 ~ 17:00	300	0.617010252	0.052968437	0.669978689	1.078223503	0.017514053	0.001146816	0.018660689	0.030031683	0.016128761	0.001021899	0.01715066	0.027601244
L176	LD	50	16:00 ~ 17:00	250	0.689012764	0.086318496	0.775312161	1.247771611	0.01718137	0.001730655	0.023448792	0.037373079	0.01980573	0.001513095	0.021493668	0.03459062
L177	LD	50	16:00 ~ 17:00	50	0.24273185	0.341457746	0.584189596	0.940159685	0.0069597	0.008978732	0.015938432	0.025650356	0.00622365	0.007571138	0.013794788	0.022200504
L178	LD	50	16:00 ~ 17:00	100	0.483246269	0.439875946	0.923122215	1.485617506	0.012466472	0.011008402	0.023474874	0.037779054	0.01199635	0.00920188	0.020401515	0.032832974
L179	LD	50	16:00 ~ 17:00	300	0.679346472	0.0936848	0.773031272	1.244070147	0.021254273	0.00188652	0.023140793	0.037241403	0.019550684	0.00164286	0.012193544	0.034017618
L180	PD	50	16:00 ~ 17:00	850	0.620989876	0.095282379	1.15272559	0.025989601	0.001724746	0.004601807	0.027714347	0.02389813	0.001506013	0.025404143	0.040883903	0.040883903
L181	PD	70	16:00 ~ 17:00	1550	0.411885662	0	0.411885662	0.662864072	0.013246611	0	0.013246611	0.0213183	0.0012175253	0	0.012175253	0.019594121
L182	PD	70	16:00 ~ 17:00	2550	0.431517786	0	0.431517786	0.694458833	0.013508846	0	0.013508846	0.021740325	0.0012413704	0	0.012413704	0.019977871
L183	PD	50	16:00 ~ 17:00	600	0.733579115	0.089734845	0.82331396	1.324992088	0.033827408	0.001646271	0.035473679	0.05708921	0.031108522	0.001439748	0.02534827	0.052381233
L184	PD	50	16:00 ~ 17:00	900	0.850258401	0.078660407	0.928918808	1.494946194	0.035158137	0.001316785	0.036474922	0.058700551	0.032327986	0.001129716	0.033457702	0.053844818
L185	DD	50	16:00 ~ 17:00	500	0.4612536	0.077770376	0.539023976	0.867472846	0.0134136	0.001406821	0.014820421	0.023851096	0.0123238	0.001235619	0.013559419	0.021821751
L186	DD	50	16:00 ~ 17:00	350	0.4093992	0.097289793	0.506688993	0.815434864	0.0127865	0.001858265	0.014644765	0.023568406	0.0117398	0.001626635	0.013366435	0.021511338
L187	LD	50	16:00 ~ 17:00	100	0.549131975	0.074701196	0.623833171	1.003959675	0.020070681	0.001496937	0.021567618	0.034709631	0.018484561	0.001341889	0.01982465	0.031907499
L188	LD	50	16:00 ~ 17:00	100	0.604597214	0.058910007	0.663507221	1.06780871	0.01026146	0.00126146	0.023413976	0.037680083	0.02063239	0.000952049	0.021584438	0.03076607
L189	LD	50	16:00 ~ 17:00	50	0.300422781	0.08585209	0.38630799	0.621700901	0.01277807	0.002792885	0.015570955	0.025058961	0.011760948	0.002437368	0.014198317	0.02284919
L190	LD	50	16:00 ~ 17:00	50	0.368142721	0.072447932	0.440590654	0.709060162	0.015207761	0.002442337	0.017649998	0.028404848	0.013996482	0.0021409	0.016136672	0.025969392
L191	LD	50	16:00 ~ 17:00	100	0.31347753	0.026000004	0.339477534	0.546334774	0.012472344	0.001246162	0.013718505	0.022077739	0.011520464	0.001155566	0.01267603	0.020400042
L192	LD	50	16:00 ~ 17:00	100	0.347356327	0.027444457	0.37400784	0.603181894	0.012387742	0.001120475	0.013508218	0.021739315	0.0114407	0.001037704	0.012478404	0.020081994
L193	LD	50	16:00 ~ 17:00	100	0.373253144	0.024760173	0.398013317	0.640538752	0.014767247	0.001113914	0.015881161	0.025558188	0.01362618	0.0010297	0.01465588	0.023586294
L194	LD	50	16:00 ~ 17:00	100	0.329763219	0.02509641	0.354859629	0.571089795	0.01301872	0.001244428	0.0142632	0.022954338	0.012022828	0.001154699	0.013177528	0.021207122
L195	LD	50	16:00 ~ 17:00	200	0.359850087	0.069141509	0.									

Table 5: Hourly composite emission rate for each road link

L030	DD	50	17:00 ~ 18:00	1450	0.7553979	0.085262267	0.840660167	1.352908034	0.0288047	0.001610422	0.030415122	0.048948272	0.0264794	0.001408511	0.027887911	0.044881131
L031	DD	50	17:00 ~ 18:00	1150	0.1986503	0.087052522	0.285702822	0.45979298	0.0041139	0.001382303	0.005496203	0.008845259	0.0037684	0.001207969	0.004976369	0.00800867
L032	DD	50	17:00 ~ 18:00	550	1.037464	0.107765128	1.145229128	1.843063044	0.0428115	0.001486182	0.004297682	0.017290032	0.039367	0.00130094	0.04066794	0.065448542
L033	LD	50	17:00 ~ 18:00	300	0.376498092	0.076778139	0.453276232	0.72947557	0.014430589	0.002496909	0.016927497	0.027242099	0.013280712	0.002188465	0.015469177	0.024895166
L034	LD	50	17:00 ~ 18:00	300	0.362762795	0.065234732	0.427997526	0.688793539	0.013769467	0.002189458	0.015958924	0.025683335	0.01268258	0.001934295	0.014616875	0.023523521
L035	LD	50	17:00 ~ 18:00	50	0.467357745	0.020163331	0.487521076	0.784587169	0.019835259	0.001521507	0.021356767	0.034370299	0.018299096	0.001416265	0.019715361	0.03172872
L036	LD	50	17:00 ~ 18:00	50	0.236349955	0.021959937	0.258345892	0.415766378	0.013264004	0.001663039	0.021492704	0.020422688	0.012260853	0.001548899	0.013809752	0.022224587
L037	LD	50	17:00 ~ 18:00	300	0.328517632	0.067800046	0.396317678	0.637808992	0.012718023	0.002277397	0.01499542	0.024132729	0.011714416	0.002011561	0.013725977	0.022089764
L038	LD	50	17:00 ~ 18:00	350	0.33389761	0.061873827	0.395771437	0.636930804	0.012858166	0.002124562	0.014982728	0.024112304	0.011848579	0.001884191	0.01373277	0.022100695
L039	LD	50	17:00 ~ 18:00	50	0.309041862	0.053294559	0.36233642	0.583122495	0.011330238	0.00181301	0.013143249	0.021151956	0.010454907	0.001622012	0.012076919	0.019435869
L040	LD	50	17:00 ~ 18:00	50	0.335367678	0.026700961	0.36206864	0.582691545	0.013476881	0.001277369	0.014754249	0.023744603	0.012444485	0.001186518	0.013631003	0.021936919
L041	LD	50	17:00 ~ 18:00	100	0.332286298	0.050602354	0.382888652	0.516198023	0.0130328	0.001871948	0.014975228	0.024100233	0.012088336	0.001675803	0.013759149	0.022143149
L042	LD	50	17:00 ~ 18:00	150	0.310545506	0.026640341	0.337185847	0.542646672	0.012832281	0.001269116	0.014010398	0.022693943	0.011851134	0.001177827	0.013028962	0.020968029
L043	LD	50	17:00 ~ 18:00	300	0.751201179	0.069108858	0.820310037	1.320157756	0.02363471	0.001624752	0.023988223	0.038605207	0.020579885	0.00142651	0.022006394	0.03541577
L044	LD	50	17:00 ~ 18:00	450	0.610661251	0.082789757	0.693451007	1.11598844	0.02794999	0.002003424	0.0139727	0.035904439	0.019039758	0.001391792	0.02043155	0.032881311
L045	LD	50	17:00 ~ 18:00	200	0.313967263	0.05405206	0.368019323	0.592268217	0.01704913	0.001708413	0.014708413	0.023670838	0.011713562	0.001791485	0.013505047	0.021734212
L046	LD	50	17:00 ~ 18:00	250	0.334201402	0.027405834	0.361607237	0.58194899	0.012745919	0.001226781	0.013355104	0.022466825	0.011771938	0.001138933	0.012910871	0.020777981
L047	DD	50	17:00 ~ 18:00	1350	0.7704551	0.085371676	0.855826776	1.377316263	0.0297677	0.001587404	0.031355104	0.022466825	0.011771938	0.001138933	0.012910871	0.020777981
L048	DD	50	17:00 ~ 18:00	500	1.1433654	0.115121993	1.258487393	2.025334101	0.0481108	0.001383752	0.049494552	0.079653562	0.0273662	0.001390102	0.028756302	0.046278667
L049	DD	50	17:00 ~ 18:00	700	0.4691441	0.061098314	0.530242414	0.853340327	0.0138309	0.001455134	0.015286034	0.024600427	0.0127115	0.00121058	0.04545358	0.073150264
L050	DD	50	17:00 ~ 18:00	600	0.4868525	0.077308363	0.564160863	0.907926644	0.0186222	0.001781666	0.020403866	0.028367558	0.0171141	0.001579247	0.01869347	0.030383951
L051	DD	50	17:00 ~ 18:00	650	0.4996931	0.06533076	0.56502386	0.9093155	0.0144919	0.001497245	0.025730002	0.0133188	0.001305739	0.014624539	0.023535856	0.032444213
L052	DD	50	17:00 ~ 18:00	500	0.5281244	0.076838912	0.604963312	0.973591657	0.0203849	0.001598535	0.021983435	0.035378821	0.018738	0.00142195	0.02015995	0.032444213
L053	DD	50	17:00 ~ 18:00	800	0.4234132	0.07206332	0.49547652	0.797390182	0.0133765	0.001509712	0.014886212	0.023959977	0.0122993	0.00134952	0.013638252	0.021948585
L054	DD	50	17:00 ~ 18:00	500	0.6209445	0.076590357	0.697534857	1.122570747	0.0226882	0.001433656	0.024121856	0.038820267	0.0208603	0.001275091	0.022135991	0.035623771
L055	LD	50	17:00 ~ 18:00	150	0.363772199	0.057468202	0.421240401	0.677919026	0.01312166	0.001931148	0.015052809	0.024225087	0.012092518	0.001722486	0.013815004	0.022330338
L056	LD	50	17:00 ~ 18:00	100	0.263337346	0.052565814	0.315903159	0.50839559	0.008976124	0.001598127	0.010574251	0.017017565	0.008283748	0.001438058	0.009721806	0.015645692
L057	DD	50	17:00 ~ 18:00	850	0.4251635	0.07162332	0.496786822	0.799498904	0.0134637	0.001496673	0.024086558	0.0123731	0.001338057	0.013711157	0.022065913	0.03545598
L058	DD	50	17:00 ~ 18:00	550	0.6187436	0.07518995	0.69393355	1.116775019	0.0225758	0.001478388	0.024054188	0.038711367	0.0207556	0.001312644	0.022068244	0.035515308
L059	LD	50	17:00 ~ 18:00	150	0.893795957	0.040732515	0.934528472	1.503974051	0.031764173	0.003099917	0.03526902	0.029249558	0.001207296	0.0030456854	0.049015433	0.072335851
L060	LD	50	17:00 ~ 18:00	200	0.344700437	0.051392119	0.396092555	0.637447593	0.015589305	0.002124049	0.017173353	0.028506808	0.014371913	0.001904191	0.016276103	0.026193784
L061	LD	50	17:00 ~ 18:00	100	0.231357234	0.05298991	0.284347143	0.457611232	0.01311026	0.002315357	0.014626382	0.023538822	0.011357449	0.002075321	0.01343277	0.021617894
L062	LD	50	17:00 ~ 18:00	50	0.324151457	0.070581111	0.394732567	0.63525891	0.01360189	0.002366115	0.016238005	0.026132472	0.012525677	0.002326861	0.014852538	0.023902784
L063	LD	50	17:00 ~ 18:00	50	0.41669655	0.030321755	0.447018305	0.719404438	0.005093033	0.015477958	0.024909297	0.01344225	0.000831695	0.02297163	0.02297163	0.02297163
L064	LD	50	17:00 ~ 18:00	100	0.327051874	0.107800996	0.43485287	0.699826118	0.012152299	0.003217434	0.015369733	0.024735127	0.011163143	0.002786378	0.013949521	0.022449521
L065	DD	50	17:00 ~ 18:00	650	1.0677668	0.085878521	1.153645321	1.85607561	0.045765	0.001325331	0.045901831	0.073871653	0.0409916	0.001161429	0.042153029	0.067838556
L066	DD	50	17:00 ~ 18:00	600	0.9570836	0.089164039	1.046247639	1.683768176	0.0376802	0.001198249	0.038878449	0.062568644	0.0346505	0.001055	0.0357055	0.057462289
L067	DD	50	17:00 ~ 18:00	650	1.0538927	0.084769396	1.138662096	1.832494457	0.0439647	0.00133366	0.04529836	0.072900463	0.0404291	0.00116919	0.04159829	0.066945792
L068	DD	50	17:00 ~ 18:00	1150	0.724205	0.094079724	0.818284724	1.316898338	0.0258855	0.001442904	0.027328404	0.043980694	0.0237977	0.001266016	0.025063716	0.040336041
L069	LD	50	17:00 ~ 18:00	500	0.43864401	0.068614922	0.507258933	0.81635209	0.011983416	0.00153925	0.013522665	0.021762566	0.011034789	0.001361198	0.012395987	0.019944538
L070	LD	50	17:00 ~ 18:00	450	0.439037219	0.06591358	0.504950799	0.812637519	0.011805828	0.001421704	0.013227532	0.021287597	0.010873313	0.00126147	0.012134783	0.019528992
L071	LD	50	17:00 ~ 18:00	300	0.466953708	0.063618007	0.530571715	0.853870284	0.011164929	0.00130579	0.012470719	0.020069627	0.010281066	0.0011558	0.011436866	0.018405805
L072	LD	50	17:00 ~ 18:00	250	0.427276075	0.05521469	0.482490765	0.776491687	0.013155004	0.001574306	0.01472931	0.023704467	0.012122035	0.001406693	0.013528728	0.021773222
L073	LD	50	17:00 ~ 18:00	200	0.375957566	0.066197006	0.442154571	0.711577037	0.012240203	0.001790355	0.012240203	0.02286962	0.011273654	0.001746265	0.013019199	0.020935476
L074	LD	50	17:00 ~ 18:00	200	0.30099648	0.067837279	0.368836928	0.593584021	0.009873086	0.002021934	0.01189502	0.020914311	0.009099047	0.001795077	0.010894124	0.01753235
L075	DD	50	17:00 ~ 18:00	1000	0.388164	0.071713457	0.459877457	0.740099187	0.0113102	0.00146415	0.01277435	0.02058272	0.0103918	0.001306063	0.011699543	0.018821879
L076	DD	50	17:00 ~ 18:00	450	0.640221	0.071372026	0.713791026	1.14873245	0.0236447	0.001259542	0.024904242	0.040079933	0.0217425	0.001127333	0.022868933	0.036805338
L077	DD	50	17:00 ~ 18:00	450	0.430584398	0.066150599	0.49733557	0.79941608	0.014178913	0.00179058	0.015957971	0.02681802	0.013059607	0.001580455	0.014640062	0.023560837
L078	DD	50	17:00 ~ 18:00	450	1.4582722	0.070571251	1.528843451	2.46042892	0.064501	0.001136778	0.065637778	0.105633501	0.0593237	0.000997056	0.060320756	0.097076605
L079	DD	50	17:00 ~ 18:00	1100	0.7287855	0.069928362	0.825713862	1.320854347	0.0263125	0.001453206	0.027765706	0.046848461	0.0241899	0.001273652	0.025463542	0.040979497
L080	DD	50	17:00 ~ 18:00	400	1.6210264	0.066183239	1.687209361	2.715293961	0.026921	0.0072658	0.00935342	0.073599142	0.118446044	0.0668356	0.000826003	0.067616503
L081	DD	50	17:00 ~ 18:00	1050	0.7437605	0.099050635	0.842811335	1.356369671	0.028413411	0						

Table 5: Hourly composite emission rate for each road link

L112	LD	50	17:00 ~ 18:00	550	0.671091048	0.070619662	0.74171071	1.193664713	0.021783803	0.001150338	0.022934141	0.03690889	0.020063227	0.001043036	0.021106263	0.033967153
L113	LD	50	17:00 ~ 18:00	650	0.623153833	0.075459666	0.698613499	1.124306648	0.020791529	0.001460414	0.022251942	0.05810941	0.019145382	0.001306292	0.020451674	0.032913697
L114	LD	50	17:00 ~ 18:00	350	0.641034146	0.059932492	0.700966638	1.12809365	0.020986093	0.001765813	0.022751906	0.036615552	0.019319364	0.00156524	0.020884605	0.033610403
L115	LD	50	17:00 ~ 18:00	100	0.38488358	0.020194227	0.378488358	0.609116453	0.016876526	0.00128341	0.020519677	0.029225511	0.015571796	0.001185761	0.016757557	0.026968607
L116	LD	50	17:00 ~ 18:00	100	0.371977065	0.018112026	0.390089091	0.627785978	0.016153197	0.001353802	0.017506999	0.028174714	0.014911078	0.001256613	0.016167691	0.026201913
L117	LD	50	17:00 ~ 18:00	100	0.804279637	0.042673242	0.846951979	1.363033697	0.027308512	0.001354455	0.028662967	0.04612846	0.025144806	0.00120772	0.026352526	0.042410174
L118	PD	50	17:00 ~ 18:00	1250	0.755234831	0.083130705	0.838365536	1.349215192	0.028201873	0.001484569	0.029686441	0.047775577	0.02594325	0.001300237	0.027243487	0.043844403
L119	PD	50	17:00 ~ 18:00	850	1.055844702	0.15547428	1.211318983	1.949424091	0.042137601	0.002795046	0.044932647	0.072311906	0.038727562	0.001424821	0.041100787	0.066145141
L120	LD	50	17:00 ~ 18:00	150	0.653918695	0.090389664	0.744307658	1.197844087	0.022267176	0.00161214	0.023879316	0.038429398	0.0204987	0.001435742	0.021934441	0.035299974
L121	LD	50	17:00 ~ 18:00	300	0.542235804	0.062434461	0.604670266	0.973120045	0.015807658	0.001276464	0.017274644	0.027784682	0.014558513	0.001297163	0.015855676	0.025517173
L122	LD	50	17:00 ~ 18:00	500	0.70944603	0.06159453	0.77104056	1.240866414	0.019964617	0.000983974	0.020948591	0.033713406	0.018379786	0.000877713	0.019257499	0.030991864
L123	LD	50	17:00 ~ 18:00	150	0.634370893	0.088119738	0.722490632	1.162733073	0.021370455	0.001606687	0.022971142	0.019673352	0.019673352	0.001424821	0.021098172	0.033954133
L124	LD	50	17:00 ~ 18:00	50	0.252501433	0.027324888	0.279826321	0.450335691	0.011538854	0.001319128	0.012857891	0.020692863	0.010662369	0.001223927	0.011886296	0.019129092
L125	LD	50	17:00 ~ 18:00	50	0.329608258	0.024810944	0.354419202	0.570380998	0.014610863	0.001222188	0.015833052	0.025480763	0.013485944	0.001130592	0.014616536	0.023522976
L126	PD	50	17:00 ~ 18:00	850	1.057491845	0.1553975	1.212889345	1.951951339	0.042200918	0.002795213	0.044966131	0.072414073	0.038785786	0.002373336	0.041159122	0.066239021
L127	DD	50	17:00 ~ 18:00	100	0.2339885	0.026164645	0.260153145	0.418674862	0.0094928	0.001345301	0.010838101	0.017442189	0.0087302	0.001250946	0.009981146	0.016063057
L128	LD	50	17:00 ~ 18:00	350	0.596775327	0.067280355	0.664055682	1.068691371	0.017985635	0.00128401	0.019269645	0.03101141	0.01656579	0.001153537	0.01719326	0.028516421
L129	LD	50	17:00 ~ 18:00	400	0.725817554	0.067705502	0.793523057	1.277048396	0.020115641	0.001014083	0.021129724	0.03400491	0.018514438	0.000896736	0.019411174	0.031239179
L130	LD	50	17:00 ~ 18:00	400	0.784994705	0.07430365	0.859298355	1.382930215	0.020382664	0.000939262	0.021321925	0.034314227	0.018752553	0.000818934	0.019571486	0.031497176
L131	LD	50	17:00 ~ 18:00	350	0.596775327	0.067280355	0.664055682	1.068691371	0.017985635	0.00128401	0.019269645	0.03101141	0.01656579	0.001153537	0.01719326	0.028516421
L132	PD	50	17:00 ~ 18:00	1500	0.910640267	0.143636212	1.054276549	1.696689308	0.033789238	0.002922759	0.036711996	0.059082084	0.031047861	0.002481741	0.033529602	0.03960305
L133	DD	50	17:00 ~ 18:00	1000	0.7049619	0.055752684	0.760714848	1.224248409	0.0247984	0.001199994	0.025988394	0.041824162	0.0227914	0.001069269	0.023860669	0.038999929
L134	DD	50	17:00 ~ 18:00	650	0.818352	0.055747128	0.874999128	1.406722691	0.0300116	0.001005445	0.031017045	0.049916971	0.0275976	0.000897596	0.024895196	0.045845848
L135	LD	50	17:00 ~ 18:00	150	0.78856834	0.040562335	0.829130675	1.334353161	0.020389819	0.000527362	0.020917181	0.033662855	0.018770476	0.000474789	0.019245265	0.030972175
L136	PD	50	17:00 ~ 18:00	950	0.68372468	0.078041608	0.761768408	1.225944369	0.028039253	0.001769423	0.029799676	0.047957811	0.025795603	0.001548951	0.027344555	0.044060685
L137	PD	50	17:00 ~ 18:00	1700	0.761307533	0.086597859	0.84790539	1.364560606	0.029879089	0.001498675	0.031724664	0.051058989	0.027479967	0.001610961	0.029090529	0.046816552
L138	LD	50	17:00 ~ 18:00	350	0.574862721	0.069142182	0.644004903	1.036422851	0.015472257	0.001101161	0.016573417	0.024672263	0.014249831	0.000987846	0.015237677	0.024522603
L139	LD	50	17:00 ~ 18:00	250	0.769486995	0.09255931	0.862046305	1.387325601	0.018923541	0.001086381	0.020009922	0.032202768	0.017402832	0.000939539	0.018342371	0.029519113
L140	LD	50	17:00 ~ 18:00	250	0.488134195	0.074569105	0.57277033	0.921674328	0.018398287	0.002178028	0.020576315	0.033114287	0.016942605	0.001933175	0.01887578	0.030937547
L141	PD	50	17:00 ~ 18:00	1500	0.812340562	0.086997529	0.899338091	1.447340763	0.032162609	0.00180918	0.039971789	0.054672159	0.029580593	0.001570816	0.031151409	0.050133209
L142	LD	50	17:00 ~ 18:00	350	0.345373905	0.00563644	0.401877546	0.646757613	0.008613352	0.0006475959	0.01525004	0.007953668	0.000801969	0.000755636	0.014090796	0.024509796
L143	LD	50	17:00 ~ 18:00	450	0.48334734	0.0794893	0.56286364	0.90583897	0.014507851	0.001579465	0.016087317	0.025889692	0.013356878	0.001397983	0.014754861	0.023745588
L144	LD	50	17:00 ~ 18:00	200	0.483269364	0.152023599	0.635292962	1.022402376	0.016213617	0.018564891	0.029877221	0.014903008	0.020246659	0.016948697	0.027277777	0.044881259
L145	LD	50	17:00 ~ 18:00	100	0.369750605	0.082436353	0.452194258	0.727734308	0.014715262	0.00285515	0.017570411	0.028276766	0.013539947	0.002498018	0.016037964	0.025810537
L146	PD	50	17:00 ~ 18:00	900	0.694906859	0.078212049	0.773127907	1.244225666	0.028633423	0.00175651	0.030389933	0.048097734	0.026342197	0.001545793	0.027887921	0.044881259
L147	PD	50	17:00 ~ 18:00	1300	0.852932902	0.083470251	0.936403154	1.506991052	0.033657935	0.00176691	0.035374625	0.0569298	0.030956872	0.001490594	0.032447465	0.052219004
L148	LD	50	17:00 ~ 18:00	350	0.340500899	0.060917435	0.401418334	0.646018581	0.008778514	0.00102684	0.009805354	0.015780149	0.008102429	0.000941368	0.009403797	0.014554545
L149	LD	50	17:00 ~ 18:00	650	0.472451329	0.100166073	0.572617401	0.921536089	0.014724231	0.001759507	0.016483738	0.026527939	0.013549964	0.001548675	0.015098639	0.024298844
L150	LD	50	17:00 ~ 18:00	300	0.528109982	0.114704285	0.642814268	1.034506714	0.016112684	0.017877133	0.028931319	0.014819138	0.001628143	0.001647326	0.024636034	0.039479418
L151	LD	50	17:00 ~ 18:00	800	0.694855255	0.08020049	0.75055744	1.247328212	0.02500235	0.001728104	0.026730454	0.04301839	0.023007694	0.00152374	0.024531434	0.039479418
L152	LD	50	17:00 ~ 18:00	700	0.84281758	0.076597524	0.821419282	1.321942907	0.027199566	0.01663797	0.028857752	0.046441935	0.025024967	0.026492869	0.024636034	0.039479418
L153	LD	50	17:00 ~ 18:00	350	0.41807872	0.092281616	0.573360337	0.922731724	0.015099728	0.002025149	0.017124878	0.02759575	0.01389095	0.001772959	0.01566391	0.025208557
L154	LD	50	17:00 ~ 18:00	300	0.434746192	0.116640499	0.55138669	0.887368656	0.011353451	0.001029291	0.013462742	0.021666129	0.010433193	0.012266107	0.019740337	0.025208557
L155	LD	50	17:00 ~ 18:00	350	0.456476909	0.083060662	0.539562971	0.868340271	0.014215591	0.001942479	0.016158071	0.026030829	0.013081725	0.001707468	0.014789194	0.023800841
L156	LD	50	17:00 ~ 18:00	300	0.40206531	0.11308437	0.515149679	0.829050985	0.009734548	0.01872755	0.011607302	0.018680096	0.008949399	0.001630084	0.010582484	0.023800841
L157	LD	50	17:00 ~ 18:00	100	0.505558933	0.079498834	0.585058767	0.941558476	0.013492271	0.001917453	0.015409724	0.024799485	0.012407946	0.001672356	0.014080302	0.022592158
L158	LD	50	17:00 ~ 18:00	100	0.548003262	0.10228671	0.650285932	1.046531162	0.015611297	0.00157214	0.017183437	0.027653992	0.01435312	0.001362737	0.015715857	0.022592158
L159	PD	50	17:00 ~ 18:00	900	0.694906859	0.078212049	0.773127907	1.244225666	0.028633423	0.00175651	0.030389933	0.048097734	0.026342197	0.001545793	0.027887991	0.044881259
L160	PD	50	17:00 ~ 18:00	850	0.707948488	0.081188073	0.789136562	1.26989034	0.01753622	0.010349896	0.04996839	0.026950961	0.028492544	0.001541583	0.024854519	0.045845848
L161	PD	50	17:00 ~ 18:00	800	1.033114348	0.091016788	1.124131135	1.809109201	0.040984029	0.001718051	0.04270208	0.068722165	0.03768923	0.001475311	0.039164541	0.063029063
L162	LD	50	17:00 ~ 18:00	50	0.381080858	0.073995919	0.455083976	0.732384847	0.01020522	0.0011708159	0.011708159	0.018842408	0.009401439	0.001334504	0.010735943	0.0172

Table 5: Hourly composite emission rate for each road link

L194	LD	50	17:00 ~ 18:00	150	0.314737861	0.025192403	0.339930263	0.54706337	0.01272788	0.001333009	0.014060889	0.022628751	0.01175841	0.001239369	0.012997779	0.020917846
L195	LD	300	17:00 ~ 18:00	300	0.326092688	0.071964135	0.398056823	0.640608768	0.012640419	0.002385083	0.015025502	0.024181141	0.011640717	0.002101299	0.013742017	0.022115577
L196	LD	250	17:00 ~ 18:00	250	0.340934032	0.054179534	0.395116566	0.635872067	0.012803671	0.001909329	0.014712999	0.023678218	0.011803862	0.001704648	0.012373978	0.022139786
L001	LD	250	18:00 ~ 19:00	250	0.478822064	0.06957641	0.548398474	0.882559601	0.015037399	0.001660708	0.016698447	0.026873478	0.013850295	0.001474763	0.015532507	0.024663228
L002	LD	250	18:00 ~ 19:00	250	0.789229308	0.066460496	0.855689804	1.377095829	0.020258882	0.000492025	0.020750907	0.039359265	0.018647594	0.000441419	0.019098013	0.030720712
L003	LD	50	18:00 ~ 19:00	50	0.565032513	0.044528009	0.604854521	0.973416575	0.018342533	0.00142998	0.019772513	0.031820996	0.016907415	0.001298136	0.018205551	0.029298932
L004	DD	1000	18:00 ~ 19:00	1000	0.747094	0.068411272	0.810451272	1.304231649	0.0265904	0.00158808	0.020808848	0.045203915	0.01243634	0.001393335	0.021575675	0.041451369
L005	DD	900	18:00 ~ 19:00	900	0.9498013	0.059526653	1.009327953	1.624351848	0.0389509	0.000972265	0.034923165	0.056203247	0.0312276	0.003864787	0.032092387	0.051647562
L006	LD	250	18:00 ~ 19:00	250	0.917661007	0.073045895	0.990706902	1.594384246	0.02523453	0.000932346	0.026185799	0.042141854	0.023240208	0.000821405	0.024061612	0.038723315
L007	LD	400	18:00 ~ 19:00	400	0.792369815	0.060323349	0.852693164	1.372273216	0.022709602	0.000959331	0.0238668932	0.038091359	0.020908447	0.000858732	0.021767179	0.035030791
L008	LD	150	18:00 ~ 19:00	150	0.838655057	0.056437983	0.89509304	1.440509032	0.024083705	0.001078116	0.025161821	0.040493925	0.023172913	0.000958997	0.023313191	0.037272708
L009	LD	300	18:00 ~ 19:00	300	0.373580444	0.07431919	0.447899634	0.720822797	0.014093565	0.002358601	0.016452166	0.026477128	0.01298048	0.002083629	0.01506411	0.024243274
L010	LD	50	18:00 ~ 19:00	50	0.3194528	0.109373016	0.428825816	0.690126539	0.009747988	0.002857635	0.012605623	0.020286733	0.008947388	0.00246739	0.011414778	0.018370259
L011	LD	150	18:00 ~ 19:00	150	0.841302978	0.057518646	0.898821624	1.446509592	0.024917889	0.001237839	0.026155727	0.042093458	0.02293958	0.001097384	0.024036964	0.038683648
L012	LD	50	18:00 ~ 19:00	50	0.444340255	0.03795351	0.482293765	0.776717468	0.016480601	0.001561501	0.018042102	0.029035877	0.015197467	0.001420213	0.016617681	0.026743498
L013	LD	150	18:00 ~ 19:00	150	0.51597129	0.045843158	0.561454448	0.903571102	0.015789485	0.001177856	0.016967342	0.027306221	0.014549812	0.001060381	0.015610194	0.025122209
L014	DD	1100	18:00 ~ 19:00	1100	0.7680866	0.069156075	0.837242675	1.347408126	0.0278951	0.001401124	0.029296224	0.047147586	0.0256495	0.001235529	0.026885029	0.043267153
L015	DD	400	18:00 ~ 19:00	400	1.3443384	0.045347706	1.389713106	2.23652089	0.0549018	0.000689279	0.055591079	0.089464948	0.0505008	0.000616158	0.051116958	0.082264565
L016	LD	100	18:00 ~ 19:00	100	0.482557687	0.052819725	0.535377412	0.861604285	0.012006348	0.001071433	0.013077781	0.021046596	0.011064341	0.000964311	0.010208654	0.019358194
L017	LD	150	18:00 ~ 19:00	150	0.841302978	0.057518646	0.898821624	1.446509592	0.024917889	0.001237839	0.026155727	0.042093458	0.02293958	0.001097384	0.024036964	0.038683648
L018	LD	300	18:00 ~ 19:00	300	0.373580444	0.07431919	0.447899634	0.720822797	0.014093565	0.002358601	0.016452166	0.026477128	0.01298048	0.002083629	0.01506411	0.024243274
L019	LD	300	18:00 ~ 19:00	300	0.835795777	0.082503847	0.918299624	1.477856316	0.026614168	0.0028801019	0.036450631	0.024483727	0.01901398	0.026385125	0.042462637	0.042462637
L020	LD	300	18:00 ~ 19:00	300	0.624414923	0.06383409	0.697299013	1.114494164	0.018405833	0.001673375	0.020079208	0.032314273	0.016943154	0.00147495	0.018418104	0.029640991
L021	DD	1000	18:00 ~ 19:00	1000	0.8110366	0.070316569	0.881353169	1.418396909	0.0239379	0.001348217	0.030746117	0.048408957	0.0270321	0.00118561	0.028217761	0.045411972
L022	DD	500	18:00 ~ 19:00	500	1.144544	0.061218927	1.205762927	1.940482509	0.0457499	0.000758762	0.046508662	0.07484825	0.0402802	0.00068229	0.04276449	0.068822605
L023	LD	400	18:00 ~ 19:00	400	0.836106285	0.060435891	0.896542176	1.442841185	0.024993825	0.00137642	0.026370245	0.04243869	0.023007232	0.001215755	0.024222986	0.038983021
L024	LD	300	18:00 ~ 19:00	300	0.227173576	0.069000041	0.296174417	0.476645336	0.01052635	0.002370679	0.012897029	0.020755705	0.00970479	0.002105034	0.011809824	0.019006022
L025	DD	100	18:00 ~ 19:00	100	0.9543702	0.027781837	1.580616559	0.02781837	0.000840954	0.032619	0.033459954	0.035848442	0.0300134	0.000778934	0.030792334	0.049555334
L026	DD	400	18:00 ~ 19:00	400	1.0016495	0.02705428	1.028705928	1.655537597	0.0337165	0.000797023	0.034513523	0.05543993	0.0310225	0.000738136	0.031760636	0.051113662
L027	DD	400	18:00 ~ 19:00	400	0.6580563	0.061605999	0.719662299	1.158181324	0.0196955	0.001138749	0.020834249	0.03352939	0.0181108	0.001013115	0.019123915	0.030776881
L028	DD	300	18:00 ~ 19:00	300	0.6805843	0.063969897	0.744554197	1.198240851	0.0203652	0.001089615	0.021454815	0.034528092	0.0187264	0.00096821	0.01969461	0.031695324
L029	LD	100	18:00 ~ 19:00	100	0.211725345	0.026343184	0.23806853	0.38313208	0.010163452	0.00146737	0.011630822	0.018717947	0.009403465	0.001365819	0.010769284	0.01733144
L030	DD	1200	18:00 ~ 19:00	1200	0.6614187	0.079892322	0.741311022	1.19302148	0.0244568	0.001722763	0.026179563	0.042131818	0.0224798	0.00150835	0.023988335	0.038605387
L031	DD	950	18:00 ~ 19:00	950	0.2036503	0.079276692	0.30286992	0.455325725	0.0038839	0.001472933	0.005356833	0.008620966	0.0035566	0.001290014	0.004846614	0.00779985
L032	DD	450	18:00 ~ 19:00	450	0.9295457	0.09019279	1.01973849	1.641105941	0.0397278	0.00169884	0.03967164	0.063845157	0.0349154	0.001488637	0.036404037	0.058586472
L033	LD	300	18:00 ~ 19:00	300	0.293377476	0.080297702	0.37367518	0.601370411	0.01852153	0.014476747	0.023298007	0.010911745	0.00230393	0.0013215137	0.021267649	0.021267649
L034	LD	250	18:00 ~ 19:00	250	0.279933396	0.069732328	0.349656624	0.562716391	0.01188798	0.002331922	0.01352072	0.021759436	0.01031091	0.002060848	0.012371758	0.019910364
L035	LD	50	18:00 ~ 19:00	50	0.27961437	0.021995937	0.485393531	0.4197962	0.01479962	0.015861002	0.025525744	0.013118643	0.00154889	0.014667542	0.023605062	0.023605062
L036	LD	50	18:00 ~ 19:00	50	0.24425626	0.021995937	0.266252197	0.428490311	0.013184739	0.001663039	0.014847778	0.023895124	0.012187666	0.00154889	0.013736564	0.021106803
L037	LD	250	18:00 ~ 19:00	250	0.269123553	0.069784489	0.338908042	0.545418268	0.010854653	0.002359095	0.013213748	0.021265413	0.01000272	0.002086619	0.012089388	0.019455856
L038	LD	300	18:00 ~ 19:00	300	0.269376125	0.062813391	0.337189516	0.534605876	0.010858764	0.002172898	0.013031662	0.020972375	0.010011546	0.001930971	0.011942517	0.01921957
L039	LD	50	18:00 ~ 19:00	50	0.248482165	0.030558556	0.488528927	0.609136782	0.01846268	0.01098305	0.017675461	0.008436709	0.001653753	0.010090462	0.016238984	0.016238984
L040	LD	50	18:00 ~ 19:00	50	0.270013258	0.027580425	0.297593683	0.478929418	0.01164935	0.001346718	0.012511652	0.020135502	0.010319789	0.001251891	0.01157168	0.018627727
L041	LD	100	18:00 ~ 19:00	100	0.285277137	0.051304534	0.336581671	0.541674347	0.011623577	0.00194098	0.013564557	0.021829985	0.010721988	0.001741252	0.01246325	0.020057607
L042	LD	150	18:00 ~ 19:00	150	0.253105192	0.02763718	0.280740372	0.45180671	0.010876966	0.00131346	0.012190426	0.01961854	0.010054571	0.001220149	0.01127472	0.018144587
L043	LD	250	18:00 ~ 19:00	250	0.784122877	0.072511433	0.85663431	1.378615861	0.023203593	0.001656088	0.024859682	0.04000768	0.021354959	0.001452009	0.022806605	0.036703581
L044	LD	400	18:00 ~ 19:00	400	0.532495526	0.070413671	0.602853197	0.970195764	0.017890621	0.001643235	0.019533856	0.031436616	0.016476971	0.001464174	0.017941144	0.028973401
L045	LD	200	18:00 ~ 19:00	200	0.263662231	0.055037974	0.318602005	0.512736053	0.011084302	0.002054727	0.013139029	0.021145165	0.010225592	0.001840042	0.012065634	0.019417707
L046	LD	200	18:00 ~ 19:00	200	0.262351634	0.028317323	0.291165357	0.468584056	0.001300778	0.011878582	0.019116677	0.009797194	0.001298828	0.001298828	0.017683878	0.017683878
L047	LD	1150	18:00 ~ 19:00	1150	0.6702347	0.07939242	0.749543942	1.206271048	0.0251791	0.001706609	0.026885709	0.043268247	0.0231451	0.001496474	0.024641574	0.039656671
L048	DD	400	18:00 ~ 19:00	400	1.03074932	0.093341572	1.124909772	1.809044243	0.0490051	0.001605727	0.044610827	0.071793988	0.0395454	0.001406581	0.040951981	0.065905662
L049	DD	650														

Table 5: Hourly composite emission rate for each road link

L080	DD	50	18:00 ~ 19:00	300	1.453644	0.0613172124	1.514959214	2.438084462	0.0645883	0.001099929	0.065688229	0.105714694	0.0594062	0.000970358	0.060376558	0.09716641
L081	DD	50	18:00 ~ 19:00	900	0.6581128	0.086932612	0.745045412	1.199031384	0.0229449	0.001604437	0.024549337	0.03950823	0.0210912	0.001404669	0.022495869	0.036203502
L082	LD	50	18:00 ~ 19:00	100	0.275116388	0.134854506	0.409501794	0.659027618	0.00982693	0.003855281	0.013737974	0.022109072	0.009061362	0.003319729	0.01238109	0.019925384
L083	LD	50	18:00 ~ 19:00	100	0.237540061	0.109165063	0.346705123	0.557966423	0.009469938	0.003350698	0.012820636	0.008704223	0.002913932	0.001618155	0.018697562	0.021784346
L084	LD	50	18:00 ~ 19:00	50	0.189765252	0.021116473	0.210881725	0.393980395	0.010956732	0.00159369	0.012550422	0.020197896	0.01013868	0.001483527	0.011622207	0.018704082
L085	LD	50	18:00 ~ 19:00	50	0.688906867	0.012503693	0.70141056	1.12880807	0.023154233	0.000924956	0.024079189	0.038751602	0.021340284	0.00085588	0.022196164	0.035721174
L086	LD	50	18:00 ~ 19:00	100	0.246213515	0.107598606	0.353812121	0.569403999	0.010598022	0.003440926	0.014038948	0.022593441	0.009739554	0.002998009	0.012737562	0.020490969
L087	LD	50	18:00 ~ 19:00	650	0.530494986	0.077349943	0.607844929	0.978229158	0.016597349	0.018425027	0.02652133	0.02718436	0.015279225	0.001612387	0.01689161	0.027184346
L088	LD	50	18:00 ~ 19:00	100	0.510177392	0.104561533	0.614738924	0.89892394	0.015172003	0.002680987	0.01785299	0.02873153	0.013948868	0.002321801	0.016270669	0.026185039
L089	LD	50	18:00 ~ 19:00	200	0.471893856	0.062504061	0.534397917	0.860027943	0.013514221	0.01518267	0.015032488	0.024192384	0.012446835	0.001348042	0.013794877	0.022206472
L090	LD	50	18:00 ~ 19:00	400	0.664636854	0.083492657	0.748129511	1.203994747	0.020794557	0.00108504	0.021879597	0.03521171	0.019147093	0.000975512	0.020122606	0.032384114
L091	LD	50	18:00 ~ 19:00	150	0.455390283	0.052548311	0.507938594	0.817445896	0.016420572	0.001739974	0.018160546	0.029226493	0.015137356	0.001566719	0.026882536	0.034545465
L092	LD	50	18:00 ~ 19:00	300	0.718387162	0.100792728	0.81917989	1.318338964	0.02214434	0.001225584	0.023369924	0.037610153	0.020379883	0.001085727	0.02146561	0.034545465
L093	LD	50	18:00 ~ 19:00	150	0.467526227	0.081009311	0.548535538	0.882780182	0.015416813	0.002080423	0.017497235	0.028159001	0.014192068	0.001830585	0.016022652	0.025785896
L094	LD	50	18:00 ~ 19:00	300	0.584957832	0.089779826	0.674737658	1.085882302	0.019336313	0.001049923	0.020386236	0.032808385	0.017810646	0.00096038	0.018771026	0.030208963
L095	LD	50	18:00 ~ 19:00	500	0.407013219	0.053907577	0.460920796	0.74178274	0.012773037	0.001217556	0.013990593	0.022515621	0.011782457	0.001118031	0.012900488	0.030207612
L096	LD	50	18:00 ~ 19:00	350	0.728003921	0.083097894	0.811101815	1.305338595	0.023358943	0.000974549	0.024333492	0.039160862	0.021513627	0.000892875	0.022406302	0.036059681
L097	LD	50	18:00 ~ 19:00	400	0.642136093	0.084625782	0.726761875	1.169606956	0.021262285	0.001395951	0.022658236	0.036464806	0.019580617	0.001255385	0.020836502	0.033532212
L098	LD	50	18:00 ~ 19:00	300	0.29769383	0.086288102	0.383981932	0.617957483	0.012622968	0.002826143	0.01544911	0.024862871	0.011616718	0.00246911	0.014085828	0.022668887
L099	LD	50	18:00 ~ 19:00	600	0.584400515	0.063226247	0.647251653	1.04251653	0.018811411	0.002034912	0.020334912	0.032725787	0.017328786	0.001368071	0.018969857	0.030089599
L100	LD	50	18:00 ~ 19:00	50	0.427459369	0.027907129	0.455366498	0.73283952	0.014884259	0.001133637	0.016017895	0.02577824	0.013737092	0.000105184	0.014788932	0.02380042
L101	LD	50	18:00 ~ 19:00	200	0.565679513	0.06461456	0.631925669	1.016983256	0.018982105	0.001899114	0.020881219	0.03360498	0.017478984	0.00167918	0.019158164	0.030831999
L102	LD	50	18:00 ~ 19:00	150	0.288860684	0.073515629	0.362012313	0.582600896	0.011375793	0.002422622	0.013798415	0.022206341	0.010480063	0.002137779	0.012617842	0.020306398
L103	LD	50	18:00 ~ 19:00	200	0.416678112	0.047257157	0.463935269	0.74629586	0.013470953	0.00146914	0.014940093	0.024043688	0.01242076	0.001326571	0.013747331	0.02212413
L104	PD	50	18:00 ~ 19:00	850	0.608706862	0.075684933	0.684391796	1.101410992	0.022632519	0.001364351	0.02399687	0.038619123	0.020814016	0.001205184	0.0202192	0.03543638
L105	PD	50	18:00 ~ 19:00	700	0.785839497	0.123557263	0.90937676	1.463496395	0.030878779	0.02357386	0.033236165	0.0548829	0.02021548	0.003403047	0.04892884	0.030430347
L106	LD	50	18:00 ~ 19:00	150	0.422793635	0.111698421	0.534494786	0.860183839	0.015626033	0.002970972	0.018597004	0.029298903	0.01366671	0.002586508	0.016952139	0.027282005
L107	LD	50	18:00 ~ 19:00	450	0.576530748	0.057106687	0.633637435	1.01973807	0.017798816	0.001376853	0.018175669	0.029250831	0.015475647	0.001231329	0.016706976	0.026887204
L108	LD	50	18:00 ~ 19:00	200	0.657269767	0.059890145	0.713249913	1.147861615	0.01776297	0.00091981	0.01866495	0.030038251	0.016358194	0.00081012	0.017168314	0.027629655
L109	LD	50	18:00 ~ 19:00	200	0.616976347	0.052108479	0.646985722	1.076785616	0.01863713	0.001295105	0.019928818	0.030207244	0.017166205	0.001833086	0.029500563	0.031830846
L110	LD	50	18:00 ~ 19:00	250	0.36808975	0.115511764	0.483601514	0.838061514	0.010888141	0.003077142	0.013859983	0.022466359	0.000994432	0.002650156	0.010349441	0.012644588
L111	LD	50	18:00 ~ 19:00	250	0.609306613	0.055429384	0.664735996	1.069786227	0.019830623	0.001486057	0.02131668	0.034305786	0.018263715	0.001323394	0.015857109	0.031522318
L112	LD	50	18:00 ~ 19:00	500	0.614060992	0.058179028	0.672240019	1.081862753	0.019625865	0.001224989	0.020850854	0.033556113	0.018082364	0.001110489	0.019192852	0.030887825
L113	LD	50	18:00 ~ 19:00	550	0.564108098	0.06524944	0.629357537	1.012850259	0.018582992	0.001560219	0.020143211	0.032417275	0.017117284	0.001396395	0.018513678	0.029794803
L114	LD	50	18:00 ~ 19:00	350	0.555722416	0.062784269	0.618506685	0.995387548	0.018226009	0.001797641	0.020023649	0.03222486	0.016783505	0.001595474	0.018378979	0.02957802
L115	LD	50	18:00 ~ 19:00	100	0.289885479	0.021652645	0.311538124	0.501370764	0.014416241	0.001348508	0.015764749	0.025370841	0.013311711	0.001248299	0.01456001	0.028430722
L116	LD	50	18:00 ~ 19:00	100	0.267504442	0.019669508	0.28717395	0.462160524	0.013365244	0.001475871	0.014841115	0.023884401	0.01235049	0.001371487	0.013721977	0.022083636
L117	LD	50	18:00 ~ 19:00	100	0.793288355	0.044204438	0.837492793	1.347810651	0.026566055	0.001422387	0.027988442	0.045042519	0.024462583	0.001272147	0.02573473	0.041415931
L118	PD	50	18:00 ~ 19:00	1100	0.587336211	0.076539041	0.663875252	1.068400998	0.021544285	0.001563109	0.023107394	0.037187653	0.019810244	0.001371679	0.02118192	0.034088915
L119	PD	50	18:00 ~ 19:00	700	0.846936683	0.147713961	1.017080791	1.6368288	0.034401754	0.003074185	0.034749539	0.060311528	0.031610318	0.002612274	0.034272592	0.050575786
L120	LD	50	18:00 ~ 19:00	150	0.54095271	0.071699666	0.615794937	0.991023424	0.01825361	0.001694464	0.019948074	0.032103234	0.016813706	0.001512954	0.01832666	0.029493826
L121	LD	50	18:00 ~ 19:00	400	0.566590405	0.064231371	0.630821776	1.015206718	0.016437947	0.00144694	0.017884888	0.028782865	0.015139393	0.001288224	0.016427617	0.026437621
L122	LD	50	18:00 ~ 19:00	350	0.656511411	0.05863017	0.71514153	1.150905869	0.017969668	0.001013426	0.018983093	0.030550252	0.016547157	0.000905569	0.017452725	0.028087369
L123	LD	50	18:00 ~ 19:00	150	0.539868542	0.06943044	0.609289893	0.980569225	0.017823152	0.001683011	0.019506164	0.031920249	0.016416728	0.001502033	0.017918761	0.028837378
L124	LD	50	18:00 ~ 19:00	50	0.20871768	0.029033045	0.237750725	0.382621752	0.006921155	0.01349551	0.011041707	0.01776986	0.008965168	0.00125378	0.010218947	0.016445761
L125	LD	50	18:00 ~ 19:00	50	0.287105297	0.02561673	0.312722027	0.503276067	0.012858833	0.001288704	0.014147536	0.022768196	0.011875746	0.0013906921	0.021033786	0.021033786
L126	PD	50	18:00 ~ 19:00	700	0.870488415	0.147656405	1.01814482	1.638541185	0.03069434	0.00369434	0.037519708	0.060381967	0.031654913	0.00260774	0.034262652	0.055140257
L127	DD	50	18:00 ~ 19:00	100	0.2125287	0.02660437	0.239133077	0.384846426	0.0088717	0.001379975	0.010251675	0.016498491	0.0081591	0.001283632	0.009442732	0.015196566
L128	LD	50	18:00 ~ 19:00	350	0.594133644	0.060620161	0.654753805	1.053721488	0.017505055	0.001292703	0.018797758	0.030251984	0.016126738	0.001162066	0.017288804	0.027823564
L129	LD	50	18:00 ~ 19:00	350	0.668150833	0.06372434	0.731875173	1.177835991	0.017896636	0.001063646	0.018960283	0.030513541	0.016475517	0.000942121	0.017417638	0.028030901
L130	LD	50	18:00 ~ 19:00	350	0.743419536	0.070939179	0.814358715	1.310580055	0.018704336	0.000983292	0.019687628	0.031684088	0.017210088	0.000858008	0.018068096	0.02907771
L131																

Table 5: Hourly composite emission rate for each road link

L162	LD	50	18:00 ~ 19:00	450	0.324289869	0.06917029	0.393460159	0.633211172	0.008107734	0.001539165	0.009646899	0.015525141	0.007473661	0.001368758	0.008842414	0.014230451
L163	LD	50	18:00 ~ 19:00	450	0.358802549	0.082418269	0.441220819	0.710074312	0.010426884	0.001917042	0.012343926	0.019865574	0.00960135	0.001688768	0.011288903	0.018167683
L164	LD	50	18:00 ~ 19:00	50	0.310897137	0.032847296	0.343744433	0.553201666	0.009836323	0.00124502	0.010960825	0.017639694	0.009090859	0.001046328	0.010137188	0.016314811
L165	LD	50	18:00 ~ 19:00	50	0.33414992	0.022467551	0.356617471	0.573918761	0.014161767	0.001379975	0.015541742	0.025011948	0.001283632	0.001436408	0.023116702	
L166	LD	50	18:00 ~ 19:00	500	0.356154029	0.083571155	0.439725184	0.707667328	0.010378223	0.001949249	0.012327472	0.019839093	0.009555377	0.001715584	0.01127096	0.018138808
L167	LD	50	18:00 ~ 19:00	450	0.324511448	0.06920687	0.393718317	0.633626636	0.008100946	0.001542546	0.009643491	0.015519656	0.007467576	0.00137207	0.008839646	0.014225996
L168	DD	50	18:00 ~ 19:00	100	0.2198803	0.050900152	0.278970452	0.448958307	0.0079351	0.002095425	0.010030525	0.016142524	0.0072829	0.001871137	0.009154037	0.014731958
L169	DD	50	18:00 ~ 19:00	300	0.4401288	0.069817419	0.509946219	0.820676848	0.0131451	0.001779133	0.014924233	0.024018165	0.0120741	0.00156413	0.01363823	0.02194855
L170	LD	50	18:00 ~ 19:00	200	0.519898363	0.050845787	0.57074415	0.918521391	0.015440386	0.001327277	0.016767662	0.026984869	0.014229598	0.001194688	0.015424286	0.02482292
L171	LD	50	18:00 ~ 19:00	100	0.312872984	0.083322697	0.396195682	0.637613558	0.00930407	0.001626569	0.017591114	0.008574722	0.001052568	0.001450158	0.010023479	0.016133439
L172	LD	50	18:00 ~ 19:00	50	0.289195347	0.063030165	0.352225513	0.566850606	0.011222449	0.002173893	0.013396342	0.021559269	0.010349762	0.001925724	0.0122755431	0.019755431
L173	LD	50	18:00 ~ 19:00	500	0.447802388	0.091978454	0.539780842	0.8686909	0.011796043	0.001801939	0.013597982	0.021883777	0.010854045	0.00157907	0.012433115	0.020090109
L174	LD	50	18:00 ~ 19:00	600	0.49419462	0.071338876	0.565533496	0.910135677	0.014382966	0.001718305	0.01610127	0.025912418	0.013242494	0.001516821	0.014759315	0.023752756
L175	LD	50	18:00 ~ 19:00	350	0.477884745	0.05718749	0.535072235	0.861113151	0.013012287	0.001352695	0.014364982	0.02311814	0.011989522	0.001208391	0.013197913	0.021239929
L176	LD	50	18:00 ~ 19:00	300	0.512730938	0.08696556	0.599727495	0.965165446	0.015676841	0.002090802	0.017767643	0.028594178	0.014423888	0.001829539	0.016253427	0.02615729
L177	LD	50	18:00 ~ 19:00	50	0.24448225	0.341457746	0.585939996	0.942976674	0.0071173	0.008978732	0.016096032	0.025039988	0.00636475	0.007571138	0.013935888	0.022427582
L178	LD	50	18:00 ~ 19:00	100	0.418254603	0.492509638	0.910764241	1.465729323	0.009419336	0.012350927	0.021725263	0.034963335	0.008359248	0.010287053	0.018646031	0.030008239
L179	LD	50	18:00 ~ 19:00	300	0.501705114	0.093782165	0.595487279	0.958341498	0.015348434	0.002247984	0.017596418	0.028318619	0.01411676	0.0019603	0.016077061	0.025873457
L180	PD	50	18:00 ~ 19:00	950	0.467280755	0.089247636	0.556528392	0.895643402	0.01835622	0.00212856	0.020484781	0.032966977	0.016871651	0.001862666	0.018734317	0.030149888
L181	PD	70	18:00 ~ 19:00	1850	0.349301713	0	0.349301713	0.562145218	0.010336471	0	0.010336471	0.016634897	0.009495617	0	0.009495617	0.015281676
L182	PD	70	18:00 ~ 19:00	2950	0.372204376	0	0.372204376	0.599003391	0.010778998	0	0.010778998	0.01734072	0.009900373	0	0.009900373	0.015930666
L183	PD	50	18:00 ~ 19:00	650	0.54758544	0.083788054	0.61373494	1.016094619	0.024078805	0.002053227	0.026132032	0.042055325	0.022136602	0.001801021	0.023937623	0.038523755
L184	PD	50	18:00 ~ 19:00	850	0.737509185	0.086942348	0.824451532	1.326822829	0.028609608	0.001830275	0.030439882	0.04898812	0.026302898	0.001576794	0.027879692	0.044687904
L185	DD	50	18:00 ~ 19:00	550	0.3487444	0.076856295	0.425600695	0.684936222	0.0091747	0.010738926	0.017507408	0.00824239	0.001501293	0.00925193	0.01597301	
L186	DD	50	18:00 ~ 19:00	400	0.3001917	0.089619193	0.389810893	0.627338263	0.0089347	0.002243787	0.011178487	0.01789987	0.0081954	0.001969237	0.010164637	0.016358357
L187	LD	50	18:00 ~ 19:00	150	0.353322078	0.062540047	0.415862125	0.669263553	0.013617801	0.00183297	0.015450771	0.024865543	0.01255449	0.001641674	0.014196165	0.022846456
L188	LD	50	18:00 ~ 19:00	100	0.363839117	0.034606727	0.398445844	0.641234834	0.014723083	0.001332261	0.016055343	0.025838506	0.013595247	0.001239409	0.014831956	0.023866599
L189	LD	50	18:00 ~ 19:00	50	0.212100172	0.088875724	0.300975896	0.484372548	0.009842912	0.012768827	0.02025916	0.020549384	0.009063198	0.002564334	0.011627533	0.018712653
L190	LD	50	18:00 ~ 19:00	50	0.293985472	0.087093889	0.381079361	0.613286259	0.012252303	0.002892658	0.015144961	0.024373392	0.011279152	0.002532593	0.013811744	0.022227793
L191	LD	50	18:00 ~ 19:00	100	0.236696379	0.027471131	0.264167511	0.425135342	0.010286602	0.01380292	0.011666894	0.01775999	0.009514725	0.010798279	0.01283554	0.017781033
L192	LD	50	18:00 ~ 19:00	100	0.256572401	0.029107811	0.285680211	0.459756591	0.010049514	0.001289514	0.011339027	0.01824835	0.009294166	0.001198967	0.010493132	0.016887018
L193	LD	50	18:00 ~ 19:00	100	0.282125443	0.026488061	0.308613504	0.496664057	0.011833497	0.00131346	0.013146957	0.021157924	0.010934517	0.001220149	0.012154666	0.019560991
L194	LD	50	18:00 ~ 19:00	150	0.232823804	0.026509258	0.259333062	0.41735507	0.01004969	0.00140367	0.011808638	0.019004114	0.009623802	0.001305712	0.010929514	0.017589303
L195	LD	50	18:00 ~ 19:00	250	0.270612086	0.074743153	0.345355239	0.555794001	0.010894589	0.002466884	0.013361472	0.021503152	0.010037065	0.002174331	0.012211396	0.019652288
L196	LD	50	18:00 ~ 19:00	200	0.283577792	0.056176275	0.339754067	0.54677981	0.01099928	0.001974113	0.012973393	0.020878601	0.01014607	0.001764551	0.011910621	0.019168238
L001	LD	50	19:00 ~ 20:00	200	0.378043021	0.06449451	0.442537532	0.712193351	0.009976704	0.001398254	0.011374958	0.018306174	0.009198537	0.001254117	0.010452654	0.016821874
L002	LD	50	19:00 ~ 20:00	200	0.590974741	0.095245924	0.686220664	1.104362364	0.01363653	0.0006914	0.01432793	0.02305851	0.012550944	0.000619064	0.013170007	0.02119502
L003	LD	50	19:00 ~ 20:00	50	0.353077279	0.031084767	0.384162046	0.618247348	0.01168294	0.001155858	0.012838799	0.020661993	0.010790423	0.001074997	0.011865421	0.019095496
L004	DD	50	19:00 ~ 20:00	900	0.7030418	0.064282616	0.767324416	1.234885876	0.0265707	0.00134375	0.02791445	0.04492384	0.0244334	0.001194543	0.025627943	0.041244074
L005	DD	50	19:00 ~ 20:00	750	0.9548384	0.066815425	1.021653825	1.644188366	0.0368482	0.001006796	0.037854996	0.060921559	0.038918	0.000897016	0.034788816	0.055987032
L006	LD	50	19:00 ~ 20:00	200	0.64443672	0.099376394	0.743813114	1.197048197	0.016118221	0.00096358	0.017024579	0.027398335	0.014832097	0.000803645	0.015635742	0.025163225
L007	LD	50	19:00 ~ 20:00	400	0.544882597	0.078902696	0.623785293	1.003882623	0.013978551	0.001010916	0.014989466	0.024123148	0.01287401	0.000909304	0.013782944	0.022181443
L008	LD	50	19:00 ~ 20:00	250	0.521076025	0.052943935	0.57401996	0.923793283	0.012855185	0.000940345	0.01379553	0.022201698	0.011846676	0.000853584	0.01270026	0.020439037
L009	LD	50	19:00 ~ 20:00	250	0.278662026	0.059657618	0.338319644	0.544471335	0.009844045	0.001803848	0.011647893	0.01874542	0.009083624	0.001622577	0.0107729918	
L010	LD	50	19:00 ~ 20:00	50	0.20727077	0.115399859	0.322670629	0.51928675	0.004566044	0.002912816	0.00756886	0.012180868	0.004271687	0.002523317	0.006795004	0.010935472
L011	LD	50	19:00 ~ 20:00	150	0.522499657	0.055167062	0.577666718	0.929662156	0.013979813	0.00112544	0.015105253	0.024309487	0.012884141	0.001015362	0.013899505	0.022369026
L012	LD	50	19:00 ~ 20:00	50	0.233046363	0.042396644	0.275443007	0.443281448	0.00960737	0.001697366	0.011304736	0.018193163	0.008876065	0.001549069	0.010425134	0.016777584
L013	LD	50	19:00 ~ 20:00	150	0.298961364	0.049450476	0.348413206	0.560713464	0.007859717	0.001199789	0.009095906	0.014579826	0.007758826	0.001095431	0.008354257	0.01344484
L014	DD	50	19:00 ~ 20:00	1000	0.7370619	0.079506023	0.816567953	1.314135469	0.0276771	0.001157483	0.028834583	0.046404648	0.0254532	0.001034689	0.026487889	0.04262802
L015	DD	50	19:00 ~ 20:00	350	1.4879127	0.053453268	1.541365968	2.480581907	0.0637921	0.000767158	0.064559258	0.0586775	0.000677982	0.009355482	0.095523151	
L016	LD	50	19:00 ~ 20:00	150	0.318053686	0.050304613	0.371088298	0.597207242	0.005996778	0.001233903	0.007020081	0.011297697	0.005540973	0.00093182	0.006472794	0.010416926
L017	LD	50	19:00 ~ 20:00	150	0.522499657	0.055167062	0.577666718	0.929662156	0.013979813	0.0011254						

Table 5: Hourly composite emission rate for each road link

L048	DD	50	19:00 ~ 20:00	350	1.343006	0.117092594	1.460098594	2.349795071	0.0577449	0.001052621	0.058797521	0.094625203	0.0531118	0.0009302	0.054042	0.086971952
L049	DD	50	19:00 ~ 20:00	550	0.3332058	0.060692773	0.393898573	0.63916729	0.007445	0.001231465	0.008676465	0.013963381	0.0068426	0.001108707	0.007951307	0.012796357
L050	DD	50	19:00 ~ 20:00	450	0.4429876	0.079316194	0.522303794	0.840564388	0.0155646	0.001463585	0.017028185	0.027404139	0.0143089	0.001313407	0.015623207	0.025141603
L051	DD	50	19:00 ~ 20:00	400	0.3448331	0.061796739	0.406629839	0.654405665	0.007618	0.001200989	0.008818989	0.014192752	0.007002	0.001079659	0.008081659	0.013030637
L052	DD	50	19:00 ~ 20:00	400	0.5003835	0.079762266	0.580145766	0.933651787	0.0177283	0.001268322	0.018996622	0.030572023	0.0163021	0.001145138	0.017447238	0.028078538
L053	DD	50	19:00 ~ 20:00	650	0.3325479	0.07304935	0.405597835	0.65274482	0.0085161	0.001235323	0.009769623	0.015722645	0.0087279	0.001132609	0.008960509	0.014420506
L054	DD	50	19:00 ~ 20:00	400	0.5461486	0.079244158	0.625392758	1.006469582	0.0189167	0.001171251	0.020087951	0.032328343	0.0173969	0.001058628	0.018455528	0.029701122
L055	LD	50	19:00 ~ 20:00	150	0.192102463	0.048294509	0.240396972	0.386880463	0.007376494	0.009121393	0.014679423	0.006820803	0.001582756	0.008403559	0.013524183	0.019252138
L056	LD	50	19:00 ~ 20:00	100	0.143043153	0.05085448	0.193897633	0.312047217	0.006538616	0.001842266	0.008380882	0.013487688	0.006052667	0.001667805	0.007720472	0.012424865
L057	DD	50	19:00 ~ 20:00	700	0.33357	0.073414177	0.406984177	0.654975916	0.0081077	0.001211585	0.009319285	0.014997898	0.0074525	0.001093717	0.008546217	0.013753768
L058	DD	50	19:00 ~ 20:00	400	0.5858041	0.075852839	0.634356939	1.020895997	0.0189753	0.001119648	0.020094948	0.032339604	0.0174515	0.001014393	0.018465893	0.0297179
L059	LD	50	19:00 ~ 20:00	150	0.44012061	0.036034389	0.476154999	0.766295287	0.015949828	0.001372452	0.01732228	0.027877439	0.014715141	0.001266652	0.015981792	0.025720138
L060	LD	50	19:00 ~ 20:00	150	0.353177029	0.053603006	0.406780035	0.654647382	0.013590104	0.00181	0.015400104	0.024784004	0.01253453	0.001632037	0.014166567	0.022798823
L061	LD	50	19:00 ~ 20:00	50	0.102027743	0.035654247	0.13768199	0.221577134	0.008561542	0.002022189	0.010583731	0.017032822	0.007925035	0.001851744	0.00977678	0.015734162
L062	LD	50	19:00 ~ 20:00	100	0.229195653	0.04742738	0.276623033	0.445180511	0.0116111	0.002216559	0.01827659	0.022253405	0.010720628	0.002000771	0.012721399	0.020479057
L063	LD	50	19:00 ~ 20:00	50	0.12843245	0.035559366	0.163991816	0.263913589	0.00523995	0.001335383	0.006575333	0.010581946	0.00486765	0.001244334	0.006111984	0.00983626
L064	LD	50	19:00 ~ 20:00	50	0.164564466	0.117776053	0.282340519	0.454381889	0.006487505	0.00356353	0.010411035	0.016754895	0.006282733	0.00397996	0.00937983	0.015095335
L065	DD	550	19:00 ~ 20:00	550	1.0356659	0.08117025	1.11686715	1.797418979	0.0429002	0.001017082	0.043917282	0.070677838	0.0394578	0.000909401	0.040367201	0.064064552
L066	DD	50	19:00 ~ 20:00	450	1.0382457	0.092404653	1.130650353	1.81960839	0.0416029	0.000920608	0.042523508	0.068434782	0.0382651	0.000821561	0.039086661	0.062903727
L067	DD	50	19:00 ~ 20:00	550	1.0202785	0.081213574	1.101492074	1.772675255	0.0423735	0.001020363	0.043357863	0.069777543	0.0389401	0.000912459	0.039852559	0.064136317
L068	DD	50	19:00 ~ 20:00	850	0.7126808	0.09853037	0.81211117	1.305514584	0.025695	0.001719707	0.026831407	0.043180857	0.0235956	0.001404729	0.024636329	0.03964823
L069	LD	50	19:00 ~ 20:00	450	0.381226853	0.066227346	0.447454199	0.720105941	0.009856542	0.001400323	0.011256866	0.018116124	0.009085584	0.001254541	0.010340035	0.016640562
L070	LD	50	19:00 ~ 20:00	400	0.39609452	0.065307	0.461445221	0.742622251	0.010059591	0.001310443	0.011370035	0.018298251	0.009273112	0.001176399	0.010449511	0.016816816
L071	LD	50	19:00 ~ 20:00	200	0.334339736	0.061234424	0.39557416	0.636613319	0.006907826	0.001204587	0.008112413	0.013055631	0.006372321	0.001081866	0.007454186	0.01199632
L072	LD	50	19:00 ~ 20:00	200	0.39316412	0.05691536	0.45007948	0.724330911	0.009816838	0.001289712	0.01110655	0.017874215	0.009052602	0.001161118	0.01021372	0.016437348
L073	LD	50	19:00 ~ 20:00	200	0.357145858	0.063941537	0.421087395	0.677672789	0.006505662	0.001096313	0.007601975	0.012234163	0.005998809	0.000980974	0.006979782	0.01123843
L074	LD	50	19:00 ~ 20:00	150	0.252278863	0.059139285	0.311418147	0.501177681	0.005120796	0.001376236	0.006497032	0.010455934	0.004732538	0.001239388	0.005971925	0.009610858
L075	DD	50	19:00 ~ 20:00	800	0.3224575	0.071747094	0.394204594	0.634409221	0.0071373	0.001169466	0.008306766	0.013368411	0.0065596	0.001054917	0.007614517	0.012254347
L076	DD	50	19:00 ~ 20:00	350	0.5743045	0.077973422	0.49736951	0.809736951	0.0200585	0.001070411	0.021128911	0.034003602	0.0184486	0.000973834	0.019422434	0.031257299
L077	LD	50	19:00 ~ 20:00	350	0.436598832	0.068872759	0.505471591	0.81347565	0.01214915	0.001464976	0.013614125	0.021909756	0.011195065	0.001309601	0.012504666	0.020124259
L078	DD	50	19:00 ~ 20:00	400	1.3331978	0.074413963	1.407611763	2.265325914	0.0583396	0.000850335	0.059189935	0.09525673	0.0536625	0.000764014	0.054426514	0.087597516
L079	DD	50	19:00 ~ 20:00	800	0.7265998	0.101209993	0.827809793	1.332227411	0.0263638	0.001172725	0.027536525	0.044315631	0.0242431	0.001040369	0.025283469	0.040689698
L080	DD	50	19:00 ~ 20:00	400	1.4070812	0.07705033	1.484086233	2.388399399	0.062086	0.000864003	0.062950003	0.101307958	0.0571081	0.000772679	0.093149854	0.093149854
L081	DD	50	19:00 ~ 20:00	800	0.731466	0.104596381	0.836062381	1.345508632	0.0266139	0.001248978	0.027862878	0.044840844	0.024472	0.001103469	0.025575469	0.041159625
L082	LD	50	19:00 ~ 20:00	100	0.173911696	0.099144101	0.273057597	0.439439616	0.007034039	0.003060897	0.010094936	0.016246185	0.006469638	0.002676459	0.009146097	0.0147918
L083	LD	50	19:00 ~ 20:00	100	0.168222614	0.080988719	0.249211333	0.401065767	0.006909366	0.002594356	0.009503722	0.015294721	0.006368295	0.002289452	0.008657746	0.013933258
L084	LD	50	19:00 ~ 20:00	50	0.132120885	0.021922259	0.154043144	0.247907793	0.0014811	0.001660206	0.010808315	0.017394254	0.008475403	0.00154701	0.010022412	0.016129469
L085	LD	50	19:00 ~ 20:00	100	0.140593602	0.021922259	0.162515861	0.261543275	0.009330773	0.001660206	0.010909978	0.017688221	0.008643166	0.00154701	0.010190176	0.016399458
L086	LD	50	19:00 ~ 20:00	100	0.120552236	0.0966382	0.217216056	0.349574487	0.006993516	0.010233454	0.012323938	0.016469107	0.006437863	0.009281531	0.014937138	0.014937138
L087	LD	50	19:00 ~ 20:00	600	0.388421035	0.091513313	0.479934347	0.772377543	0.011882586	0.001707866	0.013590452	0.021871657	0.010944208	0.001519076	0.012463284	0.020057662
L088	LD	50	19:00 ~ 20:00	50	0.349017707	0.097729172	0.446746879	0.718967622	0.00956095	0.002664774	0.012320869	0.019828467	0.008878558	0.002323252	0.01120181	0.018027521
L089	LD	50	19:00 ~ 20:00	250	0.325306393	0.064211136	0.389517529	0.626866141	0.00789716	0.00148681	0.00938397	0.015101998	0.007281919	0.001327005	0.008086925	0.013854687
L090	LD	50	19:00 ~ 20:00	400	0.566235025	0.12696202	0.692931227	1.115161941	0.017740898	0.000990338	0.0030144927	0.016332242	0.008897757	0.017229998	0.027728926	0.027728926
L091	LD	50	19:00 ~ 20:00	100	0.297301828	0.05187187	0.349173698	0.561399199	0.011340648	0.001759876	0.013100524	0.021083198	0.010467739	0.001590896	0.012058635	0.019406443
L092	LD	50	19:00 ~ 20:00	350	0.65006378	0.155772654	0.805836434	1.296864806	0.020224481	0.001041788	0.021266269	0.034224658	0.018605841	0.000928913	0.019534754	0.031438061
L093	LD	50	19:00 ~ 20:00	100	0.28707314	0.06907744	0.356150579	0.573167373	0.008962156	0.001919106	0.01081262	0.01571165	0.00826091	0.001707828	0.009968738	0.016040389
L094	LD	50	19:00 ~ 20:00	300	0.619432548	0.15714619	0.776578738	1.249779226	0.020869432	0.000852535	0.021721967	0.03495803	0.019208633	0.00078313	0.019991763	0.032173543
L095	LD	50	19:00 ~ 20:00	500	0.325842669	0.07411897	0.39996164	0.643674265	0.0098886	0.001151177	0.011039777	0.017766755	0.0091257	0.001059811	0.010185511	0.016391951
L096	LD	50	19:00 ~ 20:00	300	0.669470532	0.149640278	0.81911081	1.31822779	0.022169364	0.000845258	0.023014622	0.037038351	0.020405629	0.000776383	0.021182012	0.03408906
L097	LD	50	19:00 ~ 20:00	350	0.604590739	0.140745566	0.745336305	1.199499529	0.020564521	0.001172216	0.021736737	0.0349818	0.018928692	0.001059821	0.01988513	0.032168314
L098	LD	50	19:00 ~ 20:00	50	0.168931884	0.076073971	0.245005855	0.394297722	0.00869945	0.002741556	0.011431501	0.018397172	0.008008768	0.002425864	0.010434632	0.016792871
L099	LD	50	19:00 ~ 20:00	550	0.4016250											

Table 5: Hourly composite emission rate for each road link

L130	LD		50	19:00 ~ 20:00	300	0.513758433	0.082215211	0.595973644	0.959124224	0.010351526	0.000918037	0.011269564	0.018136559	0.009527702	0.000811758	0.010339459	0.016639706
L131	LD		50	19:00 ~ 20:00	250	0.418921095	0.071613751	0.490534846	0.78943735	0.011729306	0.0012411	0.012970136	0.020873359	0.010812515	0.001123952	0.011936468	0.019209835
L132	PD		50	19:00 ~ 20:00	1150	0.609278364	0.742378544	0.194739487	0.021494186	0.001936265	0.023430451	0.037077562	0.019762945	0.00166755	0.021430495	0.034488953	0.034488953
L133	DD		50	19:00 ~ 20:00	800	0.4889689	0.066288326	0.555257226	0.893597663	0.016734	0.001334113	0.018068113	0.029077737	0.0153858	0.001198255	0.016584055	0.026689383
L134	DD		50	19:00 ~ 20:00	500	0.6019656	0.067598036	0.669563636	0.107755542	0.0215786	0.001117411	0.022696011	0.036525598	0.0198442	0.00100815	0.02085235	0.03555821
L135	LD		50	19:00 ~ 20:00	150	0.375997169	0.06022402	0.436191218	0.701979976	0.008425317	0.001218392	0.009643709	0.015250006	0.007768259	0.001091395	0.008859655	0.014258197
L136	PD		50	19:00 ~ 20:00	800	0.602395463	0.07939209	0.681787553	0.109722998	0.025568771	0.01641617	0.027210388	0.043790767	0.023521555	0.001447216	0.024968771	0.040183241
L137	PD		50	19:00 ~ 20:00	1350	0.515553858	0.091099638	0.606653496	0.976311738	0.019277569	0.001425339	0.020702908	0.033118017	0.017735236	0.001262265	0.018997502	0.030573439
L138	LD		50	19:00 ~ 20:00	300	0.419759945	0.075785183	0.495545128	0.797500596	0.010631245	0.001184515	0.01181576	0.019015576	0.009797777	0.001065916	0.010863693	0.017483376
L139	LD		50	19:00 ~ 20:00	200	0.561022841	0.097177736	0.658200577	1.059268516	0.011227196	0.000873511	0.012100708	0.019474153	0.010326942	0.000765528	0.011092471	0.017851557
L140	LD		50	19:00 ~ 20:00	200	0.332708437	0.072183126	0.404891564	0.651608189	0.01076807	0.001509947	0.012278018	0.019759505	0.009931944	0.001366781	0.011298725	0.018183491
L141	PD		50	19:00 ~ 20:00	1200	0.56491853	0.093653576	0.658572106	1.059966433	0.021687765	0.001455053	0.023142818	0.037244663	0.019590741	0.001281854	0.012325294	0.034170463
L142	LD		50	19:00 ~ 20:00	250	0.318320538	0.070343557	0.388664095	0.625492675	0.00802497	0.001173353	0.009198323	0.001480329	0.007405789	0.001067242	0.008473031	0.013635598
L143	LD		50	19:00 ~ 20:00	350	0.396993571	0.084094815	0.481088386	0.774234783	0.009432382	0.001168042	0.010600424	0.017059687	0.008690355	0.001046705	0.009737006	0.01567024
L144	LD		50	19:00 ~ 20:00	200	0.405118128	0.116560226	0.521678355	0.939557843	0.010711112	0.001414475	0.012125587	0.019514193	0.009857421	0.001254028	0.011111446	0.017882095
L145	LD		50	19:00 ~ 20:00	100	0.245741956	0.06501644	0.310758397	0.500115918	0.00577342	0.001640047	0.007413467	0.011930789	0.005328774	0.001464217	0.006795991	0.010932233
L146	PD		50	19:00 ~ 20:00	750	0.61250355	0.079248795	0.691758149	1.11327406	0.026119955	0.001627467	0.027747421	0.044655035	0.024628566	0.001434963	0.025463529	0.040979477
L147	PD		50	19:00 ~ 20:00	1000	0.596744072	0.094547006	0.691281077	1.11327406	0.023500933	0.001484593	0.024955326	0.040210207	0.021618025	0.001305863	0.022923856	0.036892278
L148	LD		50	19:00 ~ 20:00	250	0.314984427	0.072343168	0.387327596	0.623341793	0.008115651	0.001252401	0.009368052	0.015075881	0.007488097	0.001134538	0.008622635	0.013876753
L149	LD		50	19:00 ~ 20:00	550	0.411480719	0.094878446	0.681490409	0.910063321	0.001239651	0.011302972	0.018190325	0.009267673	0.001105225	0.010372898	0.016693519	0.016693519
L150	LD		50	19:00 ~ 20:00	250	0.419457279	0.110492907	0.529950186	0.852870032	0.010658279	0.00144631	0.01210459	0.0194804	0.009808561	0.001276746	0.011085307	0.017840027
L151	LD		50	19:00 ~ 20:00	650	0.567198504	0.652988497	0.652988497	0.019171436	0.020542808	0.001371372	0.017648669	0.030603662	0.017648669	0.001225723	0.018874392	0.030375314
L152	LD		50	19:00 ~ 20:00	600	0.592253653	0.081662664	0.673916317	1.084650485	0.020771337	0.001433206	0.022204542	0.035734658	0.01912255	0.001281561	0.020404111	0.032837152
L153	LD		50	19:00 ~ 20:00	250	0.402782214	0.083014174	0.485796387	0.781811558	0.011952718	0.001576026	0.012772348	0.011009545	0.00140413	0.001413675	0.010977824	0.010977824
L154	LD		50	19:00 ~ 20:00	250	0.399895237	0.107620496	0.501605732	0.807254169	0.009564552	0.001448693	0.011013245	0.017724055	0.008802564	0.001279471	0.010082035	0.016225423
L155	LD		50	19:00 ~ 20:00	300	0.394313682	0.074453631	0.468767314	0.754405988	0.001178178	0.001608458	0.013326636	0.021447088	0.010796047	0.001434235	0.012230282	0.019682683
L156	LD		50	19:00 ~ 20:00	200	0.3916015	0.108202391	0.49980308	0.804354393	0.009063973	0.001342292	0.010406266	0.01674722	0.008381478	0.001186764	0.009528554	0.015334667
L157	LD		50	19:00 ~ 20:00	100	0.302847086	0.080014375	0.382861461	0.616154264	0.00549822	0.001383201	0.038388024	0.01499182	0.006030422	0.001616669	0.007647091	0.01230677
L158	LD		50	19:00 ~ 20:00	50	0.400742982	0.108633257	0.50937624	0.819759558	0.008833469	0.001262504	0.010959752	0.016247852	0.00812579	0.00111204	0.009237829	0.014868808
L159	PD		50	19:00 ~ 20:00	750	0.612509355	0.079248795	0.691758149	1.11327406	0.026119955	0.001627467	0.027747421	0.044655035	0.024028566	0.001434963	0.025463529	0.040979477
L160	PD		50	19:00 ~ 20:00	750	0.64377944	0.081546861	0.725326301	1.167296629	0.027698168	0.001593592	0.02929176	0.047140402	0.02548013	0.001403449	0.026883579	0.043264819
L161	LD		50	19:00 ~ 20:00	600	0.747449112	0.852242635	1.371548162	0.030170471	0.01533975	0.031704447	0.027748926	0.050102324	0.009332508	0.029081434	0.046801915	0.046801915
L162	LD		50	19:00 ~ 20:00	400	0.323152934	0.07522729	0.398380834	0.641130212	0.00798741	0.001418514	0.009405924	0.015137329	0.007363858	0.00126931	0.008631638	0.013893703
L163	LD		50	19:00 ~ 20:00	450	0.363081929	0.081169883	0.444251812	0.714952211	0.008202348	0.001244861	0.009447208	0.015203771	0.007559276	0.001113328	0.008672160	0.013957169
L164	LD		50	19:00 ~ 20:00	50	0.11761638	0.033754624	0.151371004	0.243607412	0.005435575	0.001407566	0.00684314	0.011012939	0.005049376	0.001311595	0.008360971	0.010236965
L165	LD		50	19:00 ~ 20:00	50	0.123060226	0.263040024	0.423320833	0.139979798	0.00676321	0.004315086	0.011078296	0.017828745	0.009734529	0.009926525	0.015975153	0.015975153
L166	LD		50	19:00 ~ 20:00	500	0.352363613	0.079666664	0.432030277	0.695283606	0.007764662	0.001232365	0.008997027	0.01479275	0.007156725	0.001103093	0.008259819	0.013292856
L167	LD		50	19:00 ~ 20:00	400	0.323117813	0.075340697	0.39845851	0.641255218	0.007980459	0.001414002	0.005394462	0.015118883	0.007357884	0.001265106	0.008622919	0.013877323
L168	DD		50	19:00 ~ 20:00	150	0.2301197	0.062772792	0.292890692	0.471360707	0.0056938	0.001730748	0.007424548	0.011948621	0.0052266	0.001548609	0.006775209	0.010903614
L169	DD		50	19:00 ~ 20:00	250	0.356024	0.07592508	0.43194908	0.695152933	0.0100109	0.001183384	0.019029293	0.0091928	0.001599702	0.001092923	0.017368805	0.017368805
L170	LD		50	19:00 ~ 20:00	150	0.354185286	0.060678805	0.415064091	0.667979245	0.009513077	0.001532044	0.011045121	0.017775355	0.00877329	0.001373967	0.010147256	0.016330385
L171	LD		50	19:00 ~ 20:00	100	0.458795294	0.144860642	0.603655937	0.971487645	0.01539117	0.016906844	0.02720886	0.014165244	0.001347454	0.015512698	0.024965205	0.024965205
L172	LD		50	19:00 ~ 20:00	50	0.154981217	0.073674871	0.228656088	0.367985388	0.005237669	0.002196282	0.007433951	0.011963755	0.004835142	0.001951087	0.006786229	0.010921349
L173	LD		50	19:00 ~ 20:00	450	0.407939861	0.104818416	0.512758277	0.701479339	0.010715409	0.01962459	0.012194747	0.01962459	0.009863286	0.001310859	0.01174145	0.017982998
L174	LD		50	19:00 ~ 20:00	550	0.398072332	0.070582935	0.468655267	0.754225668	0.011339004	0.001619273	0.012943178	0.020829973	0.010432693	0.001441424	0.011874116	0.01910949
L175	LD		50	19:00 ~ 20:00	350	0.388768886	0.060227774	0.44899666	0.722588285	0.00955679	0.00141618	0.011371859	0.018301188	0.009178836	0.001268824	0.01044766	0.016813837
L176	LD		50	19:00 ~ 20:00	200	0.420500884	0.090647383	0.511148266	0.82261135	0.013617765	0.002022845	0.01564061	0.025171059	0.012532576	0.001780513	0.014313089	0.023034627
L177	LD		50	19:00 ~ 20:00	50	0.200424	0.076937293	0.478355906	0.0697212	0.0075439	0.01451602	0.02361211	0.0062838	0.006393217	0.010267701	0.02062043	0.02062043
L178	LD		50	19:00 ~ 20:00	50	0.081859629	0.445902789	0.827762419	1.332151171	0.007748401	0.011063582	0.018811983	0.030274877	0.006862341	0.009257806	0.016120147	0.025942797
L179	LD		50	19:00 ~ 20:00	250	0.41280001	0.091829438	0.504629447	0.812120354	0.013078692	0.002064965	0.015143657	0.024371292	0.012035568	0.00181504	0.013850607	0.022290336
L180	PD		50	19:00 ~ 20:00	90												

Table 5: Hourly composite emission rate for each road link

L016	LD	50	20:00 ~ 21:00	100	0.284177694	0.0570501997	0.341679691	0.549878794	0.003081399	0.000830637	0.003912036	0.006295795	0.00285185	0.000757719	0.003609569	0.005809023
L017	LD	50	20:00 ~ 21:00	100	0.400723915	0.059957222	0.460681138	0.741392582	0.008622572	0.00103893	0.009661503	0.015548643	0.00794749	0.000935013	0.008882503	0.014294968
L018	LD	50	20:00 ~ 21:00	200	0.260498368	0.071084602	0.33158297	0.53629737	0.008039678	0.001843445	0.009883123	0.015905305	0.007413928	0.001645349	0.009095277	0.014579456
L019	LD	50	20:00 ~ 21:00	150	0.348912851	0.098391176	0.447250227	0.719780577	0.010434824	0.00236743	0.013071567	0.021036596	0.009593135	0.002296289	0.011889424	0.0193134125
L020	LD	50	20:00 ~ 21:00	200	0.319270884	0.077330053	0.396600936	0.638265751	0.006503591	0.00153801	0.008157392	0.013128017	0.005989831	0.001457663	0.007447494	0.01398555
L021	DD	50	20:00 ~ 21:00	800	0.8630195	0.096133684	0.959153184	1.543603586	0.0323776	0.00957268	0.03334866	0.053647136	0.0297762	0.000847496	0.030626399	0.049283969
L022	DD	50	20:00 ~ 21:00	400	1.3421157	0.111216712	1.453332412	2.38905983	0.0558292	0.000597561	0.056426761	0.09080844	0.0513528	0.000532546	0.051885346	0.083501163
L023	LD	50	20:00 ~ 21:00	200	0.419279409	0.065506429	0.484785837	0.78018524	0.009301065	0.00124308	0.010543373	0.016967872	0.008567991	0.001105387	0.009673378	0.015547754
L024	LD	50	20:00 ~ 21:00	200	0.195763339	0.059354466	0.255117804	0.410571287	0.006757677	0.001697192	0.008472869	0.013635727	0.0062613	0.00153281	0.00779411	0.012543373
L025	DD	50	20:00 ~ 21:00	50	0.3377875	0.025409659	0.363197159	0.584507715	0.01461	0.001498261	0.01608261	0.025923668	0.0134425	0.00139508	0.01483758	0.023878711
L026	DD	50	20:00 ~ 21:00	50	0.348916	0.020205544	0.369121544	0.594042065	0.014068	0.001480681	0.015548681	0.025023115	0.012944	0.001377673	0.014321673	0.023048442
L027	DD	50	20:00 ~ 21:00	250	0.4389928	0.113258713	0.552251513	0.88876045	0.0110371	0.001117381	0.012154481	0.019560693	0.0101447	0.000995246	0.011739946	0.017929691
L028	DD	50	20:00 ~ 21:00	200	0.4795145	0.125642734	0.605157234	0.973903742	0.0123688	0.000999187	0.013367987	0.021513636	0.0113696	0.000889906	0.012259506	0.019729714
L029	LD	50	20:00 ~ 21:00	50	0.11441355	0.030644618	0.145058168	0.233447912	0.00649402	0.001482582	0.007931984	0.012765259	0.005982636	0.001380746	0.007363382	0.011850185
L030	DD	50	20:00 ~ 21:00	100	0.7711766	0.100284428	0.871461028	1.402477091	0.0284908	0.001111104	0.029601904	0.047639528	0.0261989	0.000982109	0.027181009	0.043743485
L031	DD	50	20:00 ~ 21:00	850	0.1912273	0.103845281	0.295072581	0.474872108	0.0019269	0.000878474	0.002805374	0.0045148	0.000778713	0.002543713	0.004093699	0.069454449
L032	DD	50	20:00 ~ 21:00	350	1.2851475	0.137432308	1.422579808	2.289414589	0.0537852	0.000960491	0.054745691	0.08810443	0.0494676	0.00084443	0.05031203	0.080969162
L033	LD	50	20:00 ~ 21:00	200	0.199606599	0.101493098	0.301096997	0.484567441	0.00989078	0.002972519	0.009961597	0.016031597	0.006425339	0.002593544	0.009018883	0.014514449
L034	LD	50	20:00 ~ 21:00	200	0.192322582	0.074809235	0.267131816	0.429905918	0.006578431	0.002262976	0.008841408	0.014228831	0.006065834	0.002004153	0.008069897	0.012987352
L035	LD	50	20:00 ~ 21:00	50	0.0315764	0.023828543	0.08916539	0.095404943	0.00816539	0.001804571	0.003928309	0.008654671	0.001681532	0.000843532	0.012944778	0.019347851
L036	LD	50	20:00 ~ 21:00	50	0.0315764	0.023828543	0.08916539	0.095404943	0.00816539	0.001804571	0.003928309	0.008654671	0.001681532	0.000843532	0.012944778	0.019347851
L037	LD	50	20:00 ~ 21:00	200	0.18850445	0.077759934	0.26626379	0.42851133	0.006528037	0.002343741	0.008871778	0.013859411	0.0060617584	0.002071457	0.00808904	0.013018016
L038	LD	50	20:00 ~ 21:00	200	0.188116257	0.068491762	0.256680818	0.412069548	0.006492642	0.002119718	0.008611895	0.013859411	0.0060617584	0.002071457	0.00808904	0.013018016
L039	LD	50	20:00 ~ 21:00	50	0.155318499	0.061872073	0.21190572	0.349533476	0.004978218	0.001873224	0.006851442	0.01103263	0.000851442	0.001180851	0.007307485	0.011388348
L040	LD	50	20:00 ~ 21:00	50	0.170609069	0.03475358	0.205362649	0.330498325	0.006362262	0.001268867	0.00761113	0.012280172	0.005990094	0.001717391	0.007307485	0.011388348
L041	LD	50	20:00 ~ 21:00	50	0.16535424	0.059664438	0.225018679	0.36213156	0.006051758	0.001912149	0.007963907	0.012816634	0.005590094	0.001717391	0.007307485	0.011388348
L042	LD	50	20:00 ~ 21:00	100	0.167489262	0.034955026	0.202444288	0.325901691	0.006202649	0.001285496	0.007488145	0.012050971	0.005748773	0.001196722	0.006945495	0.01177663
L043	LD	50	20:00 ~ 21:00	150	0.425367654	0.072205567	0.497573221	0.800764487	0.008751242	0.001321078	0.010072319	0.016209787	0.008058632	0.001166437	0.009225069	0.014846272
L044	LD	50	20:00 ~ 21:00	300	0.478823501	0.112236556	0.591060057	0.951216592	0.01420317	0.01236689	0.015439859	0.024847983	0.013074232	0.001109026	0.014183258	0.022825684
L045	LD	50	20:00 ~ 21:00	150	0.197950154	0.061009533	0.258959687	0.416754183	0.00635625	0.001839966	0.008196216	0.013190499	0.005871829	0.00165013	0.007521959	0.01210539
L046	LD	50	20:00 ~ 21:00	150	0.178765836	0.035812504	0.21457834	0.345329506	0.005954768	0.001242551	0.007197318	0.011582932	0.005519939	0.00115683	0.006767797	0.010745193
L047	DD	50	20:00 ~ 21:00	950	0.7859393	0.099473062	0.885412362	1.424929531	0.029492	0.001089818	0.030581818	0.049216542	0.0271208	0.000966249	0.028080748	0.045201612
L048	DD	50	20:00 ~ 21:00	300	1.4219757	0.150666193	1.572641893	2.530915504	0.0607046	0.000879607	0.0061584207	0.099109928	0.0558334	0.000771488	0.056604888	0.09109651
L049	DD	50	20:00 ~ 21:00	450	0.3006863	0.068633063	0.369319363	0.594360423	0.0049522	0.001115797	0.006067997	0.00976547	0.0045484	0.000999771	0.005548171	0.006928993
L050	DD	50	20:00 ~ 21:00	400	0.5048186	0.099560778	0.604379378	0.972651907	0.0168565	0.001350622	0.018207122	0.029301497	0.0154961	0.00120207	0.026973032	0.041674811
L051	DD	50	20:00 ~ 21:00	400	0.316874	0.069758155	0.386632155	0.62222592	0.005177	0.001087644	0.006264644	0.010081942	0.0047551	0.000972802	0.005727902	0.009218142
L052	DD	50	20:00 ~ 21:00	300	0.5480828	0.100153529	0.648236329	1.04323653	0.0185488	0.00110191	0.019658991	0.0170557	0.00099658	0.01805228	0.029052257	0.041674811
L053	DD	50	20:00 ~ 21:00	550	0.352413	0.090386665	0.442781665	0.712586245	0.0097236	0.001122848	0.009046448	0.01455881	0.0072816	0.001007751	0.008289351	0.013340385
L054	DD	50	20:00 ~ 21:00	300	0.5741987	0.099564204	0.673762904	1.084313593	0.0193272	0.00971957	0.020299157	0.032668245	0.0177742	0.000878502	0.018652702	0.030018539
L055	LD	50	20:00 ~ 21:00	100	0.177745785	0.060078785	0.237824569	0.382740592	0.00596437	0.001892687	0.007857057	0.012644676	0.005511785	0.001699632	0.007211417	0.011605622
L056	LD	50	20:00 ~ 21:00	100	0.163489472	0.054214225	0.217703697	0.350359268	0.00565581	0.001732575	0.007398156	0.011906148	0.005239726	0.001565969	0.006085094	0.010952674
L057	DD	50	20:00 ~ 21:00	550	0.3562563	0.090080994	0.446607294	0.718742983	0.0076441	0.001086756	0.008730856	0.014050917	0.0070245	0.000974121	0.007998621	0.0128725
L058	DD	50	20:00 ~ 21:00	350	0.5831971	0.097325032	0.680522132	1.095191489	0.0192445	0.000957405	0.020201905	0.032511735	0.0176981	0.000864066	0.018562166	0.029872836
L059	LD	50	20:00 ~ 21:00	100	0.339862936	0.039629341	0.379492277	0.610732101	0.012502269	0.001402144	0.013904413	0.022376928	0.011537904	0.001288995	0.01286899	0.020642841
L060	LD	50	20:00 ~ 21:00	100	0.289129047	0.058712478	0.347841525	0.55979528	0.010725981	0.001886731	0.012612712	0.020298141	0.001692817	0.011587318	0.018647934	0.020642841
L061	LD	50	20:00 ~ 21:00	50	0.047075815	0.049238879	0.096314694	0.155003089	0.006848616	0.002378504	0.009227212	0.014849574	0.006343186	0.002152701	0.008495886	0.01367277
L062	LD	50	20:00 ~ 21:00	50	0.124531798	0.060530009	0.190584807	0.306715753	0.007046803	0.002451756	0.009498559	0.015286411	0.006509496	0.00218839	0.006997886	0.013997886
L063	LD	50	20:00 ~ 21:00	50	0.159520927	0.03916885	0.198689778	0.317759407	0.004569017	0.001919107	0.005760034	0.009269853	0.004243454	0.001109811	0.005353265	0.008615224
L064	LD	50	20:00 ~ 21:00	50	0.195604813	0.132285963	0.372890776	0.527687742	0.00672882	0.003778314	0.010451195	0.016819527	0.006108605	0.003265419	0.009374024	0.015085992
L065	DD	50	20:00 ~ 21:00	500	1.1243789	0.100260677	1.224639577	1.970861456	0.0461845	0.000867801	0.047052301	0.07572315	0.0424783	0.000769825	0.043248125	0.069600937
L066	DD	50	20:00 ~ 21:00	400	1.1048927	0.114667224	1.219559924	1.962686567	0.0441675	0.000746568	0.044914068	0.072282007	0.0406237	0.000663252	0.041286952	0.06644743
L067	DD	50	20:00 ~ 21:00	500	1.1138112	0.099473337	1.213284537	1.952587336	0.045735							

Table 5: Hourly composite emission rate for each road link

L098	LD	50	20:00 ~ 21:00	50	0.177836923	0.102460356	0.280297279	0.451093623	0.008293748	0.003282231	0.011575979	0.018629685	0.007625881	0.002865291	0.010491172	0.016883862
L099	LD	450	20:00 ~ 21:00	450	0.422872769	0.102143265	0.525016034	0.844929304	0.012461318	0.003135665	0.013776983	0.02217185	0.01147687	0.001183258	0.012660128	0.020374451
L100	LD	50	20:00 ~ 21:00	50	0.151836571	0.038266479	0.190103051	0.305940443	0.004706019	0.001227108	0.005933127	0.009548419	0.004370694	0.001143442	0.005514136	0.00887412
L101	LD	100	20:00 ~ 21:00	100	0.315879919	0.067128915	0.383008833	0.616391436	0.00903374	0.001866386	0.010900126	0.017542008	0.008324282	0.001658327	0.009982609	0.016065412
L102	LD	100	20:00 ~ 21:00	100	0.179353601	0.076965741	0.256319342	0.41250497	0.006618728	0.002363478	0.008982207	0.014455425	0.006101496	0.002089472	0.008190968	0.013182053
L103	LD	150	20:00 ~ 21:00	150	0.230742249	0.053718591	0.284460684	0.457794209	0.005387248	0.001336451	0.0067147	0.010806235	0.004974882	0.001212762	0.006187644	0.009958023
L104	PD	850	20:00 ~ 21:00	850	0.65517389	0.124757673	0.779931258	1.255175062	0.024901299	0.001044421	0.025945721	0.01755486	0.022902599	0.000923723	0.023826322	0.038344653
L105	PD	650	20:00 ~ 21:00	650	0.77610719	0.169478598	0.945585788	1.521769033	0.029421812	0.001220793	0.030642605	0.049134369	0.027055339	0.001053899	0.028109238	0.045237321
L106	LD	50	20:00 ~ 21:00	50	0.368770591	0.151508281	0.519278872	0.835696259	0.013302302	0.002562744	0.015955046	0.025677093	0.01222488	0.002312042	0.014536922	0.02339485
L107	LD	300	20:00 ~ 21:00	300	0.309213111	0.062203052	0.371416162	0.597734887	0.006019016	0.001248248	0.007267263	0.011695498	0.005553401	0.001119921	0.006673322	0.010739644
L108	LD	250	20:00 ~ 21:00	250	0.437681988	0.08958949	0.484992732	0.527540937	0.00848103	0.000791917	0.009272948	0.014923325	0.007810826	0.000711513	0.008522339	0.013715342
L109	LD	150	20:00 ~ 21:00	150	0.33266691	0.055195731	0.387862641	0.624202863	0.007321932	0.001197312	0.008519244	0.013710359	0.006755652	0.001083018	0.00783867	0.012615085
L110	LD	150	20:00 ~ 21:00	150	0.272862611	0.115882413	0.388745024	0.625622918	0.00584527	0.002807375	0.008652645	0.013925048	0.005358623	0.002424314	0.007782937	0.012525392
L111	LD	150	20:00 ~ 21:00	150	0.306794701	0.064842467	0.371277168	0.597511197	0.007629535	0.001606472	0.009236608	0.014863876	0.007035537	0.001431964	0.008467501	0.013627088
L112	LD	400	20:00 ~ 21:00	400	0.464081377	0.104264904	0.568346281	0.914662403	0.013534467	0.001055884	0.014590351	0.023480836	0.012465848	0.000959397	0.013425245	0.021605784
L113	LD	400	20:00 ~ 21:00	400	0.428431813	0.106071521	0.534503334	0.860197595	0.013145977	0.00138868	0.014532777	0.02338818	0.012105161	0.001243829	0.013348991	0.021489064
L114	LD	200	20:00 ~ 21:00	200	0.302408525	0.066072677	0.368481203	0.593011539	0.008196025	0.001753216	0.009949242	0.016011712	0.007554947	0.001561272	0.009116219	0.014671096
L115	LD	50	20:00 ~ 21:00	50	0.162878129	0.027631196	0.190509325	0.306594277	0.008975309	0.001454992	0.010430301	0.0167859	0.008305825	0.001352782	0.005986068	0.015543984
L116	LD	50	20:00 ~ 21:00	50	0.105898301	0.022069615	0.127967916	0.205943885	0.008089214	0.001668737	0.010475087	0.008161334	0.001550788	0.009712122	0.015630106	0.014889504
L117	LD	50	20:00 ~ 21:00	50	0.191796625	0.05748646	0.190509325	0.306594277	0.008975309	0.001454992	0.010430301	0.0167859	0.008305825	0.001352782	0.005986068	0.015543984
L118	PD	950	20:00 ~ 21:00	950	0.619622692	0.107303937	0.726926628	1.1698721	0.023799041	0.001298101	0.025097141	0.016213577	0.007528873	0.001723059	0.009712122	0.015630106
L119	PD	650	20:00 ~ 21:00	650	0.805819039	0.173925283	0.979744321	1.576741726	0.031257573	0.001505337	0.02376291	0.016213577	0.007528873	0.001723059	0.009712122	0.015630106
L120	LD	100	20:00 ~ 21:00	100	0.446278959	0.138709505	0.58498464	0.941445335	0.015018169	0.001388289	0.016406459	0.026460357	0.013824171	0.001245533	0.015069704	0.024252277
L121	LD	200	20:00 ~ 21:00	200	0.309460973	0.068947531	0.378408504	0.608987942	0.005757606	0.001347362	0.007104968	0.01434309	0.005308723	0.001199185	0.006057908	0.010473437
L122	LD	850	20:00 ~ 21:00	850	0.416095065	0.080632099	0.496727165	0.799402896	0.008158193	0.00087016	0.009028353	0.01452969	0.007515863	0.000782662	0.008298525	0.013355148
L123	LD	100	20:00 ~ 21:00	100	0.409647807	0.122087893	0.5317357	0.85574531	0.013597827	0.001486079	0.013597827	0.02315885	0.01144556	0.001365844	0.021992989	0.037058641
L124	LD	50	20:00 ~ 21:00	50	0.136467859	0.036461737	0.172925987	0.278302517	0.004980023	0.001299291	0.006279314	0.010105551	0.004625174	0.001210703	0.005835877	0.009391911
L125	LD	50	20:00 ~ 21:00	50	0.216284403	0.03947793	0.250232197	0.402708683	0.008023145	0.001202352	0.014846961	0.007421507	0.001117368	0.000838875	0.013741954	0.021479561
L126	PD	650	20:00 ~ 21:00	650	0.802152069	0.17162459	0.973774528	1.567134299	0.031047855	0.001455769	0.032503624	0.025309383	0.028548209	0.001248264	0.029796473	0.047952566
L127	DD	50	20:00 ~ 21:00	50	0.1233132	0.156664931	0.25212714	0.004472	0.01374308	0.005846308	0.009408697	0.0041136	0.001279854	0.005393454	0.008795951	0.016695551
L128	LD	200	20:00 ~ 21:00	200	0.409140873	0.08789959	0.497040833	0.799907694	0.010187967	0.001078	0.011265966	0.01813077	0.009385957	0.000972051	0.010358008	0.016669551
L129	LD	300	20:00 ~ 21:00	300	0.450053455	0.089565351	0.539709805	0.868576578	0.008595372	0.000882085	0.009477457	0.007911434	0.000783901	0.008695335	0.013993751	0.021539528
L130	LD	300	20:00 ~ 21:00	300	0.466579936	0.095011606	0.561591542	0.903791732	0.007727033	0.000809608	0.008581641	0.013810777	0.007148217	0.000711063	0.00785928	0.012648254
L131	LD	200	20:00 ~ 21:00	200	0.409140873	0.08789959	0.497040833	0.799907694	0.010187967	0.001078	0.011265966	0.01813077	0.009385957	0.000972051	0.010358008	0.016669551
L132	PD	1000	20:00 ~ 21:00	1000	0.640476366	0.153013174	0.793849541	1.276994458	0.022505352	0.001718839	0.024224191	0.038984959	0.020688352	0.001473499	0.022161851	0.035665954
L133	DD	650	20:00 ~ 21:00	650	0.5244372	0.077251179	0.601688379	0.968321176	0.0171881	0.001196177	0.018384277	0.029586552	0.0158029	0.001069562	0.016872462	0.027153528
L134	DD	450	20:00 ~ 21:00	450	0.6504611	0.082088294	0.732549394	1.178921041	0.0226784	0.000962376	0.023640776	0.038046046	0.0208553	0.000863827	0.021791927	0.034953461
L135	LD	100	20:00 ~ 21:00	100	0.321171604	0.061859846	0.383031449	0.616427833	0.004689116	0.000976954	0.005666071	0.009118634	0.004327532	0.0008774	0.005204932	0.008376506
L136	PD	650	20:00 ~ 21:00	650	0.671939239	0.090792616	0.762731855	1.227494883	0.028154022	0.001504074	0.029658096	0.047729961	0.025892065	0.001317539	0.027209604	0.043789505
L137	PD	1150	20:00 ~ 21:00	1150	0.570066091	0.109637999	0.67970409	0.919374979	0.020905969	0.001254726	0.022160695	0.035664093	0.019226399	0.001104671	0.020330999	0.03271949
L138	LD	250	20:00 ~ 21:00	250	0.40680762	0.091328584	0.498136204	0.801670519	0.008862524	0.001049936	0.00991246	0.015952519	0.008162536	0.000939514	0.00910205	0.014648293
L139	LD	200	20:00 ~ 21:00	200	0.510305876	0.112867604	0.62317348	0.908772637	0.008772637	0.000778655	0.009551292	0.015371276	0.008063547	0.000677396	0.008740943	0.014067149
L140	LD	150	20:00 ~ 21:00	150	0.364958211	0.096068886	0.461027097	0.741943948	0.010609172	0.001477751	0.012086923	0.019451969	0.009773636	0.001323718	0.011097354	0.017859415
L141	PD	1000	20:00 ~ 21:00	1000	0.619465654	0.110898192	0.730363846	1.175403751	0.02380723	0.001287405	0.024668128	0.039699405	0.02150154	0.001127149	0.02262689	0.036417255
L142	LD	300	20:00 ~ 21:00	300	0.359853193	0.088511481	0.448364674	0.721571204	0.00793222	0.00106159	0.009044812	0.014556177	0.007357228	0.000956791	0.008314019	0.013380083
L143	LD	50	20:00 ~ 21:00	50	0.41922307	0.159886863	0.836674724	0.008728599	0.001022599	0.009751197	0.015692992	0.008034324	0.00909828	0.00844152	0.014394182	0.021539528
L144	LD	150	20:00 ~ 21:00	150	0.484803719	0.141379422	0.626183141	1.007741576	0.012369178	0.001116837	0.013486015	0.021703584	0.011375471	0.000987411	0.012362883	0.019898601
L145	LD	100	20:00 ~ 21:00	100	0.269527111	0.03864106	0.069113949	0.544988604	0.004845678	0.001479052	0.006324731	0.010178642	0.00471326	0.001313824	0.00578515	0.009310274
L146	PD	650	20:00 ~ 21:00	650	0.683665048	0.090038978	0.773704026	1.245152837	0.02887545	0.001476665	0.030352115	0.04846873	0.026556058	0.001294389	0.027850447	0.044820838
L147	PD	850	20:00 ~ 21:00	850	0.653720559	0.110970132	0.764690691	1.230647316	0.025474566	0.001294255	0.026768821	0.034080134	0.02342735	0.001132016	0.024559366	0.03952437
L148	LD	250	20:00 ~ 21:00	250	0.356429919	0.088150532	0.444580451	0.715481103	0.008051683	0.001076026	0.009127709	0.014698587	0.007420339	0.00		

Table 5: Hourly composite emission rate for each road link

L180	PD		70	20:00 ~ 21:00	750	0.648844885	0.111266106	0.760110991	1.223277023	0.02650844	0.001553201	0.028061641	0.045160721	0.024377212	0.001357446	0.025734658	0.041415814
L181	PD		70	20:00 ~ 21:00	1400	0.319652532	0	0.319652532	0.514429605	0.007117897	0	0.007117897	0.01455117	0.00653789	0	0.00653789	0.010521688
L182	PD		70	20:00 ~ 21:00	2000	0.285847233	0	0.285847233	0.460025385	0.005634103	0	0.005634103	0.009067188	0.005175817	0	0.005175817	0.00832965
L183	PD		50	20:00 ~ 21:00	550	0.772005839	0.106662029	0.878667263	1.414075346	0.033594561	0.001451581	0.035046142	0.056401158	0.030890661	0.001267979	0.03216404	0.051762876
L184	PD		50	20:00 ~ 21:00	600	0.729137867	0.120167904	0.84930577	1.366821749	0.028979009	0.001469048	0.030448049	0.049001263	0.026647512	0.001269613	0.027971124	0.044928145
L185	DD		50	20:00 ~ 21:00	450	0.3297209	0.09243461	0.67938899	0.67938899	0.0069229	0.001448006	0.008370906	0.0035555	0.001277877	0	0.012283894	0.012283894
L186	DD		50	20:00 ~ 21:00	450	0.3340636	0.08959645	0.423623245	0.681753833	0.0054814	0.001013987	0.006495387	0.010453286	0.0050348	0.000905548	0.005940348	0.009560039
L187	LD		50	20:00 ~ 21:00	100	0.296003044	0.094072203	0.390075247	0.627736398	0.010310797	0.01553534	0.011864331	0.019093742	0.009506653	0.00139876	0.010905413	0.017550517
L188	LD		50	20:00 ~ 21:00	100	0.336056094	0.074038402	0.410094496	0.659981476	0.012060325	0.001156342	0.013216668	0.021270112	0.011128403	0.001077125	0.012205528	0.019642845
L189	LD		50	20:00 ~ 21:00	50	0.175370564	0.081062397	0.256432961	0.412687822	0.007162973	0.00259719	0.009760163	0.015707421	0.006598308	0.002291341	0.008889649	0.014306468
L190	LD		50	20:00 ~ 21:00	50	0.171892425	0.058359174	0.230251599	0.370553108	0.006614232	0.001914983	0.008529215	0.013726408	0.00611215	0.001719281	0.007831431	0.012603435
L191	LD		50	20:00 ~ 21:00	100	0.137166939	0.033351731	0.170518669	0.274422515	0.00598993	0.001374308	0.007364238	0.011851562	0.005556264	0.001279854	0.006836118	0.01001638
L192	LD		50	20:00 ~ 21:00	50	0.184122068	0.035796396	0.219918464	0.353923581	0.006228503	0.001231676	0.007460178	0.012005964	0.005772143	0.001146198	0.006918341	0.011133964
L193	LD		50	20:00 ~ 21:00	50	0.161295713	0.035156473	0.196452185	0.31615836	0.006097835	0.001302125	0.00739996	0.011909052	0.005652872	0.001212592	0.006865465	0.011048867
L194	LD		50	20:00 ~ 21:00	100	0.143275604	0.031521603	0.174797207	0.281308137	0.006500738	0.001392354	0.007893091	0.012702667	0.006027439	0.001296669	0.007324108	0.01178698
L195	LD		50	20:00 ~ 21:00	200	0.194345047	0.083886925	0.278231972	0.447769843	0.006646371	0.002496247	0.009142618	0.014713581	0.006122335	0.002197657	0.008319991	0.013389695
L196	LD		50	20:00 ~ 21:00	150	0.182684514	0.056219711	0.238904225	0.384478125	0.006076231	0.001768005	0.007844236	0.012624043	0.005617557	0.001593682	0.007211239	0.011605335
L001	LD		50	21:00 ~ 22:00	150	0.32450723	0.073931781	0.398439011	0.641223838	0.005470761	0.001109173	0.006579934	0.010589351	0.005043733	0.000989874	0.006033607	0.009710125
L002	LD		50	21:00 ~ 22:00	200	0.524172744	0.10938804	0.633560784	1.019641712	0.009642509	0.000445488	0.010087997	0.016235018	0.008868671	0.000398079	0.00926675	0.014913351
L003	LD		50	21:00 ~ 22:00	50	0.228951357	0.038862542	0.267813899	0.343100362	0.006107812	0.001055153	0.007162965	0.011527646	0.005652219	0.000908956	0.006633175	0.010675034
L004	DD		50	21:00 ~ 22:00	700	0.8349384	0.064677849	0.899616249	1.447788414	0.0315617	0.000963077	0.032524777	0.052343424	0.0290259	0.000855486	0.02988186	0.04808931
L005	DD		50	21:00 ~ 22:00	600	1.0747613	0.0719851	1.146746481	1.845504981	0.042239	0.000898747	0.029937747	0.069101434	0.0388506	0.000621252	0.039471852	0.06352563
L006	LD		50	21:00 ~ 22:00	200	0.53943217	0.117190076	0.656622246	1.066728445	0.010936747	0.000660446	0.011597192	0.018663825	0.01005756	0.00058266	0.01064022	0.017123732
L007	LD		50	21:00 ~ 22:00	300	0.472766909	0.095485358	0.568252267	0.914511103	0.009786143	0.000755157	0.0105413	0.016964535	0.009007955	0.000675903	0.009683858	0.01558462
L008	LD		50	21:00 ~ 22:00	100	0.383493833	0.057122011	0.440615384	0.709100702	0.005803544	0.000891939	0.006495483	0.010453441	0.005351215	0.000626975	0.005978189	0.009620939
L009	LD		50	21:00 ~ 22:00	150	0.250845033	0.0510758374	0.317371329	0.510758374	0.006901259	0.001566688	0.008467946	0.013627805	0.006167322	0.001405338	0.00777266	0.012508453
L010	LD		50	21:00 ~ 22:00	50	0.25565817	0.132614883	0.388275053	0.224866573	0.003848353	0.003019125	0.006867678	0.011052428	0.003507124	0.002590748	0.009813548	0.012508453
L011	LD		50	21:00 ~ 22:00	100	0.373390012	0.053688588	0.4270786	0.687314674	0.006495161	0.000798796	0.007293958	0.011738458	0.005990504	0.000726922	0.006717426	0.010810622
L012	LD		50	21:00 ~ 22:00	50	0.177250336	0.03475358	0.212003916	0.341186382	0.006488318	0.001168867	0.007752685	0.012476706	0.006004773	0.001180851	0.007185624	0.011564112
L013	LD		50	21:00 ~ 22:00	100	0.299180392	0.053501073	0.352681465	0.567584389	0.004852679	0.000889025	0.005741704	0.009240354	0.004483515	0.000810999	0.005294514	0.008520673
L014	DD		800	21:00 ~ 22:00	800	0.8663636	0.090177441	0.95641041	1.539399758	0.0324031	0.000813766	0.033218866	0.05345723	0.0298013	0.000725526	0.009128042	0.049128042
L015	DD		300	21:00 ~ 22:00	300	1.6849317	0.049079676	1.734011376	2.790613867	0.0729467	0.000463414	0.073410114	0.118141832	0.0671007	0.000410231	0.067510931	0.108480401
L016	LD		50	21:00 ~ 22:00	100	0.307838883	0.060660295	0.368499179	0.593040468	0.002769418	0.000704317	0.003833736	0.00545581	0.002477131	0.000640012	0.006311742	0.005016542
L017	LD		50	21:00 ~ 22:00	100	0.373390012	0.053688588	0.4270786	0.687314674	0.006495161	0.000798796	0.007293958	0.011738458	0.005990504	0.000726922	0.006717426	0.010810622
L018	LD		50	21:00 ~ 22:00	150	0.250845033	0.0510758374	0.317371329	0.510758374	0.006901259	0.001566688	0.008467946	0.013627805	0.006167322	0.001405338	0.00777266	0.012508453
L019	LD		50	21:00 ~ 22:00	150	0.322119868	0.086015324	0.408135192	0.65682829	0.008957061	0.002274257	0.011231318	0.018075009	0.008237455	0.001989925	0.01022738	0.016459331
L020	LD		50	21:00 ~ 22:00	250	0.319413488	0.071574745	0.390988232	0.629233002	0.005442321	0.001371911	0.006814232	0.010966417	0.005014439	0.001213687	0.006228127	0.010023174
L021	DD		700	21:00 ~ 22:00	700	0.9335947	0.088859368	1.022454068	1.64547623	0.035432	0.000762298	0.036194298	0.058248931	0.0325278	0.000677541	0.033265341	0.053535243
L022	DD		400	21:00 ~ 22:00	400	1.4468281	0.101557041	1.548385141	2.491878144	0.0607789	0.000456189	0.0061235089	0.098548078	0.0559074	0.000408801	0.056316201	0.009631915
L023	LD		50	21:00 ~ 22:00	200	0.363781243	0.060707538	0.42448878	0.683146774	0.006620128	0.000970057	0.007590184	0.012215187	0.006102503	0.000870409	0.006972912	0.011221787
L024	LD		50	21:00 ~ 22:00	150	0.224468238	0.05638574	0.280853978	0.45198954	0.006950692	0.001458645	0.008409337	0.013534383	0.006421272	0.001326124	0.007747396	0.012468194
L025	DD		50	21:00 ~ 22:00	50	0.2762194	0.019760438	0.295979838	0.476332192	0.0127252	0.00148825	0.01421345	0.022874273	0.0117057	0.001384524	0.013090224	0.010266621
L026	DD		50	21:00 ~ 22:00	50	0.2501992	0.019760438	0.295979838	0.434456844	0.0119496	0.00148825	0.01343785	0.021626069	0.0109896	0.001384524	0.012374124	0.019914173
L027	DD		50	21:00 ~ 22:00	250	0.4375472	0.108098656	0.545645856	0.878129702	0.0104143	0.000907322	0.011321622	0.018220339	0.0095726	0.000811839	0.010384439	0.016712093
L028	DD		50	21:00 ~ 22:00	200	0.4769791	0.120755532	0.59774632	0.962022626	0.0116755	0.000845153	0.012520653	0.020149988	0.0107329	0.000753647	0.011486577	0.018485747
L029	LD		50	21:00 ~ 22:00	50	0.159233804	0.031070418	0.190304221	0.306264196	0.006994647	0.001410399	0.008405047	0.013526578	0.006479111	0.001313484	0.007792595	0.012540935
L030	DD		900	21:00 ~ 22:00	900	0.8419093	0.092774703	0.934684003	1.504224353	0.031354	0.000893663	0.032247663	0.051897454	0.028835	0.000792885	0.029627883	0.04768134
L031	DD		750	21:00 ~ 22:00	750	0.2060087	0.09653365	0.30254235	0.486893506	0.0018743	0.000708389	0.002582689	0.004156425	0.0017182	0.000629694	0.002347894	0.003778599
L032	DD		350	21:00 ~ 22:00	350	1.4103509	0.127151777	1.537502677	2.474364558	0.060102	0.000799732	0.008011593	0.0552798	0.000703547	0.005598347	0.009096239	0.010281833
L033	LD		50	21:00 ~ 22:00	150	0.223329085	0.087924719	0.311253804	0.500913197	0.007250661	0.002537972	0.009788633	0.015753239	0.006672876	0.002225055	0.008897931	0.014319796
L034	LD		50	21:00 ~ 22:00	150	0.217694032	0.068688479	0.286382511	0.46088683	0.00670487	0.001998598	0.008703468	0.				

Table 5: Hourly composite emission rate for each road link

L066	DD	50	21:00 ~ 22:00	350	1.1914525	0.105601758	1.297054258	2.0874013	0.0482439	0.000594751	0.048838651	0.078597995	0.0443752	0.000530616	0.044905816	0.072268726
L066	DD	50	21:00 ~ 22:00	450	1.2177722	0.09000398	1.307772598	2.104650753	0.050409	0.000667025	0.051076025	0.08219869	0.0463666	0.000596049	0.046962649	0.07557887
L068	DD	50	21:00 ~ 22:00	700	0.8289319	0.113002155	0.941934055	0.515892151	0.0296725	0.000804859	0.030477359	0.049048433	0.0272884	0.000712923	0.028001323	0.045063649
L069	LD	50	21:00 ~ 22:00	300	0.362647281	0.073518541	0.436165822	0.70139104	0.0066247	0.001096198	0.007720898	0.01242555	0.006104329	0.000978034	0.007082363	0.011397931
L070	LD	50	21:00 ~ 22:00	300	0.346661783	0.072462241	0.419124024	0.674513058	0.005823452	0.00102431	0.006847761	0.011020376	0.005367371	0.000916349	0.00628372	0.010112642
L071	LD	50	21:00 ~ 22:00	150	0.173509419	0.064847834	0.382357253	0.615342821	0.00378197	0.000891324	0.004269521	0.00687111	0.003117351	0.000797984	0.003915335	0.006301105
L072	LD	50	21:00 ~ 22:00	150	0.293205577	0.062140721	0.353462984	0.571873011	0.003733597	0.000999598	0.007617321	0.003449132	0.000898876	0.004348008	0.006997424	0.009914254
L073	LD	50	21:00 ~ 22:00	150	0.334104143	0.066953366	0.401057509	0.645437892	0.002898497	0.000807111	0.003705607	0.005963582	0.002674917	0.000719513	0.00339443	0.005462791
L074	LD	50	21:00 ~ 22:00	150	0.292821637	0.062140721	0.354962358	0.571255121	0.003740353	0.000999598	0.004739951	0.007628193	0.003455404	0.000898876	0.00435428	0.007007518
L075	DD	50	21:00 ~ 22:00	600	0.3538738	0.082274948	0.436148748	0.701911627	0.0061191	0.000847593	0.006966693	0.011211777	0.005623	0.000762225	0.006385225	0.010275597
L076	DD	50	21:00 ~ 22:00	250	0.6399004	0.098655354	0.738555754	1.188587317	0.0217317	0.000789135	0.022520835	0.03624368	0.01199869	0.000715339	0.020702239	0.033316941
L077	LD	50	21:00 ~ 22:00	250	0.365123899	0.076344901	0.4414688	0.710473399	0.007362776	0.001163073	0.008525849	0.01372099	0.006782608	0.001038328	0.007820936	0.012586546
L078	DD	50	21:00 ~ 22:00	350	1.5665868	0.083766242	1.650353042	2.655979164	0.0683707	0.000583787	0.068954487	0.110971214	0.0628908	0.000520613	0.063411413	0.102050524
L079	DD	50	21:00 ~ 22:00	650	0.8487119	0.114392856	0.963104756	1.549963008	0.0305657	0.000772214	0.031337914	0.050433358	0.0281103	0.000684663	0.028794963	0.046340886
L080	DD	50	21:00 ~ 22:00	350	1.6451742	0.085156463	1.730330663	2.78469035	0.0722849	0.000562758	0.072847658	0.117236651	0.0664916	0.000501126	0.066992726	0.107814074
L081	DD	50	21:00 ~ 22:00	650	0.8542414	0.11768947	0.97193087	1.564167227	0.0308507	0.000843989	0.031694689	0.051007531	0.0283713	0.000744203	0.029115503	0.046856744
L082	LD	50	21:00 ~ 22:00	50	0.193722577	0.097779943	0.291502519	0.469126664	0.006049639	0.002773049	0.008822688	0.014198704	0.005559292	0.002424144	0.007983436	0.012848063
L083	LD	50	21:00 ~ 22:00	50	0.192874197	0.089777146	0.278851342	0.448766619	0.005875647	0.002449991	0.008325638	0.013398782	0.005049709	0.002154929	0.007564638	0.012174075
L084	LD	50	21:00 ~ 22:00	50	0.0313784	0.023828543	0.052069435	0.088846741	0.0067556	0.001804571	0.008560171	0.013776226	0.006272	0.001681532	0.007935332	0.012799938
L085	LD	50	21:00 ~ 22:00	50	0.0313784	0.023828543	0.052069435	0.088846741	0.0067556	0.001804571	0.008560171	0.013776226	0.006272	0.001681532	0.007935332	0.012799938
L086	LD	50	21:00 ~ 22:00	50	0.152125748	0.108070885	0.257196633	0.413911629	0.006776541	0.003276564	0.010053104	0.016778863	0.006228415	0.000989927	0.014628783	0.018037312
L087	LD	50	21:00 ~ 22:00	400	0.422869616	0.107488436	0.530380852	0.853516428	0.010884036	0.001344638	0.012228674	0.019690095	0.0101192364	0.011207894	0.011097312	0.018037312
L088	LD	50	21:00 ~ 22:00	50	0.266214786	0.092793221	0.359008006	0.577765945	0.005211091	0.002182002	0.007839093	0.011811096	0.004789486	0.001854143	0.006643629	0.010691858
L089	LD	50	21:00 ~ 22:00	200	0.309262246	0.064014624	0.37327687	0.600729399	0.004464855	0.002578604	0.005544219	0.008925374	0.004122124	0.000961519	0.005081831	0.00811831
L090	LD	50	21:00 ~ 22:00	350	0.63778408	0.162001648	0.799785728	1.287127164	0.018728617	0.000684026	0.019412643	0.031241542	0.017227496	0.000618026	0.017845522	0.028719513
L091	LD	50	21:00 ~ 22:00	50	0.226072191	0.048786919	0.27485911	0.44234176	0.006875539	0.00144756	0.008333099	0.013394696	0.006352884	0.00135543	0.00766844	0.012341111
L092	LD	50	21:00 ~ 22:00	300	0.696186833	0.190223636	0.888210469	1.429432637	0.02074688	0.000681557	0.021428437	0.03448564	0.019075745	0.000607697	0.019683447	0.03167735
L093	LD	50	21:00 ~ 22:00	100	0.271019402	0.073460344	0.344479746	0.554385035	0.00559572	0.001678089	0.00727381	0.011706033	0.005155661	0.001483006	0.006368668	0.010638174
L094	LD	50	21:00 ~ 22:00	250	0.748672397	0.198640653	0.947313051	1.524548785	0.023930719	0.000543852	0.024474571	0.039387907	0.022008546	0.000499657	0.022508203	0.036223351
L095	LD	50	21:00 ~ 22:00	400	0.412735338	0.099368662	0.506672199	0.815407837	0.010288895	0.000821347	0.01780157	0.009480325	0.000757204	0.010237529	0.016475665	0.030596257
L096	LD	50	21:00 ~ 22:00	250	0.742500906	0.195907931	0.938408836	1.510218877	0.023701543	0.000557155	0.024258698	0.039040494	0.017988616	0.000512353	0.022311169	0.035906277
L097	LD	50	21:00 ~ 22:00	300	0.683785712	0.186329202	0.870114913	1.400310735	0.022198202	0.000855527	0.023053728	0.037101287	0.020416248	0.00076993	0.02186178	0.034095764
L098	LD	50	21:00 ~ 22:00	50	0.183405783	0.078451869	0.261857652	0.421417994	0.000856768	0.002602858	0.011770525	0.007894687	0.002295119	0.010188906	0.016398863	0.016398863
L099	LD	50	21:00 ~ 22:00	400	0.420510012	0.100910791	0.521420803	0.839143355	0.011467223	0.001172403	0.012639626	0.020341456	0.010560059	0.001056297	0.011616356	0.018694671
L100	LD	50	21:00 ~ 22:00	50	0.1791953	0.040973592	0.220168892	0.354326605	0.004154694	0.001118834	0.005273528	0.0084689	0.00385728	0.00104255	0.00489983	0.00785492
L101	LD	50	21:00 ~ 22:00	100	0.252307494	0.063784854	0.316092348	0.508700059	0.006579086	0.001645714	0.0082248	0.013236499	0.006069104	0.001467995	0.0075371	0.012129756
L102	LD	50	21:00 ~ 22:00	50	0.222235786	0.071490142	0.293725928	0.472704886	0.007125483	0.002129767	0.009255249	0.014894483	0.006567192	0.001887604	0.008454796	0.013606641
L103	LD	50	21:00 ~ 22:00	150	0.251641808	0.055689826	0.307331634	0.494601092	0.005005895	0.001241152	0.006247047	0.010053623	0.004626428	0.00112371	0.005750138	0.009253927
L104	PD	50	21:00 ~ 22:00	750	0.704283102	0.116356739	0.82063984	1.32068852	0.027376863	0.000843954	0.028220817	0.04541689	0.025180726	0.000749301	0.025930027	0.041730029
L105	PD	50	21:00 ~ 22:00	600	0.822540838	0.156028263	0.978569101	1.574850397	0.032113748	0.009727972	0.033086721	0.042347783	0.029533525	0.00084237	0.030375962	0.048885251
L106	LD	50	21:00 ~ 22:00	100	0.37175582	0.144338953	0.516094773	0.830571962	0.012799409	0.002449108	0.015248517	0.024540049	0.011764111	0.002138199	0.01390231	0.022373543
L107	LD	50	21:00 ~ 22:00	300	0.310434376	0.060042824	0.370477139	0.596237776	0.005055882	0.001048665	0.006104547	0.009824291	0.004666731	0.000944347	0.005611078	0.009030132
L108	LD	50	21:00 ~ 22:00	200	0.43124464	0.085121358	0.516365998	0.831008455	0.007382717	0.000616922	0.007999639	0.012874139	0.006799024	0.000557824	0.007356848	0.01183967
L109	LD	50	21:00 ~ 22:00	150	0.308535335	0.059140453	0.367675788	0.591715352	0.005450538	0.001084756	0.006355294	0.010517511	0.005031379	0.000977977	0.006090357	0.009671098
L110	LD	50	21:00 ~ 22:00	150	0.300224477	0.097145651	0.397370128	0.639503642	0.0058625	0.002198652	0.008061152	0.012973134	0.00538353	0.001909621	0.007293151	0.011737159
L111	LD	50	21:00 ~ 22:00	150	0.277583278	0.058602694	0.336185972	0.541037532	0.005955008	0.00130355	0.007258558	0.011681488	0.005495694	0.001170497	0.006666191	0.010728167
L112	LD	50	21:00 ~ 22:00	350	0.440648498	0.102327936	0.542976434	0.873833695	0.011873587	0.00092989	0.012803477	0.020605148	0.010936166	0.000847557	0.011783723	0.018964017
L113	LD	50	21:00 ~ 22:00	350	0.421128668	0.102757331	0.523885999	0.843110693	0.01206326	0.00120255	0.013265811	0.021349199	0.011108034	0.001083338	0.012913172	0.019620062
L114	LD	50	21:00 ~ 22:00	200	0.295728594	0.062577911	0.358306506	0.576636992	0.007125494	0.001517002	0.008642495	0.013908713	0.00656988	0.001356764	0.007926645	0.012756666
L115	LD	50	21:00 ~ 22:00	50	0.18863683	0.028130674	0.216767504	0.348852615	0.009325545	0.001385643	0.010711188	0.017237943	0.008623324	0.00128741	0.009910734	0.015949741
L116	LD	50	21:00 ~ 22:00	50	0.156891326	0.02126828	0.178155154	0.286712235	0.010310476	0.001599358	0.011909833	0.019166971	0.009537068	0.001487305	0.011024373	0.017741965
L117	LD	50	21:00 ~ 22:00	50												

Table 5: Hourly composite emission rate for each road link

L148	LD	50	21:00 ~ 22:00	200	0.35961103	0.08432625	0.44393728	0.714446022	0.007032629	0.00091211	0.007944739	0.012785786	0.006480961	0.000825495	0.007306456	0.011758572
L149	LD	50	21:00 ~ 22:00	450	0.452973465	0.109263893	0.56237358	0.90483107	0.009248238	0.010103275	0.016259604	0.008509135	0.000760636	0.009269771	0.014918213	
L150	LD	50	21:00 ~ 22:00	200	0.477919969	0.12485479	0.602774759	0.97006953	0.010171184	0.000975363	0.011692547	0.018817284	0.009855767	0.000860197	0.010715965	0.017245631
L151	LD	50	21:00 ~ 22:00	500	0.714205929	0.098388014	0.812593943	1.307739936	0.022662325	0.009975641	0.023638876	0.038042989	0.020851949	0.000870039	0.021721988	0.034958065
L152	LD	50	21:00 ~ 22:00	450	0.746987387	0.09185853	0.838845917	1.349988288	0.024830171	0.000996214	0.025826385	0.041563435	0.022847656	0.000891938	0.023739593	0.038205077
L153	LD	50	21:00 ~ 22:00	200	0.440937556	0.092212752	0.533150309	0.858020118	0.011118876	0.011228372	0.019768681	0.010234184	0.001036758	0.011270942	0.011387788	
L154	LD	50	21:00 ~ 22:00	200	0.451049364	0.119062917	0.570112281	0.915750448	0.009306095	0.000982261	0.010282956	0.016548772	0.000855367	0.000806718	0.009420815	0.015162195
L155	LD	50	21:00 ~ 22:00	200	0.427516586	0.081166223	0.508682809	0.818643592	0.010580634	0.001201643	0.011782277	0.01896169	0.009742605	0.001071088	0.010813466	0.017402544
L156	LD	50	21:00 ~ 22:00	200	0.457322444	0.118972212	0.576299656	0.927462089	0.009083538	0.000864696	0.009948235	0.016010092	0.008354635	0.000765782	0.009120417	0.014677852
L157	LD	50	21:00 ~ 22:00	100	0.324555358	0.079776941	0.404332299	0.650708142	0.004530993	0.01425449	0.005956443	0.009585941	0.004168717	0.001247592	0.005416309	0.008716682
L158	LD	50	21:00 ~ 22:00	50	0.494720287	0.121880371	0.616600658	0.992320102	0.010045352	0.000722897	0.010768249	0.017329774	0.009235841	0.000641041	0.009876882	0.015895262
L159	PD	50	21:00 ~ 22:00	600	0.759510165	0.08083872	0.840348885	1.352407075	0.032717992	0.01188549	0.033906541	0.054567153	0.030092202	0.001045466	0.031137688	0.050111095
L160	PD	50	21:00 ~ 22:00	550	0.793544847	0.082321171	0.875866018	1.409566217	0.034435323	0.001096325	0.035531648	0.057182503	0.031672383	0.000956564	0.032638047	0.052525714
L161	PD	50	21:00 ~ 22:00	500	0.844728881	0.108818609	0.95354749	1.534582118	0.035203015	0.00105933	0.036262344	0.058358441	0.030918786	0.000918786	0.033294319	0.053581879
L162	LD	50	21:00 ~ 22:00	300	0.346681909	0.084443579	0.431125488	0.693827493	0.006037102	0.001071665	0.007108767	0.011440424	0.00556131	0.000955299	0.006516609	0.010487439
L163	LD	50	21:00 ~ 22:00	350	0.405012062	0.08875473	0.493766792	0.794638648	0.007172036	0.000856951	0.008028987	0.01292137	0.006603015	0.000765955	0.00736897	0.011859177
L164	LD	50	21:00 ~ 22:00	50	0.16690922	0.040071221	0.206980441	0.333101903	0.004370873	0.01154926	0.005525799	0.008982889	0.004057984	0.001076181	0.005134165	0.008262617
L165	LD	50	21:00 ~ 22:00	50	0.12734718	0.140882169	0.268229349	0.431672221	0.006345826	0.004278995	0.010624821	0.017098949	0.005810643	0.003700898	0.009511542	0.015307304
L166	LD	50	21:00 ~ 22:00	400	0.398483065	0.087648125	0.487131189	0.783959708	0.006835836	0.000853729	0.007689565	0.012375124	0.006294693	0.000762952	0.007057645	0.01358151
L167	LD	50	21:00 ~ 22:00	300	0.345015696	0.0428368403	0.083352707	0.1007075643	0.006003978	0.001071665	0.007075643	0.01387115	0.005530996	0.000955299	0.006485795	0.010438653
L168	DD	50	21:00 ~ 22:00	100	0.231275	0.295378468	0.054010468	0.032415	0.0032415	0.001334152	0.004575652	0.00736378	0.0029719	0.001195087	0.004166987	0.006706099
L169	DD	50	21:00 ~ 22:00	200	0.3905248	0.075504458	0.466029258	0.749999526	0.0095176	0.01407845	0.010925445	0.017582756	0.0087414	0.001239142	0.009980524	0.016062085
L170	LD	50	21:00 ~ 22:00	100	0.282448623	0.059805365	0.342253988	0.558002033	0.004451193	0.001083812	0.005539004	0.008907704	0.004109627	0.000977348	0.005068975	0.008186672
L171	LD	50	21:00 ~ 22:00	50	0.604422946	0.18412606	0.788549007	1.269034358	0.018632206	0.00909821	0.018537726	0.031434788	0.017134438	0.000807305	0.017941743	0.028874364
L172	LD	50	21:00 ~ 22:00	50	0.1849544	0.041875963	0.226830363	0.365047177	0.00405336	0.001082743	0.008265736	0.0037632	0.001080919	0.004772119	0.0106779963	
L173	LD	50	21:00 ~ 22:00	350	0.464893649	0.130870703	0.585764352	0.942694002	0.010505335	0.001051463	0.011556797	0.018598816	0.009662323	0.000929131	0.010591454	0.01704225
L174	LD	50	21:00 ~ 22:00	390	0.396062184	0.074859613	0.470921797	0.757873284	0.0089351	0.010204322	0.01642223	0.008227469	0.001127068	0.009354537	0.010505463	
L175	LD	50	21:00 ~ 22:00	250	0.35537215	0.062192777	0.417564927	0.672003939	0.006256012	0.001076807	0.007332819	0.01800999	0.005767479	0.000964307	0.006731786	0.010833732
L176	LD	50	21:00 ~ 22:00	150	0.4882339	0.097210457	0.585444357	0.942179021	0.013890422	0.01599125	0.015489546	0.024927947	0.01404248	0.014182878	0.024250272	
L177	LD	50	21:00 ~ 22:00	50	0.18759215	0.265226738	0.452818888	0.728739548	0.00673175	0.007256933	0.013988683	0.022512547	0.00607745	0.006157633	0.012235083	0.019690404
L178	LD	50	21:00 ~ 22:00	50	0.406213058	0.840812528	0.43459947	1.353153234	0.008056581	0.010671175	0.018727756	0.030139327	0.007171134	0.008923219	0.016094353	0.025901286
L179	LD	50	21:00 ~ 22:00	150	0.476752973	0.095329505	0.572082478	0.920675215	0.013112765	0.001587094	0.014699886	0.023657072	0.012061308	0.001393038	0.013454346	0.021652618
L180	PD	50	21:00 ~ 22:00	650	0.723212745	0.100772615	0.82398536	1.326072599	0.030217767	0.001455936	0.050623296	0.027791327	0.001086686	0.028878013	0.046474541	
L181	PD	70	21:00 ~ 22:00	1250	0.347464619	0	0.347464619	0.559188709	0.007962943	0	0.007962943	0.012815083	0.007318269	0	0.007318269	0.011775583
L182	PD	70	21:00 ~ 22:00	1800	0.301875511	0	0.301875511	0.485820334	0.005790319	0	0.005790319	0.009318593	0.005321635	0	0.005321635	0.00856432
L183	PD	50	21:00 ~ 22:00	500	0.855167116	0.095218506	0.950385622	1.529493596	0.037794943	0.0011371	0.038932043	0.052654894	0.034761948	0.000998143	0.035760091	0.057550146
L184	PD	50	21:00 ~ 22:00	550	0.792180731	0.106142528	0.898323258	1.445707553	0.032456364	0.001143615	0.033599979	0.040473791	0.029848659	0.00991761	0.03084042	0.049632721
L185	DD	50	21:00 ~ 22:00	400	0.3475469	0.085521673	0.433068573	0.696954578	0.0068298	0.001173645	0.008003445	0.012880265	0.0062722	0.001040983	0.007313183	0.011769399
L186	DD	50	21:00 ~ 22:00	400	0.3442966	0.086603799	0.693466181	0.430900979	0.0051085	0.000862752	0.009697774	0.0046929	0.00077136	0.00546426	0.008793852	
L187	LD	50	21:00 ~ 22:00	100	0.347032283	0.088193711	0.453225994	0.700426602	0.011153606	0.001286565	0.012440171	0.020020464	0.010281621	0.00116515	0.011446771	0.018421746
L188	LD	50	21:00 ~ 22:00	100	0.314596251	0.075641697	0.390237949	0.62802554	0.01059555	0.011627081	0.018711926	0.009743661	0.009939993	0.010737654	0.017280537	
L189	LD	50	21:00 ~ 22:00	50	0.194874467	0.095572307	0.290446774	0.467427612	0.007130845	0.002811974	0.009942819	0.016001376	0.006560074	0.002459664	0.009019738	0.014515825
L190	LD	50	21:00 ~ 22:00	50	0.195162846	0.060169316	0.255326762	0.410907572	0.006481083	0.0018428	0.03395958	0.005982465	0.001652019	0.007634484	0.010226488	
L191	LD	50	21:00 ~ 22:00	50	0.180817615	0.034679902	0.215497517	0.346808773	0.00639846	0.001266034	0.007664493	0.012334775	0.00592689	0.001178962	0.010145372	
L192	LD	50	21:00 ~ 22:00	50	0.236447806	0.037124567	0.440270963	0.6064965	0.001123402	0.007973052	0.012831352	0.006384348	0.001045306	0.007383744	0.011882955	
L193	LD	50	21:00 ~ 22:00	50	0.181604555	0.036558322	0.218168277	0.351098245	0.005873505	0.001196665	0.00707019	0.011378339	0.005446115	0.00111359	0.006555205	0.010549553
L194	LD	50	21:00 ~ 22:00	100	0.164624921	0.033326346	0.179751267	0.318570891	0.006358762	0.001320171	0.007678932	0.012358013	0.005891968	0.001229408	0.007121375	0.014671014
L195	LD	50	21:00 ~ 22:00	150	0.219049646	0.072622744	0.29167239	0.469400045	0.006799782	0.002106284	0.008906066	0.014332888	0.006266353	0.001865472	0.008131826	0.013068872
L196	LD	50	21:00 ~ 22:00	150	0.21143718	0.053520802	0.264957982	0.426407479	0.006193239	0.001577995	0.007771234	0.012506558	0.005722609	0.001426731	0.00714934	0.01150572
L001	LD	50	22:00 ~ 23:00	150	0.411443013	0.081837729	0.493280742	0.79385643	0.007368674	0.001098981	0.008468566	0.013628802	0.006780944	0.000971029	0.007751973	0.010247556
L002	LD	50	22:00 ~ 23:00	200	0.583133026	0.117568601	0.700701627	1.127667156	0.011302018	0.000465595	0.011767613	0.01893809	0.010392935	0.000409168	0.010802102	0.017384256
L003	LD	50	22:00 ~ 23:00	50	0.209821465	0.044583076	0.254404541	0.409423405	0.003542531	0.000974468	0.004517	0.007269388	0.003287544	0.000908027		

Table 5: Hourly composite emission rate for each road link

L034	LD	50	22:00 ~ 23:00	150	0.227759182	0.088462478	0.31622166	0.508908166	0.005705307	0.002319178	0.008024485	0.012914125	0.005246125	0.002032536	0.007278661	0.01171384
L035	LD	50	22:00 ~ 23:00	50	0.0311496	0.023828543	0.054978143	0.088478524	0.0064644	0.001804571	0.008450971	0.013600486	0.006168	0.001681532	0.007849532	0.012632566
L036	LD	50	22:00 ~ 23:00	50	0.0311496	0.023828543	0.054978143	0.088478524	0.0064644	0.001804571	0.008450971	0.013600486	0.006168	0.001681532	0.007849532	0.012632566
L037	LD	150	22:00 ~ 23:00	150	0.227320092	0.0966318	0.323952092	0.52134906	0.005710495	0.002525219	0.008233015	0.01324972	0.005248117	0.002200802	0.007448919	0.011987844
L038	LD	150	22:00 ~ 23:00	150	0.227601022	0.084575027	0.31217603	0.502397371	0.00566851	0.002211125	0.007879636	0.012681013	0.005217968	0.001942457	0.007160424	0.011523557
L039	LD	50	22:00 ~ 23:00	50	0.1841876	0.041875963	0.226063563	0.363813135	0.00398784	0.001082743	0.005070583	0.008160292	0.0037008	0.001008919	0.004709719	0.00757954
L040	LD	50	22:00 ~ 23:00	50	0.179979055	0.040973592	0.220952647	0.355587933	0.00460905	0.001118834	0.005179785	0.008336034	0.003768648	0.00104255	0.004811198	0.007742853
L041	LD	50	22:00 ~ 23:00	50	0.206819835	0.0637734	0.270593235	0.435476518	0.005566738	0.001698435	0.007265173	0.011692133	0.005140336	0.001517497	0.006657833	0.010714717
L042	LD	50	22:00 ~ 23:00	50	0.255675877	0.039191757	0.294867634	0.47452278	0.006822366	0.001085576	0.007907942	0.012728568	0.006308178	0.001010809	0.007318987	0.011778738
L043	LD	150	22:00 ~ 23:00	150	0.537120274	0.073564907	0.160685181	0.982800089	0.011009974	0.00102613	0.012036105	0.019370185	0.010130014	0.009092114	0.011032127	0.017544444
L044	LD	200	22:00 ~ 23:00	200	0.513808748	0.126888065	0.640696813	1.031099009	0.01596305	0.001090768	0.014687073	0.023636494	0.01250678	0.000967784	0.013474564	0.021685155
L045	LD	100	22:00 ~ 23:00	100	0.238645377	0.065493011	0.304138388	0.489462073	0.00563248	0.001676138	0.007308619	0.011762052	0.005195982	0.001497848	0.00669383	0.010772648
L046	LD	100	22:00 ~ 23:00	100	0.21012453	0.045154239	0.251667822	0.405019093	0.004711097	0.001104835	0.005760032	0.00926985	0.004654566	0.000976667	0.005342132	0.008597307
L047	DD	850	22:00 ~ 23:00	850	1.0751667	0.99970954	1.174896654	1.890883181	0.0423792	0.000892897	0.043271887	0.069693178	0.0389763	0.000782599	0.039758999	0.063985586
L048	DD	300	22:00 ~ 23:00	300	1.8679695	0.133723942	2.001693442	3.221405324	0.0062573	0.000674911	0.083302211	0.13406158	0.00760016	0.000586176	0.076587776	0.123255772
L049	DD	350	22:00 ~ 23:00	350	0.3697124	0.474780282	0.444492682	0.715339853	0.0056979	0.000932952	0.010671296	0.006270852	0.0052344	0.000827806	0.009755828	0.039755828
L050	DD	300	22:00 ~ 23:00	300	0.679369	0.105280435	0.784649435	1.262767722	0.0233885	0.001166895	0.024567195	0.039536969	0.0215143	0.001030009	0.022544309	0.036281459
L051	DD	350	22:00 ~ 23:00	350	0.3731763	0.072777349	0.450453649	0.724933076	0.005552	0.000953076	0.010468879	0.0051	0.000841919	0.009562568	0.009562568	0.009562568
L052	DD	250	22:00 ~ 23:00	250	0.7517253	0.105432054	0.857157354	1.379457617	0.0266003	0.000912789	0.027513089	0.044277914	0.0244627	0.000811717	0.025274417	0.040675131
L053	DD	450	22:00 ~ 23:00	450	0.3961417	0.09897045	0.976803787	0.0078869	0.00094273	0.00862963	0.013880009	0.0070645	0.000838671	0.007903171	0.012718889	0.012718889
L054	DD	250	22:00 ~ 23:00	250	0.7732207	0.10483087	0.87805157	1.413083514	0.0270383	0.000815221	0.027853521	0.044825786	0.0248675	0.000727111	0.025594611	0.041190431
L055	LD	100	22:00 ~ 23:00	100	0.206469661	0.06552985	0.271999511	0.437739692	0.004872331	0.001677555	0.006549886	0.010540994	0.004498266	0.0005997059	0.004498266	0.009651306
L056	LD	50	22:00 ~ 23:00	50	0.206281988	0.052373496	0.258655485	0.416246418	0.005038246	0.001408634	0.006446881	0.010375223	0.004656898	0.001280023	0.005936921	0.009554524
L057	DD	450	22:00 ~ 23:00	450	0.4004684	0.097735844	0.498204244	0.801780018	0.0074496	0.000910334	0.008359934	0.013453977	0.0068458	0.000808567	0.007654367	0.012318479
L058	DD	300	22:00 ~ 23:00	300	0.77069	0.103904047	0.874594047	1.407519184	0.0265762	0.000799834	0.027376034	0.044057346	0.0244428	0.000712766	0.025155566	0.040483858
L059	LD	100	22:00 ~ 23:00	100	0.447748982	0.05129827	0.803136704	0.05129827	0.001318872	0.015439316	0.024847109	0.013013843	0.001195355	0.012409198	0.022867431	0.022867431
L060	LD	100	22:00 ~ 23:00	100	0.424106888	0.070344569	0.494451457	0.795740507	0.013196344	0.001802042	0.014998386	0.024137503	0.012151028	0.001598233	0.01374926	0.022127235
L061	LD	50	22:00 ~ 23:00	50	0.055629361	0.061944047	0.117573408	0.06643898	0.00266547	0.009309369	0.01498194	0.006139114	0.002388285	0.0008527399	0.013723484	0.013723484
L062	LD	50	22:00 ~ 23:00	50	0.12815563	0.058262589	0.18641822	0.300010298	0.005262168	0.00201759	0.007279758	0.011715605	0.004864493	0.001816394	0.006680887	0.01075819
L063	LD	50	22:00 ~ 23:00	50	0.2224471	0.046387819	0.268834919	0.432646788	0.0032322	0.000902286	0.004225486	0.006800243	0.003084	0.000840766	0.003924766	0.006316283
L064	LD	50	22:00 ~ 23:00	50	0.239004516	0.16090289	0.399907406	0.643586984	0.006408988	0.004174623	0.010584521	0.017034092	0.005849571	0.003570324	0.009418955	0.015159813
L065	DD	450	22:00 ~ 23:00	450	1.5239409	0.092119759	1.616060659	2.600791061	0.0651155	0.000623004	0.065738504	0.105795604	0.0059848	0.000548262	0.060443062	0.097273438
L066	DD	400	22:00 ~ 23:00	400	1.4639217	0.108544458	1.572466158	2.530632687	0.0612757	0.000590274	0.061865974	0.099563386	0.0563629	0.000517674	0.056880574	0.091540182
L067	DD	450	22:00 ~ 23:00	450	1.5043326	0.092556359	2.569372777	0.0642321	0.000670929	0.005693029	0.10451041	0.0590815	0.00589385	0.0059670885	0.096030742	0.096030742
L068	DD	700	22:00 ~ 23:00	700	1.0033697	0.120741548	1.124111248	1.809077196	0.037512	0.000846037	0.038417237	0.061826396	0.0345531	0.000737901	0.035291001	0.05679522
L069	LD	300	22:00 ~ 23:00	300	0.39145598	0.08536213	0.476792193	0.767320748	0.006587047	0.001207105	0.007794152	0.012543441	0.006059902	0.001060452	0.007120354	0.01145907
L070	LD	250	22:00 ~ 23:00	250	0.402677959	0.085317735	0.48795694	0.785350991	0.00675451	0.001124196	0.007899647	0.012713218	0.006234374	0.000989565	0.007223939	0.011625774
L071	LD	150	22:00 ~ 23:00	150	0.369599951	0.067530388	0.437130339	0.004706458	0.000782105	0.004858563	0.007819081	0.003754806	0.004451269	0.0007163605	0.007163605	0.007163605
L072	LD	150	22:00 ~ 23:00	150	0.391959578	0.069245536	0.461205114	0.742235838	0.0062123	0.000996487	0.007208787	0.011601389	0.00572075	0.000885439	0.006066188	0.010636103
L073	LD	150	22:00 ~ 23:00	150	0.337962544	0.074623778	0.412586322	0.663991671	0.00198329	0.000816664	0.002799954	0.004506078	0.001825418	0.00071831	0.002543728	0.004093723
L074	LD	100	22:00 ~ 23:00	100	0.298967031	0.071560721	0.370527753	0.596305133	0.002741972	0.01063466	0.003805438	0.006124244	0.00252736	0.000942857	0.003470218	0.00558476
L075	DD	550	22:00 ~ 23:00	550	0.3812093	0.092582559	0.76249219	0.0061482	0.000862873	0.007011073	0.0112832	0.0056487	0.00067507	0.006414407	0.010322962	0.010322962
L076	DD	250	22:00 ~ 23:00	250	0.8100382	0.106467872	0.916506072	1.474698882	0.0286758	0.000735606	0.029411406	0.047332953	0.0263749	0.000659438	0.027034338	0.043507441
L077	LD	200	22:00 ~ 23:00	200	0.432225595	0.08464594	0.517090189	0.832173925	0.00873215	0.001161801	0.00893952	0.015922732	0.008035107	0.001026208	0.009061316	0.014582738
L078	DD	350	22:00 ~ 23:00	350	1.0930970	0.080313608	1.989621508	3.201977477	0.0852961	0.000484205	0.085780305	0.138049676	0.0784607	0.000428251	0.078888951	0.12699144
L079	DD	650	22:00 ~ 23:00	650	1.0348357	0.12225784	1.862156918	1.15709355	0.0391128	0.00082313	0.03993593	0.064270489	0.0359714	0.000717555	0.036688955	0.09045003
L080	DD	350	22:00 ~ 23:00	350	1.9988851	0.081942543	2.071827643	3.334275099	0.0894137	0.000514889	0.089946589	0.144754643	0.0822648	0.000452297	0.082717097	0.133119932
L081	DD	650	22:00 ~ 23:00	650	1.0400114	0.124974541	1.874858474	1.164985941	0.0394028	0.000872831	0.040275631	0.064817184	0.036237	0.000758972	0.036959972	0.036959972
L082	LD	50	22:00 ~ 23:00	50	0.230095474	0.137297296	0.36739277	0.591259881	0.005857296	0.003528507	0.009385803	0.015104949	0.005957387	0.003031894	0.008389281	0.013501205
L083	LD	50	22:00 ~ 23:00	50	0.228711077	0.112789331	0.341504008	0.549592066	0.00669218	0.002918483	0.008577701	0.013804439	0.005187524	0.002527095	0.007714619	0.012415446
L084	LD	50	22:00 ~ 23:00	50	0.0311496	0.023828543	0.054978143	0.088478524	0.0064644	0.001804571	0.008450971	0.013600486	0.006168	0.001681532	0.007849532	0.012632566
L085	LD	50	22:00 ~ 23:00	50</												

Table 5: Hourly composite emission rate for each road link

L116	LD	50	22:00 ~ 23:00	50	0.083534495	0.022619863	0.106154358	0.170838455	0.00836608	0.001704798	0.010070878	0.016207467	0.00774745	0.001586308	0.009333758	0.01052119
L117	LD	50	22:00 ~ 23:00	50	0.350234985	0.071036478	0.421271463	0.677969016	0.010876521	0.001988235	0.012864756	0.020703766	0.010016497	0.00175497	0.011771467	0.018944293
L118	PD	850	22:00 ~ 23:00	850	0.82583109	0.107856221	0.933688201	1.50262177	0.034342919	0.001092468	0.035435387	0.057027586	0.031580543	0.000949939	0.032530482	0.052352606
L119	PD	600	22:00 ~ 23:00	600	1.000260069	0.165459155	1.165719224	1.876038577	0.042059569	0.001313455	0.043373024	0.069801942	0.038673797	0.001117465	0.039791262	0.064037667
L120	LD	100	22:00 ~ 23:00	100	0.559516207	0.158713769	0.718229976	1.155876229	0.017029575	0.001298595	0.018328127	0.02946258	0.015661256	0.001145671	0.016806927	0.027048059
L121	LD	150	22:00 ~ 23:00	150	0.388355258	0.072491654	0.460846912	0.741659369	0.006277118	0.001116203	0.007393322	0.011898368	0.005777925	0.000986387	0.006764313	0.010886979
L122	LD	300	22:00 ~ 23:00	300	0.450432835	0.086743198	0.537176033	0.864498877	0.007477162	0.000740154	0.008217315	0.013224454	0.006881153	0.00065703	0.007538182	0.01213149
L123	LD	100	22:00 ~ 23:00	100	0.519601895	0.142092157	0.661694053	1.064890706	0.015528168	0.001173158	0.016701326	0.026878111	0.014284703	0.001044694	0.015329397	0.024670211
L124	LD	50	22:00 ~ 23:00	50	0.174622725	0.040973592	0.215596317	0.346967777	0.004154	0.001118834	0.005272834	0.008485783	0.003855	0.00104255	0.00489755	0.007881823
L125	LD	50	22:00 ~ 23:00	50	0.174622725	0.040973592	0.215596317	0.346967777	0.004154	0.001118834	0.005272834	0.008485783	0.003855	0.00104255	0.00489755	0.007881823
L126	PD	600	22:00 ~ 23:00	600	0.997451221	0.163039628	1.160490849	1.867624343	0.041872535	0.001256749	0.043129284	0.069409682	0.038502411	0.001069866	0.039572277	0.063685248
L127	LD	50	22:00 ~ 23:00	50	0.1640561	0.037861586	0.203919686	0.328176108	0.00413884	0.001193851	0.008742338	0.005432251	0.00398984	0.0011117	0.008062955	0.008062955
L128	LD	250	22:00 ~ 23:00	250	0.463018841	0.098860721	0.560879562	0.902645914	0.009989022	0.000998284	0.010958307	0.017636641	0.009192509	0.000862794	0.010055304	0.016182402
L129	LD	250	22:00 ~ 23:00	250	0.480291755	0.094624997	0.574916752	0.925326526	0.007939168	0.000740117	0.008679285	0.01396792	0.00730226	0.000650668	0.007952929	0.012798967
L130	LD	250	22:00 ~ 23:00	250	0.506402286	0.099236407	0.605638693	0.974678574	0.007701561	0.000703672	0.008405233	0.013526878	0.007079601	0.000610345	0.007689947	0.012375739
L131	LD	200	22:00 ~ 23:00	200	0.462018841	0.098860721	0.560879562	0.902645914	0.009989022	0.000998284	0.010958307	0.017636641	0.009192509	0.000862794	0.010055304	0.016182402
L132	PD	900	22:00 ~ 23:00	900	0.794565431	0.150508636	0.945074067	1.520945498	0.030535359	0.001541794	0.032077153	0.051623045	0.028070706	0.001312198	0.029382904	0.047287083
L133	DD	500	22:00 ~ 23:00	500	0.6997842	0.082410395	0.782194595	1.25881705	0.024	0.000988968	0.024898968	0.040215745	0.0220698	0.000876114	0.022945914	0.036927777
L134	DD	350	22:00 ~ 23:00	350	0.8733566	0.084957378	0.958313978	1.542253018	0.0321802	0.000771788	0.032951988	0.053030952	0.029597	0.000686452	0.030283452	0.048736371
L135	LD	100	22:00 ~ 23:00	100	0.327947198	0.067262618	0.395209817	0.636026967	0.002924691	0.000813126	0.003737817	0.006015418	0.002695474	0.000725118	0.003420591	0.005508494
L136	PD	600	22:00 ~ 23:00	600	0.937014418	0.088757973	1.025773148	1.650817757	0.042520529	0.001050097	0.043500356	0.038852594	0.001084736	0.003993733	0.064272742	0.064272742
L137	PD	1000	22:00 ~ 23:00	1000	0.74832241	0.111536845	0.859859255	1.383805894	0.029829117	0.001064013	0.03089313	0.04971755	0.027428541	0.000927154	0.028356596	0.045639951
L138	LD	200	22:00 ~ 23:00	200	0.463182436	0.095564387	0.558746822	0.899213611	0.008841331	0.000797583	0.009638914	0.01551229	0.008135783	0.000710941	0.008486725	0.014237388
L139	LD	200	22:00 ~ 23:00	200	0.549844478	0.109746331	0.659590809	1.061505872	0.009123714	0.000599347	0.009723062	0.015647712	0.008385064	0.00051808	0.008903144	0.014328186
L140	LD	100	22:00 ~ 23:00	100	0.363656155	0.103498806	0.467154962	0.751811166	0.008544661	0.001225878	0.009770539	0.015724119	0.007865676	0.001094195	0.008959872	0.01441948
L141	PD	900	22:00 ~ 23:00	900	0.812478537	0.111469765	0.923948302	1.48694696	0.033321961	0.001112937	0.034434898	0.055417459	0.030640624	0.000963887	0.031604511	0.050862404
L142	LD	100	22:00 ~ 23:00	100	0.419543213	0.095551342	0.515094855	0.828962271	0.007842182	0.000856823	0.008999006	0.007219416	0.007798527	0.000766211	0.012851589	0.012851589
L143	LD	250	22:00 ~ 23:00	250	0.457807963	0.107251916	0.565059878	0.909373465	0.00610583	0.000875777	0.008981607	0.014454459	0.007453954	0.000771017	0.00822497	0.013236774
L144	LD	150	22:00 ~ 23:00	150	0.524590149	0.152007285	0.676597434	1.088875315	0.011899141	0.001044066	0.012943207	0.010933568	0.000908997	0.011842565	0.009089977	0.019058713
L145	LD	50	22:00 ~ 23:00	50	0.300031301	0.075907117	0.375938418	0.605012733	0.003507522	0.001262504	0.004770026	0.007676594	0.003227227	0.00111204	0.004339267	0.006983356
L146	PD	550	22:00 ~ 23:00	550	0.952506469	0.088345465	1.040851934	1.675084651	0.043157149	0.01232982	0.044390131	0.017138813	0.039687028	0.001070181	0.040757209	0.065592207
L147	PD	750	22:00 ~ 23:00	750	0.862934664	0.111782938	0.974717602	1.568652025	0.036378438	0.001124306	0.037502743	0.060354665	0.033452072	0.000972623	0.034424695	0.055401039
L148	LD	200	22:00 ~ 23:00	200	0.418708099	0.098276449	0.516984547	0.832003911	0.007960978	0.000946611	0.008907589	0.007326546	0.007326546	0.000841923	0.008168469	0.013145394
L149	LD	400	22:00 ~ 23:00	400	0.485080503	0.12163911	0.606719613	0.976418142	0.009544693	0.000894881	0.010434173	0.016792133	0.008775467	0.0007807	0.009556167	0.015379122
L150	LD	200	22:00 ~ 23:00	200	0.516819545	0.143160676	0.65980221	1.062132568	0.01167171	0.001126011	0.012293182	0.01978391	0.010254951	0.00077087	0.011236539	0.018083411
L151	LD	450	22:00 ~ 23:00	450	0.856108673	0.110693356	0.966802029	1.559913177	0.028005165	0.001049291	0.029054456	0.046758498	0.0257568	0.000920685	0.026677485	0.042933343
L152	LD	400	22:00 ~ 23:00	400	0.930457424	0.103454816	1.03391224	1.63916324	0.031934228	0.001043574	0.032977802	0.053072496	0.029373838	0.000920028	0.030293866	0.048753131
L153	LD	150	22:00 ~ 23:00	150	0.517848745	0.108432918	0.626281663	1.000790131	0.013142543	0.001213437	0.01435598	0.023103653	0.01208604	0.001065699	0.013152609	0.02116702
L154	LD	200	22:00 ~ 23:00	200	0.486194666	0.135631773	0.621826439	1.000730161	0.009689454	0.001124594	0.0140814049	0.017403481	0.008901252	0.000976143	0.009877395	0.015896077
L155	LD	200	22:00 ~ 23:00	200	0.492345598	0.097861539	0.590207137	0.949843954	0.012229684	0.001320119	0.013459803	0.02180624	0.011248656	0.001157704	0.01240636	0.019966051
L156	LD	100	22:00 ~ 23:00	100	0.497640896	0.130761012	0.628401797	1.011312325	0.009827047	0.000912319	0.010739366	0.017283291	0.009031685	0.000796838	0.009828523	0.015817435
L157	LD	100	22:00 ~ 23:00	100	0.318588732	0.09586724	0.414455973	0.667000575	0.003108474	0.001658662	0.004765336	0.007690946	0.00284684	0.001431786	0.004278626	0.006885764
L158	LD	100	22:00 ~ 23:00	100	0.517905711	0.148193078	0.666098789	1.071979425	0.010300873	0.001260739	0.011561611	0.018606564	0.009455982	0.001078579	0.010534561	0.016953691
L159	PD	550	22:00 ~ 23:00	550	0.952506469	0.088345465	1.040851934	1.675084651	0.043157149	0.01232982	0.044390131	0.017138813	0.039687028	0.001070181	0.040757209	0.065592207
L160	PD	500	22:00 ~ 23:00	500	0.992143554	0.090352401	1.082495955	1.742104041	0.045149828	0.001173826	0.046323654	0.07455051	0.041520544	0.001018019	0.045238631	0.068459011
L161	PD	500	22:00 ~ 23:00	500	1.018813428	0.116912651	1.135726079	1.827769408	0.044605617	0.001156303	0.04576192	0.073646488	0.04101884	0.000990679	0.042009519	0.067607599
L162	LD	250	22:00 ~ 23:00	250	0.385950343	0.095318461	0.481268804	0.774525137	0.006269787	0.00110139	0.007379926	0.01187611	0.00576637	0.000976728	0.006743098	0.010851937
L163	LD	300	22:00 ~ 23:00	300	0.422854202	0.100268477	0.523122679	0.841882252	0.006854365	0.000988872	0.007753237	0.012477595	0.006033799	0.000791558	0.007095357	0.01148842
L164	LD	50	22:00 ~ 23:00	50	0.19528255	0.042778334	0.238061189	0.383121995	0.003795094	0.001046651	0.004841746	0.007792015	0.003521928	0.000975289	0.004497217	0.007237551
L165	LD	50	22:00 ~ 23:00	50	0.167564755	0.191702842	0.359267597	0.578189715	0.00618977	0.000542686	0.011616631	0.018695109	0.005624021	0.000463235	0.010267256	0.016523506
L166	LD	350	22:00 ~ 23:00	350	0.417801598	0.101306025	0.519007623	0.835299728	0.006590229	0.00094						

Table 5: Hourly composite emission rate for each road link

L002	LD	50	23:00 ~ 00:00	150	0.59817074	0.117569573	0.715740313	1.151869516	0.011959262	0.00042795	0.012387212	0.019935236	0.010996074	0.000376718	0.011372791	0.018302688
L003	LD	50	23:00 ~ 00:00	50	0.20605965	0.044583076	0.250642726	0.403369365	0.003427759	0.000974468	0.004402228	0.007084681	0.003177428	0.000908027	0.004084555	0.006574887
L004	DD	50	23:00 ~ 00:00	550	1.0757288	0.07118138	1.845768361	0.0422934	0.00099381	0.043292781	0.069672804	0.0388965	0.000872542	0.039769042	0.0640041	0.00640191
L005	DD	50	23:00 ~ 00:00	500	1.3467621	0.074211661	1.420973761	2.286829913	0.054819	0.000684812	0.055536082	0.089376439	0.0504538	0.000598663	0.051052463	0.082160771
L006	LD	50	23:00 ~ 00:00	150	0.64953953	0.12471943	0.77425896	1.246045914	0.014219298	0.000695725	0.024003343	0.013073283	0.013678467	0.000605183	0.022013304	0.022013304
L007	LD	50	23:00 ~ 00:00	250	0.550734334	0.104807951	0.655542285	1.054990421	0.011701005	0.000778963	0.012479968	0.020084512	0.010760954	0.000686551	0.011447505	0.018422927
L008	LD	50	23:00 ~ 00:00	100	0.512656869	0.072979708	0.585636578	0.94248837	0.009453977	0.000901521	0.010355499	0.00691928	0.000790928	0.009488277	0.015269864	0.015269864
L009	LD	50	23:00 ~ 00:00	150	0.361426227	0.084925242	0.446351469	0.718331273	0.009157056	0.001736686	0.010893742	0.017531735	0.008423239	0.001532339	0.009955578	0.01602191
L010	LD	50	23:00 ~ 00:00	50	0.26474046	0.168927645	0.433668105	0.697919428	0.00382892	0.003954207	0.007781327	0.012522801	0.00346374	0.006828902	0.010989381	0.010989381
L011	LD	50	23:00 ~ 00:00	100	0.520488056	0.070723781	0.591211837	0.951460857	0.01084029	0.00099175	0.01183204	0.019041775	0.009973398	0.000875004	0.010848402	0.017458768
L012	LD	50	23:00 ~ 00:00	50	0.353062658	0.034725716	0.387788374	0.624083342	0.011009224	0.001055153	0.012064377	0.019415684	0.010154266	0.000980956	0.011135222	0.017920358
L013	LD	50	23:00 ~ 00:00	100	0.344805335	0.062072746	0.406878081	0.654805171	0.005015517	0.000940863	0.00595638	0.009585841	0.004623393	0.00084377	0.005467162	0.008798523
L014	DD	50	23:00 ~ 00:00	650	1.0899928	0.096572514	1.186565314	1.909587023	0.0424525	0.000813792	0.043266292	0.069630175	0.0390449	0.000713512	0.039758412	0.063984802
L015	DD	50	23:00 ~ 00:00	250	2.008071	0.051212976	2.059283976	3.314088074	0.089175	0.000486129	0.089661129	0.144295422	0.0820286	0.000420812	0.082449412	0.132689137
L016	LD	50	23:00 ~ 00:00	100	0.368753149	0.067237233	0.701656761	0.003711532	0.000759899	0.004470521	0.007194588	0.003418525	0.000674672	0.000493196	0.006587345	0.006587345
L017	LD	50	23:00 ~ 00:00	100	0.520488056	0.070723781	0.591211837	0.951460857	0.01084029	0.00099175	0.01183204	0.019041775	0.009973398	0.000875004	0.010848402	0.017458768
L018	LD	50	23:00 ~ 00:00	150	0.361426227	0.084925242	0.446351469	0.718331273	0.009157056	0.001736686	0.010893742	0.017531735	0.008423239	0.001532339	0.009955578	0.01602191
L019	LD	50	23:00 ~ 00:00	100	0.511025815	0.114217689	0.625243503	1.00622938	0.013515292	0.002699567	0.016214859	0.020269522	0.01472965	0.023704395	0.023704395	0.023704395
L020	LD	50	23:00 ~ 00:00	200	0.42270795	0.080378493	0.503086443	0.809637137	0.007748305	0.001381654	0.009129999	0.014693208	0.007123096	0.001206952	0.008330048	0.013405789
L021	DD	50	23:00 ~ 00:00	600	1.1674698	0.09452173	1.26199153	2.03097345	0.0461038	0.000781433	0.046885233	0.075454282	0.043085238	0.006933924	0.06933924	0.06933924
L022	DD	50	23:00 ~ 00:00	350	1.7415304	0.103361318	1.844891718	2.969058038	0.0753287	0.000481037	0.075809737	0.122003642	0.0692914	0.000420714	0.069712114	0.112190493
L023	LD	50	23:00 ~ 00:00	150	0.525137331	0.071246145	0.596383476	0.959783784	0.010951563	0.0012003643	0.009317943	0.010075241	0.000925793	0.011001004	0.017704404	0.017704404
L024	LD	50	23:00 ~ 00:00	200	0.247587724	0.075597431	0.323185155	0.520114797	0.006304895	0.00146288	0.007947775	0.012790673	0.005808256	0.001466106	0.007274362	0.011706922
L025	DD	50	23:00 ~ 00:00	50	0.6343284	0.027943916	0.662272316	1.06582133	0.0239117	0.001264133	0.025175833	0.040516475	0.0220036	0.001175889	0.023179489	0.037303679
L026	DD	50	23:00 ~ 00:00	50	0.7364572	0.031431316	0.767888516	1.235793704	0.0274177	0.001102188	0.028519888	0.045898196	0.0252276	0.00102396	0.02625156	0.042247685
L027	DD	50	23:00 ~ 00:00	200	0.5134441	0.124381652	0.637825752	1.026478496	0.0122655	0.000960762	0.013226262	0.0112746	0.000846198	0.012120798	0.019506485	0.019506485
L028	DD	50	23:00 ~ 00:00	150	0.5448735	0.139495353	0.684368853	1.101382169	0.0132324	0.000951555	0.014183955	0.022826806	0.0121643	0.000831963	0.012996263	0.020915406
L029	LD	50	23:00 ~ 00:00	50	0.16654914	0.03475358	0.20130272	0.323964519	0.005971019	0.001268867	0.007239887	0.01651439	0.005524441	0.001180851	0.006705292	0.010791279
L030	DD	50	23:00 ~ 00:00	700	1.0563455	0.100681279	1.157026779	1.862049476	0.0401322	0.000907876	0.041940076	0.067495843	0.0377364	0.00079295	0.03852935	0.062066244
L031	DD	50	23:00 ~ 00:00	650	0.1998195	0.101856089	0.301675589	0.485498592	0.0013923	0.000697256	0.002089556	0.003362805	0.0012732	0.000610031	0.001883231	0.003030759
L032	DD	50	23:00 ~ 00:00	300	1.7014561	0.126825248	1.828281348	2.942326305	0.073804	0.000761915	0.074565915	0.12000191	0.0678846	0.000661775	0.068546375	0.110314423
L033	LD	50	23:00 ~ 00:00	100	0.271828879	0.117418849	0.389247728	0.626431938	0.007291918	0.003080879	0.010372797	0.016693357	0.006681638	0.002662136	0.009343774	0.010317431
L034	LD	50	23:00 ~ 00:00	100	0.237574193	0.094704172	0.332278365	0.534748864	0.005760087	0.002451392	0.008211479	0.013215062	0.005288431	0.002139577	0.007428008	0.01195419
L035	LD	50	23:00 ~ 00:00	50	0.0305732	0.023828543	0.054401743	0.087550901	0.0063713	0.001804571	0.008175871	0.013157757	0.005906	0.001681532	0.007587532	0.012210919
L036	LD	50	23:00 ~ 00:00	50	0.0305732	0.023828543	0.054401743	0.087550901	0.0063713	0.001804571	0.008175871	0.013157757	0.005906	0.001681532	0.007587532	0.012210919
L037	LD	50	23:00 ~ 00:00	100	0.234092998	0.094704172	0.32879717	0.529146438	0.005804625	0.002451392	0.013286738	0.005330571	0.002139577	0.007470148	0.010220098	0.010220098
L038	LD	50	23:00 ~ 00:00	150	0.236030046	0.079983893	0.316013939	0.508573873	0.005828838	0.002081846	0.007910684	0.01273098	0.005361514	0.001831846	0.007193361	0.011576563
L039	LD	50	23:00 ~ 00:00	50	0.188666814	0.042778334	0.231745148	0.372956737	0.003714468	0.001466551	0.004761119	0.00766226	0.003443198	0.000975289	0.004418487	0.007110847
L040	LD	50	23:00 ~ 00:00	50	0.18250952	0.041875963	0.224385483	0.361112534	0.00382278	0.001082743	0.004905523	0.007894564	0.0035436	0.001008919	0.004552519	0.007326552
L041	LD	50	23:00 ~ 00:00	50	0.217006555	0.0637734	0.280779956	0.451870414	0.005669736	0.001698435	0.00736817	0.011857891	0.005225775	0.001517497	0.010852216	0.010852216
L042	LD	50	23:00 ~ 00:00	50	0.278379667	0.040094128	0.318473795	0.512532618	0.006982872	0.001049485	0.008032357	0.012926793	0.006448729	0.000977178	0.007425907	0.011950809
L043	LD	50	23:00 ~ 00:00	100	0.57681453	0.078393082	0.655207612	1.054451818	0.01079679	0.01239811	0.01331949	0.021435588	0.011256004	0.000941406	0.01219741	0.01962978
L044	LD	50	23:00 ~ 00:00	200	0.524575498	0.130189226	0.654764724	1.053739061	0.013802783	0.001145173	0.014947955	0.024056342	0.012691697	0.001010337	0.013702035	0.022051233
L045	LD	50	23:00 ~ 00:00	100	0.25735363	0.071857049	0.329210679	0.529811914	0.005808703	0.001766901	0.007575604	0.01219723	0.005348422	0.001566138	0.006914556	0.011127788
L046	LD	50	23:00 ~ 00:00	100	0.215687416	0.041210622	0.256898038	0.413436289	0.004704437	0.001015127	0.005719565	0.009204724	0.004353906	0.000944414	0.00529832	0.008526799
L047	DD	50	23:00 ~ 00:00	700	1.0832004	0.10056345	1.18376385	1.905078515	0.0426005	0.000893914	0.043494414	0.069997301	0.0391796	0.000782232	0.0399613275	0.064312175
L048	DD	50	23:00 ~ 00:00	1.8594741	0.137930325	1.997404425	3.214502838	0.0819877	0.000752147	0.082739847	0.133156545	0.0754128	0.000649602	0.070662402	0.122410267	0.122410267
L049	DD	50	23:00 ~ 00:00	300	0.3803165	0.076997848	0.457314348	0.735974273	0.0059411	0.00933287	0.006874387	0.011063225	0.0054584	0.000826014	0.006284414	0.010113758
L050	DD	50	23:00 ~ 00:00	250	0.7022111	0.105537132	0.807748232	1.299941539	0.0242811	0.001136063	0.025417163	0.040904857	0.0223266	0.001005029	0.023237129	0.037541281
L051	DD	50	23:00 ~ 00:00	300	0.3885583	0.077943961	0.466502261	0.750760748	0.0059938	0.000902922	0.006896722	0.01109917	0.000797844	0.006304944	0.010146798	0.010146798
L052	DD	50	23:00 ~ 00:00	200	0.767534	0.1057072	0.8732412	1.405341993	0.0271789	0.000906833	0.028085733	0.045199493	0.0249947	0.000804413	0.025799113	0.041519544
L053	DD	50	23:00 ~ 00:00	350	0.4143747	0.09946349	0.51383819	0.82694035								

Table 5: Hourly composite emission rate for each road link

L084	LD	50	23:00 ~ 00:00	50	0.0305732	0.023828543	0.054401743	0.087550901	0.0063713	0.001804571	0.008175871	0.013157757	0.005906	0.001681532	0.007587532	0.012210919
L085	LD	50	23:00 ~ 00:00	50	0.0305732	0.023828543	0.054401743	0.087550901	0.0063713	0.001804571	0.008175871	0.013157757	0.005906	0.001681532	0.007587532	0.012210919
L086	LD	50	23:00 ~ 00:00	50	0.188258592	0.143685867	0.331944459	0.534211495	0.006645885	0.004068114	0.010713998	0.017242466	0.006077299	0.003502893	0.009580192	0.015417786
L087	LD	300	23:00 ~ 00:00	300	0.12777266	0.060997931	0.01157845	0.098008473	0.01157845	0.01506396	0.013084846	0.021057966	0.010639373	0.001312823	0.011952196	0.023251548
L088	LD	50	23:00 ~ 00:00	50	0.257614631	0.120814085	0.378428716	0.60902047	0.003380412	0.002692667	0.006076679	0.009779442	0.003083063	0.002321533	0.005045095	0.008697832
L089	LD	150	23:00 ~ 00:00	150	0.352598054	0.078130811	0.430728865	0.693189192	0.005097432	0.00184461	0.006281893	0.010109702	0.004686506	0.001038976	0.005725482	0.022142488
L090	LD	250	23:00 ~ 00:00	250	0.697432966	0.177495403	0.874927469	1.408055773	0.020032539	0.000648118	0.020680657	0.033282208	0.018419267	0.000577368	0.018996636	0.030572046
L091	LD	50	23:00 ~ 00:00	50	0.311730333	0.075525427	0.38725576	0.008276972	0.001913218	0.001639948	0.01019019	0.01639948	0.007615517	0.00168582	0.009301336	0.014960133
L092	LD	200	23:00 ~ 00:00	200	0.767709648	0.20828308	0.975992728	1.570704136	0.022576624	0.000717353	0.023293977	0.037487929	0.020751689	0.000627852	0.021379541	0.03440695
L093	LD	50	23:00 ~ 00:00	50	0.364350567	0.086662551	0.451013118	0.725833452	0.008486395	0.001709514	0.010195909	0.016406884	0.007800239	0.001496481	0.00929672	0.014961584
L094	LD	200	23:00 ~ 00:00	200	0.758389871	0.223990253	0.982380124	1.580983629	0.023637079	0.000452118	0.024089198	0.038767709	0.021733161	0.000412962	0.022146124	0.035640643
L095	LD	300	23:00 ~ 00:00	300	0.431880751	0.108946861	0.540827612	0.870375509	0.009709636	0.000823383	0.010533019	0.016951209	0.008937241	0.000745907	0.009683149	0.015583478
L096	LD	200	23:00 ~ 00:00	200	0.797843647	0.21518945	1.013033097	1.630314684	0.024915834	0.000456038	0.025371872	0.040631968	0.022911241	0.000416546	0.023327787	0.037542341
L097	LD	200	23:00 ~ 00:00	200	0.742920797	0.206958133	0.94987893	1.528678157	0.023404172	0.000780246	0.024184418	0.038920951	0.021518205	0.000694598	0.022212803	0.035747953
L098	LD	50	23:00 ~ 00:00	50	0.219573277	0.131480177	0.351053454	0.564964365	0.007204139	0.003711798	0.010915937	0.017567454	0.006590673	0.003201936	0.009792609	0.015759637
L099	LD	300	23:00 ~ 00:00	300	0.482303711	0.115675081	0.597978792	0.962351189	0.012403721	0.001146418	0.01355014	0.021806782	0.011410731	0.001020187	0.012430918	0.020005573
L100	LD	50	23:00 ~ 00:00	50	0.199222515	0.043680705	0.242903221	0.390913869	0.003542443	0.001010556	0.004553003	0.007327329	0.003283736	0.000941658	0.004225394	0.006800096
L101	LD	100	23:00 ~ 00:00	100	0.419960539	0.087598431	0.50755897	0.816834953	0.010878973	0.001099968	0.012788942	0.020581755	0.010002449	0.001666608	0.018779481	0.023948099
L102	LD	50	23:00 ~ 00:00	50	0.213564234	0.010084163	0.00545701	0.005466911	0.00545701	0.002631517	0.008088527	0.013017191	0.005011034	0.002291511	0.007302544	0.011752277
L103	LD	100	23:00 ~ 00:00	100	0.297487678	0.064602946	0.362090623	0.582726924	0.005126033	0.001284803	0.006390836	0.010285027	0.004724343	0.001129715	0.005854058	0.00942117
L104	PD	600	23:00 ~ 00:00	600	0.852309962	0.126483708	0.97879367	1.575211806	0.034922919	0.001898999	0.035822818	0.057651094	0.00784695	0.003300408	0.029480899	0.052948089
L105	PD	500	23:00 ~ 00:00	500	0.96377973	0.163817844	1.127597574	1.614468788	0.039557093	0.001075378	0.040632471	0.06391461	0.00367473	0.000918904	0.037293634	0.062018138
L106	LD	100	23:00 ~ 00:00	100	0.483638626	0.175206805	0.658845431	1.060306305	0.015059832	0.002717146	0.017776979	0.028690237	0.013821979	0.003340237	0.016145217	0.026010502
L107	LD	50	23:00 ~ 00:00	50	0.406493552	0.07023988	0.477189232	0.767959719	0.007150238	0.0010637	0.008213938	0.013219019	0.006580941	0.000942561	0.007523503	0.012017874
L108	LD	150	23:00 ~ 00:00	150	0.49843117	0.086198524	0.584629694	0.940867952	0.009082858	0.000627289	0.009705686	0.015619749	0.008054122	0.00055097	0.008095092	0.014331321
L109	LD	100	23:00 ~ 00:00	100	0.391949888	0.069907837	0.461857725	0.743286111	0.007076907	0.001090768	0.008167676	0.013144567	0.006513247	0.000967784	0.007481003	0.012039522
L110	LD	100	23:00 ~ 00:00	100	0.34454112	0.133090975	0.477632095	0.786672436	0.005828094	0.002895839	0.008723933	0.014039774	0.005323154	0.002474893	0.012549767	0.02159767
L111	LD	50	23:00 ~ 00:00	50	0.388069886	0.077698257	0.465768143	0.749579303	0.008449666	0.001524934	0.00996699	0.016044959	0.00776866	0.00133978	0.009108441	0.014658578
L112	LD	250	23:00 ~ 00:00	250	0.536741195	0.115526276	0.651967471	1.09423733	0.014124149	0.000858884	0.014124149	0.024112795	0.012996164	0.00077416	0.013770323	0.022161132
L113	LD	250	23:00 ~ 00:00	250	0.501158635	0.119020162	0.620178797	0.998078545	0.01354206	0.001197298	0.014739359	0.023720639	0.012457074	0.001063897	0.013520971	0.021759839
L114	LD	150	23:00 ~ 00:00	150	0.331222006	0.066137566	0.06907811	0.006907811	0.001760159	0.00866797	0.01394971	0.006352261	0.001543606	0.000785866	0.012707133	0.021739497
L115	LD	50	23:00 ~ 00:00	50	0.197287275	0.029033045	0.22631577	0.364219022	0.006911608	0.001349551	0.01026116	0.016513695	0.000823096	0.00125378	0.00948474	0.015264171
L116	LD	50	23:00 ~ 00:00	50	0.495490007	0.022619863	0.11720987	0.188630533	0.008499484	0.001704798	0.010204282	0.01642216	0.007860865	0.001586308	0.009447173	0.015203713
L117	LD	50	23:00 ~ 00:00	50	0.376545037	0.071036478	0.447581516	0.720310836	0.01173295	0.001988235	0.01372116	0.022082012	0.010794711	0.00175497	0.012549681	0.020196754
L118	PD	700	23:00 ~ 00:00	700	0.830693647	0.08500793	0.51204645	0.93544444	0.03483036	0.001111973	0.035495009	0.057123537	0.031617192	0.000964823	0.032582015	0.052495604
L119	PD	500	23:00 ~ 00:00	500	1.009312396	0.167084878	1.176397274	1.89323129	0.042351313	0.001357154	0.043708467	0.070341784	0.03894172	0.001152731	0.040094451	0.064525603
L120	LD	50	23:00 ~ 00:00	50	0.552170535	0.158713769	0.710884303	1.144054545	0.016557137	0.001298595	0.017855732	0.028375943	0.001145671	0.001636735	0.026340671	0.04367375
L121	LD	150	23:00 ~ 00:00	150	0.410723276	0.0761398	0.486863076	0.783528223	0.006941511	0.01193969	0.00813548	0.013092753	0.006383327	0.001047903	0.007431123	0.011959376
L122	LD	250	23:00 ~ 00:00	250	0.462127648	0.086365108	0.548492757	0.882711333	0.007784601	0.000725653	0.013695891	0.007162054	0.006643294	0.007805348	0.012561459	0.024556124
L123	LD	100	23:00 ~ 00:00	100	0.529986419	0.148444741	0.67843116	1.091826403	0.01553178	0.001316641	0.016848421	0.027114838	0.014282144	0.001162486	0.01544463	0.024855661
L124	LD	150	23:00 ~ 00:00	150	0.157060186	0.03916885	0.196229037	0.315799238	0.001191017	0.005440674	0.008755894	0.003939302	0.001109811	0.005049113	0.00812574	0.01052674
L125	LD	50	23:00 ~ 00:00	50	0.157060186	0.03916885	0.196229037	0.315799238	0.001191017	0.005440674	0.008755894	0.003939302	0.001109811	0.005049113	0.00812574	0.01052674
L126	PD	500	23:00 ~ 00:00	500	1.002856729	0.164091208	1.166947937	1.878015993	0.041994033	0.001292716	0.043287619	0.069664496	0.038614526	0.001098974	0.0397135	0.063912524
L127	DD	50	23:00 ~ 00:00	50	0.1834582	0.039265435	0.222723635	0.358438055	0.0042806	0.00108841	0.00536901	0.008640563	0.003937	0.001012698	0.004949698	0.007965477
L128	LD	150	23:00 ~ 00:00	150	0.479603993	0.096363646	0.575967639	0.92692776	0.010451403	0.000859514	0.011310917	0.018203111	0.009616329	0.000768502	0.010384831	0.016712724
L129	LD	200	23:00 ~ 00:00	200	0.491391435	0.098842665	0.5902341	0.949887346	0.0083224	0.000816871	0.009139271	0.014708195	0.00706515	0.000712443	0.008363943	0.013460248
L130	LD	150	23:00 ~ 00:00	150	0.519227285	0.096493166	0.615720452	0.990093552	0.008187378	0.0064517	0.008832548	0.014214572	0.007525394	0.000650591	0.008085985	0.013013099
L131	LD	50	23:00 ~ 00:00	50	0.479603993	0.096363646	0.575967639	0.92692776	0.010451403	0.000859514	0.011310917	0.018203111	0.009616329	0.000768502	0.010384831	0.016712724
L132	PD	750	23:00 ~ 00:00	750	0.80074681	0.152101078	0.952847888	1.533456222	0.030697649	0.001588947	0.032286596	0.05196011	0.028219537	0.001350331	0.029569868	0.047587971
L133	DD	450	23:00 ~ 00:00	450	0.7204277	0.084924925	0.805397625	1.296158614	0.024853	0.001018965	0.025871965	0.041636788	0.0228359	0.000898935	0.023752835	0.038226388
L134	DD	300	23:00 ~ 00:00	300	0.8948738	0.084712466	0.576487361	0.9305051	0.0075682	0.00330692	0.054406829	0.0303969	0.00067223	0.003106913	0.050007094	0.08126674
L135	LD															

Table 5: Hourly composite emission rate for each road link

L166	LD	50	23:00 ~ 00:00	300	0.421999844	0.10017275	0.522172594	0.840353243	0.006677735	0.000910628	0.007588363	0.012212256	0.006139525	0.000800143	0.006939669	0.011168286
L167	LD	50	23:00 ~ 00:00	200	0.389793384	0.094710467	0.484503851	0.779731427	0.006270851	0.001081779	0.00735263	0.011832881	0.005765917	0.00095178	0.006717696	0.010811057
L168	DD	50	23:00 ~ 00:00	100	0.2804418	0.078173646	0.358615446	0.577134182	0.0038854	0.001514261	0.005399661	0.008689891	0.0035594	0.001330724	0.004890124	0.007869872
L169	DD	50	23:00 ~ 00:00	150	0.4587991	0.091978045	0.550777145	0.886387691	0.0112572	0.001622009	0.012879209	0.020727027	0.0103355	0.001404361	0.011739861	0.018893429
L170	LD	50	23:00 ~ 00:00	100	0.383580745	0.066817755	0.4503985	0.724844323	0.006542582	0.001098607	0.007641189	0.012297271	0.006021213	0.000974952	0.006996165	0.011259207
L171	LD	50	23:00 ~ 00:00	50	0.65840301	0.20608357	0.86448658	1.391252833	0.019377775	0.001079213	0.020456987	0.032922248	0.017810783	0.000941997	0.01875278	0.0301796
L172	LD	50	23:00 ~ 00:00	50	0.193524903	0.042778334	0.236303238	0.380292252	0.003638012	0.001046651	0.004684664	0.007599217	0.003572326	0.000975289	0.004347615	0.00699679
L173	LD	50	23:00 ~ 00:00	250	0.505074738	0.139474389	0.644549126	1.037296691	0.010968811	0.001198835	0.012167646	0.019581879	0.010077663	0.001040232	0.011117894	0.017892472
L174	LD	50	23:00 ~ 00:00	300	0.462834861	0.087904932	0.550739792	0.886327578	0.010030842	0.001387441	0.011418283	0.018375899	0.009222078	0.001212834	0.010434412	0.016792517
L175	LD	50	23:00 ~ 00:00	200	0.411479684	0.075488055	0.48696774	0.783696662	0.007004903	0.001203263	0.008208166	0.013209718	0.006443455	0.00105677	0.007500225	0.012070412
L176	LD	50	23:00 ~ 00:00	100	0.53167742	0.116401065	0.648078485	1.042978629	0.014481511	0.001898413	0.016379924	0.026360866	0.013303237	0.001640827	0.014944058	0.024050071
L177	LD	50	23:00 ~ 00:00	50	0.265342534	0.392278419	0.657620953	1.058335704	0.00638883	0.010126597	0.016515427	0.026578937	0.005678	0.008513475	0.014191474	0.022838908
L178	LD	50	23:00 ~ 00:00	50	0.382879576	0.487531193	0.87041077	1.400786868	0.005546451	0.011882722	0.017429173	0.028049466	0.004827416	0.00992715	0.014754566	0.023745113
L179	LD	50	23:00 ~ 00:00	150	0.526174861	0.120338848	0.64651371	1.040460373	0.014001747	0.002002697	0.016004444	0.025756592	0.012859737	0.001727344	0.01458708	0.023475572
L180	PD	50	23:00 ~ 00:00	550	0.900788328	0.113395272	1.014183601	1.632166236	0.039538241	0.001363937	0.040902176	0.06582551	0.036355634	0.001177608	0.037533243	0.064003749
L181	PD	70	23:00 ~ 00:00	950	0.401004725	0	0.401004725	0.645352944	0.009785823	0	0.009785823	0.015748716	0.008983585	0	0.008983585	0.014457643
L182	PD	70	23:00 ~ 00:00	1350	0.341655393	0	0.341655393	0.54983969	0.006668825	0	0.006668825	0.010732407	0.006119009	0	0.006119009	0.009847566
L183	PD	50	23:00 ~ 00:00	400	1.058805817	0.103497786	1.162303603	1.870541681	0.049003568	0.001193812	0.05019738	0.080784652	0.045064212	0.001032124	0.046096336	0.074184677
L184	PD	50	23:00 ~ 00:00	450	0.956854295	0.116280213	1.073134507	1.727038288	0.041113989	0.001267417	0.042381406	0.068206091	0.037805872	0.001084293	0.038890165	0.062587499
L185	DD	50	23:00 ~ 00:00	300	0.4008029	0.102978029	0.503780929	0.810754801	0.0078203	0.001330402	0.009150702	0.014726591	0.00718	0.001159355	0.008339355	0.013420857
L186	DD	50	23:00 ~ 00:00	300	0.3826766	0.09497805	0.47765465	0.768708735	0.0053917	0.000862328	0.006254028	0.010064857	0.0049525	0.000760564	0.005713064	0.009194263
L187	LD	50	23:00 ~ 00:00	50	0.362159457	0.116309487	0.478468944	0.770019211	0.010087644	0.001479052	0.011566697	0.018614748	0.009279068	0.001313824	0.010592892	0.017047564
L188	LD	50	23:00 ~ 00:00	50	0.429028958	0.089520209	0.518549167	0.834521917	0.013092979	0.000905119	0.013998098	0.022527699	0.012061762	0.000842655	0.012904417	0.020767594
L189	LD	50	23:00 ~ 00:00	50	0.181634249	0.13629834	0.317932589	0.511661633	0.004904258	0.003667205	0.008571463	0.013794398	0.004480222	0.003162638	0.00764286	0.012299961
L190	LD	50	23:00 ~ 00:00	50	0.195368349	0.090891894	0.286260243	0.46069006	0.004098926	0.002302791	0.006401717	0.010302539	0.003769208	0.002018518	0.005787725	0.009314418
L191	LD	50	23:00 ~ 00:00	50	0.1905928	0.038363064	0.228955864	0.36846783	0.00536442	0.001124502	0.006488922	0.010442882	0.004963279	0.001046328	0.006009607	0.009671501
L192	LD	50	23:00 ~ 00:00	50	0.226599222	0.0413231067	0.267950289	0.431223118	0.004750197	0.001014027	0.005764224	0.009276597	0.004396622	0.000943392	0.005340014	0.008593898
L193	LD	50	23:00 ~ 00:00	50	0.21432447	0.038862542	0.253187012	0.407463985	0.00583038	0.001055153	0.006885533	0.011081163	0.00539038	0.000980956	0.006371336	0.010253646
L194	LD	50	23:00 ~ 00:00	50	0.175256399	0.036961215	0.212217614	0.341530295	0.005367542	0.001229942	0.006597484	0.010617595	0.004967593	0.001145331	0.006112924	0.009837773
L195	LD	50	23:00 ~ 00:00	100	0.237401599	0.100404124	0.337805723	0.543644262	0.005930467	0.002596292	0.008526759	0.013722455	0.005444336	0.002258314	0.00770265	0.012396182
L196	LD	50	23:00 ~ 00:00	100	0.23099187	0.065197179	0.296189049	0.476668884	0.005354343	0.001643747	0.00699809	0.011262307	0.00494387	0.00146654	0.00640141	0.010302045

APPENDIX 3.3: PREDICTED AIR QUALITY IMPACT ASSESSMENT RESULTS

ASR Label	Location		Local Ground, Z (mPD)	Floor	Flag Pole Receiver Height, mPD	NO2		RSP		FSP	
	x	y				19th Hourly Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.
	Easting	Northing									
A01_01	837471	819520	5.05	Retail	6.55	164	45	90	41	68	30
A01_02	837471	819520	5.05	Retail	12.55	156	41	90	41	68	30
A01_03	837471	819520	5.05	Retail	17.05	147	38	90	41	68	30
A01_04	837471	819520	5.05	Clubhouse	21.55	144	36	90	41	67	30
A01_05	837471	819520	5.05	1/F	29.05	139	34	89	41	67	29
A01_06	837471	819520	5.05	2/F	32.2	136	33	89	41	67	29
A01_07	837471	819520	5.05	3/F	35.35	135	32	89	41	67	29
A01_08	837471	819520	5.05	4/F	38.5	133	32	89	41	67	29
A01_09	837471	819520	5.05	5/F	41.65	132	31	89	41	67	29
A01_10	837471	819520	5.05	7/F	47.95	132	30	89	41	67	29
A01_11	837471	819520	5.05	9/F	54.25	131	30	89	41	67	29
A01_12	837471	819520	5.05	11/F	60.55	131	29	89	41	67	29
A01_13	837471	819520	5.05	13/F	66.85	130	29	89	41	67	29
A01_14	837471	819520	5.05	15/F	73.15	129	29	89	41	67	29
A01_15	837471	819520	5.05	17/F	79.45	129	29	89	41	67	29
A01_16	837471	819520	5.05	20/F	88.9	129	29	89	41	67	29
A01_17	837471	819520	5.05	23/F	98.35	129	29	89	41	67	29
A02_01	837458	819506	5.05	Retail	6.55	162	43	90	41	68	30
A02_02	837458	819506	5.05	Retail	12.55	158	41	90	41	69	31
A02_03	837458	819506	5.05	Retail	17.05	150	38	90	41	68	31
A02_04	837458	819506	5.05	Clubhouse	21.55	145	36	90	41	68	31
A02_05	837458	819506	5.05	1/F	29.05	139	34	89	41	67	29
A02_06	837458	819506	5.05	2/F	32.2	136	33	89	41	67	29
A02_07	837458	819506	5.05	3/F	35.35	134	32	89	41	67	29
A02_08	837458	819506	5.05	4/F	38.5	133	32	89	41	67	29
A02_09	837458	819506	5.05	5/F	41.65	132	31	89	41	67	29
A02_10	837458	819506	5.05	7/F	47.95	132	30	89	41	67	29
A02_11	837458	819506	5.05	9/F	54.25	131	30	89	41	67	29
A02_12	837458	819506	5.05	11/F	60.55	131	29	89	41	67	29
A02_13	837458	819506	5.05	13/F	66.85	129	29	89	41	67	29
A02_14	837458	819506	5.05	15/F	73.15	129	29	89	41	67	29
A02_15	837458	819506	5.05	17/F	79.45	129	29	89	41	67	29
A02_16	837458	819506	5.05	20/F	88.9	129	29	89	41	67	29
A02_17	837458	819506	5.05	23/F	98.35	129	29	89	41	67	29
A03_01	837446	819491	5.05	Retail	6.55	163	43	90	41	68	31
A03_02	837446	819491	5.05	Retail	12.55	158	41	90	41	68	30
A03_03	837446	819491	5.05	Retail	17.05	151	38	90	41	68	30
A03_04	837446	819491	5.05	Clubhouse	21.55	145	36	90	41	68	30
A03_05	837446	819491	5.05	1/F	29.05	139	34	89	41	67	29
A03_06	837446	819491	5.05	2/F	32.2	136	33	89	41	67	29
A03_07	837446	819491	5.05	3/F	35.35	134	32	89	41	67	29
A03_08	837446	819491	5.05	4/F	38.5	133	32	89	41	67	29
A03_09	837446	819491	5.05	5/F	41.65	132	31	89	41	67	29
A03_10	837446	819491	5.05	7/F	47.95	132	30	89	41	67	29
A03_11	837446	819491	5.05	9/F	54.25	131	30	89	41	67	29
A03_12	837446	819491	5.05	11/F	60.55	130	29	89	41	67	29
A03_13	837446	819491	5.05	13/F	66.85	129	29	89	41	67	29
A03_14	837446	819491	5.05	15/F	73.15	129	29	89	41	67	29
A03_15	837446	819491	5.05	17/F	79.45	129	29	89	41	67	29
A03_16	837446	819491	5.05	20/F	88.9	129	29	89	41	67	29
A03_17	837446	819491	5.05	23/F	98.35	129	29	89	41	67	29
A04_01	837467	819482	5.05	Retail	6.55	157	41	90	41	68	30
A04_02	837467	819482	5.05	Retail	12.55	155	40	90	41	68	30
A04_03	837467	819482	5.05	Retail	17.05	147	38	90	41	68	30
A04_04	837467	819482	5.05	Clubhouse	21.55	144	36	90	41	67	30
A04_05	837467	819482	5.05	1/F	29.05	137	34	89	41	67	29
A04_06	837467	819482	5.05	2/F	32.2	136	33	89	41	67	29
A04_07	837467	819482	5.05	3/F	35.35	135	32	89	41	67	29
A04_08	837467	819482	5.05	4/F	38.5	133	32	89	41	67	29
A04_09	837467	819482	5.05	5/F	41.65	132	31	89	41	67	29
A04_10	837467	819482	5.05	7/F	47.95	132	30	89	41	67	29
A04_11	837467	819482	5.05	9/F	54.25	132	30	89	41	67	29
A04_12	837467	819482	5.05	11/F	60.55	131	29	89	41	67	29
A04_13	837467	819482	5.05	13/F	66.85	130	29	89	41	67	29
A04_14	837467	819482	5.05	15/F	73.15	129	29	89	41	67	29
A04_15	837467	819482	5.05	17/F	79.45	129	29	89	41	67	29
A04_16	837467	819482	5.05	20/F	88.9	129	29	89	41	67	29
A04_17	837467	819482	5.05	23/F	98.35	129	29	89	41	67	29
A05_01	837488	819473	5.05	Retail	6.55	153	41	90	41	68	30
A05_02	837488	819473	5.05	Retail	12.55	150	39	90	41	68	30
A05_03	837488	819473	5.05	Retail	17.05	146	37	90	41	67	30
A05_04	837488	819473	5.05	Clubhouse	21.55	145	36	90	41	68	30
A05_05	837488	819473	5.05	1/F	29.05	138	33	89	41	67	29
A05_06	837488	819473	5.05	2/F	32.2	137	33	89	41	67	29
A05_07	837488	819473	5.05	3/F	35.35	134	32	89	41	67	29
A05_08	837488	819473	5.05	4/F	38.5	133	32	89	41	67	29
A05_09	837488	819473	5.05	5/F	41.65	133	31	89	41	67	29
A05_10	837488	819473	5.05	7/F	47.95	132	30	89	41	67	29
A05_11	837488	819473	5.05	9/F	54.25	131	30	89	41	67	29

ASR Label	Location		Local Ground, Z (mPD)	Floor	Flag Pole Receiver Height, mPD	NO2		RSP		FSP	
	x	y				19th Hourly Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.
	Easting	Northing									
A05_12	837488	819473	5.05	11/F	60.55	131	29	89	41	67	29
A05_13	837488	819473	5.05	13/F	66.85	130	29	89	41	67	29
A05_14	837488	819473	5.05	15/F	73.15	130	29	89	41	67	29
A05_15	837488	819473	5.05	17/F	79.45	129	29	89	41	67	29
A05_16	837488	819473	5.05	20/F	88.9	129	29	89	41	67	29
A05_17	837488	819473	5.05	23/F	98.35	129	29	89	41	67	29
A06_01	837495	819489	5.05	Retail	6.55	153	41	90	41	68	30
A06_02	837495	819489	5.05	Retail	12.55	148	39	90	41	68	30
A06_03	837495	819489	5.05	Retail	17.05	146	37	90	41	68	30
A06_04	837495	819489	5.05	Clubhouse	21.55	144	35	90	41	68	30
A06_05	837495	819489	5.05	1/F	29.05	138	33	89	41	67	29
A06_06	837495	819489	5.05	2/F	32.2	137	33	89	41	67	29
A06_07	837495	819489	5.05	3/F	35.35	135	32	89	41	67	29
A06_08	837495	819489	5.05	4/F	38.5	133	31	89	41	67	29
A06_09	837495	819489	5.05	5/F	41.65	133	31	89	41	67	29
A06_10	837495	819489	5.05	7/F	47.95	132	30	89	41	67	29
A06_11	837495	819489	5.05	9/F	54.25	131	30	89	41	67	29
A06_12	837495	819489	5.05	11/F	60.55	131	29	89	41	67	29
A06_13	837495	819489	5.05	13/F	66.85	130	29	89	41	67	29
A06_14	837495	819489	5.05	15/F	73.15	130	29	89	41	67	29
A06_15	837495	819489	5.05	17/F	79.45	129	29	89	41	67	29
A06_16	837495	819489	5.05	20/F	88.9	129	29	89	41	67	29
A06_17	837495	819489	5.05	23/F	98.35	129	29	89	41	67	29
A07_01	837503	819506	5.05	Retail	6.55	154	42	90	41	68	30
A07_02	837503	819506	5.05	Retail	12.55	148	39	90	41	68	30
A07_03	837503	819506	5.05	Retail	17.05	146	37	90	41	68	30
A07_04	837503	819506	5.05	Clubhouse	21.55	143	35	90	41	68	30
A07_05	837503	819506	5.05	1/F	29.05	138	33	89	41	67	29
A07_06	837503	819506	5.05	2/F	32.2	137	33	89	41	67	29
A07_07	837503	819506	5.05	3/F	35.35	136	32	89	41	67	29
A07_08	837503	819506	5.05	4/F	38.5	133	31	89	41	67	29
A07_09	837503	819506	5.05	5/F	41.65	133	31	89	41	67	29
A07_10	837503	819506	5.05	7/F	47.95	131	30	89	41	67	29
A07_11	837503	819506	5.05	9/F	54.25	131	30	89	41	67	29
A07_12	837503	819506	5.05	11/F	60.55	131	29	89	41	67	29
A07_13	837503	819506	5.05	13/F	66.85	130	29	89	41	67	29
A07_14	837503	819506	5.05	15/F	73.15	130	29	89	41	67	29
A07_15	837503	819506	5.05	17/F	79.45	129	29	89	41	67	29
A07_16	837503	819506	5.05	20/F	88.9	129	29	89	41	67	29
A07_17	837503	819506	5.05	23/F	98.35	129	29	89	41	67	29
A08_01	837487	819513	5.05	Retail	6.55	157	43	90	41	68	30
A08_02	837487	819513	5.05	Retail	12.55	153	40	90	41	68	30
A08_03	837487	819513	5.05	Retail	17.05	146	38	90	41	68	30
A08_04	837487	819513	5.05	Clubhouse	21.55	143	36	90	41	68	30
A08_05	837487	819513	5.05	1/F	29.05	137	33	89	41	67	29
A08_06	837487	819513	5.05	2/F	32.2	136	33	89	41	67	29
A08_07	837487	819513	5.05	3/F	35.35	135	32	89	41	67	29
A08_08	837487	819513	5.05	4/F	38.5	133	32	89	41	67	29
A08_09	837487	819513	5.05	5/F	41.65	133	31	89	41	67	29
A08_10	837487	819513	5.05	7/F	47.95	132	30	89	41	67	29
A08_11	837487	819513	5.05	9/F	54.25	131	30	89	41	67	29
A08_12	837487	819513	5.05	11/F	60.55	131	29	89	41	67	29
A08_13	837487	819513	5.05	13/F	66.85	130	29	89	41	67	29
A08_14	837487	819513	5.05	15/F	73.15	130	29	89	41	67	29
A08_15	837487	819513	5.05	17/F	79.45	129	29	89	41	67	29
A08_16	837487	819513	5.05	20/F	88.9	129	29	89	41	67	29
A08_17	837487	819513	5.05	23/F	98.35	129	29	89	41	67	29
B01_01	837516	819500	5.05	Retail	6.55	150	41	90	41	68	30
B01_02	837516	819500	5.05	Retail	12.55	149	38	90	41	68	30
B01_03	837516	819500	5.05	Retail	17.05	145	37	90	41	68	30
B01_04	837516	819500	5.05	Clubhouse	21.55	143	35	90	41	68	30
B01_05	837516	819500	5.05	1/F	29.05	138	33	89	41	67	29
B01_06	837516	819500	5.05	2/F	32.2	137	32	89	41	67	29
B01_07	837516	819500	5.05	3/F	35.35	135	32	89	41	67	29
B01_08	837516	819500	5.05	4/F	38.5	133	31	89	41	67	29
B01_09	837516	819500	5.05	5/F	41.65	133	31	89	41	67	29
B01_10	837516	819500	5.05	7/F	47.95	132	30	89	41	67	29
B01_11	837516	819500	5.05	9/F	54.25	131	30	89	41	67	29
B01_12	837516	819500	5.05	11/F	60.55	131	29	89	41	67	29
B01_13	837516	819500	5.05	13/F	66.85	130	29	89	41	67	29
B01_14	837516	819500	5.05	15/F	73.15	130	29	89	41	67	29
B01_15	837516	819500	5.05	17/F	79.45	130	29	89	41	67	29
B01_16	837516	819500	5.05	20/F	88.9	129	29	89	41	67	29
B01_17	837516	819500	5.05	23/F	98.35	129	29	89	41	67	29
B02_01	837509	819483	5.05	Retail	6.55	150	40	90	41	68	30
B02_02	837509	819483	5.05	Retail	12.55	148	38	90	41	68	30
B02_03	837509	819483	5.05	Retail	17.05	145	37	90	41	68	30
B02_04	837509	819483	5.05	Clubhouse	21.55	143	35	90	41	68	30
B02_05	837509	819483	5.05	1/F	29.05	138	33	89	41	67	29

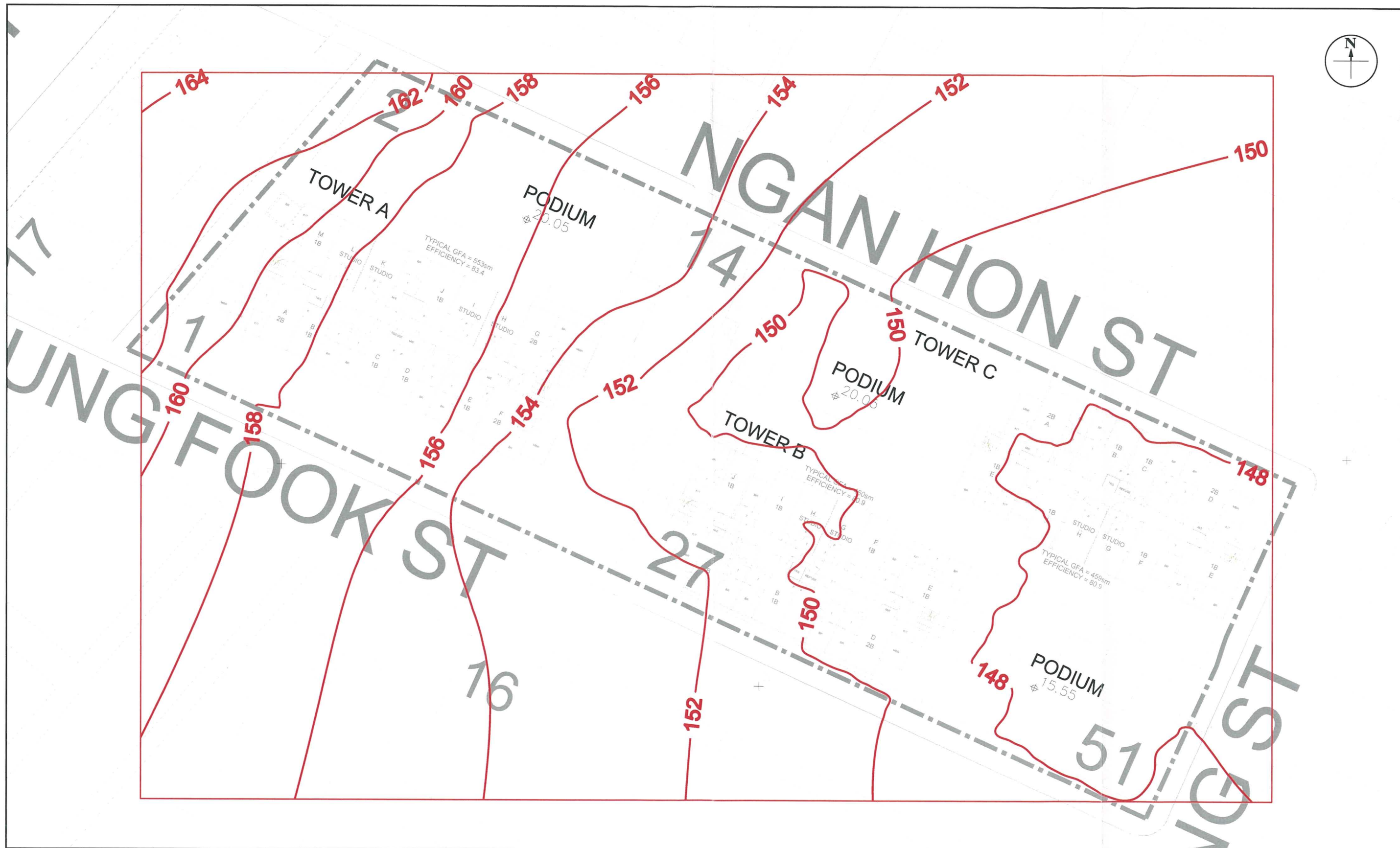
ASR Label	Location		Local Ground, Z (mPD)	Floor	Flag Pole Receiver Height, mPD	NO2		RSP		FSP	
	x	y				19th Hourly Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.
	Easting	Northing									
B02_06	837509	819483	5.05	2/F	32.2	136	33	89	41	67	29
B02_07	837509	819483	5.05	3/F	35.35	134	32	89	41	67	29
B02_08	837509	819483	5.05	4/F	38.5	133	31	89	41	67	29
B02_09	837509	819483	5.05	5/F	41.65	132	31	89	41	67	29
B02_10	837509	819483	5.05	7/F	47.95	132	30	89	41	67	29
B02_11	837509	819483	5.05	9/F	54.25	131	30	89	41	67	29
B02_12	837509	819483	5.05	11/F	60.55	131	29	89	41	67	29
B02_13	837509	819483	5.05	13/F	66.85	130	29	89	41	67	29
B02_14	837509	819483	5.05	15/F	73.15	130	29	89	41	67	29
B02_15	837509	819483	5.05	17/F	79.45	130	29	89	41	67	29
B02_16	837509	819483	5.05	20/F	88.9	129	29	89	41	67	29
B02_17	837509	819483	5.05	23/F	98.35	129	29	89	41	67	29
B03_01	837501	819466	5.05	Retail	6.55	153	40	90	41	68	30
B03_02	837501	819466	5.05	Retail	12.55	148	38	90	41	68	30
B03_03	837501	819466	5.05	Retail	17.05	146	37	90	41	68	30
B03_04	837501	819466	5.05	Clubhouse	21.55	144	35	90	41	67	30
B03_05	837501	819466	5.05	1/F	29.05	138	33	89	41	67	29
B03_06	837501	819466	5.05	2/F	32.2	136	33	89	41	67	29
B03_07	837501	819466	5.05	3/F	35.35	133	32	89	41	67	29
B03_08	837501	819466	5.05	4/F	38.5	133	31	89	41	67	29
B03_09	837501	819466	5.05	5/F	41.65	132	31	89	41	67	29
B03_10	837501	819466	5.05	7/F	47.95	132	30	89	41	67	29
B03_11	837501	819466	5.05	9/F	54.25	131	30	89	41	67	29
B03_12	837501	819466	5.05	11/F	60.55	130	29	89	41	67	29
B03_13	837501	819466	5.05	13/F	66.85	130	29	89	41	67	29
B03_14	837501	819466	5.05	15/F	73.15	130	29	89	41	67	29
B03_15	837501	819466	5.05	17/F	79.45	130	29	89	41	67	29
B03_16	837501	819466	5.05	20/F	88.9	129	29	89	41	67	29
B03_17	837501	819466	5.05	23/F	98.35	129	29	89	41	67	29
B04_01	837531	819453	5.05	Retail	6.55	149	39	90	41	67	30
B04_02	837531	819453	5.05	Retail	12.55	147	37	90	41	67	30
B04_03	837531	819453	5.05	Retail	17.05	145	36	90	41	67	30
B04_04	837531	819453	5.05	Clubhouse	21.55	142	35	89	41	67	30
B04_05	837531	819453	5.05	1/F	29.05	137	33	89	41	67	29
B04_06	837531	819453	5.05	2/F	32.2	134	32	89	41	67	29
B04_07	837531	819453	5.05	3/F	35.35	133	32	89	41	67	29
B04_08	837531	819453	5.05	4/F	38.5	133	31	89	41	67	29
B04_09	837531	819453	5.05	5/F	41.65	132	31	89	41	67	29
B04_10	837531	819453	5.05	7/F	47.95	132	30	89	41	67	29
B04_11	837531	819453	5.05	9/F	54.25	131	30	89	41	67	29
B04_12	837531	819453	5.05	11/F	60.55	130	29	89	41	67	29
B04_13	837531	819453	5.05	13/F	66.85	130	29	89	41	67	29
B04_14	837531	819453	5.05	15/F	73.15	130	29	89	41	67	29
B04_15	837531	819453	5.05	17/F	79.45	130	29	89	41	67	29
B04_16	837531	819453	5.05	20/F	88.9	129	29	89	41	67	29
B04_17	837531	819453	5.05	23/F	98.35	129	29	89	41	67	29
B05_01	837555	819442	5.05	Retail	6.55	148	40	90	41	67	30
B05_02	837555	819442	5.05	Retail	12.55	146	37	90	41	67	29
B05_03	837555	819442	5.05	Retail	17.05	144	35	89	41	67	29
B06_01	837562	819456	5.05	Retail	6.55	148	40	90	41	67	29
B06_02	837562	819456	5.05	Retail	12.55	146	37	90	41	67	30
B06_03	837562	819456	5.05	Retail	17.05	144	35	89	41	68	30
B06_04	837562	819456	5.05	Clubhouse	21.55	141	34	89	41	67	30
B06_05	837562	819456	5.05	1/F	29.05	136	32	89	41	67	29
B06_06	837562	819456	5.05	2/F	32.2	135	32	89	41	67	29
B06_07	837562	819456	5.05	3/F	35.35	133	31	89	41	67	29
B06_08	837562	819456	5.05	4/F	38.5	133	31	89	41	67	29
B06_09	837562	819456	5.05	5/F	41.65	132	31	89	41	67	29
B06_10	837562	819456	5.05	7/F	47.95	132	30	89	41	67	29
B06_11	837562	819456	5.05	9/F	54.25	131	30	89	41	67	29
B06_12	837562	819456	5.05	11/F	60.55	130	29	89	41	67	29
B06_13	837562	819456	5.05	13/F	66.85	130	29	89	41	67	29
B06_14	837562	819456	5.05	15/F	73.15	130	29	89	41	67	29
B06_15	837562	819456	5.05	17/F	79.45	130	29	89	41	67	29
B06_16	837562	819456	5.05	20/F	88.9	130	29	89	41	67	29
B06_17	837562	819456	5.05	23/F	98.35	129	29	89	41	67	29
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B07_02	837570	819475	5.05	Retail	12.55	146	37	90	41	67	30
B07_03	837570	819475	5.05	Retail	17.05	143	35	89	41	68	30
B07_04	837570	819475	5.05	Clubhouse	21.55	142	34	89	41	67	30
B07_05	837570	819475	5.05	1/F	29.05	136	32	89	41	67	29
B07_06	837570	819475	5.05	2/F	32.2	135	32	89	41	67	29
B07_07	837570	819475	5.05	3/F	35.35	133	31	89	41	67	29
B07_08	837570	819475	5.05	4/F	38.5	133	31	89	41	67	29
B07_09	837570	819475	5.05	5/F	41.65	132	31	89	41	67	29
B07_10	837570	819475	5.05	7/F	47.95	132	30	89	41	67	29
B07_11	837570	819475	5.05	9/F	54.25	131	30	89	41	67	29
B07_12	837570	819475	5.05	11/F	60.55	130	29	89	41	67	29
B07_13	837570	819475	5.05	13/F	66.85	130	29	89	41	67	29

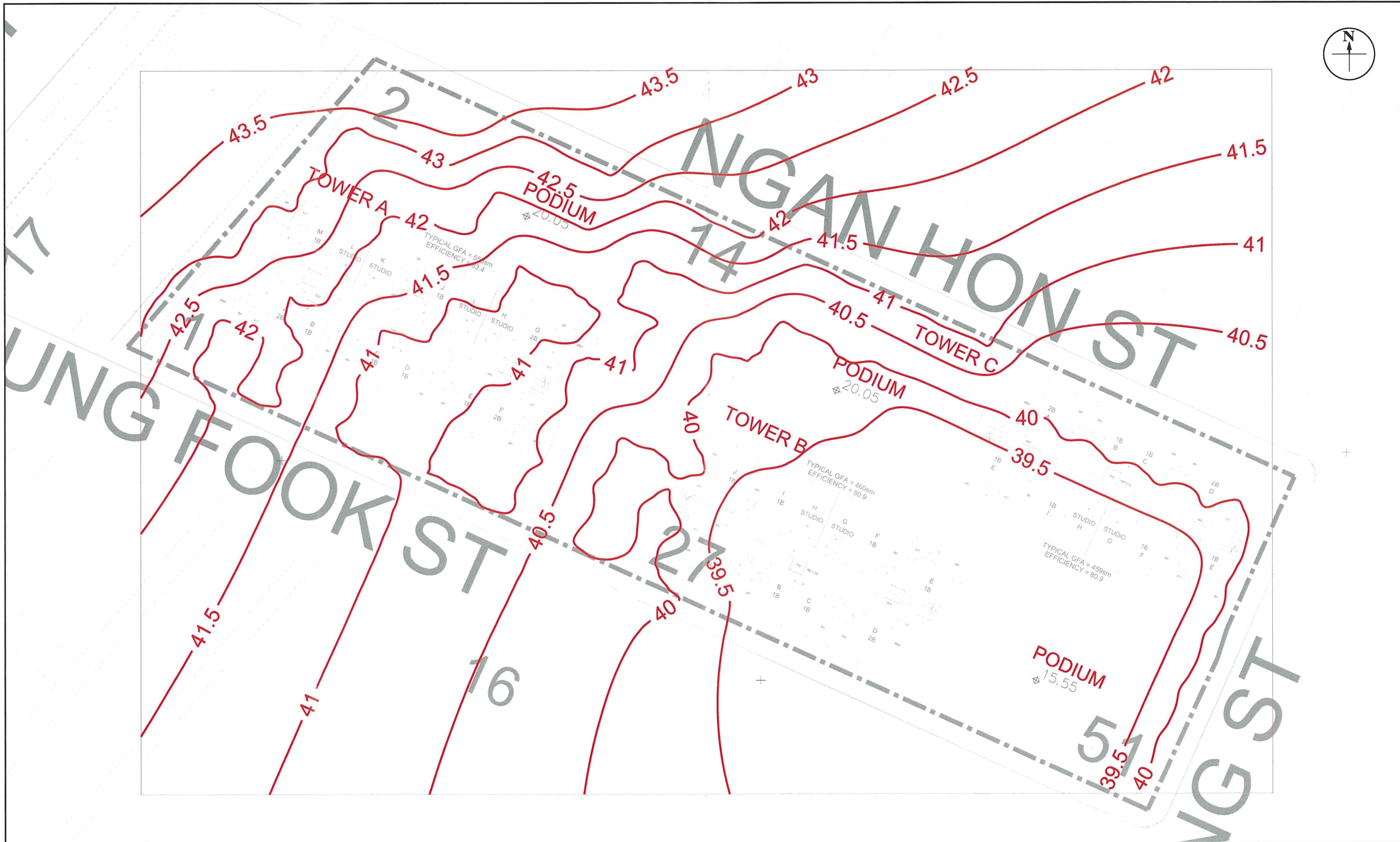
ASR Label	Location		Local Ground, Z (mPD)	Floor	Flag Pole Receiver Height, mPD	NO2		RSP		FSP	
	x	y				19th Hourly Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.	10th Highest Daily Average Conc.	Annual Average Conc.
	Easting	Northing									
B07_14	837570	819475	5.05	15/F	73.15	130	29	89	41	67	29
B07_15	837570	819475	5.05	17/F	79.45	130	29	89	41	67	29
B07_16	837570	819475	5.05	20/F	88.9	130	29	89	41	67	29
B07_17	837570	819475	5.05	23/F	98.35	129	29	89	41	67	29
B08_01	837543	819488	5.05	Retail	6.55	149	40	90	41	67	30
B08_02	837543	819488	5.05	Retail	12.55	147	37	90	41	67	29
B08_03	837543	819488	5.05	Retail	17.05	144	36	90	41	67	29
B08_04	837543	819488	5.05	Clubhouse	21.55	142	35	89	41	67	29
B08_05	837543	819488	5.05	1/F	29.05	138	33	89	41	67	29
B08_06	837543	819488	5.05	2/F	32.2	135	32	89	41	67	29
B08_07	837543	819488	5.05	3/F	35.35	133	32	89	41	67	29
B08_08	837543	819488	5.05	4/F	38.5	133	31	89	41	67	29
B08_09	837543	819488	5.05	5/F	41.65	132	31	89	41	67	29
B08_10	837543	819488	5.05	7/F	47.95	132	30	89	41	67	29
B08_11	837543	819488	5.05	9/F	54.25	131	30	89	41	67	29
B08_12	837543	819488	5.05	11/F	60.55	130	29	89	41	67	29
B08_13	837543	819488	5.05	13/F	66.85	130	29	89	41	67	29
B08_14	837543	819488	5.05	15/F	73.15	130	29	89	41	67	29
B08_15	837543	819488	5.05	17/F	79.45	130	29	89	41	67	29
B08_16	837543	819488	5.05	20/F	88.9	130	29	89	41	67	29
B08_17	837543	819488	5.05	23/F	98.35	129	29	89	41	67	29
Max						164	45	90	41	69	31
AQO						200	40	100	50	75	35

Note: Cell in Orange denotes exceedance compared with the relevant AQOs.

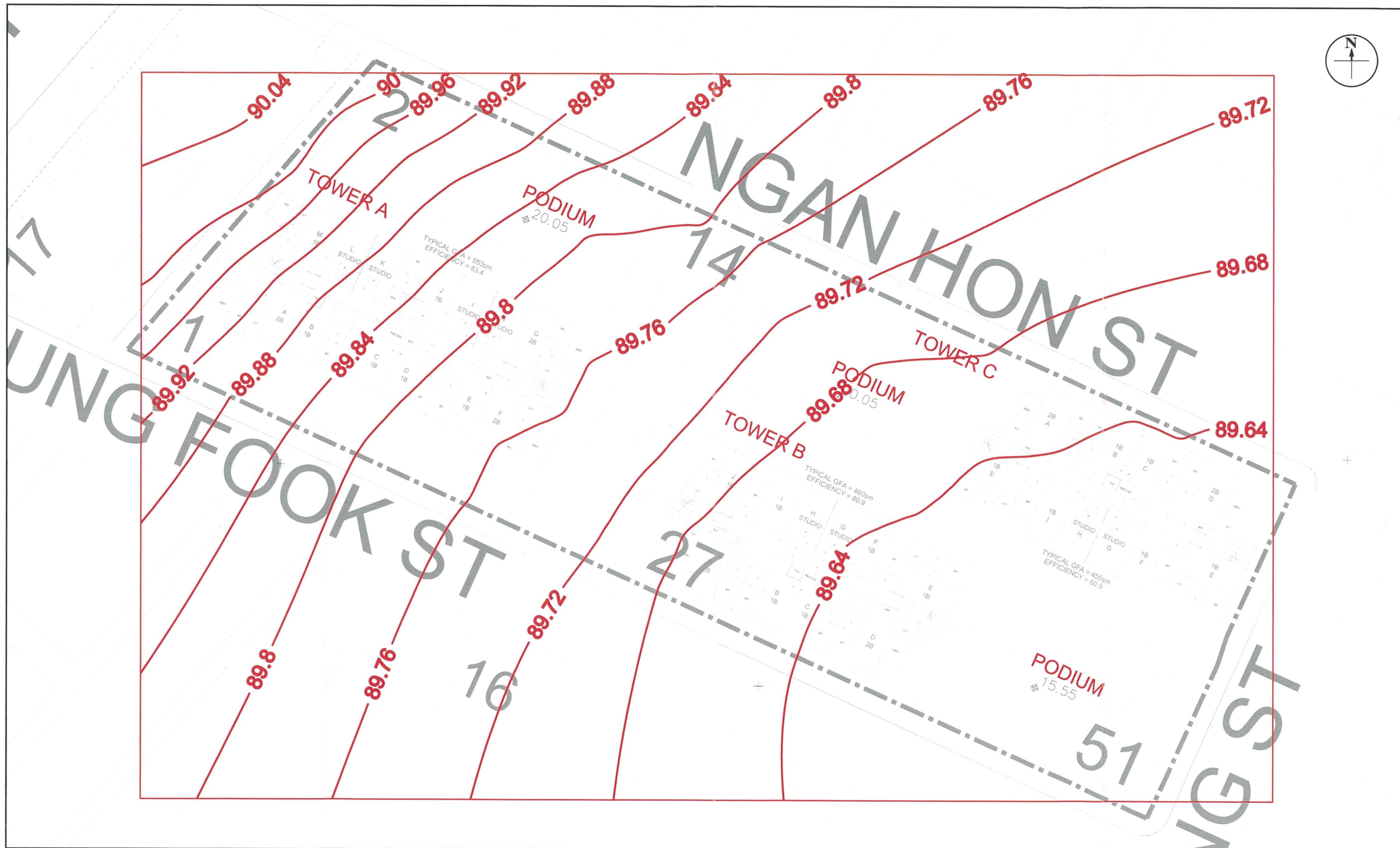
APPENDIX 3.4:

PREDICTED AIR QUALITY IMPACT CONCENTRATION CONTOUR

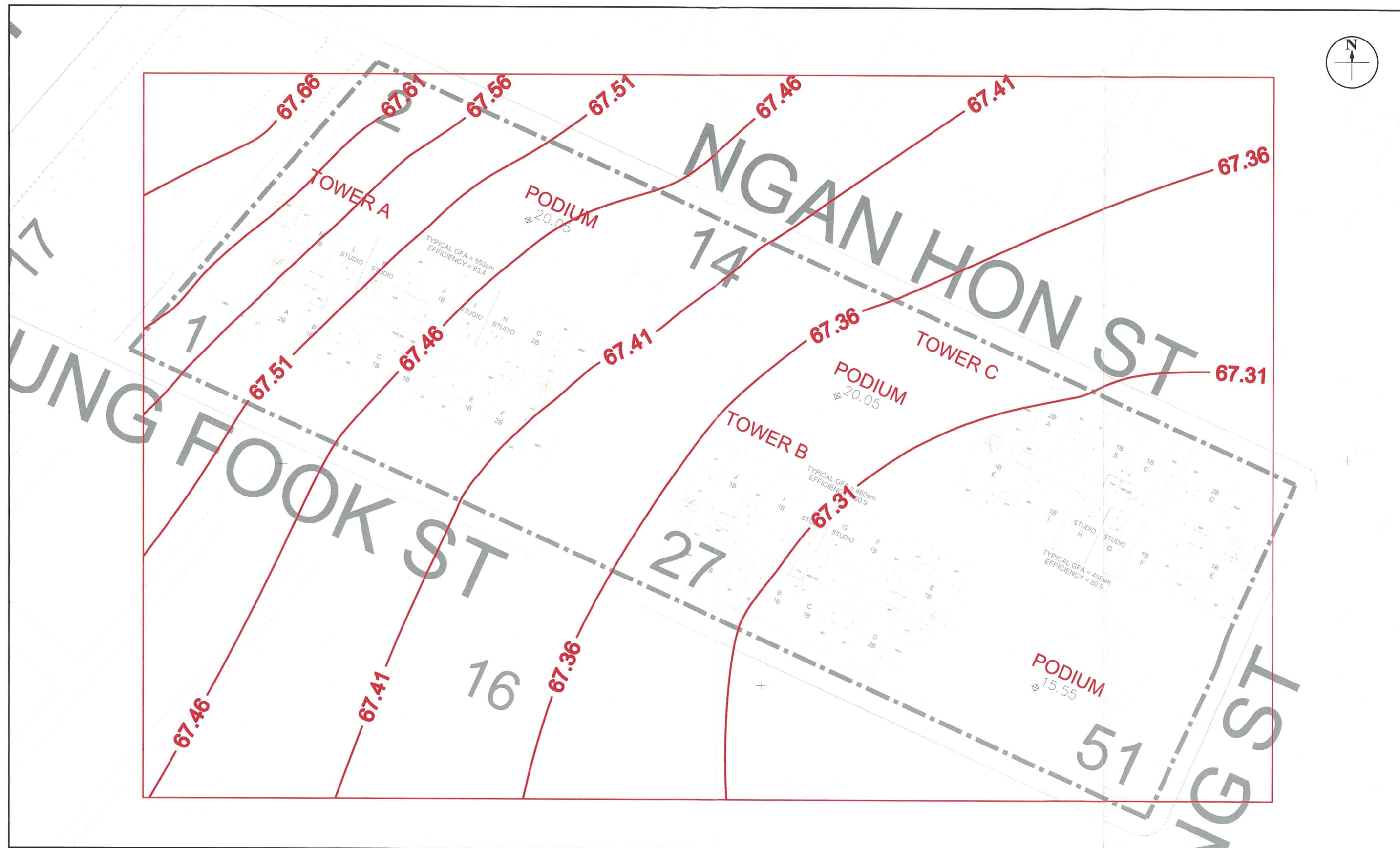




Appendix: 3.4b		RAMBOLL ENVIRON
Title: Contour Map of Annual Average Concentration of NO ₂ in µgm-3 (AQO standard: 40 µgm-3)		Drawn by: RC
		Checked by: TC
Project: Term Environmental Consultancy Services for a Project at Hung Fook Street/ Nan Hon Street, Kowloon City District (KC-010)		Rev.: 1.0
		Date: Jul 2016



Appendix: 3.4c		RAMBOLL ENVIRON	
Title: Contour Map of 10th Highest Daily Average Concentration of RSP in μgm^{-3} (AQO standard: 100 μgm^{-3})		Drawn by: RC	
		Checked by: TC	
Project: Term Environmental Consultancy Services for a Project at Hung Fook Street/ Nan Hon Street, Kowloon City District (KC-010)		Rev.: 1.0	
		Date: Jul 2016	



Appendix: 3.4e

Title: Contour Map of 10th Highest Daily Average Concentration of FSP in $\mu\text{gm-3}$ (AQO standard: $75 \mu\text{gm-3}$)

Project: Term Environmental Consultancy Services for a Project at Hung Fook Street/ Nan Hon Street, Kowloon City District (KC-010)

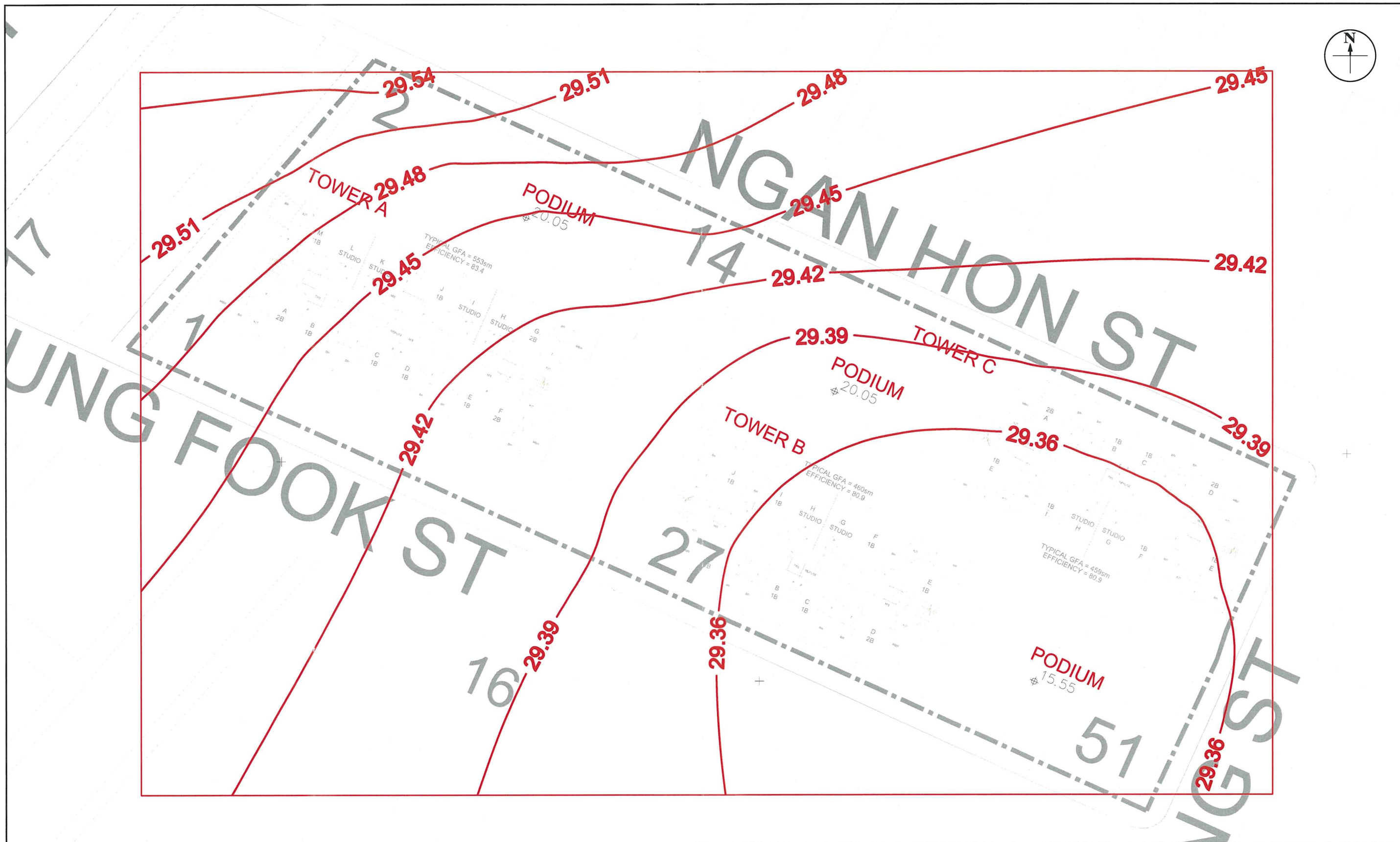
RAMBOLL ENVIRON

Drawn by: RC

Checked by: TC

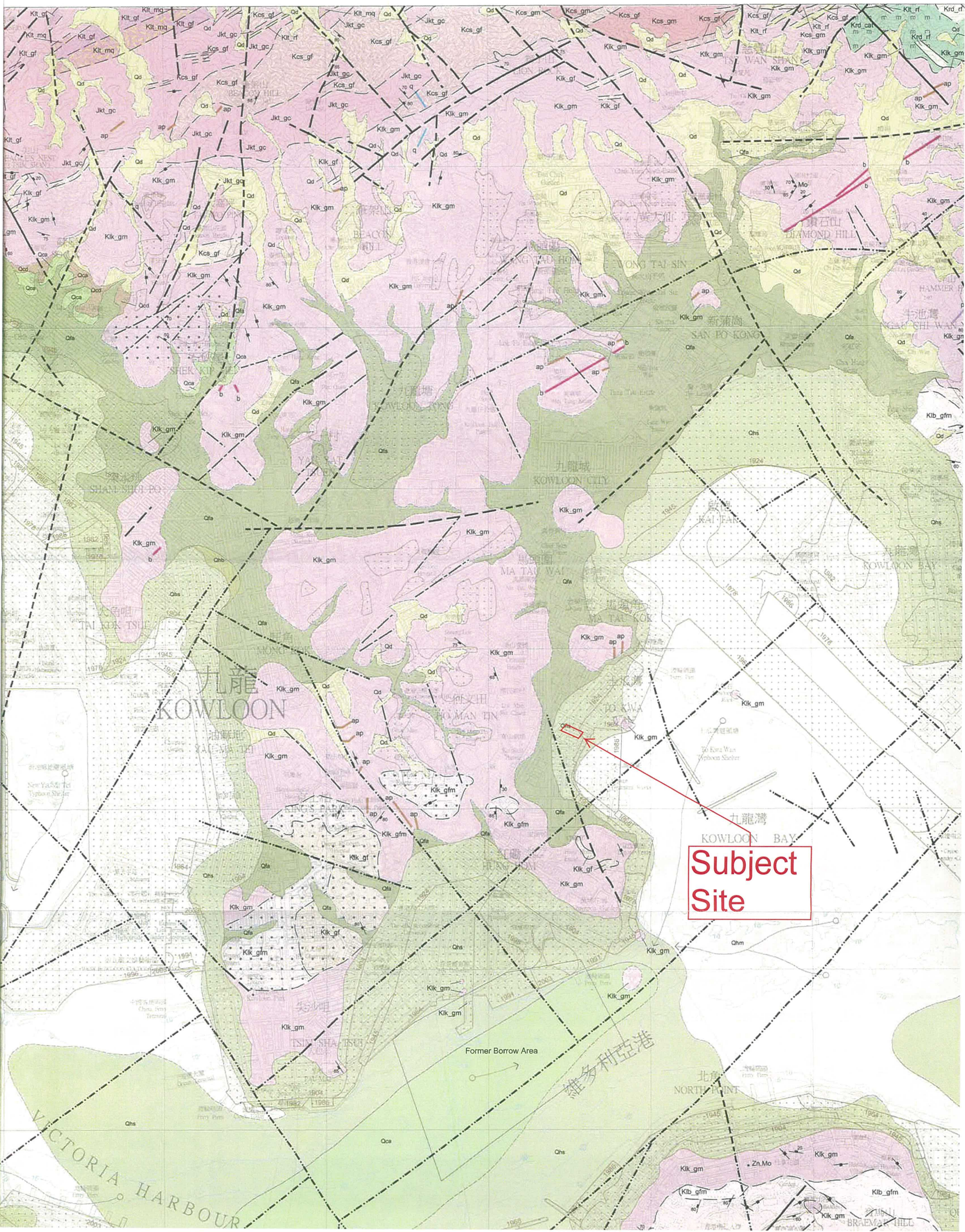
Rev.: 1.0

Date: Jul 2016



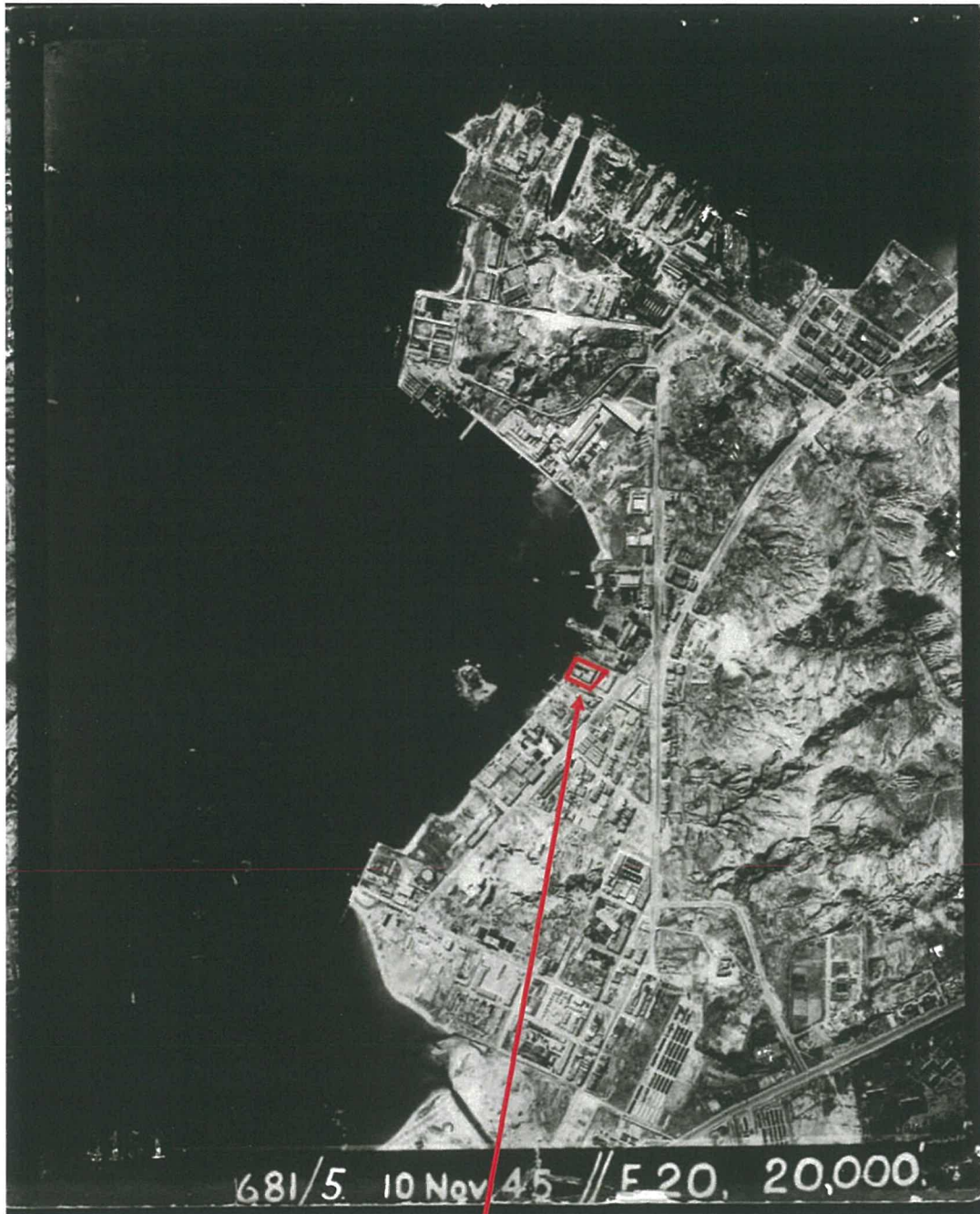
Appendix: 3.4f		RAMBOLL ENVIRON	
Title:	Contour Map of Annual Average Concentration of FSP in µgm-3 (AQO standard: 35 µgm-3)	Drawn by: RC	
		Checked by: TC	
Project:	Term Environmental Consultancy Services for a Project at Hung Fook Street/ Nan Hon Street, Kowloon City District (KC-010)	Rev.: 1.0	
		Date: Jul 2016	

APPENDIX 5.1: EXTRACTED MAP FROM HONG KONG GEOLOGICAL SURVEY



APPENDIX 5.2: HISTORICAL AERIAL PHOTOS

Historical Aerial Photo – Year 1945



Subject Site

Height of Aerial Photographs: 20,000 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 1967

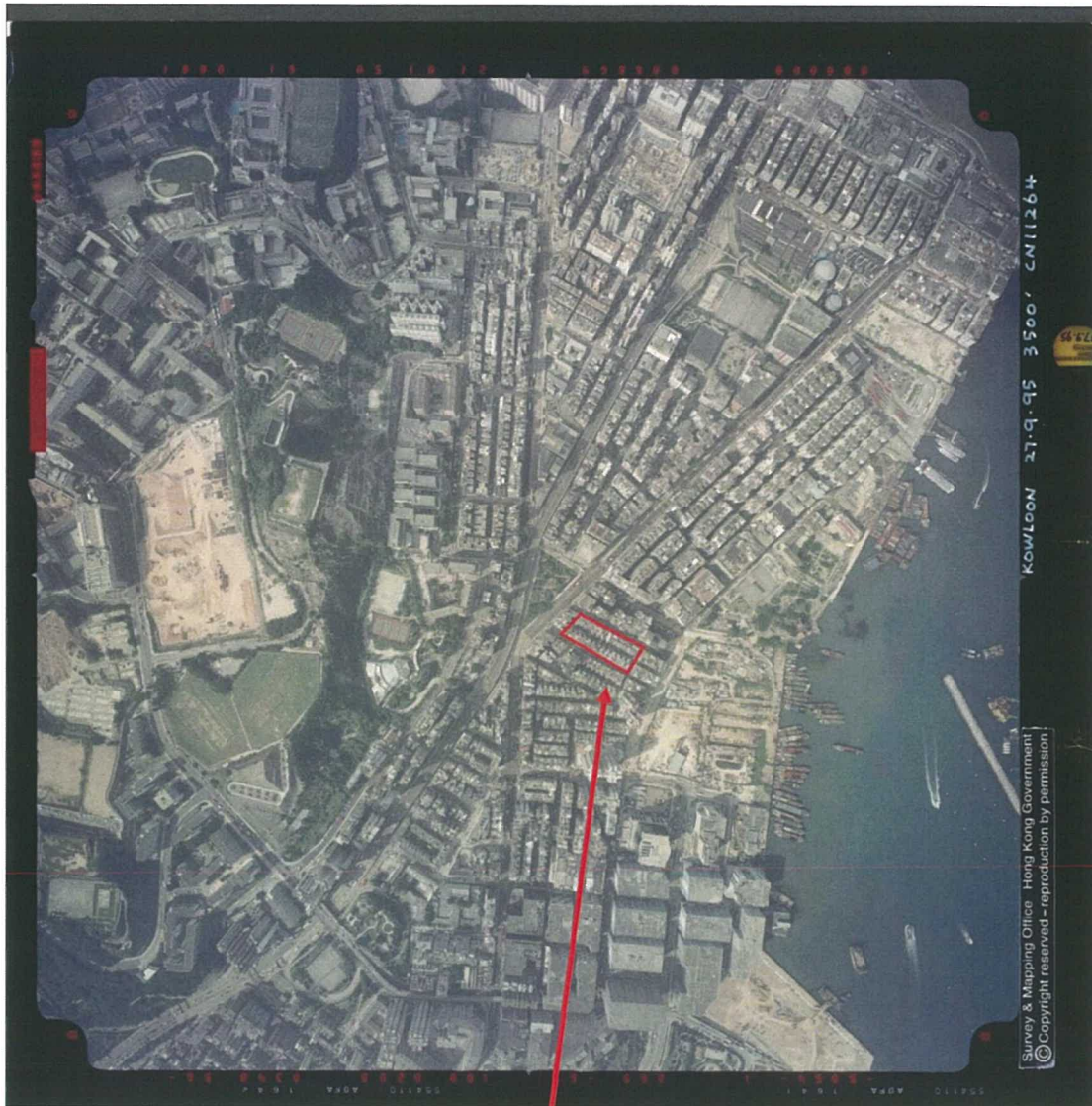


Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 1995

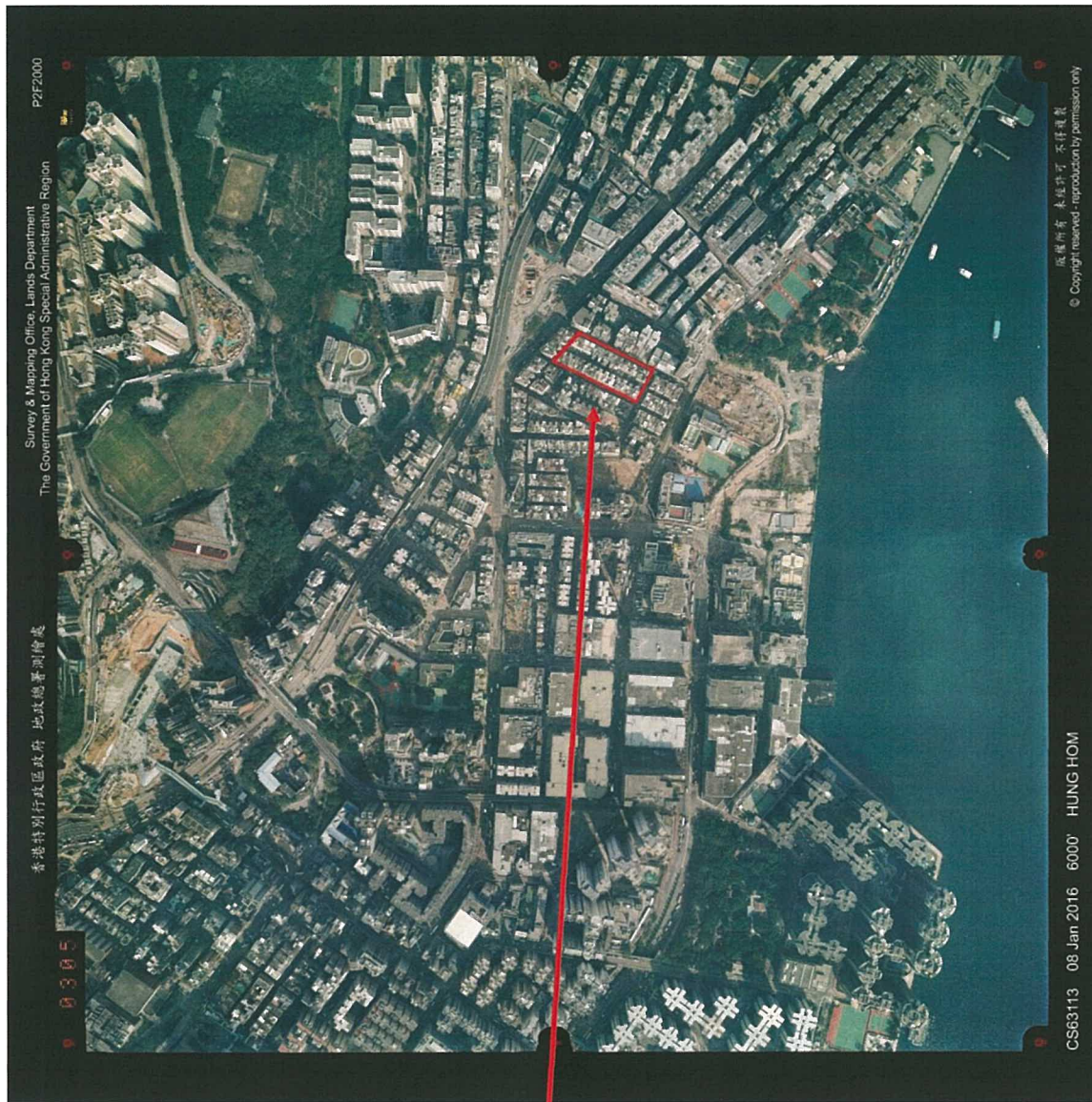


Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

Historical Aerial Photo – Year 2016



Subject Site

Height of Aerial Photographs: 6250 Ft

Period / Year	Landuse / Description
1945	The Subject Site is situated in an urban built area.
1967	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
1995	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.
2016	The buildings located at the Subject Site has remained since its erection from 1957 to 1960. However, it is unable to identify their usage and there may be a change in landuse over time.

APPENDIX 5.3: COPY OF THE LETTERS FROM VARIOUS GOVERNMENT DEPARTMENTS

Mr. Tony Cheng
c/o Ramboll Environ Hong Kong Ltd.
Rm 2403, 24/F, Jubilee Centre
18 Fenwick Street, Wanchai, Hong Kong

1 August 2016

Dear Sir,

**Proposed Development Scheme at Kowloon City (KC010) at
Hung Fook Street / Ngan Hon Street for Urban Renewal Authority**

I refer to your letter dated 26 July 2016 and received on 28 July 2016.

2. Please clarify what type of documents you are required from this department for the purpose of "information pertaining to the change of land uses / past activities / incidents / accidents at the subject site".
3. You are also advised that records for completed private buildings may be inspected at the Building Information Centre of the Buildings Department or via the "Building Records Access and Viewing On-line" system over the Internet by the public upon application and payment of the prescribed fee. For the details, you may refer to the website of "http://www.bd.gov.hk/english/services/index_support4.html".


Yours faithfully,

Ramboll Environ Hong Kong Ltd.

Project: URAKC010E2006/0013

Exchange no.: _____

Circulation:	Read	Action
IC	<input type="checkbox"/>	<input type="checkbox"/>
BT	<input type="checkbox"/>	<input type="checkbox"/>
tole	<input type="checkbox"/>	<input type="checkbox"/>
mvv	<input type="checkbox"/>	<input type="checkbox"/>
IC	<input checked="" type="checkbox"/>	<input type="checkbox"/>
PC	<input checked="" type="checkbox"/>	<input type="checkbox"/>


(George CHEUNG Hiu-wai)
Building Surveyor/Kowloon 6
Buildings Department

Our Ref: (149) in CP OPS EOD 6-20/1 II

Telefax No. : +852 2203 4273

Telephone No. : +852 2203 4324

Date : 3rd August 2016



Explosive Ordnance Disposal Bureau

Hong Kong Police

EOD Depot,
150 Mount Butler Road,
Jardine's Lookout,
Hong Kong.

FACSIMILE TRANSMISSION

To: Ramboll Environ Hong Kong Limited

Addressee's Telefax No.: +852 3465 2899

(Attn.: Tony CHENG)

Your Ref.: URAKC010EI00 0 0006L.15

Total Number of Pages Transmitted: 1
(including this page)

Dated: 3rd August 2016

Please notify Mr. KO Lo-wang (ip-sip-hq-eod@police.gov.hk) if the message is incomplete

Dear Mr. CHENG,

Re: Proposed Development Scheme at Kowloon City (KC010) at Hung Fook Street / Ngan Hon Street for Urban Renewal Authority

Your letter dated 26th July 2016 refers.

We have not found any records of our attending incidents at the mentioned locations. However, searching would be enhanced with specific details such as the specific road number or building names.

Yours faithfully,

KO Lo-wang
Inspector of Police
Explosive Ordnance Disposal Bureau
Hong Kong Police Force

規 劃 署

in future correspondence

香港北角濟華道三百三十三號
北角政府合署

Planning Department

North Point Government Offices
333 Java Road, North Point,
Hong Kong

本函檔號 Your Reference URAKC010EI00_0_0008L.15
本署檔號 Our Reference () in K-R/URA/15
電話號碼 Tel. No. : 2231 4965
傳真機號碼 Fax No. : 2894 9502

By Fax (3465 2899)

3 August 2016

Ramboll Environ Hong Kong Limited
Room 2403, 24/F Jubilee Centre
18 Fenwick Street
Wanchai, Hong Kong
(Attn: Ms Monique Wong)

Dear Ms Wong,

**Proposed URA Development Scheme at Kowloon City (KC010) at
Hung Fook Street/Ngan Hon Street for Urban Renewal Authority**
(Land Contamination Assessment)

I refer to your letter dated 26.7.2016 and our telephone conversation today regarding the captioned. Regarding the zoning history of the site, the site was zoned "commercial/residential" on the first version of the Hung Hom Outline Zoning Plan (OZP) that was exhibited on 1984. It was subsequently rezoned to "Residential (Group A)" in 1987 in the Hung Hom OZP No. S/K9/3 and had remained under the same zoning as of the extant OZP. We have no other information regarding the past uses/activities of the site on our records, you may wish to further consult Lands Department or Building Department to see if they have other available information.

Yours sincerely,

(Johanna CHENG)
for District Planning Officer/Kowloon
Planning Department

Internal

Site Records (K9/319, 396, 400 and 401)