

This document is received on 2025-08-25
The Town Planning Board will formally acknowledge
the date of receipt of the application only upon receipt
of all the required information and documents.

Form No. S16-III
表格第 S16-III 號

APPLICATION FOR PERMISSION
UNDER SECTION 16 OF
THE TOWN PLANNING ORDINANCE
(CAP. 131)

根據《城市規劃條例》(第131章)
第16條遞交的許可申請

**Applicable to Proposal Only Involving Temporary Use/Development of Land
and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas,
or Renewal of Permission for such Temporary Use or Development***

適用於祇涉及位於鄉郊地區或受規管地區土地上及/或建築物內進行
為期不超過三年的臨時用途/發展或該等臨時用途/發展的許可續期的建議*

**Form No. S16-I should be used for other Temporary Use/Development of Land and/or Building (e.g. temporary use/developments in the Urban Area) and Renewal of Permission for such Temporary Use or Development.*

**其他土地上及/或建築物內的臨時用途/發展 (例如位於市區內的臨時用途或發展) 及有關該等臨時用途/發展的許可續期，應使用表格第 S16-I 號。*

Applicant who would like to publish the notice of application in local newspapers to meet one of the Town Planning Board's requirements of taking reasonable steps to obtain consent of or give notification to the current land owner, please refer to the following link regarding publishing the notice in the designated newspapers:
https://www.tpb.gov.hk/en/plan_application/apply.html

申請人如欲在本地報章刊登申請通知，以採取城市規劃委員會就取得現行土地擁有人的同意或通知現行土地擁有人所指定的其中一項合理步驟，請瀏覽以下網址有關在指定的報章刊登通知：
https://www.tpb.gov.hk/tc/plan_application/apply.html

General Note and Annotation for the Form

填寫表格的一般指引及註解

"Current land owner" means any person whose name is registered in the Land Registry as that of an owner of the land to which the application relates, as at 6 weeks before the application is made

「現行土地擁有人」指在提出申請前六星期，其姓名或名稱已在土地註冊處註冊為該申請所關乎的土地的擁有人的人

& Please attach documentary proof 請夾附證明文件

^ Please insert number where appropriate 請在適當地方註明編號

Please fill "NA" for inapplicable item 請在不適用的項目填寫「不適用」

Please use separate sheets if the space provided is insufficient 如所提供的空間不足，請另頁說明

Please insert a 「✓」 at the appropriate box 請在適當的方格內上加上「✓」號

For Official Use Only 請勿填寫此欄	Application No. 申請編號	A/NB-LYT/860
	Date Received 收到日期	2025-08-25

- The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.
申請人須把填妥的申請表格及其他支持申請的文件 (倘有), 送交香港北角渣華道 333 號北角政府合署 15 樓城市規劃委員會(下稱「委員會」)秘書收。
- Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.tpb.gov.hk/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).
請先細閱《申請須知》的資料單張, 然後填寫此表格。該份文件可從委員會的網頁下載 (網址: <http://www.tpb.gov.hk/>), 亦可向委員會秘書處 (香港北角渣華道 333 號北角政府合署 15 樓 - 電話: 2231 4810 或 2231 4835) 及規劃署的規劃資料查詢處(熱線: 2231 5000) (香港北角渣華道 333 號北角政府合署 17 樓及新界沙田上禾輦路 1 號沙田政府合署 14 樓)索取。
- This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.
此表格可從委員會的網頁下載, 亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全, 委員會可拒絕處理有關申請。

1. Name of Applicant 申請人姓名/名稱

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構)

Wealth Generation International Limited (新世代國際有限公司)

2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱 (如適用)

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構)

Lawson David & Sung Surveyors Limited (羅迅測計師行有限公司)

3. Application Site 申請地點

(a) Full address / location / demarcation district and lot number (if applicable) 詳細地址/地點/丈量約份及地段號碼 (如適用)	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.
(b) Site area and/or gross floor area involved 涉及的地盤面積及/或總樓面面積	<input checked="" type="checkbox"/> Site area 地盤面積 1,143 sq.m 平方米 <input checked="" type="checkbox"/> About 約 <input checked="" type="checkbox"/> Gross floor area 總樓面面積 111 sq.m 平方米 <input checked="" type="checkbox"/> About 約
(c) Area of Government land included (if any) 所包括的政府土地面積 (倘有)	..... 570 sq.m 平方米 <input checked="" type="checkbox"/> About 約

(d) Name and number of the related statutory plan(s) 有關法定圖則的名稱及編號	Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan No. S/NE-LYT/19
(e) Land use zone(s) involved 涉及的土地用途地帶	"Agriculture" ("AGR")
(f) Current use(s) 現時用途	Vehicle Repair Workshops (If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)

4. "Current Land Owner" of Application Site 申請地點的「現行土地擁有人」

The applicant 申請人 –

- ☐ is the sole "current land owner"^{#&} (please proceed to Part 6 and attach documentary proof of ownership).
是唯一的「現行土地擁有人」^{#&} (請繼續填寫第 6 部分，並夾附業權證明文件)。
- ☐ is one of the "current land owners"^{#&} (please attach documentary proof of ownership).
是其中一名「現行土地擁有人」^{#&} (請夾附業權證明文件)。
- ☒ is not a "current land owner"[#].
並不是「現行土地擁有人」[#]。
- ☐ The application site is entirely on Government land (please proceed to Part 6).
申請地點完全位於政府土地上 (請繼續填寫第 6 部分)。

5. Statement on Owner's Consent/Notification 就土地擁有人的同意/通知土地擁有人的陳述

- (a) According to the record(s) of the Land Registry as at (DD/MM/YYYY), this application involves a total of "current land owner(s)"[#].
根據土地註冊處截至 年 月 日的記錄，這宗申請共牽涉 名「現行土地擁有人」[#]。

(b) The applicant 申請人 –

- ☐ has obtained consent(s) of "current land owner(s)"[#].
已取得 名「現行土地擁有人」[#]的同意。

Details of consent of "current land owner(s)" [#] obtained 取得「現行土地擁有人」 [#] 同意的詳情		
No. of 'Current Land Owner(s)' 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where consent(s) has/have been obtained 根據土地註冊處記錄已獲得同意的地段號碼/處所地址	Date of consent obtained (DD/MM/YYYY) 取得同意的日期 (日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has notified “current land owner(s)”[#]
已通知 名「現行土地擁有人」[#]。

Details of the “current land owner(s)” [#] notified 已獲通知「現行土地擁有人」 [#] 的詳細資料		
No. of ‘Current Land Owner(s)’ 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where notification(s) has/have been given 根據土地註冊處記錄已發出通知的地段號碼／處所地址	Date of notification given (DD/MM/YYYY) 通知日期(日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☒ has taken reasonable steps to obtain consent of or give notification to owner(s):
已採取合理步驟以取得土地擁有人的同意或向該人發給通知。詳情如下：

Reasonable Steps to Obtain Consent of Owner(s) 取得土地擁有人的同意所採取的合理步驟

- ☐ sent request for consent to the “current land owner(s)” on _____ (DD/MM/YYYY)^{#&}
於_____ (日/月/年)向每一名「現行土地擁有人」[#]遞交要求同意書[&]

Reasonable Steps to Give Notification to Owner(s) 向土地擁有人發出通知所採取的合理步驟

- ☐ published notices in local newspapers on _____ (DD/MM/YYYY)[&]
於_____ (日/月/年)在指定報章就申請刊登一次通知[&]

- ☒ posted notice in a prominent position on or near application site/premises on
15.5.2025 - 29.5.2025 (DD/MM/YYYY)[&]
於_____ (日/月/年)在申請地點／申請處所或附近的顯明位置貼出關於該申請的通知[&]

- ☒ sent notice to relevant owners’ corporation(s)/owners’ committee(s)/mutual aid committee(s)/management office(s) or rural committee on 22.4.2025 (DD/MM/YYYY)[&]
於_____ (日/月/年)把通知寄往相關的業主立案法團/業主委員會/互助委員會或管理處，或有關的鄉事委員會[&]

Others 其他

- ☐ others (please specify)
其他（請指明）

Note: May insert more than one 「✓」.

Information should be provided on the basis of each and every lot (if applicable) and premises (if any) in respect of the application.

註：可在多於一個方格內加上「✓」號

申請人須就申請涉及的每一地段（倘適用）及處所（倘有）分別提供資料

6. Type(s) of Application 申請類別	
(A) Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas 位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展 (For Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas, please proceed to Part (B)) (如屬位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期，請填寫(B)部分)	
(a) Proposed use(s)/development 擬議用途/發展	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years (Please illustrate the details of the proposal on a layout plan) (請用平面圖說明擬議詳情)
(b) Effective period of permission applied for 申請的許可有效期	<input checked="" type="checkbox"/> year(s) 年 3 <input type="checkbox"/> month(s) 個月
(c) Development Schedule 發展細節表	
Proposed uncovered land area 擬議露天土地面積	..... 1,032 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed covered land area 擬議有上蓋土地面積	..... 111 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed number of buildings/structures 擬議建築物/構築物數目	..... 3
Proposed domestic floor area 擬議住用樓面面積	..... NA .....sq.m <input type="checkbox"/> About 約
Proposed non-domestic floor area 擬議非住用樓面面積	..... 111 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed gross floor area 擬議總樓面面積	..... 111 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed height and use(s) of different floors of buildings/structures (if applicable) 建築物/構築物的擬議高度及不同樓層的擬議用途 (如適用) (Please use separate sheets if the space below is insufficient) (如以下空間不足，請另頁說明) One 1-storey Transformer / Switch Room (Height: About 3.5m) Two 1-storey Containers for EV Chargers (Height: About 3m)	
Proposed number of car parking spaces by types 不同種類停車位的擬議數目	
Private Car Parking Spaces 私家車車位	..... NA
Motorcycle Parking Spaces 電單車車位	..... NA
Light Goods Vehicle Parking Spaces 輕型貨車泊車位	..... NA
Medium Goods Vehicle Parking Spaces 中型貨車泊車位	..... NA
Heavy Goods Vehicle Parking Spaces 重型貨車泊車位	..... NA
Others (Please Specify) 其他 (請列明)	..... 19 for Electric Taxi (2.5m x 5m)
Proposed number of loading/unloading spaces 上落客貨車位的擬議數目	
Taxi Spaces 的士車位	..... NA
Coach Spaces 旅遊巴車位	..... NA
Light Goods Vehicle Spaces 輕型貨車車位	..... NA
Medium Goods Vehicle Spaces 中型貨車車位	..... NA
Heavy Goods Vehicle Spaces 重型貨車車位	..... NA
Others (Please Specify) 其他 (請列明)	..... NA

Proposed operating hours 擬議營運時間 24 hours from Mondays to Sundays (including Public Holidays)																																	
(d) Any vehicular access to the site/subject building? 是否有車路通往地盤／有關建築物？	Yes 是 No 否	<input checked="" type="checkbox"/> There is an existing access. (please indicate the street name, where appropriate) 有一條現有車路。(請註明車路名稱(如適用)) Sha Tau Kok Road - Lung Yeuk Tau <input type="checkbox"/> There is a proposed access. (please illustrate on plan and specify the width) 有一條擬議車路。(請在圖則顯示，並註明車路的闊度) <input type="checkbox"/>																															
(e) Impacts of Development Proposal 擬議發展計劃的影響 (If necessary, please use separate sheets to indicate the proposed measures to minimise possible adverse impacts or give justifications/reasons for not providing such measures. 如需要的話，請另頁註明可盡量減少可能出現不良影響的措施，否則請提供理據/理由。)																																	
(i) Does the development proposal involve alteration of existing building? 擬議發展計劃是否包括現有建築物的改動？	Yes 是 No 否	<input type="checkbox"/> Please provide details 請提供詳情 <input checked="" type="checkbox"/>																															
(ii) Does the development proposal involve the operation on the right? 擬議發展是否涉及右列的工程？	Yes 是 No 否	<input type="checkbox"/> (Please indicate on site plan the boundary of concerned land/pond(s), and particulars of stream diversion, the extent of filling of land/pond(s) and/or excavation of land) (請用地盤平面圖顯示有關土地／池塘界線，以及河道改道、填塘、填土及／或挖土的細節及／或範圍) <input type="checkbox"/> Diversion of stream 河道改道 <input type="checkbox"/> Filling of pond 填塘 Area of filling 填塘面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填塘深度 m 米 <input type="checkbox"/> About 約 <input type="checkbox"/> Filling of land 填土 Area of filling 填土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填土厚度 m 米 <input type="checkbox"/> About 約 <input type="checkbox"/> Excavation of land 挖土 Area of excavation 挖土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of excavation 挖土深度 m 米 <input type="checkbox"/> About 約 <input checked="" type="checkbox"/>																															
(iii) Would the development proposal cause any adverse impacts? 擬議發展計劃會否造成不良影響？	<table border="0"> <tr> <td>On environment 對環境</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On traffic 對交通</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On water supply 對供水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On drainage 對排水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On slopes 對斜坡</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Affected by slopes 受斜坡影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Landscape Impact 構成景觀影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Tree Felling 砍伐樹木</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Visual Impact 構成視覺影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Others (Please Specify) 其他 (請列明)</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> </table>			On environment 對環境	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On traffic 對交通	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On water supply 對供水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On drainage 對排水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On slopes 對斜坡	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Affected by slopes 受斜坡影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Landscape Impact 構成景觀影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Tree Felling 砍伐樹木	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Visual Impact 構成視覺影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Others (Please Specify) 其他 (請列明)	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>
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Others (Please Specify) 其他 (請列明)	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>																															

	<p>Please state measure(s) to minimise the impact(s). For tree felling, please state the number, diameter at breast height and species of the affected trees (if possible)</p> <p>請註明盡量減少影響的措施。如涉及砍伐樹木，請說明受影響樹木的數目、及胸高度的樹幹直徑及品種(倘可)</p> <p>NA</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p>
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(B) Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas
位於鄉郊地區或受規管地區臨時用途/發展的許可續期

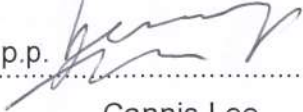
(a) Application number to which the permission relates 與許可有關的申請編號	A/ _____ / _____
(b) Date of approval 獲批給許可的日期	..... (DD 日/MM 月/YYYY 年)
(c) Date of expiry 許可屆滿日期	..... (DD 日/MM 月/YYYY 年)
(d) Approved use/development 已批給許可的用途/發展	
(e) Approval conditions 附帶條件	<p><input type="checkbox"/> The permission does not have any approval condition 許可並沒有任何附帶條件</p> <p><input type="checkbox"/> Applicant has complied with all the approval conditions 申請人已履行全部附帶條件</p> <p><input type="checkbox"/> Applicant has not yet complied with the following approval condition(s): 申請人仍未履行下列附帶條件：</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>Reason(s) for non-compliance: 仍未履行的原因：</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>(Please use separate sheets if the space above is insufficient) (如以上空間不足，請另頁說明)</p>
(f) Renewal period sought 要求的續期期間	<p><input type="checkbox"/> year(s) 年</p> <p><input type="checkbox"/> month(s) 個月</p>

8. Declaration 聲明

I hereby declare that the particulars given in this application are correct and true to the best of my knowledge and belief.
本人謹此聲明，本人就這宗申請提交的資料，據本人所知及所信，均屬真實無誤。

I hereby grant a permission to the Board to copy all the materials submitted in this application and/or to upload such materials to the Board's website for browsing and downloading by the public free-of-charge at the Board's discretion.
本人現准許委員會酌情將本人就此申請所提交的所有資料複製及/或上載至委員會網站，供公眾免費瀏覽或下載。

Signature
簽署

p.p. 
.....
Cannis Lee

☐ Applicant 申請人 / ☒ Authorised Agent 獲授權代理人

Name in Block Letters
姓名（請以正楷填寫）

Associate Director (Planning)

Position (if applicable)
職位（如適用）

Professional Qualification(s) ☒ Member 會員 / ☐ Fellow of 資深會員

專業資格

- ☐ HKIP 香港規劃師學會 / ☐ HKIA 香港建築師學會 /
☐ HKIS 香港測量師學會 / ☐ HKIE 香港工程師學會 /
☐ HKILA 香港園境師學會 / ☐ HKIUD 香港城市設計學會
☐ RPP 註冊專業規劃師
Others 其他 MPIA

on behalf of
代表 **Lawson David & Sung Surveyors Limited**



☒ Company 公司 / ☐ Organisation Name and Chop (if applicable) 機構名稱及蓋章（如適用）

Date 日期

6/8/2025

(DD/MM/YYYY 日/月/年)

Remark 備註

The materials submitted in this application and the Board's decision on the application would be disclosed to the public. Such materials would also be uploaded to the Board's website for browsing and free downloading by the public where the Board considers appropriate.

委員會會向公眾披露申請人所遞交的申請資料和委員會對申請所作的決定。在委員會認為合適的情況下，有關申請資料亦會上載至委員會網頁供公眾免費瀏覽及下載。

Warning 警告

Any person who knowingly or wilfully makes any statement or furnish any information in connection with this application, which is false in any material particular, shall be liable to an offence under the Crimes Ordinance.

任何人在明知或故意的情況下，就這宗申請提出在任何要項上是虛假的陳述或資料，即屬違反《刑事罪行條例》。

Statement on Personal Data 個人資料的聲明

- The personal data submitted to the Board in this application will be used by the Secretary of the Board and Government departments for the following purposes:
委員會就這宗申請所收到的個人資料會交給委員會秘書及政府部門，以根據《城市規劃條例》及相關的城市規劃委員會規劃指引的規定作以下用途：
(a) the processing of this application which includes making available the name of the applicant for public inspection when making available this application for public inspection; and
處理這宗申請，包括公布這宗申請供公眾查閱，同時公布申請人的姓名供公眾查閱；以及
(b) facilitating communication between the applicant and the Secretary of the Board/Government departments.
方便申請人與委員會秘書及政府部門之間進行聯絡。
- The personal data provided by the applicant in this application may also be disclosed to other persons for the purposes mentioned in paragraph 1 above.
申請人就這宗申請提供的個人資料，或亦會向其他人士披露，以作上述第 1 段提及的用途。
- An applicant has a right of access and correction with respect to his/her personal data as provided under the Personal Data (Privacy) Ordinance (Cap. 486). Request for personal data access and correction should be addressed to the Secretary of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.
根據《個人資料(私隱)條例》(第 486 章)的規定，申請人有權查閱及更正其個人資料。如欲查閱及更正個人資料，應向委員會秘書提出有關要求，其地址為香港北角渣華道 333 號北角政府合署 15 樓。

Gist of Application 申請摘要

(Please provide details in both English and Chinese as far as possible. This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information.)

(請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)

Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)
Location/address 位置／地址	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.
Site area 地盤面積	1,143 sq. m 平方米 <input checked="" type="checkbox"/> About 約 (includes Government land of 包括政府土地 570 sq. m 平方米 <input checked="" type="checkbox"/> About 約)
Plan 圖則	Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan No. S/NE-LYT/19
Zoning 地帶	"Agriculture" ("AGR")
Type of Application 申請類別	<input checked="" type="checkbox"/> Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區的臨時用途/發展為期 <input checked="" type="checkbox"/> Year(s) 年 <u>3</u> <input type="checkbox"/> Month(s) 月 _____ <input type="checkbox"/> Renewal of Planning Approval for Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期為期 <input type="checkbox"/> Year(s) 年 _____ <input type="checkbox"/> Month(s) 月 _____
Applied use/ development 申請用途/發展	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years

(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	111 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.1 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用	NA	
	Non-domestic 非住用	3	
(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	NA <input type="checkbox"/> (Not more than 不多於) m 米	
		NA <input type="checkbox"/> (Not more than 不多於) Storeys(s) 層	
	Non-domestic 非住用	3 - 3.5 <input checked="" type="checkbox"/> (Not more than 不多於) m 米	
		1 <input checked="" type="checkbox"/> (Not more than 不多於) Storeys(s) 層	
(iv) Site coverage 上蓋面積	10 % <input checked="" type="checkbox"/> About 約		
(v) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數		19
	Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) Electric Taxi _____ _____		NA NA NA NA NA 19 (2.5m x 5m)
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數		NA
	Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) _____ _____		NA NA NA NA NA NA

Submitted Plans, Drawings and Documents 提交的圖則、繪圖及文件		
	Chinese 中文	English 英文
Plans and Drawings 圖則及繪圖		
Master layout plan(s)/Layout plan(s) 總綱發展藍圖／布局設計圖	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Block plan(s) 樓宇位置圖	<input type="checkbox"/>	<input type="checkbox"/>
Floor plan(s) 樓宇平面圖	<input type="checkbox"/>	<input type="checkbox"/>
Sectional plan(s) 截視圖	<input type="checkbox"/>	<input type="checkbox"/>
Elevation(s) 立視圖	<input type="checkbox"/>	<input type="checkbox"/>
Photomontage(s) showing the proposed development 顯示擬議發展的合成照片	<input type="checkbox"/>	<input type="checkbox"/>
Master landscape plan(s)/Landscape plan(s) 園境設計總圖／園境設計圖	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Location Plan, Lot Index Plan, Plan showing Vehicular Access to the Application Site, Photos of Electric Charging Device and Site Photos		
Reports 報告書		
Planning Statement/Justifications 規劃綱領/理據	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Environmental assessment (noise, air and/or water pollutions) 環境評估（噪音、空氣及／或水的污染）	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on vehicles) 就車輛的交通影響評估	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Traffic impact assessment (on pedestrians) 就行人的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Visual impact assessment 視覺影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Landscape impact assessment 景觀影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Tree Survey 樹木調查	<input type="checkbox"/>	<input type="checkbox"/>
Geotechnical impact assessment 土力影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Drainage impact assessment 排水影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Sewerage impact assessment 排污影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Risk Assessment 風險評估	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Drainage Proposal		
Note: May insert more than one 「✓」. 註：可在多於一個方格內加上「✓」號		

Note: The information in the Gist of Application above is provided by the applicant for easy reference of the general public. Under no circumstances will the Town Planning Board accept any liabilities for the use of the information nor any inaccuracies or discrepancies of the information provided. In case of doubt, reference should always be made to the submission of the applicant.

註：上述申請摘要的資料是由申請人提供以方便市民大眾參考。對於所載資料在使用上的問題及文義上的歧異，城市規劃委員會概不負責。若有任何疑問，應查閱申請人提交的文件。



Ref: LDS/PLAN/7140

Section 16 Planning Application

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.

Planning Statement

Applicant

Wealth Generation International Limited (新世代國際有限公司)

Prepared by

Lawson David & Sung Surveyors Limited

August 2025

Executive Summary

This planning statement is prepared in support of a planning application for proposed temporary electric vehicle (EV) charging station (for electric taxi) ("the proposed development") for a period of 3 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (the "Application Site").

The Application Site, covering an area of about 1,143 sq.m. (including Government Land of about 570 sq.m.), falls within an area zoned "Agriculture" ("AGR") on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022. According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

The Application Site is currently used as vehicle repair workshops. In consideration of the growing popularity of the EVs in Hong Kong and introduction of 3,000 electric taxis expected in 2026, the Applicant, in cooperation with one of the selected taxi fleet operators, intends to convert the Application Site into an EV charging station to provide EV charging facilities for electric taxis.

The proposed EV charging station will provide a total of 19 parking spaces (2.5m x 5m) for electric taxis. 12 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) quick chargers with an output power of 120kW. The remaining 7 parking spaces will be designated as parking / waiting spaces for the charging facilities and free of charge.

Two 1-storey (height: about 3m) containers for EV chargers, each with a floor area of about 29 sq.m., will be situated at the southern boundary of the Application Site. Each container will provide 6 EV charging devices for electric taxis. A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices.

The Applicant will maintain the existing ingress/egress point of about 9m wide at the northern boundary of the Application Site, connecting to Sha Tau Kok Road – Lung Yeuk Tau. The proposed hours of operation at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays).

The justifications of this application are:

1. The proposed development is in line with Government policy to promote EVs usage;
2. The proposed development meets the demand for quick EV charging facilities;
3. The proposed development does not contravene the planning intention of "AGR" zone;
4. The proposed development is considered not incompatible with surrounding land uses;
5. The proposed development will promote optimization of valuable land resources;
6. The proposed development will not hinder future development of New Territories North New Town; and
7. The proposed development will not generate adverse traffic, drainage and environmental impacts on the surrounding areas.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

行政摘要

此規劃報告書旨在支持在新界粉嶺沙頭角公路 - 龍躍頭段丈量約份第 83 約補租地段第 464 號 A 分段餘段(部分)及毗連政府土地 (“申請地點”), 作為期三年的擬議臨時電動車充電站(電動的士) (“擬議發展”)的規劃申請。

申請地點的面積約 1,143 平方米 (包括政府土地約 570 平方米), 座落於在 2022 年 12 月 16 日刊憲的龍躍頭及軍地南分區計劃大綱核准圖 (圖則編號: S/NE-LYT/19) 上的「農業」地帶。根據該大綱圖的註釋, 不超過三年土地或建築物的臨時用途或發展, 須先向城規會提出申請。

申請地點現時為汽車修理工場。考慮到本地電動車日趨普及以及政府將於 2026 年引入 3,000 輛電動的士, 申請人將與一支被政府選中的的士車隊合作, 擬將申請地點改作成電動車充電站, 為電動的士提供充電設施。

擬議電動車充電站將提供 19 個供電動的士停泊的停車位(2.5 米 x 5 米), 其中 12 個停車位為配備電動車充電設備的收費停車位, 所有充電裝置均是輸出功率為 120kW 的直流快充裝置。剩餘的 7 個停車位將用作充電設施的停車 / 等候區, 並免費使用。申請地點南邊將設置兩個一層高(高度約 3 米, 樓面面積各約 29 平方米)的改裝貨櫃, 以容納電動車充電器, 每個貨櫃可提供 6 個電動的士充電器。貨櫃之間將提供一個一層高(高度約 3.5 米, 樓面面積約 53 平方米)的電力變壓器房 / 掣房, 為所有充電設備提供足夠的電力。

申請人將繼續使用申請地點北面連接到沙頭角公路 - 龍躍頭段的出入口(約 9 米闊)。申請地點的營運時間為星期一至日(包括公眾假期)24 小時運作。

本規劃申請的理據為:

1. 擬議發展支持政府推動電動車普及化的政策;
2. 擬議發展能滿足對電動車快速充電設施的需求;
3. 擬議發展不會違背「農業」地帶的規劃意向;
4. 擬議發展與周邊土地用途兼容;
5. 擬議發展可有效利用寶貴的土地資源;
6. 擬議發展不會阻礙未來新界北新市鎮的發展; 及
7. 擬議發展不會對附近的交通、排水及環境構成不良的影響。

基於本規劃報告書所述的理據, 敬希 各城規會委員及有關政府部門能批准此項申請, 作為期三年的臨時用途。

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1. Introduction

1.1 Purpose

Pursuant to Section 16 of the Town Planning Ordinance, this Planning Statement Report is submitted to the Town Planning Board (“the Board”) in support of a planning application for proposed temporary electric vehicle charging station (for electric taxi) (“the proposed development”) for a period of 3 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (hereafter referred to as “the Application Site”). **Figure 1** shows the location of the Application Site and the Lot Index Plan indicating the concerned lot is shown per **Figure 2**.

The Application Site, covering an area of about 1,143 sq.m. (including Government land of about 570 sq.m.), falls within an area zoned “Agriculture” (“AGR”) on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022. According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

The Application Site is currently occupied by vehicle repair workshops. In view of the growing demand for electric vehicles (EVs), the Applicant intends to convert the Application Site into an EV charging station to provide EV charging facilities for electric taxis. It is considered a preferred location for the proposed development.

1.2 Background of the Application

In response of combating global climate change, electrification of transportation system has become a global trend. EVs are considered a preferable choice to conventional vehicles with no tailpipe emissions of air pollutants, which can help improve air quality and reduce greenhouse gas emissions.

To drive Hong Kong towards the vision of “Zero Carbon Emissions ● Clean Air ● Smart City”, the Government has announced the “Hong Kong Roadmap on Popularisation of EV” on 17 March 2021, which outlines the long-term objectives to achieve zero vehicular emissions before 2050 and promote wider or eventual full adoption of EVs in Hong Kong. The roadmap includes measures such as phasing out conventional fuel-propelled vehicles by 2035, expanding EV charging network in both private and public sectors, enhancing education and training programs for maintenance services and introducing trials of electric commercial vehicles for public transportations.

On 10 December 2024, the Government has also announced the “Green Transformation Roadmap of Public Buses and Taxis” to expand the adoption of EVs in public transportation with introduction of 3,000 electric taxis by mid-2026. The Government has also highlighted a multi-pronged approach in promoting the expansion of EV charging facilities, so as to gradually develop a territory-wide network for ensuring sufficient and convenient charging support for suiting the needs of various EVs.

As at February 2025, the number of EVs account for about 12.6% of the total number of vehicles in Hong Kong, and is expected to increase with the ongoing promotion and campaigns by the Government. In the Chief Executive’s 2024 Policy Address, the Government has also reiterated their efforts to expand the charging network for EVs with new scheme to encourage private sector to install quick-charging facilities. It is targeted to have a total of 3,000 quick chargers installed by 2030.

Additionally, the Government has announced 5 selected taxi fleet operators to enhance taxi service quality and reform the taxi trade on 3 March 2025, in which all selected operators will provide electric taxis to promote green transport in Hong Kong.

In support of the Government's initiatives, the Applicant, in partnership with one of the selected taxi fleets (see endorsement letter from the Transport Department at **Appendix 1**), intends to convert the Application Site into an EV charging station to provide charging facilities for electric taxi. All chargers will be direct current (DC) chargers with output power of 120kW to increase the charging efficiency.

1.3 Organization of the Planning Statement

This planning statement is divided into 6 chapters. Chapter 1 is an introduction outlining the above background of the planning application. Chapter 2 will then illustrate the site context and land status followed by Chapter 3 which describes the planning context in details. Chapter 4 gives the particulars of the proposed development. Detailed accounts of planning justifications of the proposed development are presented in Chapter 5. The planning statement finally concludes with a summary in Chapter 6.

2. Site Context

2.1 The Application Site and Its Existing Condition

The Application Site, with an area of about 1,143 sq.m. (including Government land of about 570 sq.m.), is located next to Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T. The Application Site is a piece of flat land, partly hard-paved and currently being used as vehicle repair workshops. The Application Site is partially fenced off by chain-link fence of about 2.5m high (see **Site Photos**). All the existing temporary structures will be demolished.

2.2 Surrounding Land Uses

The surrounding areas are predominantly rural in character and comprise of village houses, vehicle repair workshops, parking of vehicles, warehouses / storage yards and vacant land. To the east and further east are Kwan Tei Children's Playground and the village settlements of Kwan Tei. To the further southeast is a temporary public vehicle park (Application No. A/NE-LYT/815) within the same "AGR" zone approved by the Board on 26.1.2024. To the north across Sha Tau Kok Road – Lung Yeuk Tau are a series of vehicle repair workshops and warehouses / storage yards within the "Open Storage" ("OS") zone. To the west is the industrial premises of Tung Chun Soy Sauce & Canned Food Company Limited. To the south is vacant land intermixed with temporary structures.

2.3 Accessibility

The Application Site is directly accessible via Sha Tak Kok Road – Lung Yeuk Tau. The existing entrance to the Application Site is about 9m wide which is located to the north. A plan showing the vehicular access to the Application Site is provided at **Figure 3**.

2.4 Land Status

According to the records of the Land Registry, the subject lot is held under Block Government Lease and is demised as agricultural use with lease term for 75 years, from 1.7.1898 and is renewable for a further term of 24 years.

The subject lot is an old schedule agricultural lot. The Applicant will apply for a Short Term Waiver (STW) for the proposed structures and a Short Term Tenancy (STT) for the use of the Government land to the Lands Department upon obtaining planning approval for this application.

3. Planning Context

3.1 Outline Zoning Plan

The Application Site currently falls within an area zoned “Agriculture” (“AGR”) on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022 (see **Figure 1**).

The planning intention of “AGR” zone is “primarily to retain and safeguard good quality agricultural land/farm/fish ponds for agricultural purposes. It is also intended to retain fallow arable land with good potential for rehabilitation for cultivation and other agricultural purposes.”

According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

3.2 Previous Application

Part of the Application Site is subject to 1 previously approved planning application and the details are as follows:-

Application No.	Applied Use	Location	TPB's Decision (Date)
A/NE-LYT/161	Cover Structures for Vehicle and Construction Machinery Repair Workshop	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land	Approved with Conditions (31.7.1998)

3.3 Similar Application

There is no similar application for EV charging station use within the same “AGR” zone under the Lung Yeuk Tau and Kwan Tei South OZP.

4. Development Proposal

4.1 Site Configuration and Layout

The Applicant proposes to use the Application Site for a temporary EV charging station for a period of 3 years. The proposed development intends to provide EV charging facilities for electric taxis of a taxi fleet.

The Application Site has an area of about 1,143 sq.m. The proposed EV charging station will provide 19 parking spaces (2.5m x 5m) for electric taxis (see Layout Plan at **Figure 4**). 12 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) ultra fast chargers with output power of 120kW. The remaining 7 parking spaces will be designated as parking / waiting spaces for the charging facilities and will be free of charge.

Two 1-storey (height: about 3m) containers for placements of EV chargers, each with a floor area of about 29 sq.m., will be situated at the southern boundary of the Application Site. Each container will provide 6 EV chargers for electric taxis (see **Figure 5**). A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices.

The Application Site will be fenced off by 2.5m high chain-link fence on all sides. Only electric taxis with valid licenses issued under the Road Traffic (Registration and Licensing of Vehicles) Regulations are allowed to be parked on the Application Site.

The key development parameters of the application are shown below:-

Applied Use	Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years
Site Area	About 1,143 sq.m. (including 570 sq.m. of Government land)
Total Floor Area (Non-domestic)	About 111 sq.m.
No. of Structures	3 - 1 proposed transformer / switch room - 2 proposed containers for EV chargers
Height of Structures	About 3 – 3.5m (1 storey)
No. of Parking Spaces	19 Nos. (2.5m x 5m) - 12 EV charging spaces for electric taxis - 7 parking / waiting spaces for electric taxis

4.2 Site Operations

The proposed development will only provide parking and EV charging facilities for electric taxis. All EV chargers onsite will be quick chargers that will provide electric taxis 600km of mileage when charged for about 30 minutes. The proposed hours of operation at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays). The Applicant also confirms that there will be no repairing, car washing, paint spraying and other workshop activities conducted at the Application Site. No land filling work will be conducted at the Application Site.

The Applicant will collaborate with Sino Development (International) Company Limited (新科發展(國際)有限公司), one of the 5 selected taxi fleet operators by the Government, to provide various quick EV charging facilities in various locations in Hong Kong (see **Appendix 1**). The fee arrangement will be similar to other EV charging station in the market, in which users will be charged at time basis as they go. A mobile application catered for electric taxi drivers will be adopted, which will include the payment platform for EV charging and provide real-time availability of EV charging spaces at the EV charging stations managed by the Applicant.

4.3 Traffic Arrangement and Traffic Impact Assessment

The Application Site can be accessed via Sha Tau Kok Road – Lung Yeuk Tau (see **Figure 3**). The Applicant will maintain the existing ingress/egress point at the northern boundary of about 9m wide. A Traffic Impact Assessment (TIA) (see **Appendix 2**) is submitted to assess the potential traffic impact on the existing and future road network.

To analyze the existing traffic conditions, a traffic count survey was conducted on 22 May 2025 at the nearby critical links and junctions. The peak hour was identified to be between 1630 - 1730. Based on the existing traffic flows, all concerned junctions were performing satisfactorily and all concerned road sections were operating with spare capacity during the peak hours.

A swept path analysis is also included in the TIA to demonstrate that satisfactory manoeuvring of vehicles entering to and existing from the Application Site and manoeuvring within the Application Site. There will be no difficulties in internal traffic circulation sense as sufficient spaces for vehicle manoeuvring have been reserved within the Application Site such that no vehicle has to queue back to or reverse onto/from Sha Tau Kok Road – Lung Yeuk Tau.

As identified in the TIA report, the peak traffic generation of the proposed development will be either between 1500 – 1700 or after midnight, before the shift changes of the electric taxi drivers. The proposed development is expected to generate a two-way traffic of 48 taxis/hour during the peak hour. By analyzing the Year 2028 Design Flows, which were derived from applying a nominal growth to the existing traffic flow plus the additional traffic generated and attracted by the proposed development, both the junction and link capacity assessments show that the concerned junction and

road links would perform satisfactorily during the peak hour. Based on the assessment result, it can be concluded that the proposed development will not induce additional adverse traffic impact on the surrounding road network and is considered acceptable from traffic engineering point of view.

The mobile application adopted by the Applicant will provide real-time availability of EV charging space and facilitate electric taxi drivers to locate the available EV chargers. The drivers will be directed to the nearest alternative sites if all the EV chargers in the proposed EV charging station are in use. In addition, since quick chargers will be used in the Application Site, the charging time will be much shorter than standard chargers, which allow high turnover rate and cut down waiting time. 7 parking / waiting spaces have been reserved in the Application Site to allow drivers to wait for EV charging space. Therefore, no electric taxi will be queuing outside the Application Site along Sha Tau Kok Road – Lung Yeuk Tau.

4.4 Drainage Proposal

Drainage facilities including 375mm peripheral U-channels and catchpit with trap are proposed to collect the surface runoff and divert them to the existing watercourse to the south. A 100mm high gap will also be proposed at the bottom of the security hoarding/fence wall along the site boundary to ensure no surface runoff from the surroundings flowing onto the Application Site to be obstructed. A drainage proposal (see **Appendix 3**) is submitted to show the proposed drainage design. The Applicant will provide the drainage facilities to the satisfaction of Drainage Services Department.

4.5 Landscape

Majority of the Application Site is occupied by the existing vehicle repairing workshops while wild overgrown and some weeds trees are found at the vacant land. No old and valuable trees are found within the site. The Applicant will remove the weed trees within the site boundary. Meanwhile, existing trees of various species are found immediate outside the eastern and southern boundaries. These existing trees outside the site boundary will not be disturbed by the Applicant and would act as the natural hedges bounding the edge of the development.

The Application Site is located within an area of rural character intermixed with village settlements, vehicle repair workshops, warehouse / storage yards and vacant / unused land, in which private vehicles of the villagers / residents are commonly parked at vacant Government land. Since the proposed development intends to provide EV charging facilities for electric taxis only, it is considered not incompatible with the landscape character of the surrounding areas.

4.6 Fire Service Installations

To minimize the fire hazard, the Applicant will provide fire service installations (FSIs) with Firemen's Emergency Switch to the satisfaction of the Fire Services Department.

4.7 Environmental Consideration

The nature of the proposed use will merely involve provision of car parking space with EV charging facilities. To minimize the possible environmental nuisance, the Applicant will follow the 'Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites' and undertake the following mitigation measures:-

- (a) No more than 19 electric taxis will be allowed at the Application Site at the same time;
- (b) Only electric taxis will be allowed to enter or park at the Application Site;
- (c) No repairing, car washing, paint spraying and other workshop activities will be conducted at the Application Site;
- (d) No land filling works will be conducted at the Application Site; and
- (e) The Application Site will be fenced off by 2.5m high chain-link fence on along the site boundary.

5. Planning Justifications

5.1 In Line with Government Policy to Promote EV Usage

The proposed EV charging station would provide car parking spaces with quick EV charging facilities for electric taxis. It aligns with the Government's initiative to expand the charging network for EVs in Hong Kong, especially in meeting the target of installing 3,000 quick chargers by 2030. With the introduction of 3,000 electric taxis expected in mid-2026, the proposed development can also meet the increasing demand for EV charging spaces catered for electric taxis in Hong Kong. Additionally, the Applicant has received support from the Transport Department to collaborate with one of the selected taxi fleet operators to provide EV charging facilities in Hong Kong (see **Appendix 1**). Therefore, the proposed development is considered in line with Government policy to promote the wider use of EVs and support the operations of electric commercial vehicles and achieve the aforesaid target.

5.2 Meeting the Demand for Quick EV Charging Facilities

In view of the emerging popularity of commercial EVs with the Government's initiative, the demand for EV charging space for commercial EVs will increase as well. At present, the nearest EV charging facilities in the locality is located within Queens Hill Estate, which only provide standard chargers for private vehicles and are considered too slow for electric taxis. Since the proposed EV charging station will provide EV charging spaces with quick chargers, it can meet the demand of electric taxis for quick charging EV facilities.

5.3 Not Contravene the Planning Intention of "AGR" Zone

The Application Site falls within an area zoned "AGR" on the Lung Yeuk Tau and Kwan Tei South OZP No. S/NE-LYT/19. Having considered the Application Site has been occupied by the existing vehicle repair workshops for a long time, it has altered the natural landscape of the area with majority of the Application Site being paved. In addition, in view of the surrounding developments of the Application Site, which is mainly village houses, vehicle repair workshops, warehouse / storage yards and vacant land, the planning intention of the "AGR" zone has not been materialized at the current capacity. Due to the temporary nature of the Application, the proposed development will not jeopardize the long-term planning intention of the "AGR" zone.

5.4 Not Incompatible with Surrounding Land Uses

The proposed development is considered not incompatible with the surrounding land uses. The surrounding areas comprise of village houses, vehicle repair workshops, parking of vehicles, warehouses / storage yards, soy sauce factory and vacant land. Since the proposed development is intended to serve electric taxis, the proposed development could co-exist well with the existing surrounding land uses without inflicting any environmental impacts. As the proposed development is considered not incompatible with other developments/facilities in the adjacent areas in terms of nature and scale of use, approval of the application would therefore not result in any interface problems with the surrounding areas.

5.5 Optimization of Valuable Land Resources

The Application Site is considered a suitable and preferred venue for the proposed development. The Application Site is a piece of flat land with majority of the land being paved, in which the conversion into the proposed EV charging station will be smooth. The proposed development will also improve the currently under-utilized adjoining Government land, in which the Applicant will manage the site and therefore improve the existing condition. In addition, the Application Site is directly accessible to Sha Tau Kok Road – Lung Yeuk Tau, which is connected to strategic road networks to different parts of New Territories, electric taxis could easily access and leave the site

after charging. Meanwhile, the Application Site is situated close to densely populated areas such as Sheung Shui and Fanling and future new towns such as Kwu Tung and Ping Che. The provision of EV charging spaces, especially catered for electric taxis at the Application Site will be able to meet the growing demand for EV charging facilities in these areas. The proposed development is therefore considered fully commensurate with its local geographical settings and ideal to attain utmost land use maximization.

5.6 Will not Hinder Future Development of New Territories North New Town

The Application Site falls within the project boundary of New Territories North New Town. The Applicant understands that the proposed development will only be temporary and agrees to move out when the Application Site is resumed by the government for development of the New Town in the future. Due to the temporary nature of the Application, the proposed development will not hinder the future New Town development at the Application Site.

5.7 No Adverse Impacts on the Surrounding Areas

Owing to the nature and size of the proposed development, no adverse impacts are anticipated on the surrounding areas, as discussed below:

Traffic

The design of the proposed EV charging station has taken into account of vehicular circulation and manoeuvring. The Applicant will undertake traffic management measures to ensure pedestrian safety. Waiting spaces have been reserved within the Application Site to prevent any electric taxis from queuing outside the Application Site. Based on the submitted TIA (see **Appendix 2**), it is concluded that the proposed development will not induce significant traffic impact on the surrounding areas and is considered acceptable from traffic engineering point of view.

Drainage

Drainage facilities will be provided within the Application Site for this application, in which surface runoff will be effectively collected from and discharged out of the Application Site. The Applicant has accordingly submitted a drainage proposal (see **Appendix 3**) to elaborate. Therefore, no adverse drainage impact on the surrounding areas is anticipated.

Environment

As the proposed EV charging station will only be used for parking and EV charging facilities for electric taxis, it would not generate adverse noise, air pollution or visual intrusion. In addition, there will be no repairing, car washing, paint spraying and other workshop activities carried out on the premises. Therefore, the proposed development will not generate any pollution or adverse environmental impacts on the surrounding areas.

6. Conclusion

The Application Site falls within “AGR” zone on the Lung Yeuk Tau and Kwan Tei South OZP No. S/NE-LYT/19. The Application Site is currently used as vehicle repair workshops and the Applicant intends to convert the Application Site into an EV charging station for electric taxis. In view of the site location, the existing site conditions and surrounding land uses, the proposed development is considered not incompatible with surrounding land uses with the planning intention of “AGR” zone not being materialized. Given the temporary nature of the proposed development, it will therefore not contravene the long-term planning intention of the “AGR” zone.

In contrast, the Application Site is considered a suitable venue for the proposed development with its geographical settings and ideal to attain utmost land use maximization. Additionally, the proposed development will meet local demand for EV charging facilities, especially with provision of quick chargers for electric taxis. It therefore aligns with Government policy to expand EV charging networks and promote wider EVs usage in Hong Kong.

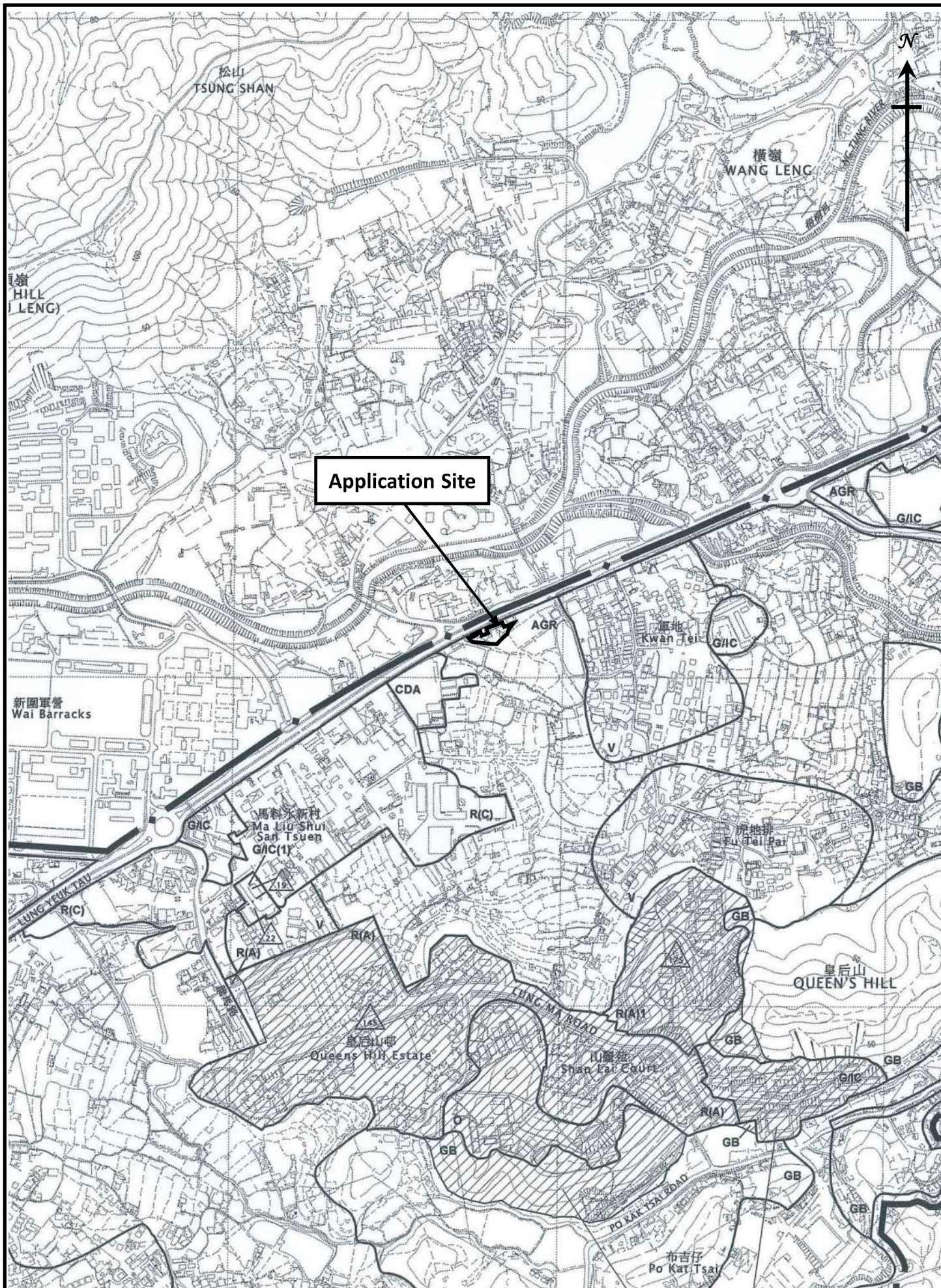
Meanwhile, the temporary nature of the proposed development will also not hinder future development of New Territories North New Town. It is anticipated that the proposed development will not generate adverse impacts to the surrounding environment, and the technical concerns of relevant government departments could be addressed through the implementation of approval conditions.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

Lawson David & Sung Surveyors Limited
August 2025

Figures

- Figure 1 Location Plan
- Figure 2 Lot Index Plan
- Figure 3 Vehicular Access to the Site
- Figure 4 Indicative Layout Plan
- Figure 5 Photos of Electric Charging Device



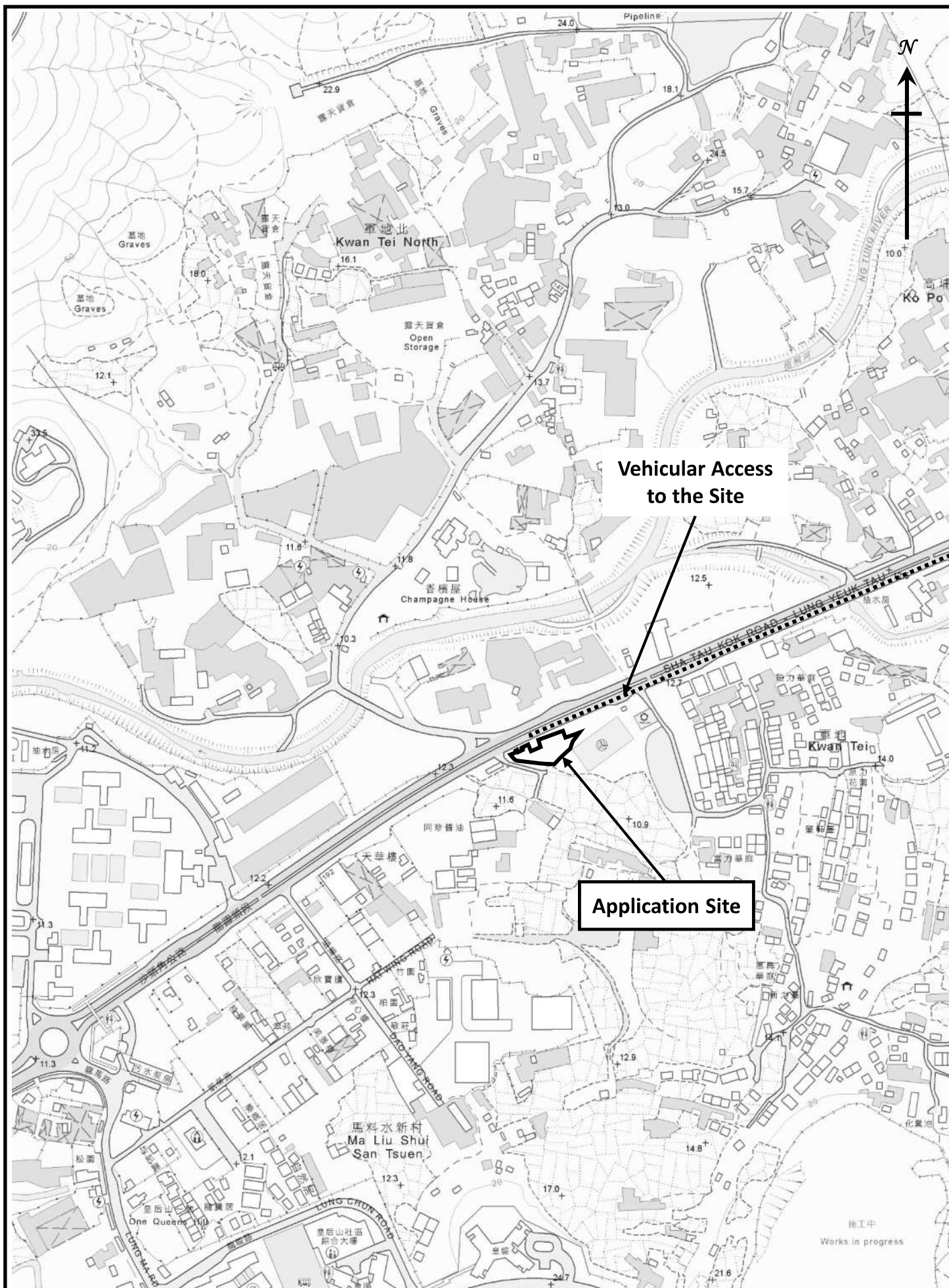
Location Plan
(Extract from Approved Lung Yeuk Tau and Kwan Tei
South Outline Zoning Plan No. S/NE-LYT/19)

Figure 1

1 : 7500



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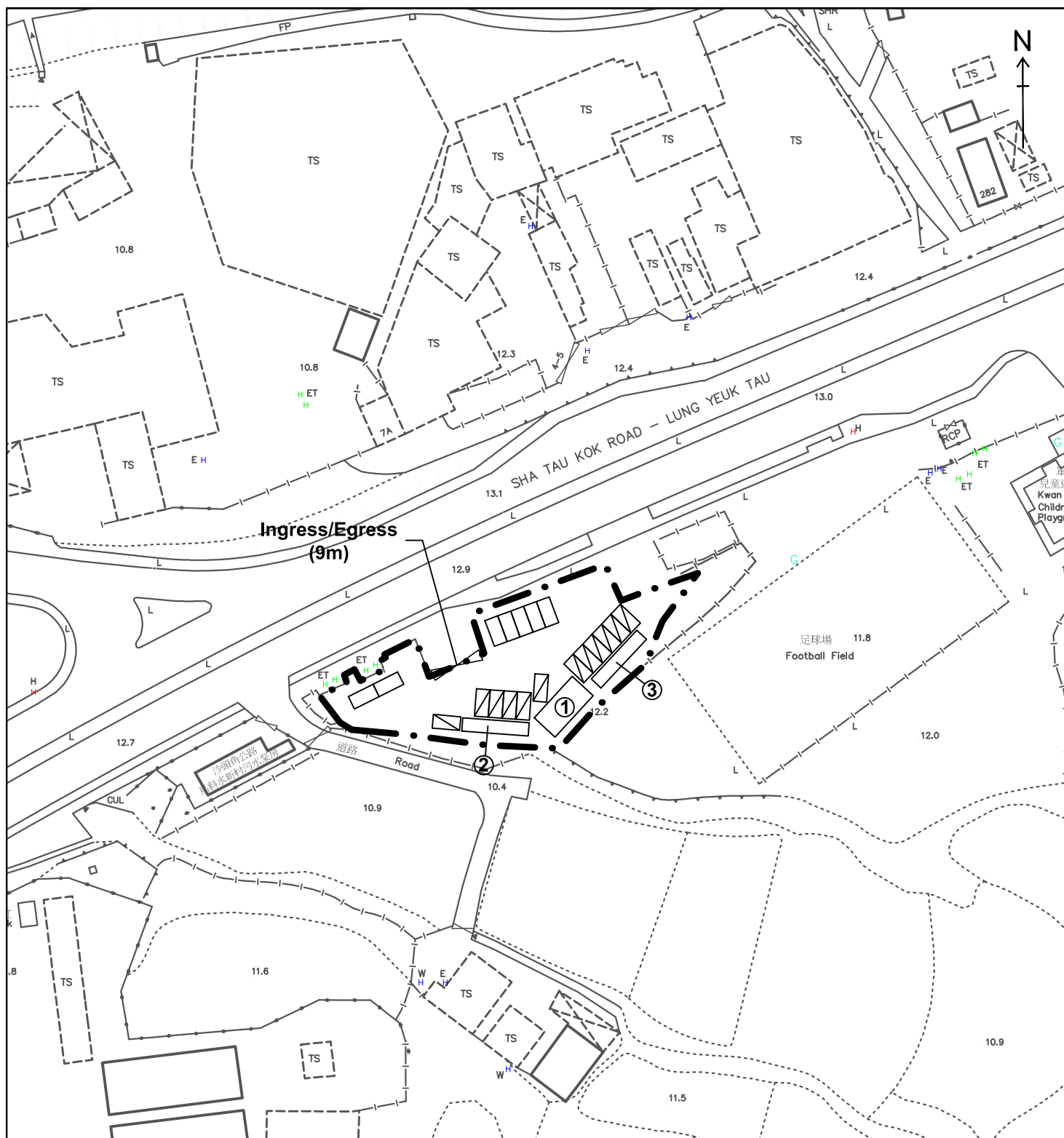
Vehicular Access to the Application Site

Figure 3

1 : 5000



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Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- • — Application Site (Area: about 1,143m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (7 nos.)

Layout Plan

Figure 4

Scale 1:1000



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Container for EV Chargers



EV Charging Device for Electric Taxis

For Reference Only

Photos of Electric Charging Device

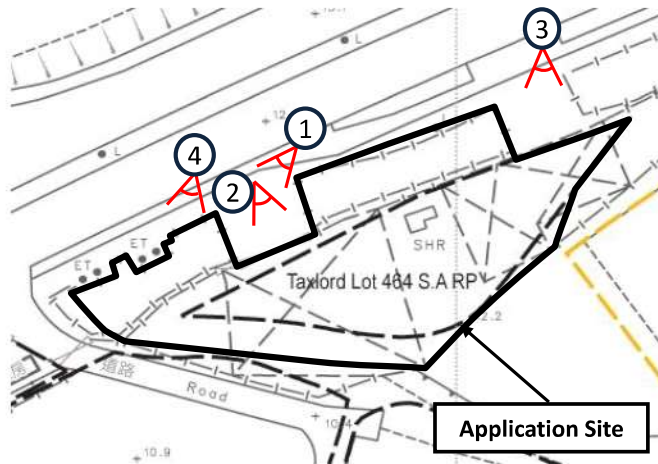
Figure 5

Not to Scale





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Site Photos



Legend:

-  Viewpoint of the Photo
-  Photo No.



Existing Condition of the Application Site

Site Photos

Appendix 1

Endorsement Letter from the Transport Department



運輸署

Transport Department

本署檔號： 064-101-000-FP-006-005
電 話： 3842 6416
圖文傳真： 2824 2176

新世代國際有限公司

執事先生：

有關新科發展（國際）有限公司的電動車充電設施

香港政府早前推出一系列措施以提升個人化點對點交通服務的整體質素，當中包括引入的士車隊制度。政府希望藉着引入的士車隊制度，讓提供優質服務的的士車隊在行業中起示範和帶頭作用，透過市場演化逐漸改變行業生態，同時促進不同車隊，以及車隊和非車隊的士之間的良性競爭，鼓勵業界精益求精，提升服務水平和行業形象。

當中，新科發展（國際）有限公司（下稱「新科」）為其中一個獲選的的士車隊，並會分階段以1,000輛吉利幸福號電動車來營運。由於其車隊具一定規模，在投入服務後將對充電的配套有龐大的需求。本署支持新科與 貴公司合作，在全港不同位置發展電動車充電設施和配套，好讓其的士車隊能盡快投入服務。

如對上述事宜有任何查詢，請致電3842 6416與我聯絡。

運輸署署長

（李朝傑



代行)

2025 年 4 月 22 日

Appendix 2

Traffic Impact Assessment

Document Status Control Record

**Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi)
for a Period of 3 Years
at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.**

Traffic Impact Assessment Report

Originating Organisation: LLA Consultancy Limited Unit 610, 6/F Island Place Tower 510 King's Road North Point, Hong Kong	Prepared by: SKL	SKL	Date: 23 June 2025
	Approved by: SLN	SLN	Date: 23 June 2025
	Revision No.: -		Date of Issue: 23 June 2025

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1 INTRODUCTION

1.1 Background

- 1.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi) for a period of 3 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T..
- 1.1.2 The Site is zoned as “Agriculture” use under the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14. A planning application is required for the proposed temporary use.
- 1.1.3 LLA Consultancy Limited was commissioned to undertake the TIA to support the planning application. This TIA report presents the findings of the study.

1.2 Objectives

- 1.2.1 The objectives of the study are as follows:
- to review the existing traffic conditions in the vicinity of the Site;
 - to estimate the traffic generation and attraction of the proposed temporary use;
 - to project the future traffic situations in the surrounding road network;
 - to appraise the potential traffic impact of the proposed temporary use; and
 - to recommend the internal transport facilities for the proposed temporary use.

2 THE PROPOSED DEVELOPMENT

2.1 The Site

- 2.1.1 As shown in **Figure 2.1**, the Site is located at Sha Tau Kok Road – Lung Yeuk Tau area. It has a total site area of about 1,143 m².

2.2 The Proposed Temporary Use and Internal Transport Facilities Provisions

- 2.2.1 The Site is used to provide charging facilities for electric taxis. The key development parameters are listed out in **Table 2.1**.

Table 2.1 Key Development Parameters

Use	Quantity
Site Area	1,143 m ²
EV Charging Facilities for Electric Taxi only (2.5m x 5.0m)	12 nos.
Parking Spaces / Waiting Spaces for Electric Taxi only (2.5m x 5.0m)	7 spaces

2.3 Swept Path Analysis

- 2.3.1 The Site is directly abutted to Sha Tau Kok Road – Lung Yeuk Tau, the development traffic is anticipated to enter and leave the Site with a left-in/left out movement onto Sha Tau Kok Road south-westbound. In order to ensure the safety of pedestrians walking along the public footpath, a standard run-in/out in accordance with Highways Department's standards will be formed and flashing warning lights will be installed at both sides of the run-in/out as shown in **Figure 2.2**.
- 2.3.2 To ensure smooth manoeuvring of the parking area, swept path analysis was conducted to demonstrate that adequate space is provided for the vehicles for manoeuvring as shown in **Figures SP-01**.

3 EXISTING TRAFFIC SITUATION

3.1 Existing Road Network

- 3.1.1 The Site has a frontage along Sha Tau Kok Road – Lung Yeuk Tau and an existing run-in/out is provided for the current use of the Site.
- 3.1.2 Sha Tau Kok Road – Lung Yeuk Tau is a dual two lanes distributor road connecting Fanling and Sha Tau Kok. In 2023, it recorded an AADT of 22,810 vehicles.

3.2 Traffic Count Surveys

- 3.2.1 In order to appraise the existing traffic conditions, a traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30, the peak traffic generation period of the vehicle charging station. More details will be discussed in **Section 4.2**.
- 3.2.2 The locations of the surveyed junctions are shown in **Figure 3.1**.
- J1 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road
- J2 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road
- 3.2.3 The peak hour identified are 16:30 – 17:30. The observed 2025 traffic flows are presented in **Figure 3.2**.

3.3 Existing Junction Capacity Assessment

- 3.3.1 Based on the observed traffic flows, the performance of the junctions is assessed. It is calculated that all junctions are operating satisfactorily. The results are presented in **Table 3.1** and the detailed calculation sheets are attached in **Appendix A**.

Table 3.1 Existing Junction Capacity Assessment

Ref.	Junction Location	Type/Index ⁽¹⁾	Peak Hour of the Vehicle Charing Station
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.45
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.43

Note: (1) DFC = Design Flow to Capacity

3.4 Existing Link Capacity Assessment

- 3.4.1 The Volume to Capacity (V/C) Ratios of Sha Tau Kok Road – Lung Yeuk Tau is assessed and the results are presented in **Table 3.2**.

Table 3.2 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	Peak Hour Traffic Volume (pcu/hr)	Peak Hour V/C Ratio
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	980	0.31
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	934	0.30

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

- 3.4.2 As shown in **Table 3.2**, the concerned road sections are operating with spare capacity during the peak hour.

4 FUTURE TRAFFIC SITUATION

4.1 Design Year

- 4.1.1 The proposed temporary use, if approved by the Town planning Board, will be valid for 3 years until 2028. Hence, Year 2028 is adopted as the design assessment year.

4.2 Traffic Generation of the Proposed Temporary Use

Traffic Generation for Electric Vehicle Charging Station

- 4.2.1 For the charging facilities, all EV chargers are direct current (DC) quick chargers with output power of 120kW which can provide 600km of mileage when an electric taxi charged for about 30 minutes.
- 4.2.2 From the taxi trade, the drivers are usually working on two shifts, i.e., the day shift and the night shift. In general, the day shift is 05:00 to 17:00 while the night shift is 17:00 to 05:00. The peak hour usage of charging facilities is expected to be the time periods before the shift such that a fully charged taxi can be passed to the driver in the next shift. As a result, the peak traffic generation of the proposed charging station will be either between 15:00 to 17:00 or after midnight. It is anticipated that minimal drivers will charge their taxis during the normal highway peak hours because these time periods will be the busiest hour in their business.
- 4.2.3 According to the Hong Kong Planning Standard and Guidelines, Section 3.9.4, a minimum of two waiting spaces should be provided in each EV charging station to avoid the awaiting vehicles to queue on public roads. In view of the size of the Site, 7 waiting spaces can be provided. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.
- 4.2.4 The estimated traffic generation and attraction for Electric Vehicle Charging Station during the peak operation hour will be a two-way traffic of 24 taxis (24 Nos. of vehicles = 12 EV charging stations x 2 because of 30 minutes charging/servicing time for each taxi).
- 4.2.5 Based on the above, the traffic generation of the proposed temporary use has been estimated and presented in **Table 4.1**.

Table 4.1 Traffic Generation of Proposed Temporary Use

Proposed Use	Unit /Content	Operation Peak Hour		
		Generation	Attraction	Total
Estimated Traffic Generation/Attraction				
EV Charging Station	12 charging station	24	24	48

- 4.2.6 As shown in **Table 4.1**, the Site will generate a two-way traffic flows of 48 pcu/hour in the peak hour. The distribution of the development traffic of proposed temporary use is shown in **Figure 4.1**.

4.3 Traffic Growth

Annual Traffic Census (ATC) Data

- 4.3.1 Reference was made to the 2019 to 2023 Annual Traffic Census Reports, published by the Transport Department, to determine the natural traffic growth. The traffic data recorded at counting station in the vicinity of the Site is shown in **Table 4.2**.

Table 4.2 Annual Traffic Census Data

Stn. No.	Road Section			AADT (vehicle/day) ⁽¹⁾					Average Annual Growth (%)
	Road	From	To	2019	2020	2021	2022	2023	
5660	Sha Tau Kok Rd	On Kui St	Ping Che Rd	33,630	23,740 (-29.4%)	22,980 (-3.2%)	22,280 (-3%)	22,810 (2.4%)	-1.3% (Between 2020 and 2023)

Note: (1) Figures in bracket indicated the % increase between two successive years.

- 4.3.2 From **Table 4.2**, the recorded traffic flows in 2020 dropped significantly by almost 30% and was due to the opening of Lung Shan Tunnel in 2019. From 2020 to 2023, the recorded traffic flows have been quite steady with only -1.3% decrease annually.

Territorial Population and Employment Data Matrix (TPEDM)

- 4.3.3 Reference was also made to the 2021-based TPEDM published by Planning Department. The population and employment data of year 2026 and 2031 in Northeast New Territories are summarized in **Table 4.3**.

Table 4.3 Population and Employment Data in Northeast New Territories

Year	Population	Employment	Total
2026	1,361,150	421,350	1,782,500
2031	1,416,800	448,600	1,865,400
Average Annual Growth Rate			+0.9%

- 4.3.4 As shown in **Table 4.3**, the projected average annual growth rate of the population and employment total number under the TPEDM in Northeast New Territories is +0.9% between the years 2026 to 2031, which is larger than the annual growth rate derived from ATC. To be conservative, the larger growth rate of +0.9% is adopted for the subsequent assessments.

4.4 Reference and Design Flows

- 4.4.1 The 2028 Reference Flows, i.e. the future traffic flows in the local road network WITHOUT the development traffic, were estimated based on the following equation:

$$2028 \text{ Reference Flows} = 2025 \text{ Existing Flows} \times (1 + 0.9\%)^3$$

- 4.4.2 The 2028 Design Flows, i.e. the future traffic flows in the local road network WITH the development traffic, are estimated based on the following equations.

$$2028 \text{ Design Flows} = 2028 \text{ Reference Flows} + \text{Traffic generated and attracted by the proposed temporary use}$$

- 4.4.3 The forecast traffic flows for the two scenarios are shown in **Figures 4.2 and 4.3** respectively.

4.5 Junction Capacity Assessment

4.5.1 Junction capacity analysis is carried out for the assessment year 2028. The assessment results are shown in **Table 4.4** and the detailed calculation sheets are attached in **Appendix B**.

Table 4.4 2028 Junction Capacity Assessment

Ref.	Junction Location	Type/ Index ⁽¹⁾	2028 Reference Peak Hour	2028 Design Peak Hour
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.46	0.47
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.44	0.46

Note: (1) DFC = Design Flow to Capacity.

4.5.2 The results in **Table 4.4** show that the assessed junctions will operate satisfactorily in both reference and design scenarios. Therefore, it is anticipated that the proposed temporary use will not induce significant traffic impact to the surrounding road network.

4.6 Link Capacity Assessment

4.6.1 The V/C Ratios of Sha Tau Kok Road were assessed and the results are presented in **Table 4.5**.

Table 4.5 2028 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	2028 Traffic Volume		2028 V/C Ratio	
		Reference Scenario	Design Scenario	Reference Scenario	Design Scenario
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	1,007	0.32	1,012	0.32
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	959	0.31	983	0.32

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

4.6.2 As shown in **Table 4.5**, all the concerned road sections will operate with capacity during peak hour in all scenarios. Hence, it can be concluded that the traffic induced by the proposed temporary use will not induce significant traffic impact to the road network.

5 SUMMARY AND CONCLUSION

5.1 Summary

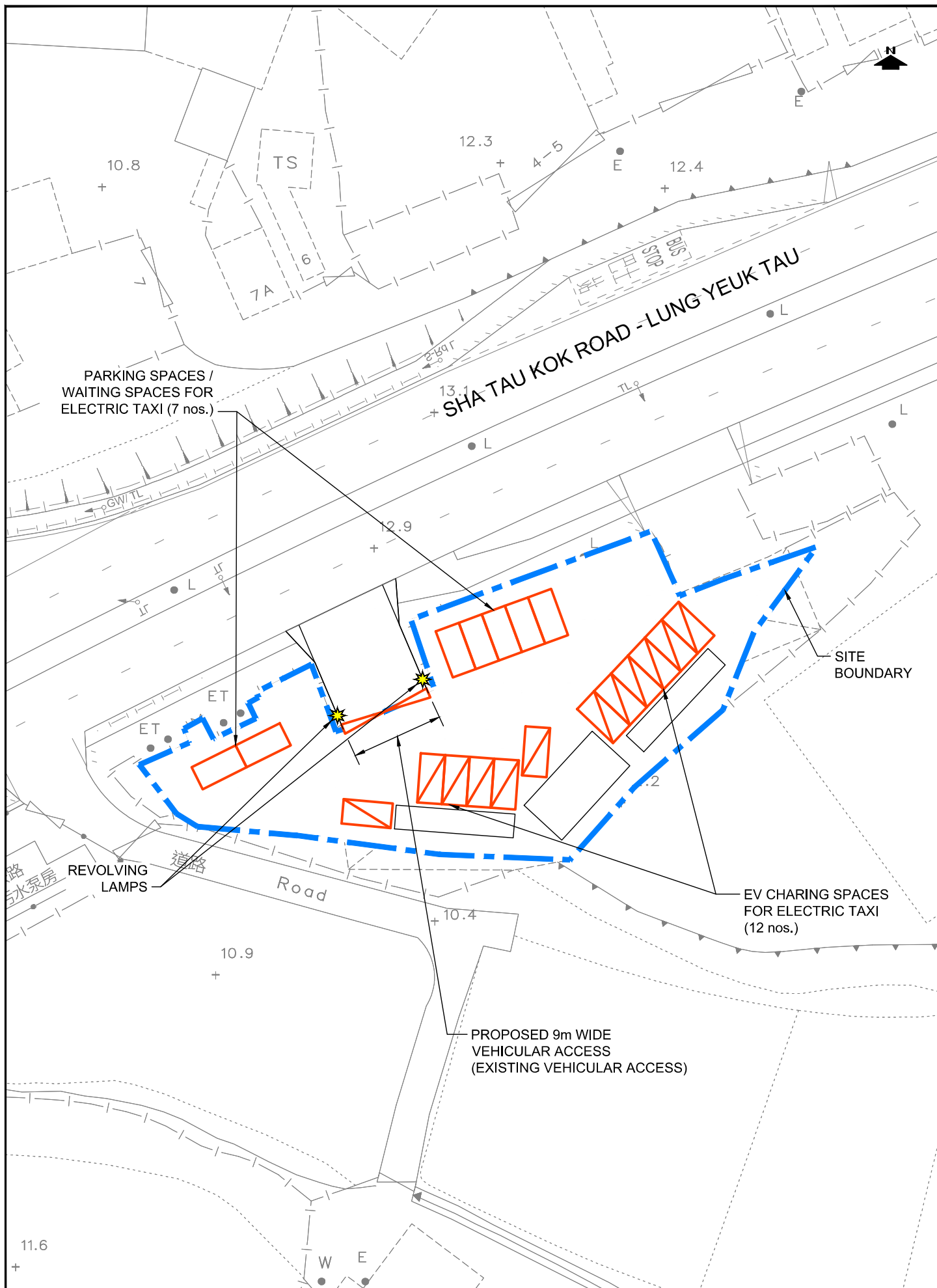
- 5.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi) for a period of 3 Years. The piece of land (hereinafter referred to “the Site”) is located at Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T. The main function of the Site is to provide charging facilities for electric taxis.
- 5.1.2 Traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30 which the time period will be the peak traffic generation period of the proposed development. Based on the existing traffic flows, the junction and link capacity assessments show that they are operating satisfactorily.
- 5.1.3 The proposed development is estimated to generate a two-way traffic of 48 pcu/hour in the operation peak hour. Junction and link capacity assessment was carried out for the reference and design scenarios in 2028 and the results revealed that the concerned junctions and road links would perform satisfactorily with spare capacity in 2028. Therefore, it is anticipated that the proposed temporary use will not induce significant traffic impact to the surrounding road network.
- 5.1.4 For the 12 nos. of EV charging stations, 7 nos. of waiting spaces can be provided (minimum two spaces in each EV charging station) to avoid the awaiting vehicles to queue on public roads. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.

5.2 Conclusion

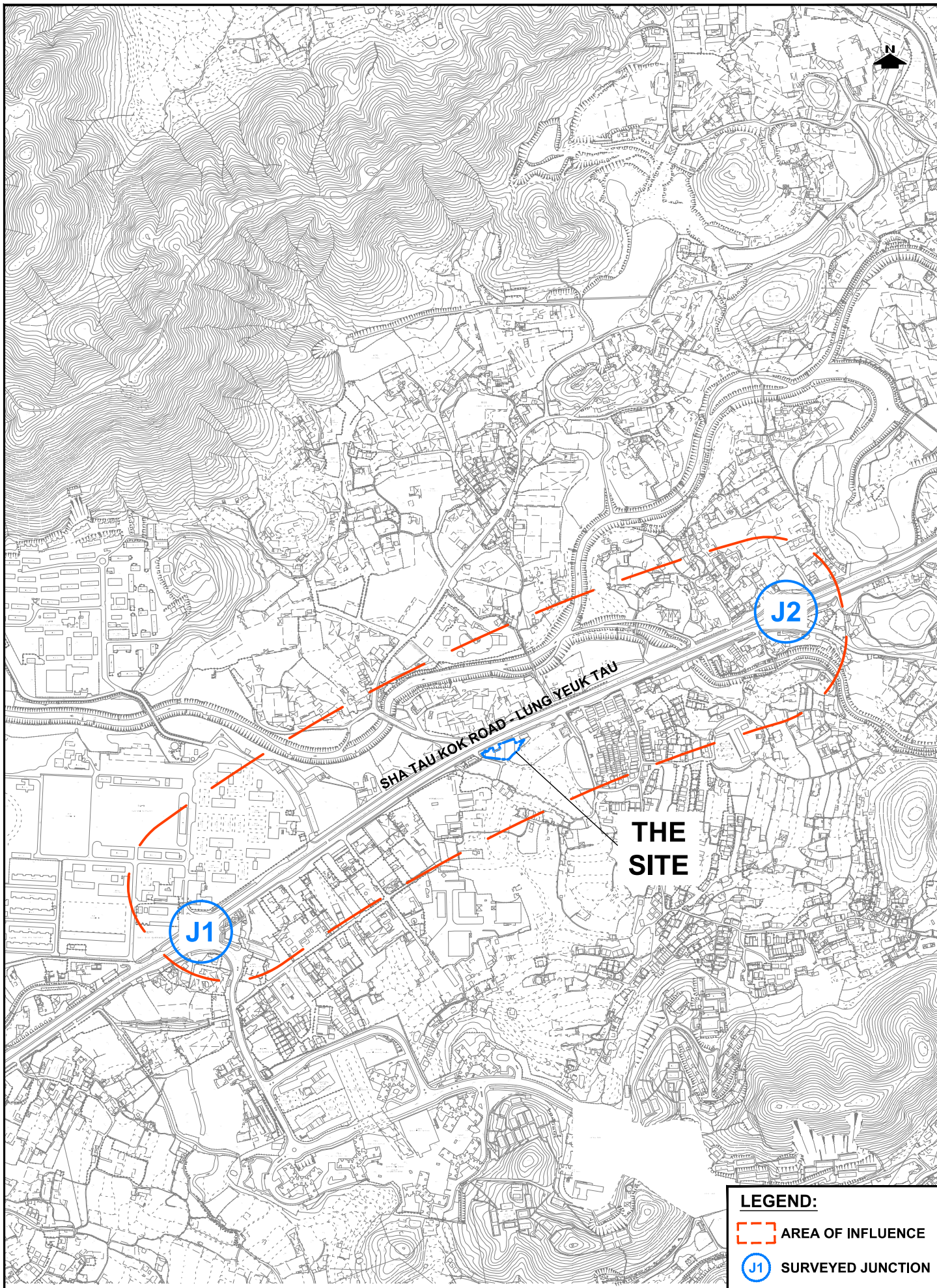
- 5.2.1 This planning application is to allow a temporary vehicle charging station for a period of 3 years. Based on the assessment result, it can be concluded that the proposed use will not induce additional adverse traffic impact on the surrounding road network and it is considered acceptable from traffic engineering point of view.



PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 2.1	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE LOCATION PLAN		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:5000				
CHECKED SLN					




PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOT 464 S.A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 2.2	REV. .
DESIGNED SLN	DATE JUL 2025	DRAWING TITLE PROPOSED LAYOUT PLAN		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:500				
CHECKED SLN					

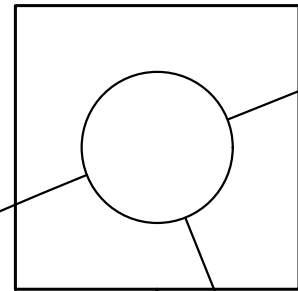
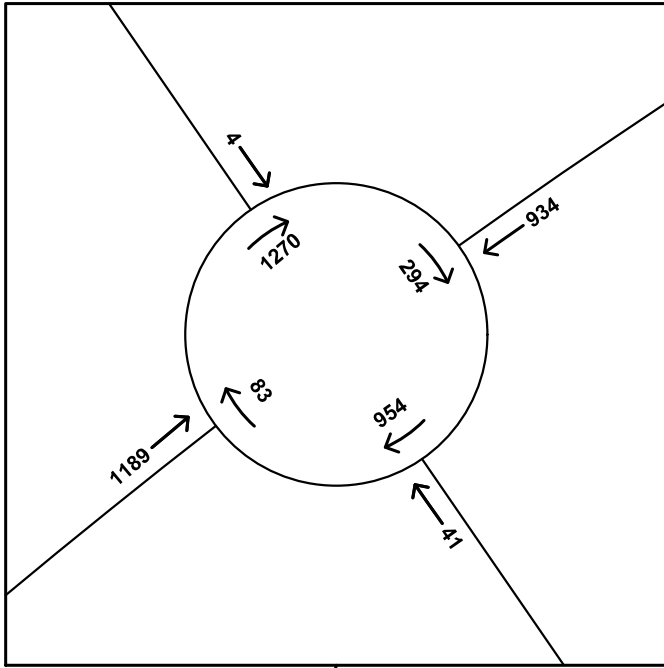


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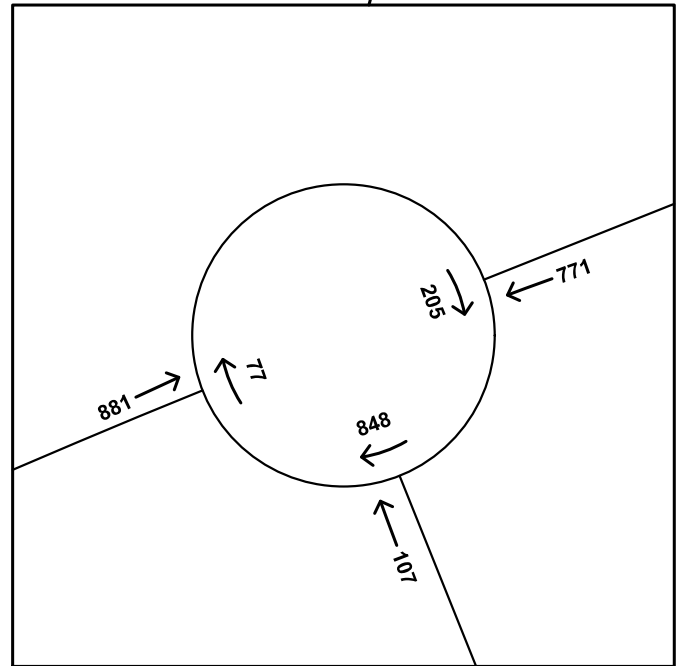
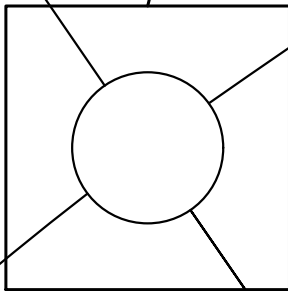
 **AREA OF INFLUENCE**

 **SURVEYED JUNCTION**

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 3.1	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE AREA OF INFLUENCE		 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:5000				
CHECKED SLN					



SHA TAU KOK ROAD - LUNG YEUK TAU



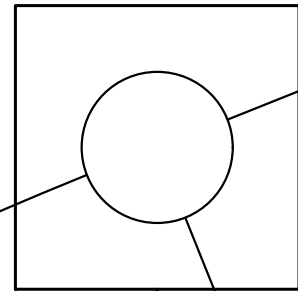
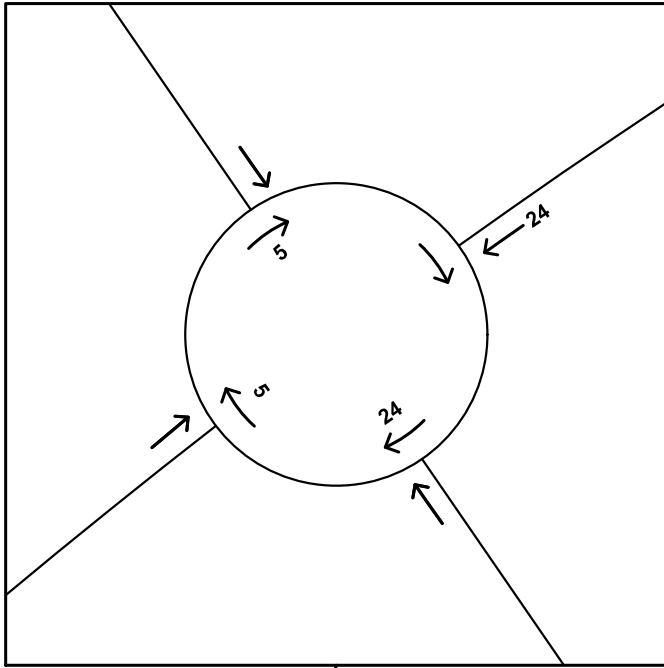
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123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

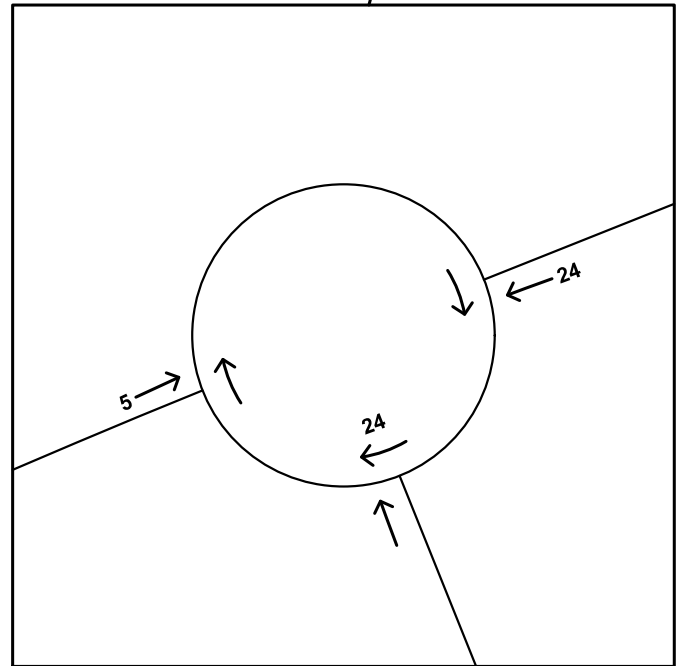
NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOR LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 3.2	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2025 OBSERVED TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



SHA TAU KOK ROAD - LUNG YEUK TAU



LEGEND:

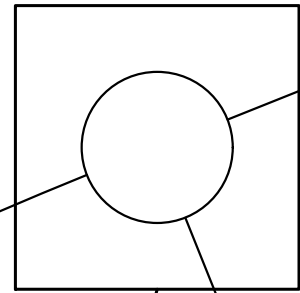
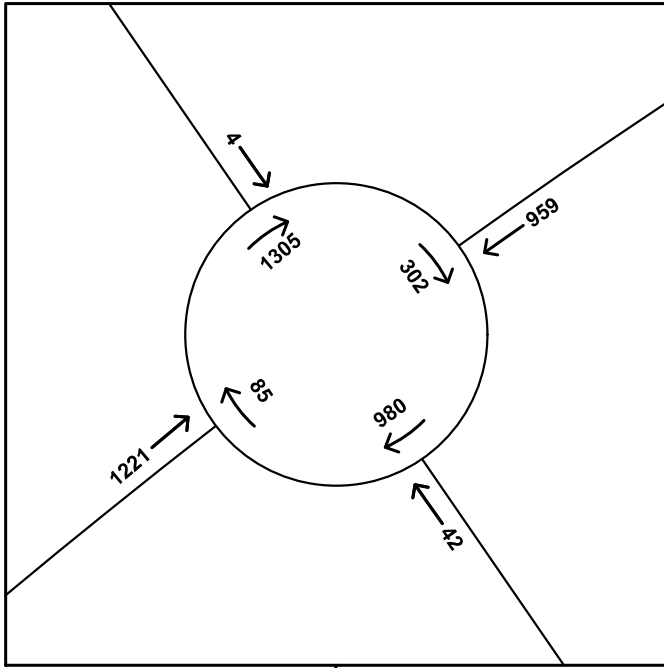
123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

- 1, ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

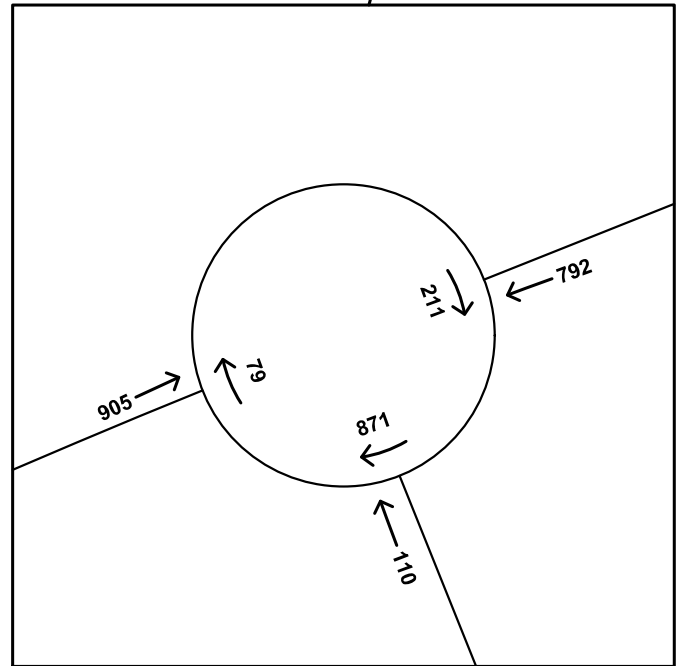
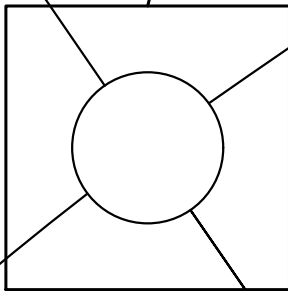
PROJECT NO. 40989	PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOR LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.	DRAWING NO. FIGURE 4.1	REV. .
DESIGNED SLN	DATE JUN 2025	DEVELOPMENT TRAFFIC FLOWS	
DRAWN CLL	SCALE N.T.S.		
CHECKED SLN			

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Consultancy Limited



SHA TAU KOK ROAD - LUNG YEUK TAU

THE SITE



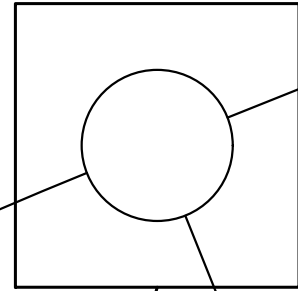
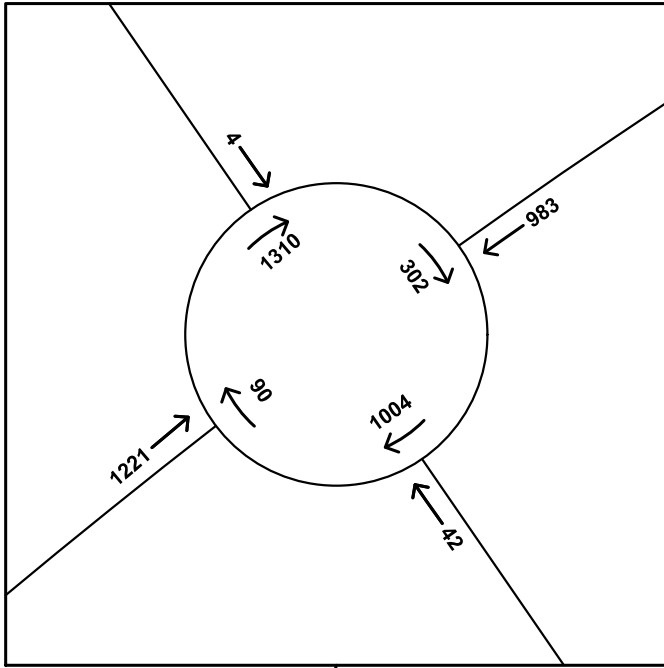
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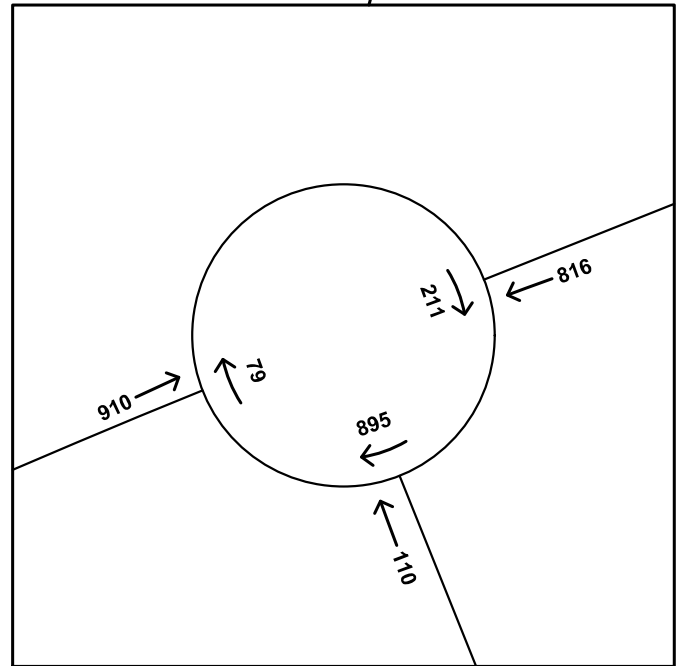
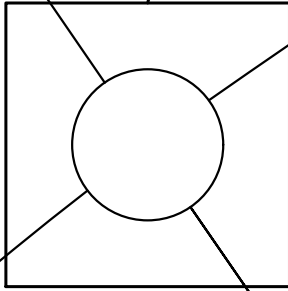
NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.2	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2028 REFERENCE TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



SHA TAU KOK ROAD - LUNG YEUK TAU



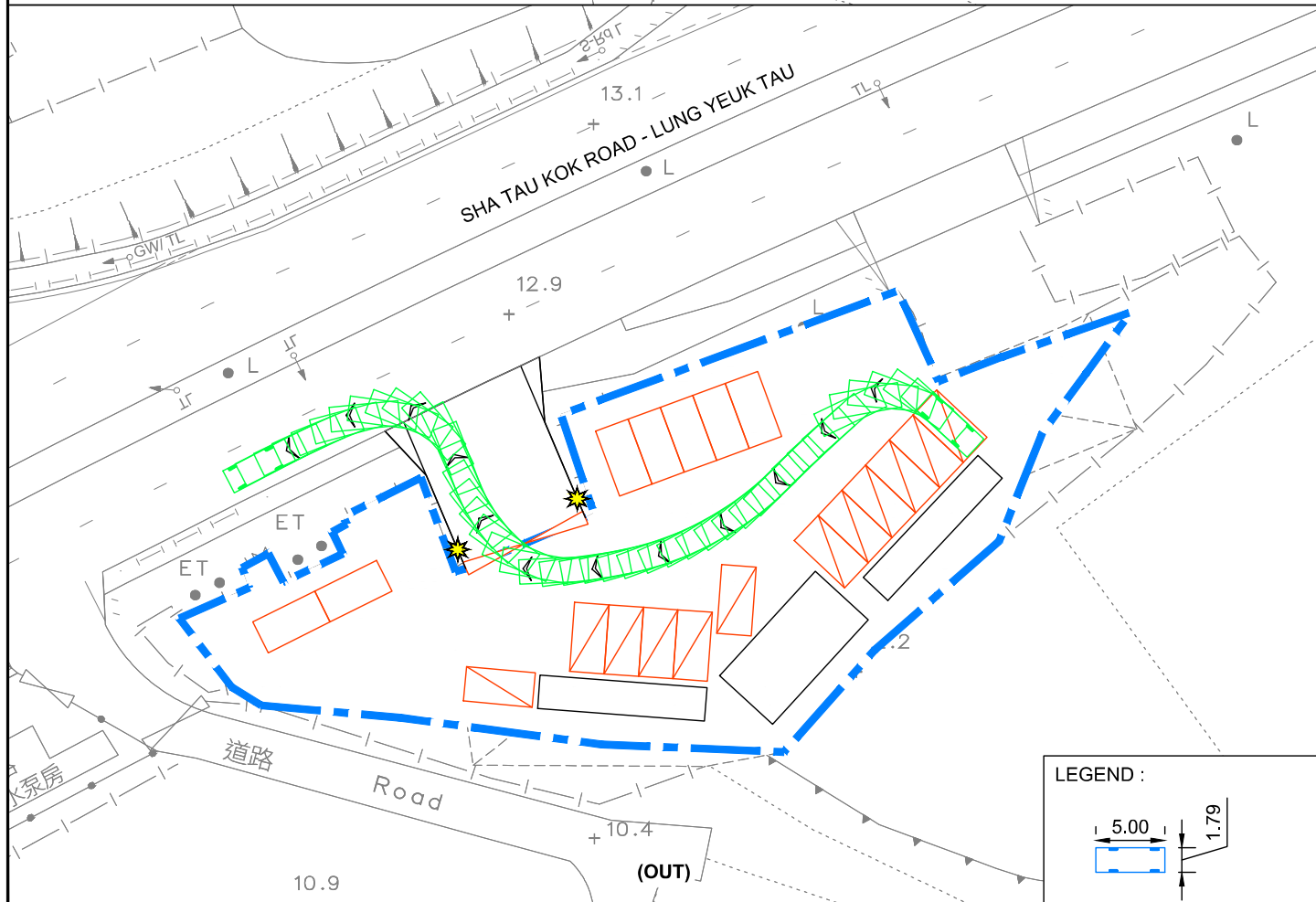
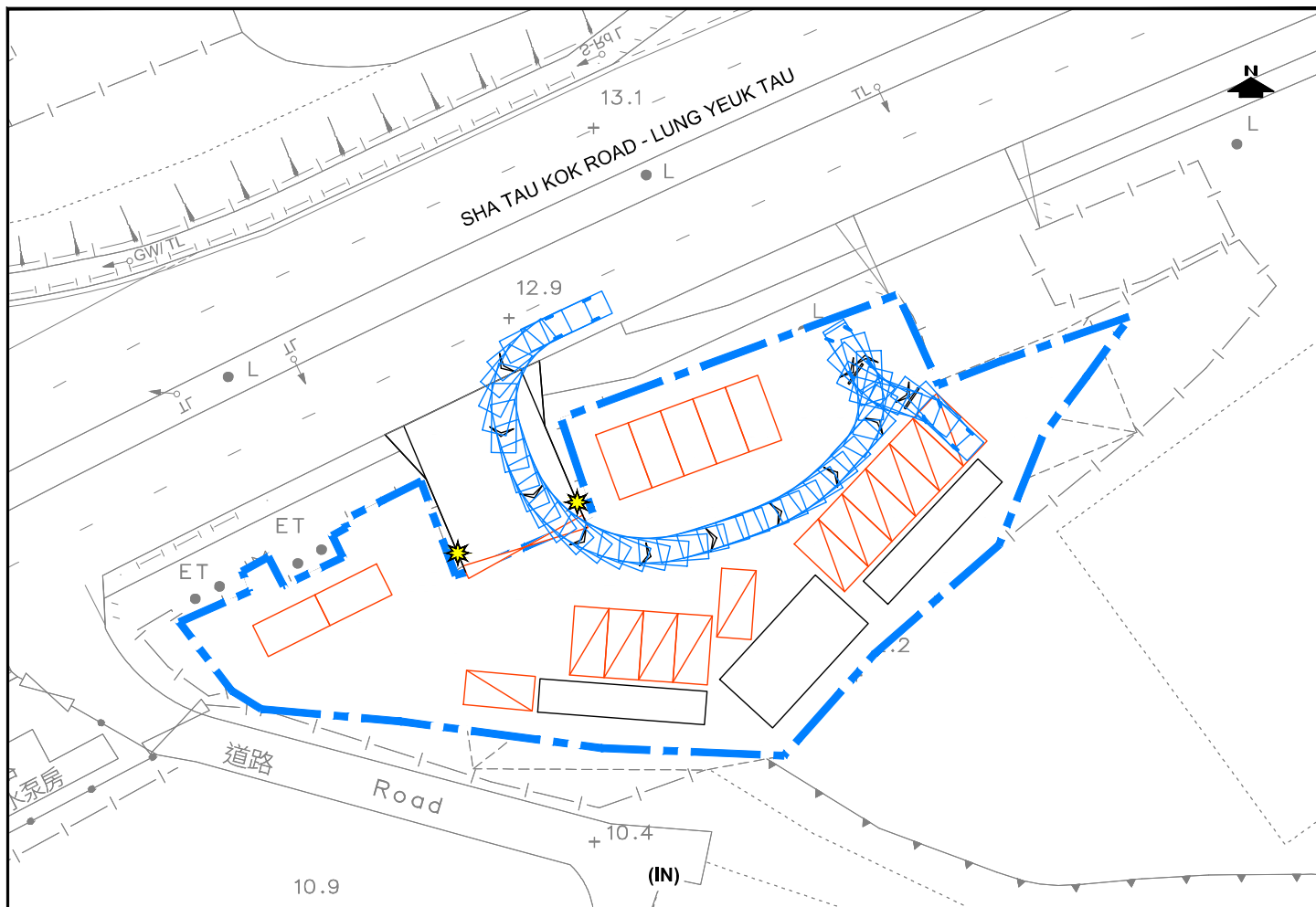
LEGEND:

123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXI LOR LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.3	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2028 DESIGN TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLOD LOT 464 S.A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD—LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. SP-01	REV. .
DESIGNED SLN	DATE JUL 2025	SWEPT PATH ANALYSIS - PC			LLA 顧問有限公司 Consultancy Limited
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Appendix A
Junction Capacity Assessments
- Existing Scenario

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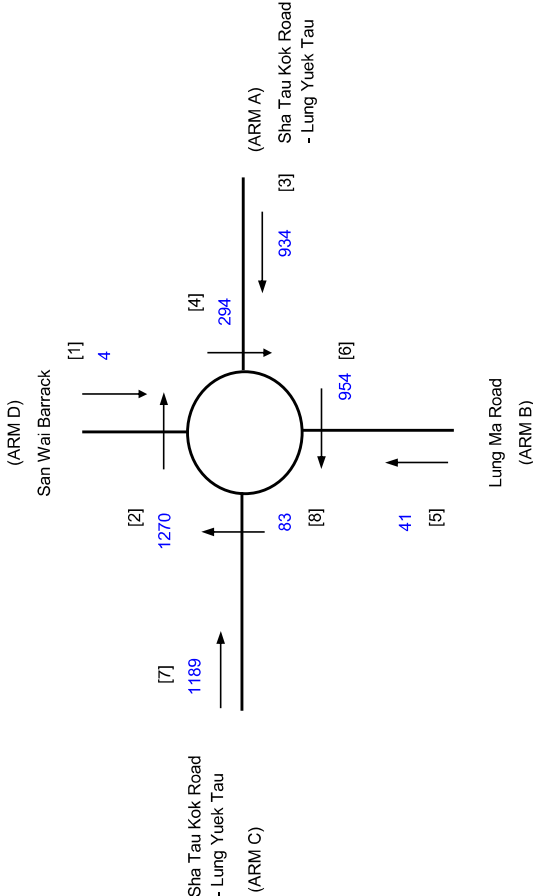
ROUNDAABOUT CALCULATION

INITIALS				DATE
SKL				Jun-25
SLN				Jun-25
SLN				Jun-25

PROJECT NO.: 40989		PREPARED BY:
FILENAME : J1_STKR_LMR.xl		CHECKED BY:
REFERENCE NO.:		REVIEWED BY:

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	934	41	1189	4
Qc = Circulating flow across entry (pcu/h)	294	954	83	1270

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.32	0.23	0.20	0.09
K = 1-0.00347/(A-30)-0.978(1/R-0.05)	1.06	1.10	1.01	1.07
X2 = V + ((E-V)/(1+2S))	7.22	4.67	8.81	4.62
M = EXP((D-60)/10)	0.50	0.50	0.50	0.50
F = 303*X2	2188	1415	2669	1401
Td = 1+(0.5/(1+M))	1.33	1.33	1.33	1.33
Fc = 0.21*Td(1+0.2*X2)	0.68	0.54	0.77	0.54
Qe = K(F-Fc*Qc)	2113	992	2632	765

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.45

Total In Sum = 2168 PCU

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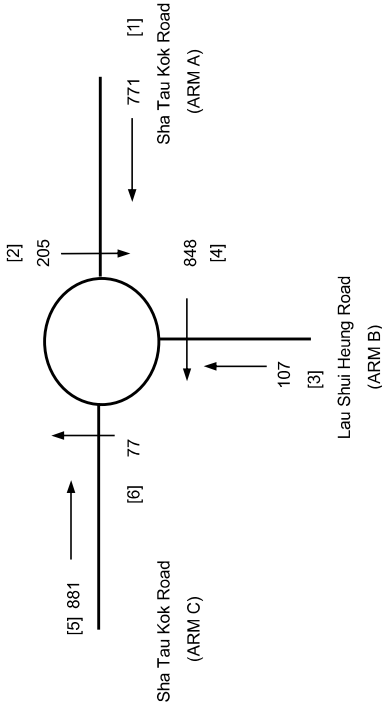
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Tuxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDABOUT CALCULATION

PROJECT NO.:	40989	PREPARED BY:		INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:		SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:		SLN	Jun-25
				SLN	Jun-25

2025 Existing 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	771	107	881
Qc = Circulating flow across entry (pcu/h)	205	848	77

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.16	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.92	0.93	1.09
X2 = $V + ((E-V)/(1+2S))$	6.88	4.61	6.48
M = $EXP((D-60)/10)$	0.45	0.45	0.45
F = $303 \times X2$	2083	1398	1962
Td = $1+(0.5/(1+M))$	1.34	1.34	1.34
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.67	0.54	0.65
Qe = $K(F-Fc \times Qc)$	1790	870	2093

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.43

Total In Sum = 1759 PCU

Appendix B

Junction Capacity Assessments

- Reference & Design Scenarios

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Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDAABOUT CALCULATION

2028 Reference 1630-1730

PROJECT NO.: 40989

FILENAME : J1_STKR_LMR.xl

REFERENCE NO.:

PREPARED BY:

CHECKED BY:

REVIEWED BY:

INITIALS

SKL

SLN

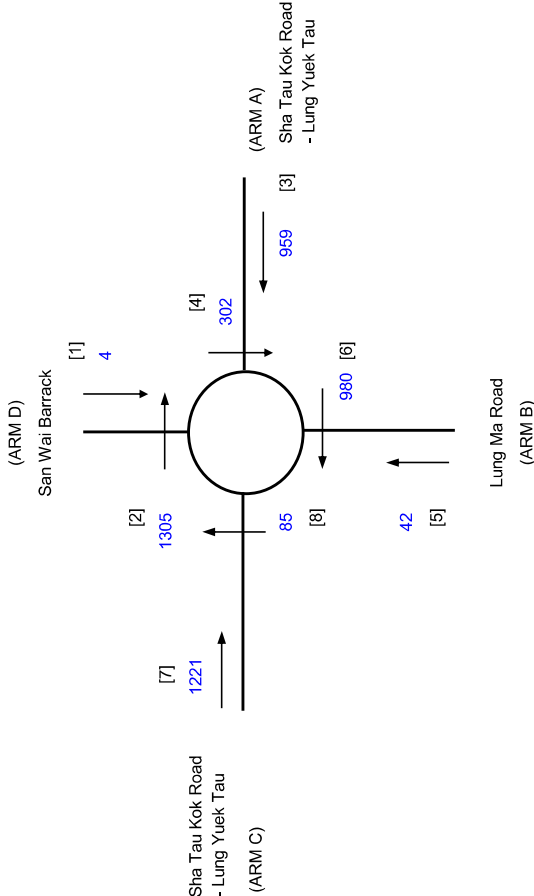
SLN

DATE

Jun-25

Jun-25

Jun-25



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	959	42	1221	4
Qc = Circulating flow across entry (pcu/h)	302	980	85	1305

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2 = $V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M = $EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F = $303 \times X2$	2188	1415	2669	1401
Td = $1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe = $K(F-Fc \times Qc)$	2107	976	2631	744

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.46

Total In Sum =

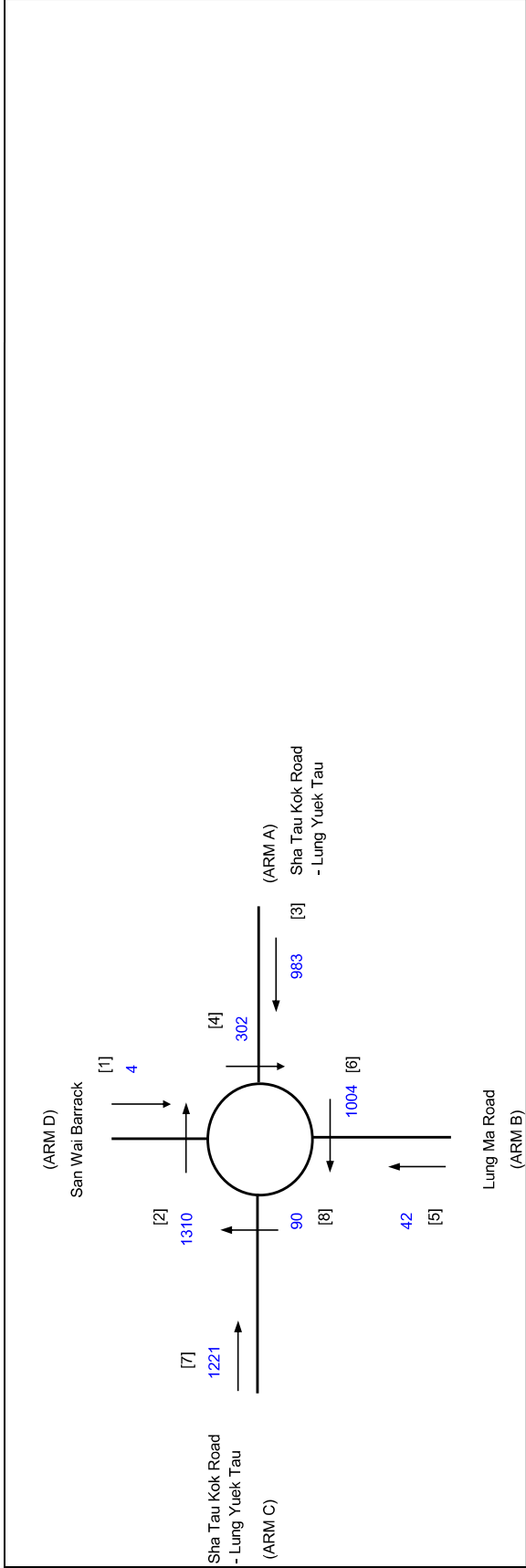
2226 PCU

INITIALS				DATE
SKL				Jun-25
SLN				Jun-25
SLN				Jun-25

PROJECT NO.: 40989		PREPARED BY:
FILENAME : J1_STKR_LMR.xl		CHECKED BY:
REFERENCE NO.:		REVIEWED BY:

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road



ARM	A	B	C	D
INPUT PARAMETERS:				
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	983	42	1221	4
Qc = Circulating flow across entry (pcu/h)	302	1004	90	1310
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.32	0.23	0.20	0.09
K = 1-0.00347/(A-30)-0.978(1/R-0.05)	1.06	1.10	1.01	1.07
X2 = V + ((E-V)/(1+2S))	7.22	4.67	8.81	4.62
M = EXP((D-60)/10)	0.50	0.50	0.50	0.50
F = 303*X2	2188	1415	2669	1401
Td = 1+(0.5/(1+M))	1.33	1.33	1.33	1.33
Fc = 0.21*Td(1+0.2*X2)	0.68	0.54	0.77	0.54
Qe = K(F-Fc*Qc)	2107	962	2627	742
Total In Sum =				2250 PCU
DFC = Design flow/Capacity = Q/Qe	0.47	0.04	0.46	0.01
DFC of Critical Approach =				0.47

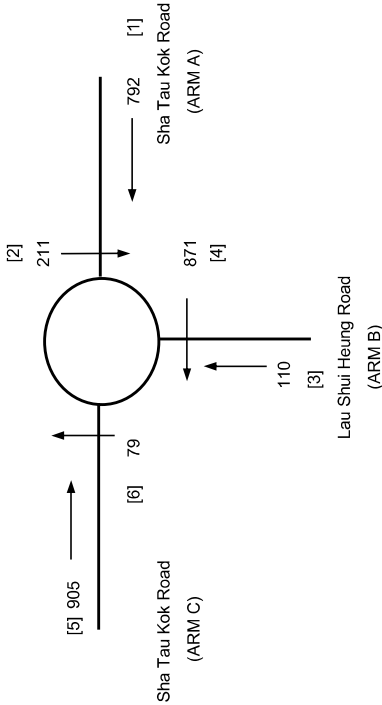
LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Tuxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDBABOUT CALCULATION

PROJECT NO.:	40989	PREPARED BY:		INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:		SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:		SLN	Jun-25
				SLN	Jun-25



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	792	110	905
Qc = Circulating flow across entry (pcu/h)	211	871	79

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1786	859	2092

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.44

Total In Sum = 1807 PCU

LLA CONSULTANCY LIMITED

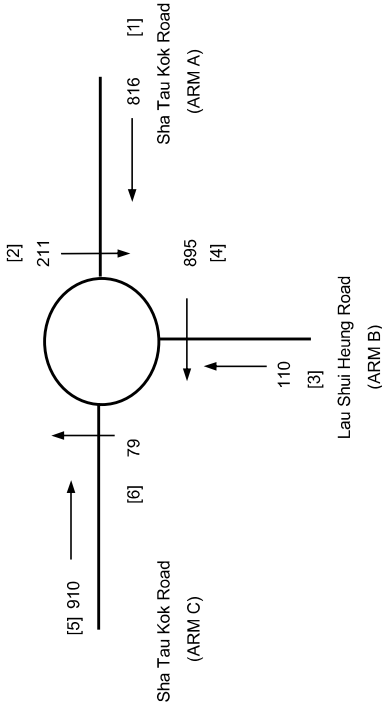
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Tuxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDABOUT CALCULATION

PROJECT NO.:	40989	PREPARED BY:		INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:		SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:		SLN	Jun-25
				SLN	Jun-25

2028 Design 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	816	110	910
Qc = Circulating flow across entry (pcu/h)	211	895	79

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1786	847	2092

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.46

Total In Sum = 1836 PCU

Appendix 3

Drainage Proposal

Drainage Submission in support of

S16 Planning Application for

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years in “Agriculture” zone

at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories

(HT25040)

May 2025

Drainage Consultant:

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

Prepared & approved by	LEE Kwok Cheung <i>RPE(Civil)</i>	
------------------------	--------------------------------------	---

1. Background

- 1.1 With respect to a S16 Planning Application for Proposed Temporary Public Vehicle Park with Electric Charging Device for a Period of 3 Years in “Agriculture” zone at Taxlord Lot 464SA RP(Part) in DD83 and adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories, Messrs. Ho Tin & Associates Consulting Engineers Limited was appointed to prepare a drainage submission.

2. Approach to Prepare this Proposal

- 2.1 This Drainage Submission is prepared in line with the “Technical Note to prepare a Drainage Submission (Relating to applications for temporary change of land use such as temporary storage areas, car parks, workshops, small factories ... etc. under S.16 of the Town Planning Ordinance)” issued by Drainage Services Department in December 2024.





3. The Subject Site and Proposed Development



- 3.1 The subject site of approximately triangular shape with a total site area of about 1,143m² comprises of Taxlord Lot 464SA RP(Part) in DD83 and Adjoining Government Land (about 570m²), Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories. The subject site is on the south side of Sha Tau Kok Road – Lung Yeuk Tau and the west of Kwan Tei Children’s Playground. A Site Location Plan is shown in **Figure D1**.
- 3.2 The subject development consists of one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m²), and 2 numbers of container for EV chargers (3m high, total floor area = 29m² each) accompanying with 12 numbers of EV charging spaces for electric taxi and 7 numbers of parking spaces/waiting spaces for electric taxi. A plan showing the proposed site layout is at **Figure D2**.

4. Existing Drainage Conditions of the Site

- 4.1 At present, the subject site is hard paved, partitioned into sub-units and partly used for storage use and car repairing workshop and partly vacant. In front of the northern subject site boundary is a cycle track beside Sha Tau Kok Road – Lung Yeuk Tau. (refer to **Plate 1 to 4**).

- 4.2 The subject site is encroached upon an access road of the adjacent site to its southwest side (refer to **Plate 5** and **6**). To the southeast side of the subject site is an existing hard paved football field (refer to **Plate 7**).
- 4.3 At present, surface runoff of the subject site would flow into an existing watercourse at its southern corner (refer to **Plate 8** and **9**) and from which the existing watercourse runs southeastward to join a major existing watercourse of the area at the further southeast.
- 4.4 Current conditions of the subject site and its existing drainage conditions are shown in the following photos (photo taking locations are shown on **Figure D3**):

	
Plate 1 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (1 of 4)	Plate 2 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (2 of 4)
	
Plate 3 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (3 of 4)	Plate 4 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (4 of 4)

	
Plate 5 – Entrance of the adjacent site on the southwest side of the subject site	Plate 6 – The adjacent site on the southwest side of the subject site
	
Plate 7 – View to the subject site from the existing football field on the southeast side of the subject site	Plate 8 – Existing watercourse at the southern corner of the subject site to which surface runoff would flow into
	
Plate 9 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into	Extract of the Drainage Records from LandsD's GeoInfo Map in May 2025

5. Drainage Assessment and Proposal

- 5.1 The subject site is a simple small site with a total site area of about 1,143m² (very much less than 1 ha in size). The existing subject site levels are generally same as those of the area close to Sha Tau Kok Road – Lung Yeuk Tau and relatively higher than the surroundings at the remaining three sides. There is no prominent existing surface channels intercepting surface runoff on the area. In general, at present surface runoff of the area would flow toward the existing watercourses with respect to the ground levels. The subject site is obviously not the relatively lower spot of the concerned area. Hence, it is not anticipated that the subject site would receive noticeable runoff from the surroundings.
- 5.2 The existing site levels would be maintained and no site formation/leveling works would be carried out such that the subject proposed development would be maintained at levels relatively higher than its adjacent developments/areas to its east, west and north, and would be at similar levels of the existing cycle track running along its northern boundary. For conservative, in order to ensure no surface runoff from the surroundings, due to any unexpected reasons, flowing onto the subject site to be obstructed, 100mm high gap will be formed at the bottom of the security hoarding/fence wall along the subject site boundary. Cross sections of the proposed subject development and typical details of the gap at the bottom of proposed security hoarding/fence are shown in **Figure D5**.
- 5.3 With respect to the “Technical Note to prepare a Drainage Submission” (the “TN”) published by Drainage Services Department, for the subject site area = 1,143m² ≤ 1,800m², peripheral 375mm U channel at 1 in 200 gradient would be appropriate. It is therefore proposed to construct new peripheral 375mm U channel at 1 in 200 gradient for the subject development. A new catchpit with trap will be constructed at the end of the proposed 375 U channels at the southern corner of the subject site before discharging into the existing watercourse. The discharge point is basically same as that of the existing. A Proposed Stormwater Drainage Layout Plan is shown in **Figure D4**.
- 5.4 Besides, the Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the drainage system to the satisfaction of relevant Government departments.

5.5 Details of proposed drainage provisions shall follow relevant details shown in Government departments’ Standard Drawings as follows:

<i>Proposed Drainage Provisions</i>	<i>Standard Drawings</i>	<i>Drawing No. & Title</i>
Catchpit	CEDD Standard Drawings	C 2405/1 to /5 – Standard Catchpit Details
Catchpit with trap		C 2406/1 to /2A – Catchpit with Trap
Catchpit precast concrete cover		C 2407B – Precast Concrete Covers for Catchpit and Sand Trap
U-channel		C 2409J – Details of Half-round and U-channels
Channel cover		C 2412E – Cover Slab and Cast Iron Grating for Channels

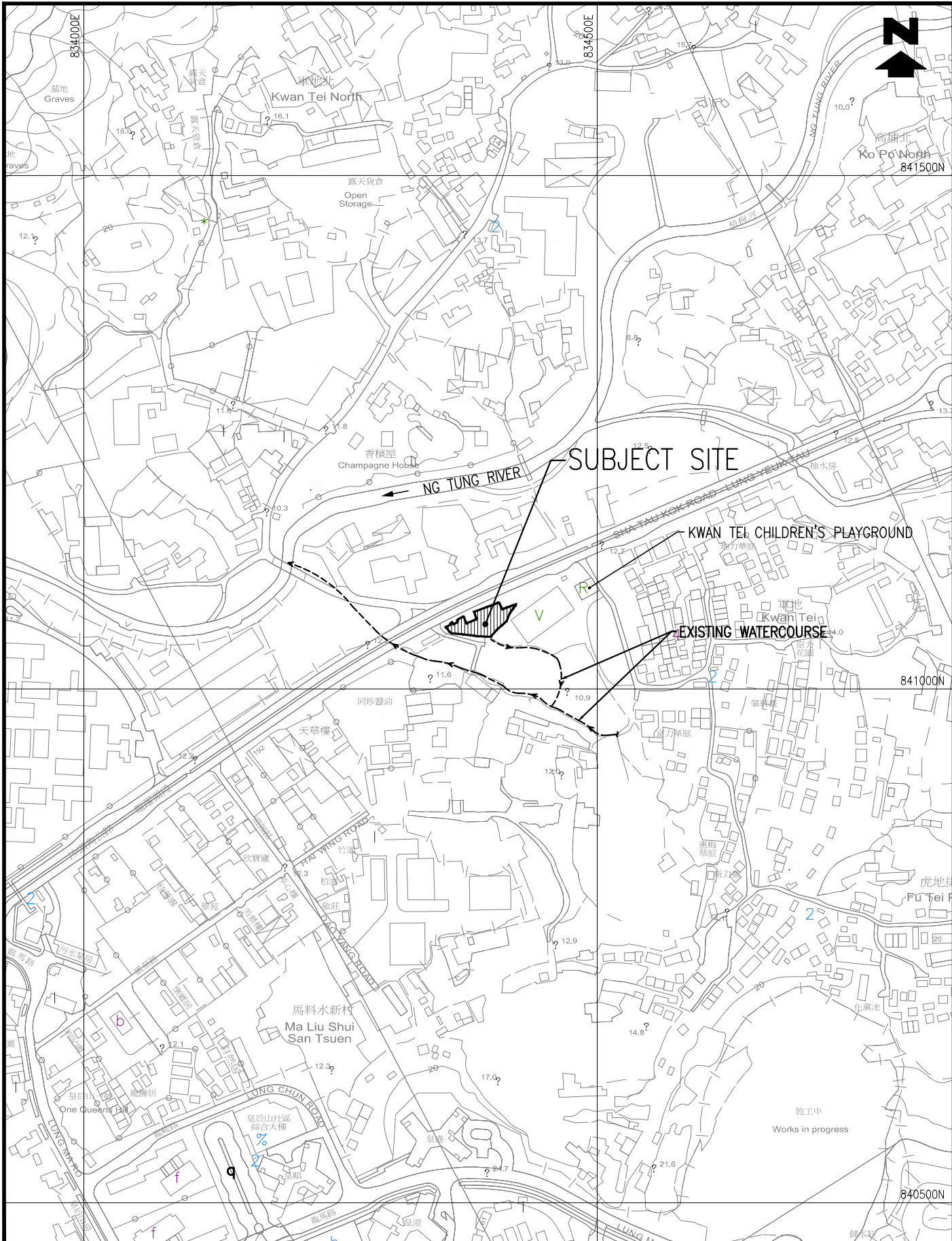
6. Conclusion and Recommendations

- 6.1 The subject development as Proposed Temporary Public Vehicle Park with Electric Charging Device for a Period of 3 Years. The subject site area has been hard paved for a very long period without complaints on drainage conditions. The existing site levels would be maintained and no site formation/leveling works would be carried out.
- 6.2 At present, the levels of the subject site are relatively higher than/similar to those of the surroundings. In principle, no surface runoff would flow onto the subject site from its surroundings. 375mm U channel at 1 in 200 gradient will be constructed at the peripheral of the subject site to intercept all crossing surface runoff. For conservative, 100mm high gap will be formed at the bottom of the security hoarding/fence along the subject site boundary to ensure no surface runoff from the adjacent, due to any unexpected incidents, to be obstructed. A new catchpit with trap will be constructed

at the ends of the 375mm U channel before discharging into the existing watercourse at the southern corner.

- 6.3 The Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the system to the satisfaction of relevant Government departments.
- 6.4 The subject development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works, i.e. debris clearance and damage repair. Therefore, in conclusion, the subject development would not cause any adverse drainage impact onto the area.

H:\25040_LungYeukTau_Site2\DRAWING\25040_FIGD1_00.dwg, 22/5/2025 17:11:21



TITLE

SITE LOCATION PLAN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

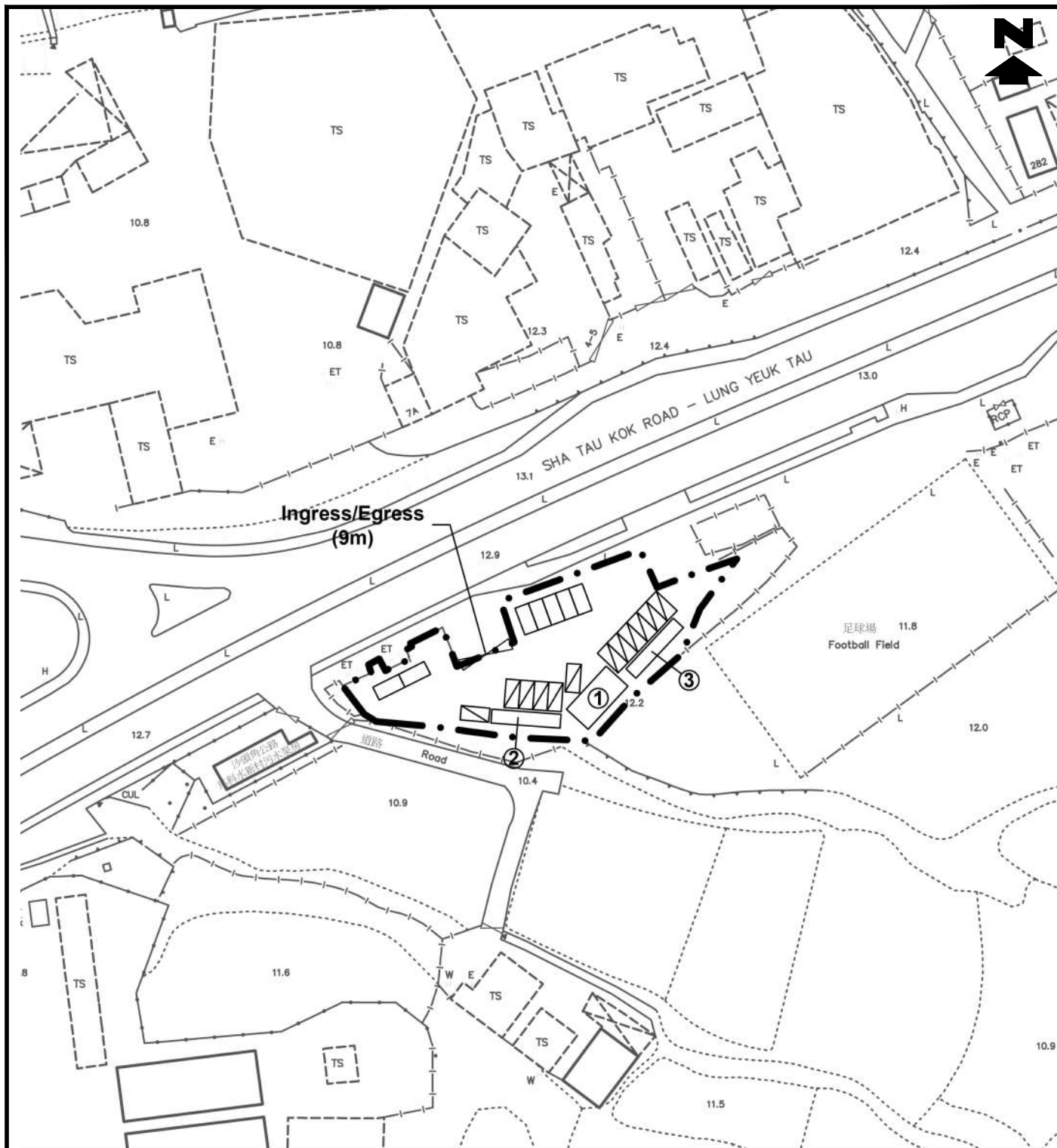
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DRAWING No.

FIGURE D1

H:\25040_LungYeukTau_Site2\DRAWING\25040_FIGD2_00 FIGD3_00 FIGD4_00 & FIGD5_00.dwg, 20/5/2025 10:46:06



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 1,143m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (7 nos.)

TITLE

LAYOUT PLAN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

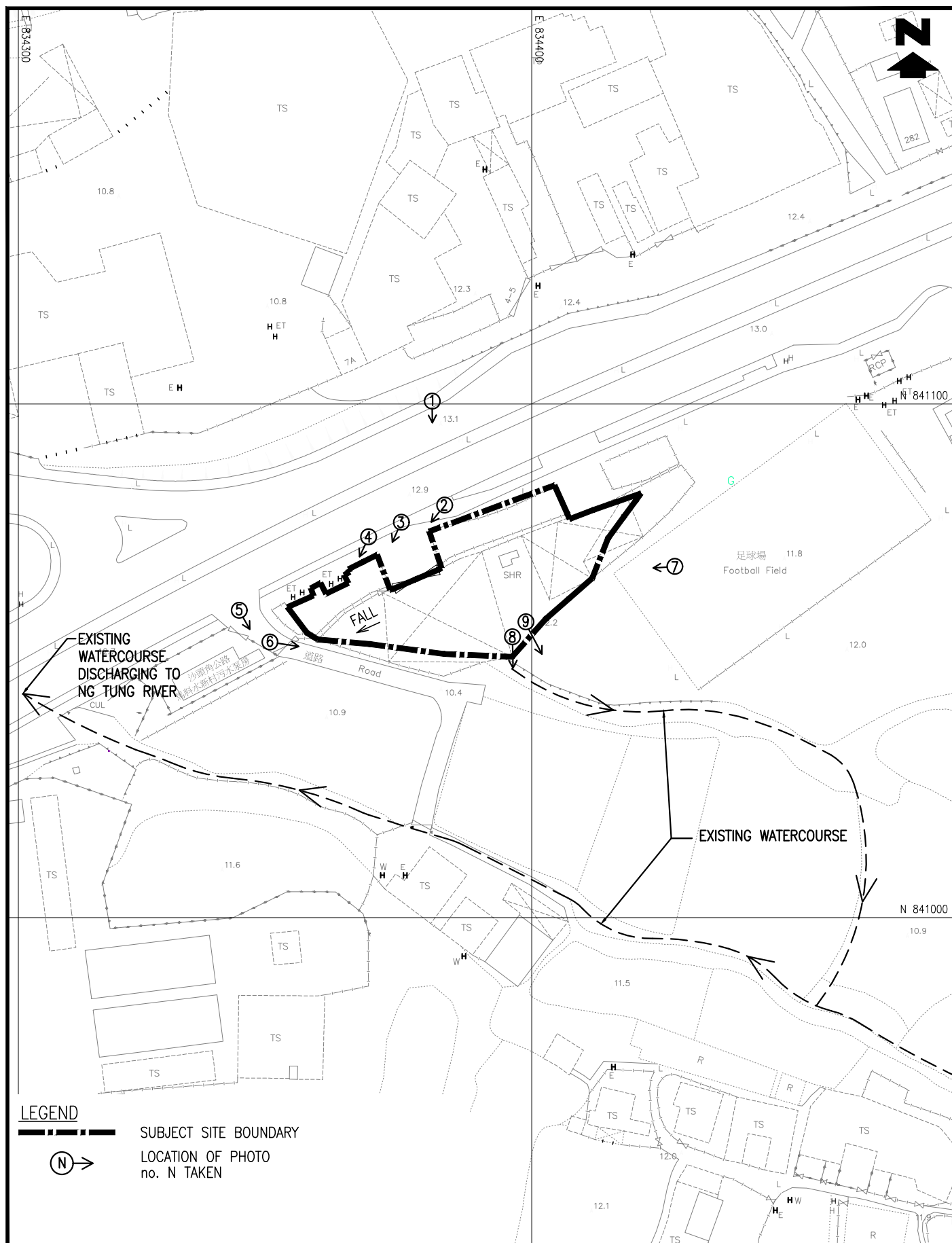
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DRAWING No.

FIGURE D2

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TITLE

EXISTING DRAINAGE AND LOCATIONS
OF PHOTO TAKEN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

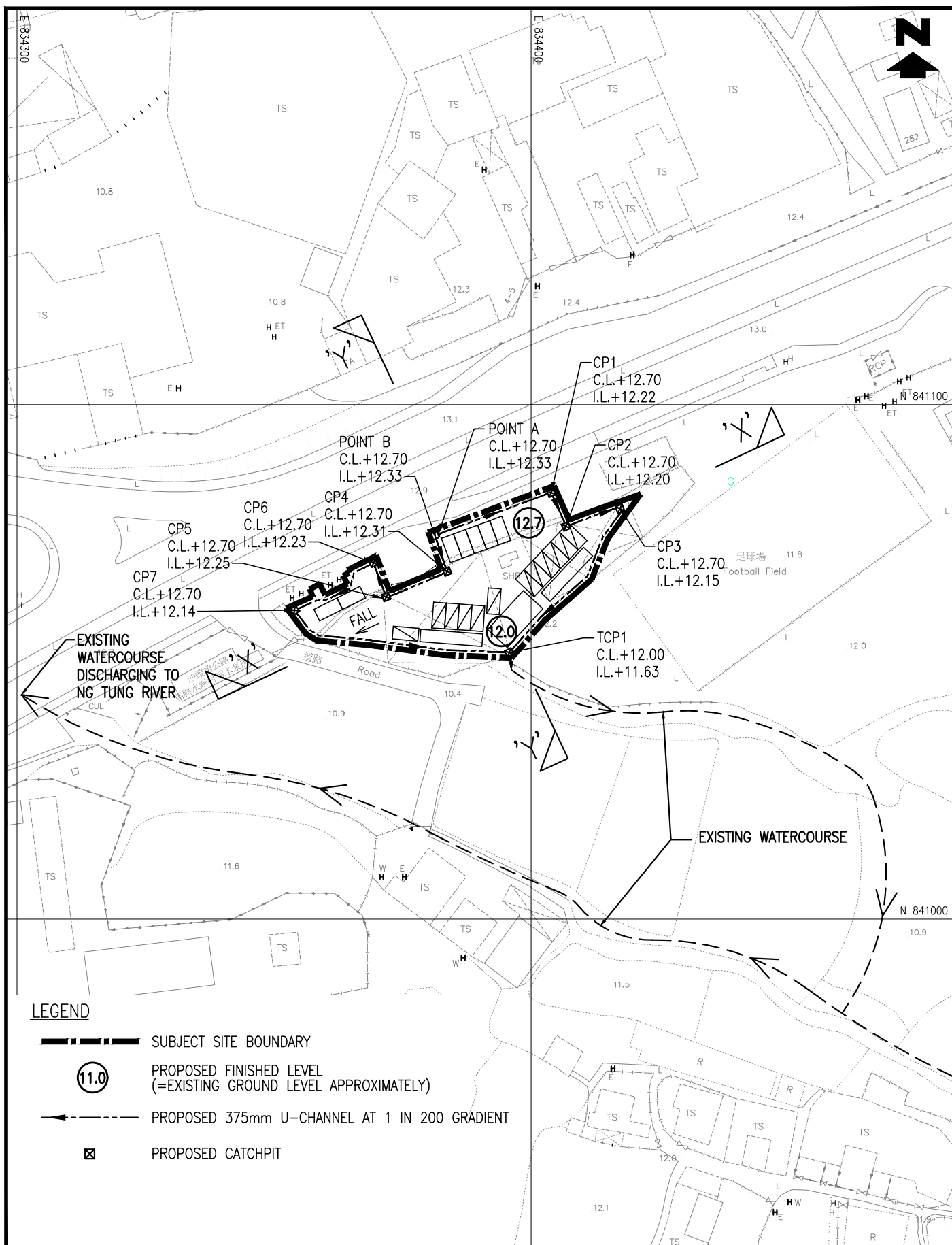
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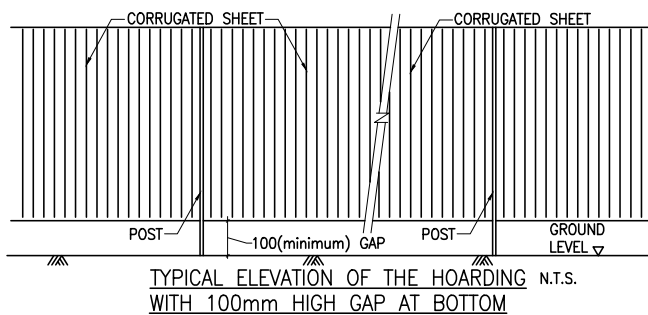
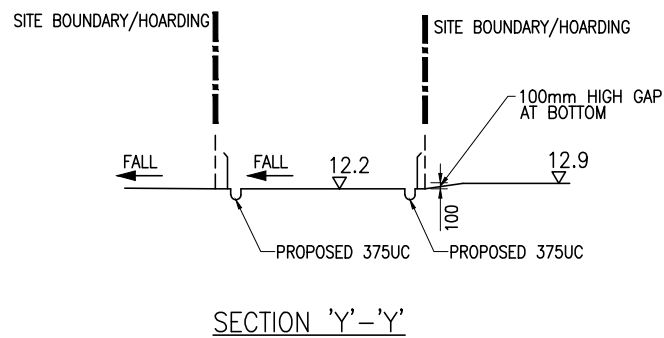
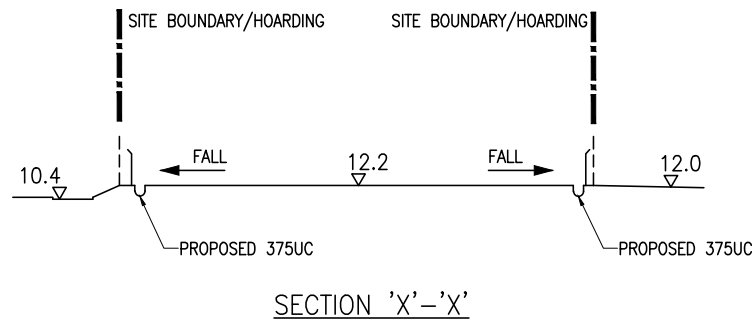
1 : 1000 - A4

DRAWING No.

FIGURE D3

H:\25040_LungYeukTau_Site2\DRAWING\25040_FIGD2_00 FIGD3_00 FIGD4_00 & FIGD5_00.dwg, 22/5/2025 11:15:54





TITLE

SITE CROSS SECTIONS

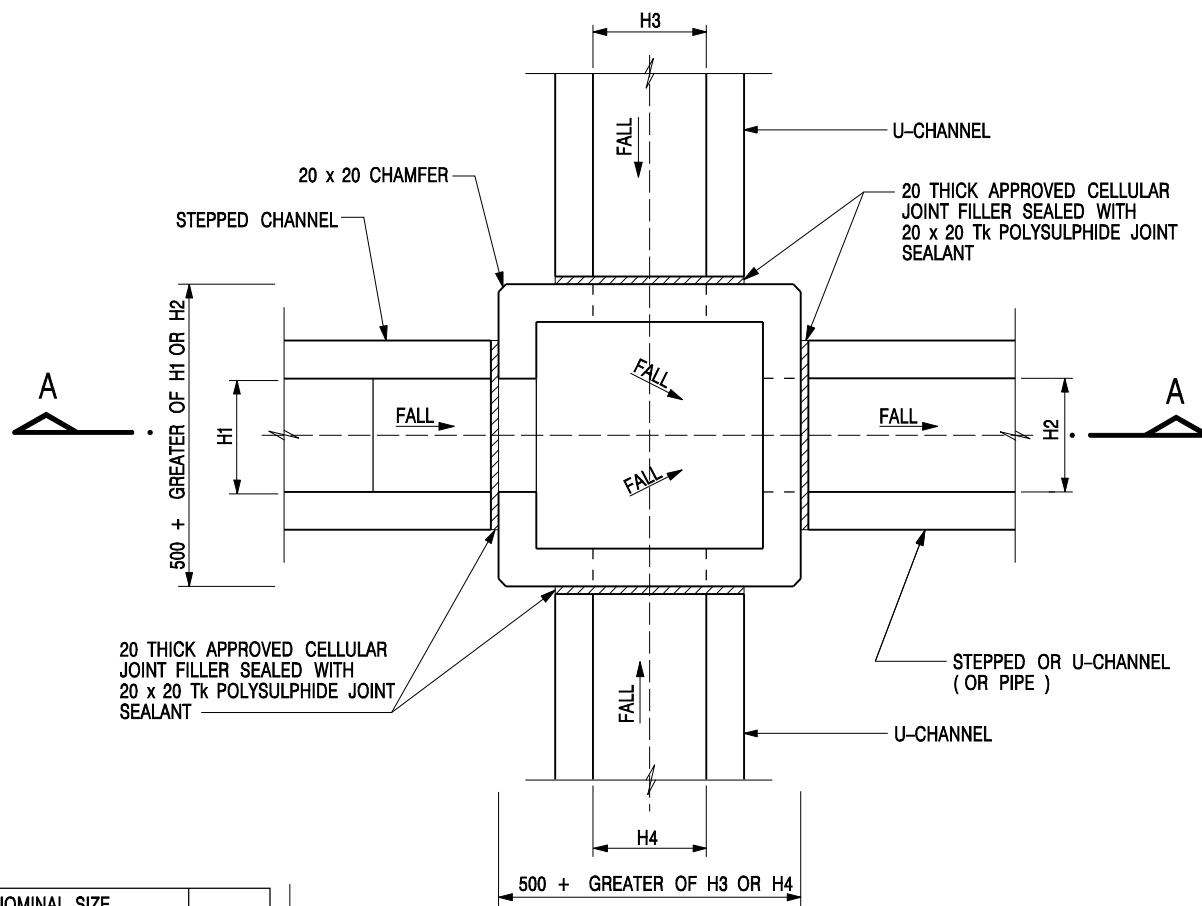
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 500 - A4

DRAWING No.

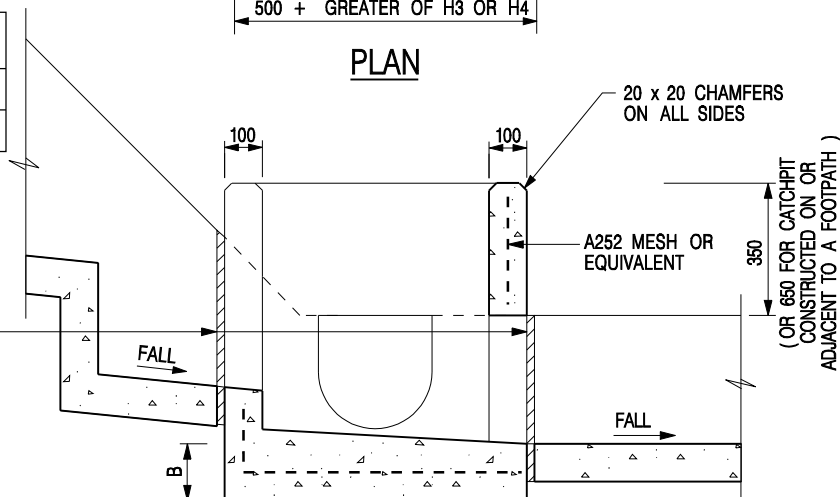
FIGURE D5



PLAN

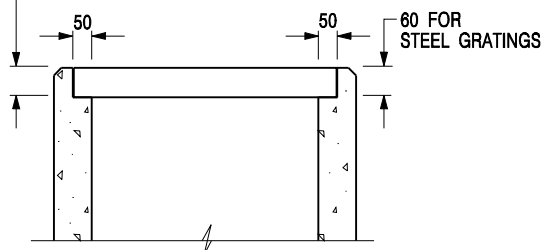
NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT



SECTION A - A

DEPTH OF RECESS AND DETAILS OF PRECAST CONCRETE COVERS (SEE STD. DRG. NO. C2407)




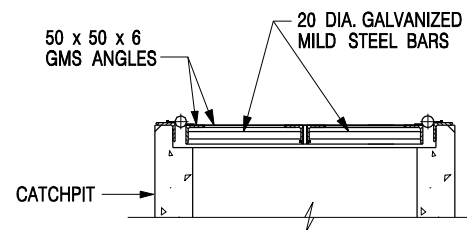
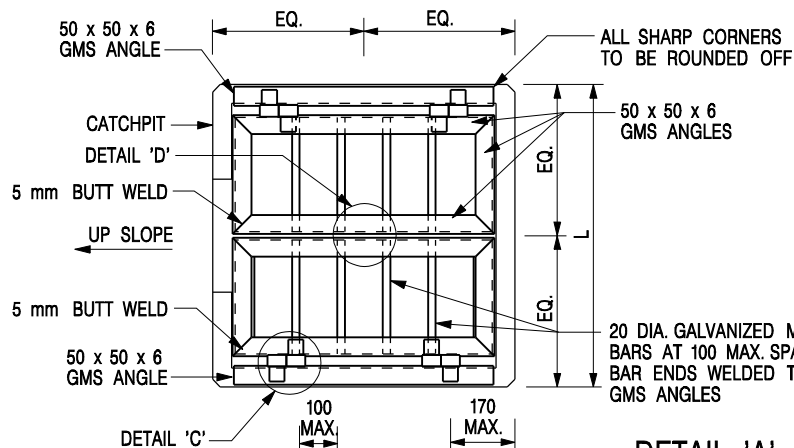
NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

**ALTERNATIVE TOP SECTION FOR
PRECAST CONCRETE COVERS / GRATINGS**

**STANDARD CATCHPIT DETAILS
(SHEET 1 OF 5)**

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
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SCALE 1 : 20		DRAWING NO. C2405 /1	
DATE JAN 1991			

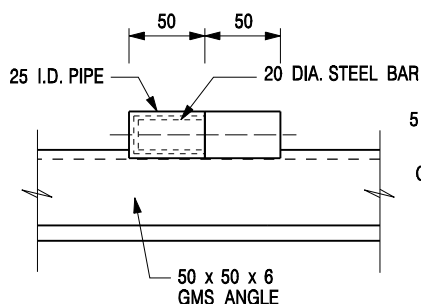


SECTIONAL ELEVATION

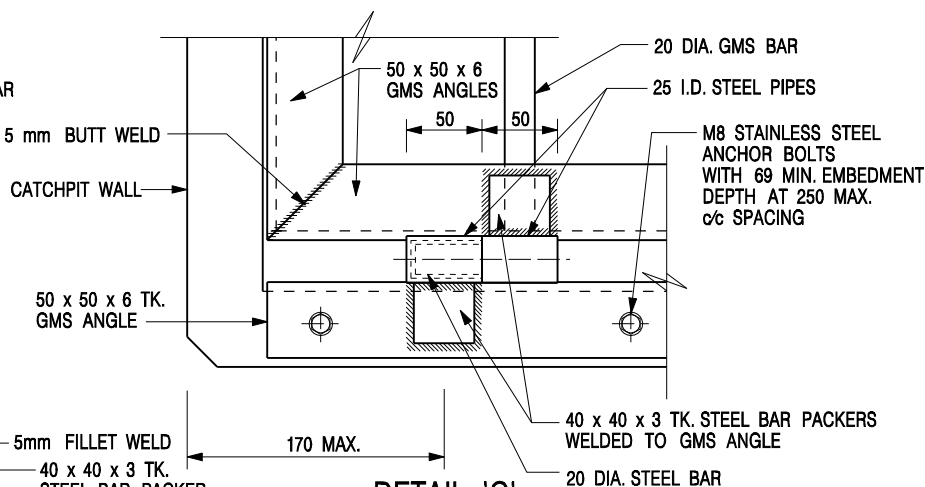
DETAIL 'A'

(DETAILS OF DOUBLE SIDE OPENING STEEL GRATING FOR L > 900mm)

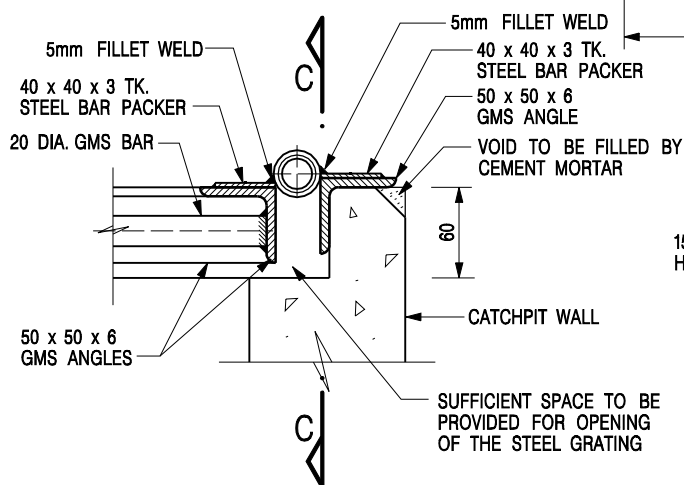
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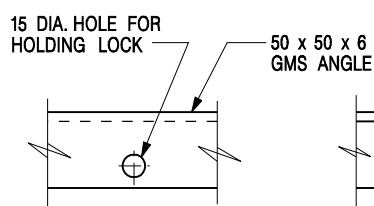
SECTION C - C



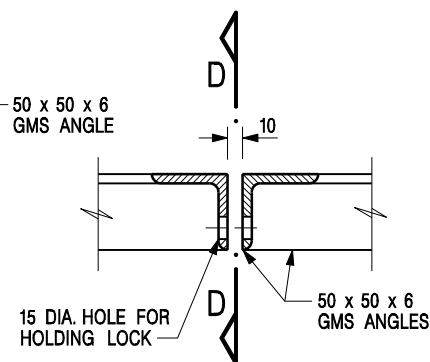
DETAIL 'C'
(DETAILS OF HINGE)
SCALE 1 : 5



SECTIONAL ELEVATION
(DETAIL 'C')



SECTION D - D




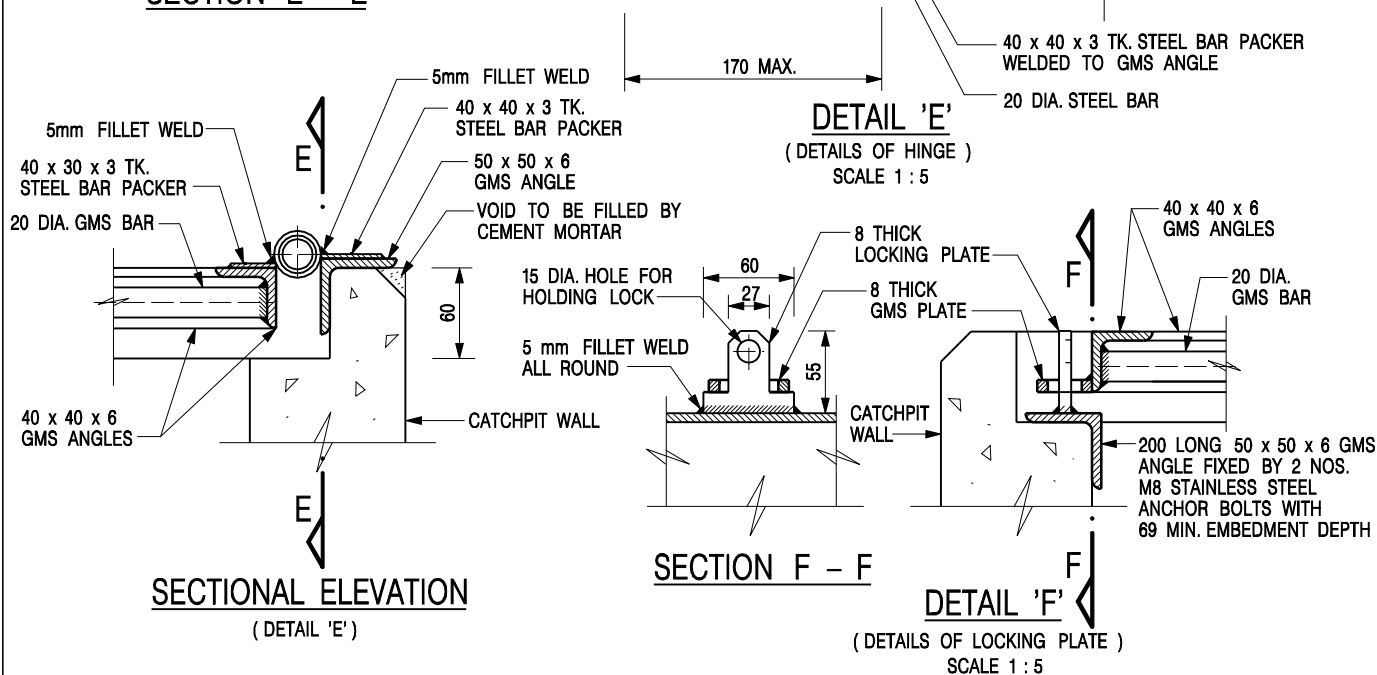
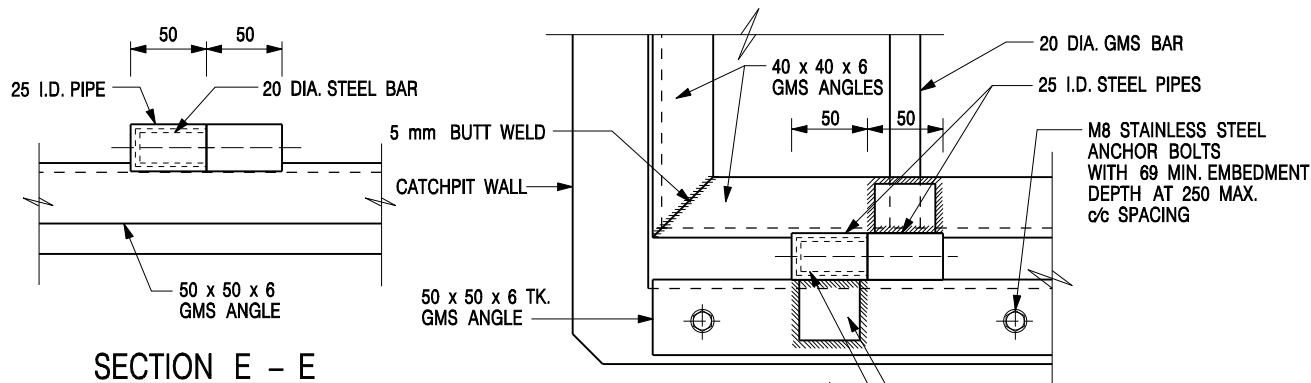
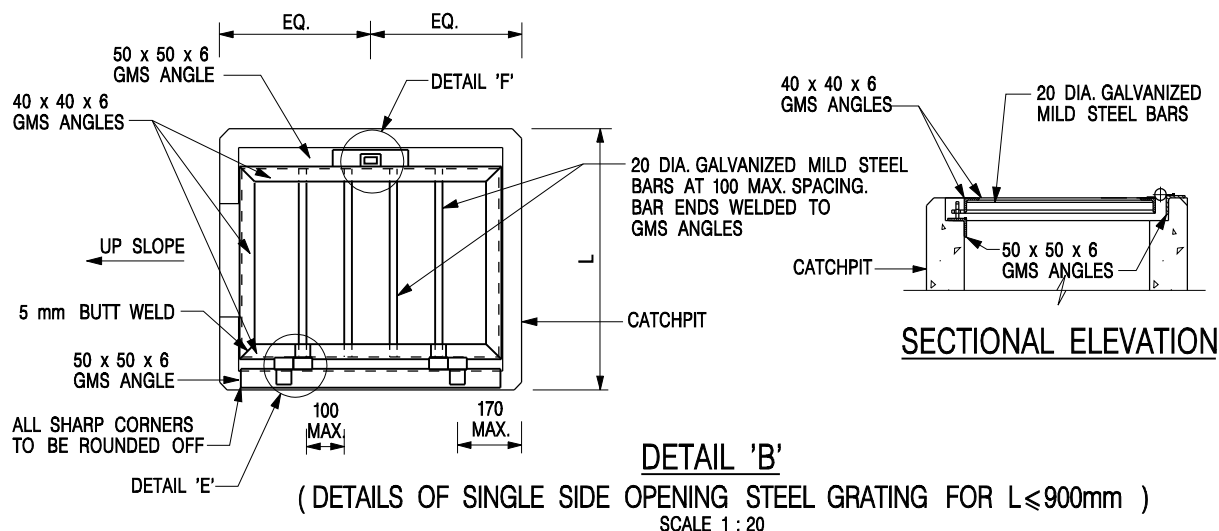
DETAIL 'D'
(DETAILS OF HOLE FOR LOCK)
SCALE 1 : 5

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 2 OF 5)


-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /2	

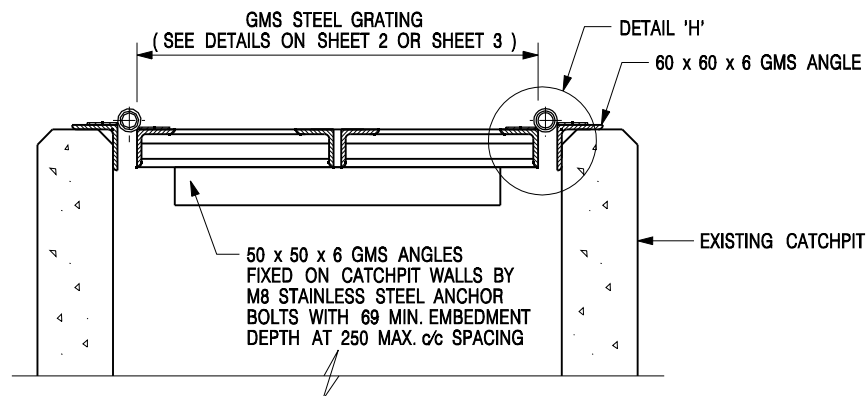


NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

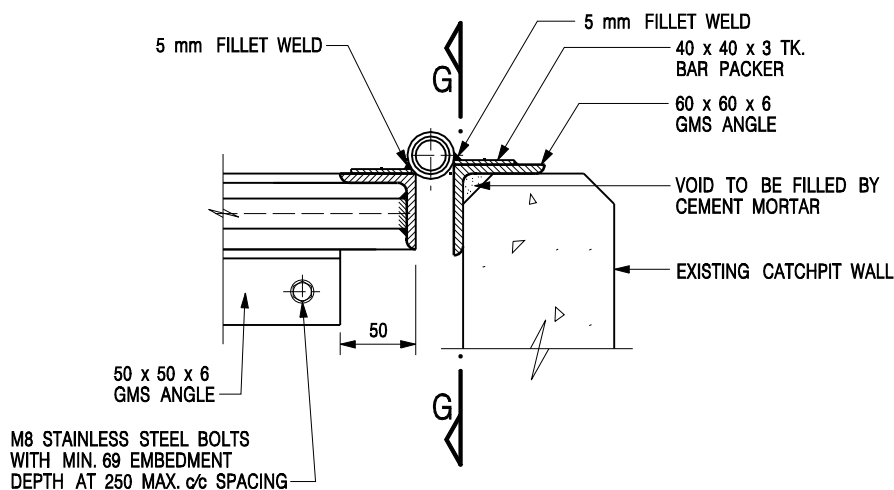
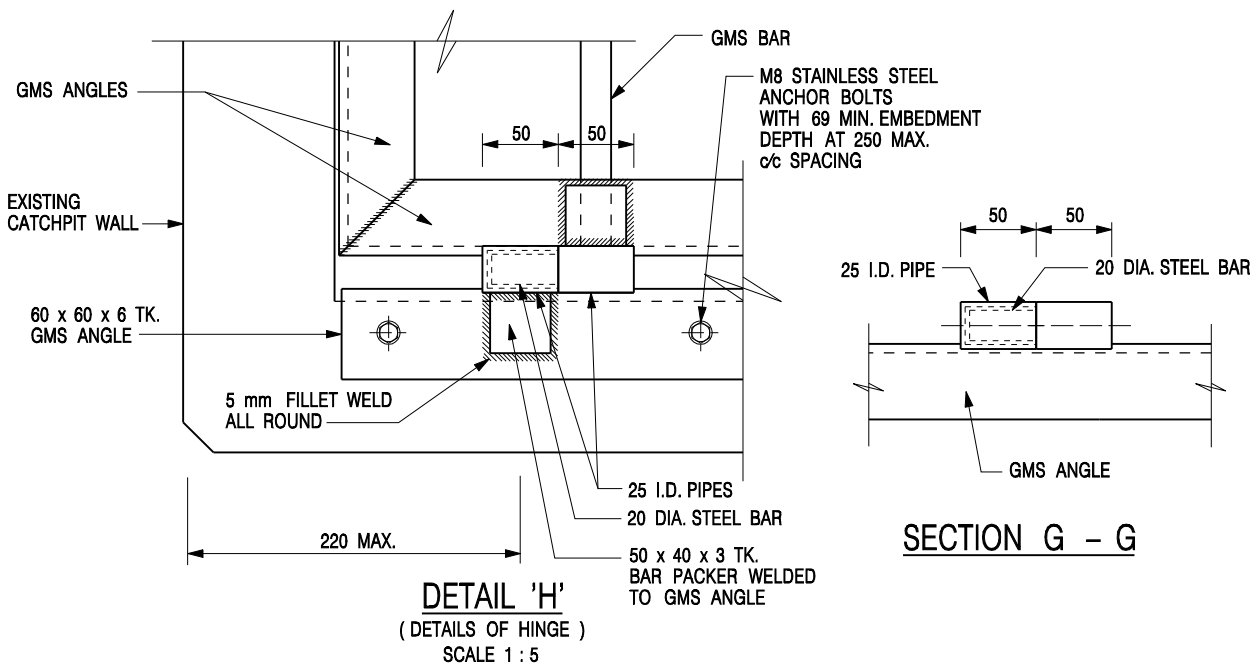
STANDARD CATCHPIT DETAILS
(SHEET 3 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /3	



DETAIL 'G' - DETAILS OF STEEL GRATING CONSTRUCTED ON EXISTING CATCHPIT

SCALE 1 : 10




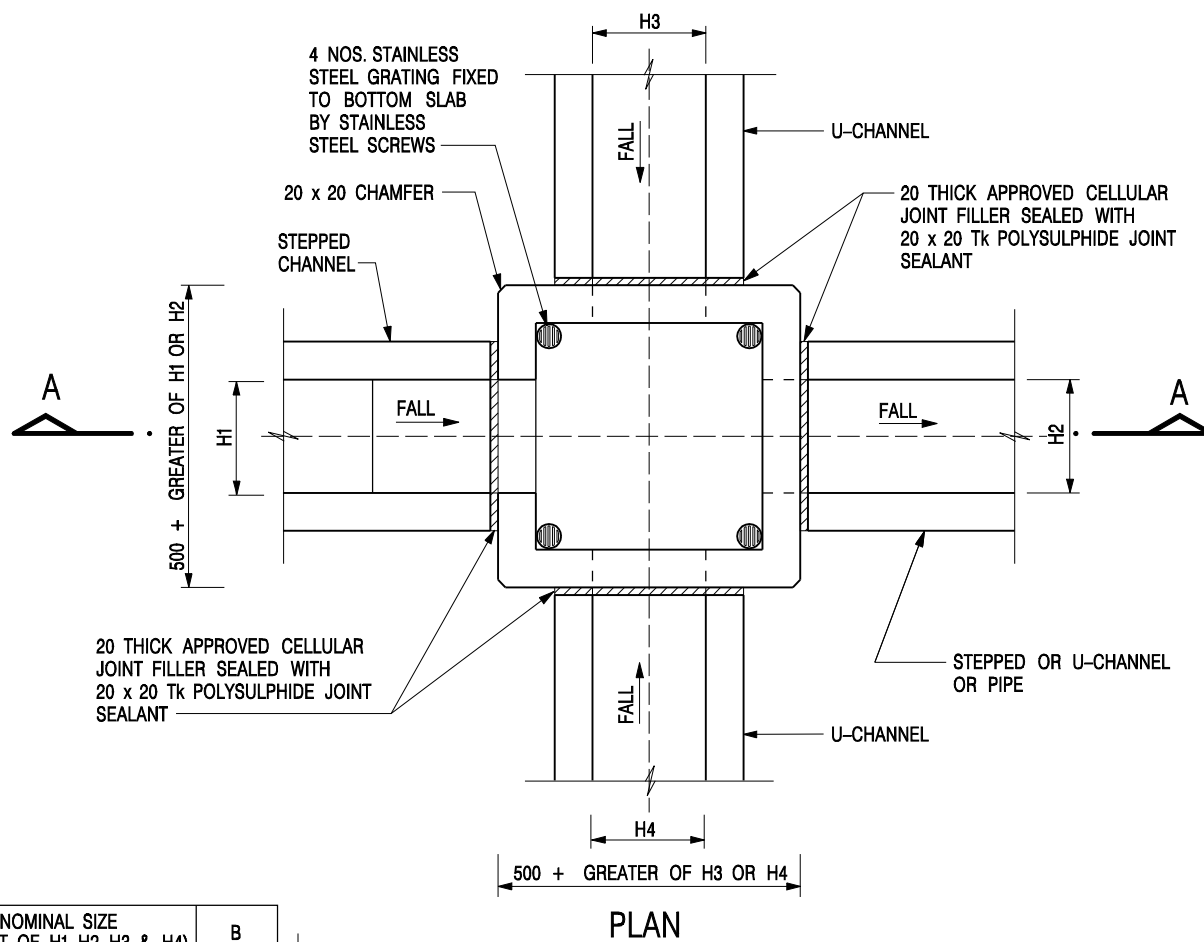
SECTIONAL ELEVATION (DETAIL 'H')

NOTES:

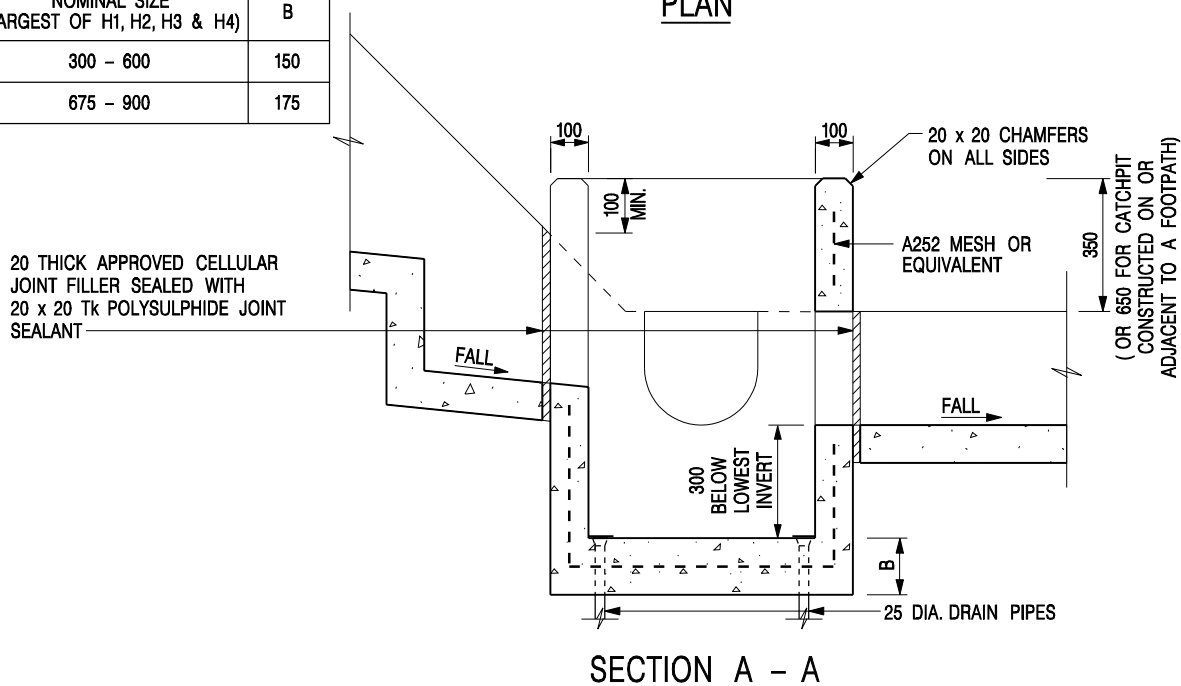
1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 4 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /4	




NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

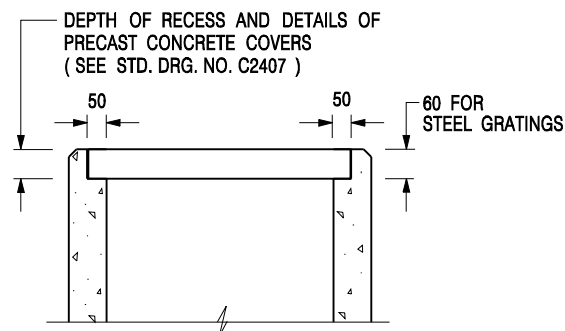


NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

CATCHPIT WITH TRAP
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE 1 : 20 DATE JAN 1991	
		DRAWING NO. C2406 /1	



ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAIL 'A' ON STD. DRG. NO. C2405 /2) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON STD. DRG. NO. C2405 /5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON STD. DRG. NO. C2405 /4.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

A	MINOR AMENDMENT.	Original Signed	04.2016
-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP
(SHEET 2 OF 2)**



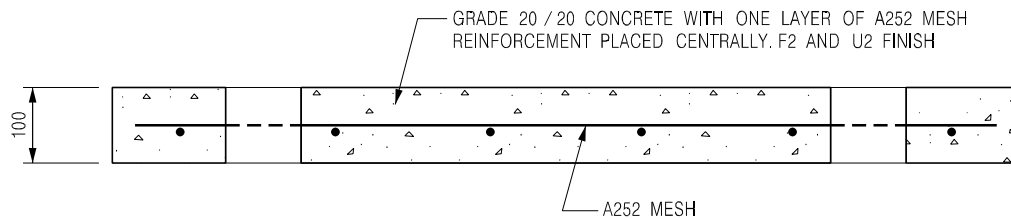
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

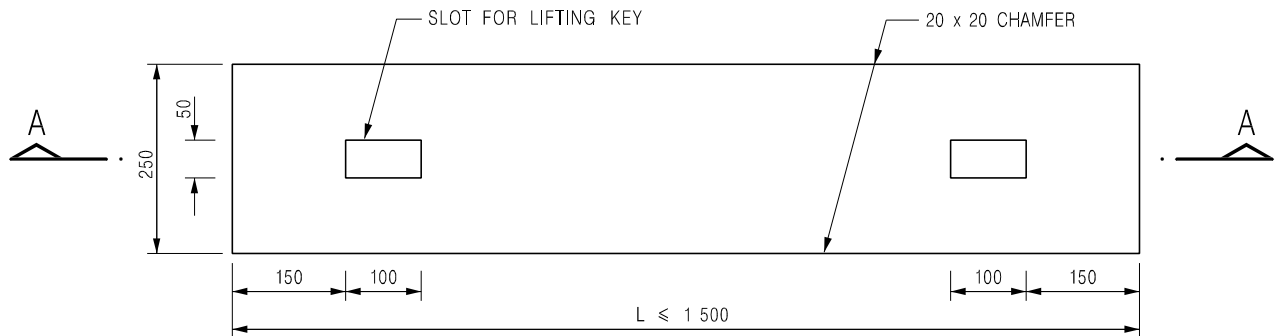
DATE JAN 1991

DRAWING NO.

C2406 /2A

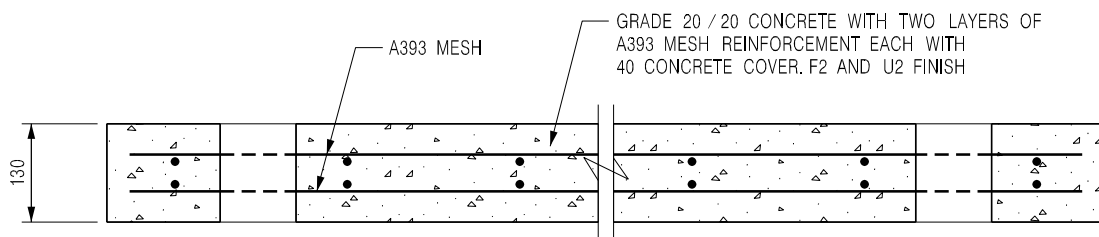


SECTION A - A

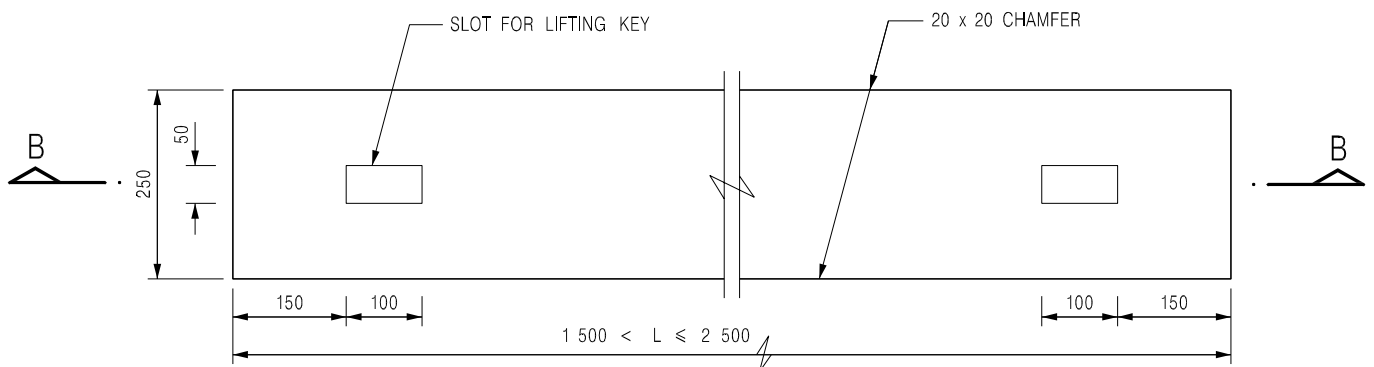


PLAN

TYPE 1 - FOR SPAN UP TO 1.5 m



SECTION B - B



PLAN

TYPE 2 - FOR SPANS 1.5 m TO 2.5 m

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL EXTERNAL EDGES OF THE COVERS SHALL BE 20mm CHAMFERED.

B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	GENERAL REVISION	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

PRECAST CONCRETE COVERS
FOR CATCHPIT AND SAND TRAP

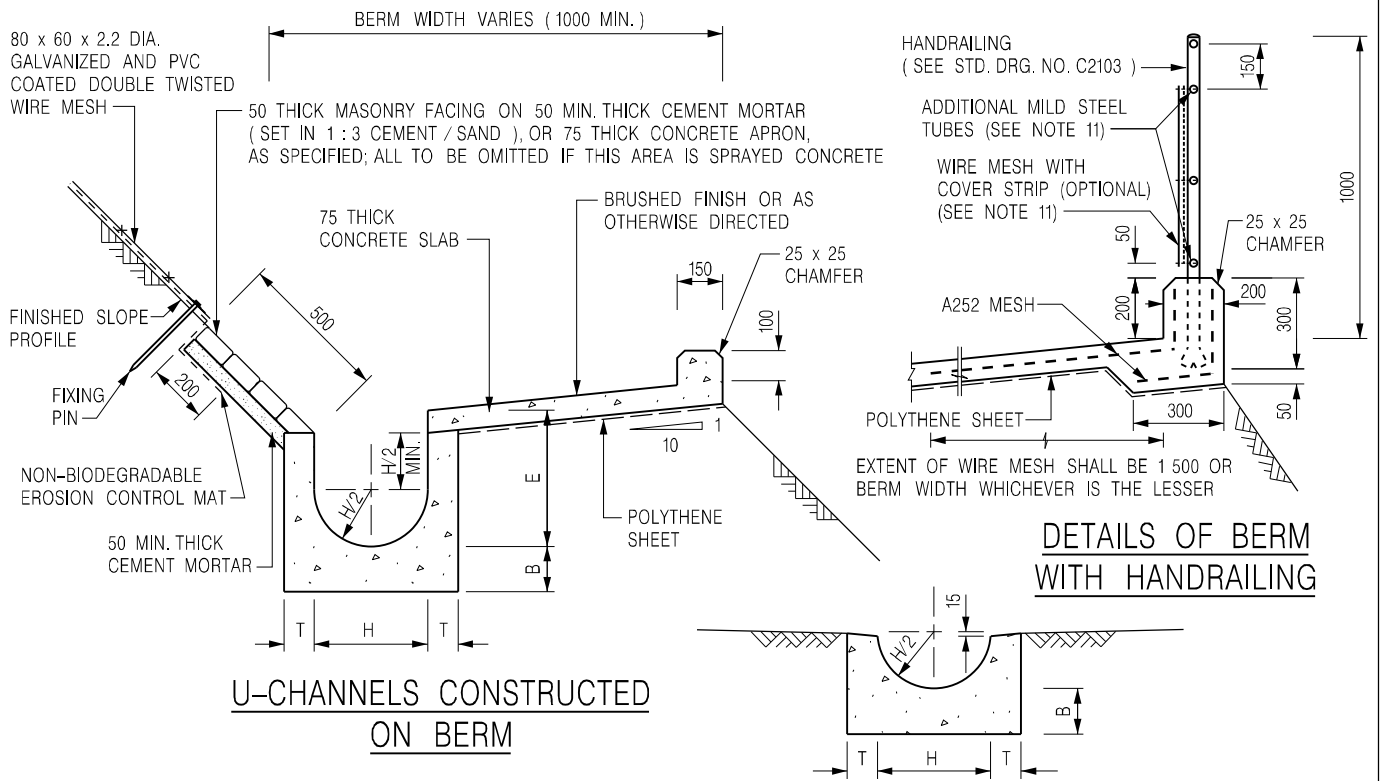


CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT

SCALE 1 : 10

DATE JAN 1991

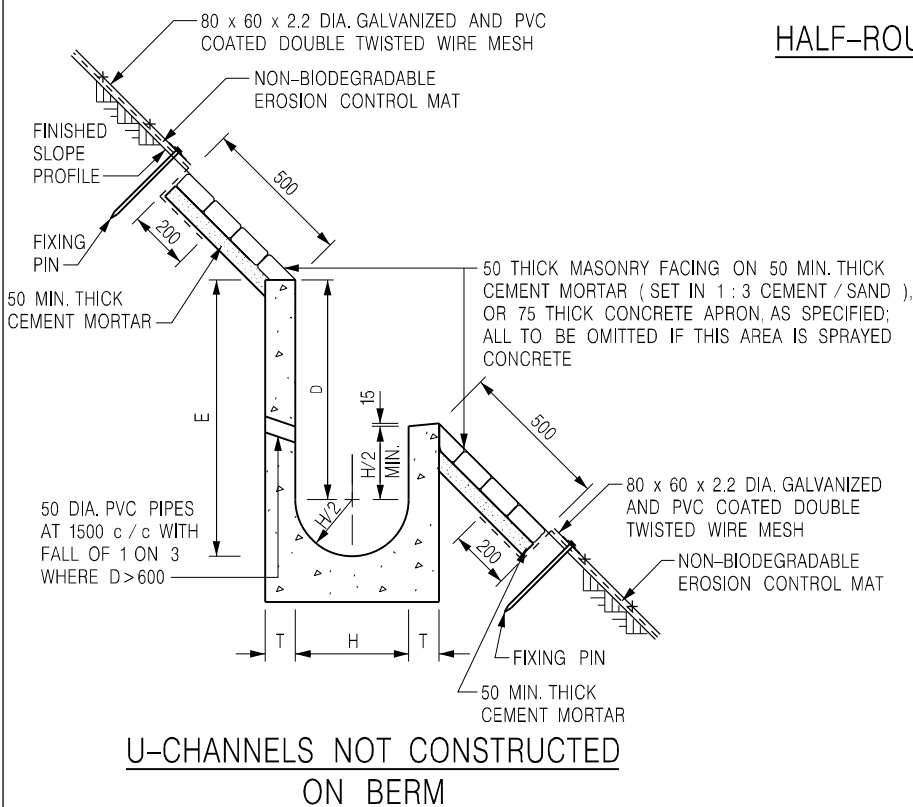
DRAWING NO.
C2407B



HALF-ROUND CHANNEL

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE TO BE GRADE 20 / 20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2, F2 OR BRUSHED FINISH AS DIRECTED.
4. SPACING OF EXPANSION JOINT IN CHANNELS, BERM SLABS AND APRONS TO BE 10 METRES MAXIMUM, SEE STD. DRG. NO. C2413 FOR DETAILS.
5. JOINTS FOR CHANNELS, BERM SLABS, APRONS AND WALLS, ETC. TO BE ON THE SAME ALIGNMENT.
6. FOR DIMENSIONS T, H, & B, SEE TABLE BELOW.
7. BIODEGRADABLE EROSION CONTROL MAT IF REQUIRED, SEE STD. DRG. NO. C2511/E.
8. CONCRETE TO BE COLOURED AS SPECIFIED.
9. CONCRETE U-CHANNEL CAN BE CAST IN-SITU OR PRECAST CONCRETE SUBJECT TO THE ENGINEER'S AGREEMENT ON THE DETAILS.
10. DETAILS OF EROSION CONTROL MAT AND WESH MESH ON BERM. (SEE STD. DRG. NO. C2511/E)
11. THE WIRE MESH ON HANDRAILING IS OPTIONAL. THE COVER STRIP AND ADDITIONAL MILD STEEL TUBES ARE NEEDED ONLY IF WIRE MESH IS PROVIDED. (SEE STD. DRG. NO. C2103)



NOMINAL SIZE H	T	B	REINFORCEMENT
300	80	100	A252 MESH PLACED CENTRALLY AND T=100 WHEN E>650
375 - 600	100	150	
675 - 900	125	175	A252 MESH PLACED CENTRALLY

DETAILS OF HALF-ROUND AND U-CHANNELS (TYPE A - WITH MASONRY APRON)

J	DETAILS OF HANDRAILING AMENDED.	Original Signed	08.2024
I	MINOR AMENDMENT.	Original Signed	07.2018
H	THICKNESS OF MASONRY FACING AMENDED.	Original Signed	01.2005
G	MINOR AMENDMENT.	Original Signed	01.2004
F	GENERAL REVISION.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE



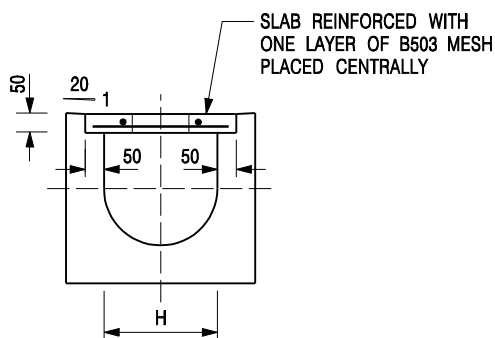
CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT

SCALE 1 : 25

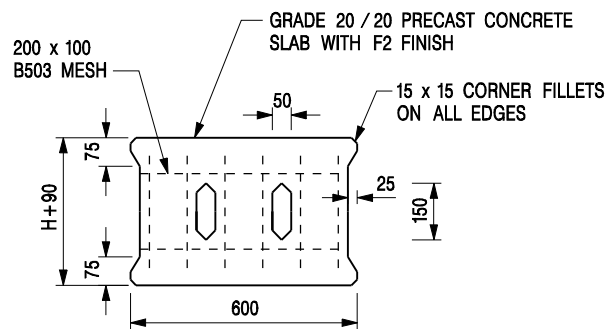
DATE JAN 1991

DRAWING NO.

C2409J



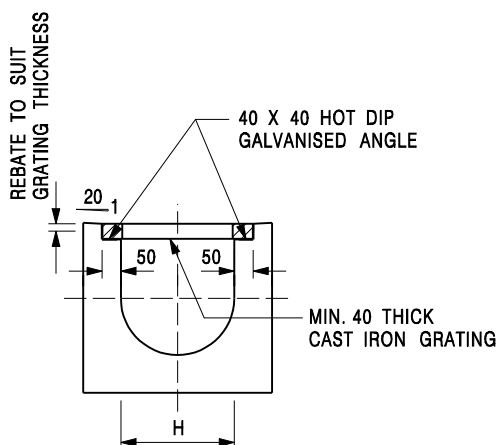
TYPICAL SECTION



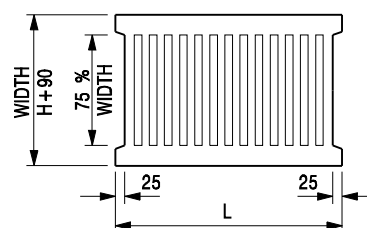
PLAN OF SLAB

U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION



L = 600mm FOR H ≤ 375mm
L = 400mm FOR H > 375mm

CAST IRON GRATING

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**COVER SLAB AND CAST IRON
GRATING FOR CHANNELS**



**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

DATE JAN 1991

DRAWING NO.
C2412E



羅迅測計師行
Lawson David & Sung
SURVEYORS LIMITED



RICS
The mark of
property professionalism worldwide

Appendix Ib of
RNTPC Paper No. A/NE-LYT/860A



宋梓華
Sung Tze Wah
FRICS FHKIS MSISV MCIREA ACIarb R.P.S.(GP)

李霧儀
Lee Mo Yi
MPIA RPP MUDD BA (Hons)

吳恆廣
Ng Hang Kwong, BBS
FRICS FHKIS R.P.S.(GP)
Honorary World Valuer (WAVO)

林桂金
Daniel K.K. Lam
MRICS MHKIS MCIREA R.P.S.(GP) BSc

宋樹鴻
Sung Shu Hung
FRICS MHKIS R.P.S.(GP)(PD) MCIREA
MHIREA BSc (Hons)

趙慧姿
Chiu Wai Chi
MRICS MHKIS MSc BBus (Mktg)

Consultant :

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RPE PMgr CEnv FIHE FCIOB MICE
MHKIE MSOE FCMi MCIarb MSc

劉志光
Lau Chi Kwong
FRICS FHKIS ALS MHKIS
R.P.S.(LS)(PFM) MSc

潘孝維
Pun How Wai
B.Arch.HK RIBA

By Email and Post

Date : 7 November 2025
Your Ref.: TPB/A/NE-LYT/860
Our Ref. : LDS/PLAN/7140

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road, North Point, Hong Kong

Dear Sir/Madam,

Application for Planning Permission for Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.
(Application No. A/NE-LYT/860)

We refer to the comments from various Government departments on the captioned application and submit herewith our response in **Table 1** for your consideration.



With the set back of application boundary in response to the comments from Drainage Services Department and Water Supplies Department, the updated development parameters are as follows:

Application Area:	About 954m ² (including 381m ² of Government land)
No. of Parking Spaces:	17 Nos. (2.5m x 5m) - 12 EV charging spaces for electric taxis - 5 parking/waiting spaces for electric taxis

The revised set of plans (**Figures 1A-4A and 4B**) is attached for your reference.

Should there be any queries, please contact our Ms. Cannis Lee at

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited

Encl.

c.c. DPO/STN (Attn.: Mr. Peter Ngan) – By Email
Client

Your Assets for Growth

Table 1: Responses to Comments from Government Departments on Planning Application No. A/NE-LYT/860 (7 November, 2025)

COMMENTS	RESPONSES
1. Drainage Services Department	
Please find the following comments to the application and drainage proposal submitted from the public drainage viewpoint.	
(a) According to the record, there are existing Drainage Services Department's (DSD) facilities including sewerage underground pipes and manholes on the Government land (GL) within the application site (the Site). Our drainage record is attached for reference (Attachment 1). The applicant shall place all the proposed works at least 3m away from outermost surfaces of DSD's facilities. All the proposed works should not disturb, interfere with or cause damage to existing drainage facilities, and all the proposed works in the vicinity of existing drainage facilities should not create any adverse drainage impacts, both during and after construction. Please review the layout plan and drainage proposal accordingly.	(a) It is noted. The subject proposed layout and the proposed drainage works are therefore amended, i.e. setback, such that all the proposed works, including the drainage, are placed at least 3m away from the outermost surfaces of the DSD's facilities. Please refer to the amended Figure D2 and D4 in the amended Drainage Proposal (see Annex A). All the proposed works should not disturb, interfere with or cause damage to existing drainage facilities, and all the proposed works in the vicinity of existing drainage facilities should not create any adverse drainage impacts, both during and after construction. For easy reference, please refer to Figure 4B for the setback distance from the outermost surfaces of the DSD's facilities.
(b) An area covering DSD's drainage facilities with 3m laterally beyond both outside edges of the concerned drainage facilities shall be demarcated where (i) no building or structure or supporting elements shall be erected and (ii) DSD shall have free and unrestricted	(b) It is noted and will be followed. Please refer to the amended Figure D2 and D4 for the proposed works in the amended Drainage Proposal.

COMMENTS	RESPONSES
<p>access at all times for construction, repairs and maintenance works to the drainage facilities during the planning approval period. Please review the general arrangement and layout plan accordingly.</p>	
<p>(c) The applicant should check and ensure that the existing drainage downstream to which the proposed connection will be made have adequate capacity and satisfactory condition to cater for the additional discharge from the Site. He should also ensure that the flow from the Site will not overload the existing drainage system. Besides, more site photos of the existing watercourse should be provided to demonstrate its condition.</p>	<p>(c) There is no change of the discharge point of the subject site before and after the proposed development except the flow of the subject site after the proposed development will be properly managed, i.e. the flow will be conveyed inside properly designed engineering channels/catchpits before being discharged. As the subject site is already hard paved before this application, there would be no increase in surface runoff. Besides, the subject proposed development would not alter the existing drainage flow pattern/paths of the area, therefore it would not cause any adverse drainage effects, i.e. overloading, onto the existing drainage downstream to which the proposed connection will be made.</p> <p>More site photos of the existing watercourse are provided in the amended Drainage Proposal to demonstrate its condition.</p>
<p>(d) The proposed drainage works, whether within or outside the Site, should be constructed and maintained properly by the applicant and rectify the system if it is found to be inadequate or ineffective during operation at his/her own expense.</p>	<p>(d) It is noted and agreed.</p>
<p>(e) For works to be undertaken outside the Site, the applicant should obtain prior consent and agreement from the District Lands Office/North of Lands Department and/or relevant private lot owners.</p>	<p>(e) It is noted and agreed.</p>

COMMENTS	RESPONSES
(f) The applicant should make good all the adjacent affected areas upon the completion of the drainage works.	(f) It is noted and agreed.
(g) The applicant and the successive lot owners shall allow connections from the adjacent lots to the completed drainage works on GL when so required.	(g) It is noted and agreed.
(h) The Site is within an area where connection to existing public sewerage network is available in the vicinity. Should the applicant choose to connect his proposed Sewerage systems to DSD's networks, he or she shall furnish me with their connection proposal for agreement. After obtaining my agreement, the applicant shall submit a duly completed Form HBPI with a cross cheque covering the technical audit fee and a plan showing the details of the proposed connection works to this Division for formal application for the required connection. Upon acceptance of the connection application, the applicant shall carry out the proposed connection works' in accordance with DSD Standard Drawings at the resources of the applicant.	(h) It is noted. The subject proposed development consists of one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m ²), and 2 numbers of container for EV chargers (3m high, total floor area = 29m ² each) accompanying with 12 numbers of EV charging spaces for electric taxi and 5 numbers of parking spaces/waiting spaces for electric taxi. No toilet nor kitchen, etc. which will generate sewage flow will be provided. Hence, no sewerage connection works is required.
2. Water Supplies Department	
(a) Existing water mains inside the proposed site as shown in the main record plan may be affected (Attachment 2). The applicant is required to either divert or protect the water mains found on-site.	(a) The Application Site has been set back at least 1.5m from the existing water mains (see Figure 4B). All the proposed works should not disturb, interfere with or cause damage to existing water mains.

COMMENTS	RESPONSES
<p>(b) If diversion is required, existing water mains inside the proposed site areas are needed to be diverted outside the site boundary of the Site to lie in Government land (GL). A strip of land of minimum 1.5m in width should be provided for the diversion of existing water mains. The cost of diversion of existing water mains upon request will have to be borne by the applicant; and the applicant shall submit all the relevant proposal to the Water Supplies Department (WSD) for consideration and agreement before the works commence.</p>	<p>(b) As the Application Site has been set back, no diversion is required.</p>
<p>(c) No structures shall be built or materials stored within 1.5 metres from the centre line(s) of water main(s) shown on the plan. Free access shall be made available at all times for staff of the Director of Water Supplies or their contractor to carry out construction, inspection, operation, maintenance and repair works.</p>	<p>(c) Noted.</p>
<p>(d) No trees or shrubs with penetrating roots may be planted within the Water Works Reserve or in the vicinity of the water main(s) shown on the plan. No change of existing site condition may be undertaken within the aforesaid area without the prior agreement of the Director of Water Supplies. Rigid root barriers may be required if the clear distance between the proposed tree and the pipe is 2.5m or</p>	<p>(d) Noted.</p>

COMMENTS	RESPONSES
less, and the barrier must extend below the invert level of the pipe.	
(e) No planting or obstruction of any kind except turfing shall be permitted within the space of 1.5 metres around the cover of any valve or within a distance of 1m from any hydrant outlet.	(e) Noted.
(f) Tree planting may be prohibited in the event that the Director of Water Supplies considers that there is any likelihood of damage being caused to water mains.	(f) Noted.
3. Electrical and Mechanical Services Department	
<p><u>Electricity Safety</u></p> <p>(a) In the interests of public safety and ensuring the continuity of electricity supply, the parties concerned with planning, designing, organising and supervising any activity near the underground cable under the mentioned document should approach the electricity supplier (i.e. CLP Power) for the requisition of cable plans to find out whether there is any underground cable within and/or in the vicinity of the concerned site. The applicant should also be reminded to observe the Electricity Supply Lines (Protection) Regulation and the “Code of Practice on Working near Electricity Supply Lines” established under the Regulation</p>	(a) Noted.

COMMENTS	RESPONSES
when carrying out works in the vicinity of the electricity supply lines.	
<p><u>Town Gas Safety</u></p> <p>(b) Please be informed that there are underground intermediate pressure town gas pipes running along Sha Tau Kok Road that are close to the Site.</p>	(b) Noted.
<p>(c) If there is any works to be involved in the process, in particular any works involving excavation, the relevant parties shall liaise with The Hong Kong and China Gas Company Limited in respect of the exact locations of existing or planned gas pipes/gas installations in the vicinity of the works site and any required minimum set back distance away from them during any works.</p>	(c) Noted.
<p>(d) The relevant parties are required to observe the Electrical and Mechanical Services Department's requirements on the "Avoidance of Damage to Gas Pipes 2nd Edition" for reference</p> <p>https://www.emsd.gov.hk/filemanager/en/content_286/CoP_gas_pipes_2nd_(Eng.pdf).</p>	(d) Noted.
<p>4. Urban Design & Landscape Section of Planning Department</p>	
<p>(a) With reference to paragraph 4.5 of the Planning Statement, "wild overgrown and some weeds trees are found at the vacant land. No old and valuable trees are found within the Site. The applicant will remove the weed trees within the</p>	<p>(a) With the setbacks to the application boundary, there are 6 existing trees within the application site (see Figure 6 attached). While all of the existing trees (T1 to T6) are of a common species, not old or valuable, in fair or poor condition, and in conflict with the proposed waiting spaces, it is</p>

COMMENTS	RESPONSES
<p>Site”. However, it is stated Part 6(A)(e)(iii) of Application Form No. S16-III that no tree felling will be involved. No further information on existing trees and proposed tree treatment is provided. The applicant should provide the relevant information on trees within the Site and advise the proposed tree treatment with associated mitigation measures, if any.</p>	<p>proposed to fell these trees. The Applicant will not disturb any trees outside the Application Site.</p>
<p>5. Transport Department</p>	
<p>Please find the following comments from traffic engineering point of view.</p> <p><u>Planning Statement</u></p> <p>(a) Section 4.1: In view of the necessity of roadside pick-up/drop-off activity or loading/unloading activity, queuing on public road would not be allowed. Please advise any queue management measures (e.g. site staff) would be deployed to ensure there would be no queue on public road. The applicant should also clarify whether there are any gates at the access of the application site (the Site). Such gate may cause queuing of traffic outside the Site.</p>	<p>(a) No roadside pick-up or drop-off activity, or loading or unloading activity, will be permitted outside or within the Application Site during operation. A mobile application for electric taxi drivers will be adopted to provide real-time information on the availability of EV charging and waiting spaces at EV charging stations managed by the Applicant. An electronic gate will be installed at the entrance of the Application Site and only pre-booked vehicles will be allowed to access the site.</p>
<p>(b) Section 4.2: Please advise whether any real-time display board will be set up on the Site to deter vehicles from entering the Site if not pre-booked.</p>	<p>(b) Only pre-booked vehicles will be allowed to access the site and a mobile application for electric taxi drivers will provide real-time information on the availability of EV charging and waiting spaces. As such, no real-time display board will be set up on the site.</p>

COMMENTS	RESPONSES
(c) Section 4.3: Please advise measures to prohibit early-entry or late-departure of vehicles for charging and measures in case the waiting spaces are full.	(c) An electronic gate will be installed at the entrance of the Application Site to prevent early access to the Application Site. Vehicles departing late will incur an additional charge. Charging arrangements and regulations will be communicated to all taxi drivers in advance.
(d) Drawing No. FIGURE 4: The applicant should provide the dimensions of driveways inside Site.	(d) Please refer to Figure 4A attached.
(e) The vehicular access between the Site and Sha Tau Kok Road – Lung Yeuk Tau is not managed by the Transport Department. The applicant should seek comments from the responsible party.	(e) Noted.
(f) <u>Traffic Impact Assessment</u> Paragraph 4.2.3: The applicant should advise whether there are any arrangement/message to remind taxi drivers to leave charging spaces on time after charging and not to arrive too early at the Site before charging. The applicant should also advise whether any penalty will be taken for not leaving the charging spaces on time.	(f) An electronic gate will be installed at the entrance of the Application Site to prevent early access to the Application Site. Vehicles departing late will incur an additional charge. Charging arrangements and regulations will be communicated to all taxi drivers in advance.
(g) Table 4.5: The 2028 traffic volume in design scenario and the 2028 v/c ratio in reference scenario are not in order. The applicant should review accordingly.	(g) Noted. Table 4.5 is reviewed and updated accordingly. Please refer to the updated TIA report (see Annex B) for details.
(h) Drawing nos. FIGURE 3.2, FIGURE 4.1, FIGURE 4.2 and FIGURE 4.3: The applicant should supplement the	(h) Noted. Figures 3.2, 4.1, 4.2 and 4.3 are updated to show the breakdown of flows for each traffic direction at each arm of the junctions. Please refer to the updated TIA report for details.

COMMENTS	RESPONSES
breakdown of traffic flow from each arm of the junctions to each direction.	
(i) Drawing No. SP-01: The vehicle should not encroach onto the lateral side of the proposed run-in/out. The applicant should review accordingly.	(i) Noted. Figure SP-01 is reviewed and updated accordingly. Please refer to the updated TIA report for details.
6. Environment and Ecological Bureau	
(a) The applicant has stated in the Planning Statement that the 12 spaces will be equipped with DC chargers with an output power of 120kW. Please advise whether each of the 12 spaces could be provided with at least 120kW EV charging simultaneously (i.e. when all 12 EV charging spaces are occupied by EVs and are re-charging at the same time, each of the 12 EV charging spaces could still be provided with at least 120kW EV charging).	(a) It is confirmed that each of the charging spaces could be provided with at least 120kW EV charging simultaneously.
(b) To echo with the latest version of Ch.8 of HKPSG about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the applicant is suggested to comply with the relevant requirement of HKPSG, i.e., EV chargers with output power of not less than 7kW (i.e. medium chargers) should be installed in all parking spaces for private cars, light goods vehicles and motorcycles of the subject site.	(b) The application complies with the latest version of Ch. 8 of HKPSG that all EV charging spaces are equipped with EV chargers with an output power of 120kW (i.e. not less than 7kW).

COMMENTS	RESPONSES
7. Sha Tin, Tai Po and North District Planning Office, Planning Department	
(a) Please note that the Town Planning Board Secretariat has received a number of public comments on the application. The applicant shall take note and response to those public comments as appropriate. The public comments are available at the Planning Enquiry Counters of the Planning Department (i.e. 17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong or 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories)	(a) See our responses to public comments below (Section 8).
(b) Noting that the site of a similar application No. A/NE-TKL/813 is located to the north across Sha Tau Kok Road – Lung Yeuk Tau, the applicant shall clarify whether the cumulative impact of the proposed use at both sites has already been taken into consideration in the relevant assessment(s) in the submission. The applicant is advised to consult the relevant departments, such as the Transport Department, in this regard.	(b) The cumulative impact on traffic aspect has been included in the revised Traffic Impact Assessment (see Section 4.3 of the revised TIA report) while the differing locations for rainwater discharge of the 2 planning applications will not affect drainage arrangements.
8. Public Comments	
(a) One public comment from an individual expresses concern over the provision of electric charging facilities at the Site.	(a) The Applicant intends to convert the Application Site into an EV charging station to provide EV charging facilities for electric taxis of a taxi fleet. It aligns with the Government's initiative to expand the charging network for EVs in Hong Kong. The proposed charging station is cleaner, more

COMMENTS	RESPONSES
	environmental friendly and do not generate adverse noise, air pollution and visual intrusion compared to the existing vehicle repair workshop.
(b) One public comment from an indigenous village of Kwan Tei Tsuen expresses concerns over the existing shrines within the Application Site.	(b) The shrines are currently located on private land and were established by the previous owner of the lot. Worship activities have diminished significantly in recent years. The current owner and the Applicant have agreed to consult with the villagers during the development process and to make appropriate arrangements for relocating the shrines. The support letter from the village representative of Kwan Tei Tsuen are attached for your reference (see Annex C).
9. Lands Department	
(a) The application site (the Site) comprises Government land (GL) and Old Schedule Agricultural Lot held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through GL but no right of access via GL is granted to the Site at the proposed ingress/egress point.	(a) The existing ingress/egress point, which has been used by the occupier for over 35 years, will be retained in the proposed development.
(b) Part of the application lot and the adjoining GL, not covered by the planning application is covered by Short Term Waiver (STW) No. 1226 and Short Term Tenancy (STT) No. 1108 for the purpose of storage of rattan furniture.	(b) Noted. Should this application be approved, the Applicant will apply for STW and STT for the proposed use.

COMMENTS	RESPONSES
(c) The following irregularities covered by the subject planning application have been detected:	(c)
<p><u>Unauthorised structure within the said private lot covered by the planning application</u></p> <p>LandsD has reservation on the planning application since there are unauthorised structures on the private lot which is already subject to lease enforcement actions according to case priority. The lot owner should rectify the lease breaches as demanded by LandsD,</p>	<p>Noted. Should this application be approved, the Applicant will apply for STW for the proposed use. All unauthorized structures on the private lot will be removed by the Applicant.</p>
<p><u>Unlawful occupation of GL with unauthorised structures covered by the planning application</u></p> <p>The GL within the Site (about 570m² as mentioned in the application form) has been fenced off/illegally occupied with unauthorised structures without any permission. Any occupation of GL without Government's prior approval is an offence under the Land (Miscellaneous Provisions) Ordinance (Cap. 28). This office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice.</p>	<p>Noted. Should this application be approved, the Applicant will apply for STT for the proposed use.</p>

COMMENTS	RESPONSES
<p>4. The following irregularities not covered by the subject planning application have been detected:</p>	<p>4.</p>
<p><u>Unauthorised structures within the said private lot not covered by the planning application</u></p> <p>There are unauthorised structures within the said private lot and part of the structure extended to adjoining Lot 453 RP in DD. 83 not covered by the planning application. The lot owners should immediately rectify the lease breaches and this office reserves the rights to take necessary lease enforcement action against the breaches without further notice.</p>	<p>Noted. The unauthorised structures will be removed by the Applicant.</p>
<p><u>Unlawful occupation of GL with unauthorised structures not covered by the planning application</u></p> <p>The GL adjoining the said private lot has been fenced off/illegally occupied with unauthorised structures without permission. The GL being illegally occupied is not included in the application. Any occupation of GL without Government's prior approval is an offence under Cap. 28. Please clarify the extent of the Site. This office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice.</p>	<p>Noted. The unauthorized structures on GL will be removed by the Applicant.</p>

COMMENTS	RESPONSES
<p>5. The lot owners/applicant shall either (i) remove the unauthorised structures and cease the illegal occupation of the GL not covered by the subject planning application immediately; or (ii) include the unauthorised structures and the adjoining GL being illegal occupied in the subject planning application for further consideration by the relevant departments, subject to the approval of the Town Planning Board to the planning application which shall have reflected the rectification or amendment as aforesaid required, apply to this office for modification/new STW and STT to permit the structures erected/to be erected and the Occupation of GL. The applications for modification/new STW and STT will be considered by the Government in its capacity as a landlord and there is no guarantee that they will be approved. Besides, the STW would be processed on whole lot basis. And the STW and the STT, if approved, will be subject to such terms and conditions including the payment of back-dated waiver fee/rent from the first date when the unauthorised structures were erected and the occupation of GL as well as administrative fee as considered appropriate to be imposed by LandsD. In addition, LandsD reserves the right to take enforcement action against the lot owners for any breach of the lease conditions, including the</p>	<p>5. Noted. The Applicant will remove the unauthorized structures and cease the illegal occupation of the GL not covered by the planning application.</p>

COMMENTS	RESPONSES
<p>breach(es) already in existence or to be detected at any point of time in future and land control action for any unlawful occupation of GL. Besides, given the proposed use is temporary in nature, only erection of temporary structure(s) will be considered.</p>	
<p>6. Unless and until the unauthorised structures and the unlawful occupation of GL are duly rectified by the lot owners/applicant or entirely included in the subject planning application, please take it as this office's objection to the application which must be brought to the attention of the Town Planning Board when they consider the application.</p>	<p>6. Noted.</p>

Figures

Figure 1A	Location Plan
Figure 2A	Lot Index Plan
Figure 3A	Vehicular Access to the Site
Figure 4A	Layout Plan
Figure 4B	Layout Plan showing the Setback Distance
Figure 6	Existing Trees within the Application Site

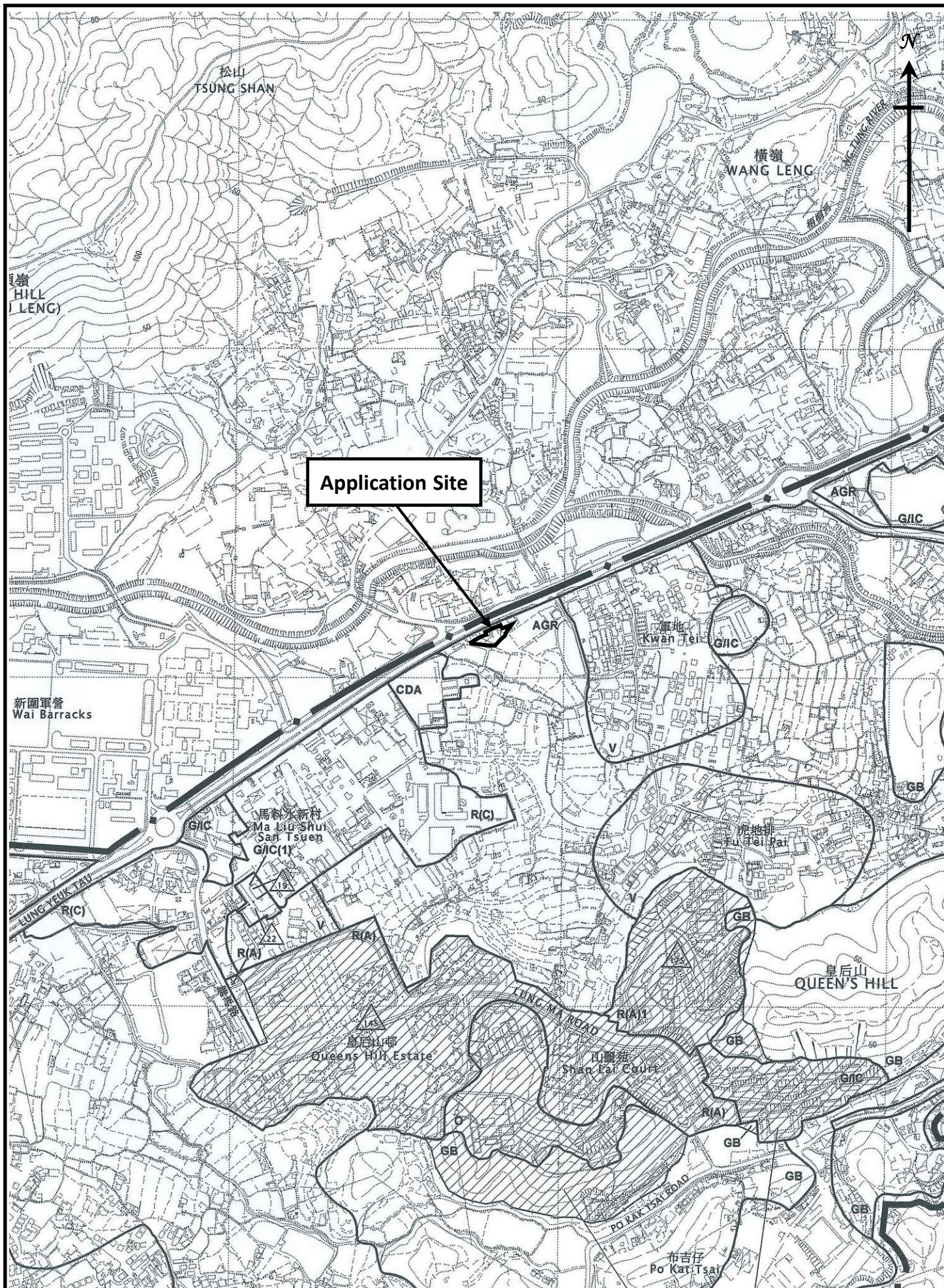


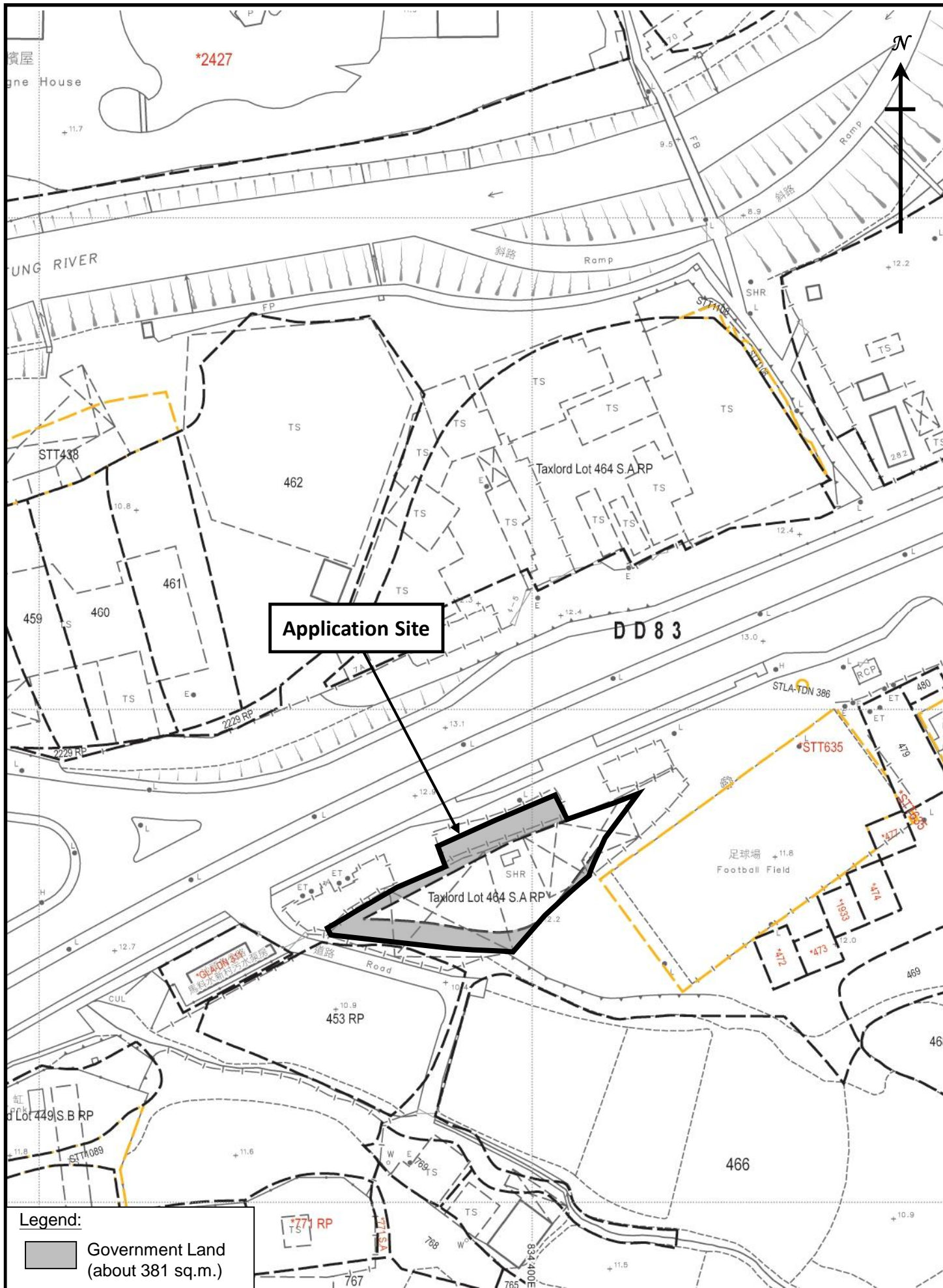
Figure 1A

1 : 7500



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Legend:



Government Land
(about 381 sq.m.)

Lot Index Plan

Figure 2A

1 : 1000



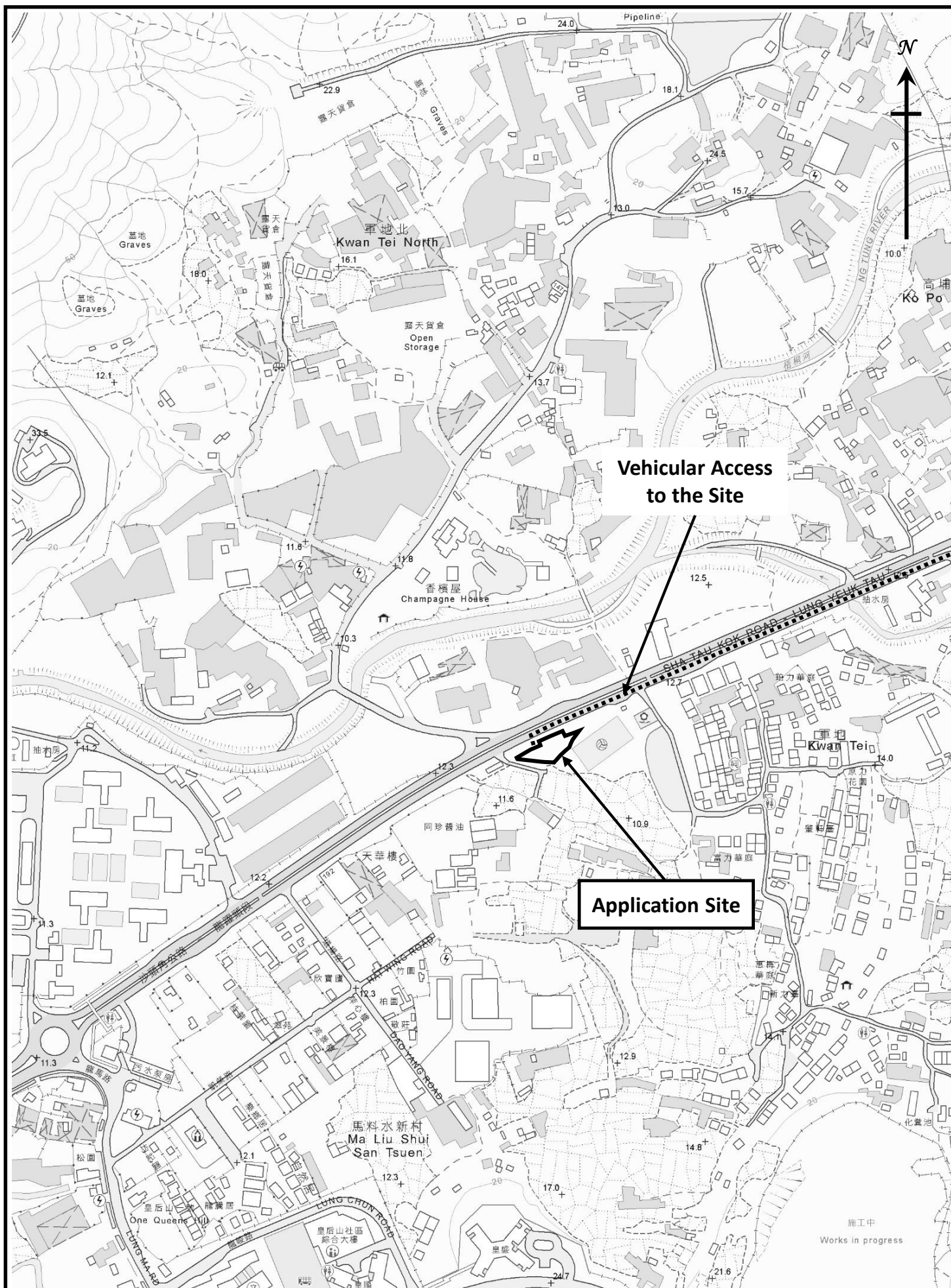
羅迅測計師行

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Vehicular Access to the Application Site

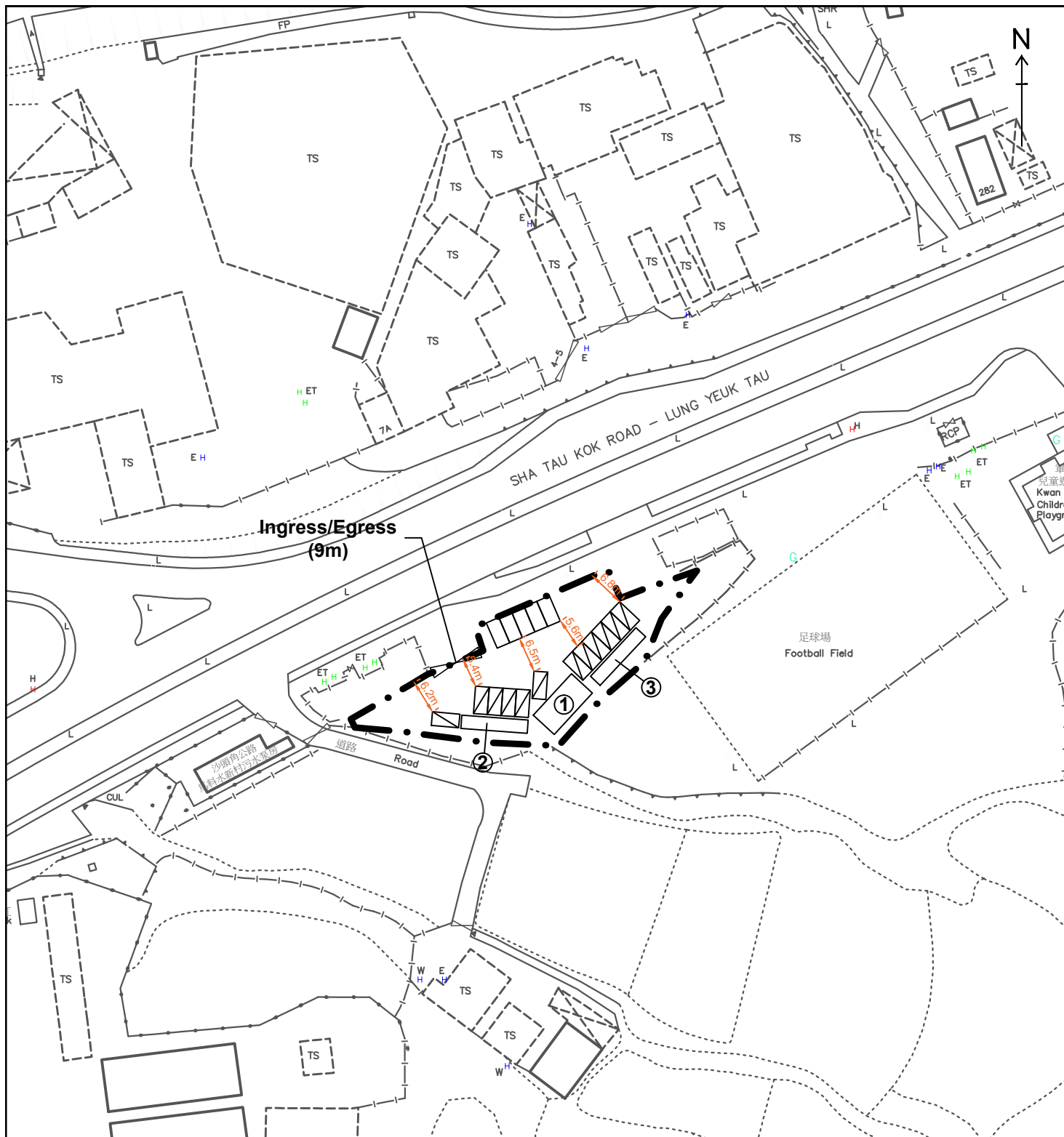
Figure 3A

1 : 5000



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Estate Agency Licence No. C-006328



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- • — Application Site (Area: about 954m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)

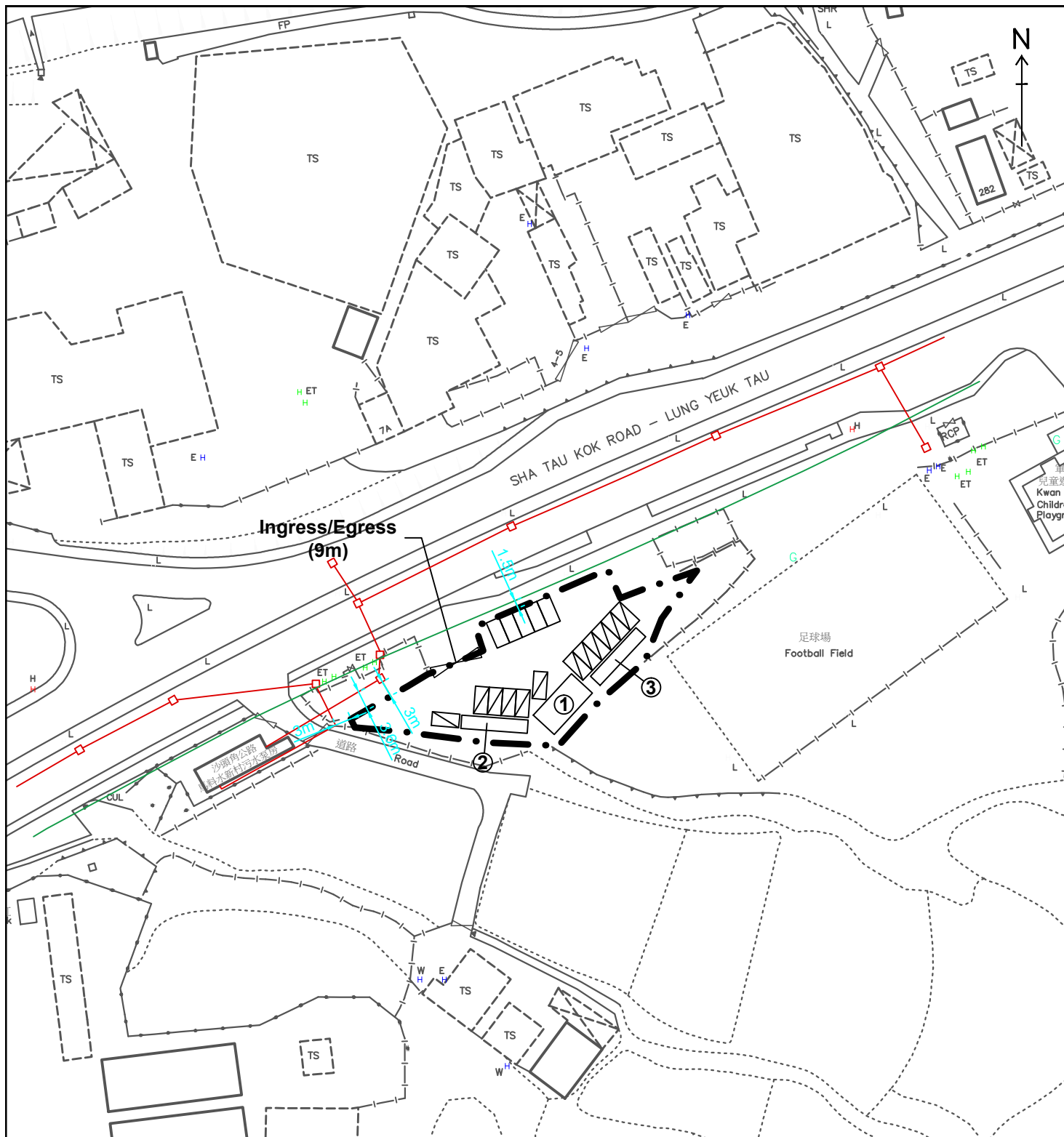
Layout Plan

Figure 4A

Scale 1:1000



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 Estate Agency Licence No. C-006328



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- ● ● Application Site (Area: about 954m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)
- Sewerage Underground Pipes and Manholes
- Water Mains

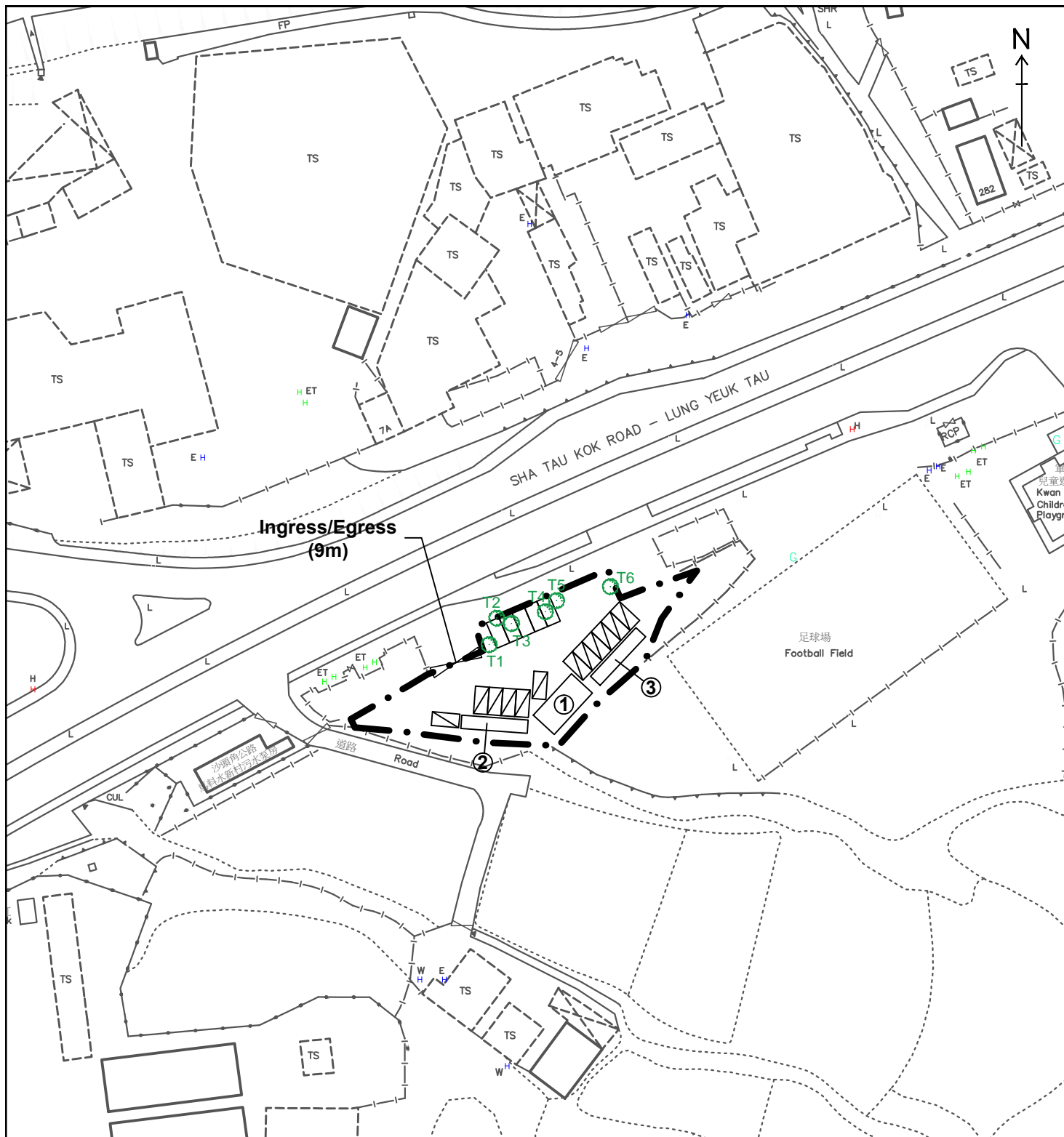
Layout Plan

Figure 4B

Scale 1:1000



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 Estate Agency Licence No. C-006328



Existing Trees within the Application Site to be Felled

Legend:

- Application Site (Area: about 954m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)

T1: *Aleurites moluccana*
 T2: *Bombax ceiba*
 T3: *Aleurites moluccana*
 T4: *Bombax ceiba*
 T5: *Terminalia catappa*
 T6: *Macaranga tanarius*
 Height: about 3m-5m

Existing Trees within the Application Site

Figure 6

Scale 1:1000



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Annex A

Revised Drainage Proposal

Drainage Submission in support of

S16 Planning Application for

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years in “Agriculture” zone

at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories

(HT25040)

October 2025

(Revision A)

Drainage Consultant:

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

Prepared & approved by	LEE Kwok Cheung <i>RPE(Civil)</i>	
------------------------	--------------------------------------	---

1. Background

- 1.1 With respect to a S16 Planning Application for Proposed Temporary Public Vehicle Park with Electric Charging Device for a Period of 3 Years in “Agriculture” zone at Taxlord Lot 464SA RP(Part) in DD83 and adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories, Messrs. Ho Tin & Associates Consulting Engineers Limited was appointed to prepare a drainage submission.

2. Approach to Prepare this Proposal

- 2.1 This Drainage Submission is prepared in line with the “Technical Note to prepare a Drainage Submission (Relating to applications for temporary change of land use such as temporary storage areas, car parks, workshops, small factories ... etc. under S.16 of the Town Planning Ordinance)” issued by Drainage Services Department in December 2024.

3. The Subject Site and Proposed Development

- 3.1 The subject site of approximately triangular shape with a total site area of about 954m² comprises of Taxlord Lot 464SA RP(Part) in DD83 and adjoining Government land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories. The subject site is on the south side of Sha Tau Kok Road – Lung Yeuk Tau and the west of Kwan Tei Children’s Playground. A Site Location Plan is shown in **Figure D1**.
- 3.2 The subject development consists of one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m²), and 2 numbers of container for EV chargers (3m high, total floor area = 29m² each) accompanying with 12 numbers of EV charging spaces for electric taxi and 5 numbers of parking spaces/waiting spaces for electric taxi. A plan showing the proposed site layout is at **Figure D2**.

4. Existing Drainage Conditions of the Site

- 4.1 At present, the subject site is hard paved, partitioned into sub-units and partly used for storage use and car repairing workshop and partly vacant. In front of the northern subject site boundary is a cycle track beside Sha Tau Kok Road – Lung Yeuk Tau. (refer to **Plate 1** to **4**).

- 4.2 The subject site is encroached upon an access road of the adjacent site to its southwest side (refer to **Plate 5** and **6**). To the southeast side of the subject site is an existing hard paved football field (refer to **Plate 7**).
- 4.3 At present, surface runoff of the subject site would flow into an existing watercourse at its southern corner (refer to **Plate 8**) and from which the existing watercourse runs southeastward to join a major existing watercourse of the area which runs to the northwest toward Sha Tau Kok Road – Lung Yeuk Tau (refer to **Plate 9 to 19**).
- 4.4 Current conditions of the subject site and its existing drainage conditions are shown in the following photos (photo taking locations are shown on **Figure D3**):





	
<p>Plate 1 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (1 of 4)</p>	<p>Plate 2 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (2 of 4)</p>
	
<p>Plate 3 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (3 of 4)</p>	<p>Plate 4 – Photo of the northern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (4 of 4)</p>



Plate 5 – Entrance of the adjacent site on the southwest side of the subject site

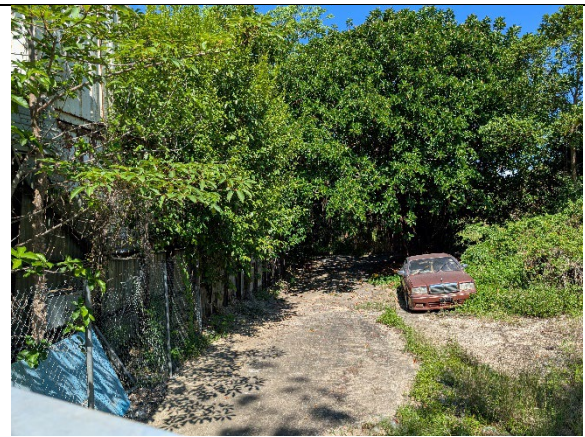


Plate 6 – The adjacent site on the southwest side of the subject site



Plate 7 – View to the subject site from the existing football field on the southeast side of the subject site

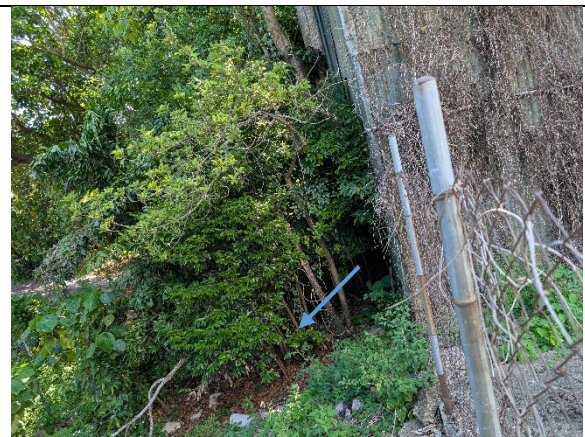


Plate 8 – Existing discharge point of the subject site at the southern corner discharging into an existing watercourse



Plate 9 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (1)



Plate 10 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (2)



Plate 11 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (3)



Plate 12 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (4)



Plate 13 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (5)







Plate 14 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (6)

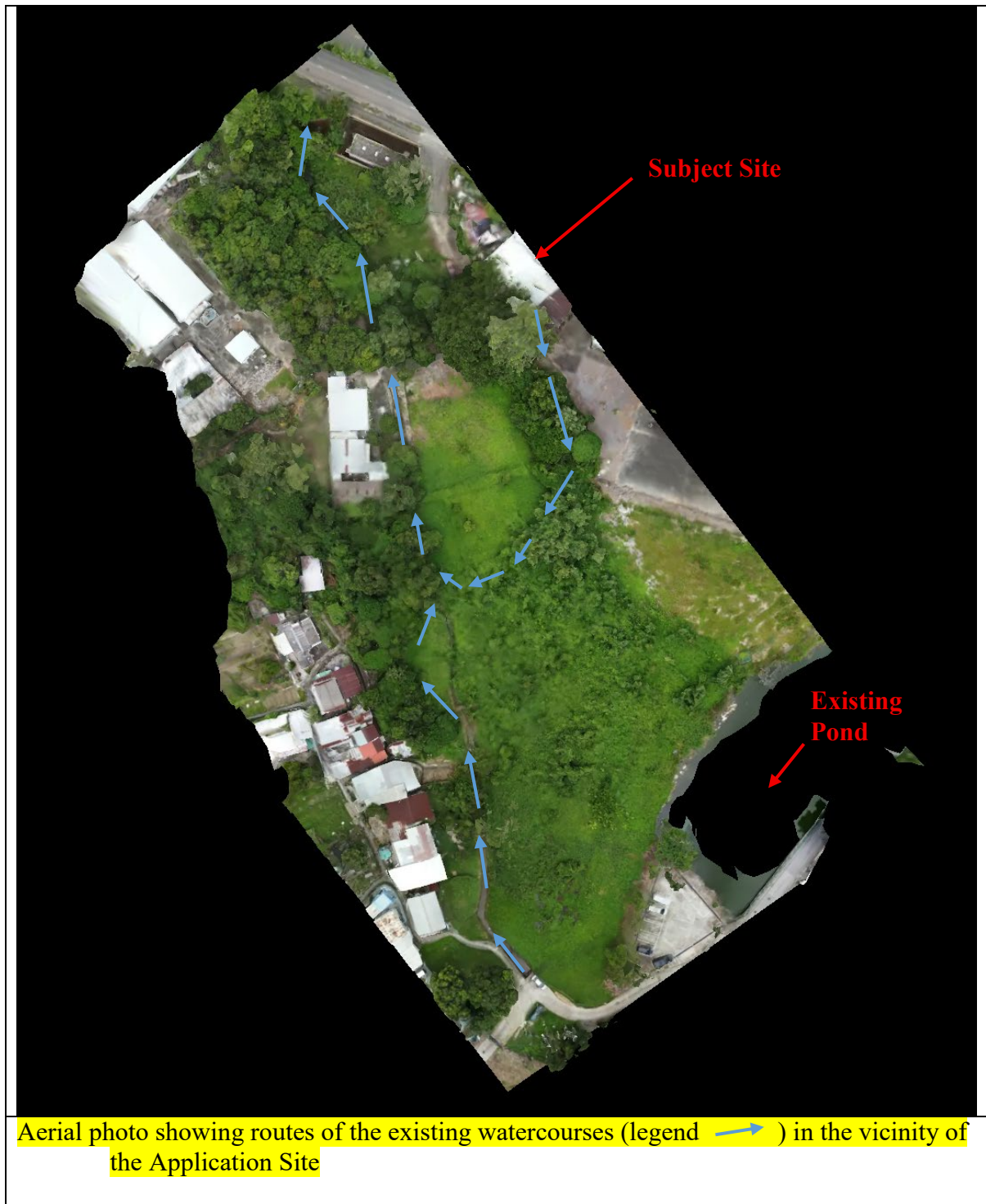


Plate 15 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (7)



Plate 16 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (8)

	
Plate 17 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (9)	Plate 18 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (10)
	
Plate 19 – Existing watercourse at the further downstream of the southern corner of the subject site to which surface runoff would flow into (11)	Extract of the Drainage Records from LandsD's GeoInfo Map in May 2025



5. Drainage Assessment and Proposal

- 5.1 The subject site is a simple small site with a total site area of about 954m² (very much less than 1 ha in size). The existing subject site levels are generally same as those of the area close to Sha Tau Kok Road – Lung Yeuk Tau and relatively higher than the surroundings at the remaining three sides. There is no prominent existing surface

channels intercepting surface runoff on the area. In general, at present surface runoff of the area would flow toward the existing watercourses with respect to the ground levels. The subject site is obviously not the relatively lower spot of the concerned area. Hence, it is not anticipated that the subject site would receive noticeable runoff from the surroundings.

- 5.2 The existing site levels would be maintained and no site formation/leveling works would be carried out such that the subject proposed development would be maintained at levels relatively higher than its adjacent developments/areas to its east, west and north, and would be at similar levels of the existing cycle track running along its northern boundary. For conservative, in order to ensure no surface runoff from the surroundings, due to any unexpected reasons, flowing onto the subject site to be obstructed, 100mm high gap will be formed at the bottom of the security hoarding/fence wall along the subject site boundary. Cross sections of the proposed subject development and typical details of the gap at the bottom of proposed security hoarding/fence are shown in **Figure D5**.
- 5.3 With respect to the “Technical Note to prepare a Drainage Submission” (the “TN”) published by Drainage Services Department, for the subject site area = $954\text{m}^2 \leq 1,800\text{m}^2$, peripheral 375mm U channel at 1 in 200 gradient would be appropriate. It is therefore proposed to construct new peripheral 375mm U channel at 1 in 200 gradient for the subject development. A new catchpit with trap will be constructed at the end of the proposed 375 U channels at the southern corner of the subject site before discharging into the existing watercourse. The discharge point is basically same as that of the existing. A Proposed Stormwater Drainage Layout Plan is shown in **Figure D4**.
- 5.4 Besides, the Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the drainage system to the satisfaction of relevant Government departments.
- 5.5 Details of proposed drainage provisions shall follow relevant details shown in Government departments’ Standard Drawings as follows:

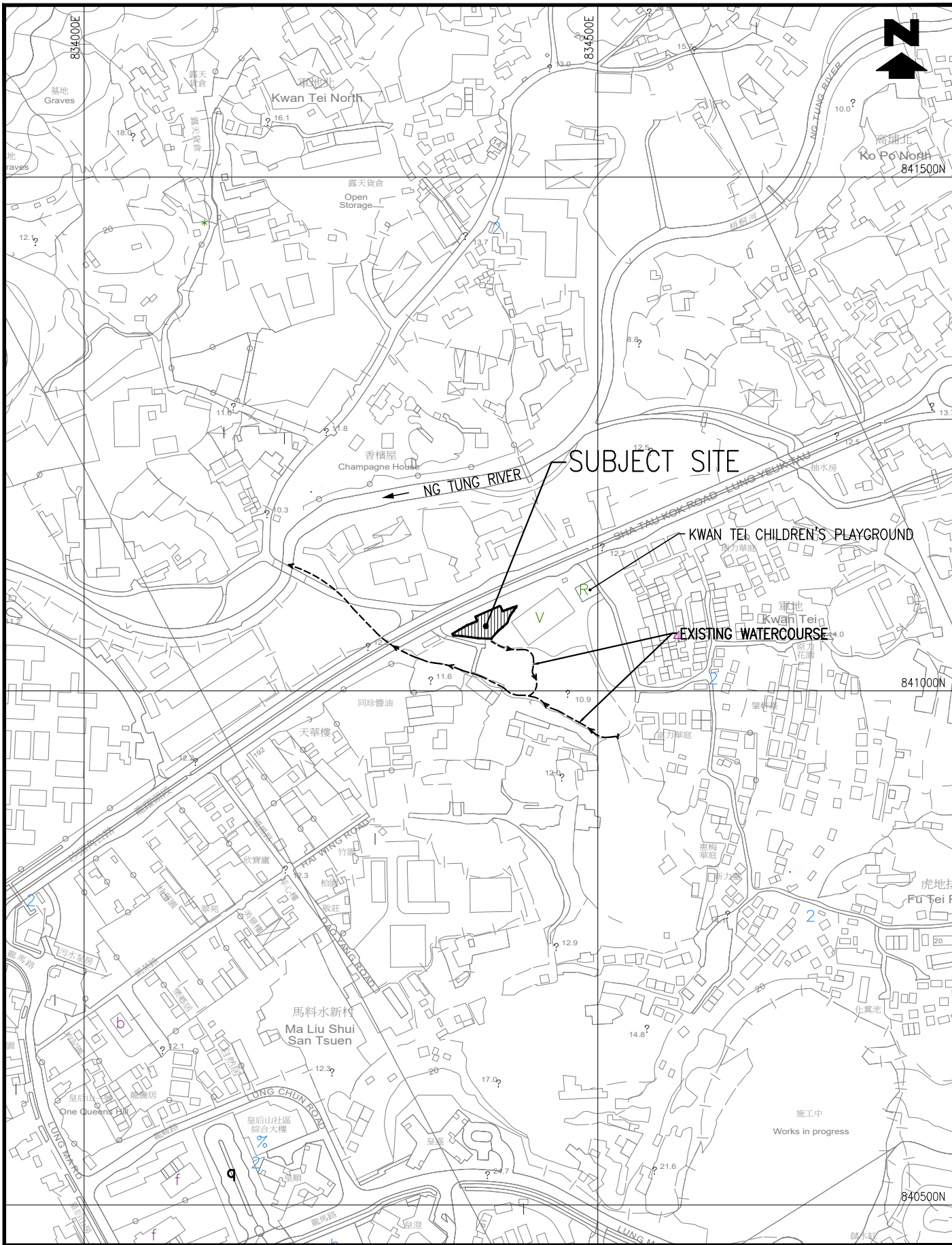
<i>Proposed Drainage Provisions</i>	<i>Standard Drawings</i>	<i>Drawing No. & Title</i>
Catchpit	CEDD Standard Drawings	C 2405/1 to /5 – Standard Catchpit Details
Catchpit with trap		C 2406/1 to /2A – Catchpit with Trap
Catchpit precast concrete cover		C 2407B – Precast Concrete Covers for Catchpit and Sand Trap
U-channel		C 2409J – Details of Half-round and U-channels
Channel cover		C 2412E – Cover Slab and Cast Iron Grating for Channels

6. Conclusion and Recommendations

- 6.1 The subject development as Proposed Temporary Public Vehicle Park with Electric Charging Device for a Period of 3 Years. The subject site area has been hard paved for a very long period without complaints on drainage conditions. The existing site levels would be maintained and no site formation/leveling works would be carried out.
- 6.2 At present, the levels of the subject site are relatively higher than/similar to those of the surroundings. In principle, no surface runoff would flow onto the subject site from its surroundings. 375mm U channel at 1 in 200 gradient will be constructed at the peripheral of the subject site to intercept all crossing surface runoff. For conservative, 100mm high gap will be formed at the bottom of the security hoarding/fence along the subject site boundary to ensure no surface runoff from the adjacent, due to any unexpected incidents, to be obstructed. A new catchpit with trap will be constructed at the ends of the 375mm U channel before discharging into the existing watercourse at the southern corner.

- 6.3 The Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the system to the satisfaction of relevant Government departments.
- 6.4 The subject development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works, i.e. debris clearance and damage repair. Therefore, in conclusion, the subject development would not cause any adverse drainage impact onto the area.

H:\25040_LungYeukTau_Site2\DRAWING\25040_FIGD1_01.dwg, 2/10/2025 10:29:54



TITLE

SITE LOCATION PLAN

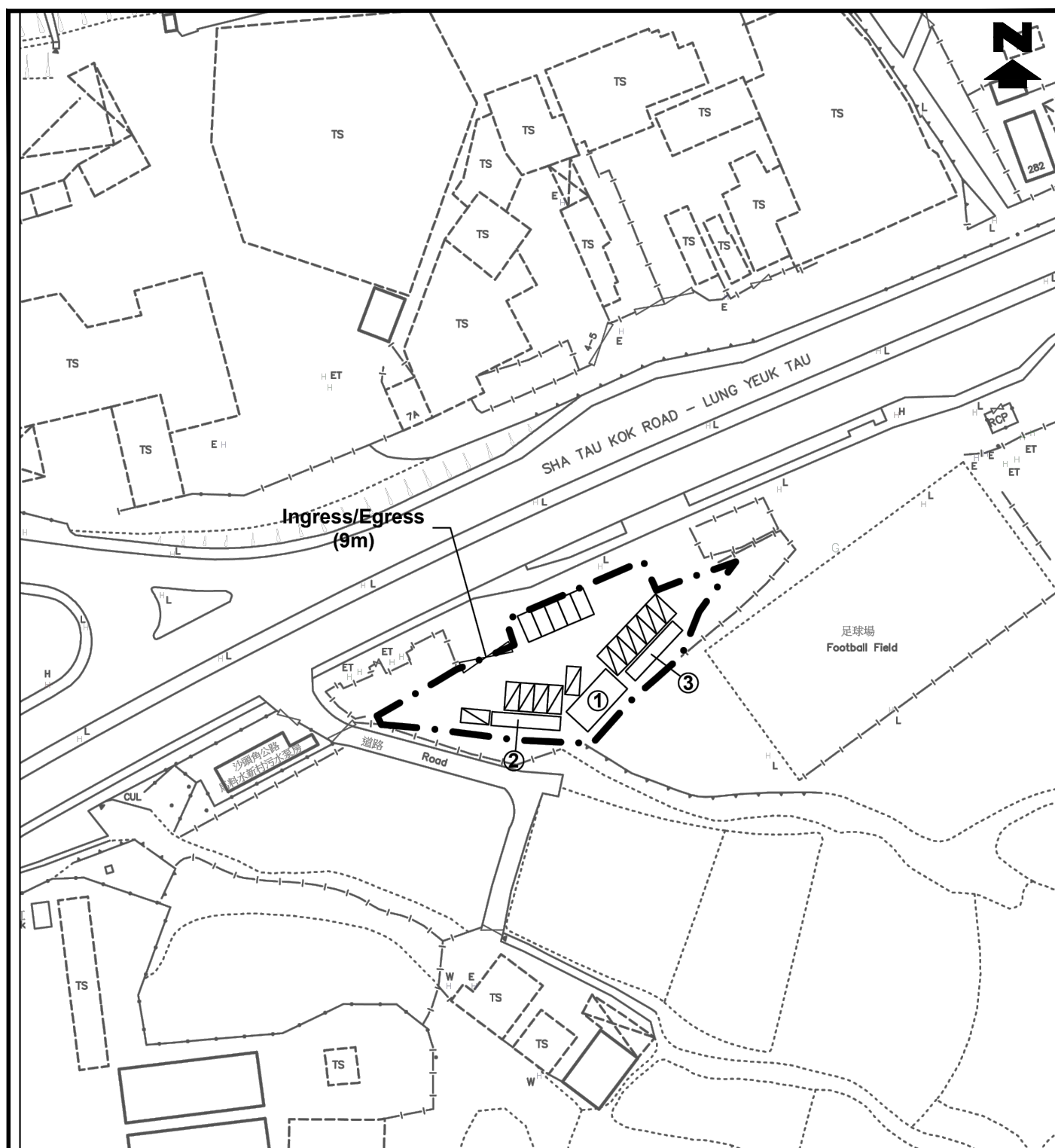
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 5000 - A4




DRAWING No.

FIGURE D1



Structure No.	Uses	No. of Storey	Floor Area (m²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

-  Application Site (Area: about 954m²)
 EV Charging Spaces for Electric Taxi (12 nos.)
 Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)

TITLE

LAYOUT PLAN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

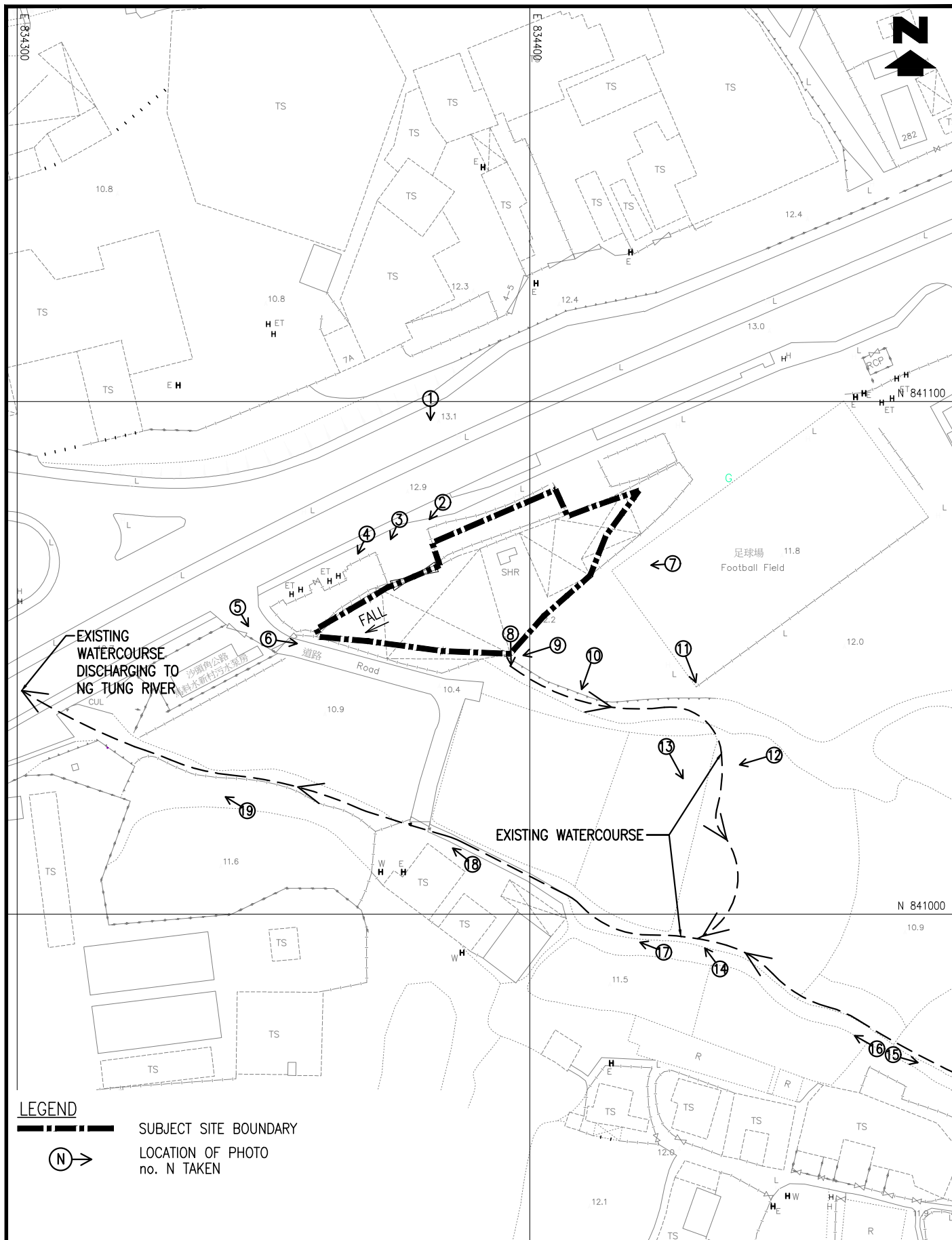
SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D2

H:\25040_LungYeukTau_Site2\DRAWING\25040_FIGD2_01 FIGD3_01 FIGD4_01 & FIGD5_01.dwg, 2/10/2025 10:04:04



TITLE

EXISTING DRAINAGE AND LOCATIONS
OF PHOTO TAKEN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

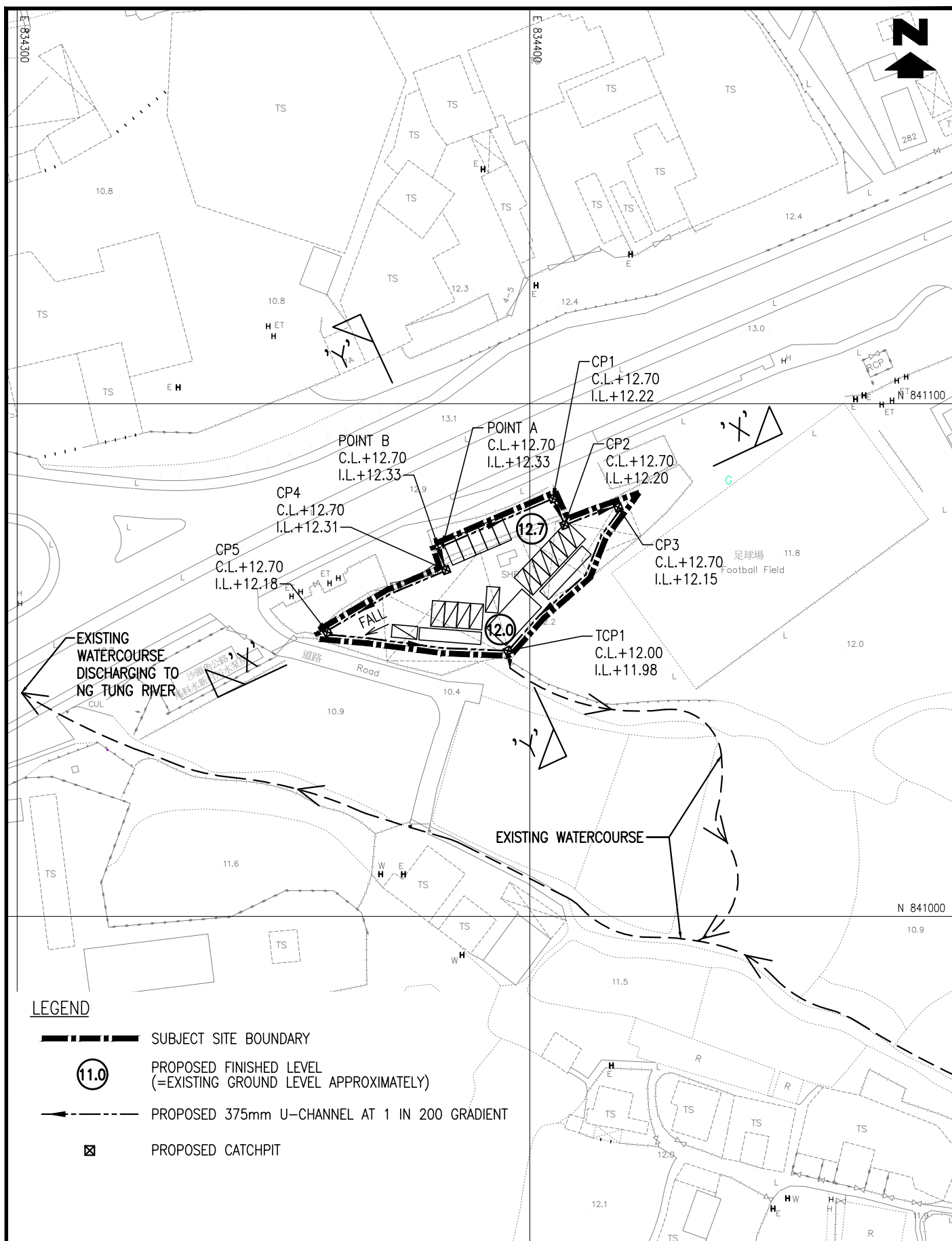
SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D3

H:\25040_LungYeukTau_Site\2\DRAWING\25040_FIGD2_01 FIGD3_01 & FIGD5_01.dwg, 2/10/2025 10:05:46



TITLE

PROPOSED STORMWATER DRAINAGE LAYOUT PLAN

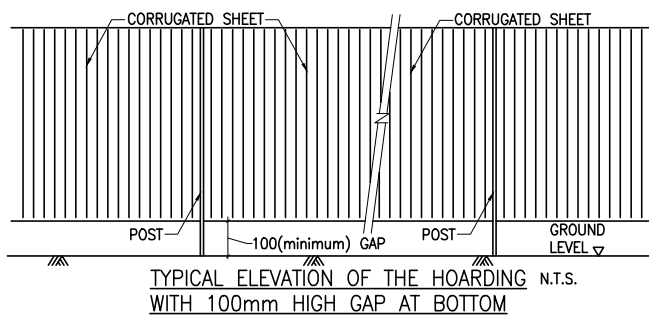
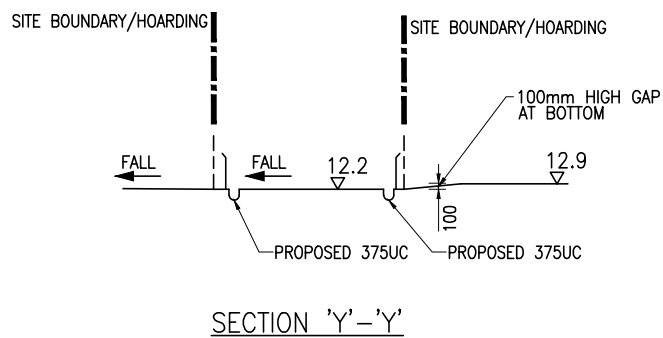
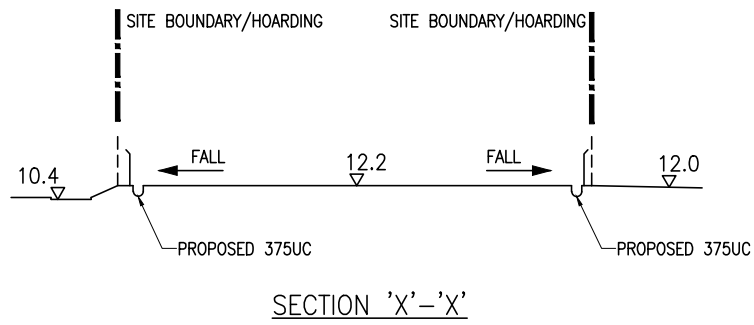
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HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D4



TITLE

SITE CROSS SECTIONS

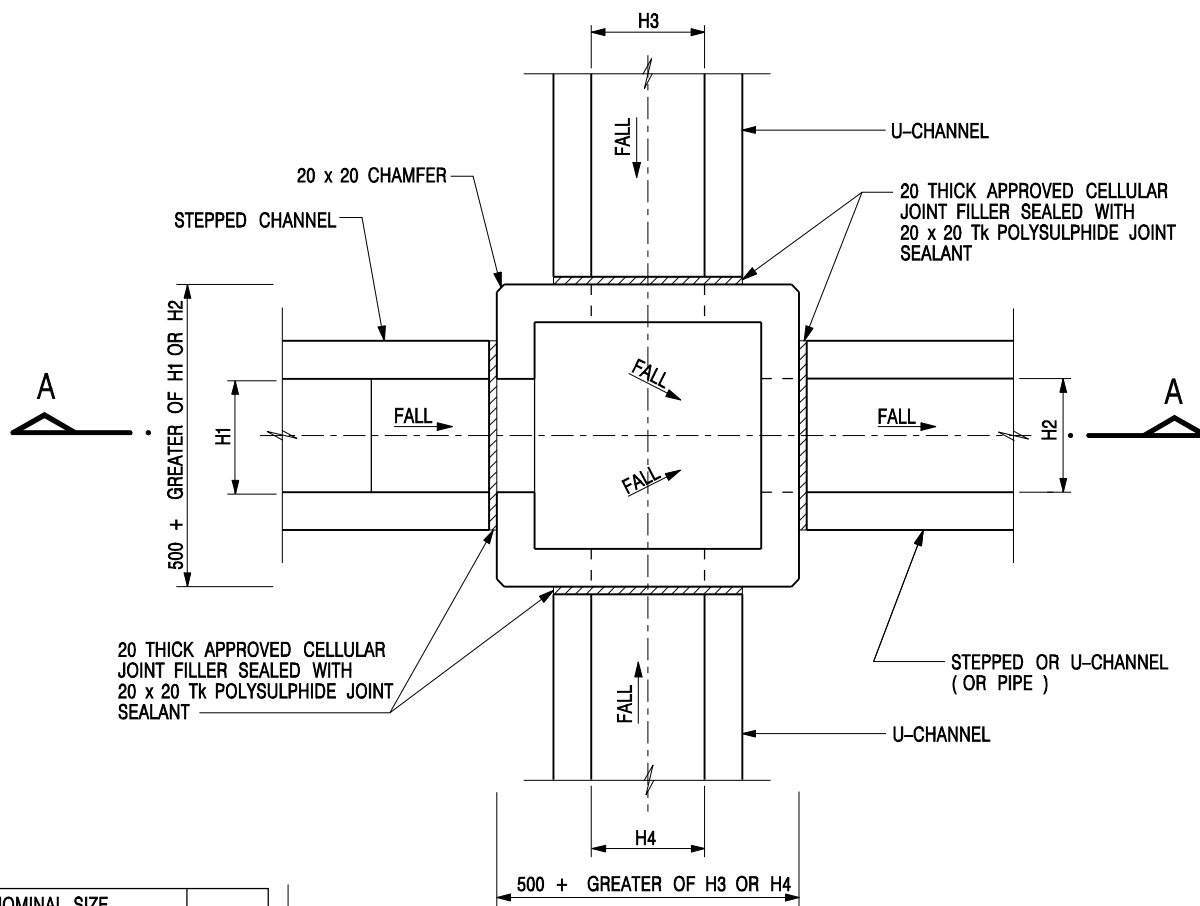
何田顧問工程師有限公司
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SCALE

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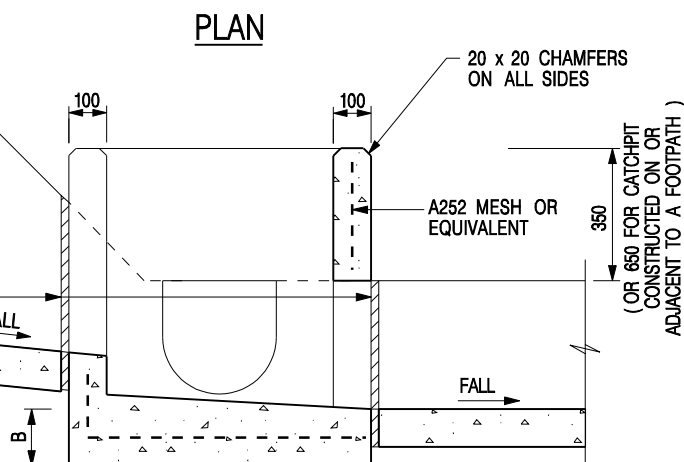
DRAWING No.

FIGURE D5



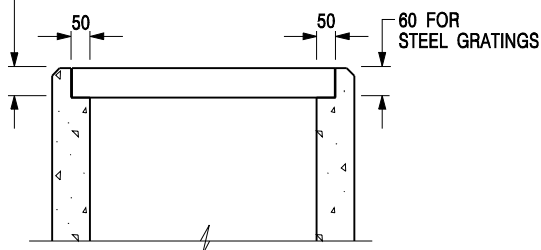
NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT



SECTION A - A

DEPTH OF RECESS AND DETAILS OF PRECAST CONCRETE COVERS (SEE STD. DRG. NO. C2407)




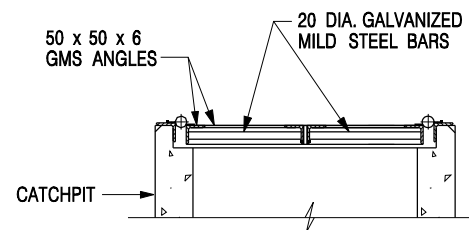
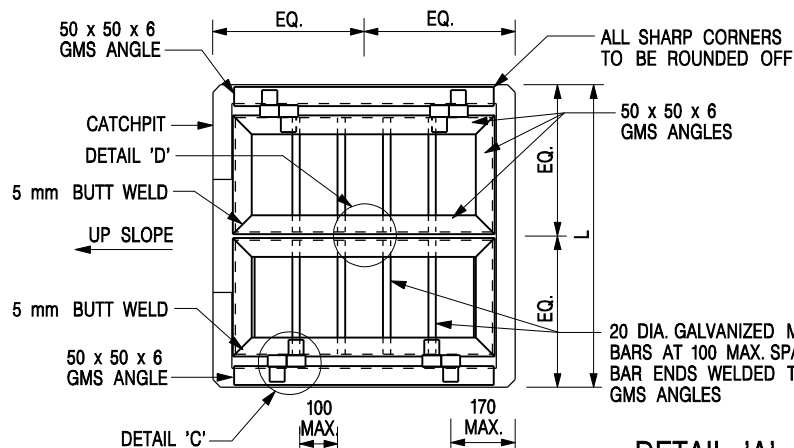
NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

STANDARD CATCHPIT DETAILS (SHEET 1 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE 1 : 20 DATE JAN 1991	
		DRAWING NO. C2405 /1	

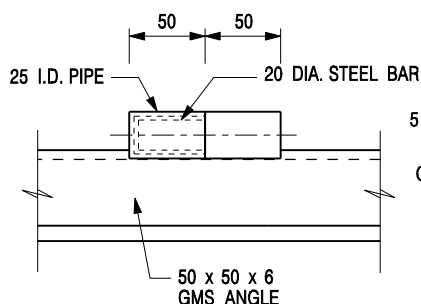


SECTIONAL ELEVATION

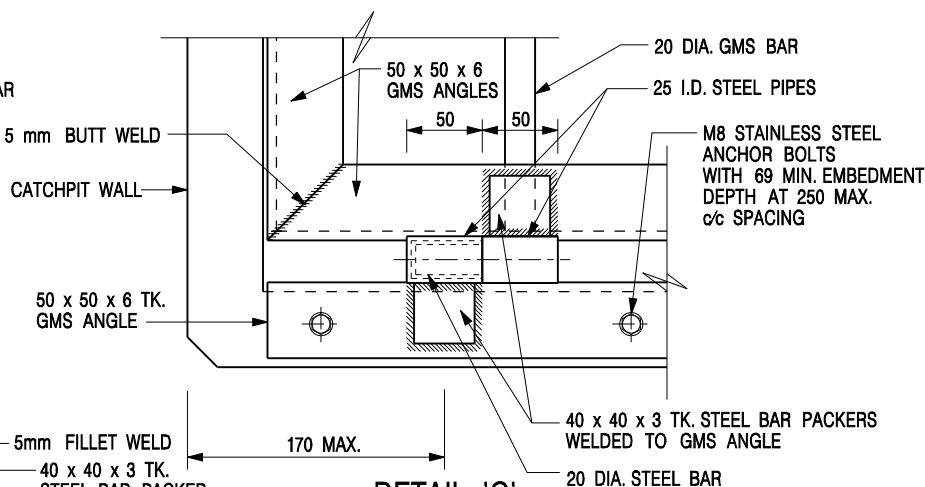
DETAIL 'A'

(DETAILS OF DOUBLE SIDE OPENING STEEL GRATING FOR L > 900mm)

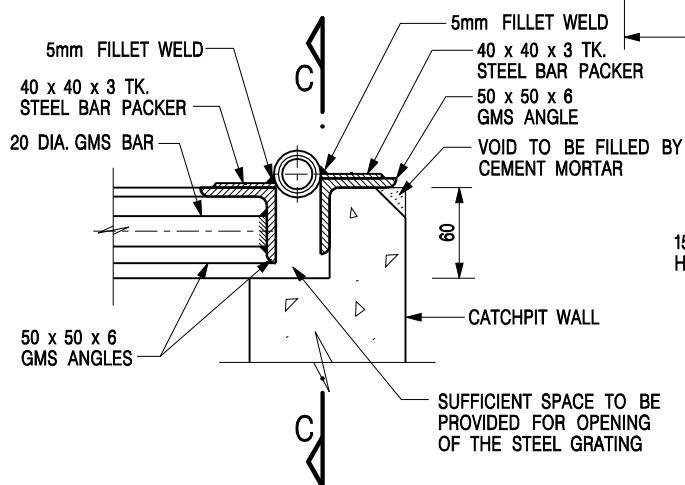
SCALE 1 : 20



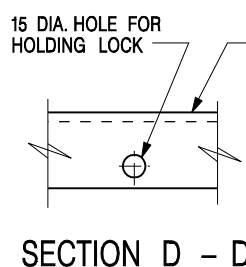
SECTION C - C



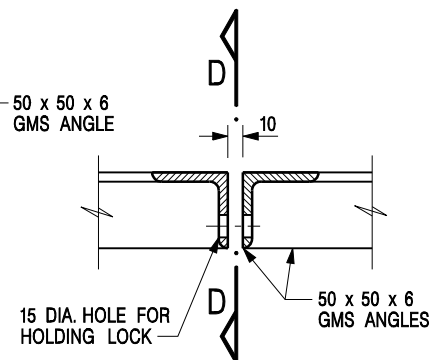
DETAIL 'C'
(DETAILS OF HINGE)
SCALE 1 : 5



SECTIONAL ELEVATION
(DETAIL 'C')



SECTION D - D




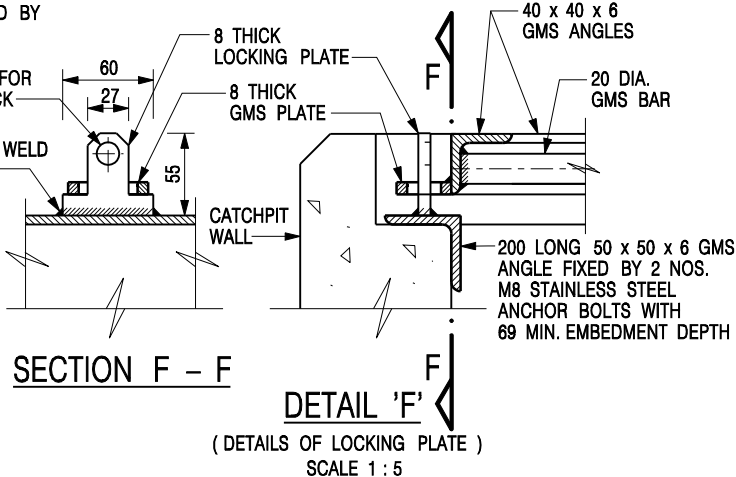
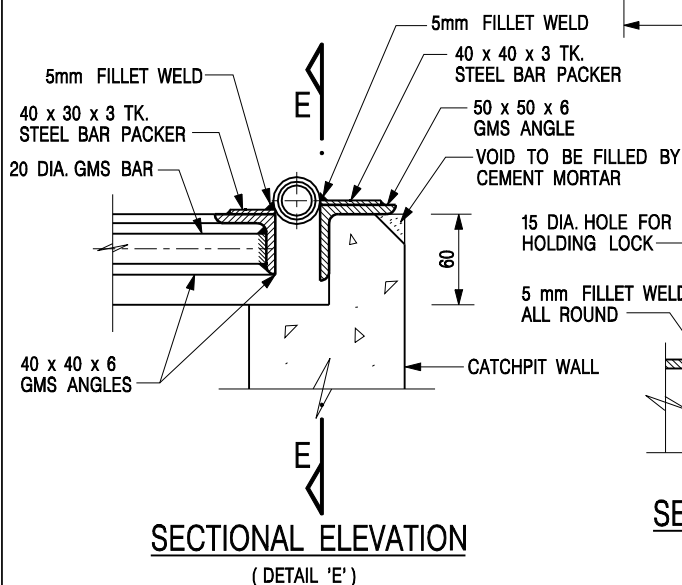
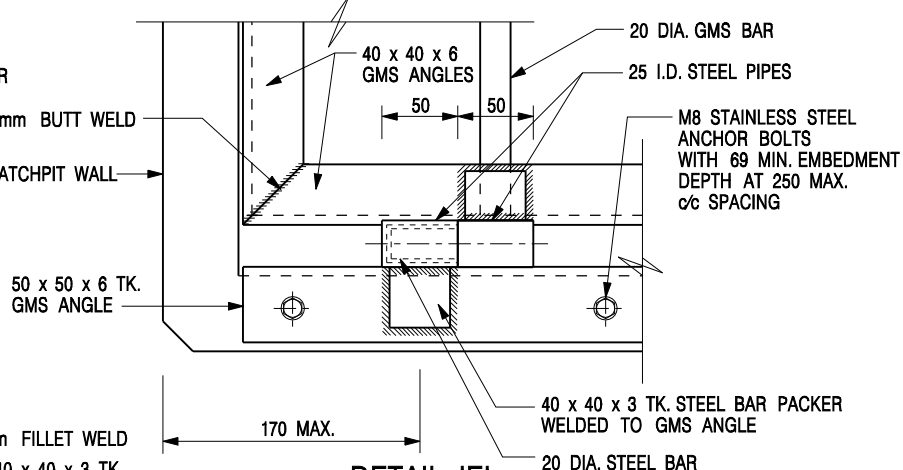
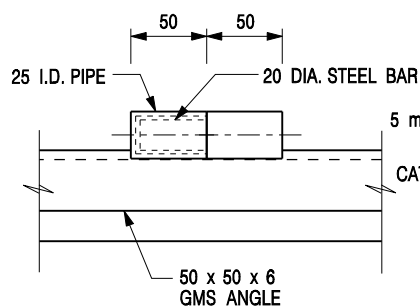
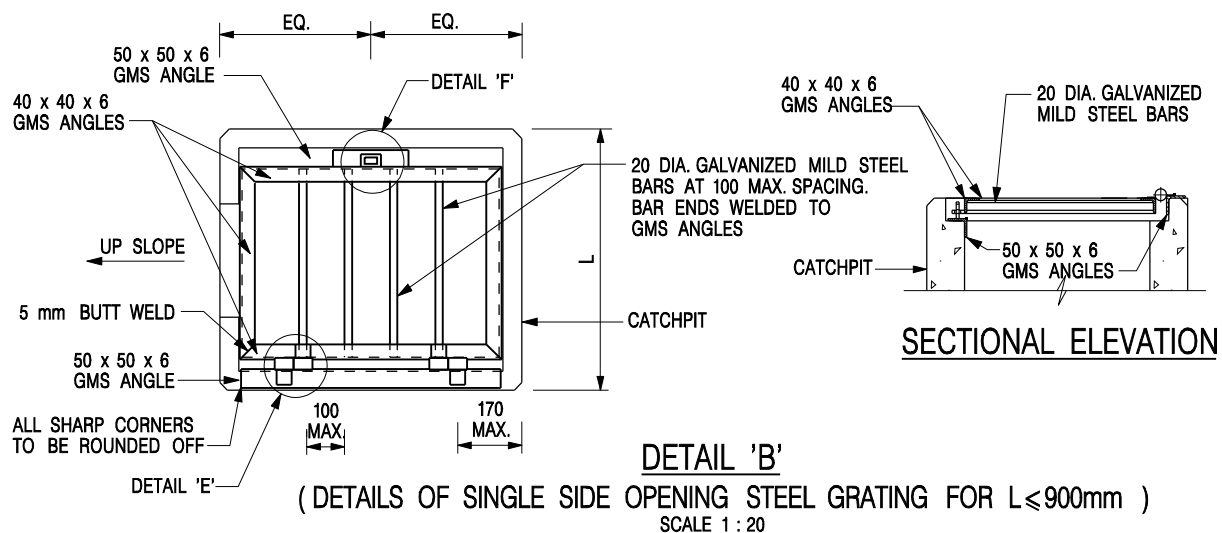
DETAIL 'D'
(DETAILS OF HOLE FOR LOCK)
SCALE 1 : 5

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 2 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /2	




NOTES:

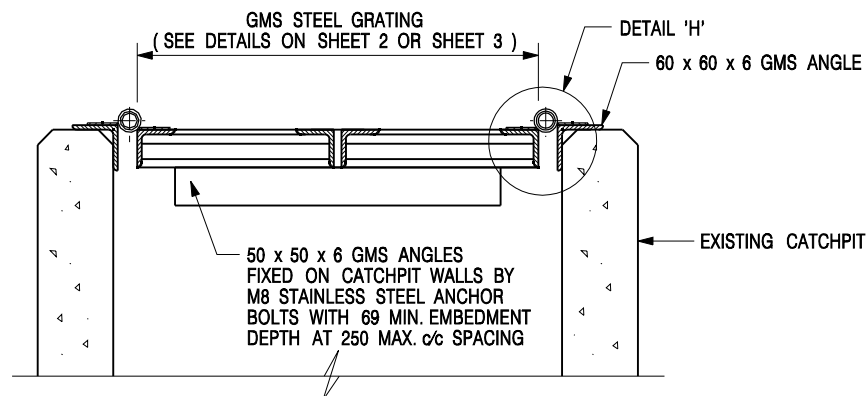
1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS (SHEET 3 OF 5)

卓越工程 建設香港

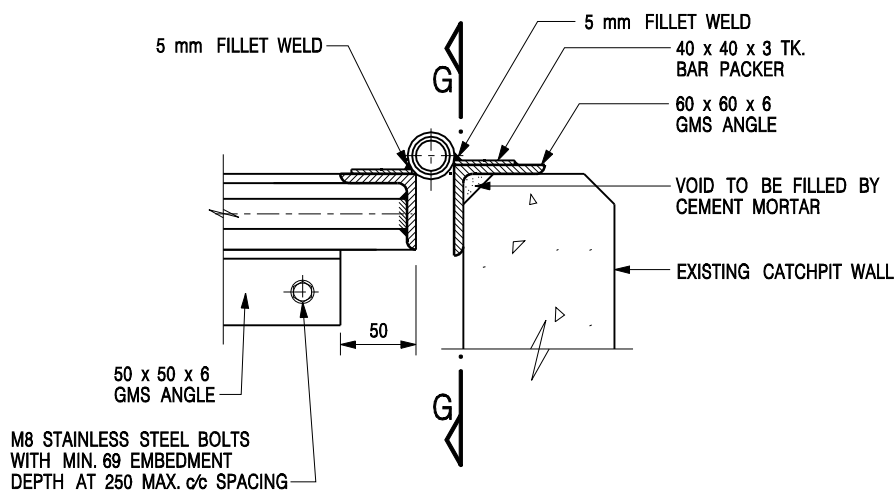
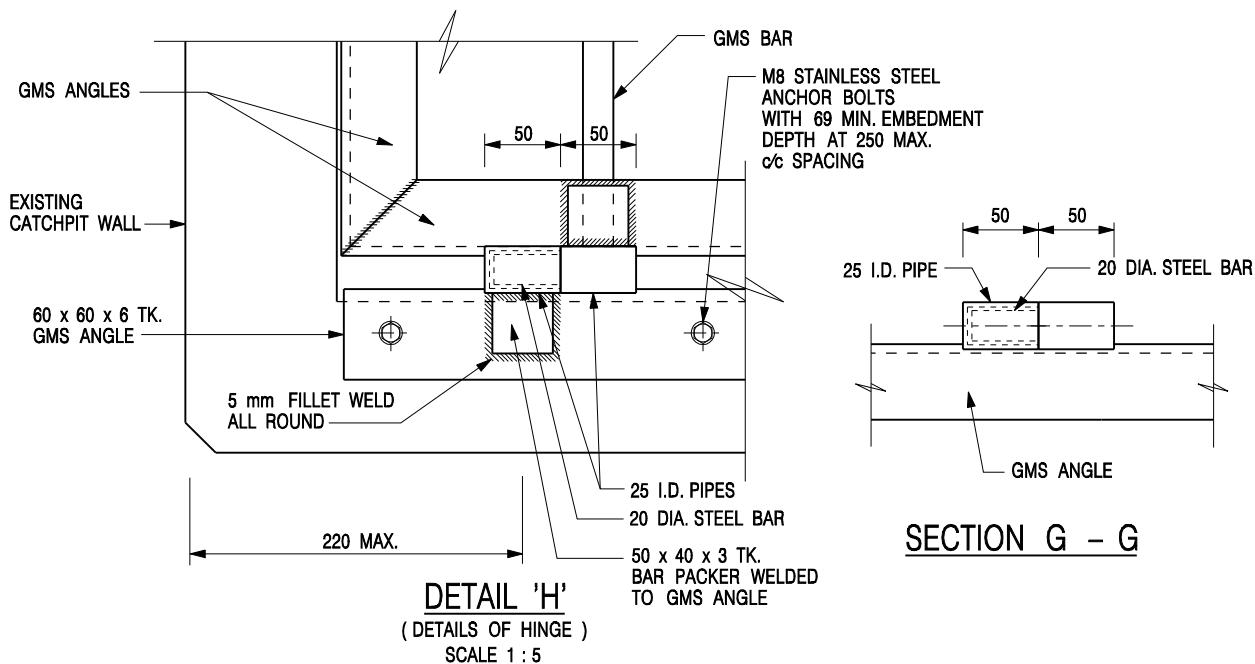
-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /3	

We Engineer Hong Kong's Development



DETAIL 'G' - DETAILS OF STEEL GRATING CONSTRUCTED ON EXISTING CATCHPIT

SCALE 1 : 10




SECTIONAL ELEVATION

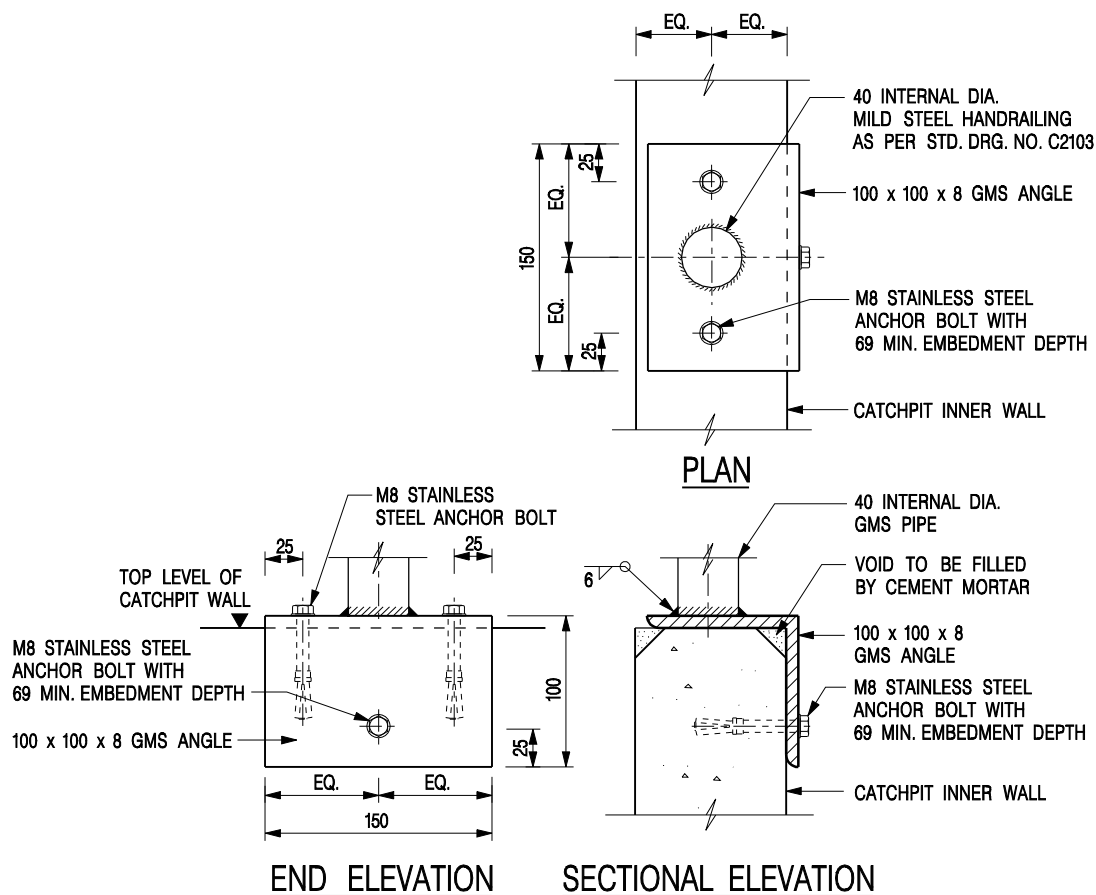
(DETAIL 'H')

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 4 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE AS SHOWN		DRAWING NO. C2405 /4	
DATE JAN 1991			




DETAIL 'J' – FIXING DETAILS FOR HANDRAILING ON TOP OF CATCHPIT WALL

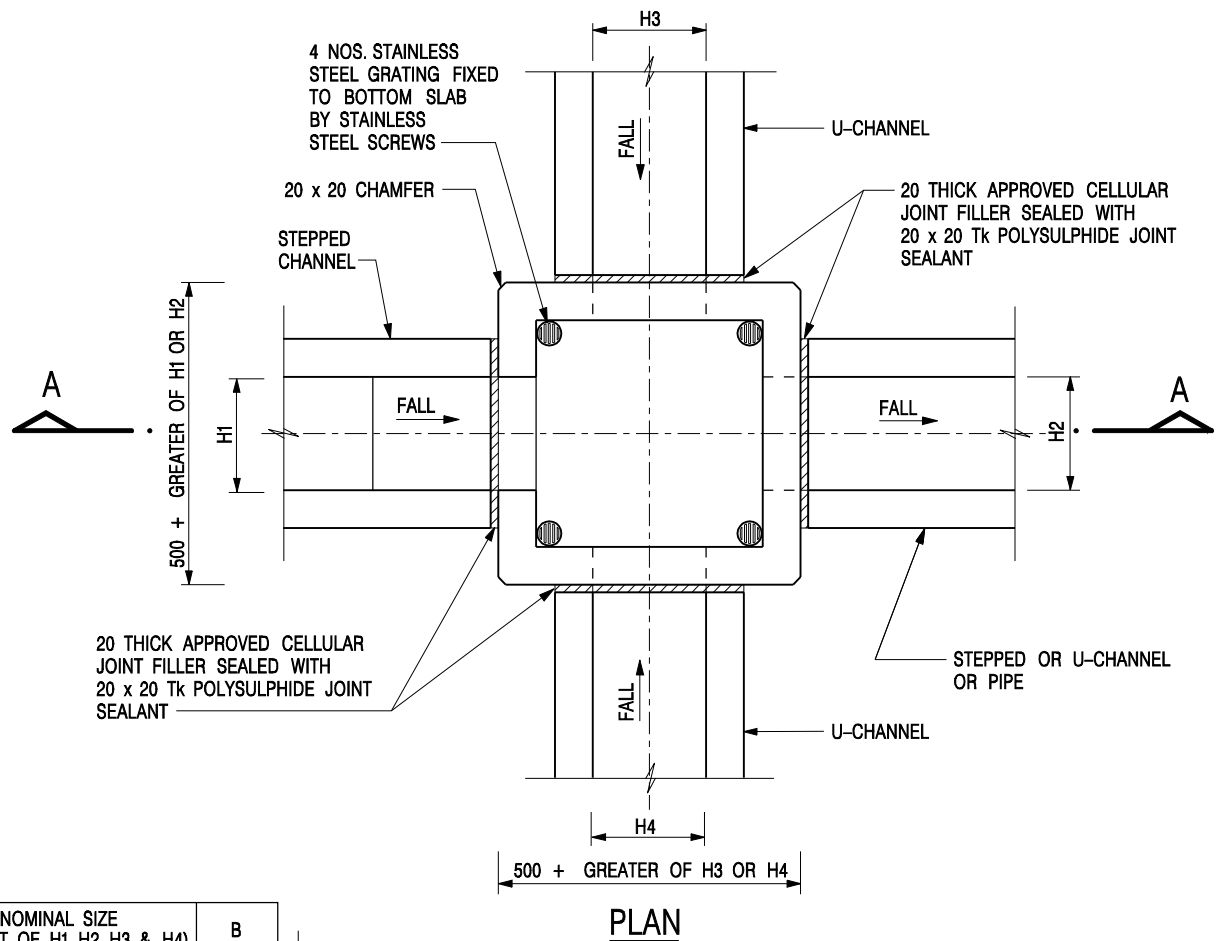
SCALE 1 : 5

NOTES:

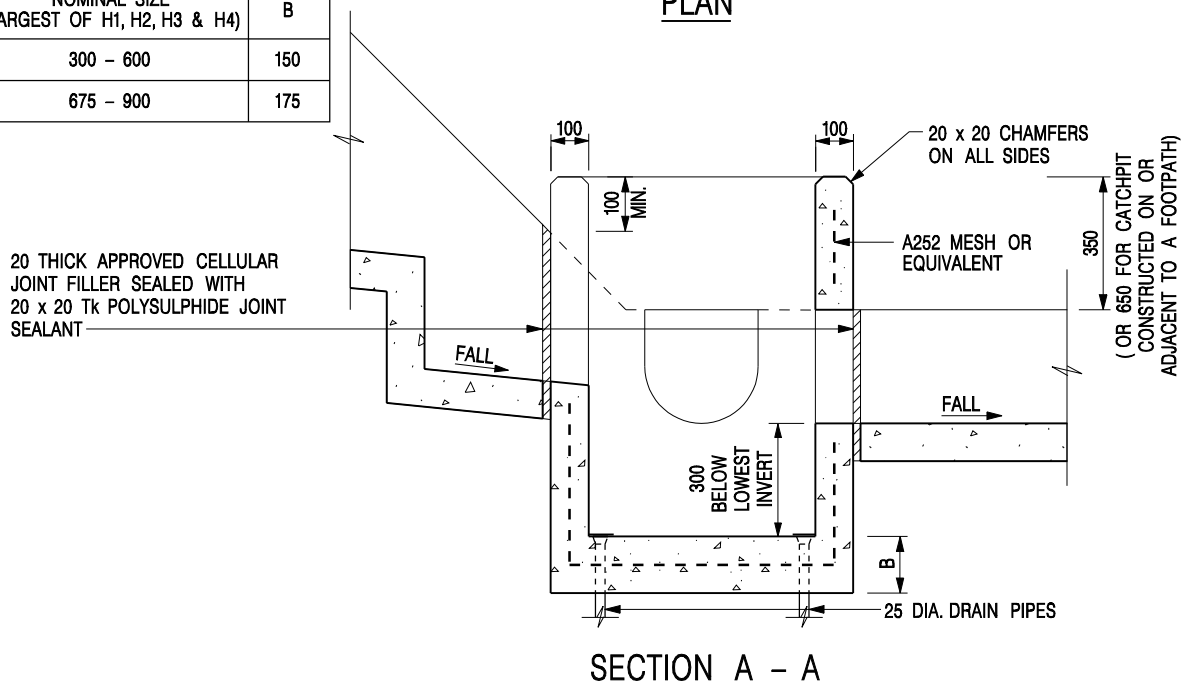
- ALL DIMENSIONS ARE IN MILLIMETRES.
- ALL CONCRETE SHALL BE GRADE 20 /20.
- CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
- FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
- CONCRETE TO BE COLOURED AS SPECIFIED.
- FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAILS ON SHEET 2 OR SHEET 3) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
- IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON SHEET 5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
- MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 mm c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
- FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON SHEET 4.
- ALL STEEL ANGLES SHALL COMPLY WITH BS EN 10025 AND BS EN 10056.
- UNLESS OTHERWISE SPECIFIED, ALL WELDS SHALL BE 5 mm CONTINUOUS FILLET WELDS.
- ALL WELDS SHALL BE CHIPPED, GROUND SMOOTH, BRUSHED TO REMOVE SLAG PRIOR TO HOT-DIP GALVANIZATION.
- ALL STEELWORK SHALL BE HOT-DIP GALVANIZED TO BS EN ISO 1461. ALL EXPOSED STEELWORK SURFACES SHALL BE TREATED AND PAINTED IN ACCORDANCE WITH THE GENERAL SPECIFICATION.
- SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

STANDARD CATCHPIT DETAILS
(SHEET 5 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE AS SHOWN		DRAWING NO. C2405 /5	
DATE JAN 1991			




NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

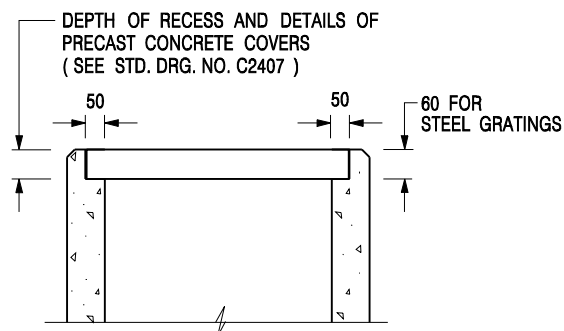


NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

CATCHPIT WITH TRAP
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE 1 : 20 DATE JAN 1991	
		DRAWING NO. C2406 /1	



ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAIL 'A' ON STD. DRG. NO. C2405 /2) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON STD. DRG. NO. C2405 /5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON STD. DRG. NO. C2405 /4.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

A	MINOR AMENDMENT.	Original Signed	04.2016
-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP
(SHEET 2 OF 2)**



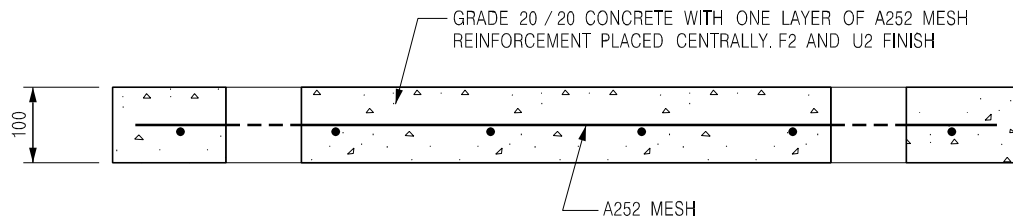
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

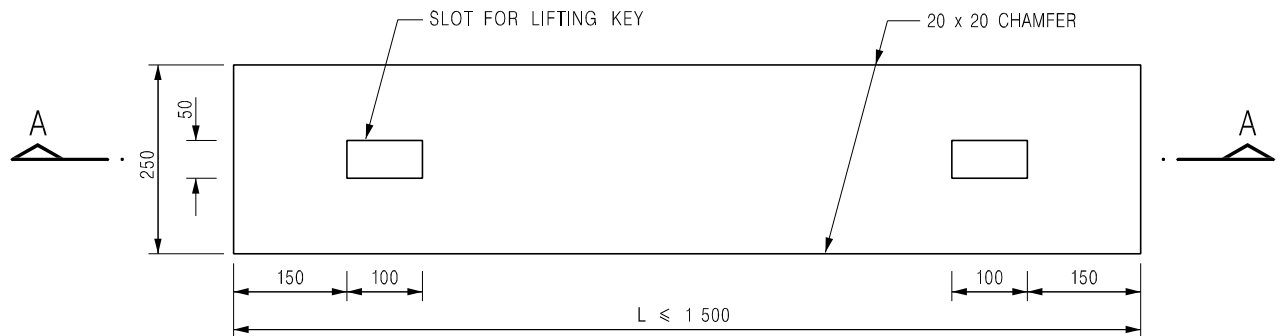
DATE JAN 1991

DRAWING NO.

C2406 /2A

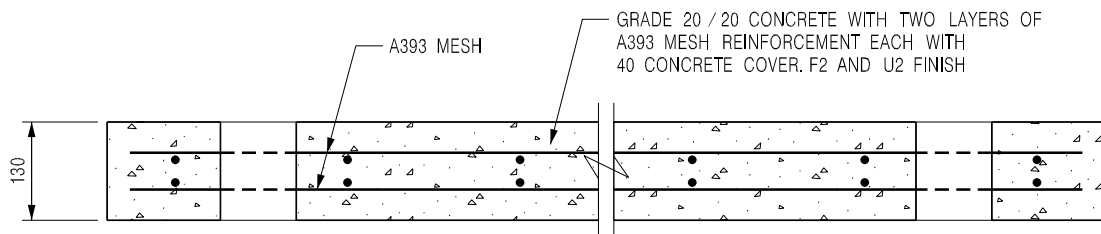


SECTION A - A

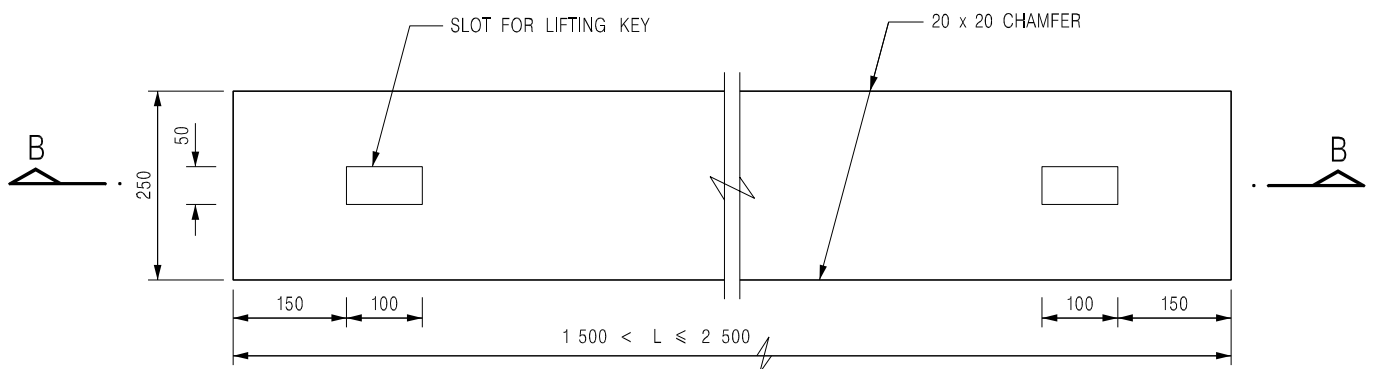


PLAN

TYPE 1 - FOR SPAN UP TO 1.5 m



SECTION B - B



PLAN

TYPE 2 - FOR SPANS 1.5 m TO 2.5 m

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL EXTERNAL EDGES OF THE COVERS SHALL BE 20mm CHAMFERED.

B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	GENERAL REVISION	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

PRECAST CONCRETE COVERS
FOR CATCHPIT AND SAND TRAP

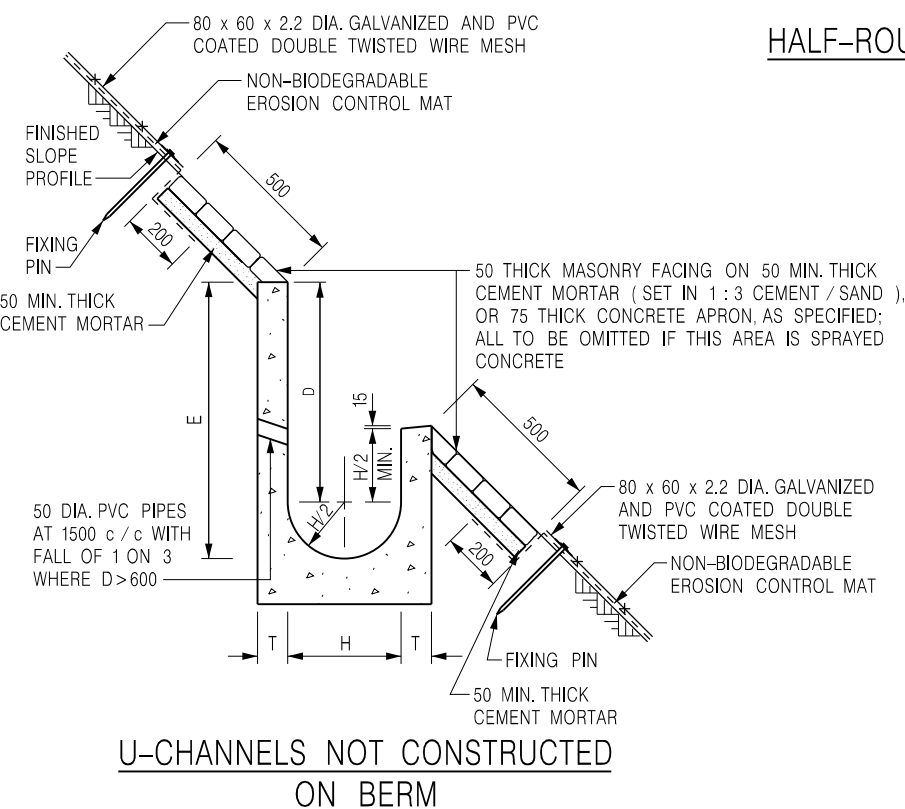
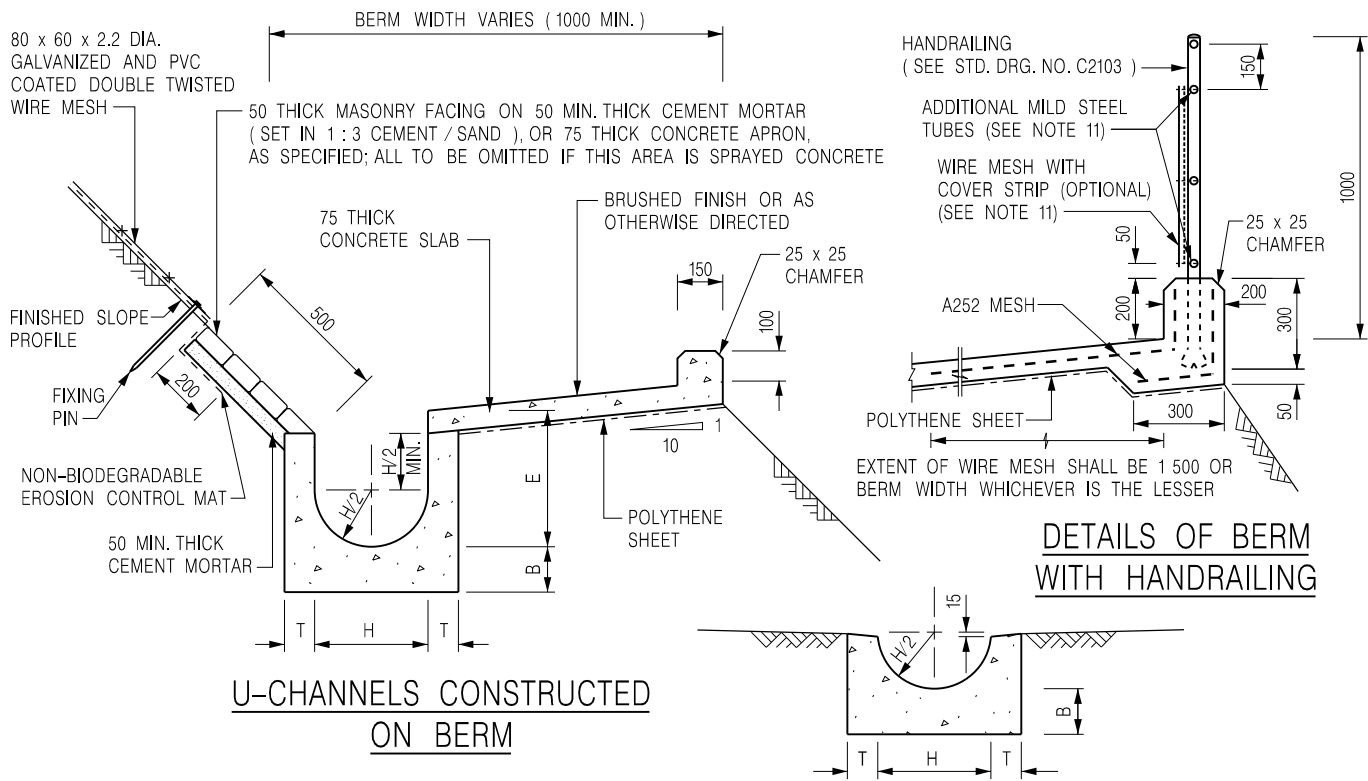


CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT

SCALE 1 : 10

DATE JAN 1991

DRAWING NO.
C2407B



NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE TO BE GRADE 20 / 20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2, F2 OR BRUSHED FINISH AS DIRECTED.
4. SPACING OF EXPANSION JOINT IN CHANNELS, BERM SLABS AND APRONS TO BE 10 METRES MAXIMUM, SEE STD. DRG. NO. C2413 FOR DETAILS.
5. JOINTS FOR CHANNELS, BERM SLABS, APRONS AND WALLS, ETC. TO BE ON THE SAME ALIGNMENT.
6. FOR DIMENSIONS T, H, & B, SEE TABLE BELOW.
7. BIODEGRADABLE EROSION CONTROL MAT IF REQUIRED, SEE STD. DRG. NO. C2511/E.
8. CONCRETE TO BE COLOURED AS SPECIFIED.
9. CONCRETE U-CHANNEL CAN BE CAST IN-SITU OR PRECAST CONCRETE SUBJECT TO THE ENGINEER'S AGREEMENT ON THE DETAILS.
10. DETAILS OF EROSION CONTROL MAT AND WESH MESH ON BERM. (SEE STD. DRG. NO. C2511/E)
11. THE WIRE MESH ON HANDRAILING IS OPTIONAL. THE COVER STRIP AND ADDITIONAL MILD STEEL TUBES ARE NEEDED ONLY IF WIRE MESH IS PROVIDED. (SEE STD. DRG. NO. C2103)

NOMINAL SIZE H	T	B	REINFORCEMENT
300	80	100	A252 MESH PLACED CENTRALLY AND T=100 WHEN E>650
375 - 600	100	150	
675 - 900	125	175	A252 MESH PLACED CENTRALLY

J	DETAILS OF HANDRAILING AMENDED.	Original Signed	08.2024
I	MINOR AMENDMENT.	Original Signed	07.2018
H	THICKNESS OF MASONRY FACING AMENDED.	Original Signed	01.2005
G	MINOR AMENDMENT.	Original Signed	01.2004
F	GENERAL REVISION.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

DETAILS OF HALF-ROUND AND U-CHANNELS (TYPE A - WITH MASONRY APRON)



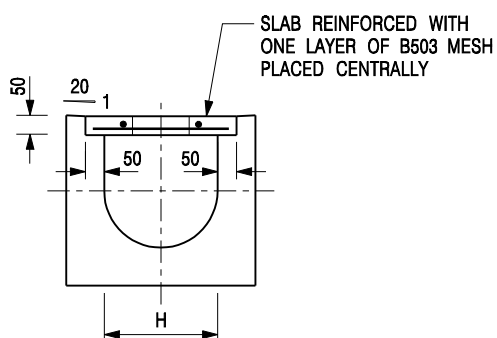
CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT

SCALE 1 : 25

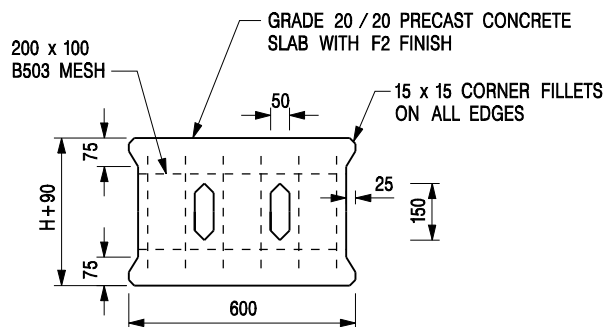
DATE JAN 1991

DRAWING NO.

C2409J



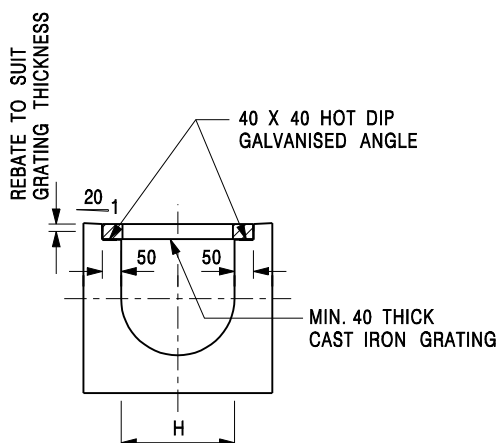
TYPICAL SECTION



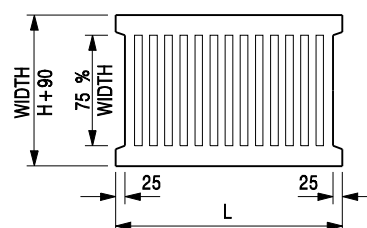
PLAN OF SLAB

U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION



L = 600mm FOR H ≤ 375mm
L = 400mm FOR H > 375mm

CAST IRON GRATING

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

COVER SLAB AND CAST IRON
GRATING FOR CHANNELS



**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

DATE JAN 1991

DRAWING NO.
C2412E

Annex B

Revised Traffic Impact Assessment

Document Status Control Record

**Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi)
for a Period of 3 Years
at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.**

Traffic Impact Assessment Report

Originating Organisation: LLA Consultancy Limited Unit 610, 6/F Island Place Tower 510 King's Road North Point, Hong Kong	Prepared by: SKL	SKL	Date: 4 November 2025
	Approved by: SLN	SLN	Date: 4 November 2025
	Revision No.: -		Date of Issue: 4 November 2025

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1 INTRODUCTION

1.1 Background

- 1.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi) for a period of 3 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T..
- 1.1.2 The Site is zoned as “Agriculture” use under the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14. A planning application is required for the proposed temporary use.
- 1.1.3 LLA Consultancy Limited was commissioned to undertake the TIA to support the planning application. This TIA report presents the findings of the study.

1.2 Objectives

- 1.2.1 The objectives of the study are as follows:
- to review the existing traffic conditions in the vicinity of the Site;
 - to estimate the traffic generation and attraction of the proposed temporary use;
 - to project the future traffic situations in the surrounding road network;
 - to appraise the potential traffic impact of the proposed temporary use; and
 - to recommend the internal transport facilities for the proposed temporary use.

2 THE PROPOSED DEVELOPMENT

2.1 The Site

- 2.1.1 As shown in **Figure 2.1**, the Site is located at Sha Tau Kok Road – Lung Yeuk Tau area. It has a total site area of about 954 m².

2.2 The Proposed Temporary Use and Internal Transport Facilities Provisions

- 2.2.1 The Site is used to provide charging facilities for electric taxis. The key development parameters are listed out in **Table 2.1**.

Table 2.1 Key Development Parameters

Use	Quantity
Site Area	954 m ²
EV Charging Facilities for Electric Taxi only (2.5m x 5.0m)	12 nos.
Parking Spaces / Waiting Spaces for Electric Taxi only (2.5m x 5.0m)	5 spaces

2.3 Swept Path Analysis

- 2.3.1 The Site is directly abutted to Sha Tau Kok Road – Lung Yeuk Tau, the development traffic is anticipated to enter and leave the Site with a left-in/left out movement onto Sha Tau Kok Road south-westbound. In order to ensure the safety of pedestrians walking along the public footpath, a standard run-in/out in accordance with Highways Department's standards will be formed and flashing warning lights will be installed at both sides of the run-in/out as shown in **Figure 2.2**.
- 2.3.2 To ensure smooth manoeuvring of the parking area, swept path analysis was conducted to demonstrate that adequate space is provided for the vehicles for manoeuvring as shown in **Figures SP-01**.

3 EXISTING TRAFFIC SITUATION

3.1 Existing Road Network

- 3.1.1 The Site has a frontage along Sha Tau Kok Road – Lung Yeuk Tau and an existing run-in/out is provided for the current use of the Site.
- 3.1.2 Sha Tau Kok Road – Lung Yeuk Tau is a dual two lanes distributor road connecting Fanling and Sha Tau Kok. In 2023, it recorded an AADT of 22,810 vehicles.

3.2 Traffic Count Surveys

- 3.2.1 In order to appraise the existing traffic conditions, a traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30, the peak traffic generation period of the vehicle charging station. More details will be discussed in **Section 4.2**.
- 3.2.2 The locations of the surveyed junctions are shown in **Figure 3.1**.
- J1 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road
- J2 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road
- 3.2.3 The peak hour identified are 16:30 – 17:30. The observed 2025 traffic flows are presented in **Figure 3.2**.

3.3 Existing Junction Capacity Assessment

- 3.3.1 Based on the observed traffic flows, the performance of the junctions is assessed. It is calculated that all junctions are operating satisfactorily. The results are presented in **Table 3.1** and the detailed calculation sheets are attached in **Appendix A**.

Table 3.1 Existing Junction Capacity Assessment

Ref.	Junction Location	Type/Index ⁽¹⁾	Peak Hour of the Vehicle Charing Station
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.46
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.43

Note: (1) DFC = Design Flow to Capacity

3.4 Existing Link Capacity Assessment

- 3.4.1 The Volume to Capacity (V/C) Ratios of Sha Tau Kok Road – Lung Yeuk Tau is assessed and the results are presented in **Table 3.2**.

Table 3.2 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	Peak Hour Traffic Volume (pcu/hr)	Peak Hour V/C Ratio
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	811	0.26
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	935	0.30

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

- 3.4.2 As shown in **Table 3.2**, the concerned road sections are operating with spare capacity during the peak hour.

4 FUTURE TRAFFIC SITUATION

4.1 Design Year

- 4.1.1 The proposed temporary use, if approved by the Town planning Board, will be valid for 3 years until 2028. Hence, Year 2028 is adopted as the design assessment year.

4.2 Traffic Generation of the Proposed Temporary Use

Traffic Generation for Electric Vehicle Charging Station

- 4.2.1 For the charging facilities, all EV chargers are direct current (DC) quick chargers with output power of 120kW which can provide 600km of mileage when an electric taxi charged for about 30 minutes.
- 4.2.2 From the taxi trade, the drivers are usually working on two shifts, i.e., the day shift and the night shift. In general, the day shift is 05:00 to 17:00 while the night shift is 17:00 to 05:00. The peak hour usage of charging facilities is expected to be the time periods before the shift such that a fully charged taxi can be passed to the driver in the next shift. As a result, the peak traffic generation of the proposed charging station will be either between 15:00 to 17:00 or after midnight. It is anticipated that minimal drivers will charge their taxis during the normal highway peak hours because these time periods will be the busiest hour in their business.
- 4.2.3 According to the Hong Kong Planning Standard and Guidelines, Section 3.9.4, a minimum of two waiting spaces should be provided in each EV charging station to avoid the awaiting vehicles to queue on public roads. In view of the size of the Site, 5 waiting spaces can be provided. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant. An electronic gate will be installed at the entrance of the Application Site to prevent early access to the Application Site. Vehicles departing late will incur an additional charge. Charging arrangements and regulations will be communicated to all taxi drivers in advance.
- 4.2.4 The estimated traffic generation and attraction for Electric Vehicle Charging Station during the peak operation hour will be a two-way traffic of 24 taxis (24 Nos. of vehicles = 12 EV charging stations x 2 because of 30 minutes charging/servicing time for each taxi).
- 4.2.5 Based on the above, the traffic generation of the proposed temporary use has been estimated and presented in **Table 4.1**.

Table 4.1 Traffic Generation of Proposed Temporary Use

Proposed Use	Unit /Content	Operation Peak Hour		
		Generation	Attraction	Total
Estimated Traffic Generation/Attraction				
EV Charging Station	12 charging station	24	24	48

- 4.2.6 As shown in **Table 4.1**, the Site will generate a two-way traffic flows of 48 pcu/hour in the peak hour. The distribution of the development traffic of proposed temporary use is shown in **Figure 4.1**.

4.3 Traffic Generation of the Adjacent New Planning Application

- 4.3.1 One new application (Planning Application No. A/NE-TKL/813) at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, is identified in the vicinity of the Site and it is a temporary electric vehicle charging station. Having considered this application would have similar operation hours and patterns as the proposed temporary use, the traffic generation and attraction of this application are incorporated in the subsequent assessment and shown in **Table 4.2**.

Table 4.2 Traffic Generation of the Adjacent New Planning Application

Proposed Use	Unit /Content	Operation Peak Hour ⁽¹⁾		
		Generation	Attraction	Total
EV Charging Station	18 stations	36	36	72
Ancillary Office and Retail Use	2,235 m ²	7	8	15
Total		43	44	87

Note: (1) Traffic generation and attraction numbers are abstracted from the TIA report of the application.

- 4.3.2 As shown in **Table 4.2**, this development would generate two-way traffic flows of 87 pcu/hr in the operation peak hour.

4.4 Traffic Growth

Annual Traffic Census (ATC) Data

- 4.4.1 Reference was made to the 2019 to 2023 Annual Traffic Census Reports, published by the Transport Department, to determine the natural traffic growth. The traffic data recorded at counting station in the vicinity of the Site is shown in **Table 4.3**.

Table 4.3 Annual Traffic Census Data

Stn. No.	Road Section			AADT (vehicle/day) ⁽¹⁾					Average Annual Growth (%)
	Road	From	To	2019	2020	2021	2022	2023	
5660	Sha Tau Kok Rd	On Kui St	Ping Che Rd	33,630	23,740 (-29.4%)	22,980 (-3.2%)	22,280 (-3%)	22,810 (2.4%)	-1.3% (Between 2020 and 2023)

Note: (1) Figures in bracket indicated the % increase between two successive years.

- 4.4.2 From **Table 4.3**, the recorded traffic flows in 2020 dropped significantly by almost 30% and was due to the opening of Lung Shan Tunnel in 2019. From 2020 to 2023, the recorded traffic flows have been quite steady with only -1.3% decrease annually.

Territorial Population and Employment Data Matrix (TPEDM)

- 4.4.3 Reference was also made to the 2021-based TPEDM published by Planning Department. The population and employment data of year 2026 and 2031 in Northeast New Territories are summarized in **Table 4.4**.

Table 4.4 Population and Employment Data in Northeast New Territories

Year	Population	Employment	Total
2026	1,361,150	421,350	1,782,500
2031	1,416,800	448,600	1,865,400
Average Annual Growth Rate			+0.9%

4.4.4 As shown in **Table 4.4**, the projected average annual growth rate of the population and employment total number under the TPEDM in Northeast New Territories is +0.9% between the years 2026 to 2031, which is larger than the annual growth rate derived from ATC. To be conservative, the larger growth rate of +0.9% is adopted for the subsequent assessments.

4.5 Reference and Design Flows

4.5.1 The 2028 Reference Flows, i.e. the future traffic flows in the local road network WITHOUT the development traffic, were estimated based on the following equation:

$$2028 \text{ Reference Flows} = 2025 \text{ Existing Flows} \times (1 + 0.9\%)^3 + \text{Traffic generated and attracted by adjacent new planning application}$$

4.5.2 The 2028 Design Flows, i.e. the future traffic flows in the local road network WITH the development traffic, are estimated based on the following equations.

$$2028 \text{ Design Flows} = 2028 \text{ Reference Flows} + \text{Traffic generated and attracted by the proposed temporary use}$$

4.5.3 The forecast traffic flows for the two scenarios are shown in **Figures 4.2 and 4.3** respectively.

4.6 Junction Capacity Assessment

4.6.1 Junction capacity analysis is carried out for the assessment year 2028. The assessment results are shown in **Table 4.5** and the detailed calculation sheets are attached in **Appendix B**.

Table 4.5 2028 Junction Capacity Assessment

Ref.	Junction Location	Type/ Index ⁽¹⁾	2028 Reference Peak Hour	2028 Design Peak Hour
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.47	0.48
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.44	0.45

Note: (1) DFC = Design Flow to Capacity.

4.6.2 The results in **Table 4.5** show that the assessed junctions will operate satisfactorily in both reference and design scenarios. Therefore, it is anticipated that the proposed temporary use will not induce significant traffic impact to the surrounding road network.

4.7 Link Capacity Assessment

4.7.1 The V/C Ratios of Sha Tau Kok Road were assessed and the results are presented in **Table 4.6**.

Table 4.6 2028 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	2028 Traffic Volume		2028 V/C Ratio	
		Reference Scenario	Design Scenario	Reference Scenario	Design Scenario
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	833	838	0.27	0.27
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	961	985	0.31	0.32

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

4.7.2 As shown in **Table 4.5**, all the concerned road sections will operate with capacity during peak hour in all scenarios. Hence, it can be concluded that the traffic induced by the proposed temporary use will not induce significant traffic impact to the road network.

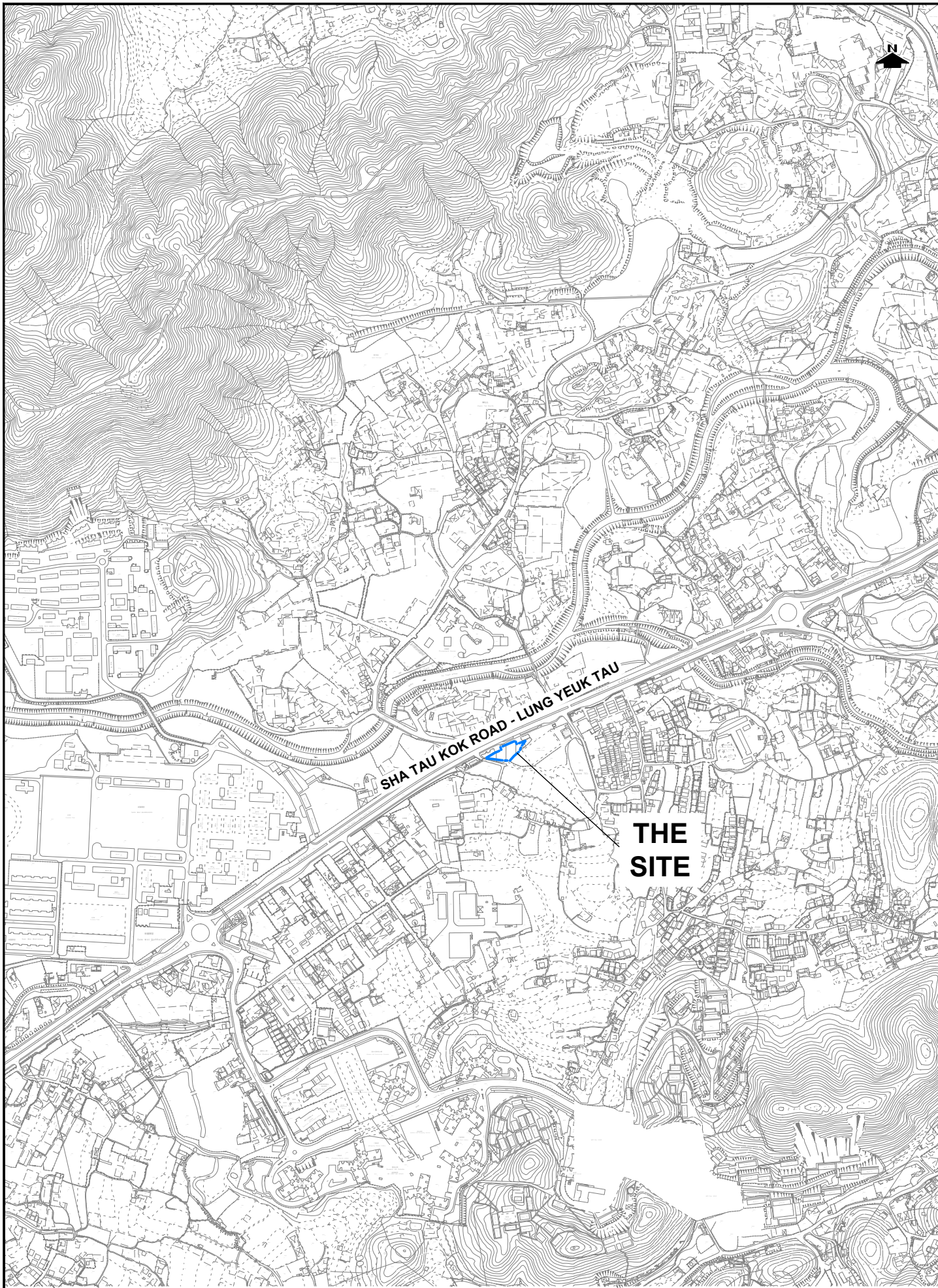
5 SUMMARY AND CONCLUSION

5.1 Summary

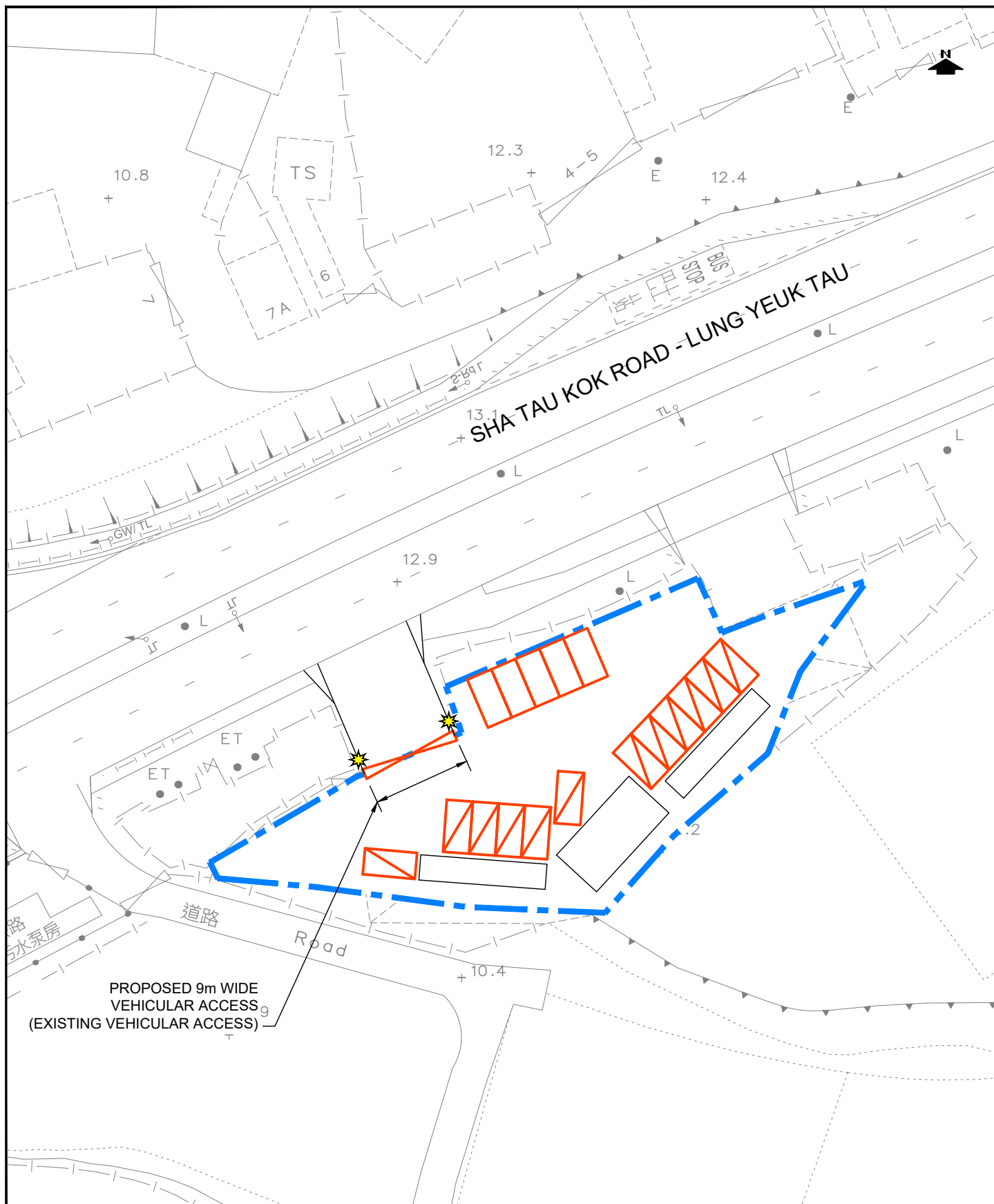
- 5.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi) for a period of 3 Years. The piece of land (hereinafter referred to “the Site”) is located at Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T. The main function of the Site is to provide charging facilities for electric taxis.
- 5.1.2 Traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 15:30 – 17:30 which the time period will be the peak traffic generation period of the proposed development. Based on the existing traffic flows, the junction and link capacity assessments show that they are operating satisfactorily.
- 5.1.3 The proposed development is estimated to generate a two-way traffic of 48 pcu/hour in the operation peak hour. Junction and link capacity assessment was carried out for the reference and design scenarios in 2028 and the results revealed that the concerned junctions and road links would perform satisfactorily with spare capacity in 2028. Therefore, it is anticipated that the proposed temporary use will not induce significant traffic impact to the surrounding road network.
- 5.1.4 For the 12 nos. of EV charging stations, 5 nos. of waiting spaces can be provided (minimum two spaces in each EV charging station) to avoid the awaiting vehicles to queue on public roads. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant. An electronic gate will be installed at the entrance of the Application Site to prevent early access to the Application Site. Vehicles departing late will incur an additional charge. Charging arrangements and regulations will be communicated to all taxi drivers in advance.

5.2 Conclusion

- 5.2.1 This planning application is to allow a temporary vehicle charging station for a period of 3 years. Based on the assessment result, it can be concluded that the proposed use will not induce additional adverse traffic impact on the surrounding road network and it is considered acceptable from traffic engineering point of view.



PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 2.1	REV. A
DESIGNED SLN	DATE OCT 2025	DRAWING TITLE LOCATION PLAN		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:5000				
CHECKED SLN					

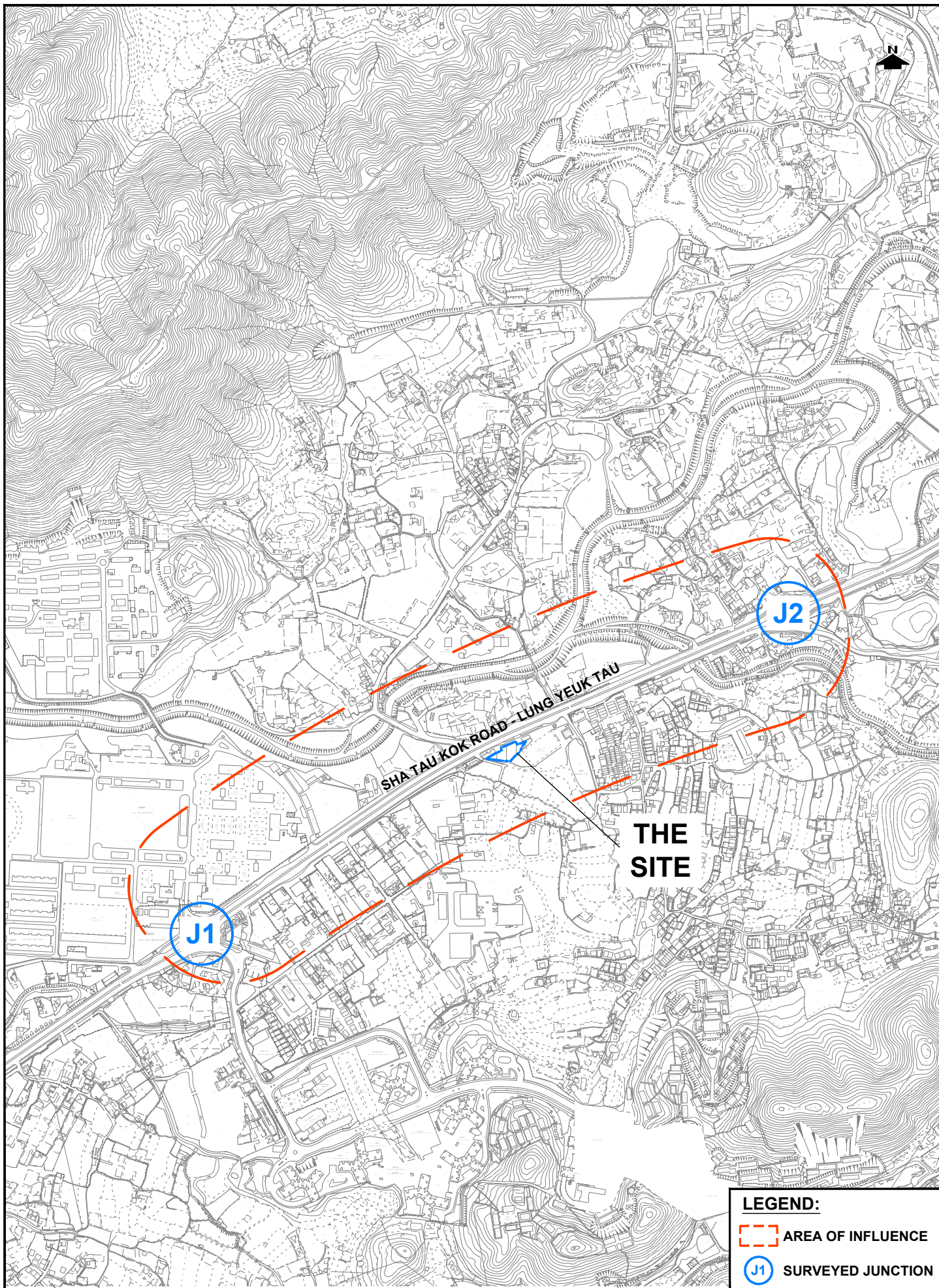


LEGEND:

- APPLICATION SITE (AREA: ABOUT 4,180M²)
- EV CHARGING SPACES FOR ELECTRIC TAXI (12 NOS.)
- PARKING SPACES/WAITING SPACES FOR ELECTRIC TAXI (5 NOS.)
- REVOLVING LAMPS

PROJECT NO. 40989	PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.	DRAWING NO. FIGURE 2.2	REV. B
DESIGNED SLN	DATE JUL 2025	PROPOSED LAYOUT PLAN	
DRAWN CLL	SCALE 1:500		
CHECKED SLN			

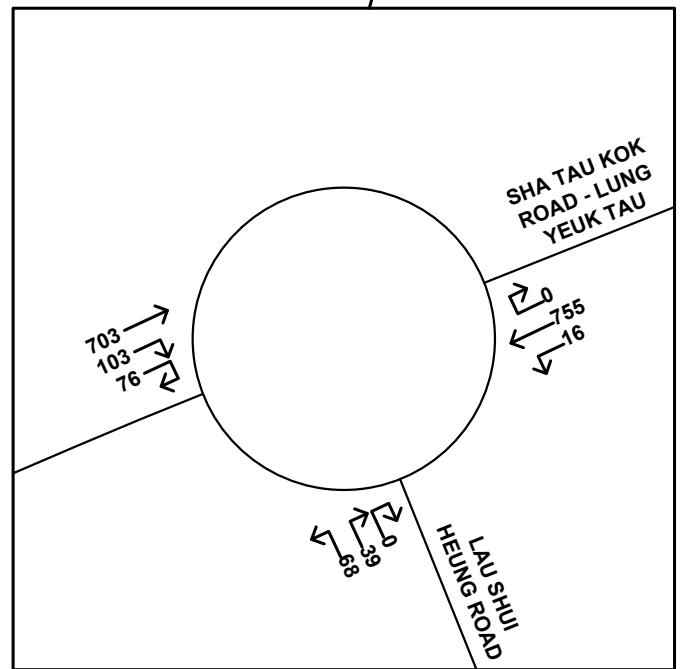
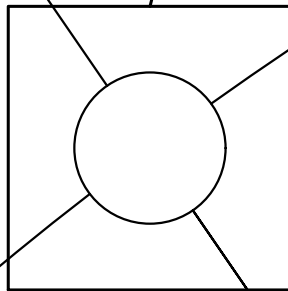
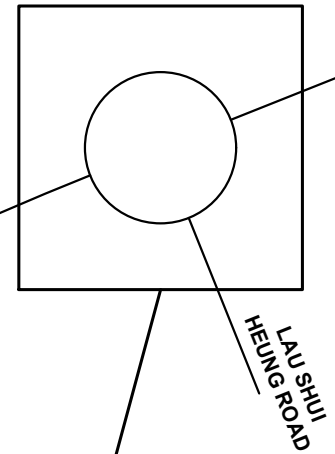
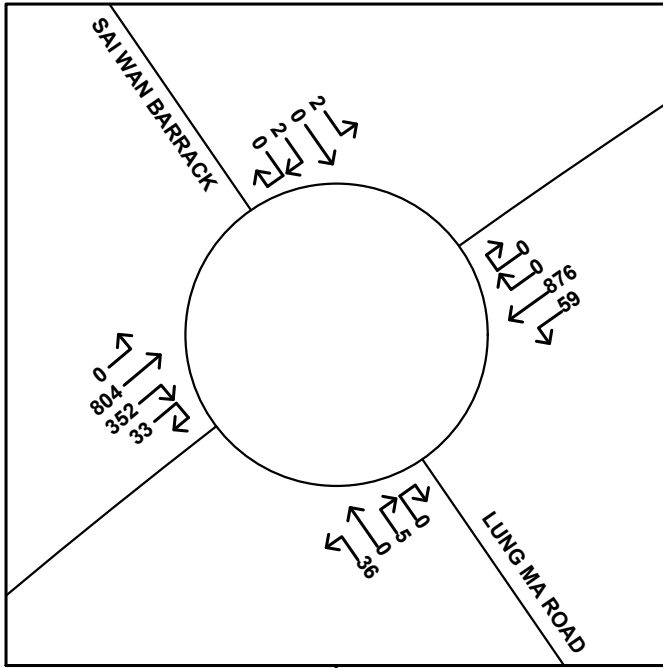
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LEGEND:

- AREA OF INFLUENCE
- J1 SURVEYED JUNCTION

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 3.1	REV. A
DESIGNED SLN	DATE OCT 2025	AREA OF INFLUENCE		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:5000				
CHECKED SLN					



LEGEND:

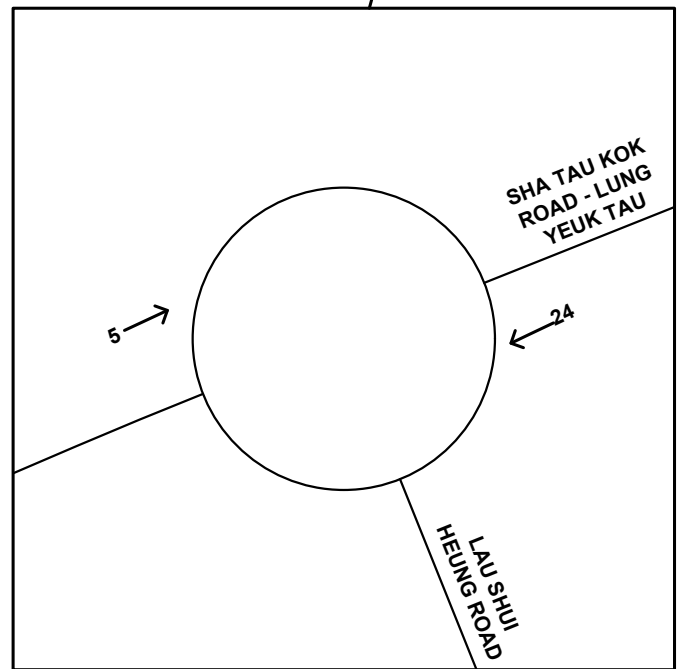
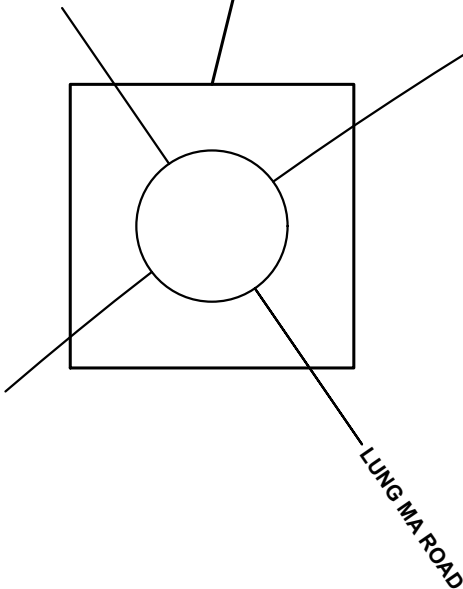
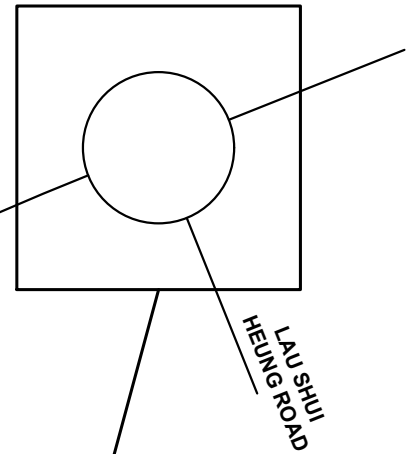
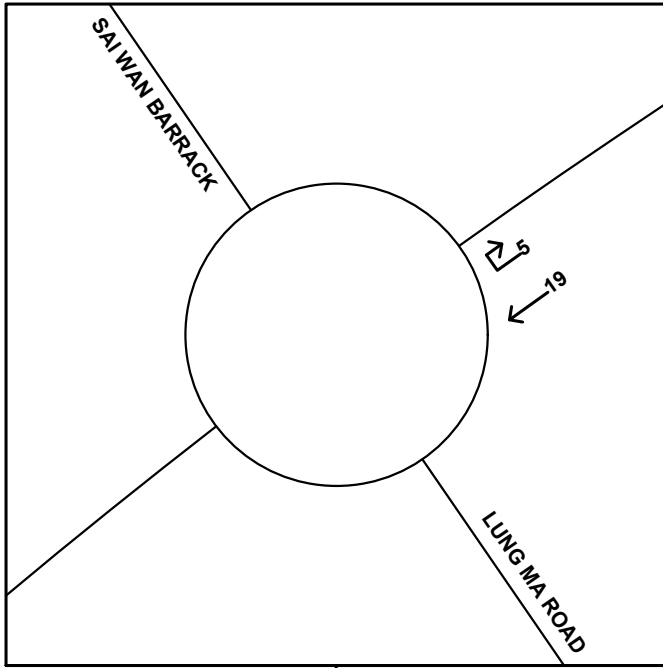
123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989	PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.	DRAWING NO. FIGURE 3.2	REV. A
DESIGNED SLN	DATE OCT 2025	2025 OBSERVED TRAFFIC FLOWS	
DRAWN CLL	SCALE N.T.S.		
CHECKED SLN			

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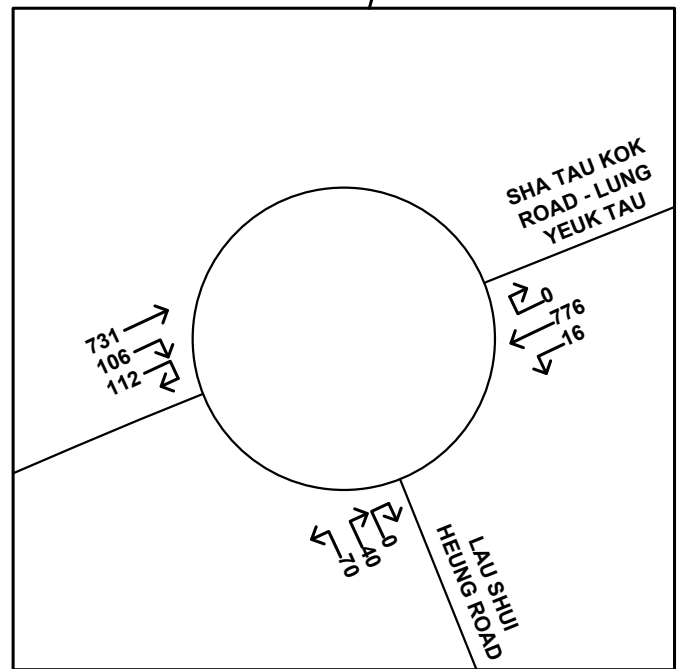
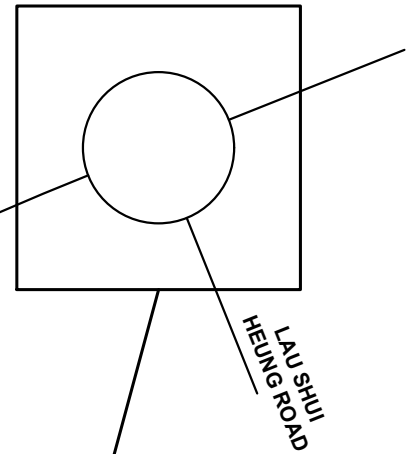
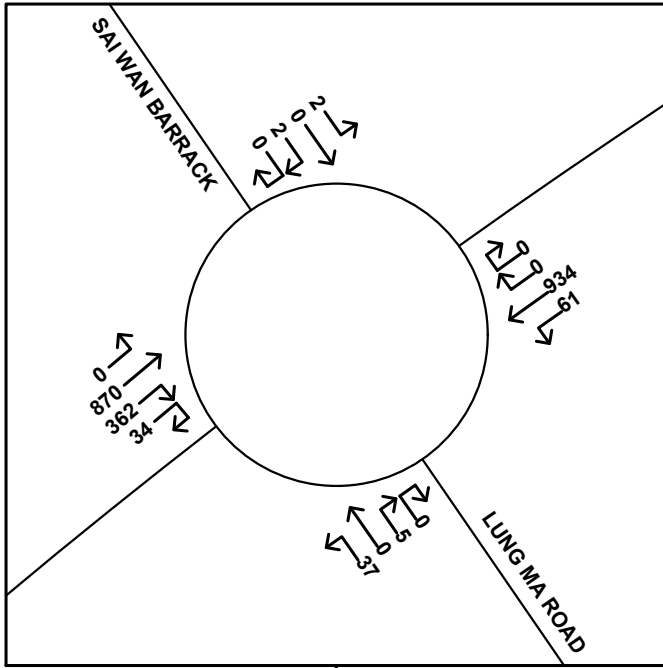
LEGEND:

123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.1	REV. A
DESIGNED SLN	DATE OCT 2025	DRAWING TITLE DEVELOPMENT TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



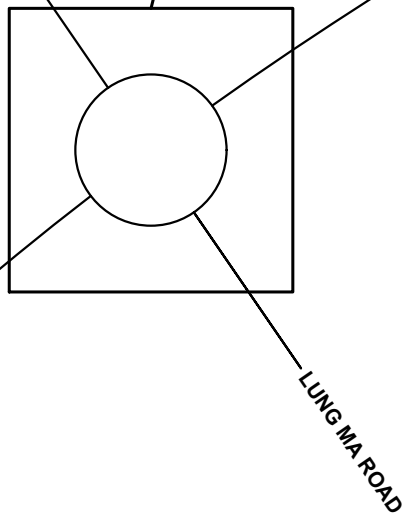
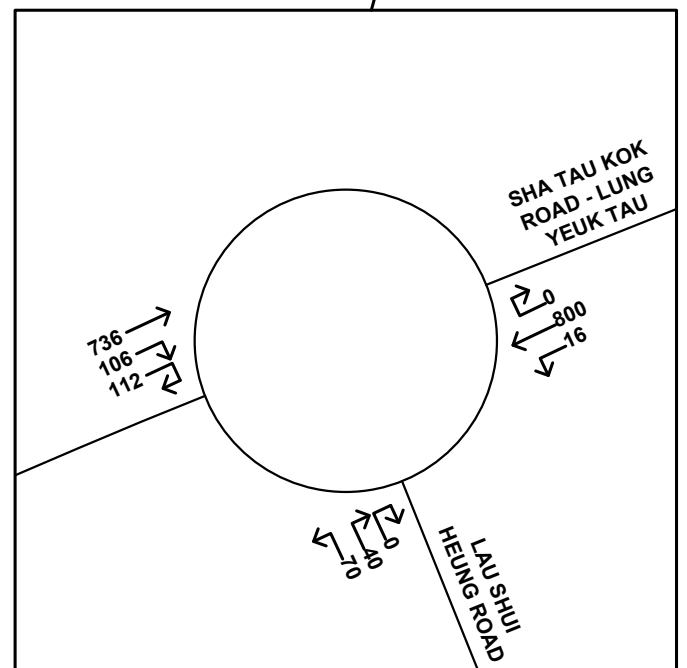
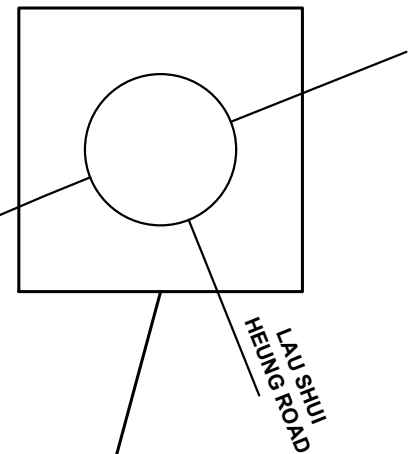
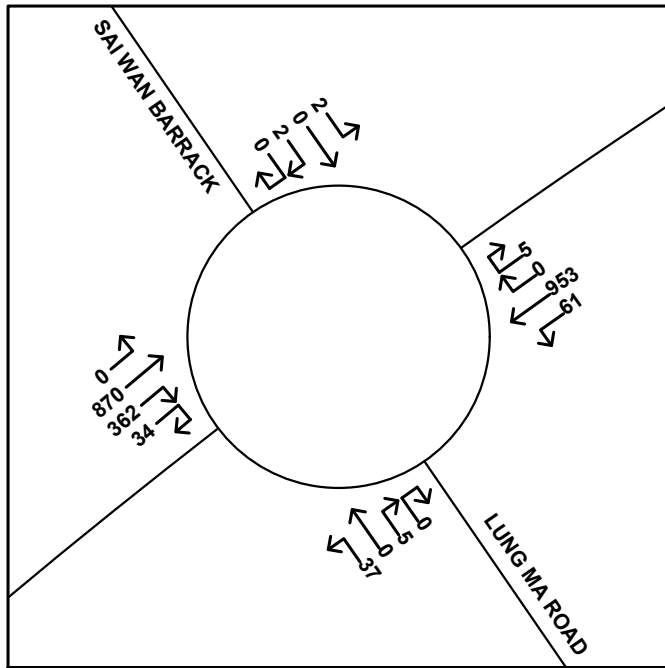
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123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.2	REV. B
DESIGNED SLN	DATE OCT 2025	DRAWING TITLE 2028 REFERENCE TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



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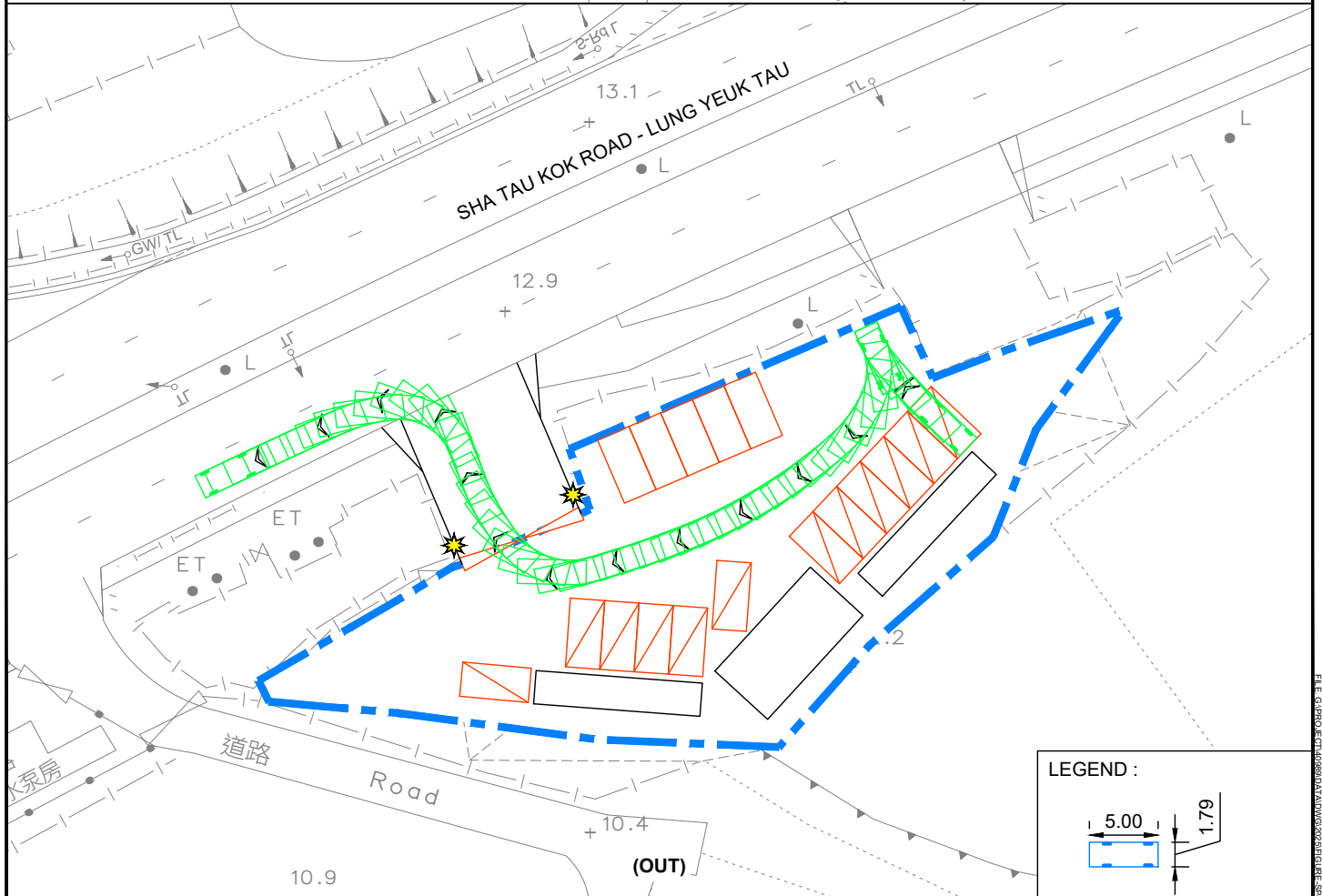
123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40989	PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.	DRAWING NO. FIGURE 4.3	REV. B
DESIGNED SLN	DATE OCT 2025	2028 DESIGN TRAFFIC FLOWS	
DRAWN CLL	SCALE N.T.S.		
CHECKED SLN			

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PROJECT NO. 40989		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI) FOR A PERIOD OF 3 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. SP-01	REV. A
DESIGNED SLN	DATE JUL 2025	DRAWING TITLE SWEPT PATH ANALYSIS - PC		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:500				
CHECKED SLN					

Appendix A
Junction Capacity Assessments
- Existing Scenario

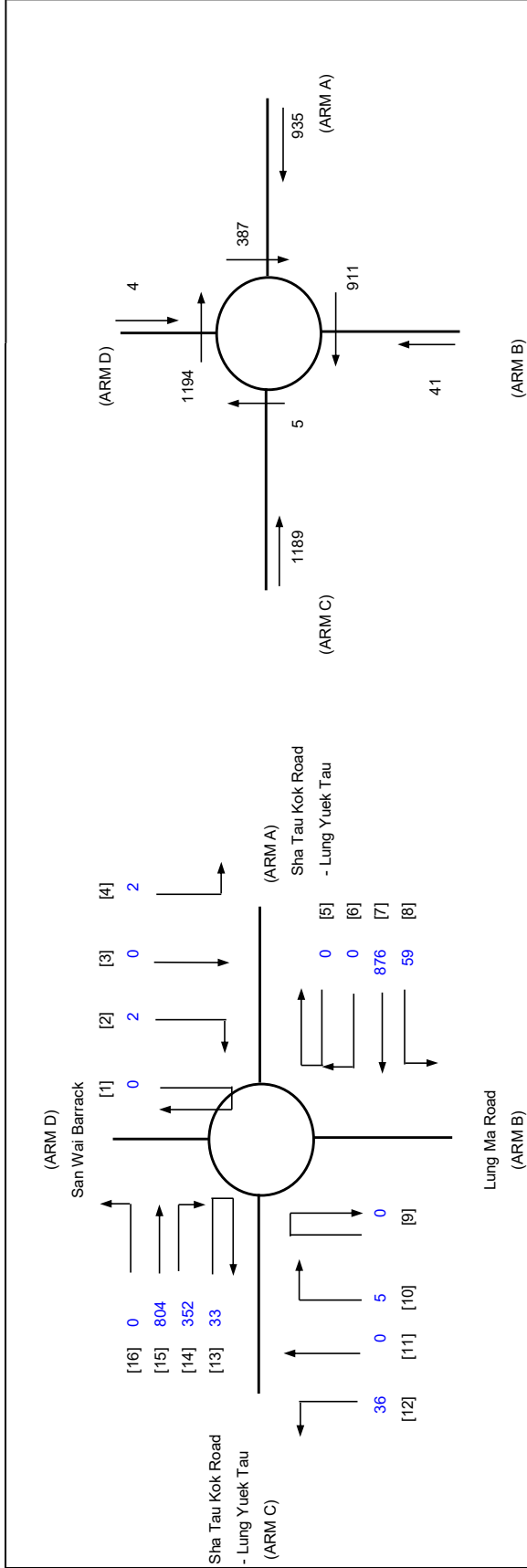
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ROUNDAABOUT CALCULATION

INITIALS	DATE
SKL	Oct-25
SLN	Oct-25
SLN	Oct-25
PROJECT NO.: 40989	
PREPARED BY:	
FILENAME : J1_STKR_LMR.xl	
CHECKED BY:	
REFERENCE NO.:	
REVIEWED BY:	

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

2025 Existing 1630-1730



ARM	A	B	C	D
INPUT PARAMETERS:				
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	935	41	1189	4
Qc = Circulating flow across entry (pcu/h)	387	911	5	1194
OUTPUT PARAMETERS:				
S = Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2 = $V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M = $EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F = $303 \times X2$	2188	1415	2669	1401
Td = $1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe = $K(F-Fc \times Qc)$	2045	1017	2693	808
DFC = Design flow/Capacity = Q/Qe				
	0.46	0.04	0.44	0.00
Total In Sum =				1292 PCU
DFC of Critical Approach =				0.46

Appendix B

Junction Capacity Assessments

- Reference & Design Scenarios

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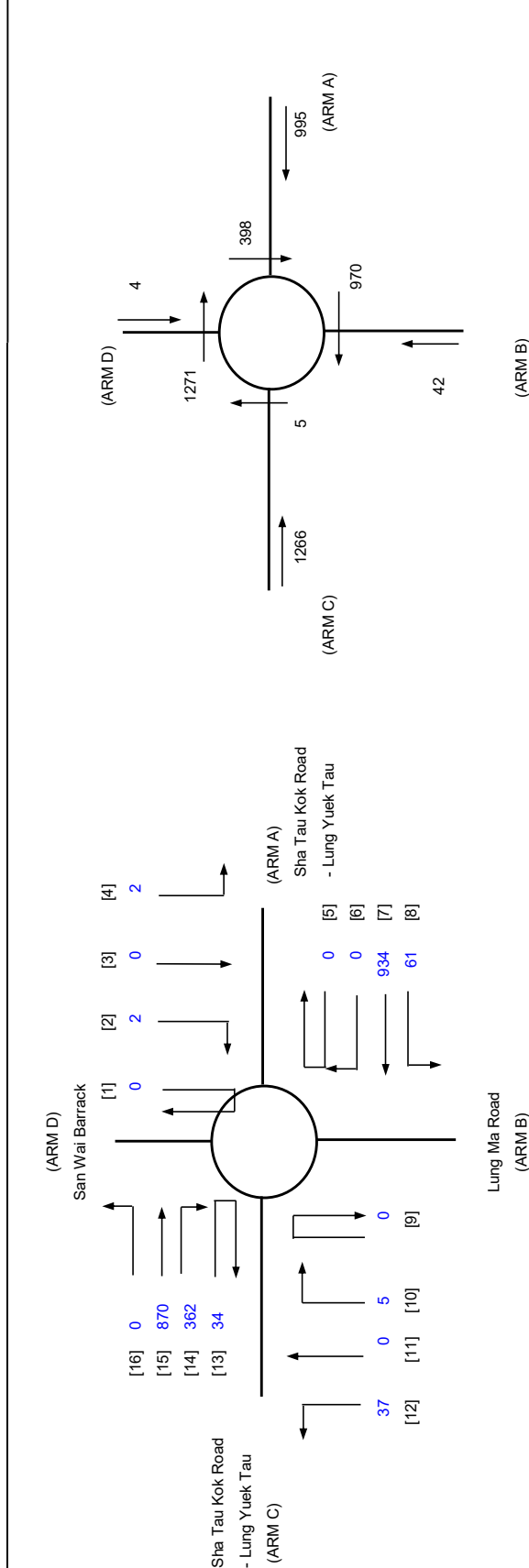
ROUNDAABOUT CALCULATION

INITIALS	DATE
SKL	Oct-25
SLN	Oct-25
SLN	Oct-25

PROJECT NO.:	40989	PREPARED BY:
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:
REFERENCE NO.:		REVIEWED BY:

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yeuk Tau / Lung Ma Road



ARM

A B C D

INPUT PARAMETERS:

V	=	Approach half width (m)	7.10	3.50	7.10	4.20
E	=	Entry width (m)	7.30	5.20	9.50	4.70
L	=	Effective length of flare (m)	1.00	12.00	19.00	9.00
R	=	Entry radius (m)	58.00	69.00	31.00	42.00
D	=	Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A	=	Entry angle (degree)	21.00	10.00	32.00	18.00
Q	=	Entry flow (pcu/h)	995	42	1266	4
Qc	=	Circulating flow across entry (pcu/h)	398	970	5	1271

OUTPUT PARAMETERS:

S	=	Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K	=	$1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2	=	$V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M	=	$EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F	=	$303 \times X2$	2188	1415	2669	1401
Td	=	$1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc	=	$0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe	=	$K(F-Fc \times Qc)$	2037	982	2693	764

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach =

Total In Sum = 1362 PCU

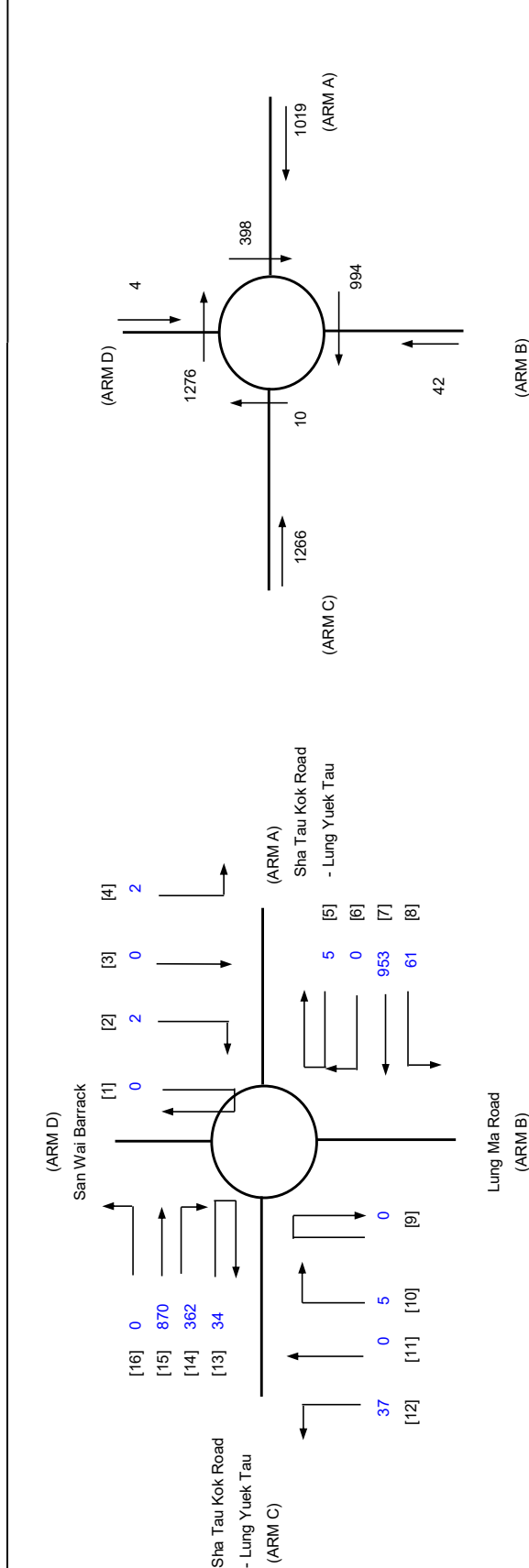
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INITIALS	DATE
SKL	Oct-25
SLN	Oct-25
SLN	Oct-25

PROJECT NO.:	40989	PREPARED BY:
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:
REFERENCE NO.:		REVIEWED BY:

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yeuk Tau / Lung Ma Road



ARM

A B C D

INPUT PARAMETERS:

V	=	Approach half width (m)	7.10	3.50	7.10	4.20
E	=	Entry width (m)	7.30	5.20	9.50	4.70
L	=	Effective length of flare (m)	1.00	12.00	19.00	9.00
R	=	Entry radius (m)	58.00	69.00	31.00	42.00
D	=	Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A	=	Entry angle (degree)	21.00	10.00	32.00	18.00
Q	=	Entry flow (pcu/h)	1019	42	1266	4
Qc	=	Circulating flow across entry (pcu/h)	398	994	10	1276

OUTPUT PARAMETERS:

S	=	Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K	=	$1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2	=	$V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M	=	$EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F	=	$303 \times X2$	2188	1415	2669	1401
Td	=	$1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc	=	$0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe	=	$K(F-Fc \times Qc)$	2037	968	2689	761

DFC = Design flow/Capacity = Q/Qe

Total In Sum =

1386 PCU

DFC of Critical Approach = 0.50

LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A.R.P (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDAABOUT CALCULATION

2028 Reference 1630-1730

PROJECT NO.: 40989

FILENAME : J2_STKR_LSHR.x

REFERENCE NO.:

PREPARED BY:

CHECKED BY:

REVIEWED BY:

INITIALS

SKL

SLN

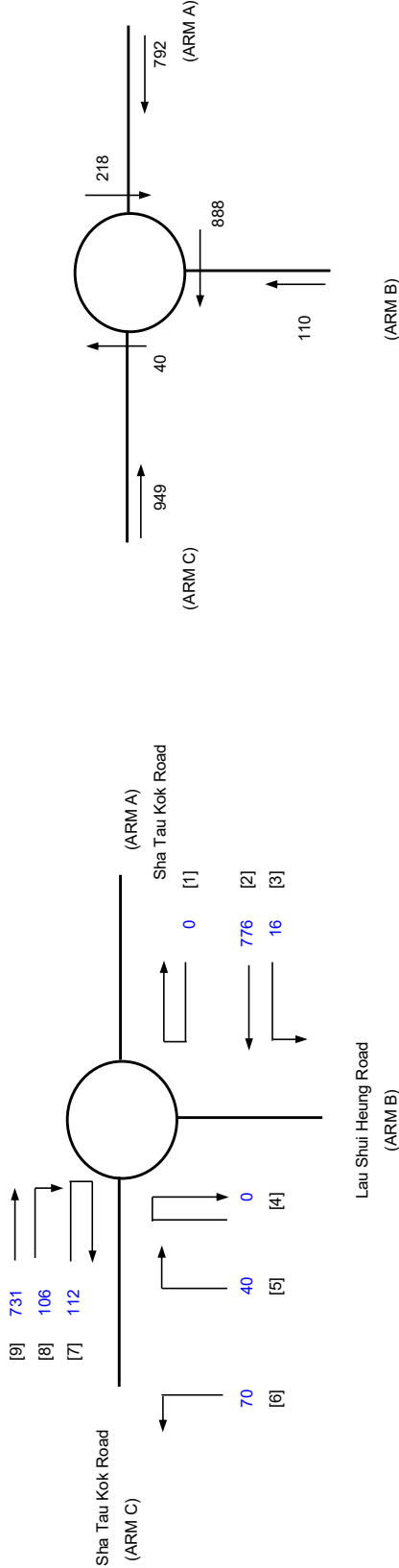
SLN

DATE

Oct-25

Oct-25

Oct-25



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	792	110	949
Qc = Circulating flow across entry (pcu/h)	218	888	40

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.16	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.92	0.93	1.09
X2 = $V + ((E-V)/(1+2S))$	6.88	4.61	6.48
M = $EXP((D-60)/10)$	0.45	0.45	0.45
F = $303 \times X2$	2083	1398	1962
Td = $1+(0.5/(1+M))$	1.34	1.34	1.34
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.67	0.54	0.65
Qe = $K(F-Fc \times Qc)$	1782	850	2120

DFC = Design flow/Capacity = Q/Qe

Total In Sum =

1851 PCU

DFC of Critical Approach = 0.45

LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi) for a Period of 3 Years at Taxford Lot 464 S.A.RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDAABOUT CALCULATION

2028 Design 1630-1730

PROJECT NO.: 40989

FILENAME : J2_STKR_LSHR.x

REFERENCE NO.:

PREPARED BY:

CHECKED BY:

REVIEWED BY:

INITIALS

SKL

SLN

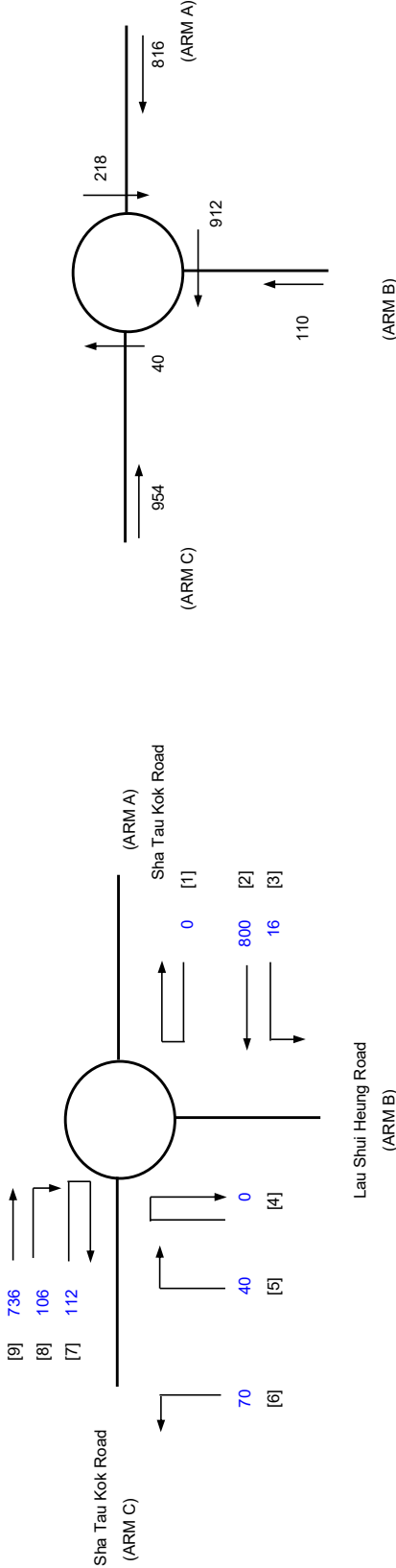
SLN

DATE

Oct-25

Oct-25

Oct-25



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	816	110	954
Qc = Circulating flow across entry (pcu/h)	218	912	40

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.16	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.92	0.93	1.09
X2 = $V + ((E-V)/(1+2S))$	6.88	4.61	6.48
M = $EXP((D-60)/10)$	0.45	0.45	0.45
F = $303 \times X2$	2083	1398	1962
Td = $1+(0.5/(1+M))$	1.34	1.34	1.34
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.67	0.54	0.65
Qe = $K(F-Fc \times Qc)$	1782	838	2120

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.46

Total In Sum =

1880 PCU

Annex C

Support Letter from the Village Representative of Kwan Tei Tsuen

致： 城市規劃委員會

由： 粉嶺區鄉事委員會副主席兼軍地原居民村代表劉永安

**新界粉嶺沙頭角公路龍躍頭段丈量約份第 83 約
補租地段第 464 號 A 分段餘段（部分）及毗連政府土地
擬議臨時電動車充電站(電動的士)(為期 3 年)的規劃申請
(規劃申請編號：A/NE-LYT/860)**

1. 本人認為，提供電動車充電設施是積極配合香港政府的環保政策，旨在減少碳排放，同時響應國家推動綠色能源與可持續發展的戰略方向。
2. 關於一名原居民關注申請地點內之「社稷大王之神位」及「開山宿老之神位」，現說明如下：該兩個神位目前位於私人地段，為該地段前業主所設立，近年祭拜活動已大幅減少。現業主已承諾，在發展過程中將與本村村民充分協商，妥善安排神位的搬遷事宜。
3. 在確保不影響本村傳統祭祀活動的前提下，本人對這宗申請表示全力支持。



劉永安

2025 年 10 月 23 日



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Appendix Ic of

RNTPC Paper No. A/NE-LYT/860A



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Pun How Wai
B.Arch.HK RIBA

By Email and Post

Date : 3 December 2025
Your Ref.: TPB/A/NE-LYT/860
Our Ref. : LDS/PLAN/7140

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road, North Point
Hong Kong



Dear Sir/Madam,

**Application for Planning Permission for Proposed Temporary Green Fuel Station
(Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years at
Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.
(Application No. A/NE-LYT/860)**

We refer to the comments from various Government departments on the captioned application and submit herewith our response in **Table 1** for your consideration.

Should there be any queries, please contact our Ms. Cannis Lee at

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited

Encl.

c.c. DPO/STN (Attn.: Mr. Peter Ngan) – By Email
Client

Table 1: Responses to Comments from Government Departments on Planning Application No. A/NE-LYT/860 (3 December, 2025)

COMMENTS	RESPONSES
1. Environment and Ecology Bureau	
(a) To echo with the latest version of Ch.8 of HKPSG about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the applicant is suggested to comply with the relevant requirement of HKPSG, i.e., EV chargers with output power of not less than 7kW should be installed in all parking spaces for private cars, light goods vehicles (LGVs) and motorcycles of the application site (the Site).	(a) All charging spaces will be equipped with EV chargers with an output power of 120 kW. The number of charging spaces provided will be determined by the available power supply and a transformer room will be provided to serve the 12 EV charging spaces. Besides, according to Section 3.9.4 of the Hong Kong Planning Standard and Guidelines (HKPSG), each EV charging station should have a minimum of 2 waiting spaces to prevent vehicles from queuing on public roads while waiting. The proposed development will provide 5 waiting spaces for electric taxis, thus meeting the HKPSG requirements.
(b) The Government announced the Green Transformation Roadmap of Public Buses and Taxis in December 2024, including measures to realise the target of introducing about 3 000 electric taxis by end-2027. A comprehensive fast charging network is needed to effectively support the operations of electric taxis and achieve the aforesaid target. In this connection, it is recommended that the applicant consider installing some fast chargers with a rated output power of 100kW or higher at the subject site and open up a certain number of charging spaces for electric commercial vehicles for use, e.g. electric LGVs.	(b) All charging spaces will be equipped with a fast-charging device, i.e. a direct-current quick charger with an output power of 120 kW. Due to the limited space within the Application Site, the proposed development is intended to serve electric taxis only and not be able to serve other electric commercial vehicles, e.g. electric LGVs.
(c) The applicant is suggested to consider arranging some of the chargers to also be compatible with Guobiao charging standard to support southbound vehicles.	(c) The proposed development is intended to serve the electric taxi of a taxi fleet only. It will not be open to the public and will not serve southbound vehicles.

COMMENTS	RESPONSES
(d) It is recommended that the applicant provide charger information, including the real-time availability data of each charger, through the government-designated mobile applications such as “HKeMobility” of the Transport Department. For any queries, please contact the EPD’s EV Hotline at 3757-6222 or email to ev@epd.gov.hk .	(d) Since the proposed development will only serve electric taxis of a taxi fleet, the Applicant will make use of a mobile application for electric taxi drivers, which provides real-time information on the availability of EV charging spaces.
2. Transport Department	
<p>Please find the following comments on the revised traffic impact assessment (TIA):</p> <p>(a) Drawings No. FIGURE 4.2 and FIGURE 4.3: The total entry traffic flows on Sha Tau Kok Road – Lung Yeuk Tau northbound arms in both junctions and those on the southbound arm in its junction with Lung Ma Road do not tally with those in the previous version of TIA. Please review.</p>	<p>(a) Please note that the development traffic of a nearby planned development (Planning Application No. A/NE-TKL/813) was incorporated into the updated TIA report. The traffic flows of the planned development were distributed onto the road network in the reference and design scenarios which lead to an increase in the total entry traffic flows at the concerned location.</p>
3. Urban Design & Landscape Section of Planning Department	
<p>Please find the following comments from landscaping planning perspective</p> <p>(a) Referring to the Response to Comments (R-to-C) Table, all existing trees (T1 to T6) are common species and are proposed to be felled as they are in conflict with the proposed waiting spaces. However, with reference to Figure 6, tree T6 (i.e. <i>Macaranga tanarius</i>) is not in conflict with the proposed parking/waiting spaces.</p>	<p>(a) T6 is in conflict with the proposed fencing and manoeuvring space, and is therefore proposed to be felled.</p>



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Appendix Id of
A/NE-LYT/860A
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By Email and Hand

Date : 16 December 2025
Your Ref.: TPB/A/NE-LYT/860
Our Ref. : LDS/PLAN/7140

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road, North Point
Hong Kong

Dear Sir/Madam,

**Application for Planning Permission for Proposed Temporary Green Fuel Station
(Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years at
Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.**

(Application No. A/NE-LYT/860)

We refer to the captioned planning application and hereby submit the updated Consolidated Planning Statement (**Appendix 1**) and replacement pages of the Application Form (**Appendix 2**) for your consideration.

Should there be any queries, please contact our Ms. Cannis Lee or Ms. Yancy Fung at

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited

Lawson David & Sung

Encl.

c.c. DPO/STN (Attn.: Mr. Peter Ngan) – By Email
Client



Ref: LDS/PLAN/7140

Section 16 Planning Application

Proposed Temporary **Green Fuel** Station (**Electric Vehicle Charging Station** for Electric Taxi) for a Period of 3 Years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.

Planning Statement

Applicant

Wealth Generation International Limited (新世代國際有限公司)

Prepared by

Lawson David & Sung Surveyors Limited

December 2025

Executive Summary

This planning statement is prepared in support of a planning application for proposed temporary **green fuel station (electric vehicle (EV) charging station** for electric taxi) ("the proposed development") for a period of 3 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (the "Application Site").

The Application Site, covering an area of about **954 sq.m. (including Government Land of about 381 sq.m.)**, falls within an area zoned "Agriculture" ("AGR") on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022. According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

The Application Site is currently used as vehicle repair workshops. In consideration of the growing popularity of the EVs in Hong Kong and introduction of 3,000 electric taxis expected in 2026, the Applicant, in cooperation with one of the selected taxi fleet operators, intends to convert the Application Site into an EV charging station to provide EV charging facilities for electric taxis.

The proposed EV charging station will provide a total of **17** parking spaces (2.5m x 5m) for electric taxis. 12 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) quick chargers with an output power of 120kW. The remaining **5** parking spaces will be designated as parking / waiting spaces for the charging facilities and free of charge.

Two 1-storey (height: about 3m) containers for EV chargers, each with a floor area of about 29 sq.m., will be situated at the southern boundary of the Application Site. Each container will provide 6 EV charging devices for electric taxis. A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices.

The Applicant will maintain the existing ingress/egress point of about 9m wide at the northern boundary of the Application Site, connecting to Sha Tau Kok Road – Lung Yeuk Tau. The proposed hours of operation at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays).

The justifications of this application are:

1. The proposed development is in line with Government policy to promote EVs usage;
2. The proposed development meets the demand for quick EV charging facilities;
3. The proposed development does not contravene the planning intention of "AGR" zone;
4. The proposed development is considered not incompatible with surrounding land uses;
5. The proposed development will promote optimization of valuable land resources;
6. The proposed development will not hinder future development of New Territories North New Town; and
7. The proposed development will not generate adverse traffic, drainage and environmental impacts on the surrounding areas.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

行政摘要

此規劃報告書旨在支持在新界粉嶺沙頭角公路 - 龍躍頭段丈量約份第 83 約補租地段第 464 號 A 分段餘段(部分)及毗連政府土地 (“申請地點”), 作為期三年的擬議臨時潔淨能源站(電動的士充電站) (“擬議發展”)的規劃申請。

申請地點的面積約 954 平方米 (包括政府土地約 381 平方米), 座落於在 2022 年 12 月 16 日刊憲的龍躍頭及軍地南分區計劃大綱核准圖 (圖則編號: S/NE-LYT/19)上的「農業」地帶。根據該大綱圖的註釋, 不超過三年土地或建築物的臨時用途或發展, 須先向城規會提出申請。

申請地點現時為汽車修理工場。考慮到本地電動車日趨普及以及政府將於 2026 年引入 3,000 輛電動的士, 申請人將與一支被政府選中的的士車隊合作, 擬將申請地點改作成電動車充電站, 為電動的士提供充電設施。

擬議電動車充電站將提供 17 個供電動的士停泊的停車位(2.5 米 x 5 米), 其中 12 個停車位為配備電動車充電設備的收費停車位, 所有充電裝置均是輸出功率為 120kW 的直流快充裝置。剩餘的 5 個停車位將用作充電設施的停車 / 等候區, 並免費使用。申請地點南邊將設置兩個一層高(高度約 3 米, 樓面面積各約 29 平方米)的改裝貨櫃, 以容納電動車充電器, 每個貨櫃可提供 6 個電動的士充電器。貨櫃之間將提供一個一層高(高度約 3.5 米, 樓面面積約 53 平方米)的電力變壓器房 / 掣房, 為所有充電設備提供足夠的電力。

申請人將繼續使用申請地點北面連接到沙頭角公路 - 龍躍頭段的出入口(約 9 米闊)。申請地點的營運時間為星期一至日(包括公眾假期)24 小時運作。

本規劃申請的理據為:

1. 擬議發展支持政府推動電動車普及化的政策;
2. 擬議發展能滿足對電動車快速充電設施的需求;
3. 擬議發展不會違背「農業」地帶的規劃意向;
4. 擬議發展與周邊土地用途兼容;
5. 擬議發展可有效利用寶貴的土地資源;
6. 擬議發展不會阻礙未來新界北新市鎮的發展; 及
7. 擬議發展不會對附近的交通、排水及環境構成不良的影響。

基於本規劃報告書所述的理據, 敬希 各城規會委員及有關政府部門能批准此項申請, 作為期三年的臨時用途。

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1. Introduction

1.1 Purpose

Pursuant to Section 16 of the Town Planning Ordinance, this Planning Statement Report is submitted to the Town Planning Board (“the Board”) in support of a planning application for proposed temporary electric vehicle charging station (for electric taxi) (“the proposed development”) for a period of 3 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (hereafter referred to as “the Application Site”). **Figure 1A** shows the location of the Application Site and the Lot Index Plan indicating the concerned lot is shown per **Figure 2A**.

The Application Site, covering an area of **about 954 sq.m. (including Government land of about 381 sq.m.)**, falls within an area zoned “Agriculture” (“AGR”) on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022. According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

The Application Site is currently occupied by vehicle repair workshops. In view of the growing demand for electric vehicles (EVs), the Applicant intends to convert the Application Site into an EV charging station to provide EV charging facilities for electric taxis. It is considered a preferred location for the proposed development.

1.2 Background of the Application

In response of combating global climate change, electrification of transportation system has become a global trend. EVs are considered a preferable choice to conventional vehicles with no tailpipe emissions of air pollutants, which can help improve air quality and reduce greenhouse gas emissions.

To drive Hong Kong towards the vision of “Zero Carbon Emissions ● Clean Air ● Smart City”, the Government has announced the “Hong Kong Roadmap on Popularisation of EV” on 17 March 2021, which outlines the long-term objectives to achieve zero vehicular emissions before 2050 and promote wider or eventual full adoption of EVs in Hong Kong. The roadmap includes measures such as phasing out conventional fuel-propelled vehicles by 2035, expanding EV charging network in both private and public sectors, enhancing education and training programs for maintenance services and introducing trials of electric commercial vehicles for public transportations.

On 10 December 2024, the Government has also announced the “Green Transformation Roadmap of Public Buses and Taxis” to expand the adoption of EVs in public transportation with introduction of 3,000 electric taxis by mid-2026. The Government has also highlighted a multi-pronged approach in promoting the expansion of EV charging facilities, so as to gradually develop a territory-wide network for ensuring sufficient and convenient charging support for suiting the needs of various EVs.

As at February 2025, the number of EVs account for about 12.6% of the total number of vehicles in Hong Kong, and is expected to increase with the ongoing promotion and campaigns by the Government. In the Chief Executive’s 2024 Policy Address, the Government has also reiterated their efforts to expand the charging network for EVs with new scheme to encourage private sector to install quick-charging facilities. It is targeted to have a total of 3,000 quick chargers installed by 2030.

Additionally, the Government has announced 5 selected taxi fleet operators to enhance taxi service quality and reform the taxi trade on 3 March 2025, in which all selected operators will provide electric taxis to promote green transport in Hong Kong.

In support of the Government's initiatives, the Applicant, in partnership with one of the selected taxi fleets (see endorsement letter from the Transport Department at **Appendix 1**), intends to convert the Application Site into an EV charging station to provide charging facilities for electric taxi. All chargers will be direct current (DC) chargers with output power of 120kW to increase the charging efficiency.

1.3 Organization of the Planning Statement

This planning statement is divided into 6 chapters. Chapter 1 is an introduction outlining the above background of the planning application. Chapter 2 will then illustrate the site context and land status followed by Chapter 3 which describes the planning context in details. Chapter 4 gives the particulars of the proposed development. Detailed accounts of planning justifications of the proposed development are presented in Chapter 5. The planning statement finally concludes with a summary in Chapter 6.

2. Site Context

2.1 The Application Site and Its Existing Condition

The Application Site, with an area of about **954 sq.m. (including Government land of about 381 sq.m.)**, is located next to Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T. The Application Site is a piece of flat land, partly hard-paved and currently being used as vehicle repair workshops. The Application Site is partially fenced off by chain-link fence of about 2.5m high (see **Site Photos**). All the existing temporary structures will be demolished.

2.2 Surrounding Land Uses

The surrounding areas are predominantly rural in character and comprise of village houses, vehicle repair workshops, parking of vehicles, warehouses / storage yards and vacant land. To the east and further east are Kwan Tei Children's Playground and the village settlements of Kwan Tei. To the further southeast is a temporary public vehicle park (Application No. A/NE-LYT/815) within the same "AGR" zone approved by the Board on 26.1.2024. To the north across Sha Tau Kok Road – Lung Yeuk Tau are a series of vehicle repair workshops and warehouses / storage yards within the "Open Storage" ("OS") zone. To the west is the industrial premises of Tung Chun Soy Sauce & Canned Food Company Limited. To the south is vacant land intermixed with temporary structures.

2.3 Accessibility

The Application Site is directly accessible via Sha Tak Kok Road – Lung Yeuk Tau. The existing entrance to the Application Site is about 9m wide which is located to the north. A plan showing the vehicular access to the Application Site is provided at **Figure 3A**.

2.4 Land Status

According to the records of the Land Registry, the subject lot is held under Block Government Lease and is demised as agricultural use with lease term for 75 years, from 1.7.1898 and is renewable for a further term of 24 years.

The subject lot is an old schedule agricultural lot. The Applicant will apply for a Short Term Waiver (STW) for the proposed structures and a Short Term Tenancy (STT) for the use of the Government land to the Lands Department upon obtaining planning approval for this application.

3. Planning Context

3.1 Outline Zoning Plan

The Application Site currently falls within an area zoned “Agriculture” (“AGR”) on the Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan (OZP) No. S/NE-LYT/19 gazetted on 16.12.2022 (see **Figure 1A**).

The planning intention of “AGR” zone is “primarily to retain and safeguard good quality agricultural land/farm/fish ponds for agricultural purposes. It is also intended to retain fallow arable land with good potential for rehabilitation for cultivation and other agricultural purposes.”

According to the covering Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires planning permission from the Board, notwithstanding that the use or development is not provided for under the Notes of the OZP.

3.2 Previous Application

Part of the Application Site is subject to 1 previously approved planning application and the details are as follows:-

Application No.	Applied Use	Location	TPB's Decision (Date)
A/NE-LYT/161	Cover Structures for Vehicle and Construction Machinery Repair Workshop	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land	Approved with Conditions (31.7.1998)

3.3 Similar Application

There is no similar application for EV charging station use within the same “AGR” zone under the Lung Yeuk Tau and Kwan Tei South OZP.

4. Development Proposal

4.1 Site Configuration and Layout

The Applicant proposes to use the Application Site for a temporary EV charging station for a period of 3 years. The proposed development intends to provide EV charging facilities for electric taxis of a taxi fleet.

The Application Site has an area of about **954 sq.m.** The proposed EV charging station will provide **17** parking spaces (2.5m x 5m) for electric taxis (see Layout Plan at **Figure 4A**). 12 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) ultra fast chargers with output power of 120kW. The remaining **5** parking spaces will be designated as parking / waiting spaces for the charging facilities and will be free of charge. **The application boundary has been taken into consideration of the existing sewerage underground pipes and manholes in the vicinity and sufficient setback distance has been provided (see Figure 4B).**

Two 1-storey (height: about 3m) containers for placements of EV chargers, each with a floor area of about 29 sq.m., will be situated at the southern boundary of the Application Site. Each container will provide 6 EV chargers for electric taxis (see **Figure 5**). A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices.

The Application Site will be fenced off by 2.5m high chain-link fence on all sides. Only electric taxis with valid licenses issued under the Road Traffic (Registration and Licensing of Vehicles) Regulations are allowed to be parked on the Application Site.

The key development parameters of the application are shown below:-

Applied Use	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years
Site Area	About 954 sq.m. (including 381 sq.m. of Government land)
Total Floor Area (Non-domestic)	About 111 sq.m.
No. of Structures	3 - 1 proposed transformer / switch room - 2 proposed containers for EV chargers
Height of Structures	About 3 – 3.5m (1 storey)
No. of Parking Spaces	17 Nos. (2.5m x 5m) - 12 EV charging spaces for electric taxis - 5 parking / waiting spaces for electric taxis

4.2 Site Operations

The proposed development will only provide parking and EV charging facilities for electric taxis. All EV chargers onsite will be quick chargers that will provide electric taxis 600km of mileage when charged for about 30 minutes. The proposed hours of operation at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays). The Applicant also confirms that there will be no repairing, car washing, paint spraying and other workshop activities conducted at the Application Site. No land filling work will be conducted at the Application Site.

The Applicant will collaborate with Sino Development (International) Company Limited (新科發展(國際)有限公司), one of the 5 selected taxi fleet operators by the Government, to provide various quick EV charging facilities in various locations in Hong Kong (see **Appendix 1**). The fee arrangement will be similar to other EV charging station in the market, in which users will be charged at time basis as they go. A mobile application catered for electric taxi drivers will be adopted, which will include the payment platform for EV charging and provide real-time availability of EV charging spaces at the EV charging stations managed by the Applicant.

4.3 Traffic Arrangement and Traffic Impact Assessment

The Application Site can be accessed via Sha Tau Kok Road – Lung Yeuk Tau (see **Figure 3**). The Applicant will maintain the existing ingress/egress point at the northern boundary of about 9m wide. A Traffic Impact Assessment (TIA) (see **Appendix 2**) is submitted to assess the potential traffic impact on the existing and future road network.

To analyze the existing traffic conditions, a traffic count survey was conducted on 22 May 2025 at the nearby critical links and junctions. The peak hour was identified to be between 1630 - 1730. Based on the existing traffic flows, all concerned junctions were performing satisfactorily and all concerned road sections were operating with spare capacity during the peak hours.

A swept path analysis is also included in the TIA to demonstrate that satisfactory manoeuvring of vehicles entering to and existing from the Application Site and manoeuvring within the Application Site. There will be no difficulties in internal traffic circulation sense as sufficient spaces for vehicle manoeuvring have been reserved within the Application Site such that no vehicle has to queue back to or reverse onto/from Sha Tau Kok Road – Lung Yeuk Tau.

As identified in the TIA report, the peak traffic generation of the proposed development will be either

between 1500 – 1700 or after midnight, before the shift changes of the electric taxi drivers. The proposed development is expected to generate a two-way traffic of 48 taxis/hour during the peak hour. By analyzing the Year 2028 Design Flows, which were derived from applying a nominal growth to the existing traffic flow plus the additional traffic generated and attracted by the proposed development, both the junction and link capacity assessments show that the concerned junction and road links would perform satisfactorily during the peak hour. Based on the assessment result, it can be concluded that the proposed development will not induce additional adverse traffic impact on the surrounding road network and is considered acceptable from traffic engineering point of view.

The mobile application adopted by the Applicant will provide real-time availability of EV charging space and facilitate electric taxi drivers to locate the available EV chargers. The drivers will be directed to the nearest alternative sites if all the EV chargers in the proposed EV charging station are in use. In addition, since quick chargers will be used in the Application Site, the charging time will be much shorter than standard chargers, which allow high turnover rate and cut down waiting time. **5** parking / waiting spaces have been reserved in the Application Site to allow drivers to wait for EV charging space. Therefore, no electric taxi will be queuing outside the Application Site along Sha Tau Kok Road – Lung Yeuk Tau.

4.4 Drainage Proposal

Drainage facilities including 375mm peripheral U-channels and catchpit with trap are proposed to collect the surface runoff and divert them to the existing watercourse to the south. A 100mm high gap will also be proposed at the bottom of the security hoarding/fence wall along the site boundary to ensure no surface runoff from the surroundings flowing onto the Application Site to be obstructed. A drainage proposal (see **Appendix 3**) is submitted to show the proposed drainage design. The Applicant will provide the drainage facilities to the satisfaction of Drainage Services Department.

4.5 Landscape

Majority of the Application Site is occupied by the existing vehicle repairing workshops and **there are 6 existing trees are found at vacant land within the Application Site (see Figure 6). While all of the existing trees (T1 to T6) are of a common species, not old or valuable trees, in fair or poor condition, and in conflict with the proposed waiting/manoeuvring spaces and fencing, it is proposed to fell these trees.** Meanwhile, existing trees of various species are found immediate outside the eastern and southern boundaries. These existing trees outside the site boundary will not be disturbed by the Applicant and would act as the natural hedges bounding the edge of the development.

The Application Site is located within an area of rural character intermixed with village settlements, vehicle repair workshops, warehouse / storage yards and vacant / unused land, in which private vehicles of the villagers / residents are commonly parked at vacant Government land. Since the proposed development intends to provide EV charging facilities for electric taxis only, it is considered not incompatible with the landscape character of the surrounding areas.

4.6 Fire Service Installations

To minimize the fire hazard, the Applicant will provide fire service installations (FSIs) with Firemen's Emergency Switch to the satisfaction of the Fire Services Department.

4.7 Environmental Consideration

The nature of the proposed use will merely involve provision of car parking space with EV charging facilities. To minimize the possible environmental nuisance, the Applicant will follow the 'Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites' and undertake the following mitigation measures:-

- (a) No more than **17 electric taxis** will be allowed at the Application Site at the same time;
- (b) Only electric taxis will be allowed to enter or park at the Application Site;
- (c) No repairing, car washing, paint spraying and other workshop activities will be conducted at the Application Site;
- (d) No land filling works will be conducted at the Application Site; and
- (e) The Application Site will be fenced off by 2.5m high chain-link fence on along the site boundary.

5. Planning Justifications

5.1 In Line with Government Policy to Promote EV Usage

The proposed EV charging station would provide car parking spaces with quick EV charging facilities for electric taxis. It aligns with the Government's initiative to expand the charging network for EVs in Hong Kong, especially in meeting the target of installing 3,000 quick chargers by 2030. With the introduction of 3,000 electric taxis expected in mid-2026, the proposed development can also meet the increasing demand for EV charging spaces catered for electric taxis in Hong Kong. Additionally, the Applicant has received support from the Transport Department to collaborate with one of the selected taxi fleet operators to provide EV charging facilities in Hong Kong (see **Appendix 1**). Therefore, the proposed development is considered in line with Government policy to promote the wider use of EVs and support the operations of electric commercial vehicles and achieve the aforesaid target.

5.2 Meeting the Demand for Quick EV Charging Facilities

In view of the emerging popularity of commercial EVs with the Government's initiative, the demand for EV charging space for commercial EVs will increase as well. At present, the nearest EV charging facilities in the locality is located within Queens Hill Estate, which only provide standard chargers for private vehicles and are considered too slow for electric taxis. Since the proposed EV charging station will provide EV charging spaces with quick chargers, it can meet the demand of electric taxis for quick charging EV facilities.

5.3 Not Contravene the Planning Intention of "AGR" Zone

The Application Site falls within an area zoned "AGR" on the Lung Yeuk Tau and Kwan Tei South OZP No. S/NE-LYT/19. Having considered the Application Site has been occupied by the existing vehicle repair workshops for a long time, it has altered the natural landscape of the area with majority of the Application Site being paved. In addition, in view of the surrounding developments of the Application Site, which is mainly village houses, vehicle repair workshops, warehouse / storage yards and vacant land, the planning intention of the "AGR" zone has not been materialized at the current capacity. Due to the temporary nature of the Application, the proposed development will not jeopardize the long-term planning intention of the "AGR" zone.

5.4 Not Incompatible with Surrounding Land Uses

The proposed development is considered not incompatible with the surrounding land uses. The surrounding areas comprise of village houses, vehicle repair workshops, parking of vehicles, warehouses / storage yards, soy sauce factory and vacant land. Since the proposed development is intended to serve electric taxis, the proposed development could co-exist well with the existing surrounding land uses without inflicting any environmental impacts. As the proposed development is considered not incompatible with other developments/facilities in the adjacent areas in terms of nature and scale of use, approval of the application would therefore not result in any interface problems with the surrounding areas.

5.5 Optimization of Valuable Land Resources

The Application Site is considered a suitable and preferred venue for the proposed development. The Application Site is a piece of flat land with majority of the land being paved, in which the conversion into the proposed EV charging station will be smooth. The proposed development will also improve the currently under-utilized adjoining Government land, in which the Applicant will manage the site and therefore improve the existing condition. In addition, the Application Site is directly accessible to Sha Tau Kok Road – Lung Yeuk Tau, which is connected to strategic road networks to different parts of New Territories, electric taxis could easily access and leave the site after charging. Meanwhile, the Application Site is situated close to densely populated areas such as Sheung Shui and Fanling and future new towns such as Kwu Tung and Ping Che. The provision of EV charging spaces, especially catered for electric taxis at the Application Site will be able to meet the growing demand for EV charging facilities in these areas. The proposed development is therefore considered fully commensurate with its local geographical settings and ideal to attain utmost land use maximization.

5.6 Will not Hinder Future Development of New Territories North New Town

The Application Site falls within the project boundary of New Territories North New Town. The Applicant understands that the proposed development will only be temporary and agrees to move out when the Application Site is resumed by the government for development of the New Town in the future. Due to the temporary nature of the Application, the proposed development will not hinder the future New Town development at the Application Site.

5.7 No Adverse Impacts on the Surrounding Areas

Owing to the nature and size of the proposed development, no adverse impacts are anticipated on the surrounding areas, as discussed below:

Traffic

The design of the proposed EV charging station has taken into account of vehicular circulation and manoeuvring. The Applicant will undertake traffic management measures to ensure pedestrian safety. Waiting spaces have been reserved within the Application Site to prevent any electric taxis from queuing outside the Application Site. Based on the submitted TIA (see **Appendix 2**), it is concluded that the proposed development will not induce significant traffic impact on the surrounding areas and is considered acceptable from traffic engineering point of view.

Drainage

Drainage facilities will be provided within the Application Site for this application, in which surface runoff will be effectively collected from and discharged out of the Application Site. The Applicant has accordingly submitted a drainage proposal (see **Appendix 3**) to elaborate. Therefore, no adverse drainage impact on the surrounding areas is anticipated.

Environment

As the proposed EV charging station will only be used for parking and EV charging facilities for electric taxis, it would not generate adverse noise, air pollution or visual intrusion. In addition, there will be no repairing, car washing, paint spraying and other workshop activities carried out on the premises. Therefore, the proposed development will not generate any pollution or adverse environmental impacts on the surrounding areas.

6. Conclusion

The Application Site falls within “AGR” zone on the Lung Yeuk Tau and Kwan Tei South OZP No. S/NE-LYT/19. The Application Site is currently used as vehicle repair workshops and the Applicant intends to convert the Application Site into an EV charging station for electric taxis. In view of the site location, the existing site conditions and surrounding land uses, the proposed development is considered not incompatible with surrounding land uses with the planning intention of “AGR” zone not being materialized. Given the temporary nature of the proposed development, it will therefore not contravene the long-term planning intention of the “AGR” zone.

In contrast, the Application Site is considered a suitable venue for the proposed development with its geographical settings and ideal to attain utmost land use maximization. Additionally, the proposed development will meet local demand for EV charging facilities, especially with provision of quick chargers for electric taxis. It therefore aligns with Government policy to expand EV charging networks and promote wider EVs usage in Hong Kong.

Meanwhile, the temporary nature of the proposed development will also not hinder future development of New Territories North New Town. It is anticipated that the proposed development will not generate adverse impacts to the surrounding environment, and the technical concerns of relevant government departments could be addressed through the implementation of approval conditions.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 3 years.

Lawson David & Sung Surveyors Limited
December 2025

Figures

- Figure 1A Location Plan
- Figure 2A Lot Index Plan
- Figure 3A Vehicular Access to the Site
- Figure 4A Indicative Layout Plan
- Figure 4B Layout Plan showing the Setback Distance from the
Existing Sewerage Underground Pipes and Manholes
- Figure 5 Photos of Electric Charging Device
- Figure 6 Existing Trees within the Application Site

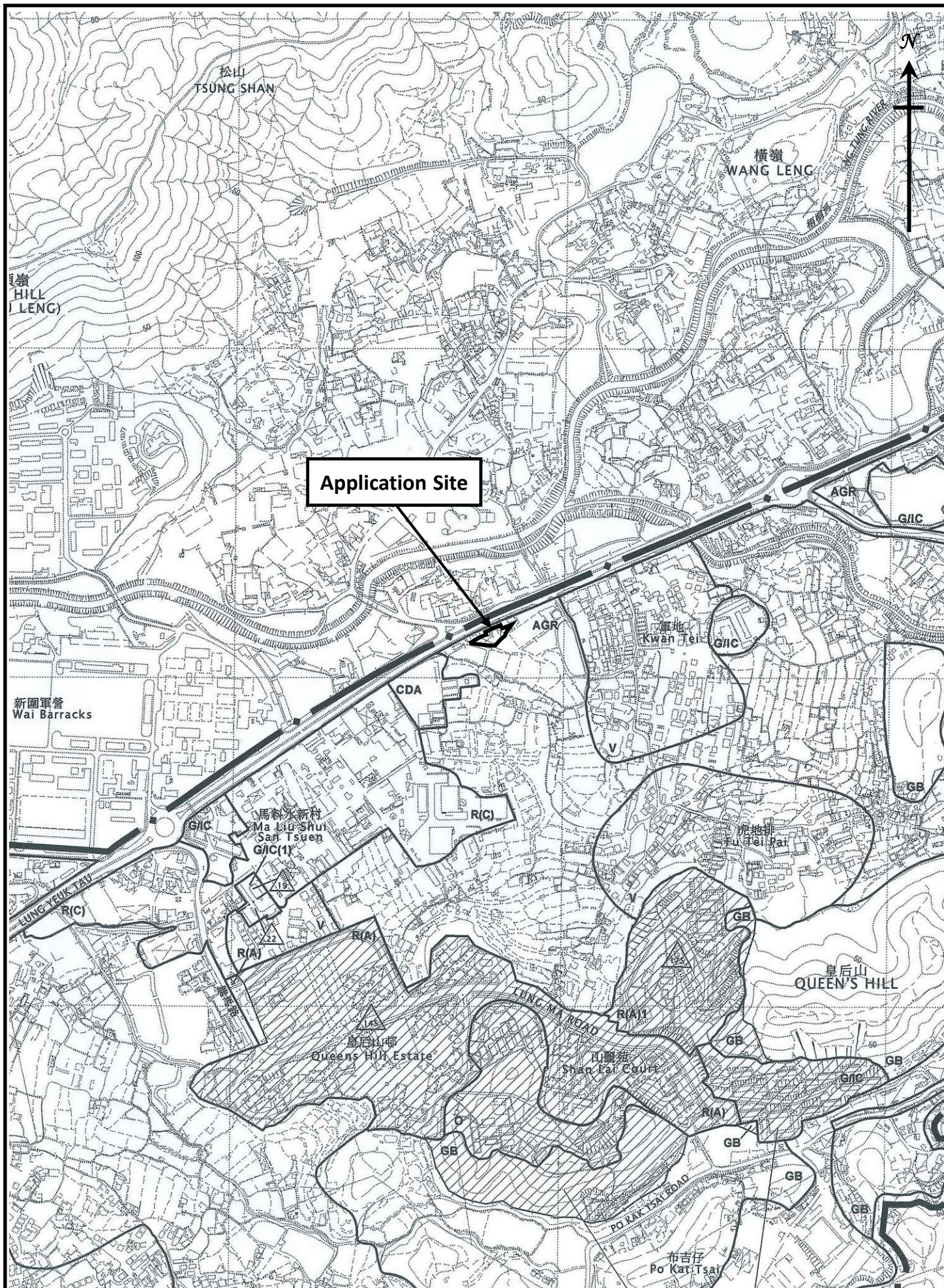
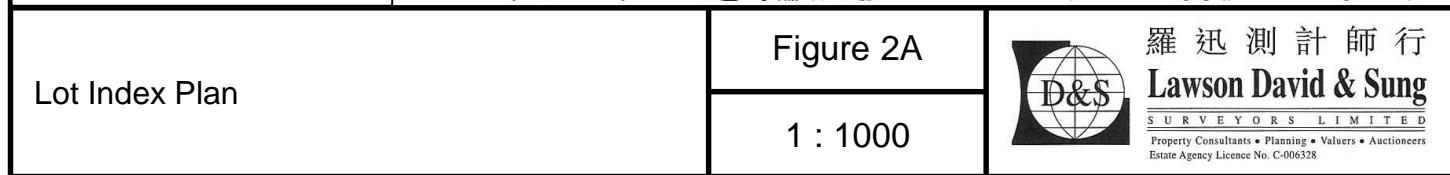


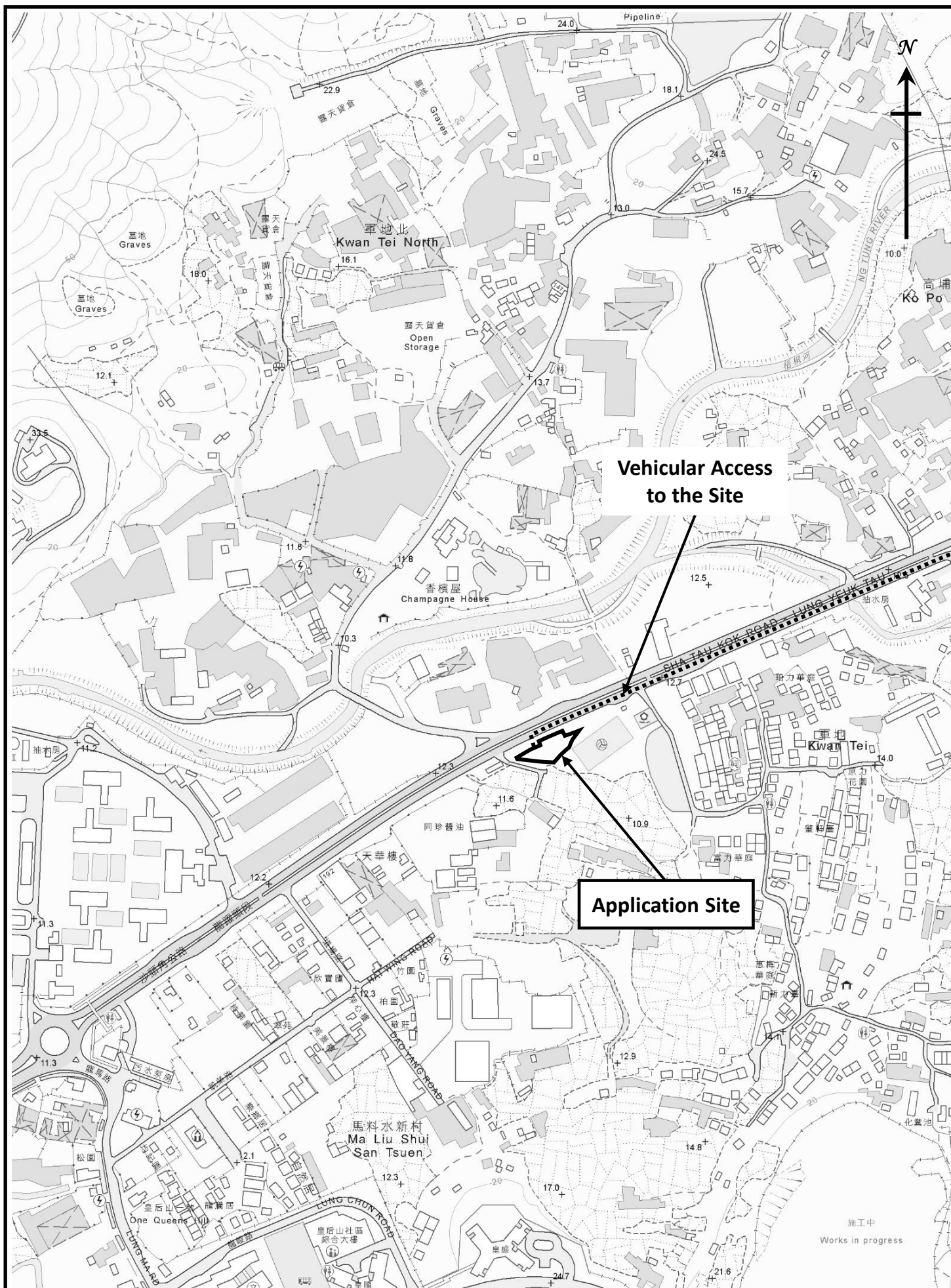
Figure 1A

1 : 7500



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Vehicular Access to the Application Site

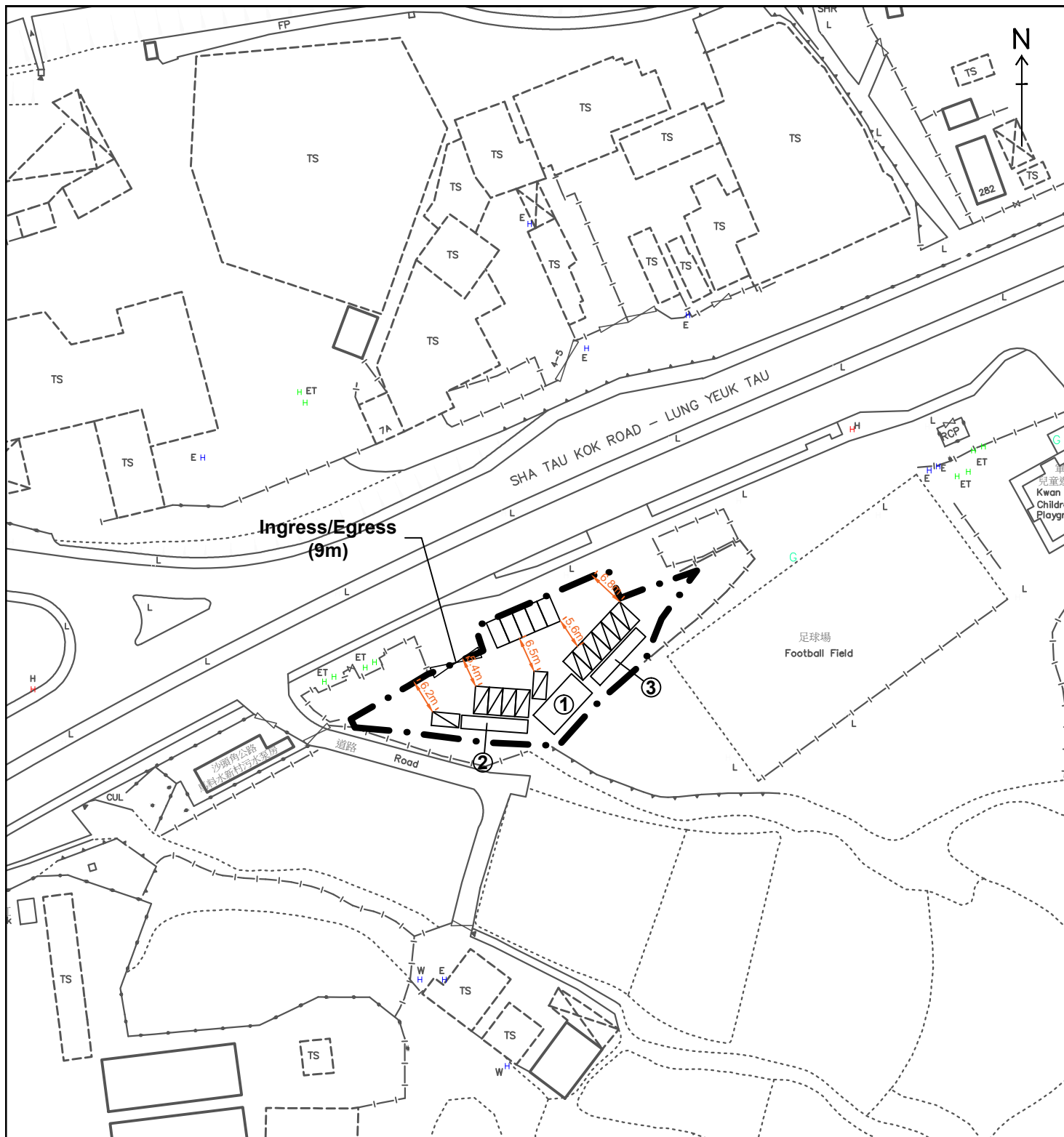
Figure 3A

1 : 5000



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Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- • — Application Site (Area: about 954m²)
- ▨ EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)

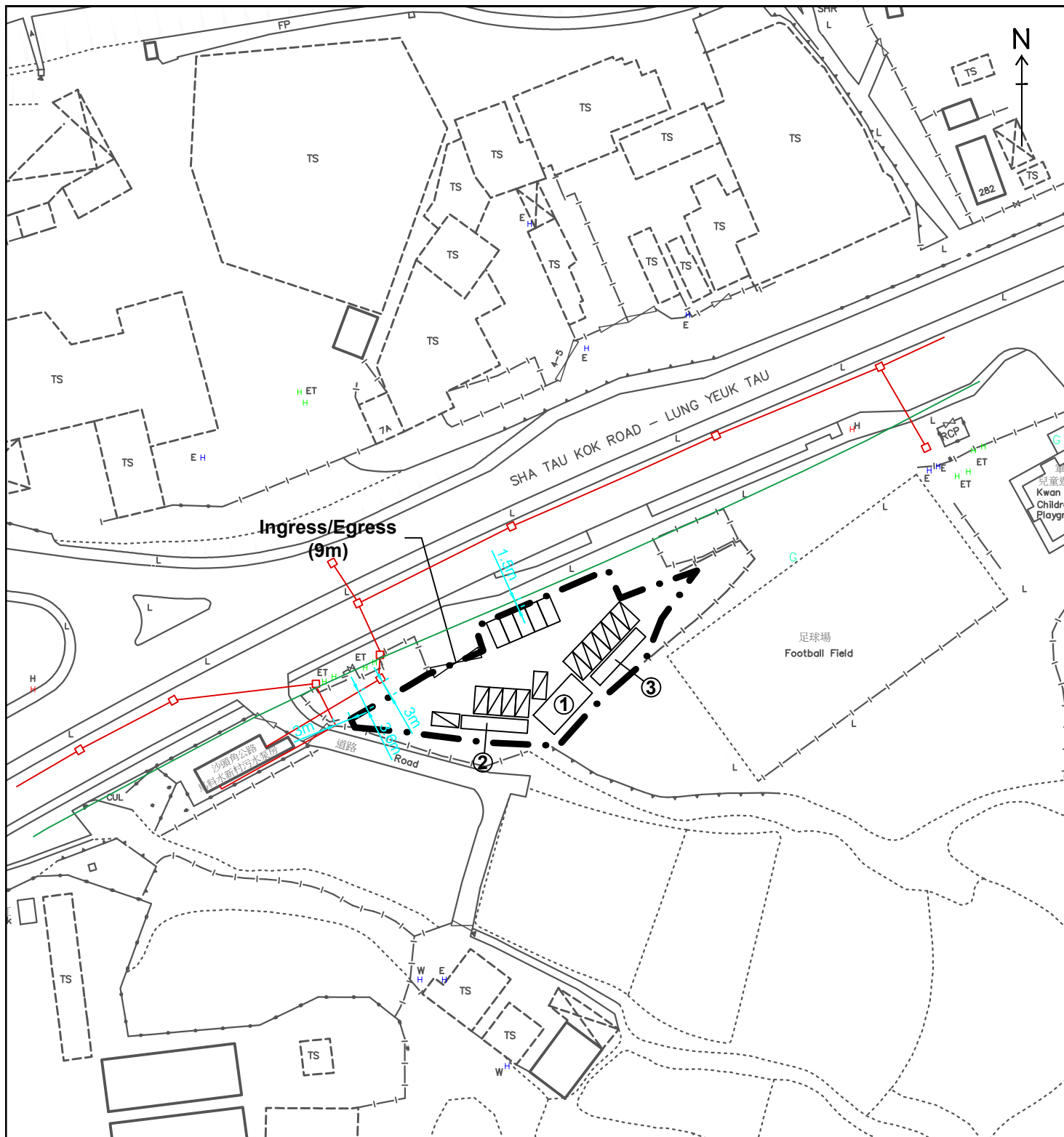
Layout Plan

Figure 4A

Scale 1:1000



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Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Transformer Room and Switch Room	1	53	3.5
2	Container for EV Chargers	1	29	3.0
3	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 954m²)
- EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)
- Sewerage Underground Pipes and Manholes
- Water Mains

Layout Plan

Figure 4B

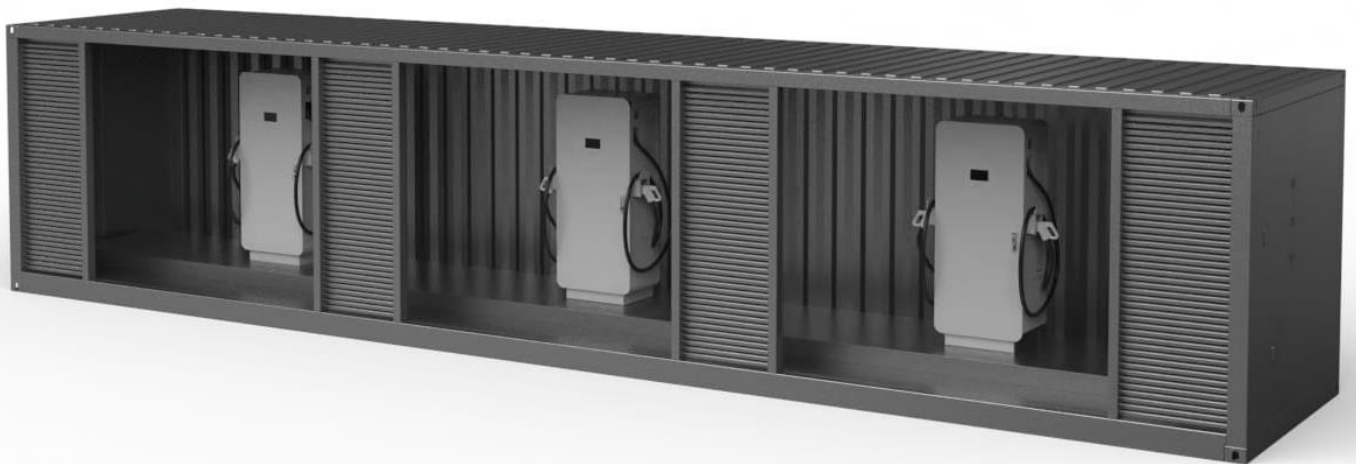
Scale 1:1000



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Container for EV Chargers



EV Charging Device for Electric Taxis

For Reference Only

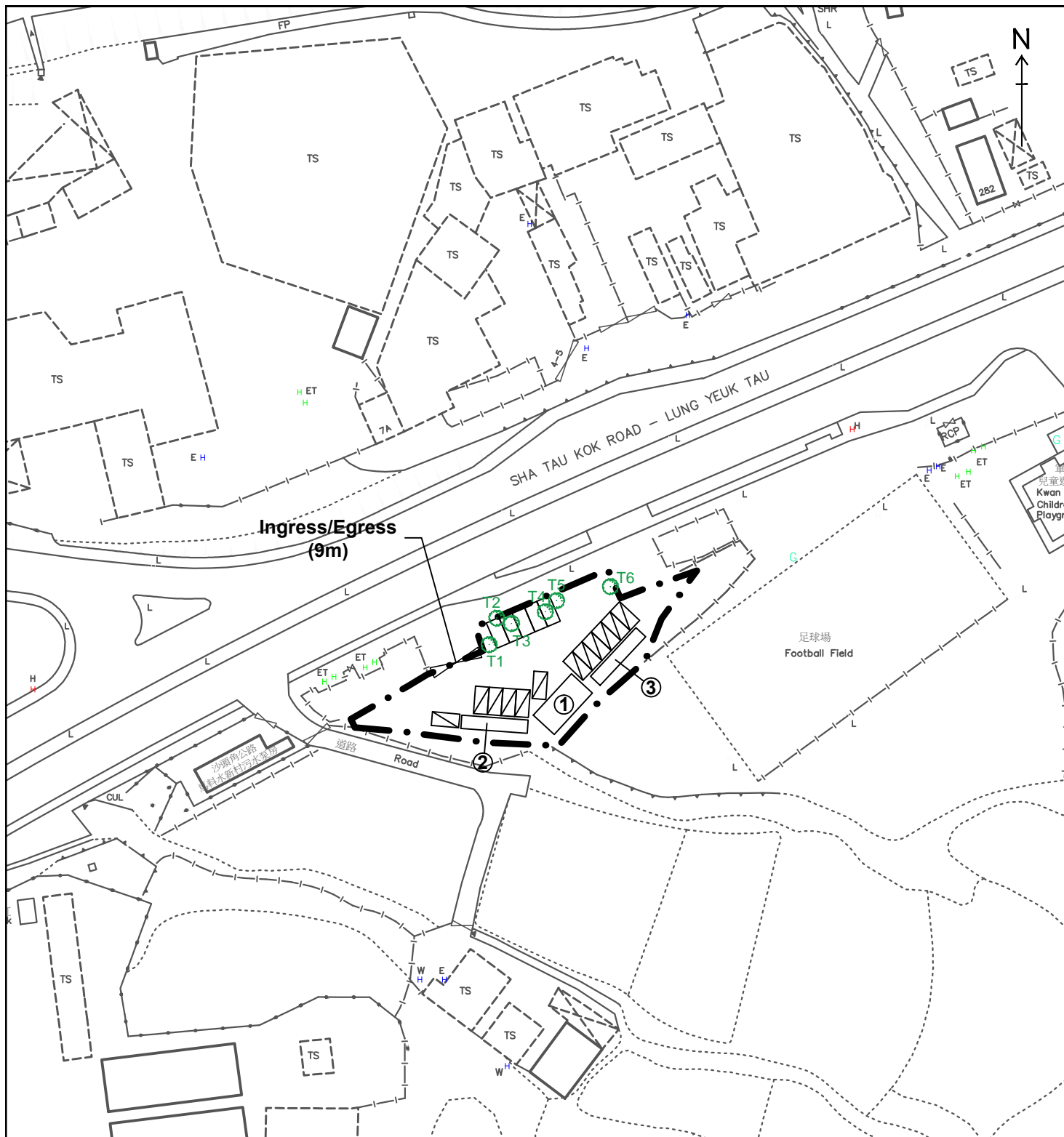
Photos of Electric Charging Device

Figure 5

Not to Scale



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Existing Trees within the Application Site to be Felled

Legend:

- Application Site (Area: about 954m²)
- EV Charing Spaces for Electric Taxi (12 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (5 nos.)

T1: Aleurites moluccana
 T2: Bombax ceiba
 T3: Aleurites moluccana
 T4: Bombax ceiba
 T5: Terminalia catappa
 T6: Macaranga tanarius
 Height: about 3m-5m

Existing Trees within the Application Site

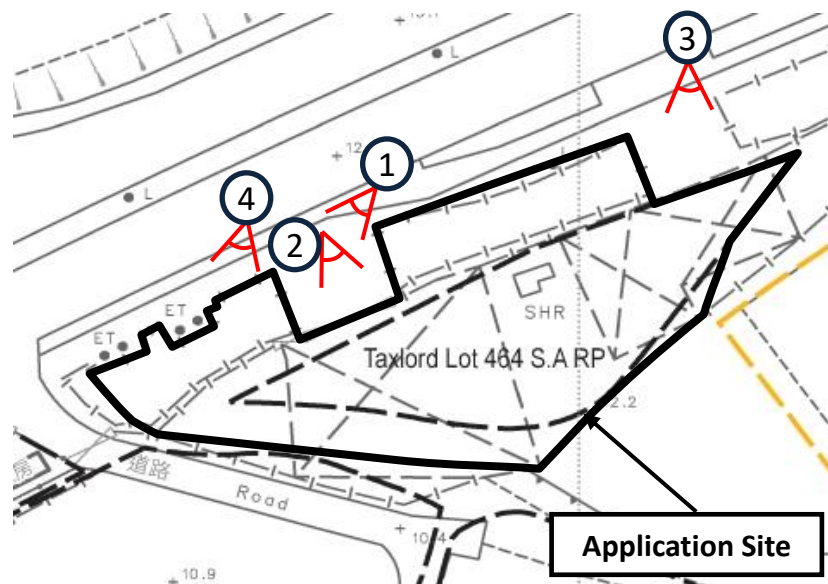
Figure 6

Scale 1:1000





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Site Photos



Legend:

-  Viewpoint of the Photo
-  Photo No.



Existing Condition of the Application Site

Site Photos

For Official Use Only 請勿填寫此欄	Application No. 申請編號	
	Date Received 收到日期	

1. The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.
申請人須把填妥的申請表格及其他支持申請的文件 (倘有), 送交香港北角渣華道 333 號北角政府合署 15 樓城市規劃委員會(下稱「委員會」)秘書收。
2. Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.tpb.gov.hk/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).
請先細閱《申請須知》的資料單張, 然後填寫此表格。該份文件可從委員會的網頁下載 (網址: <http://www.tpb.gov.hk/>), 亦可向委員會秘書處 (香港北角渣華道 333 號北角政府合署 15 樓 - 電話: 2231 4810 或 2231 4835) 及規劃署的規劃資料查詢處(熱線: 2231 5000) (香港北角渣華道 333 號北角政府合署 17 樓及新界沙田上禾輦路 1 號沙田政府合署 14 樓)索取。
3. This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.
此表格可從委員會的網頁下載, 亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全, 委員會可拒絕處理有關申請。

1. Name of Applicant 申請人姓名/名稱
<input type="checkbox"/> Mr. 先生 / <input type="checkbox"/> Mrs. 夫人 / <input type="checkbox"/> Miss 小姐 / <input type="checkbox"/> Ms. 女士 / <input checked="" type="checkbox"/> Company 公司 / <input type="checkbox"/> Organisation 機構)
Wealth Generation International Limited (新世代國際有限公司)

2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱 (如適用)
<input type="checkbox"/> Mr. 先生 / <input type="checkbox"/> Mrs. 夫人 / <input type="checkbox"/> Miss 小姐 / <input type="checkbox"/> Ms. 女士 / <input checked="" type="checkbox"/> Company 公司 / <input type="checkbox"/> Organisation 機構)
Lawson David & Sung Surveyors Limited (羅迅測計師行有限公司)

3. Application Site 申請地點	
(a) Full address / location / demarcation district and lot number (if applicable) 詳細地址/地點/丈量約份及地段號碼 (如適用)	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.
(b) Site area and/or gross floor area involved 涉及的地盤面積及/或總樓面面積	<input checked="" type="checkbox"/> Site area 地盤面積 954 sq.m 平方米 <input checked="" type="checkbox"/> About 約 <input checked="" type="checkbox"/> Gross floor area 總樓面面積 111 sq.m 平方米 <input checked="" type="checkbox"/> About 約
(c) Area of Government land included (if any) 所包括的政府土地面積 (倘有)	..... 381 sq.m 平方米 <input checked="" type="checkbox"/> About 約

6. Type(s) of Application 申請類別	
(A) Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas 位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展 (For Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas, please proceed to Part (B)) (如屬位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期，請填寫(B)部分)	
(a) Proposed use(s)/development 擬議用途/發展	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years (Please illustrate the details of the proposal on a layout plan) (請用平面圖說明擬議詳情)
(b) Effective period of permission applied for 申請的許可有效期	<input checked="" type="checkbox"/> year(s) 年 3 <input type="checkbox"/> month(s) 個月
(c) Development Schedule 發展細節表 Proposed uncovered land area 擬議露天土地面積 843 .....sq.m <input checked="" type="checkbox"/> About 約 Proposed covered land area 擬議有上蓋土地面積 111 .....sq.m <input checked="" type="checkbox"/> About 約 Proposed number of buildings/structures 擬議建築物／構築物數目 3 Proposed domestic floor area 擬議住用樓面面積 NA .....sq.m <input type="checkbox"/> About 約 Proposed non-domestic floor area 擬議非住用樓面面積 111 .....sq.m <input checked="" type="checkbox"/> About 約 Proposed gross floor area 擬議總樓面面積 111 .....sq.m <input checked="" type="checkbox"/> About 約	
Proposed height and use(s) of different floors of buildings/structures (if applicable) 建築物/構築物的擬議高度及不同樓層的擬議用途 (如適用) (Please use separate sheets if the space below is insufficient) (如以下空間不足，請另頁說明) <u>One 1-storey Transformer / Switch Room (Height: About 3.5m)</u> <u>Two 1-storey Containers for EV Chargers (Height: About 3m)</u>	
Proposed number of car parking spaces by types 不同種類停車位的擬議數目 Private Car Parking Spaces 私家車車位 NA Motorcycle Parking Spaces 電單車車位 NA Light Goods Vehicle Parking Spaces 輕型貨車泊車位 NA Medium Goods Vehicle Parking Spaces 中型貨車泊車位 NA Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 NA Others (Please Specify) 其他 (請列明) 17 for Electric Taxi (2.5m x 5m)	
Proposed number of loading/unloading spaces 上落客貨車位的擬議數目 Taxi Spaces 的士車位 NA Coach Spaces 旅遊巴車位 NA Light Goods Vehicle Spaces 輕型貨車車位 NA Medium Goods Vehicle Spaces 中型貨車車位 NA Heavy Goods Vehicle Spaces 重型貨車車位 NA Others (Please Specify) 其他 (請列明) NA	

Gist of Application 申請摘要 (Please provide details in both English and Chinese <u>as far as possible</u> . This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information.) (請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)	
Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)
Location/address 位置／地址	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.
Site area 地盤面積	<div style="text-align: right;"> 954 sq. m 平方米 <input checked="" type="checkbox"/> About 約 </div> <div> (includes Government land of 包括政府土地 381 sq. m 平方米 <input checked="" type="checkbox"/> About 約) </div>
Plan 圖則	Approved Lung Yeuk Tau and Kwan Tei South Outline Zoning Plan No. S/NE-LYT/19
Zoning 地帶	"Agriculture" ("AGR")
Type of Application 申請類別	<div> <input checked="" type="checkbox"/> Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區的臨時用途/發展為期 <div style="margin-left: 40px;"> <input checked="" type="checkbox"/> Year(s) 年 <u> 3 </u> <input type="checkbox"/> Month(s) 月 _____ </div> </div> <div style="margin-top: 10px;"> <input type="checkbox"/> Renewal of Planning Approval for Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期為期 <div style="margin-left: 40px;"> <input type="checkbox"/> Year(s) 年 _____ <input type="checkbox"/> Month(s) 月 _____ </div> </div>
Applied use/development 申請用途/發展	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi) for a Period of 3 Years

(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	111 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.1 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用	NA	
	Non-domestic 非住用	3	
(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	NA <input type="checkbox"/> (Not more than 不多於) m 米	
		NA <input type="checkbox"/> (Not more than 不多於) Storeys(s) 層	
	Non-domestic 非住用	3 - 3.5 <input checked="" type="checkbox"/> (Not more than 不多於) m 米	
		1 <input checked="" type="checkbox"/> (Not more than 不多於) Storeys(s) 層	
(iv) Site coverage 上蓋面積	12 % <input checked="" type="checkbox"/> About 約		
(v) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數 Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) Electric Taxi _____ _____		17 NA NA NA NA NA 17 (2.5m x 5m)
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數 Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) _____ _____		NA NA NA NA NA NA

Previous S.16 Application

Approved Application

Application No.	Uses/ Development	Date of Consideration
A/NE-LYT/161	Cover Structures for Vehicle and Construction Machinery Repair Workshop	31.7.1998

Government Departments' General Comments

1. Traffic

Comments of the Commissioner for Transport (C for T):

- no comment on the application; and
- her advisory comments are at **Appendix IV**.

Comments of the Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD):

- no comment on the application from highways maintenance perspective; and
- his advisory comments are at **Appendix IV**.

2. Environment

Comments of the Secretary for Environment and Ecology (SEE):

- it is noted that there will be 17 parking spaces, and 12 of them will be provided with electric vehicle (EV) charging facilities (with at least 120kW EV charging simultaneously);
- to echo with the latest version of the Chapter 8 of Hong Kong Planning Standards and Guidelines (HKPSG) about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the applicant is suggested to comply with the relevant requirement of HKPSG, i.e., EV chargers with output power of not less than 7kW should be installed in all parking spaces for private cars, light goods vehicles and motorcycles of the application site (the Site); and
- his advisory comments are at **Appendix IV**.

Comments of the Director of Environmental Protection (DEP):

- no objection to the application from environmental planning perspective;
- there was no substantiated complaint case related to the Site in the past three years; and
- his advisory comments are at **Appendix IV**.

3. Drainage

Comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD):

- no objection to the application from public drainage perspective;
- no comment on the drainage proposal submitted by the applicant;
- should the application be approved, approval conditions should be included to request the applicant to implement the accepted drainage proposal for the Site to ensure that the

proposed use will not cause adverse drainage impact on the adjacent area, and the implemented drainage facilities at the Site shall be maintained at all times during the planning approval period; and

- her advisory comments are at **Appendix IV**.

4. **Fire Safety**

Comments of the Director of Fire Services (D of FS):

- no in-principle objection to the application subject to fire service installations being provided to his satisfaction; and
- his advisory comments are at **Appendix IV**.

5. **Landscape**

Comments of the Chief Town Planner/Urban Design and Landscape, Planning Department (CTP/UD&L, PlanD):

- no adverse comment on the application from the landscape planning perspective;
- the Site is located in an area of rural landscape character comprising fallow farmlands, vacant land or unused land covered by tree groups or vegetation area and a few village houses/domestic structures. The proposed use is not entirely incompatible with the landscape character in the surrounding area of the Site;
- the Site is currently occupied by temporary structures with some trees (**Plans A-4a to A-4c**). No old and valuable trees are found within the Site. It is noted that all existing trees are common species, and are proposed to be felled. Significant adverse impact on the landscape character and the existing landscape resources within the Site arising from the proposed use is not anticipated; and
- her advisory comments are at **Appendix IV**.

6. **Water Supplies**

Comments of the Chief Engineer/Construction, Water Supplies Department (CE/C, WSD):

- no objection to the application from water supply system planning perspective, noting that diversion of the existing water main is not required; and
- his advisory comments are at **Appendix IV**.

7. **Electricity and Town Gas Safety**

Comments of the Director of Electrical and Mechanical Services (DEMS):

- no adverse comment on the application from electricity and town gas safety perspectives; and

- his advisory comments are at **Appendix IV**.

8. Project Interface

Comments of the Project Manager (North), Civil Engineering and Development Department (PM(N), CEDD):

- no adverse comment on the application from project interface perspective; and
- his advisory comments are at **Appendix IV**.

9. Building Matters

Comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):

- no objection to the application; and
- his advisory comments are at **Appendix IV**.

10. Other Departments

The following government departments have no objection to/no comment on the application:

- (a) District Officer (North), Home Affairs Department (DO(N), HAD); and
- (b) Commissioner of Police (C of P).

Recommended Advisory Clauses

- (a) to resolve any land issues relating to the proposed use with the concerned owner(s) of the application site (the Site);
- (b) the permission is given to the proposed use under application. It does not condone any other development currently exists on the Site which is not covered by the application. Immediate action should be taken to discontinue such development not covered by the permission;
- (c) failure to reinstate the Site as required under the relevant approval condition upon expiry of the planning permission might constitute an unauthorized development under the Town Planning Ordinance and be subject to enforcement and prosecution actions;
- (d) to note the comments of the District Lands Officer/North, Lands Department (DLO/N, LandsD) that:
 - (i) the Site comprises Government land (GL) and Old Schedule Agricultural Lot held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through GL but no right of access via GL is granted to the Site at the proposed ingress/egress point;
 - (ii) there are unauthorised structures on the private lots within and outside the Site. The lot owner/applicant should immediately rectify the lease breaches and his office reserves the rights to take necessary lease enforcement action against the breaches without further notice;
 - (iii) the GL within and outside the Site has been fenced off and illegally occupied without any permission. Any occupation of GL without Government's prior approval is an offence under the Land (Miscellaneous Provisions) Ordinance (Cap. 28). His office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice; and
 - (iv) the lot owners/applicant shall either (i) remove the unauthorised structures and cease the illegal occupation of the GL not covered by the planning application immediately; or (ii) include the unauthorised structures and the adjoining GL being illegal occupied in the planning application for further consideration by the relevant departments and, subject to the approval of the Board to the planning application which shall have reflected the rectification or amendment as aforesaid required, apply to his office for modification/new STW and STT to permit the structures erected/to be erected and the occupation of GL. The applications for modification/new STW and STT will be considered by the Government in its capacity as a landlord and there is no guarantee that they will be approved. Besides, the STW would be processed on whole lot basis. The STW and the STT, if approved, will be subject to such terms and conditions including the payment of back-dated waiver fee/rent from the first date when the unauthorised structures were erected and the occupation of GL as well as administrative fee as considered appropriate to be imposed by LandsD. In addition, LandsD reserves the right to take enforcement action against the lot owners for any breach of the lease conditions, including the breach(es) already in existence or to be detected at any point of time in future and land control action for any unlawful occupation of GL. Besides, given the proposed use is temporary in nature, only erection of temporary structure(s) will be considered;
- (e) to note the comments of the Commissioner for Transport (C for T) that the vehicular access between the Site and Sha Tau Kok Road – Lung Yeuk Tau is not managed by the Transport

Department (TD). The applicant should seek comment from the responsible party;

- (f) to note the comments of the Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD) that:
 - (i) HyD is not/shall not be responsible for the maintenance of any access connecting the Site and Sha Tau Kok Road – Lung Yeuk Tau;
 - (ii) adequate drainage measures should be provided to prevent surface water running from the Site to the nearby public roads and drains; and
 - (iii) the applicant should take adequate precautionary measures to avoid damaging roads, street furniture, drainage and slopes etc. maintained by his office. Damage caused to the roads, street furniture, drainage and slopes etc. maintained by his office due to the proposed work shall be repaired to his satisfaction at the applicant's own costs;
- (g) to note the comments of the Secretary for Environment and Ecology (SEE) that:
 - (i) the Government announced the Green Transformation Roadmap of Public Buses and Taxis in December 2024, including measures to realise the target of introducing about 3,000 electric taxis by end-2027. A comprehensive fast charging network is needed to effectively support the operations of electric taxis and achieve the aforesaid target. The applicant is recommended to consider installing some fast chargers with a rated output power of 100kW or higher at the Site and open up a certain number of charging spaces for electric commercial vehicles for use, e.g. electric light goods vehicles;
 - (ii) the applicant is suggested to consider arranging some of the chargers to also be compatible with Guobiao charging standard to support southbound vehicles; and
 - (iii) the applicant is recommended to provide charger information, including the real-time availability data of each charger, through the government-designated mobile applications such as "HKeMobility" of the TD. For any queries, please contact the EPD's EV Hotline at 3757-6222 or email to ev@epd.gov.hk;
- (h) to note the comments of the Director of Environmental Protection (DEP) that the applicant should comply with all environmental protection/pollution ordinances, and follow the requirement of the revised "Code of Practice on Handling Environmental Aspects of Temporary Uses and Open Storage Sites" issued by the Environmental Protection Department (EPD) in order to minimise any possible environmental nuisances;
- (i) to note the comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD) that:
 - (i) the existing watercourse to which the applicant proposed to discharge the stormwater from the Site is not maintained by her office. Consent from the owner/maintenance party, current users and DLO/N, LandsD should be sought for the proposed drainage connection. Regular maintenance should be carried out by the applicant to avoid blockage of drain;
 - (ii) the applicant is required to construct and maintain the proposed drainage works properly and rectify the drainage systems if they are found to be inadequate or ineffective during operation. The applicant shall also be liable for and shall indemnify claims and demands arising out of damage or nuisance caused by a failure of the systems. For works undertaken

outside the lot boundary, prior consent and agreement from DLO/N, LandsD and/or relevant private lot owners should be sought;

- (iii) the applicant is reminded that all existing flow paths as well as the run-off falling onto and passing through the Site should be intercepted and disposed of via proper discharge points. The applicant shall also ensure that no works, including any site formation works, shall be carried out as may adversely interfere with the free flow condition of the existing drain, channels and watercourses on or in the vicinity of the Site any time during or after the works;
 - (iv) the applicant shall take all precautionary measures to prevent any disturbance, damage and pollution from the proposed use to any parts of the existing drainage facilities, especially the sewerage pipes, in the vicinity of the lots. In the event of any damage to the existing drainage facilities, the applicant shall be held responsible for the cost of all necessary repair works, compensation and any other consequences arising there from. Besides, the applicant shall place all the proposed works at least 3m away from outermost surfaces of DSD's facilities;
 - (v) the applicant shall allow all time free access for the Government and its agent to conduct site inspection on his completed drainage works, if necessary; and
 - (vi) the applicant should also be advised that the limited desk-top checking by Government on the drainage proposal covers only the fundamental aspects of the drainage design which will by no means relieve his obligations to ensure that (i) the proposed drainage works will not cause any adverse drainage or environmental impacts in the vicinity; and (ii) the proposed drainage works and the downstream drainage systems have the adequate capacity and are in good conditions to receive the flows collected from his lot and all upstream catchments;
- (j) to note the comments of the Director of Fire Services (D of FS) that:
- (i) in consideration of the design/nature of the proposal, fire services installations (FSIs) are anticipated to be required. Therefore, the applicant is advised to submit relevant layout plans incorporated with the proposed FSIs to his department for approval. In addition, the applicant should also be advised on the following points:
 - the layout plans should be drawn to scale and depicted with dimensions and nature of occupancy; and
 - the location of where the proposed FSIs to be installed should be clearly marked on the layout plans; and
 - (ii) the applicant is reminded that if the proposed structures are required to comply with the Buildings Ordinance (BO)(Cap. 123), detailed fire safety requirements will be formulated upon receipt of the formal submission of general building plans or upon referral from the relevant licensing authority;
- (k) to note the comments of the Chief Engineer/Construction, Water Supplies Department (CE/C, WSD) that no structures shall be built or materials stored within 3m from the centre line(s) of water main(s) shown on the plan. Free access shall be made available at all times for staff of the Director of Water Supplies or their contractor to carry out construction, inspection, operation, maintenance and repair works;

- (l) to note the comments of the Chief Town Planner/Urban Design and Landscape, Planning Department (CTP/UD&L, PlanD) that approval of the application does not imply approval of tree works such as pruning, transplanting and compensatory/new tree planting. The applicant is reminded to seek approval for any proposed tree works from relevant department(s) prior to commencement of the works;
- (m) to note the comments of the Director of Electrical and Mechanical Services (DEMS) that:
 - (i) in the interests of public safety and ensuring the continuity of electricity supply, the parties concerned with planning, designing, organising and supervising any activity near the underground cable or overhead line under the mentioned document should approach the electricity supplier (i.e. CLP Power) for the requisition of cable plans and overhead line alignment drawings, where applicable to find out whether there is any underground cable and/or overhead line within and/or in the vicinity of the Site;
 - (ii) the applicant is also to observe the Electricity Supply Lines (Protection) Regulation and the “Code of Practice on Working near Electricity Supply Lines” established under the Regulation when carrying out works in the vicinity of the electricity supply lines; and
 - (iii) there are underground intermediate pressure town gas pipes running along Sha Tau Kok Road – Lung Yeuk Tau in the vicinity of the Site. If there is any works to be involved in the process, in particular any works involving excavation, the relevant parties shall liaise with The Hong Kong and China Gas Company Limited in respect of the exact locations of existing or planned gas pipes/gas installations in the vicinity of the works site and any required minimum setback distance away from them during any works. The relevant parties are required to observe the Electrical and Mechanical Services Department's requirements on the “Avoidance of Damage to Gas Pipes 2nd Edition” for reference (available at [https://www.emsd.gov.hk/filemanager/en/content_286/CoP_gas_pipes_2nd_\(Eng\).pdf](https://www.emsd.gov.hk/filemanager/en/content_286/CoP_gas_pipes_2nd_(Eng).pdf));
- (n) to note the comments of the Project Manager (North), Civil Engineering and Development Department (PM(N), CEDD) that the Site is within the proposed New Territories North (NTN) New Town under the Planning and Engineering Study for NTN New Town and Man Kam To (the P&E Study). The preliminary development proposal for NTN New Town was released in December 2024. While the implementation programme of NTN New Town is being formulated under the P&E Study, the site formation works will likely commence soon after the completion of detailed design in next stage. Hence, the applicant is reminded that subject to the land use planning in the P&E Study, the proposed development may need to be vacated for the site formation works; and
- (o) to note the comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD) that:
 - (i) before any new building works (including containers / open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on Site, prior approval and consent of the Building Authority should be obtained, otherwise they are unauthorised building works (UBWs) under BO. An Authorized Person should be appointed as the co-ordinator for the proposed building works in accordance with BO;
 - (ii) the Site shall be provided with means of obtaining access thereto from a street and emergency vehicular access in accordance with Regulations 5 and 41D of the Building (Planning) Regulations (B(P)R) respectively;

- (iii) if the Site does not abut on a specified street of not less than 4.5m wide and its permitted development intensity shall be determined under Regulation 19(3) of the B(P)R at building plan submission stage;
- (iv) for UBWs erected on leased land, enforcement action may be taken by BD to effect their removal in accordance with the prevailing enforcement policy against UBWs as and when necessary. The granting of any planning approval should not be construed as an acceptance of any existing building works or UBWs on the Site under BO;
- (v) any temporary shelters or converted containers for office, storage, washroom or other uses are considered as temporary buildings are subject to the control of Part VII of the B(P)R;
- (vi) the applicant's attention is also drawn to the provision under Regulations 40 and 41 of the Building (Standards of Sanitary Fitments, Plumbing, Drainage Works and Latrines) Regulations in respect of disposal of foul water and surface water respectively; and
- (vii) detailed checking under BO will be carried out at building plan submission stage.

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

From:
Sent: 2025-09-22 星期一 03:19:49
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: A/NE-LYT/860 DD 83 Lung Yeuk Tau

A/NE-LYT/860

Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau

Site area: About 1,143sq.m Includes Government Land of about 570sq.m

Zoning: "Agriculture"

Applied use: 19 Taxi EV Charging Station

Dear TPB Members,

Strong Objections. This is nothing more than a ruse to legitimize a decades old vehicle repair workshop operation that has probably been occupying government land for some time. The original approval for 400sq.mts was never renewed.

The Application Site is currently used as vehicle repair workshops

The site can accommodate more than 19 vehicles.

EV charging is not like the much-abused LPG scam that should have been terminated some years ago. There is no need for electric vehicles to drive to designated locations to refuel.

The application is clearly nothing more than a front to legitimize the vehicle repair operation.

Mary Mulvihill

城市規劃委員會
香港 北角 渣華道 333 號
北角政府合署 15 樓

2

A/NE-LYT/860

敬啟者：

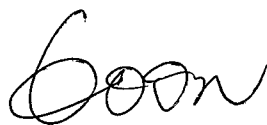
關於上述申請：

新界粉嶺沙頭角公路龍躍頭段丈量約份第 83 約 補租地段第 464 號 A 分段餘段 (部分)及毗連政府土地。

這申請範圍內有 2 個軍地村的祭祀場所「社稷大王之神位」及「開山宿老之神位」。這 2 個本村祭祀場所，在本村成立時已建造。是本村的守護者象徵、保護本村村民、「風水」的精神...

假如「社稷大王之神位」及「開山宿老之神位」無咗！或者被搬到「風水」不好的位置！恐怕破壞了「風水」！引致本村的村民有心理障礙：引致身體健康問題、出入時有交通意外等等，引申本村的村民有生命的危險！

再者，申請人所提供的相片中，蓄意隱瞞申請範圍內有「社稷大王之神位」及「開山宿老之神位」這 2 個軍地村的祭祀場所。誤導城市規劃委員會的委員的正確決定！



軍地村壹原居民

22-09-2025



致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

A/NE-LYT/860

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

「提意見人」姓名/名稱 Name of person/company making this comment

侯志強謹啟

簽署 Signature



日期 Date

2025-9-8

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

4

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

A/NE-LYT/860

意見詳情 (如有需要，請另頁說明)

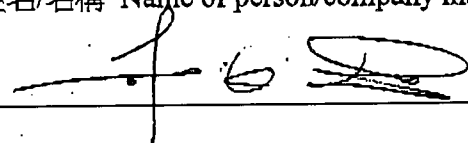
Details of the Comment (use separate sheet if necessary)

沒有意見

「提意見人」姓名/名稱 Name of person/company making this comment

龍區區議員李榮

簽署 Signature



日期 Date

11. 9. 2005