

2025年 6月 1 0日

此文件在 收到，城市規劃委員會  
只會在所有必要的資料及文件後才正式承認收到  
申請日期。

This document is received on 2025-06-10  
The Town Planning Board will formally acknowledge  
the date of receipt of the application only upon receipt  
of all the required information and documents.

Form No. S16-III  
表格第 S16-III 號

APPLICATION FOR PERMISSION  
UNDER SECTION 16 OF  
THE TOWN PLANNING ORDINANCE  
(CAP. 131)

根據《城市規劃條例》(第131章)  
第16條遞交的許可申請

**Applicable to Proposal Only Involving Temporary Use/Development of Land  
and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas,  
or Renewal of Permission for such Temporary Use or Development\***

**適用於祇涉及位於鄉郊地區或受規管地區土地上及/或建築物內進行  
為期不超過三年的臨時用途/發展或該等臨時用途/發展的許可續期的建議\***

*\*Form No. S16-I should be used for other Temporary Use/Development of Land and/or Building (e.g. temporary use/developments in the Urban Area) and Renewal of Permission for such Temporary Use or Development.*

*\*其他土地上及/或建築物內的臨時用途/發展 (例如位於市區內的臨時用途或發展) 及有關該等臨時用途/發展的許可續期，應使用表格第 S16-I 號。*

Applicant who would like to publish the notice of application in local newspapers to meet one of the Town Planning Board's requirements of taking reasonable steps to obtain consent of or give notification to the current land owner, please refer to the following link regarding publishing the notice in the designated newspapers:  
[https://www.tpb.gov.hk/en/plan\\_application/apply.html](https://www.tpb.gov.hk/en/plan_application/apply.html)

申請人如欲在本地報章刊登申請通知，以採取城市規劃委員會就取得現行土地擁有人的同意或通知現行土地擁有人所指定的其中一項合理步驟，請瀏覽以下網址有關在指定的報章刊登通知：  
[https://www.tpb.gov.hk/tc/plan\\_application/apply.html](https://www.tpb.gov.hk/tc/plan_application/apply.html)

**General Note and Annotation for the Form**

**填寫表格的一般指引及註解**

# "Current land owner" means any person whose name is registered in the Land Registry as that of an owner of the land to which the application relates, as at 6 weeks before the application is made  
「現行土地擁有人」指在提出申請前六星期，其姓名或名稱已在土地註冊處註冊為該申請所關乎的土地的擁有人的人

& Please attach documentary proof 請夾附證明文件

^ Please insert number where appropriate 請在適當地方註明編號

Please fill "NA" for inapplicable item 請在不適用的項目填寫「不適用」

Please use separate sheets if the space provided is insufficient 如所提供的空間不足，請另頁說明

Please insert a 「✓」 at the appropriate box 請在適當的方格內上加上「✓」號

2501235 6/6/2015 By Hand

Form No. S16-III 表格第 S16-III 號

For Official Use Only 請勿填寫此欄	Application No. 申請編號	A/NE-TKLA/102
	Date Received 收到日期	2025-06-10

- The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.  
申請人須把填妥的申請表格及其他支持申請的文件 (倘有), 送交香港北角渣華道 333 號北角政府合署 15 樓城市規劃委員會 (下稱「委員會」) 秘書收。
- Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.tpb.gov.hk/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).  
請先細閱《申請須知》的資料單張, 然後填寫此表格。該份文件可從委員會的網頁下載 (網址: <http://www.tpb.gov.hk/>), 亦可向委員會秘書處 (香港北角渣華道 333 號北角政府合署 15 樓 - 電話: 2231 4810 或 2231 4835) 及規劃署的規劃資料查詢處 (熱線: 2231 5000) (香港北角渣華道 333 號北角政府合署 17 樓及新界沙田上禾輦路 1 號沙田政府合署 14 樓) 索取。
- This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.  
此表格可從委員會的網頁下載, 亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全, 委員會可拒絕處理有關申請。

### 1. Name of Applicant 申請人姓名/名稱

( ☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構 )

Allied Treasure Development Limited

### 2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱 (如適用)

( ☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構 )

LCH Planning & Development Consultants Limited

### 3. Application Site 申請地點

(a) Full address / location / demarcation district and lot number (if applicable) 詳細地址/地點/丈量約份及地段號碼 (如適用)	Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai
(b) Site area and/or gross floor area involved 涉及的地盤面積及/或總樓面面積	<input checked="" type="checkbox"/> Site area 地盤面積 4628 sq.m 平方米 <input checked="" type="checkbox"/> About 約 <input checked="" type="checkbox"/> Gross floor area 總樓面面積 384 sq.m 平方米 <input checked="" type="checkbox"/> About 約
(c) Area of Government land included (if any) 所包括的政府土地面積 (倘有)	..... sq.m 平方米 <input type="checkbox"/> About 約



(d) Name and number of the related statutory plan(s) 有關法定圖則的名稱及編號	Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2
(e) Land use zone(s) involved 涉及的土地用途地帶	Recreation and Village Type Development
(f) Current use(s) 現時用途	Vehicle Park  (If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)

#### 4. "Current Land Owner" of Application Site 申請地點的「現行土地擁有人」

The applicant 申請人 –

- ☐ is the sole "current land owner"<sup>#&</sup> (please proceed to Part 6 and attach documentary proof of ownership).  
是唯一的「現行土地擁有人」<sup>#&</sup> (請繼續填寫第 6 部分，並夾附業權證明文件)。
- ☒ is one of the "current land owners"<sup>#&</sup> (please attach documentary proof of ownership).  
是其中一名「現行土地擁有人」<sup>#&</sup> (請夾附業權證明文件)。
- ☐ is not a "current land owner"<sup>#</sup>.  
並不是「現行土地擁有人」<sup>#</sup>。

- ☐ The application site is entirely on Government land (please proceed to Part 6).  
申請地點完全位於政府土地上 (請繼續填寫第 6 部分)。

#### 5. Statement on Owner's Consent/Notification

##### 就土地擁有人的同意/通知土地擁有人的陳述

- (a) According to the record(s) of the Land Registry as at ..... (DD/MM/YYYY), this application involves a total of ..... "current land owner(s)"<sup>#</sup>.  
根據土地註冊處截至 ..... 年 ..... 月 ..... 日的記錄，這宗申請共牽涉 ..... 名「現行土地擁有人」<sup>#</sup>。

(b) The applicant 申請人 –

- ☐ has obtained consent(s) of ..... "current land owner(s)"<sup>#</sup>.  
已取得 ..... 名「現行土地擁有人」<sup>#</sup>的同意。

Details of consent of "current land owner(s)" <sup>#</sup> obtained 取得「現行土地擁有人」 <sup>#</sup> 同意的詳情		
No. of "Current Land Owner(s)" 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where consent(s) has/have been obtained 根據土地註冊處記錄已獲得同意的地段號碼/處所地址	Date of consent obtained (DD/MM/YYYY) 取得同意的日期 (日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has notified ..... "current land owner(s)"<sup>#</sup>  
已通知 ..... 名「現行土地擁有人」<sup>#</sup>。

Details of the "current land owner(s)" <sup>#</sup> notified 已獲通知「現行土地擁有人」 <sup>#</sup> 的詳細資料		
No. of 'Current Land Owner(s)' 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where notification(s) has/have been given 根據土地註冊處記錄已發出通知的地段號碼／處所地址	Date of notification given (DD/MM/YYYY) 通知日期(日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☒ has taken reasonable steps to obtain consent of or give notification to owner(s):  
已採取合理步驟以取得土地擁有人的同意或向該人發給通知。詳情如下：

Reasonable Steps to Obtain Consent of Owner(s) 取得土地擁有人的同意所採取的合理步驟

- ☐ sent request for consent to the "current land owner(s)" on \_\_\_\_\_ (DD/MM/YYYY)<sup>#&</sup>  
於\_\_\_\_\_ (日/月/年)向每一名「現行土地擁有人」<sup>#</sup>郵遞要求同意書<sup>&</sup>

Reasonable Steps to Give Notification to Owner(s) 向土地擁有人發出通知所採取的合理步驟

- ☐ published notices in local newspapers on \_\_\_\_\_ (DD/MM/YYYY)<sup>&</sup>  
於\_\_\_\_\_ (日/月/年)在指定報章就申請刊登一次通知<sup>&</sup>

- ☒ posted notice in a prominent position on or near application site/premises on  
22/05/2025 \_\_\_\_\_ (DD/MM/YYYY)<sup>&</sup>  
於\_\_\_\_\_ (日/月/年)在申請地點／申請處所或附近的顯明位置貼出關於該申請的通知<sup>&</sup>

- ☒ sent notice to relevant owners' corporation(s)/owners' committee(s)/mutual aid committee(s)/management office(s) or rural committee on 22/05/2025 \_\_\_\_\_ (DD/MM/YYYY)<sup>&</sup>  
於\_\_\_\_\_ (日/月/年)把通知寄往相關的業主立案法團/業主委員會/互助委員會或管理處，或有關的鄉事委員會<sup>&</sup>

Others 其他

- ☐ others (please specify)  
其他（請指明）

Note: May insert more than one 「✓」.

Information should be provided on the basis of each and every lot (if applicable) and premises (if any) in respect of the application.

註：可在多於一個方格內加上「✓」號

申請人須就申請涉及的每一地段（倘適用）及處所（倘有）分別提供資料



**6. Type(s) of Application 申請類別****(A) Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas**

位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展

(For Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas, please proceed to Part (B))

(如屬位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期，請填寫(B)部分)

(a) Proposed use(s)/development  
擬議用途/發展Temporary Public Vehicle Park (Excluding Container Vehicle),  
Shop and Services (Convenience Store) and Ancillary Office

(Please illustrate the details of the proposal on a layout plan) (請用平面圖說明擬議詳情)

(b) Effective period of permission applied for  
申請的許可有效期☒ year(s) 年 ..... Three☐ month(s) 個月 .....**(c) Development Schedule 發展細節表**

Proposed uncovered land area 擬議露天土地面積	4436	sq.m	<input checked="" type="checkbox"/> About 約
Proposed covered land area 擬議有上蓋土地面積	192	sq.m	<input checked="" type="checkbox"/> About 約
Proposed number of buildings/structures 擬議建築物/構築物數目	2		
Proposed domestic floor area 擬議住用樓面面積		sq.m	<input type="checkbox"/> About 約
Proposed non-domestic floor area 擬議非住用樓面面積	384	sq.m	<input checked="" type="checkbox"/> About 約
Proposed gross floor area 擬議總樓面面積	384	sq.m	<input checked="" type="checkbox"/> About 約

Proposed height and use(s) of different floors of buildings/structures (if applicable) 建築物/構築物的擬議高度及不同樓層的擬議用途 (如適用) (Please use separate sheets if the space below is insufficient) (如以下空間不足，請另頁說明)

Please refer to the planning statement

**Proposed number of car parking spaces by types 不同種類停車位的擬議數目**

Private Car Parking Spaces 私家車車位	107
Motorcycle Parking Spaces 電單車車位	
Light Goods Vehicle Parking Spaces 輕型貨車泊車位	8
Medium Goods Vehicle Parking Spaces 中型貨車泊車位	
Heavy Goods Vehicle Parking Spaces 重型貨車泊車位	
Others (Please Specify) 其他 (請列明)	

**Proposed number of loading/unloading spaces 上落客貨車位的擬議數目**

Taxi Spaces 的士車位	
Coach Spaces 旅遊巴車位	
Light Goods Vehicle Spaces 輕型貨車車位	2
Medium Goods Vehicle Spaces 中型貨車車位	
Heavy Goods Vehicle Spaces 重型貨車車位	
Others (Please Specify) 其他 (請列明)	

Proposed operating hours 擬議營運時間 <b>Monday to Sunday, including Public Holidays (24 hours)</b>																																	
(d) Any vehicular access to the site/subject building? 是否有車路通往地盤／有關建築物？	Yes 是	<input checked="" type="checkbox"/> There is an existing access. (please indicate the street name, where appropriate) 有一條現有車路。(請註明車路名稱(如適用)) <b>Lin Ma Hang Road</b>																															
	No 否	<input type="checkbox"/> There is a proposed access. (please illustrate on plan and specify the width) 有一條擬議車路。(請在圖則顯示，並註明車路的闊度)																															
(e) Impacts of Development Proposal 擬議發展計劃的影響 (If necessary, please use separate sheets to indicate the proposed measures to minimise possible adverse impacts or give justifications/reasons for not providing such measures. 如需要的話，請另頁註明可盡量減少可能出現不良影響的措施，否則請提供理據/理由。)																																	
(i) Does the development proposal involve alteration of existing building? 擬議發展計劃是否包括現有建築物的改動？	Yes 是	<input type="checkbox"/> Please provide details 請提供詳情 																															
	No 否	<input checked="" type="checkbox"/>																															
(ii) Does the development proposal involve the operation on the right? 擬議發展是否涉及右列的工程？	Yes 是	<input type="checkbox"/> (Please indicate on site plan the boundary of concerned land/pond(s), and particulars of stream diversion, the extent of filling of land/pond(s) and/or excavation of land) (請用地盤平面圖顯示有關土地／池塘界線，以及河道改道、填塘、填土及／或挖土的細節及／或範圍) <input type="checkbox"/> Diversion of stream 河道改道 <input type="checkbox"/> Filling of pond 填塘 Area of filling 填塘面積 ..... sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填塘深度 ..... m 米 <input type="checkbox"/> About 約 <input type="checkbox"/> Filling of land 填土 Area of filling 填土面積 ..... sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填土厚度 ..... m 米 <input type="checkbox"/> About 約 <input type="checkbox"/> Excavation of land 挖土 Area of excavation 挖土面積 ..... sq.m 平方米 <input type="checkbox"/> About 約 Depth of excavation 挖土深度 ..... m 米 <input type="checkbox"/> About 約																															
	No 否	<input checked="" type="checkbox"/>																															
(iii) Would the development proposal cause any adverse impacts? 擬議發展計劃會否造成不良影響？	<table border="0"> <tr> <td>On environment 對環境</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On traffic 對交通</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On water supply 對供水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On drainage 對排水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On slopes 對斜坡</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Affected by slopes 受斜坡影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Landscape Impact 構成景觀影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Tree Felling 砍伐樹木</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Visual Impact 構成視覺影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Others (Please Specify) 其他 (請列明)</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> </table>			On environment 對環境	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On traffic 對交通	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On water supply 對供水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On drainage 對排水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On slopes 對斜坡	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Affected by slopes 受斜坡影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Landscape Impact 構成景觀影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Tree Felling 砍伐樹木	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Visual Impact 構成視覺影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Others (Please Specify) 其他 (請列明)	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>
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Others (Please Specify) 其他 (請列明)	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>																															



	<p>Please state measure(s) to minimise the impact(s). For tree felling, please state the number, diameter at breast height and species of the affected trees (if possible)</p> <p>請註明盡量減少影響的措施。如涉及砍伐樹木，請說明受影響樹木的數目、及胸高度的樹幹直徑及品種(倘可)</p> <p><b>Please refer to supporting planning statement</b></p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p>
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<b>(B) Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas</b> <b>位於鄉郊地區或受規管地區臨時用途/發展的許可續期</b>	
(a) Application number to which the permission relates 與許可有關的申請編號	A/ _____ / _____
(b) Date of approval 獲批給許可的日期	..... (DD 日/MM 月/YYYY 年)
(c) Date of expiry 許可屆滿日期	..... (DD 日/MM 月/YYYY 年)
(d) Approved use/development 已批給許可的用途/發展	
(e) Approval conditions 附帶條件	<div style="border: 1px solid black; padding: 5px;"> <input type="checkbox"/> The permission does not have any approval condition          許可並沒有任何附帶條件   <input type="checkbox"/> Applicant has complied with all the approval conditions          申請人已履行全部附帶條件   <input type="checkbox"/> Applicant has not yet complied with the following approval condition(s):          申請人仍未履行下列附帶條件：          .....          .....          .....          Reason(s) for non-compliance:          仍未履行的原因：          .....          .....          .....          (Please use separate sheets if the space above is insufficient)          (如以上空間不足，請另頁說明)       </div>
(f) Renewal period sought 要求的續期期間	<input type="checkbox"/> year(s) 年 ..... <input type="checkbox"/> month(s) 個月 .....

## 7. Justifications 理由

The applicant is invited to provide justifications in support of the application. Use separate sheets if necessary.  
現請申請人提供申請理由及支持其申請的資料。如有需要，請另頁說明。

Please refer to the supporting planning statement



**8. Declaration 聲明**

I hereby declare that the particulars given in this application are correct and true to the best of my knowledge and belief.  
本人謹此聲明，本人就這宗申請提交的資料，據本人所知及所信，均屬真實無誤。

I hereby grant a permission to the Board to copy all the materials submitted in this application and/or to upload such materials to the Board's website for browsing and downloading by the public free-of-charge at the Board's discretion.  
本人現准許委員會酌情將本人就此申請所提交的所有資料複製及/或上載至委員會網站，供公眾免費瀏覽或下載。

Signature  
簽署

☐ Applicant 申請人 / ☒ Authorised Agent 獲授權代理人

HO JOSEPH JUNIOR

DIRECTOR

Name in Block Letters  
姓名（請以正楷填寫）

Position (if applicable)  
職位（如適用）

Professional Qualification(s)  
專業資格

☒ Member 會員 / ☐ Fellow of 資深會員

☒ HKIP 香港規劃師學會 /

☐ HKIA 香港建築師學會 /

☒ HKIS 香港測量師學會 /

☐ HKIE 香港工程師學會 /

☐ HKILA 香港園境師學會 /

☐ HKIUD 香港城市設計學會

☐ RPP 註冊專業規劃師

Others 其他

on behalf of  
代表

LCH Planning & Development Consultants Limited

☒ Company 公司 / ☐ Organisation Name and Chop (if applicable) 機構名稱及蓋章（如適用）

Date 日期

05/06/2025

(DD/MM/YYYY 日/月/年)

**Remark 備註**

The materials submitted in this application and the Board's decision on the application would be disclosed to the public. Such materials would also be uploaded to the Board's website for browsing and free downloading by the public where the Board considers appropriate.

委員會會向公眾披露申請人所遞交的申請資料和委員會對申請所作的決定。在委員會認為合適的情況下，有關申請資料亦會上載至委員會網頁供公眾免費瀏覽及下載。

**Warning 警告**

Any person who knowingly or wilfully makes any statement or furnish any information in connection with this application, which is false in any material particular, shall be liable to an offence under the Crimes Ordinance.

任何人在明知或故意的情況下，就這宗申請提出在任何要項上是虛假的陳述或資料，即屬違反《刑事罪行條例》。

**Statement on Personal Data 個人資料的聲明**

1. The personal data submitted to the Board in this application will be used by the Secretary of the Board and Government departments for the following purposes:

委員會就這宗申請所收到的個人資料會交給委員會秘書及政府部門，以根據《城市規劃條例》及相關的城市規劃委員會規劃指引的規定作以下用途：

- (a) the processing of this application which includes making available the name of the applicant for public inspection when making available this application for public inspection; and  
處理這宗申請，包括公布這宗申請供公眾查閱，同時公布申請人的姓名供公眾查閱；以及  
(b) facilitating communication between the applicant and the Secretary of the Board/Government departments.  
方便申請人與委員會秘書及政府部門之間進行聯絡。

2. The personal data provided by the applicant in this application may also be disclosed to other persons for the purposes mentioned in paragraph 1 above.

申請人就這宗申請提供的個人資料，或亦會向其他人士披露，以作上述第 1 段提及的用途。

3. An applicant has a right of access and correction with respect to his/her personal data as provided under the Personal Data (Privacy) Ordinance (Cap. 486). Request for personal data access and correction should be addressed to the Secretary of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.

根據《個人資料(私隱)條例》(第 486 章)的規定，申請人有權查閱及更正其個人資料。如欲查閱及更正個人資料，應向委員會秘書提出有關要求，其地址為香港北角渣打道 333 號北角政府合署 15 樓。

Gist of Application 申請摘要	
(Please provide details in both English and Chinese <u>as far as possible</u> . This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information. ) (請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)	
Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)
Location/address 位置／地址	Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai 香園圍丈量約份第78約地段第377號、第380號A分段、第380號B分段、第380號C分段及第380號餘段及丈量約份第80約地段第61號B分段餘段（部分）、第62號，以及第65號B分段餘段（部分）
Site area 地盤面積	4628 sq. m 平方米 <input checked="" type="checkbox"/> About 約 (includes Government land of 包括政府土地 sq. m 平方米 <input type="checkbox"/> About 約)
Plan 圖則	Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 打鼓嶺北分區計劃大綱圖編號S/NE-TKLN/2
Zoning 地帶	Recreation and Village Type Development 康樂及鄉村式發展
Type of Application 申請類別	<input checked="" type="checkbox"/> Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區的臨時用途/發展為期 <input checked="" type="checkbox"/> Year(s) 年 3 <input type="checkbox"/> Month(s) 月 <input type="checkbox"/> Renewal of Planning Approval for Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期為期 <input type="checkbox"/> Year(s) 年 <input type="checkbox"/> Month(s) 月
Applied use/ development 申請用途/發展	Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Service (convenience store) and Ancillary Office for a period of 3 years 擬議臨時公眾停車場(貨櫃車除外)及臨時商店及服務行業(便利店)連附屬辦公室用途(為期三年)



(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	384 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.083 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用		
	Non-domestic 非住用	2	
(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	m 米 <input type="checkbox"/> (Not more than 不多於)	
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於)	
	Non-domestic 非住用	6	<input checked="" type="checkbox"/> (Not more than 不多於) m 米
		2	<input checked="" type="checkbox"/> (Not more than 不多於) Storeys(s) 層
(iv) Site coverage 上蓋面積	4.1 % <input checked="" type="checkbox"/> About 約		
(v) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數		115
	Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) _____ _____		107 8
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數		2
	Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) _____ _____		2

Submitted Plans, Drawings and Documents 提交的圖則、繪圖及文件		
	<u>Chinese</u> 中文	<u>English</u> 英文
<b><u>Plans and Drawings 圖則及繪圖</u></b>		
Master layout plan(s)/Layout plan(s) 總綱發展藍圖／布局設計圖	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Block plan(s) 樓宇位置圖	<input type="checkbox"/>	<input type="checkbox"/>
Floor plan(s) 樓宇平面圖	<input type="checkbox"/>	<input type="checkbox"/>
Sectional plan(s) 截視圖	<input type="checkbox"/>	<input type="checkbox"/>
Elevation(s) 立視圖	<input type="checkbox"/>	<input type="checkbox"/>
Photomontage(s) showing the proposed development 顯示擬議發展的合成照片	<input type="checkbox"/>	<input type="checkbox"/>
Master landscape plan(s)/Landscape plan(s) 園境設計總圖／園境設計圖	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Location Plan 位置圖, Site Plan 平面圖		
<b><u>Land Status Plan 土地現況圖, Outline Zoning Plan 分區計劃大綱圖</u></b>		
<b><u>Reports 報告書</u></b>		
Planning Statement/Justifications 規劃綱領/理據	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Environmental assessment (noise, air and/or water pollutions) 環境評估（噪音、空氣及／或水的污染）	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on vehicles) 就車輛的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on pedestrians) 就行人的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Visual impact assessment 視覺影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Landscape impact assessment 景觀影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Tree Survey 樹木調查	<input type="checkbox"/>	<input type="checkbox"/>
Geotechnical impact assessment 土力影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Drainage impact assessment 排水影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Sewerage impact assessment 排污影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Risk Assessment 風險評估	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input type="checkbox"/>
Swept Path Analysis 車輛路徑分析, Indicative Stormwater Flow 指示性雨水流量		
Note: May insert more than one 「✓」. 註：可在多於一個方格內加上「✓」號		

Note: The information in the Gist of Application above is provided by the applicant for easy reference of the general public. Under no circumstances will the Town Planning Board accept any liabilities for the use of the information nor any inaccuracies or discrepancies of the information provided. In case of doubt, reference should always be made to the submission of the applicant.

註：上述申請摘要的資料是由申請人提供以方便市民大眾參考。對於所載資料在使用上的問題及文義上的歧異，城市規劃委員會概不負責。若有任何疑問，應查閱申請人提交的文件。





**Section 16 Application for Temporary Public Vehicle Park  
(Excluding Container Vehicle), Shop and Services (Convenience  
Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380  
S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65  
S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a  
Period of 3 Years**

**Supporting Planning Statement**

Prepared by  
Planning Consultant : LCH Planning & Development  
Consultants Limited

May 2025  
Report : Version 1.0



## **Executive Summary**

*(in case of discrepancy between English and Chinese versions, English version shall prevail)*

This Application is submitted to the Town Planning Board (“**the Board**”) under Section 16 of the Town Planning Ordinance (“**the Ordinance**”) for a proposed temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Service (convenience store) and Ancillary Office for a period of 3 years (“**the Proposed Development**”) at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78, and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80 in Heung Yuen Wai, New Territories (“**the Application Site**”).

The Application Site falls within an area of “Recreation” (“**REC**”) zone and “Village Type Development” (“**V**”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 (“**the OZP**”). Upon completion of the Proposed Development, it comprises 107 parking spaces for private cars (at least 2.5m x 5m each) and 8 parking spaces for Light Goods Vehicle (LGV) (at least 3.5m x 7m each), as well as two two-storey structures for convenience store and ancillary office uses. The proposed building height of 6 m and the total floor area is about 384 sq.m. There will be two various loading and unloading bays for LGV to support the Proposed Development.

The Application echoes government’s direction to provide more parking spaces to serve the Heung Yuen Wai Boundary Control Point, which aims to accommodate the high demand of the local villagers and the cross-boundary travellers for car parking spaces. It is situated at a convenient location that is within 5 minutes walking distance from the Boundary Control Point.

The Proposed Development does not hinder the long-term planning intention of “REC” and “V” zone. Similar applications in the “REC” and “V” zone in the Ta Kwu Ling North area have also been approved and considered. The Proposed Development will not cause adverse traffic, visual, landscape, drainage and environmental impacts.

In view of the above justifications, we would sincerely seek for favourable consideration of the Board to approve this Application.





## 內容摘要

(如內文與其英文版本有差異，則以英文版本為準)

本規劃申請根據《城市規劃條例》第 16 條，就新界香園圍丈量約份第 78 約地段第 377 號、第 380 號 A 分段、第 380 號 B 分段、第 380 號 C 分段及第 380 號餘段及丈量約份第 80 約地段第 61 號 B 分段餘段（部分）、第 62 號，以及第 65 號 B 分段餘段（部分）（下稱「**申請地點**」）的用地，向城市規劃委員會（下稱「**城規會**」）申請作擬議臨時公眾停車場（貨櫃車除外）及臨時商店及服務行業（便利店）連附屬辦公室用途，為期 3 年（下稱「**擬議發展**」）。

申請地點現時於《打鼓嶺北分區計劃大綱圖編號 S/NE-TKLN/2》（下稱「**大綱圖**」）劃作「康樂」和「鄉村式發展」地帶。擬議發展包括 107 個私家車專用停車位（每個最少 2.5 米 x 5 米）和 8 個輕型貨車專用停車位，以及兩個兩層高的構築物作為臨時商店及服務行業和附屬辦公室用途。擬議構築物的高度約 6 米，總樓面面積約為 384 平方米。擬議發展將會有兩個輕型貨車的上落客貨車位。

申請響應政府提供更多泊車位服務香園圍口岸的方向，旨在滿足當地村民和過境旅客對泊車位的殷切需求。申請地點坐落在一個方便的位置，從蓮塘/香園圍口岸步行 5 分鐘即可到達。

擬議發展不會妨礙打鼓嶺北「康樂」和「鄉村式發展」地帶的長遠規劃意向。而且於打鼓嶺北的「康樂」和「鄉村式發展」地帶亦已有不少相關申請曾被批准。擬議發展不會對交通、視覺、景觀、排水和環境造成不利影響。

鑒於以上提出的依據，我們真誠地尋求城規會批准該申請。



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## 1 INTRODUCTION

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### 1.1 The Application

- 1.1.1 We are commissioned by the Applicant to prepare and submit on its behalf this planning application for a proposed temporary ‘Public Vehicle Park (Excluding Container Vehicle)’, ‘Shop and Services (Convenience Store)’ and ancillary office uses for a period of 3 years (hereinafter referred to as the “**Proposed Development**”) at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78, and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories (**Figure 1**) (hereinafter referred to as the “**Application Site**”) to the Town Planning Board (“**the Board**”) under Section 16 of the Town Planning Ordinance (“**the Ordinance**”).
- 1.1.2 The Application Site partly falls within an area designated as “Recreation” (“**REC**”) zone and partly falls within an area designated as “Village Type Development” (“**V**”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 (“**the OZP**”) (**Figure 4**).
- 1.1.3 According to the Notes of the OZP, temporary use or development of any land or building not exceeding a period of three years requires permission from the Town Planning Board. Notwithstanding that the use or development is not provided for in terms of the Plan, the Town Planning Board may grant permission, with or without conditions, for a maximum period of three years.



## 2 THE APPLICATION SITE AND DEVELOPMENT PROPOSAL

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### 2.1 Current Condition of the Application Site

- 2.1.1 The Application Site covers a total of about 4,628 square metres ("sq. m."). The Application Site is paved and fenced off. Major part of the site is currently occupied by a car park under the approved planning applications No. A/NE-TKLN/57 and 58,
- 2.1.2 The Application Site has direct access to Lin Ma Hang Road. The site is situated at the east of Heung Shui Wai Boundary Control Point and north of Tsung Yuen Ha Tsuen.
- 2.1.3 **Figure 1** illustrates the location of the Application Site and its immediate vicinity, and the existing condition of the Site (**Diagram 1**).

### 2.2 Land Status

- 2.2.1 The Application Site falls within a total of 8 lots, Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in Demarcation District 78, and 61 S.B RP (part), 62 and 65 S.B RP (part) in Demarcation District 80 ("**the Lots**") (**Figure 3**).

### 2.3 Surrounding Context

- 2.3.1 Ta Kwu Ling is a sub-urban area located northeast of Shui Hau in the New Territories. Ta Kwu Ling is one of three new development areas currently being planned for North District, in parallel with Fanling North and Kwu Tung North. The area is comprised of twenty villages and the major uses of land are parking of vehicles, storage, temporary structures, domestic structures and vacant land. Ta Kwu Ling North is located in the north of a village which is Tsung Yuen Ha Tsuen. There is a major road (Lin Ma Hang Road) in Ta Kwu Ling North, and the major transportation mode is by bus or minibus, connecting Ta Kwu Ling North to Sheung Shui MTR station.
- 2.3.2 The Application Site is located at the northern edge of Ta Kwu Ling, with village houses and temporary structures surrounding the site. The run-in and out is located at the northern boundary of the Applications Site, and the site is accessible from Ling Ma Hang Road - Ta Kwu Ling.

### 2.4 Existing Road Network

- 2.4.1 The site is accessible via Lin Ma Hang Road. Lin Ma Hang Road is a north-south single-two carriageway used as a local road. Heung Yuen Wai Highway connects Lin Ma Hang Road and Fanling Highway. It is a north-south two-way two-lane expressway.



## **2.5 Heung Yuen Wai Boundary Control Point**

- 2.5.1 The Heung Yuen Wai Boundary Control Point (HYWBCP) opened for cargo trucks since 2020, while it has been fully commenced for passengers as well since February 2023. HYWBCP is capable to handle 17,850 vehicle trips and 30,000 passenger trips daily. There is a public car park with 415 parking spaces inside the passenger Terminal Building, and a pedestrian subway linking the HYWBCP with the Lin Ma Hang Bus Stop. HYWBCP targets to redistribute the cross-boundary traffic among the control points in the east, which aim to lowered the daily passenger and vehicular flows of the Sha Tau Kok and Man Kam To Boundary Control Points by 18% and 26% respectively.
- 2.5.2 As at September 2024, the HYWBCP recorded a daily average of about 70 000 cross-boundary passenger trips, and during weekends and public holidays, a daily average of over 80 000 cross-boundary passenger trips.
- 2.5.3 Upon commissioning of cargo clearance facilities of HYWBCP since 2020, the average daily vehicles using HYWBCP by the end of year 2024 is about 3096 (two directions). This number is comparable to the increase in average daily traffic flow between 2020 and 2024 at the relevant section of HYW Highway (e.g. Lung Shan Tunnel and Cheung Shan Tunnel) from about 12964 and 3059 (both directions) to 22763 and 11360 (both directions).<sup>1</sup> This reflects that most cross-boundary vehicles have made use of the HYW Highway to commute to and from HYW BCP.
- 2.5.4 There is a public car park within the HYWBCP providing 415 parking spaces. With the overwhelming response since its opening, the public car park spaces were fully occupied over the weekends. In response to this issue, the Government has reviewed and further enhancing the current booking system by measures including adjustment of parking fees and acceptance of on-site booking during non-peak periods.<sup>23</sup> Yet, there are still unresolved demand for private car parking spaces over there.<sup>4</sup>

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<sup>1</sup>[https://www.td.gov.hk/en/transport in hong kong/transport figures/monthly traffic and transport digest/2024/202403/index.html](https://www.td.gov.hk/en/transport%20in%20hong%20kong/transport%20figures/monthly%20traffic%20and%20transport%20digest/2024/202403/index.html)

<sup>2</sup> <https://www.info.gov.hk/gia/general/202302/15/P2023021500526.htm?fontSize=1>

<sup>3</sup> <https://www.info.gov.hk/gia/general/202411/06/P2024110600327.htm>

<sup>4</sup> 批香園圍口岸停車場預約系統智能化不足 議員：釋放唔到車位, Source: [https://hk.on.cc/hk/bkn/cnt/news/20240925/bkn-20240925095215730-0925\\_00822\\_001.html](https://hk.on.cc/hk/bkn/cnt/news/20240925/bkn-20240925095215730-0925_00822_001.html)





2.5.5 Existing public transport services to the vicinity are shown below:

<u>Mode</u>	<u>Route No.</u>	<u>Origin - Destination</u>	<u>Frequency</u>
Scheduled Green Minibus	59K	Sheung Shui Station Public Minibus Terminus – Chuk Yeun	35 - 45 minutes
	59S	Sheung Shui Station Public Minibus Terminus –Heung Yuen Wai Boundary Control Point	25 – 35 minutes
Franchised Bus	B7	Fanling Station – Heung Yuen Wai Port	10 - 20 minutes
	B8	Tai Wai Station – Heung Yuen Wai Port	30 minutes
	B9	Tuen Mun Station – Heung Yuen Wai Port	60 minutes

2.5.6 It only takes about 5 minutes to walk from the Application site to the HYWBCP, by going through the subway from the bus stop along Lin Ma Hang Road. **Figure 4** illustrate the accessibility.

**2.6 Widening of the Western Section and Eastern Section of Lin Ma Hang Road**

2.6.1 The design study indicated that the Lin Ma Hang Road is going to be widen form both ends to cope with the anticipated increase in traffic volume on Lin Ma Hang Road due to the opening of the Frontier Closed Area (FCA), and to provide a safer and more efficient road network for residents and tourists. The project scope covers the western section of Lin Ma Hang Road (from Ping Yuen River to Ping Che Road), and the eastern section (from Tsung Yuen Ha to Lin Ma Hang).

2.6.2 In November 2023, the upgrading works of western section of Lin Ma Hang Road has been completed from a single-track road with passing places to a single two-lane carriageway with footpath on both sides.

2.6.3 The Application Site abuts the eastern section of the Lin Ma Hang Road which is subject to further widening works. According to the replies from Highways Department to the North District Council on May 2021, since part of the widening works in the eastern section are designated projects under the Environmental Impact Assessment Ordinance, the project consultant would submit the relevant environmental impact assessment before commencement of works. There is no detail commencement schedule at this moment.

**2.7 Existing Vehicle Parking Space and Village Clusters in the Vicinity**

2.7.1 At the southern edge of Application Site is Tsung Yuen Ha where it is situated between the middle and eastern section of Lin Ma Hang Road. Going south along the Lin Ma Hang Road are Kaw Liu Village on the west and Chuk Yuen Village on



the east, and further northern-east is Ha Heung Yuen.

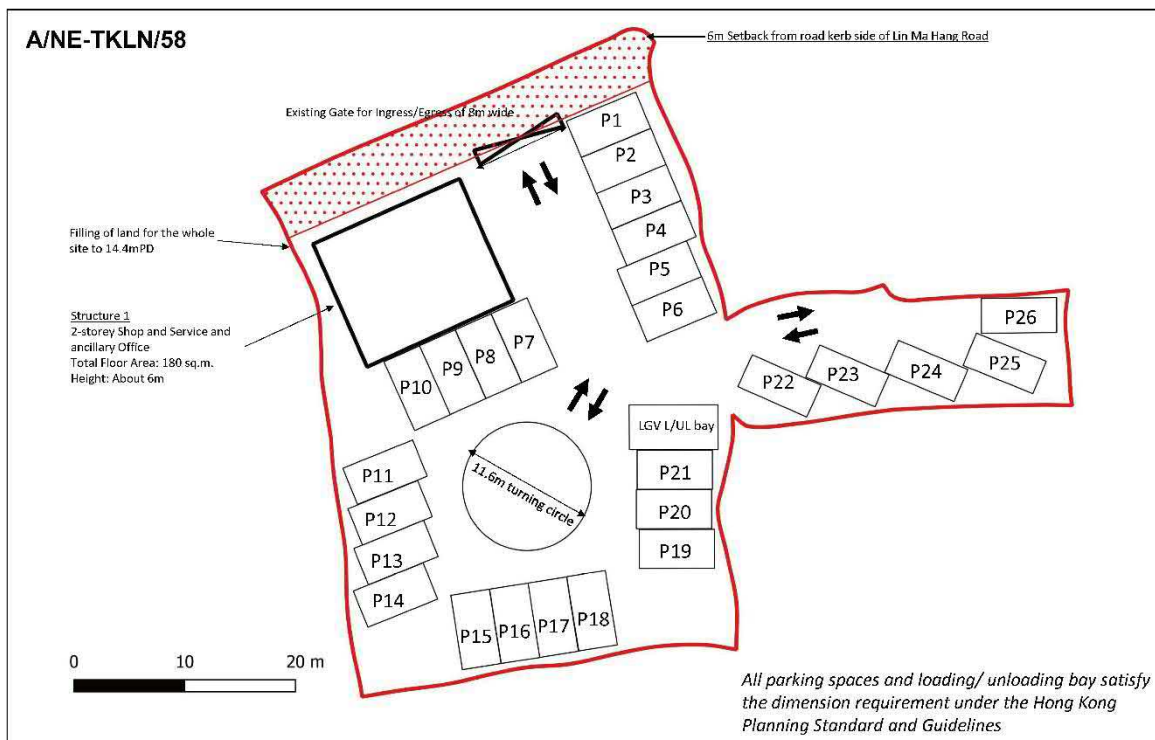
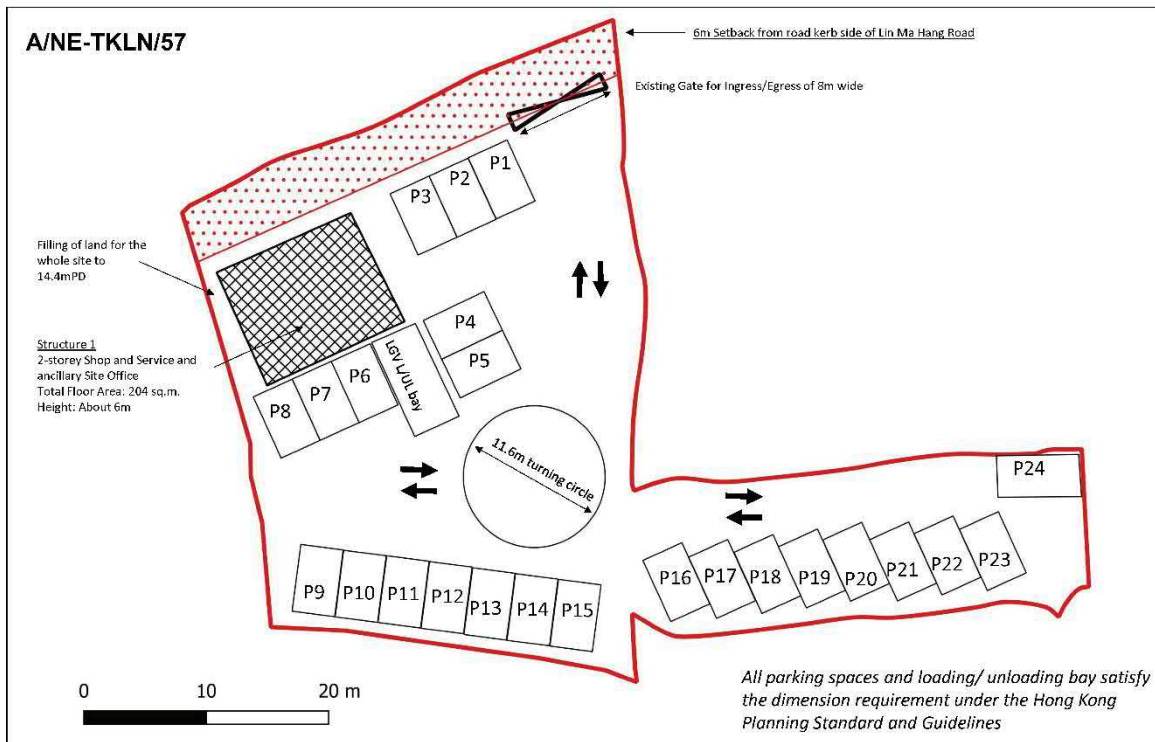
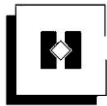
- 2.7.2 The predominant uses in the surrounding of the Application Site are car parks and associated economic activities such as shops and restaurants. The concentration of public car parks indicates the transport and logistics natures of Heung Yuen Wai area and its significant role in supporting cross-boundary travels and supply chain activities.



**Diagram 1** Existing Open Storage, Vehicle Parking Space and Village Clusters  
(Source: HK GEODATA STORE, HKSAR Government)

## 2.8 Previously Approved Schemes

- 2.8.1 Major portion of the Application Site is subject to approved applications No. A/NE-TKLN/57 and 58 for proposed temporary shop and services (convenience store) with ancillary site office and car parks. According to the approved schemes, there are two various 2-storey structures for a floor area of 204 sq.m. and 180 sq.m. for each application, providing 24 parking spaces and 1 loading/ unloading space for Light Goods Vehicle (LGV), and 26 parking spaces and 1 loading/ unloading space for LGV respectively. The applications were both approved with conditions on 22 September 2023 for a period of 3 years till 22 September 2026.



**Diagram 2** Indicative Layout Plans for Previously Approved Applications No. A/NE-TKLN/57 and 58 (Source: HK GEODATA STORE, HKSAR Government)





## 2.9 Temporary Public Vehicle Park for Private Cars, Shop and Services and Ancillary Office

2.9.1 The current Application Site covers an area of about 4,628 sq.m. The Proposed Development will provide a total of 107 parking spaces for private cars (at least 2.5m x 5m each) and 8 parking spaces for LGV (at least 3.5m x 7m each) within the Application Site.

2.9.2 Following the previously approved applications No. A/NE-TKLN/57 and 58, there will be two two-storey structures as temporary shop and services (convenience store) and ancillary office, each situated at the north-western portion and north-eastern portion of the Application Site. Together, with a total floor area of 384 sq.m. and a height of about 6 meters. There will be two loading & unloading bays at the north-western portion and north-eastern portion of the Application Site to serve the commercial use.

Parameters	Proposed Development
Site Area	About 4,628 sq.m.
No. of Structure	2
Covered Area	About 192 sq.m.
Maximum Height of Structure	About 6 metres, two-storey
Maximum Total Site Coverage	About 4.1%
Maximum Plot Ratio	About 0.083
No. of Parking Spaces for Private Cars	107
No. of Parking Spaces for LGV	8
Loading/ Unloading Space for LGV	2

2.9.3 Following the previously approved applications, a setback of 6 m from the road kerb side of Lin Ma Hang Road has been reserved for the future widening works of the eastern section of Lin Ma Hang Road. **These areas shall not form part of the application site**, hence no interfacing issue is anticipated from this Development.

2.9.4 The indicative layout plan of the Development is shown in **Annex 1**.

## 2.10 Operation Arrangement

2.10.1 The car park will operate 24 hours a day. Daily, weekly and monthly rental basis will be allowed for local residents and visitors. A longer operation hour and a varied charging scheme will provide greater flexibility to the cross- boundary travellers, so as to relieve the pressure from the existing Heung Yuen Wai Car Park.



2.10.2 According to Chapter 8 of the Hong Kong Planning Standards and Guidelines, the dimension of parking spaces and loading/ unloading bays are set. All the proposed parking spaces and loading/ unloading bays comply with the relevant standards.

Types of Parking Space and Loading/Unloading Bay	Length (m)	Width (m)
Private Cars, Van-type Light Goods Vehicles and Taxis	5	2.5
Light Goods Vehicles (LGV)	7	3.5

2.10.3 The convenience store will operate from 9a.m. to 8p.m. daily including public holidays, which is similar to the previously? approved scheme. Fencing will be provided around the boundary of the carpark.

2.10.4 Enter and exit of the car park will be through the existing gate opening along the Lin Ma Hang Road with a width of 8m.

2.10.5 The following traffic management measures are proposed to follow:

- No vehicle without valid licence issued under the Road Traffic Ordinance will be allowed to be parked/stored on or enter/exit the Site;
- Only private car as defined in the Road Traffic Ordinance will be allowed to be parked/stored on or enter/exit the Site;
- No medium or heavy goods vehicle exceeding 5.5 tonnes, including container tractor/ trailer will be permitted to park at the Site;
- A notice should be posted at the ingress/ egress point of the site to indicate that no medium or heavy goods vehicle (i.e. exceeding 5.5 tonnes) including container trailers/tractors, as defined in the Road Traffic Ordinance, will be allowed to be parked/stored on the Site; and
- No car washing, vehicle repair or other workshop activities will be allowed on the Site.



### 3 PLANNING ASSESSMENT

#### 3.1 Statutory Planning Context

- 3.1.1 The Application Site partly falls within an area designated as “Recreation” (“REC”) zone and partly falls within an area designed as “Village Type Development” (“V”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 (**Figure 3**) (also known as the “OZP”). The planning intention of the “REC” zone is “intended primarily for low-density recreational developments for the use of the general public”. The planning intention of the “V” zone is “designate both existing recognized villages, and areas of land considered suitable for village expansion and reprovioning of village houses affected by Government projects”.
- 3.1.2 The “REC” zone is roughly about 3,302 sq.m. while the “V” zone is roughly about 1,326 sq.m., which occupy about 71% and 29% respectively of the total site area.
- 3.1.3 According to Notes of “REC” zone and “V” zone of OZP, ‘Public Vehicle Park’ is a Column 2 use under “REC” zone and “V” zone. According to the Notes of the OZP, temporary use not exceeding a period of three years within “REC” zone and “V” zone requires planning permission from the Town Planning Board notwithstanding that the use is not provided for in terms of the Plan.
- 3.1.4 Approval of this application on a temporary basis for a period of 3 years would not jeopardize the long-term planning intention of the “REC” zone and “V” zone.

- 4 -

S/NE-TKLN/2

#### RECREATION

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Agricultural Use	Animal Boarding Establishment
Barbecue Spot	Broadcasting, Television and/or Film Studio
Field Study/Education/Visitor Centre	Eating Place
Government Use (Police Reporting Centre only)	Flat
Holiday Camp	Golf Course
On-Farm Domestic Structure	Government Refuse Collection Point
Picnic Area	Government Use (not elsewhere specified)
Place of Recreation, Sports or Culture	Helicopter Landing Pad
Public Convenience	Hotel
Rural Committee/Village Office	House (other than rebuilding of New Territories Exempted House or replacement of existing domestic building by New Territories Exempted House permitted under the covering Notes)
Tent Camping Ground	Place of Entertainment
	Private Club
	Public Utility Installation
	Public Vehicle Park (excluding container vehicle)
	Religious Institution
	Residential Institution
	Shop and Services
	Theme Park
	Utility Installation for Private Project
	Zoo





- 5 -

S/NE-TKLN/2

RECREATION (cont'd)

Planning Intention

This zone is intended primarily for low-density recreational developments for the use of the general public. It encourages the development of active and/or passive recreation and tourism/eco-tourism. Uses in support of the low-density recreational developments may be permitted subject to planning permission.

Remarks

- (a) No residential development (except 'New Territories Exempted House') shall result in a total development in excess of a maximum plot ratio of 0.2 and a maximum building height of 2 storeys.
- (b) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the plot ratio/building height restrictions stated in paragraph (a) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.
- (c) Any diversion of streams or filling of pond, including that to effect a change of use to any of those specified in Columns 1 and 2 above or the uses or developments always permitted under the covering Notes (except public works co-ordinated or implemented by Government, and maintenance, repair or rebuilding works), shall not be undertaken or continued on or after the date of the first publication in the Gazette of the notice of the draft development permission area plan without the permission from the Town Planning Board under section 16 of the Town Planning Ordinance.

*(Source: Town Planning Board, HKSAR Government)*



- 1 -

S/NE-TKLN/2

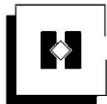
VILLAGE TYPE DEVELOPMENT

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Agricultural Use	Burial Ground
Government Use (Police Reporting Centre, Post Office only)	Eating Place
House (New Territories Exempted House only)	Flat
On-Farm Domestic Structure	Government Refuse Collection Point
Religious Institution	Government Use (not elsewhere specified) #
(Ancestral Hall only)	Hotel (Holiday House only)
Rural Committee/Village Office	House (not elsewhere specified)
	Institutional Use (not elsewhere specified) #
	Market
	Place of Recreation, Sports or Culture
	Private Club
	Public Clinic
	Public Convenience
	Public Transport Terminus or Station
	Public Utility Installation #
	Public Vehicle Park
	(excluding container vehicle)
	Religious Institution (not elsewhere specified) #
	Residential Institution #
	School #
	Shop and Services
	Social Welfare Facility #
	Utility Installation for Private Project

In addition, the following uses are always  
permitted on the ground floor of a New  
Territories Exempted House:

Eating Place  
Library  
School  
Shop and Services

(Please see next page)



VILLAGE TYPE DEVELOPMENT (cont'd)

Planning Intention

The planning intention of this zone is to designate both existing recognized villages and areas of land considered suitable for village expansion and reprovisioning of village houses affected by Government projects. Land within this zone is primarily intended for development of Small Houses by indigenous villagers. It is also intended to concentrate village type development within this zone for a more orderly development pattern, efficient use of land and provision of infrastructures and services. Selected commercial and community uses serving the needs of the villagers and in support of the village development are always permitted on the ground floor of a New Territories Exempted House. Other commercial, community and recreational uses may be permitted on application to the Town Planning Board.

Remarks

- (a) No new development, or addition, alteration and/or modification to or redevelopment of an existing building (except development or redevelopment to those annotated with #) shall result in a total development and/or redevelopment in excess of a maximum building height of 3 storeys (8.23m) or the height of the building which was in existence on the date of the first publication in the Gazette of the notice of the draft development permission area plan, whichever is the greater.
- (b) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the building height restriction stated in paragraph (a) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.
- (c) Any diversion of streams or filling of pond, including that to effect a change of use to any of those specified in Columns 1 and 2 above or the uses or developments always permitted under the covering Notes (except public works co-ordinated or implemented by Government, and maintenance, repair or rebuilding works), shall not be undertaken or continued on or after the date of the first publication in the Gazette of the notice of the draft development permission area plan without the permission from the Town Planning Board under section 16 of the Town Planning Ordinance.

*(Source: Town Planning Board, HKSAR Government)*

## **3.2 Previously Approved Applications**

- 3.2.1 Part of the Application Site is subject to planning applications No. A/NE-TKLN/57 and 58, which were approved by the Town Planning Board on 22 September 2023 on a temporary basis for a period of 3 years until 22 September 2026
- 3.2.2 In the last meeting, the Town Planning Board paper pointed out that use in support of the village and recreational development in "REC" zone and selected commercial and community uses serving the needs of the villagers in "V" zone may be permitted, and given that there is no Small House application been received for the site, it is considered that the project would not frustrate the long term planning





intentions of the “REC” and “V” zones.<sup>5</sup>

- 3.2.3 The building form and building bulk follows the approved scheme. The only difference is the increase of car parking spaces as well as site area. A table summarizing the differences between Applications No. A/NE-TKLN/57 and 58 and the current applicant is as follows:

<u>Parameters</u>	<u>Application No.</u> <u>A/NE-TKLN/57</u>	<u>Application No.</u> <u>A/NE-TKLN/58</u>	<u>Current</u> <u>Application</u>	<u>Difference</u> <u>between</u> <u>current and</u> <u>two approved</u> <u>applications</u>
Site Area	About 1,924 sq.m.	About 2,228 sq.m.	About 4,628 sq.m.	+ 476 sq.m.
Floor Area	About 204 sq.m.	About 180 sq.m.	About 384 sq.m.	Same
No. of Structure(s)	1	1	2	Same
Max. Height of Structures	6 meters	6 meters	6 meters	Same
No. of Private Car Parking Spaces	24	26	107	+ 57
No. of LGV Parking Spaces	0	0	8	+ 8
No. of Loading/ Unloading Bay for Light Goods Vehicle	1	1	2	Same

#### 4.1 Similar Approved Applications for Public Vehicle Parks

- 4.1.1 There are several similar applications for temporary vehicle park use in the vicinity of the Application Site, which falls within “REC” zone and “V” zone.
- 4.1.2 There is a similar approved application of similar uses within the same “REC” zone and “V” zone in Heung Yuen Wai area, involving different sites. The adjacent application No. A/NE-TKLN/53 for temporary public vehicle park for private cars and LGV was approved on 2023, with its access point also abuts Lin Ma Hang Road.
- 4.1.3 Planning approval for the adjacent site (No. A/NE-TKLN/53) is going to be expired in mid-2026, with another approved site (No. A/NE-TKLN/70) that will be lapsed

<sup>5</sup> [https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt\\_e.pdf](https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt_e.pdf)



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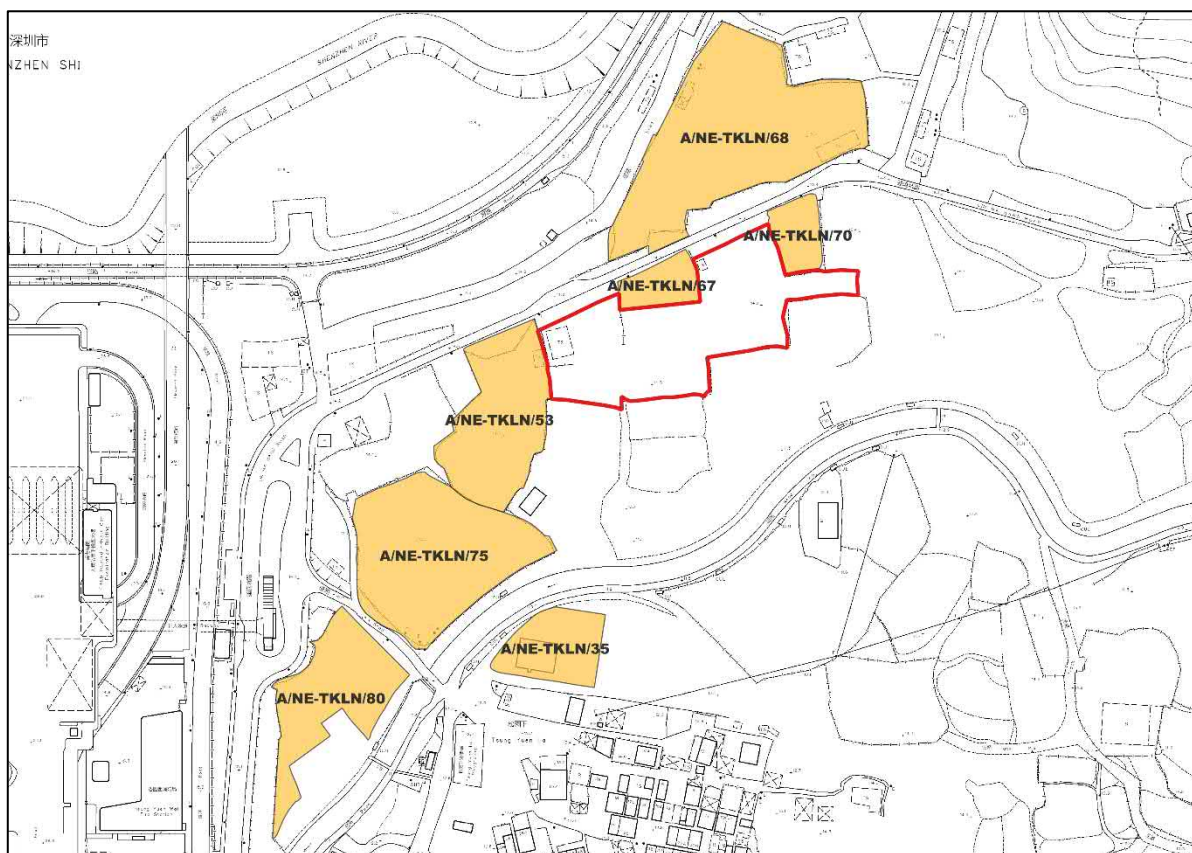
in late-2026. The shortage of car parking spaces in Heung Yuen Wai will become more serious with the lapse of application No. A/NE-TKLN/53.



<b>Application No.</b>	A/NE-TKLN/53	A/NE-TKLN/70
<b>Decision Date</b>	23/06/2023	20/04/2020
<b>Applied Use</b>	Temporary Car Park (Private Cars and Light Goods Vehicles) for a Period of 3 Years	Proposed Temporary Car Park (Private Cars and Light Goods Vehicles) for a Period of 3 Years
<b>Expire Date</b>	23/06/2026	27/10/2026
<b>Total no. of vehicle spaces</b>	69	11
<b>Private Car Parking Spaces</b>	54	11
<b>LGV Parking Spaces</b>	15	0

- 4.1.4 There are also other similar planning applications for public vehicle park and shop and services uses in the vicinity. Hence, the proposed development can also help to meet the increasing demand of the parking lots in the vicinity.

<u>Application No.</u>	A/NE-TKLN/75	A/NE-TKLN/90	A/NE-TKLN/68
<u>Decision Date</u>	15/03/2024 Approved	28/02/2025 Approved	05/04/2024 Approved
<u>Applied Use</u>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) and Shop and Services for a Period of 3 Years	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle), Eating Place and Shop and Services (Local Provision Store with Ancillary Office and Store Room) for a Period of 3 Years	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) and Shop and Services for a Period of Five Years
<u>Expire Date</u>	15/03/2027	28/02/2028	05/04/2029



**Diagram 3** Approved Similar Applications in the Vicinity  
(Source: HK GEODATA STORE, HKSAR Government)

## 4.2 High Demand for Private Car Parking Spaces in the Vicinity

- 4.2.1 HKSAR government has announced to adopt multipronged approach to cope with demand for public parking spaces at HYWBCP as of 15 February 2023. While there is a high demand for private parking spaces, the current capacity of the public car park within HYWBCP has reaches its maximum.
- 4.2.2 According to the Planning Department, the Heung Yuen Wai Port will serve cross-boundary goods vehicles and passengers traveling between Hong Kong-Shenzhen East, Huizhou, Guangdong East, Jiangxi and Fujian. Trucks moving fresh produce, livestock and poultry will continue to use the MKT BCP. It is estimated that by 2030, the average daily traffic flow of the port will be about 20,600 vehicular trips, and 30,700 passenger trips per day.





4.2.3 There is some news showing HYWBCP Public Car Park can hardly fulfil the needs from public upon the operation of the control point<sup>6</sup>, and some reserved vehicles in the car parking space did not leave on time.<sup>7</sup>

4.2.4 Among limited existing public transport services, visitors and residents may tend to drive to the nearby carparks to access the control point.

### 4.3 Drainage Consideration

4.3.1 The drainage proposal of the previously approved applications No. A/NE-TKLN/57 and 58 have been approved by the Town Planning Board on 28 February 2024. It was considered that there would be no adverse drainage impact. It is also anticipated that there will be no adverse drainage impact from the application as the existing drainage condition and pattern of the area will not be altered.

4.3.2 Under this application, peripheral channels are constructed along the site boundary. The flows inside the channels will be discharged via a terminal manhole with desilting trap into a proposed 400mm U channel, which will discharge into an existing watercourse. Water will be discharged into the existing 8m wide nullah to the south of the subject site via the existing outfalls.

4.3.3 It is anticipated that the proposed U channel will be adequate with the stated gradient. The channel flows the discharges southward which is parallel to the flow direction of adjacent surface runoff, and hence no significant quantity of surface runoff from adjacent lands will be received.

4.3.4 The design of the proposed channel will be adequate to support the surface runoff from the subject lots under this planning application. Thus, no adverse drainage impact is anticipated.

4.3.5 Details refer to indicative stormwater flow and calculation in **Annex 2**.

### 4.4 Traffic Consideration

4.4.1 Existing temporary public vehicle park does not incur adverse traffic impact. The traffic generated by the existing temporary public vehicle park is low with an estimation of 8 to 9 vehicular trips per hour on average and less than 20 vehicular trips per hour at peak hours. This estimation is similar to the previously approved application. No adverse traffic impact is anticipated.

<sup>6</sup> The standard. (February 2023). <https://www.thestandard.com.hk/section-news/section/11/249753/Border-crossing-car-park-full-up,-travelers-warned>

<sup>7</sup> RTHK. (February 2023). <https://news.rthk.hk/rthk/en/component/k2/1687545-20230212.htm>



4.4.2 While the subject carpark is proposing a flexible mode, it is anticipated that the proposed temporary vehicle park will not incur adverse traffic impact. With most of the end user would be cross-boundary travellers, they may park their cars in the Application site and stay in the Mainland for one day or more.

4.4.3 Traffic management measures proposed in section 2.10 would be fully implemented. With only 8 to 9 trips per hour generated and attracted from the application, it is expected that there will not be significant negative impacts regarding the safety of road users and the traffic network of the area concerned.

#### **4.5 Environmental Consideration**

4.5.1 The Applicant will follow the relevant mitigation measures and requirements in the latest “Code of Practice on Handling Environmental Aspects of Temporary Uses and Open Storage Sites” to minimize any potential environmental impact. No adverse environmental impact is anticipated.

#### **4.6 Visual and Landscape Compatibility**

4.6.1 The Application Site is situated in area of rural village landscape character which are surrounded by temporary structures and car parks. The proposed car park and shop and service uses are compatible with the surrounding use and will not disturb the prevailing rural village landscape character. There is no existing tree within the Application Site.

4.6.2 The visual compatibility follows the previously approved application. No additional structure would arise and the building bulk and form maintain as approved.

#### **4.7 Cumulative Impact**

4.7.1 Even with the approval of this planning application with a larger application site, it is anticipated that there will be no cumulative impact on the environmental, traffic, drainage and visual and landscape aspect. As detailed above, it is not anticipated to have interfacing issue with Lin Ma Hang Road Widening Works with setback reserved. Thus, no cumulative impact is expected from this application.



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## **5 PLANNING MERITS & JUSTIFICATIONS**

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### **5.1 Satisfy the Needs by Catering the High Demand of Parking Spaces**

- 5.1.1 Intention of the proposed temporary vehicle park is to cater the high demand of parking spaces in the HYWBCP and the vicinity. It primarily serves the local villagers and residents, as well as the cross-boundary travellers. With the relaxation of infection-control measures, it is expected that the cross-border travel between Shenzhen and Hong Kong will resume normal, and the demand of car parking spaces will increase as well. Thus, this application demonstrates strong planning merit to solve the shortage issue.

### **5.2 Echo Government's Direction to Provide More Parking Spaces**

- 5.2.1 It has been identified by the Government to process car parking spaces planning applications in a proactive manner and will expedite the process as appropriate. This planning application has strong planning merit by echoing government's direction to provide more parking spaces.
- 5.2.2 While Government eagers to provide temporary vehicle park in the vicinity, the whole land searching process takes time and could not meet the rising demand. Provision of parking spaces by private sector will be easier and quicker.

### **5.3 Situated at a Convenient Location for Vehicle Park**

- 5.3.1 The Application Site is located in a convenient location which is situated within 5 minutes walking distance to the HYWBCP. The cross-boundary travellers and local residents can easily walk to the HYWBCP via the underground tunnel.

### **5.4 Would Not Jeopardize the Long-term Planning Intention**

- 5.5 Approval of the application on a temporary basis for a period of 3 years would not jeopardize the long-term planning intention of the "REC" zone and "V" zone. Also, the site is currently vacant and partly hard-paved which does not require much site formation works. This application will help to fully utilize the land resources which is compatible with the surrounding environment.
- 5.6 With respect to the OZP, use in support of the village and recreational development in "REC" zone and selected commercial and community uses serving the needs of the villagers in "V" zone may be permitted. While part of the land within "V" zone also falls within the village 'environs' of Tsung Yuen Ha, currently there is no small house application for the Application Site. Thus, this application would not frustrate the long-term planning intentions of both "REC" and "V" zones.



## **5.7 Flexible Operation Mode to Accommodate Travellers' Need**

- 5.7.1 There are limit parking options for existing adjacent carpark, where Heung Yuen Wai Boundary Control Point Public Car Park only accepts vehicles with prior online booking to park. The proposed carpark will adopt a walk-in manner to the public and provide hourly, daily, weekly parking services upon the various needs of the cross-boundary travellers.

## **5.8 Supported by Previous Planning Approvals of Similar Applications**

- 5.8.1 Planning context has substantially changed upon the full commission of HYWBCP. There were four similar applications of temporary vehicle park within the same "REC" and "V" zone in the Ta Kwu Ling North area (Nos. A/NE-TKLN/53, 68, 75 and 90) approved with conditions by the Committee in the past 10 years mainly on the considerations of several factors, including the high compatibility of the proposed developments with the surrounding land uses and the low possibility to cause significant impacts on the traffic, environment, drainage and landscape of the area. Therefore, with the support of previous planning approvals under similar planning circumstances, the proposed temporary vehicle park and shop and services use are not inconsistent with the surrounding land uses and it is unlikely to generate adverse impacts to the surrounding area.
- 5.8.2 Besides, the applied shop and services use follows the approved scheme under the A/NE-TKLN/57 and 58(either 57 or 58??). With the full commenced?? of the HYWBCP, the applied shop and service use will continue to help satisfy the commercial needs in the vicinity.

## **5.9 Compatible with the Surrounding Development with No Adverse Visual and Landscape Impact**

- 5.9.1 The temporary public vehicle park will be visually compatible with the surrounding environment, which is mainly composed of village houses, temporary structures and storages. There is no change to the rural village landscape character as well. No adverse visual and landscape impact is anticipated.

## **5.10 No Adverse Traffic Impact**

- 5.10.1 The proposed temporary vehicle park will not incur adverse traffic impact. The traffic volume generated by the proposed temporary vehicle park is low with an estimation of around 8 to 9 trips per hour in peak hour, and most of the vehicles enter and exit the site approximately once per day only. In addition, traffic management measures have also been proposed in order to ensure pedestrian safety. Therefore, it is expected that there will not be significant negative impacts regarding the safety of road users and the traffic network of the area concerned.





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### **5.11 No Adverse Environmental Impact**

- 5.11.1 The proposed temporary vehicle park is a rental private car park. Related activities such as car washing, repairing or similar workshop activities would not be permitted on the site. Moreover, the latest “Code of Practice on Handling Environmental Aspects of Open Storage and Temporary Uses” issued by Environmental Protection Department will also be followed. Therefore, there will not be adverse environment impacts including noise and air quality.

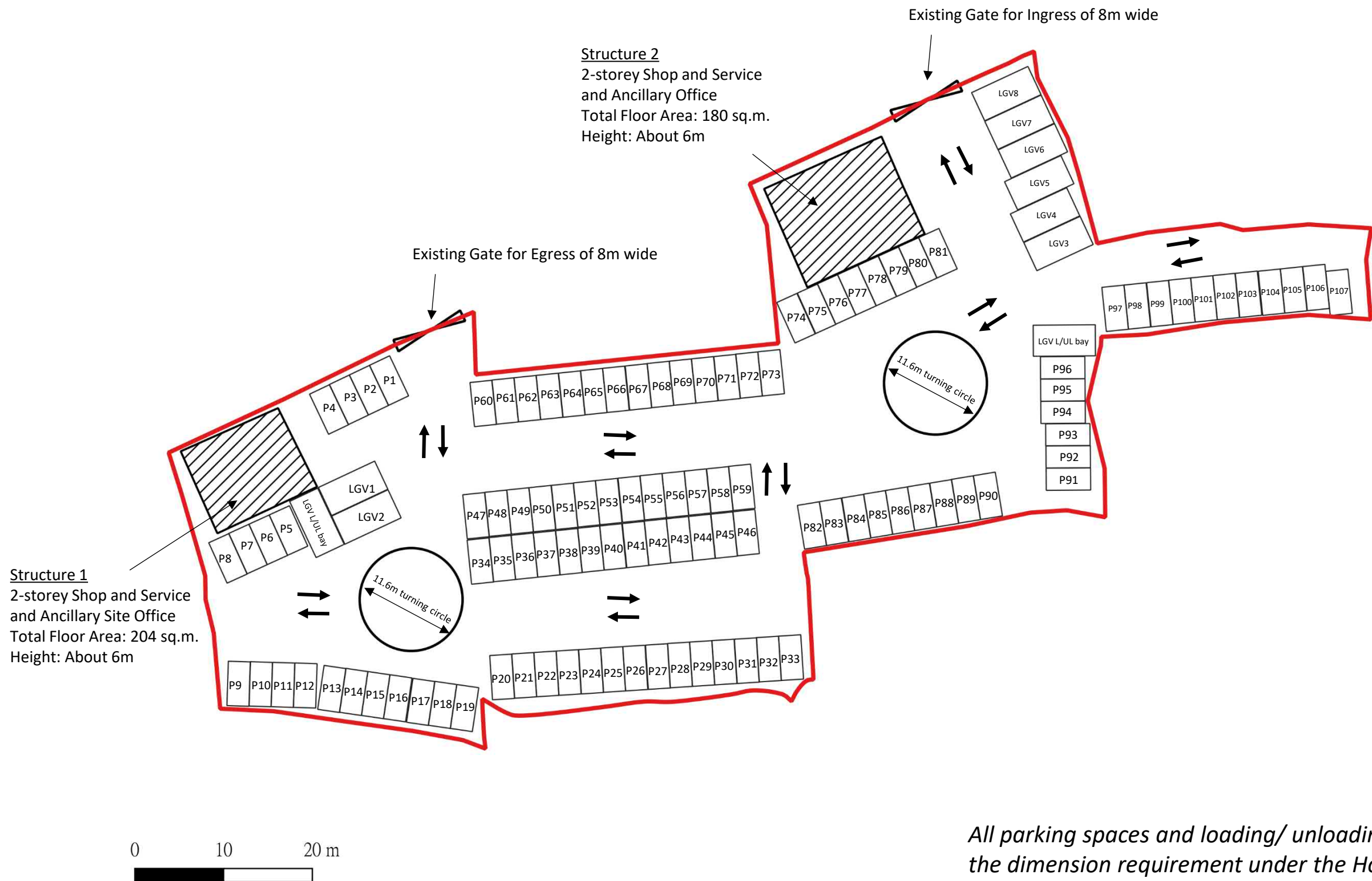
### **5.12 No Adverse Drainage Impact**

A stormwater flow diagram in Annex 2 and relevant calculation have been prepared. The proposed drainage system would improve the existing drainage conditions, and ultimate discharge to the existing water course. Thus, no adverse drainage impact is anticipated.

---

## **6 CONCLUSION**

- 6.1.1 In view of the above, we respectfully request the Board Members to give favourable consideration on this planning application.

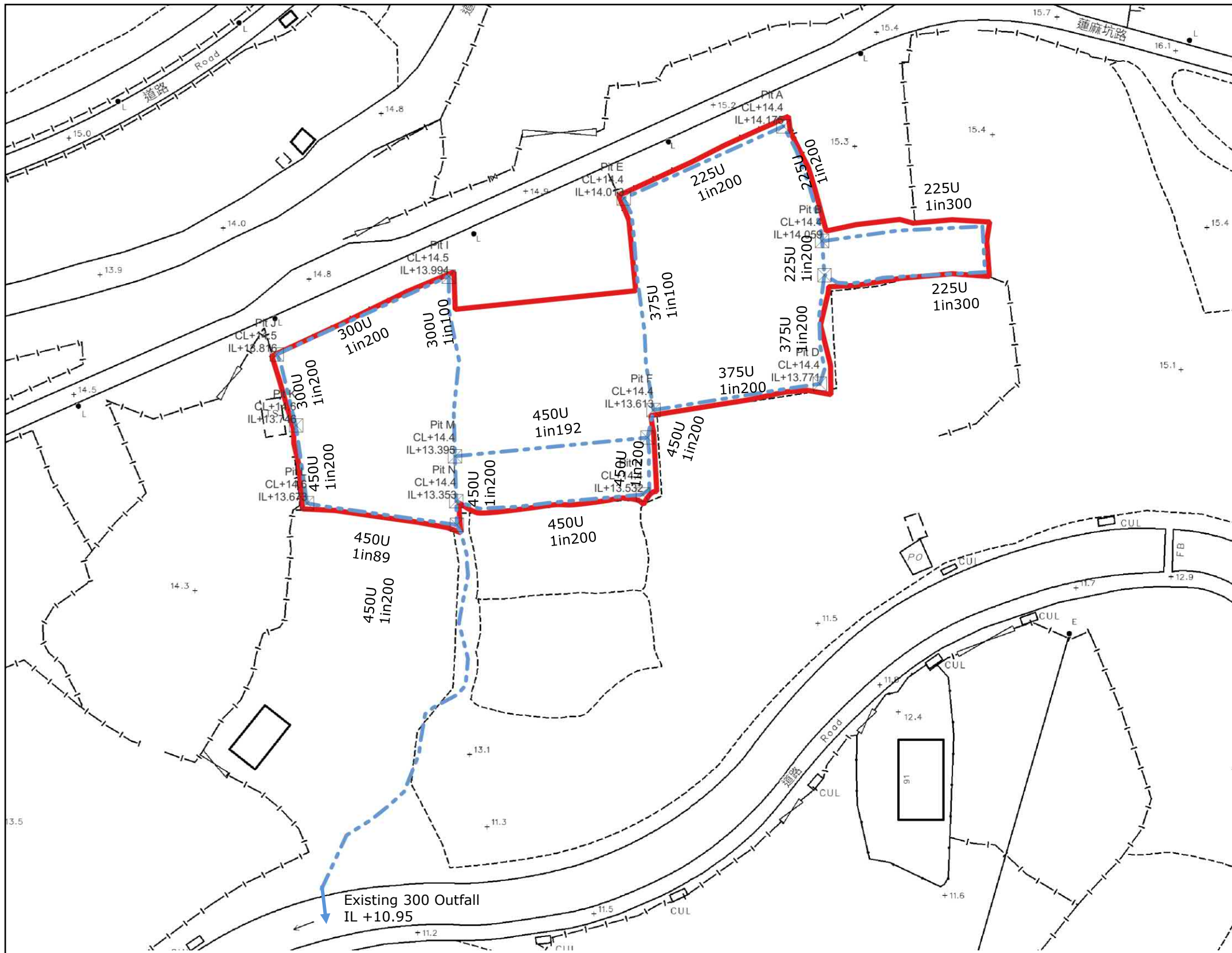


**LCH Planning and Development  
Consultants Limited**

Annex 1 : Indicative Layout Plan

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)



Note: Pit O will be following the DSD Standard Drawings No. DS 1054E for Terminal Manhole Type T2

- Application Boundary
- - - Proposed U Channel (with reinforced concrete cover)
- Proposed Catchpit
- Existing Outfall



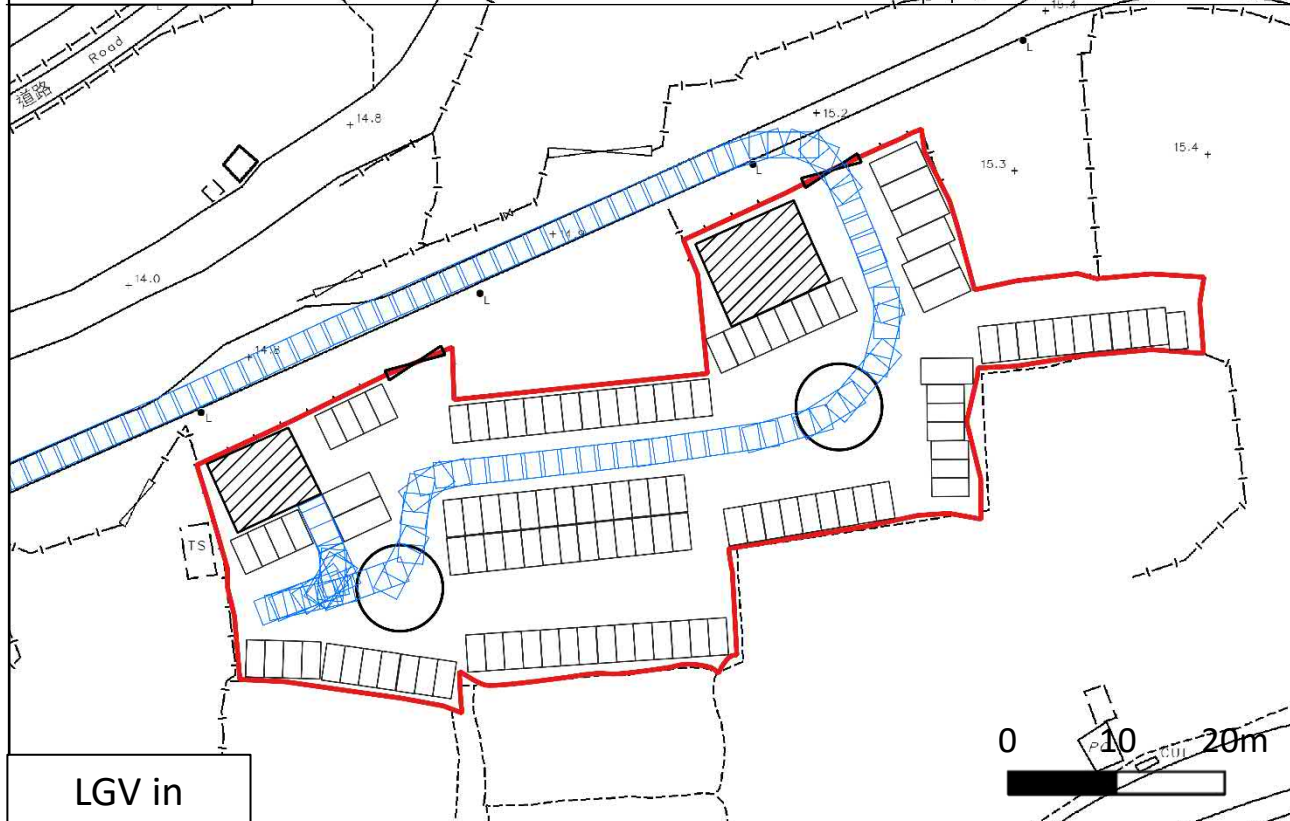
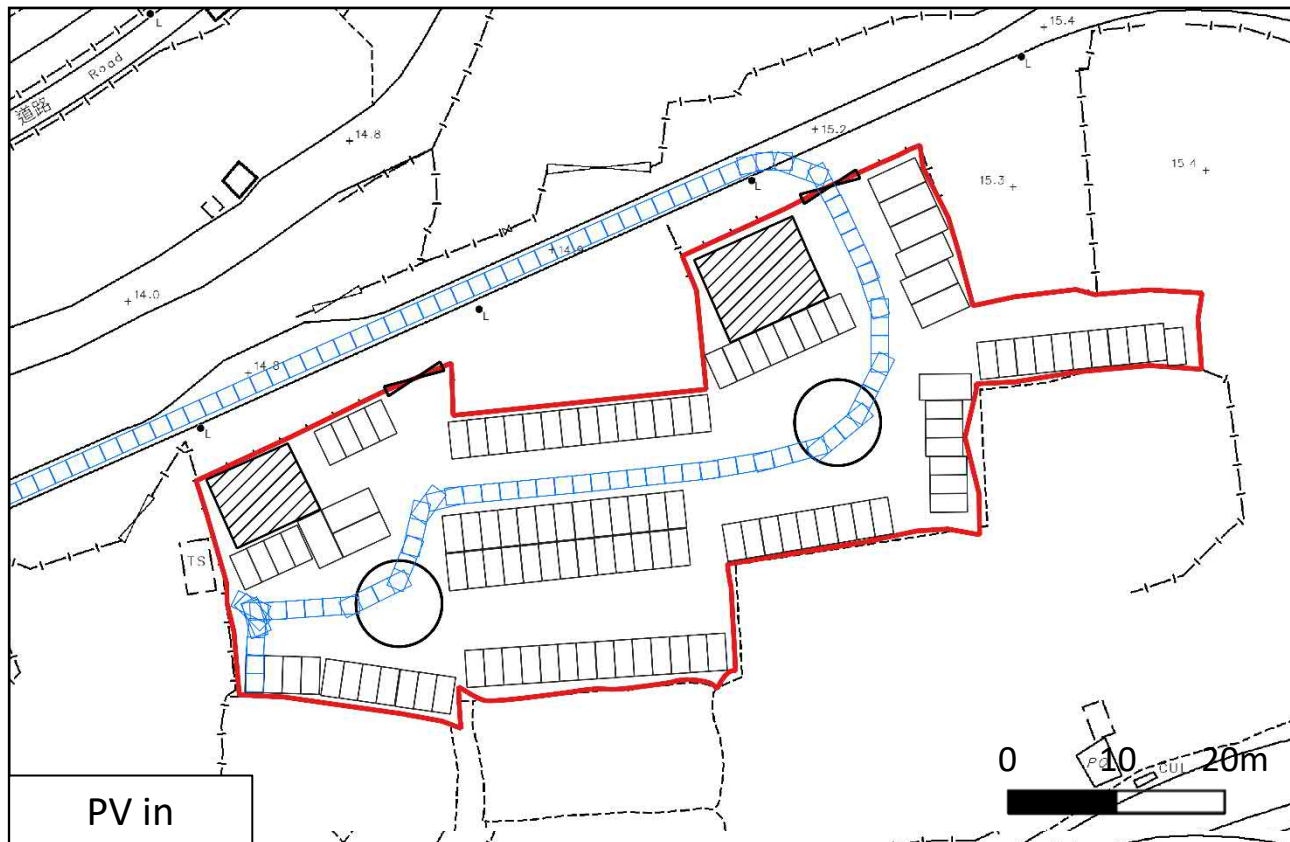
**LCH Planning and Development Consultants Limited**

Annex 2 : Indicative Stormwater Flow

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)





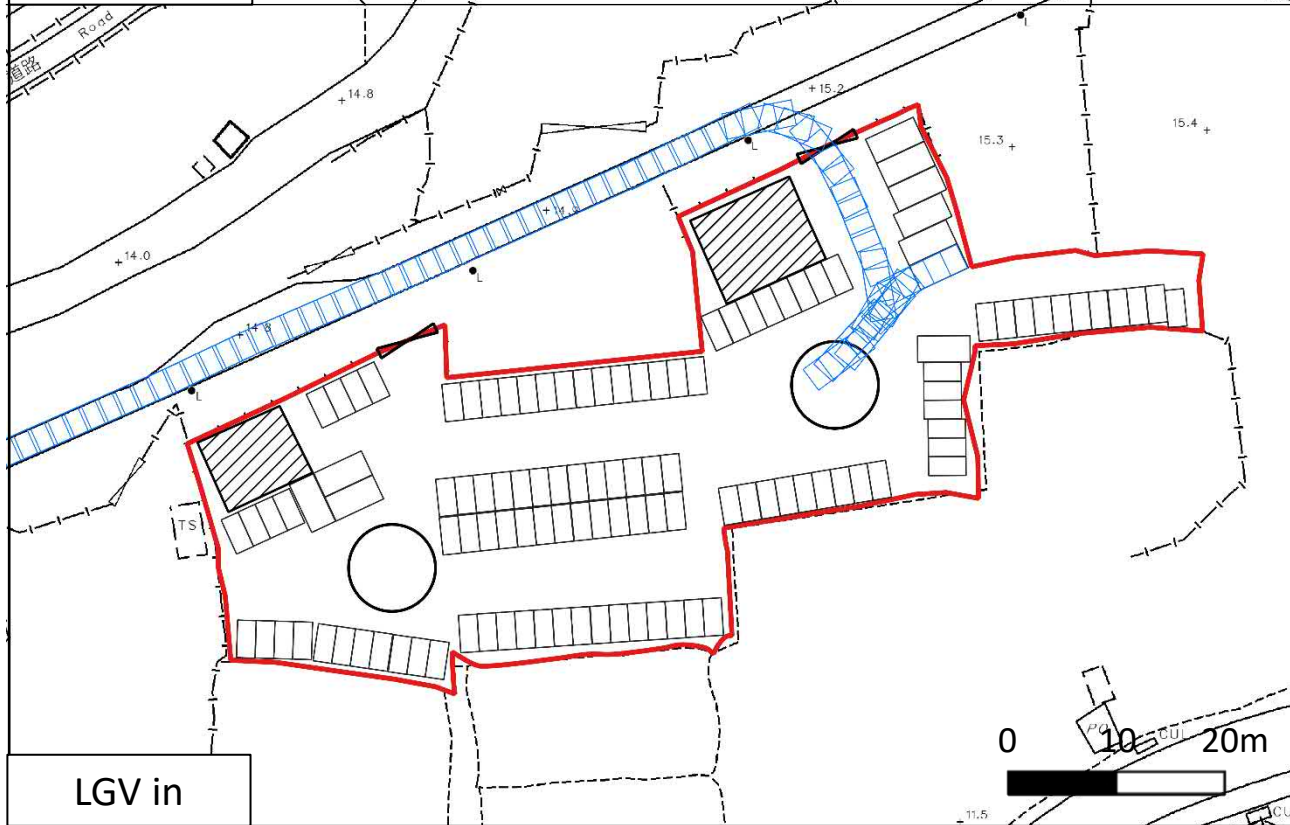
**LCH Planning and Development  
Consultants Limited**

Annex 3 : Swept Path Analysis

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

*(Source: HK GEODATA STORE, HKSAR Government)*





**LCH Planning and Development  
Consultants Limited**

Annex 3 : Swept Path Analysis

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)

## Annex 4

## 規 劃 署



沙田、大埔及北區規劃處  
香港新界沙田上禾輦路一號  
沙田政府合署  
十三樓 1301-1314 室

## Planning Department

Sha Tin, Tai Po & North District Planning Office  
Rooms 1301-1314, 13/F,  
Shatin Government Offices,  
1 Sheung Wo Che Road, Sha Tin,  
N.T., Hong Kong

來函檔號 Your Reference:  
本署檔號 Our Reference: ( ) in TPB/A/NE-TKLN/57  
電話號碼 Tel. No.: 2158 6220  
傳真機號碼 Fax No.: 2691 2806

LCH Planning & Development Consultants Ltd.  
17<sup>th</sup> Floor, Champion Building  
289-291 Des Voeux Road Central, Hong Kong  
(Attn: Junior Ho)

**By Post and Fax (2167 8557)**

28 February 2024

Dear Sir/Madam,

**Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) for a Period of 3 Years in "Village Type Development" and "Recreation" Zones, Lots 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot 61 S.B RP in D.D. 80, Heung Yuen Wai, Ta Kwu Ling North (Compliance with Approval Condition (f) for Planning Application No. A/NE-TKLN/57)**

I refer to your submission received by this office on 22.1.2024 for compliance with approval condition (f) in relation to the submission of a drainage proposal under the captioned planning application.

Chief Engineer/Mainland North, Drainage Services Department (Contact person: Mr. LEE Wai Chung; Tel. No.: 2300 1274) has been consulted and advised that approval condition (f) is considered complied with. Please proceed to implement the accepted drainage proposal for compliance with approval condition (g). In order to facilitate compliance checking, you are required to inform this office and submit completed drainage works record for inspection.

Should you have any queries, please feel free to contact Mr. William WONG of this department at 2158 6164.

Yours faithfully,

(Margaret CHAN)  
for Director of Planning

**規 劃 署**

沙田、大埔及北區規劃處  
香港新界沙田上禾輦路一號  
沙田政府合署  
十三樓 1301-1314 室

**Planning Department**

Sha Tin, Tai Po & North District Planning Office  
Rooms 1301-1314, 13/F,  
Shatin Government Offices,  
1 Sheung Wo Che Road, Sha Tin,  
N.T., Hong Kong

來函檔號 Your Reference:  
本署檔號 Our Reference: ( ) in TPB/A/NE-TKLN/58  
電話號碼 Tel. No.: 2158 6220  
傳真機號碼 Fax No.: 2691 2806

LCH Planning & Development Consultants Ltd.  
17<sup>th</sup> Floor, Champion Building  
289-291 Des Voeux Road Central, Hong Kong  
(Attn: Junior Ho)

**By Post and Fax (2167 8557)**

28 February 2024

Dear Sir/Madam,

**Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) for a Period of 3 Years in "Recreation" Zone, Lot 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, Ta Kwu Ling North (Compliance with Approval Condition (d) for Planning Application No. A/NE-TKLN/58)**

I refer to your submission received by this office on 22.1.2024 for compliance with approval condition (d) in relation to the submission of a drainage proposal under the captioned planning application.

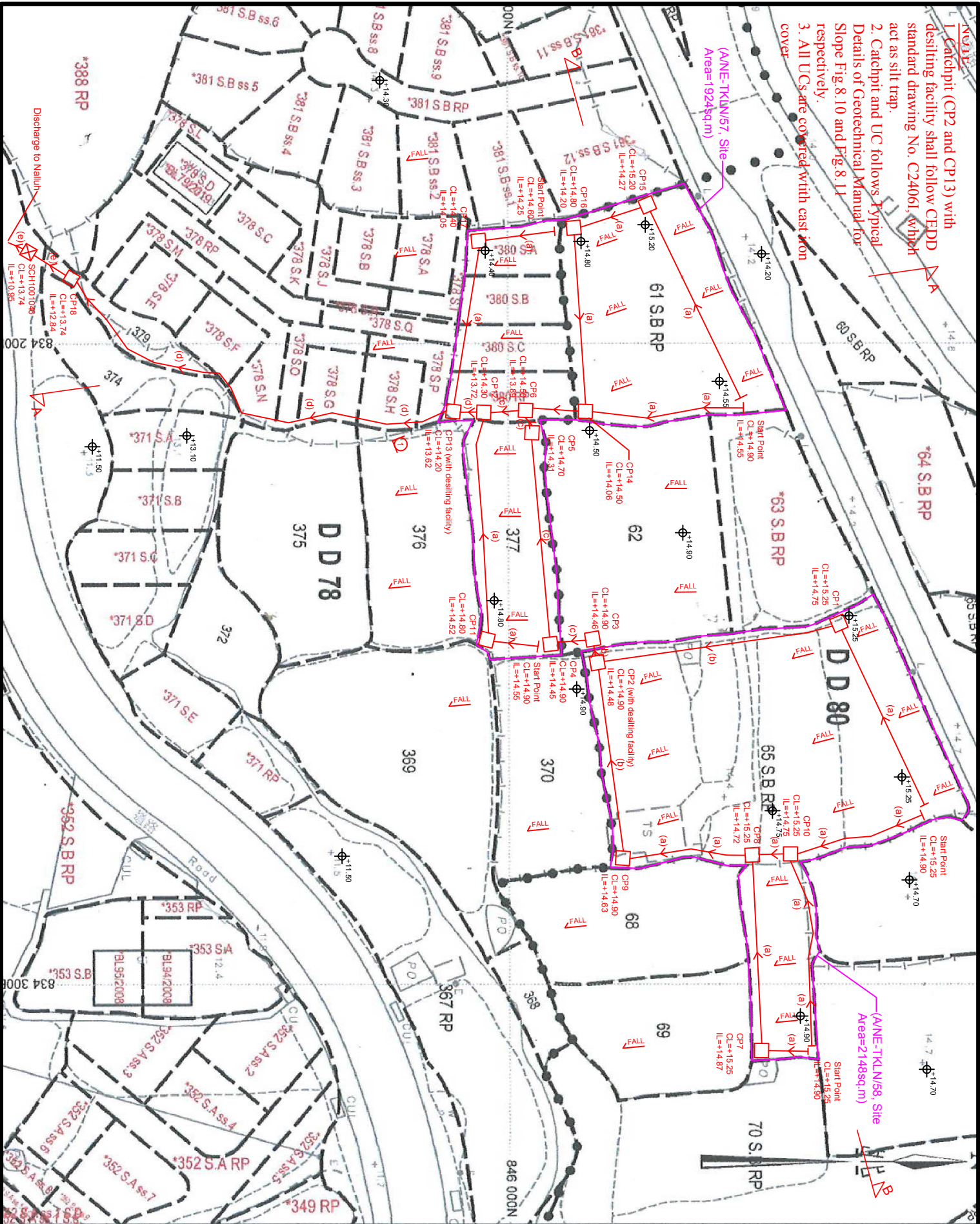
Chief Engineer/Mainland North, Drainage Services Department (Contact person: Mr. LEE Wai Chung; Tel. No.: 2300 1274) has been consulted and advised that approval condition (d) is considered complied with. Please proceed to implement the accepted drainage proposal for compliance with approval condition (e). In order to facilitate compliance checking, you are required to inform this office and submit completed drainage works record for inspection.

Should you have any queries, please feel free to contact Mr. William WONG of this department at 2158 6164.

Yours faithfully,

( Margaret CHAN )  
for Director of Planning





1. Catchpit (CP2 and CP13) with deslitting facility shall follow CEDD standard drawing No. C24061, which act as silt trap.
2. Catchpit and UC follows Typical Details of Geotechnical Manual for Slope Fig.8.10 and Fig.8.11 respectively.
3. All UCs are covered with cast iron cover.

(ANE-TKLN/57, Site Area=1924sq.m)

(ANE-TKLN/58, Site Area=2148sq.m)

LEGEND	
(a)	Proposed 300UC (1'200) with Cast iron cover
(b)	Proposed 450UC (1'200) with Cast iron cover
(c)	Proposed 525UC (1'200) with Cast iron cover
(d)	Proposed 675UC (1'200) with Cast iron cover
(e)	Proposed 675 underground concrete pipe (1'175)
(f)	Proposed 675 underground concrete pipe (1'175)
(g)	Proposed 675 underground concrete pipe (1'175)
(h)	Proposed 675 underground concrete pipe (1'175)
(i)	Proposed 675 underground concrete pipe (1'175)
(j)	Proposed 675 underground concrete pipe (1'175)
(k)	Proposed 675 underground concrete pipe (1'175)
(l)	Proposed 675 underground concrete pipe (1'175)
(m)	Proposed 675 underground concrete pipe (1'175)
(n)	Proposed 675 underground concrete pipe (1'175)
(o)	Proposed 675 underground concrete pipe (1'175)
(p)	Proposed 675 underground concrete pipe (1'175)
(q)	Proposed 675 underground concrete pipe (1'175)
(r)	Proposed 675 underground concrete pipe (1'175)
(s)	Proposed 675 underground concrete pipe (1'175)
(t)	Proposed 675 underground concrete pipe (1'175)
(u)	Proposed 675 underground concrete pipe (1'175)
(v)	Proposed 675 underground concrete pipe (1'175)
(w)	Proposed 675 underground concrete pipe (1'175)
(x)	Proposed 675 underground concrete pipe (1'175)
(y)	Proposed 675 underground concrete pipe (1'175)
(z)	Proposed 675 underground concrete pipe (1'175)

Company: 正宏工程顧問公司  
Ching Wan Engineering Consultants Company

PROJECT: Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) with Ancillary Office for a Period of 3 Years at Lots 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot 61 S.B RP in D.D. 80, Heung Yuen Wai, Ta Kwu Ling North, N.T.

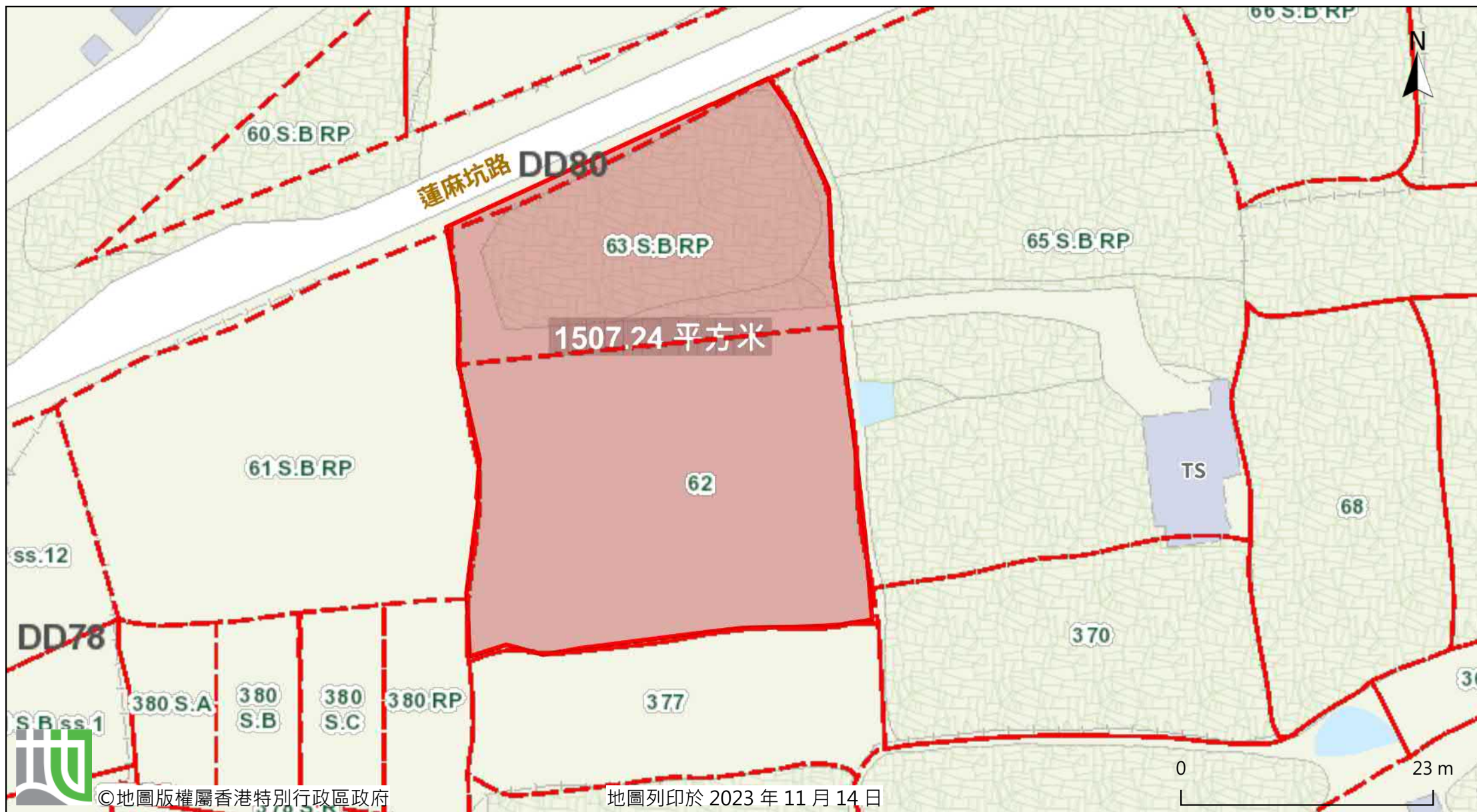
(ANE-TKLN/57)  
Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) with Ancillary Office for a Period of 3 Years at Lot 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, Ta Kwu Ling North, N.T. (ANE-TKLN/58)

TITLE: Drainage Proposal

Date: 19-1-2024  
DWG. NO. TKLN/57&58-D01

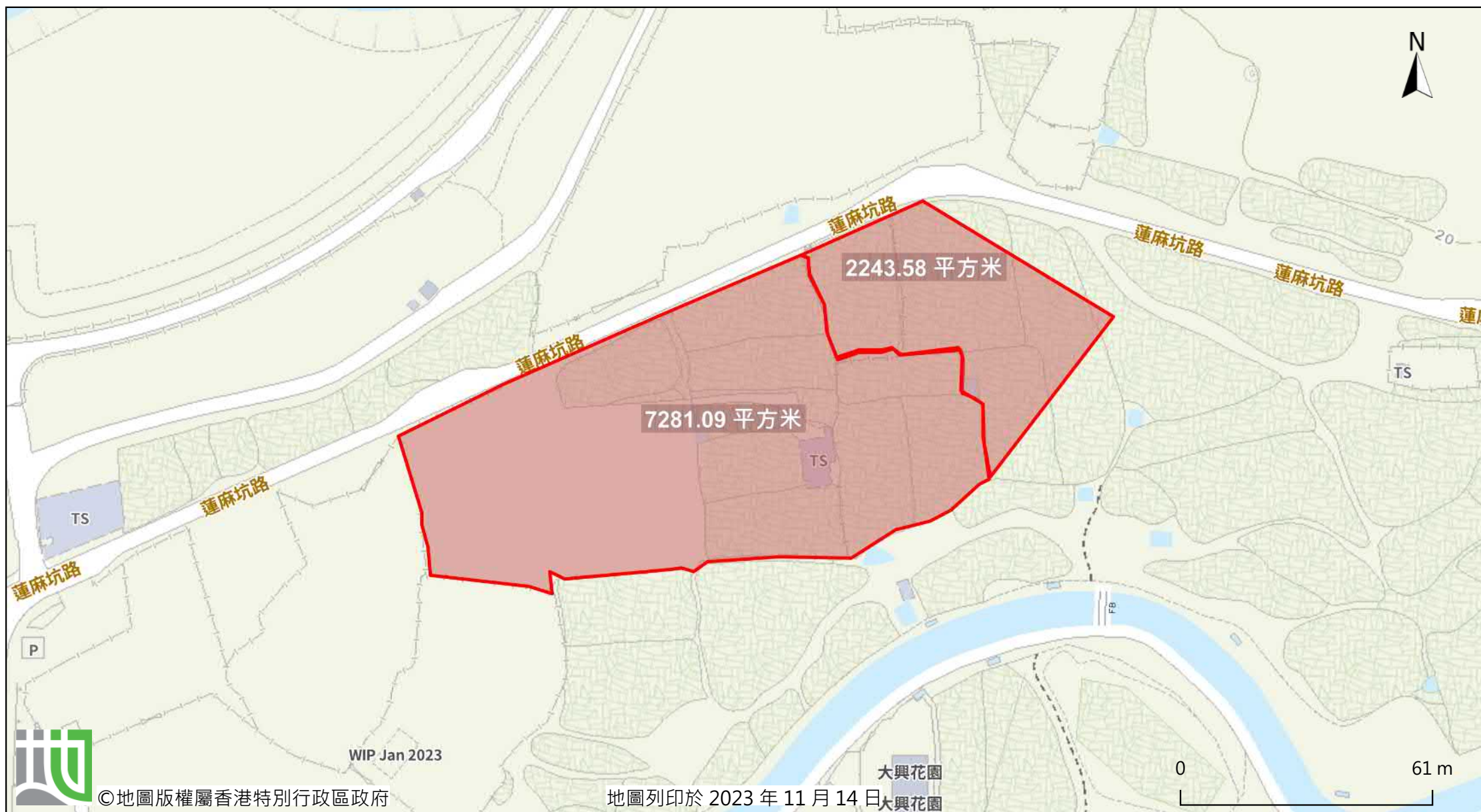






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**A/NE-TKLN/57**

$$\text{Site Area} = 962 \text{ m}^2 \quad (1924/2)$$

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$\begin{aligned} A &= 962 \text{ m}^2 \\ &= 0.000962 \text{ km}^2 \end{aligned}$$

$$\text{take } i = 250 \text{ mm/hr}$$

$$\begin{aligned} \text{Therefore, } Q &= 0.278 * 0.95 * 250 * 0.001924 \\ &= 0.064 \text{ m}^3/\text{sec} \\ &= \underline{\underline{3811}} \text{ lit/min} \end{aligned}$$

**Provide 300UC (1:200) is OK**

**A/NE-TKLN/58**

$$\text{Site Area} = 2148 \text{ m}^2 \quad (C=0.95)$$

$$\text{Outside Catchment Area} = 2244 \text{ m}^2 \quad (C=0.25)$$

$$\text{Lot 62 \& 63BRP} = 1507 \text{ m}^2 \quad (C=0.95)$$

Calculation of Runoff (Outside Catchment Area Only),

$$Q = 0.278 C i A$$

$$C = 0.25 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$\begin{aligned} A &= 2244 \text{ m}^2 \\ &= 0.002244 \text{ km}^2 \end{aligned}$$

$$\text{take } i = 250 \text{ mm/hr}$$

$$\begin{aligned} \text{Therefore, } Q &= 0.278 * 0.95 * 250 * 0.002244 \\ &= 0.039 \text{ m}^3/\text{sec} \\ &= \underline{\underline{2339}} \text{ lit/min} \end{aligned}$$

**Provide 300UC (1:200) is OK**

Calculation of Runoff (Site Area + Outside Catchment Area),

$$Q = 0.278 C i A$$

$$\text{take } i = 250 \text{ mm/hr}$$

$$\begin{aligned} \text{Therefore, } Q &= 0.278 * 0.95 * 250 * 0.002148 + 0.278 * 0.25 * 250 * 0.002244 \\ &= 0.181 \text{ m}^3/\text{sec} \\ &= \underline{\underline{10849}} \text{ lit/min} \end{aligned}$$

**Provide 450UC (1:200) is OK**

Calculation of Runoff (Site Area + Outside Catchment Area+Lot62&63BRP),

$$Q = 0.278 C i A$$

$$\text{take } i = 250 \text{ mm/hr}$$

$$\begin{aligned} \text{Therefore, } Q &= 0.278 * 0.95 * 250 * (0.002148 + 0.001507) + 0.278 * 0.25 * 250 * 0.002244 \\ &= 0.280 \text{ m}^3/\text{sec} \\ &= \underline{\underline{16819}} \text{ lit/min} \end{aligned}$$

**Provide 525UC (1:200) is OK**



Site Area = 7281 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

take  $i = 250$  mm/hr

Therefore,  $Q = 0.278 \times 0.95 \times 250 \times 0.007281 + 0.278 \times 0.25 \times 250 \times 0.002244$   
 $= 0.520$  m<sup>3</sup>/sec  
 $= \underline{\underline{31183}}$  lit/min

**Provide 675UC (1:200) is OK**

Calculation Maximum Capacity of Proposed 675mm dia. Underground pipe.

Manning Equation  $V = R^{2/3} \times S_f^{0.5} / n$

where  $R = \frac{\pi r^2}{2 \pi r}$  dia 675 mm  
 $= r/2$  r = 0.3375 m  
 $= 0.16875$  m

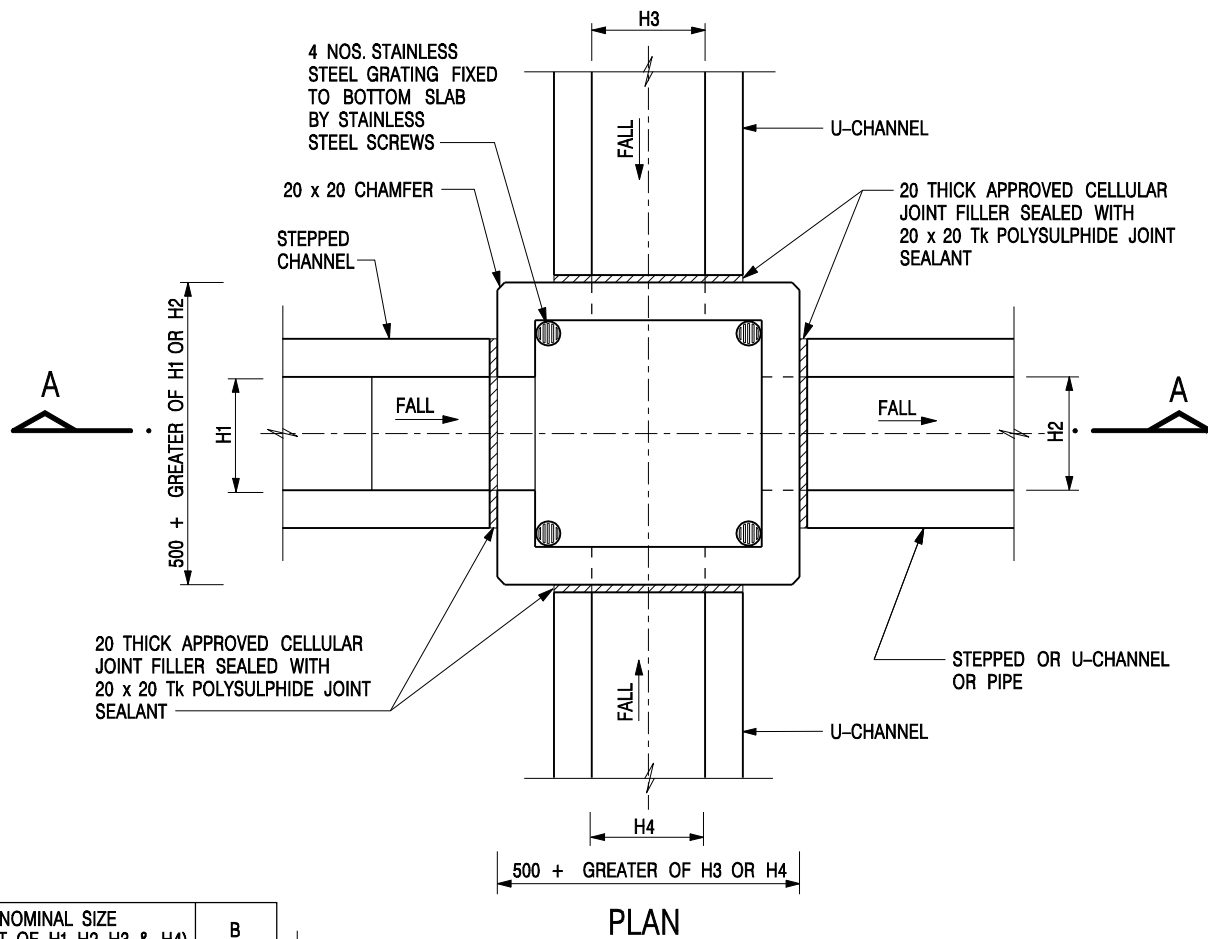
$n = 0.012$  s/m<sup>1/3</sup> (Table 13 of Stormwater Drainage Manual)

1/ 175  $S_f = 0.0057$

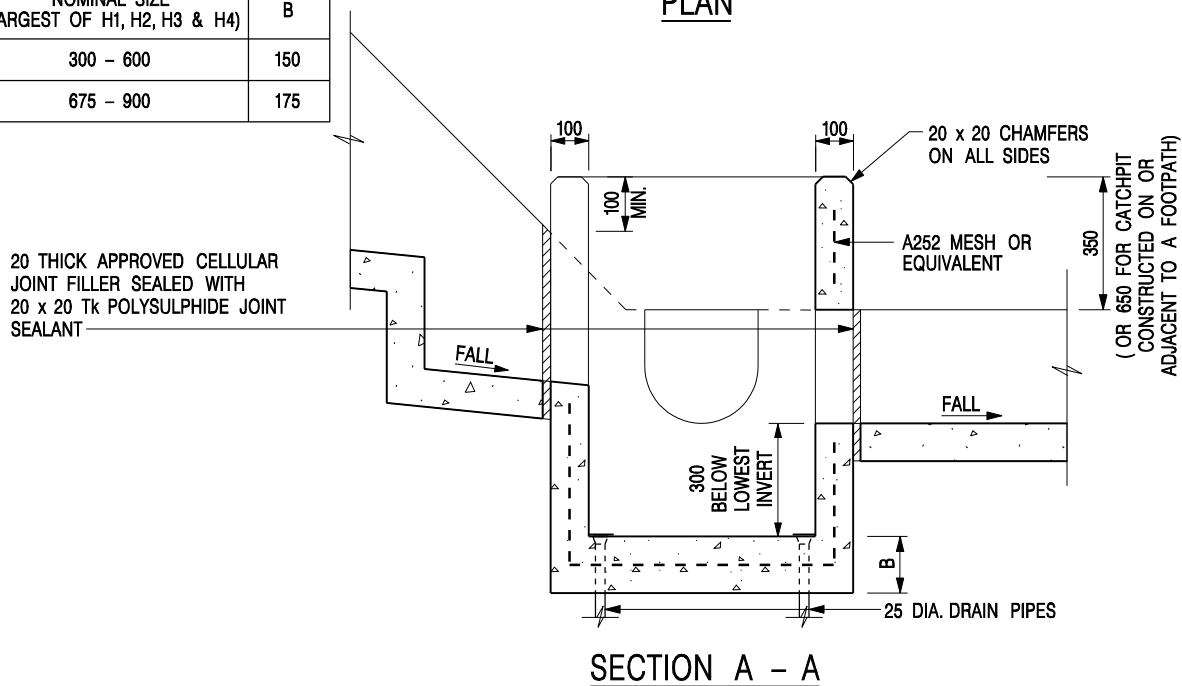
Therefore,  $V = 0.16875^{2/3} \times 0.0057^{0.5} / 0.012$   
 $= 1.924$  m/sec

Maximum Capacity ( $Q_{\max}$ )  $= V \times A$   
 $= 1.924 \times \pi r^2$   
 $= 0.688$  m<sup>3</sup>/sec  
1 nos of pipe  $= 0.688$  m<sup>3</sup>/sec  
 $= 41303$  lit/min  
 $> 31183$  lit/min

**Provide 675mm dia underground pipe (1:175) is OK**




NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

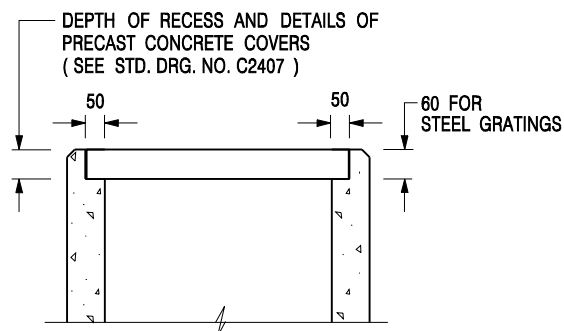


**NOTES:**

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

**CATCHPIT WITH TRAP**  
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 <b>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</b>		SCALE	1 : 20
		DATE	JAN 1991
		DRAWING NO.	C2406 /1



### ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

#### NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS ( SEE DETAIL 'A' ON STD. DRG. NO. C2405 ) OR CONCRETE COVERS ( SEE STD. DRG. NO. C2407 ) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING ( SEE DETAIL 'G' ON STD. DRG. NO. C2405; EXCEPT ON THE UPSLOPE SIDE ) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS ( SEE DSD STD. DRG. NO. DS1043 ) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'F' ON STD. DRG. NO. C2405.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP**  
**(SHEET 2 OF 2)**



**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

**DRAWING NO.**

**C2406 /2**

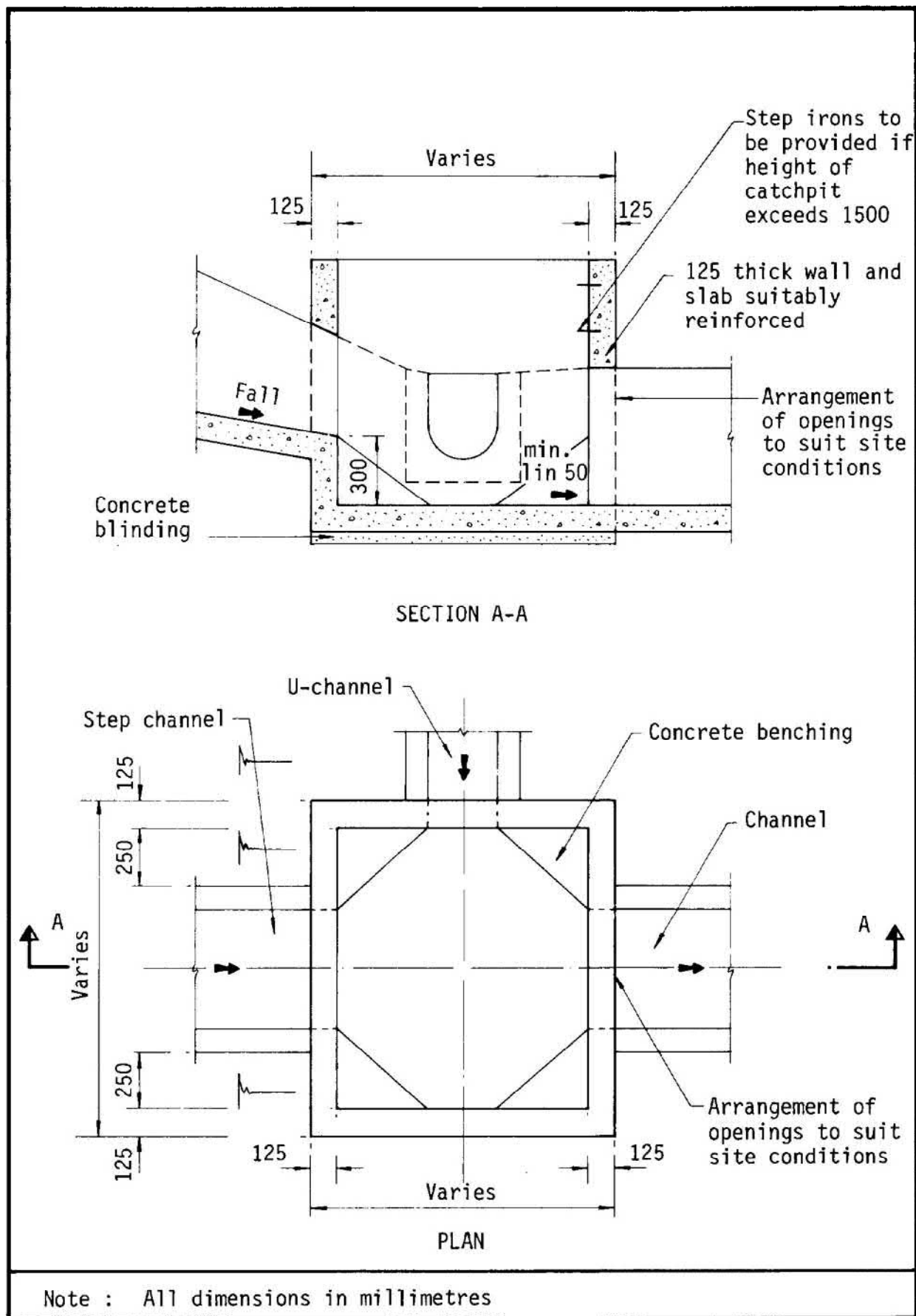
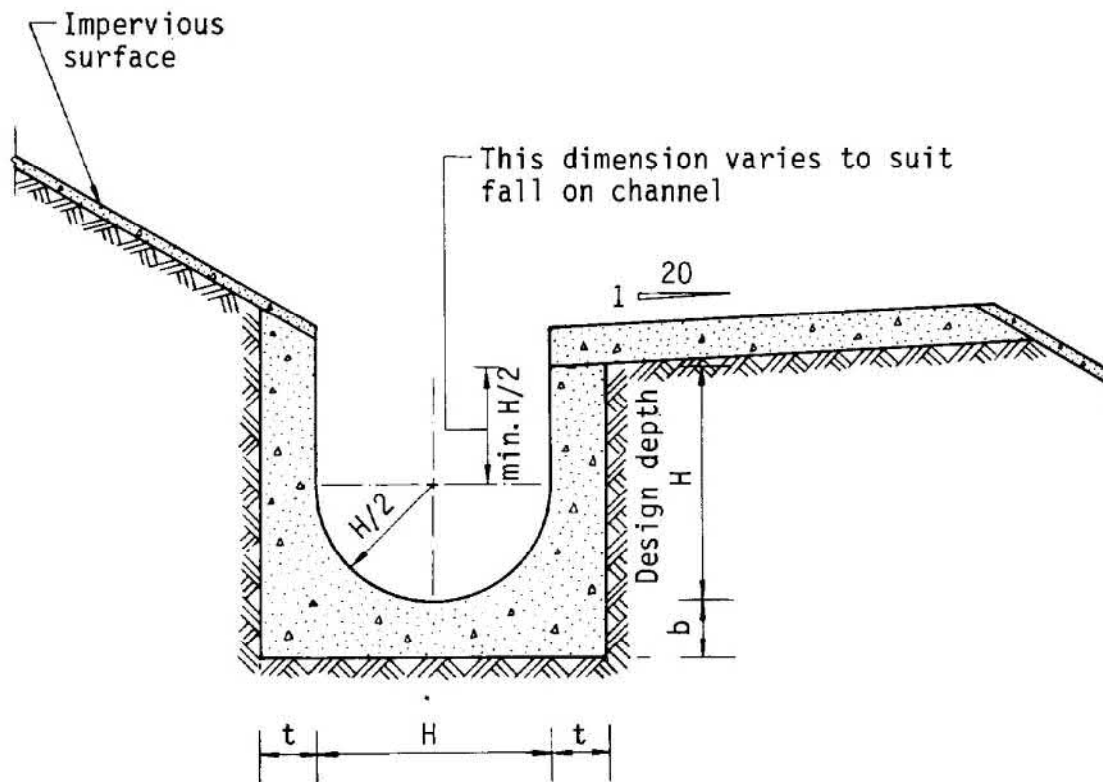


Figure 8.10 - Typical Details of Catchpits





Dimensions of U - channel

Nominal size of channel H (mm)	Thickness t (mm)	Thickness b (mm)
225 to 600	150	150
675 to 1200	175	225

Figure 8.11 - Typical U-channel Details

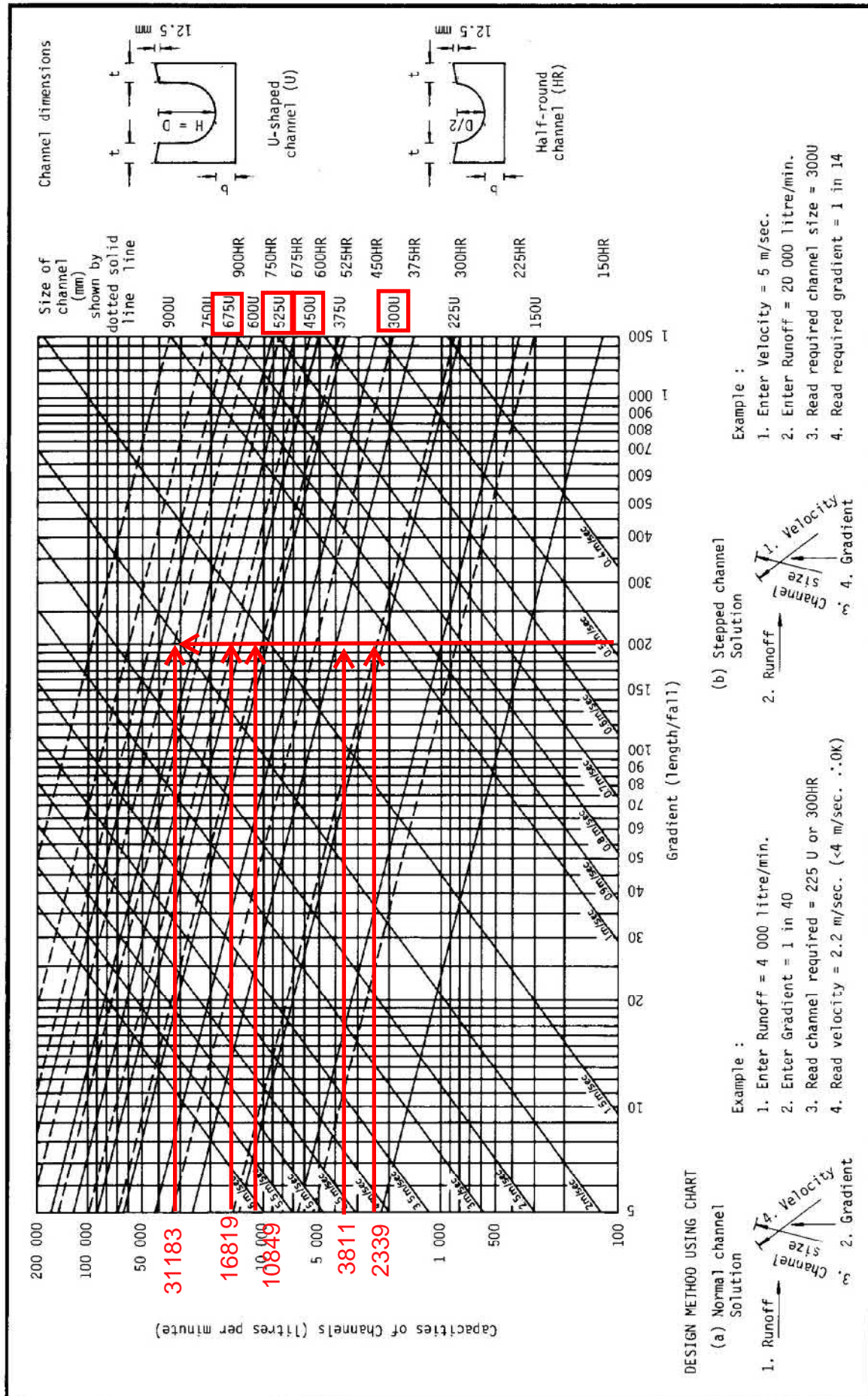
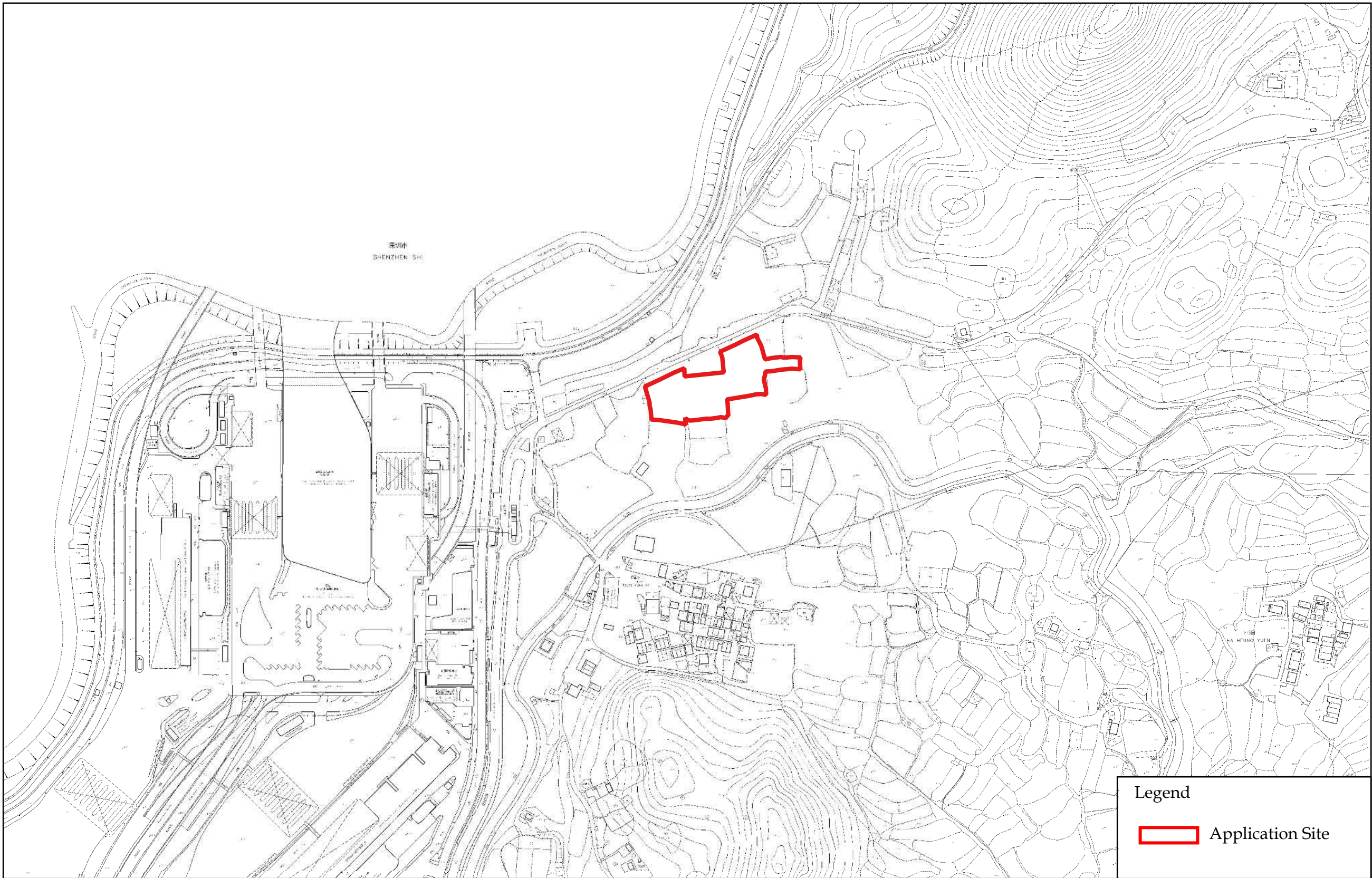


Figure 8.7 - Chart for the Rapid Design of Channels









**LCH Planning and Development  
Consultants Limited**

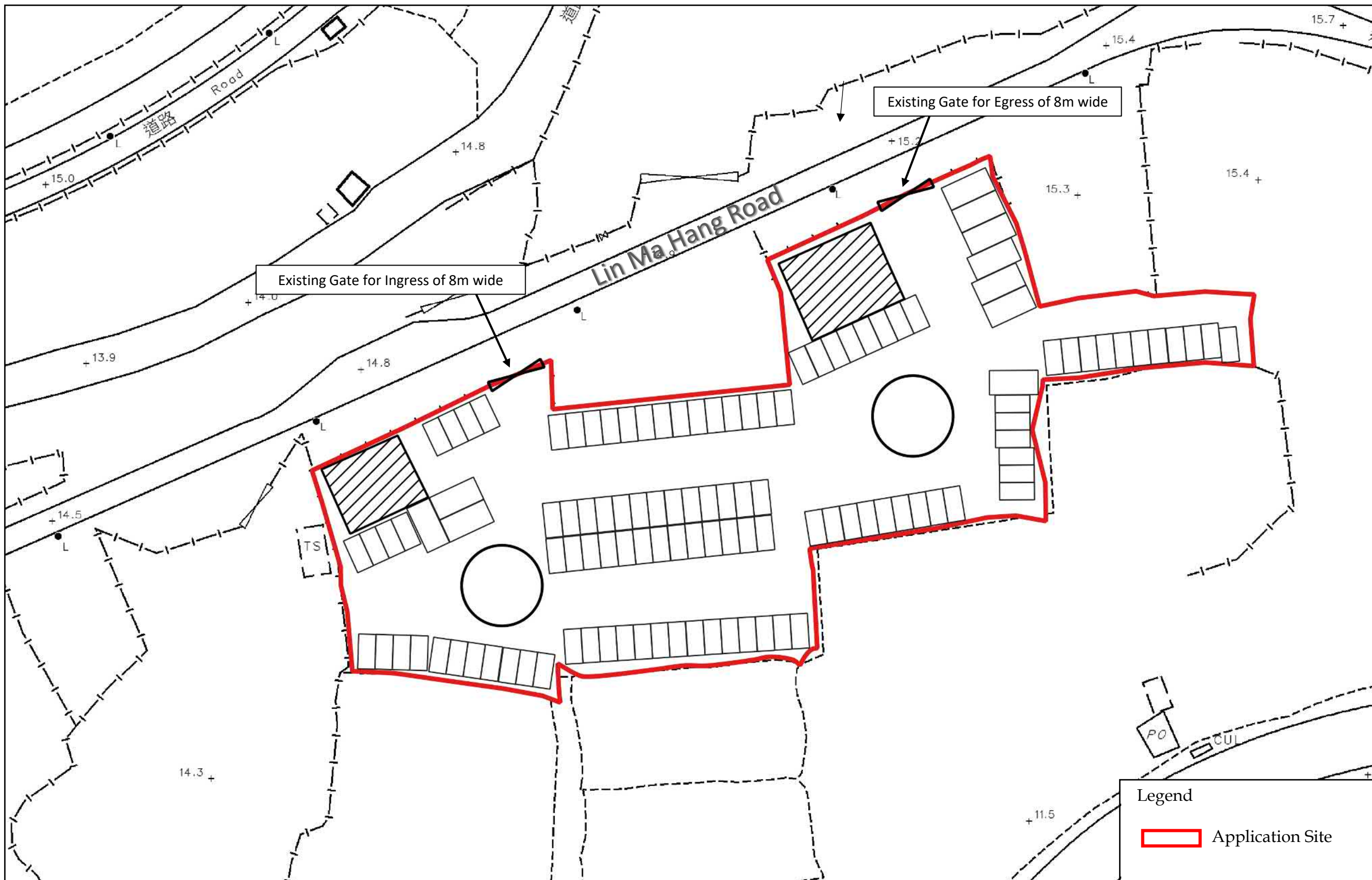
Figure 1 : Location Plan

*(For reference only. Not to scale.)*

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

*(Source: HK GEODATA STORE, HKSAR Government)*





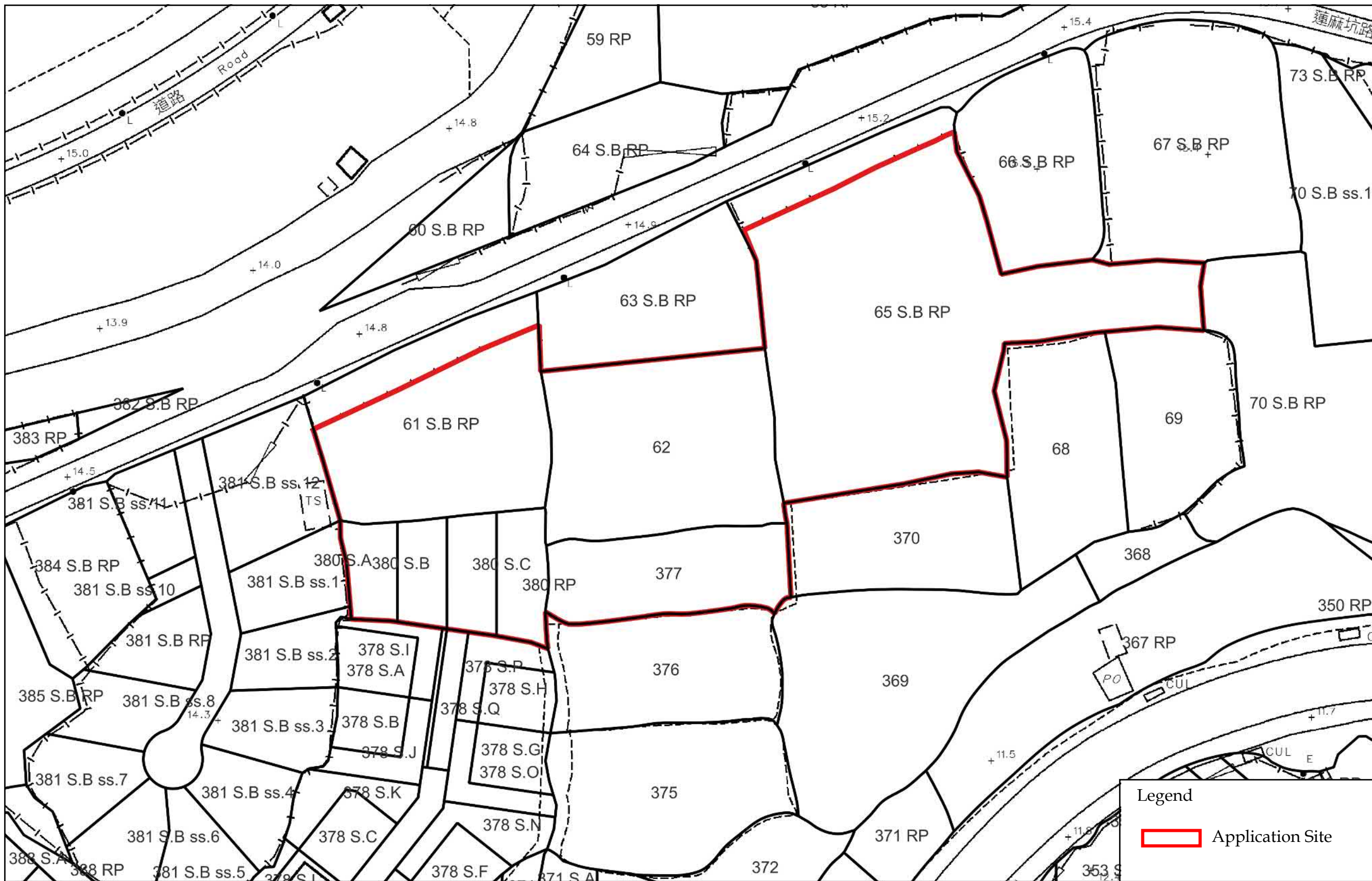
**LCH Planning and Development  
Consultants Limited**

Figure 2 : Site Plan

*(For reference only. Not to scale.)*

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

*(Source: HK GEODATA STORE, HKSAR Government)*



**LCH Planning and Development  
Consultants Limited**

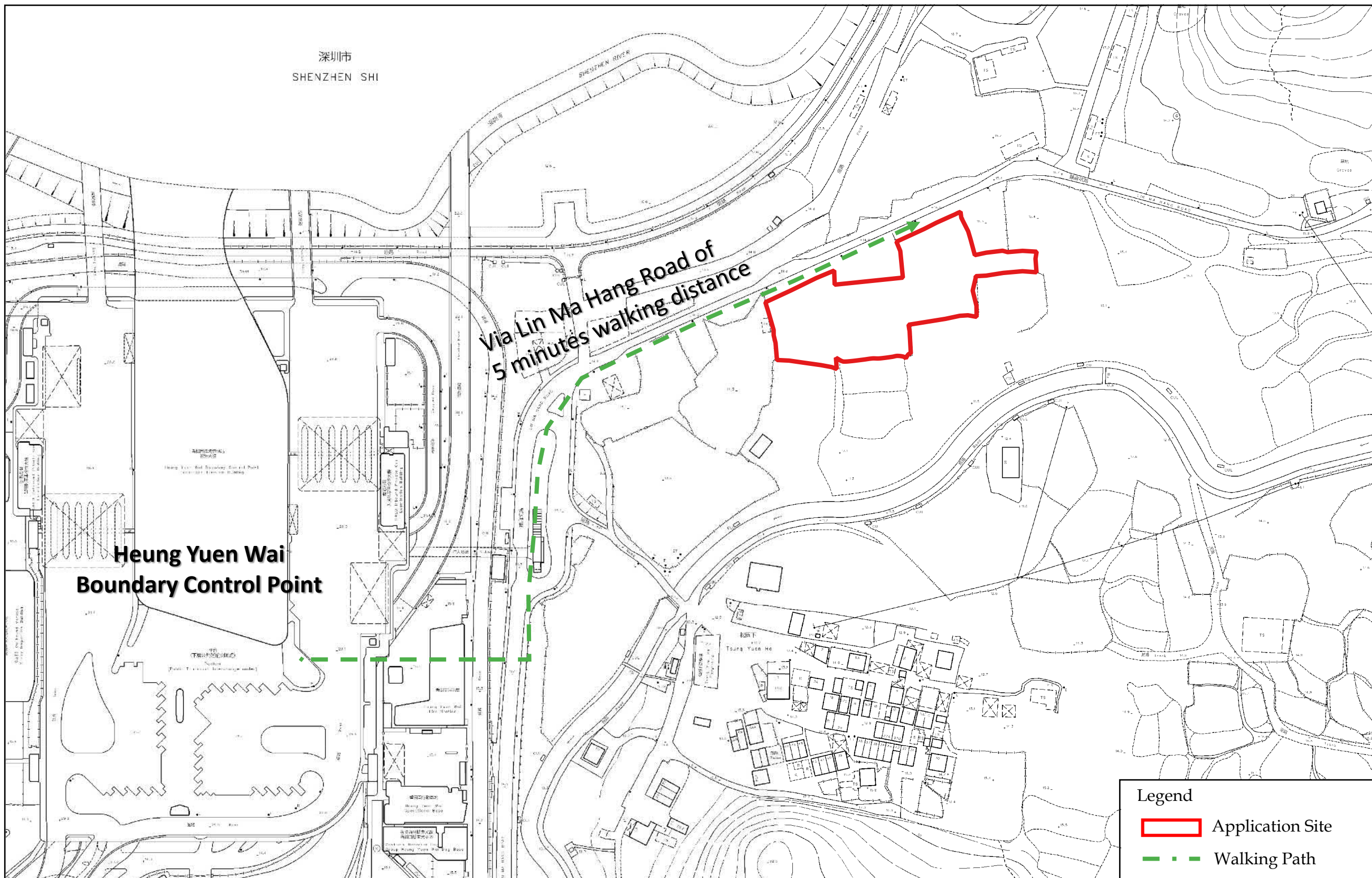
Figure 3 : Land Status

(For reference only. Not to scale.)

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)





**LCH Planning and Development  
Consultants Limited**

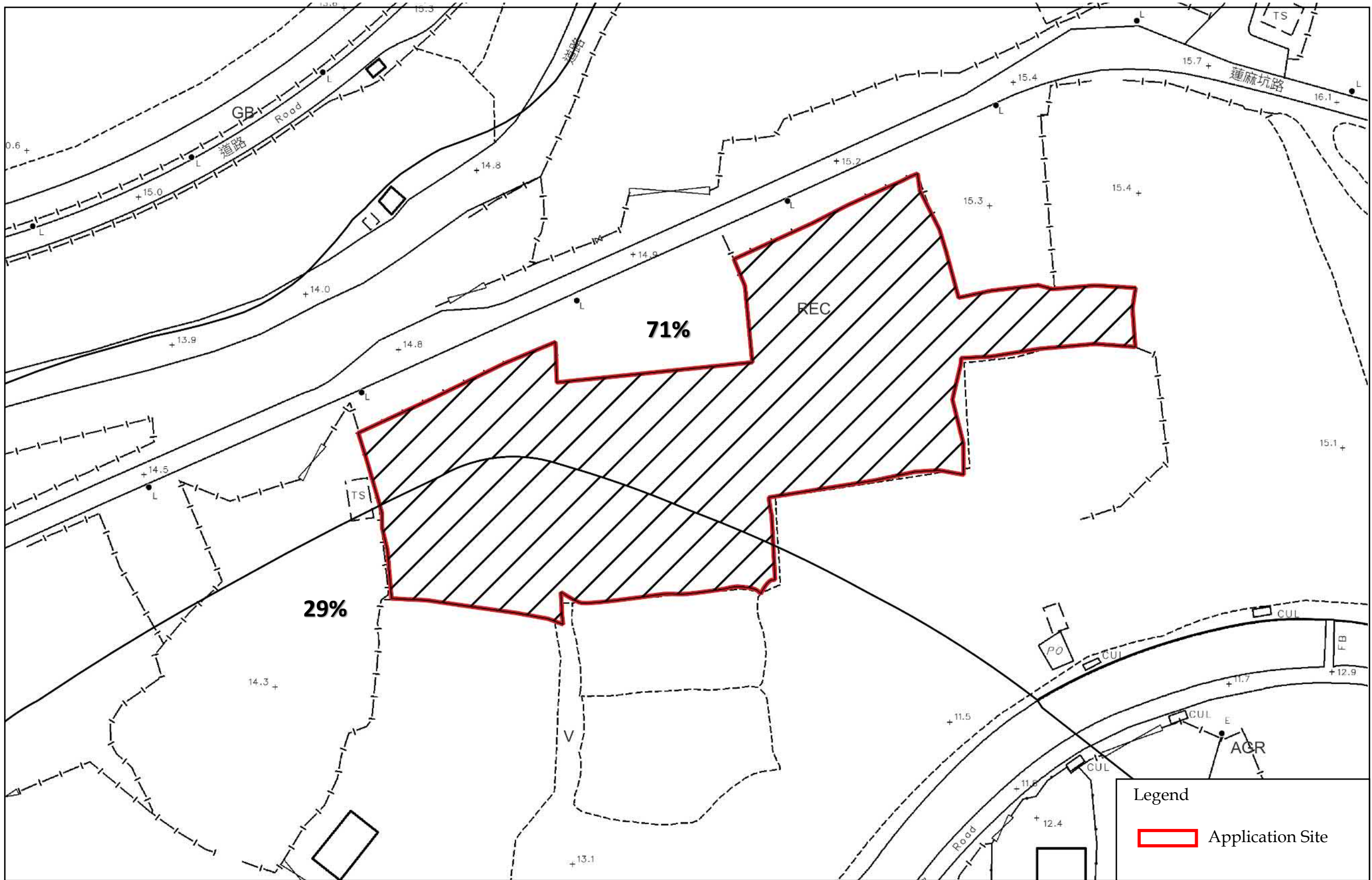
Figure 4 : Accessibility

*(For reference only. Not to scale.)*

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

*(Source: HK GEODATA STORE, HKSAR Government)*





**LCH Planning and Development  
Consultants Limited**

Figure 5 : Extract of Outline Zoning Plan

*(For reference only. Not to scale.)*

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

*(Source: Town Planning Board & HK GEODATA STORE, HKSAR Government)*



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Timothy Wai Pui WU/PLAND

---

寄件者: Cherie Lee <[REDACTED]>  
寄件日期: 2025年07月21日星期一 16:53  
收件者: tpbbpd/PLAND  
副本: Timothy Wai Pui WU/PLAND; Junior  
主旨: Regarding Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: 20250721\_A\_NE-TKLN\_102\_FI.pdf  
  
類別: Internet Email

Dear Sir/Madam,

Attached please find our letter and our responses to departmental comments.

Best regards,  
Cherie

LCH (Asia-Pacific) Surveyors Limited | LCH Planning & Development Consultants Limited

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

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**LCH Planning and Development  
Consultants Limited**

Our Ref.: PD2504003/03

Your Ref.:

21 July 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION  
UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

We refer to your emails enclosing the departmental comments regarding the captioned application.

Please find attached our responses to departmental comments with related annexes. We would like to clarify that our submission of planning statement report via our letter dated 5 June 2025 shall be the latest version. And hence, this submitted document shall supersede the previous planning statement report in our letter dated 12 May 2025.

We would also like to clarify that there is an existing structure located at Lot No. 61 s.B R.P. in D.D. 80 which is covered by a short term waiver (STW) No. 1666 for shop and services and ancillary office. The STW regarding proposed structure on Lot No. 65 s.B R.P. in D.D. 80 is under processing by Lands Department.

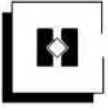
This Further Information contains the responses to comments of relevant Government departments and technical clarifications only. Thus, according to TPB Planning Guideline No. 32, this Further Information does not result in a material change of the nature of the application and should be accepted and exempted by the TPB for inclusion into the application.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee [REDACTED]

Yours faithfully,  
For and on behalf of  
**LCH Planning & Development Consultants Limited**

**Junior Ho**

[REDACTED]



Director

Encl.

c.c. the Applicant

- Response to Comments Table
- Annex 4 – Traffic Consideration
- Annex 5 – Revised Drainage Proposal



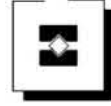
Section 16 Application No. A/NE-TKLN/102

Annex 1 - Response to Comments Table

No.	Comments Received	Our Responses
1.	<i>Comments from Transport Department received on 8 July 2025</i>	
a	The applicant should substantiate the traffic generation from and attraction to their public car park in detail including the estimates for Monday to Friday, Saturday, Sunday and Public Holiday;	Well noted with thanks.
b	The applicant should conduct traffic count surveys to the nearby road links and junctions, advise and substantiate the additional traffic flow generated by the development will not cause substantial traffic impact to the surrounding road network, in particular whether there would be any issue on the road capacity of the adjacent Lin Ma Hang Road, which is a single track access with traffic of both directions;	Well noted.  A traffic count survey is conducted and it indicates that the proposed development will not cause adverse traffic impact to the surrounding road network and any issue on the road capacity of the adjacent Lin Ma Hang Road.
c	The applicant shall highlight the proposed vehicular access arrangement including the existing run-in / out locations on the layout plans;	Please refer to <b>Annex 4</b> for further details regarding the survey result.  Please refer to the proposed vehicular access arrangement as illustrated in the Indicative Layout Plan in the previously submitted <b>Annex 1</b> , supplemented with the Site Plan in <b>Figure 2</b> .
d	The applicant shall advise the management/control measures to be implemented to ensure no queuing of vehicles outside the subject site; and	Noted. It is advised that sufficient spaces are reserved in the site and hence no queue back to or reverse onto/ from the Lin Ma Hang Road
e	The applicant shall advise the provision and management of pedestrian facilities to ensure pedestrian safety near their car park.	A “Be aware of Pedestrians” sign will be placed at the entrance of the site to warn the drivers. A temporary staff station will also be placed at the entrance to manage traffic condition when the vehicle(s) enter and exit the site.



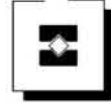
2.	<i>Comments from Drainage Services Department received on 8 July 2025</i>
a	<p data-bbox="228 1189 260 1944">Drawing no. TKLN57&amp;58-D01 drainage proposal refers.</p> <p data-bbox="260 1189 371 1944">a) The applicant should indicate the boundary of this application instead of the previous applications in this drawing.</p> <p data-bbox="403 1189 555 1944">b) The applicant should review the fall directions indicated in this drawing (e.g. it was indicated in the drawing that water will fall from +14.20mPD to +14.55mPD).</p> <p data-bbox="587 1189 810 1944">c) As it was observed on site that lot nos. 61 S.B RP and 65 S.B RP appear to be sloping towards the carriageway which contradicts the fall directions shown in this drawing. The applicant should double check the existing ground level indicated in this drawing.</p> <p data-bbox="842 1189 1217 1944">d) The applicant should take the thickness of grating/cover into account when determining the invert levels of catch pits/u-channels. For instance, it is not feasible to construct a 525UC at CP3 with CL=+14.90mPD and IL=+14.46mPD. It was also observed that the invert level of CP10 (+14.75mPD) and the invert level of u-channel at the starting point near lot no. 70S.B RP (+14.90mPD) are the same as the adjacent ground levels. The applicant should review the design of the whole drainage system.</p> <p data-bbox="1249 1189 1361 1944">e) The applicant should indicate the invert level of the proposed pipe and the existing nullah at their connection point.</p> <p data-bbox="228 853 260 1167">Well noted with thanks.</p> <p data-bbox="292 192 371 1167">Please refer to the illustration in <b>Annex 5</b> attached for the following response:</p> <p data-bbox="379 667 411 1167">(a) New boundary has been indicated.</p> <p data-bbox="419 566 451 1167">(b) The drawing has been revised accordingly.</p> <p data-bbox="459 566 491 1167">(c) The drawing has been revised accordingly.</p> <p data-bbox="499 398 531 1167">(d) Invert level has been reviewed and revised accordingly.</p> <p data-bbox="539 192 619 1167">(e) The invert level of the proposed pipe and existing pipe has been indicated.</p>



b	<p>Drawing no. TKLN57&amp;58-D01 sections refers.</p> <p>a) There are 2 section B-B. The applicant should clarify and update the sections as necessary.</p> <p>b) The road kerb shown in this drawing could not be identified on site. The applicant should review and update the sections.</p> <p>c) The applicant should supplement a section perpendicular to section B-B for lot no. 65 S.B RP.</p>	<p>Please refer to the illustration in <b>Annex 5</b> attached for the following response:</p> <p>(a) The drawing has been revised accordingly.</p> <p>(b) Sections have been reviewed and revised accordingly.</p> <p>(c) An additional section is provided.</p>
c	<p>Design calculation refers.</p> <p>a) The applicant should supplement a catchment plan showing the catchment area of the proposed drainage system (areas and runoff coefficients for each sub-catchment should be indicated on the plan). The applicant should also advise why the runoff outside catchment area is considered in the design.</p> <p>b) The applicant should state the design return period, storm constants, and time of concentration adopted in the design.</p>	<p>Please refer to the illustration in <b>Annex 5</b> attached for the following response:</p> <p>(a) Catchment area plan is provided.</p> <p>(b) These have been presented in the calculation.</p>
d	<p>The applicant should supplement details showing how the existing catch pit SCH1001046 and its connecting pipe downstream will be modified/upgraded.</p>	<p>Connection detail has been provided and shown in <b>Annex 5</b>.</p>
e	<p>Unless the applicant could submit a revised drainage proposal to my satisfaction, I do not support the application from drainage perspective.</p>	<p>Typical detail of grating has been provided as shown in <b>Annex 5</b>.</p>
3.	<p><b>Comments from Food and Environmental Hygiene Department received on 11 July 2025</b></p>	

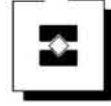


<b>a</b>	He objects to this application beyond the land reversion and site clearance date for the development of Heung Yuen Wai Food Control Facilities.	Noted.
<b>b</b>	<p>According to the Development Proposals of New Territories North New Town (NTN NT) and Ma Tso Lung as announced in December 2024, and the proposed location of the Heung Yuen Wai Food Control Facilities (FCF) as announced in February 2025, the application site for the Proposed Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office is within the Priority Development Area of NTN NT and would overlap with the FCF location. If this planning application is approved, the application site would have to be vacated for the site formation/construction works relating to the Priority Development Area of NTN NT and FCF. This Department does not agree with the proposed usage under this application beyond the land reversion and site clearance date for the above site formation/construction works. Please take note of the followings:</p> <p>The applicant should take into account the potential impact of NTN NT development, including the relocation of food control facilities (FCF) to Heung Yuen Wai. It should also be noted that the FCF falls within the Priority Development Area (PDA) of NTN NT. The proposed FCF relocation exercise will involve diversion of a section of Lin Ma Hang Road, site formation and construction of FCF. Subject to the finalisation of the project boundary, the</p>	<p>It is understood that there is the proposed development of the Heung Yuen Wai Food Control Facilities (FCF) and the corresponding land resumption schedule. The Applicant will follow the land resumption schedule after the planning applications of the relocation of FCF have obtained TPB's permission, and will move out of the concerned site boundary to facilitate the site formation/construction works relating to the Priority Development Area of New Territories North New Town (NTN NT) and the aforesaid relocation of FCF.</p>

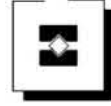


	<p>aforesaid works cover an area of about 5 hectares, of which about 3 hectares of private land will be acquired. North District Council was consulted for the relocation project at the meeting on 18 February 2025. To take forward the FCF relocation exercise, this Department targets to submit a 5.16 planning application under the Town Planning Ordinance (Cap. 131) in the second half of 2025. The statutory procedures for land resumption are planned to commence after obtaining TPB's permission. Subject to such procedures, land reversion and site clearance are expected to commence around late 2026. In general, ex-gratia land compensation may be offered to the owners of private land in the New Territories being affected by land resumption. For business undertakings affected by land resumption/clearance exercises, ex-gratia allowances may be offered to eligible business undertakings operating for at least two years immediately preceding the Pre-clearance Survey (PCS) and not in breach of the lease or conditions of relevant land instruments.</p>	
<b>c</b>	No Food and Environmental Hygiene Department's (FEHD) facilities should be affected.	Noted with thanks.
<b>d</b>	Proper licence / permit issued by this Department is required if there is any food business / catering service / activities regulated by the Director of Food and Environmental Hygiene	Noted with thanks.
<b>e</b>	(DFEH) under the Public Health and Municipal Services Ordinance (Cap. 132) and other relevant legislation for the public. In accordance with the Public Health and	Noted with thanks.



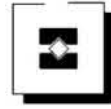


	<p>Municipal Services Ordinance (Cap. 132) and the Food Business Regulation (Cap. 132X), a food business licence shall be obtained for any premises intended to operate the relevant type of food business (e.g. restaurant, food factory, fresh provision shop, etc.) listed in the Regulation. The application for licence, if acceptable by the FEHD, will be referred to relevant government departments such as the Buildings Department, Fire Services Department and Planning Department for comment. If there is no objection from the departments concerned, a letter of requirements will be issued to the applicant for compliance and the licence will be issued upon compliance of all the requirements.</p>	
<b>f</b>	<p>Depending on the mode of operation, generally there are several types of food business licence/permits that the operator of a convenience store may apply for under the Food Business Regulation:</p> <p>if food is sold to customers for consumption on the premises, a restaurant licence should be obtained;</p> <ul style="list-style-type: none"><li>a) if food is only prepared for sale for consumption off the premises, a food factory licence should be obtained;</li><li>b) if fresh, chilled or frozen meat is sold, a fresh provision shop licence should be obtained; and</li><li>c) if restricted foods like milk, frozen confections, non-bottled drinks, cut fruit etc. are to be sold, relevant restricted food permits should be obtained.</li></ul>	<p>Well noted with thanks.</p>



<b>g</b>	<p>When choosing a premises, the applicant must ensure that the operation of food business at the subject premises is in compliance with the requirements imposed under the legislation administered by the Department, other government departments and the relevant authorities. Applicants are strongly advised to check well in advance the following documents: (a) the Government Lease, (b) the Occupation Permit of the building, and (c) the statutory plan. No part of a food premises shall be located in, under or over any structures built without the approval and consent of the Building Authority.</p>	Noted.
<b>h</b>	<p>Proper licence issued by this Department is required if related place of entertainment is involved. Any person who desires to keep or use any place of public entertainment for example a theatre and cinema or a place, building, erection or structure, whether temporary or permanent, on one occasion or more, capable of accommodating the public presenting or carrying on public entertainment within Places of Public Entertainment (PPE) Ordinance (Cap. 172) and its subsidiary legislation, such as a concert, opera, ballet, stage performance or other musical, dramatic or theatrical entertainment, cinematograph or laser projection display, a circus, a lecture or story – telling, an exhibition of any 1 or more of the following, namely, pictures, photographs, books, manuscripts or other documents or other things, a sporting exhibition or contest, a bazaar, a dance party or an amusement ride and mechanical device which is designed for amusement, a Place of Public Entertainment</p>	Noted.

	Licence (or Temporary Place of Public Entertainment Licence) should be obtained from FEHD whatever the general public is admitted with or without payment.	
<b>i</b>	There should be no encroachment on the public place and no environmental nuisance should be generated to the surroundings. Its state should not be a nuisance or injurious or dangerous to health and surrounding environment. For any waste generated from the such activity/operation, the applicant should arrange disposal properly at their own expenses.	Well noted with thanks.
<b>4.</b>	<i>Comments from Lands Department received on 17 July 2025</i>	
<b>a</b>	No objection to the application.	Noted.
<b>b</b>	The application site comprises Old Schedule Agricultural Lots held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the application site is required to pass through Government land (GL) but no right of access via GL is granted to the application site.	Noted.
<b>c</b>	Lot No. 61 S. B RP in DD. 80 is covered by Short Term Waiver No. 1666 (the STW) for the purposes of shop and services and ancillary office, it is noted that the total Built-over Area (BOA) of the existing structures erected on site has exceeded the permitted BOA of the STW. STW application for the purpose of shop and services and ancillary office on the Lot No. 65 SB RP in DD. 80 is under processing.  While there is no restriction on the number of buildings/structures to be erected under the said STWs, it is noted that there may be more than 2 existing	Well noted with thanks.  There will be no more than two temporary structures erected on the application site. Indeed, the Applicant is waiting for reply from Lands Department of the STW application on the Lot No. 65 S.B R.P. in D.D. 80.  The Applicant is not aware of any exceedance of permitted BOA under the STW. The Applicant will liaise with Lands Department for clarification and rectification.



	<p>buildings/structures erected on the application site which contravene to the information stipulated in the application form.</p> <p>The applicant is required to clarify.</p>	
<b>d</b>	<p>If the planning application is approved, the STW holder will need to apply to this office for modification of the conditions of the STW No. 1666 where appropriate to permit the structures erected/to be erected within Lot No. 61 S. B RP in DD. 80. The application will be considered by the Government in its capacity as a landlord and there is no guarantee that it will be approved. The application, if approved, will be subject to such terms and conditions including the payment of backdated waiver fee and administrative fee as considered appropriate by LandsD. Besides, given the proposed use is temporary in nature, only erection of temporary structure(s) will be considered.</p>	<p>Well noted with thanks.</p>



## **Annex 4**

# **Traffic Consideration**

## 1 Traffic Consideration

### *Operation Mode*

- 1.1 While the subject carpark is proposing a flexible mode, it is anticipated that the proposed temporary vehicle park will not incur adverse traffic impact. With most of the end user would be cross-boundary travellers, they may park their cars in the Application site and stay in the Mainland for one day or more.
- 1.2 In order to assess the existing traffic conditions, a vehicle count survey was conducted at the Lin Ma Hang Road from 8:00 am to 7:00 pm on 16 July 2025. Flow counts are recorded at 60-minute intervals; and converted to Passenger Car Unit (pcu) values. The highest 60-minute traffic volume is used as the peak hour traffic volume. Location of conducting the vehicle count survey is shown in **Diagram 1**.



**Diagram 1.** Location for Traffic Estimation

- 1.3 The morning and afternoon peak times for the network are determined to be 11:00am to 12:00pm and 5:00pm to 6:00pm respectively. The traffic flow observed in the study area is demonstrated in the following table.

### *Existing Traffic Capacity*

- 1.4 The Volume to Capacity (V/C) ratio represents the proportion of road capacity used by traffic flow during peak hours. Higher V/C ratios for roads indicate greater use of road connection problems. A V/C ratio of 0.85 or less indicates that there is sufficient capacity available and vehicles are not expected to experience significant queues and delays.

No.	Road Link (Direction)	Link Capacity (veh/hr)	Observed Flow		V/C Ratio	
			AM	PM	AM	PM
L1	Lin Ma Hang Road (Eastbound)	600 <sup>(1)</sup>	96	55	0.16	0.09
L2	Lin Ma Hang Road (Westbound)	600 <sup>(1)</sup>	81	80	0.14	0.13

(1) According to the Note of Traffic Forecast Review (Western Section) dated March 2019, the design capacity of Lin Ma Hang Road is about 600 veh/hr

Source: <https://www.legco.gov.hk/yr18-19/chinese/fc/pwsc/papers/pwsc20190213pwsc-157-1-c.pdf>

#### Development Traffic Generation and Attraction

- 1.5 A vehicle count survey was conducted at two existing public vehicle parks (Application No. A/NE-TKLN/57 and 58) from 8:00 am to 7:00 pm on 16 July 2025. The traffic flow of the existing public vehicle parks at the peak hours is recorded and shown below:

	No. of Parking Space	AM (11 am – 12 pm) (pcu/hr)		PM (5 pm – 6 pm) (pcu/hr)	
		Generation	Attraction	Generation	Attraction
A/NE-TKLN/57	24 CPS	4	3	12	3
A/NE-TKLN/58	26 CPS	3	8	7	7

- 1.6 Flow counts are recorded at 60-minute intervals; and converted to Passenger Car Unit (pcu) values. The highest 60-minute traffic volume is used as the peak hour traffic volume. According to the previous records, the usage of the car parks during peak hours of Weekends and Public Holiday is approximately 30% more than that in Weekdays. To estimate the worst cast scenario, we have applied 30% on the traffic flow generation. A summary of the result is as follow:

Public Vehicle Park		Generation		Attraction	
		AM	PM	AM	PM
	No. of Spaces	Traffic flow (pcu)			
A/NE-TKLN/57	24 CPS	(4+3) × 1.3	(5+8) × 1.3	(12+7) × 1.3	(3+7) × 1.3
A/NE-TKLN/58	26 CPS				
Total	50 CPS				
Trip rate (pcu/hr/parking space)					
		= 10/50 = 0.20	=17/50 = 0.34	= 25/50 = 0.50	= 13/50 = 0.26
Trips in veh/hr (pcu/hr)					
Proposed Parking Facilities		= 0.20 × 115 <sup>(1)</sup> = 23	= 0.36 × 115 <sup>(1)</sup> = 42	= 0.50 × 115 <sup>(1)</sup> = 58	= 0.26 × 115 <sup>(1)</sup> = 30
Existing Shop and Services		2	2	2	2
Total		25	44	60	32

(1) The total number of parking spaces for this application is 115.

(2) Additional +30% is applied on the traffic flow count to reflect the worst case scenario, i.e., weekends and public holidays.

#### Traffic Assessment

- 1.7 This application is seeking temporary permission for a maximum period of 3 years only, which targets to commence by the end of 2025. Thus, to estimate

the worst-case scenario, a growth factor will be applied to the end of this application, i.e., 2028 to demonstrate the traffic impact.

- 1.8 Besides, although the development under planning application No. A/NE-TKLN/90 is yet to be commenced, the 2028 Reference flow has also been considered in this approved application.

Public Vehicle Park	Generation		Attraction	
	AM	PM	AM	PM
A/NE-TKLN/90 (from TIA report of the planning application)	11	13	11	13

$$2028 \text{ Reference Flow} = 2025 \text{ Observed Flow} \times (1 + 14.3\%)^1$$

$$2028 \text{ Design Flow} = 2028 \text{ Reference Flow} + \text{Development Flow of this Application} \\ + \text{Traffic Flow from A/NE-TKLN/90}$$

- 1.9 The link capacity assessment results under the 2028 reference and design scenarios are summarised in table below:

**2028 Traffic Assessment**

No.	Road Link (Direction)	Link Capacity (veh/hr)	Reference Flow (veh/hr)		Reference V/C Ratio		Design Flow (veh/hr)		Design V/C Ratio	
			AM	PM	AM	PM	AM	PM	AM	PM
L1	Lin Ma Hang Road (Eastbound)	600	96 x (1.143) = 110	55 x (1.143) = 63	0.18	0.11	110 + 60 + 11 = 181	63 + 32 + 13 = 108	0.30	0.18
L2	Lin Ma Hang Road (Westbound)	600	81 x (1.143) = 93	80 x (1.143) = 92	0.16	0.15	93 + 25 + 11 = 123	92 + 44 + 13 = 149	0.21	0.25

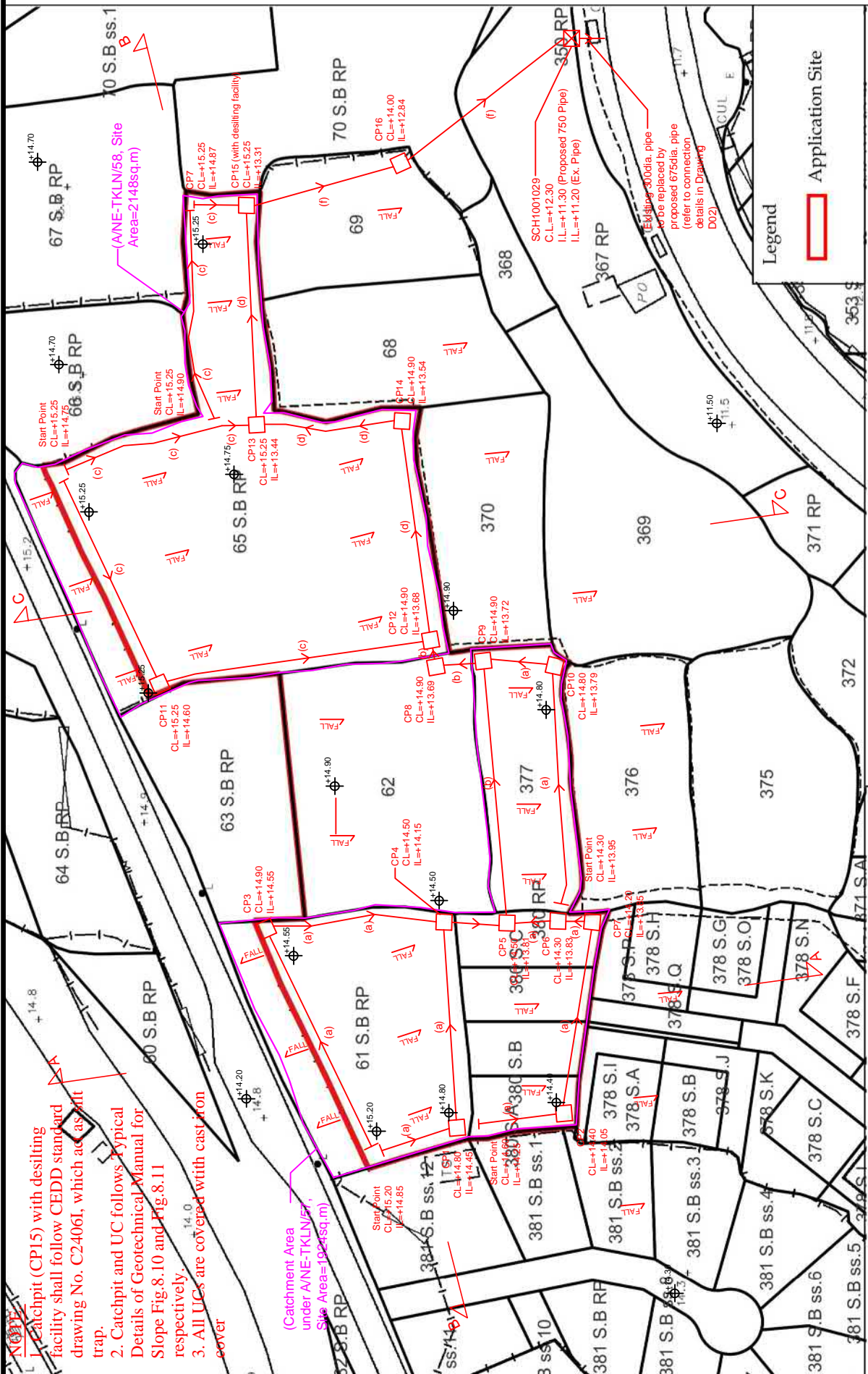
- 1.10 As shown in table above, the capacity of the Lin Ma Hang Road would be performing satisfactorily during the peak periods for both Reference and Design Scenarios. No adverse traffic impact is anticipated.
- 1.11 Traffic management measures proposed in section 2.10.5 of the planning statement would be fully implemented. It is expected that there will not be significant negative impacts regarding the safety of road users and the traffic network of the area concerned.

<sup>1</sup> According to the Highways Department Agreement No. CE 51/2013 (HY) Widening of Western Section and Eastern Section of Lin Ma Hang Road – Design and Construction Note of Traffic Forecast Review (Western Section), the steady traffic growth rate of 0.6% p.a. is anticipated. However, based on the Projected Population Distribution 2023-2031 published by the Planning Department, the projected population in North District in 2025 and 2028 are 347,400 and 397,000 respectively. Thus, a growth rate of 14.3% is adopted all over the 3 years period.



- NOTE
1. Catchpit (CP15) with desilting facility shall follow CEDD standard drawing No. C24061, which act as silt trap.
  2. Catchpit and UC follows Typical Details of Geotechnical Manual for Slope Fig.8.10 and Fig.8.11 respectively.
  3. All UCs are covered with cast iron cover

(Catchment Area under ANE-TKLN/58, Site Area=1924sq.m)



CH Planning and Development Consultants Limited

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)

Status  
only. Not to scale.)

LEGEND	Proposed 300UC (1200) with Cast Iron cover
	(a)
(b)(c)	Proposed 450UC (1200) with Cast Iron cover
	(d)
(e)	Proposed 525UC (1200) with Cast Iron cover
	(f)
(g)	Proposed 750 underground concrete pipe (1:175)
	(h)
	Existing Catchpit
	(i)
	Proposed Catchpit
	(j)
	Existing Level
	(k)
Company: 正宏工程顧問公司 Ching Wan Engineering Consultants Company	
PROJECT: Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years	
TITLE: Drainage Proposal	
Date: 17-7-2025	DWG NO. D01

<b>LEGEND</b>			
<p>Company:          正宏工程顧問公司          Ching Wan Engineering          Consultants Company</p> <p>PROJECT:          Section 16          Application for          Temporary Public          Vehicle Park          (Excluding Container          Vehicle), Shop and          Services          (Convenience Store)          and Ancillary Office          at Lot Nos. 377,          380 S.A, 380 S.B,          380 S.C and 380 RP          in D.D. 78 and 61          S.S.B RP (Part), 62          and 65 S.B RP          (Part) in D.D. 80,          Heung Yuen Wai, New          Territories for a period          of 3 Years</p>	<p>TITLE:          Section &amp; Connection Details</p> <table border="1"> <tr> <td>Date: 17-7-2025</td> <td>DWG NO. D02</td> </tr> </table>	Date: 17-7-2025	DWG NO. D02
Date: 17-7-2025	DWG NO. D02		







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**Catchment Area under A/NE-TKLN/57**

Site Area = 962 m<sup>2</sup> (1924/2)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 962 \quad \text{m}^2$$
$$= 0.000962 \quad \text{km}^2$$

$$t = 0.14465 \text{ L/H}^{0.2} A^{0.1}$$
$$= 0.14465 * 10 / 1^{0.2} * 962^{0.1}$$
$$= 0.728 \quad \text{min}$$

$$i = 1.111 * a / (t + b)^c \quad (\text{50 yrs return period, Table 3d, Corrigendum 2024, SDM) and (11.1\% increase due to climate change)})$$
$$= 1.111 * 474.6 / (0.728 + 2.60)^{0.371}$$
$$= 326.9 \quad \text{mm/hr}$$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001924$

$$= 0.083 \quad \text{m}^3/\text{sec}$$
$$= 4983 \quad \text{lit/min}$$

**Provide 300UC (1:200) is OK**

**Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP**

Lot 62 & 63BRP = 1507 m<sup>2</sup> (C=0.95)

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 1507 \quad \text{m}^2$$
$$= 0.001507 \quad \text{km}^2$$

take  $i = 326.9 \quad \text{mm/hr}$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001507$

$$= 0.130 \quad \text{m}^3/\text{sec}$$
$$= 7806 \quad \text{lit/min}$$

For Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP

$$Q = 4983 + 7806$$
$$= 12790 \quad \text{lit/min}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/58+Outside Catchment Area**

Site Area = 2148 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278 * 0.95 * 326.9 * 0.002148 + 0.278 * 0.25 * 326.9 * 0.002244 \\ &= 0.236 \text{ m}^3/\text{sec} \\ &= \mathbf{14186} \text{ lit/min}\end{aligned}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/57+Lot 62 & 63BRP+A/NE-TKLN/58+Outside Catchment Area**

$$Q = 12790 + 14186$$

$$= 26975 \text{ lit/min}$$

**Provide 525UC (1:200) is OK**

**Outfall (all catchment area)**

Site Area = 7281 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278 * 0.95 * 326.9 * 0.007281 + 0.278 * 0.25 * 326.9 * 0.002244 \\ &= 0.680 \text{ m}^3/\text{sec} \\ &= \mathbf{40775} \text{ lit/min}\end{aligned}$$

**Provide 675UC (1:200) is OK**

Check 750mm dia. Pipes by Colebrook-White Equation

$$V = -\sqrt{(8gDs)} \log\left(\frac{ks}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}}\right)$$

where :

V	=		mean velocity (m/s)
g	=	9.81	m/s <sup>2</sup> gravitational acceleration (m/s <sup>2</sup> )
D	=	0.75	m internal pipe diameter (m)
ks	=	0.00015	m hydraulic pipeline roughness (m)
v	=	1.14E-06	m <sup>2</sup> /s kinematic viscosity of fluid (m <sup>2</sup> /s)
s	=	0.00571429	hydraulic gradient

(Table 14, from DSD SDM 2018, concrete pipe)

Therefore, design V of pipe capacity

$$= 2.4199 \text{ m/s}$$

Q= 0.8VA (0.8 factor for sedimentation)

$$= 0.855 \text{ m}^3/\text{s}$$

$$= 51315 \text{ lit/min}$$

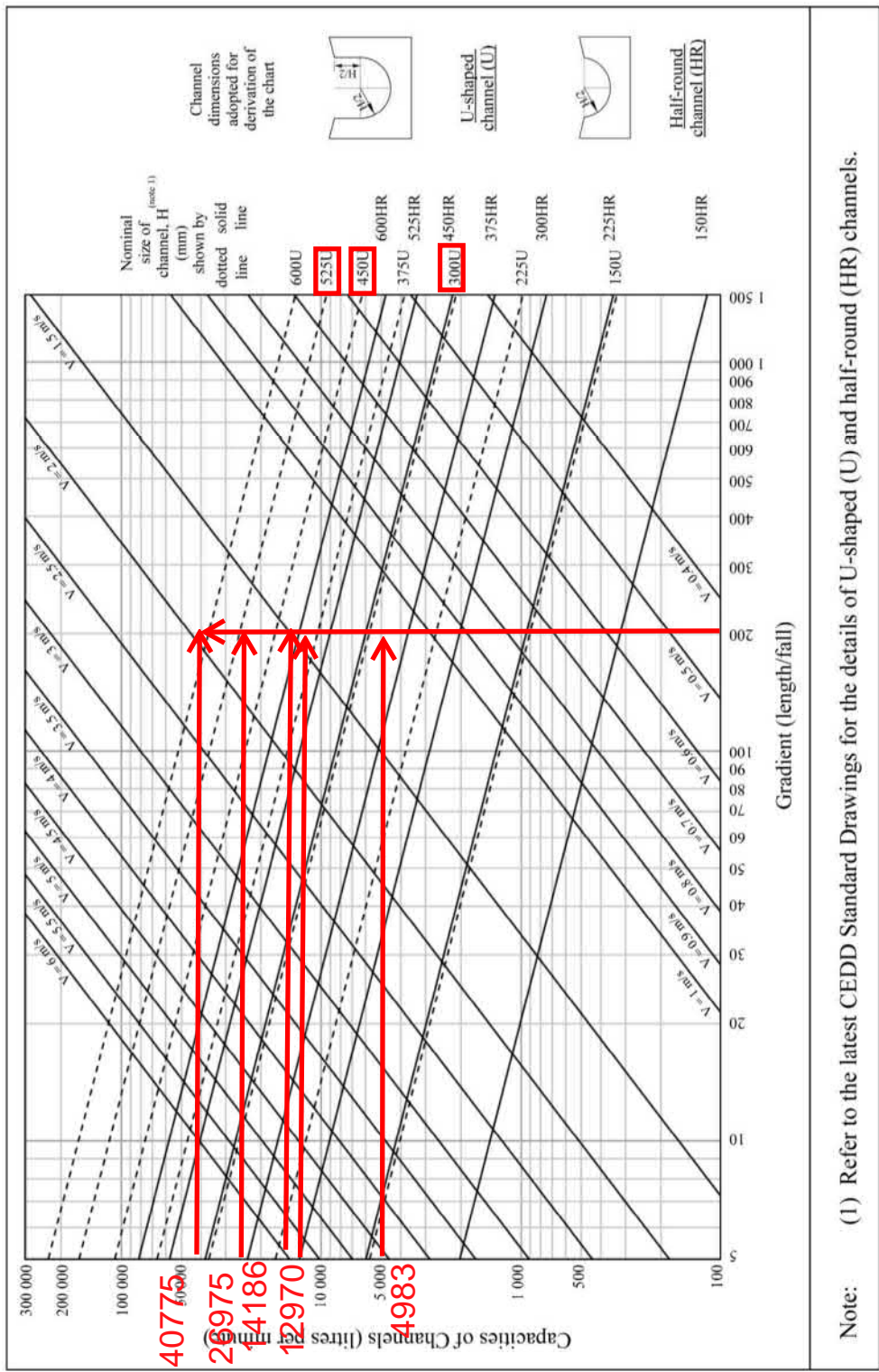
$$> 40775 \text{ lit/min}$$

Ok

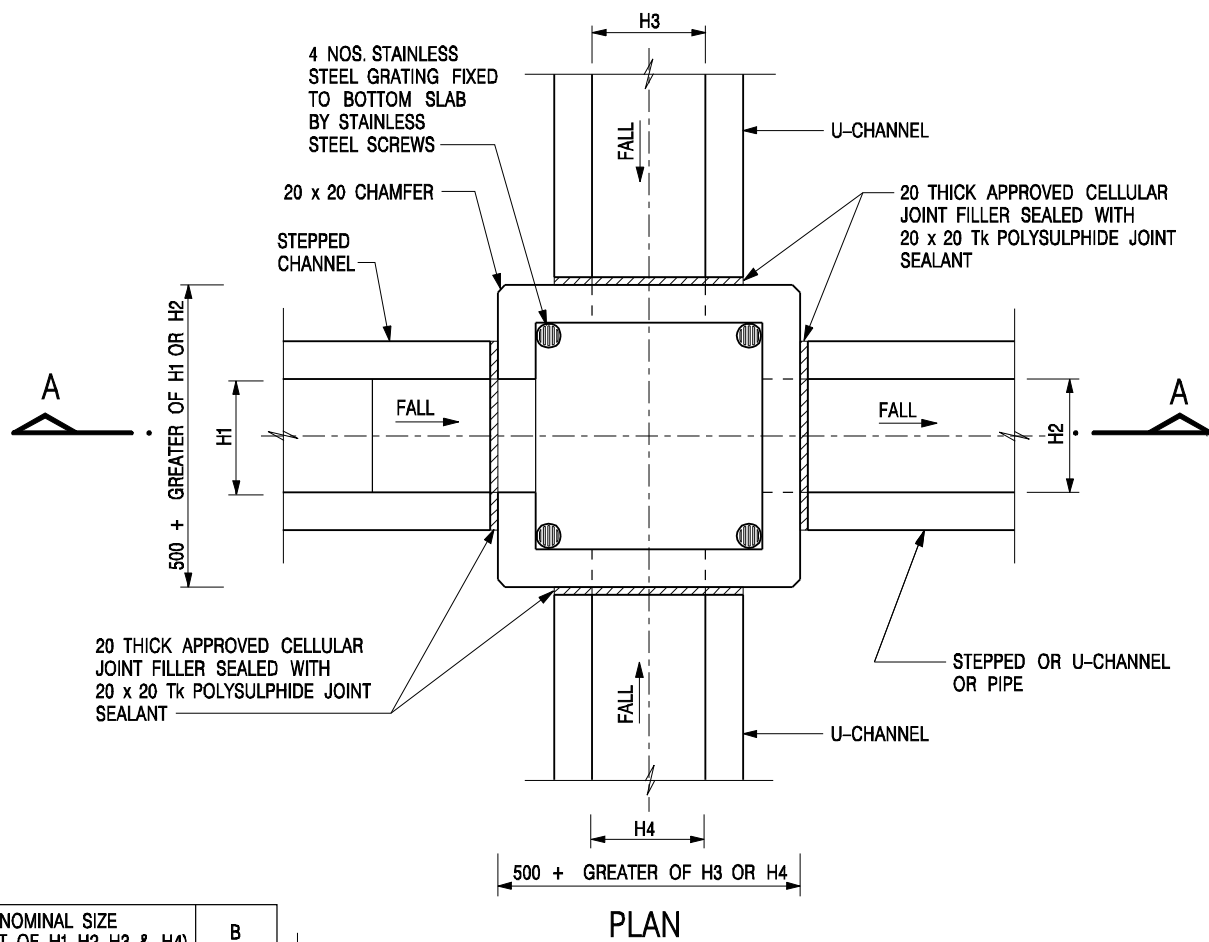
**GEO Technical Guidance Note No. 43 (TGN 43)**  
**Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes**

Issue No.: 1      Revision: -      Date: 05.06.2014      Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm

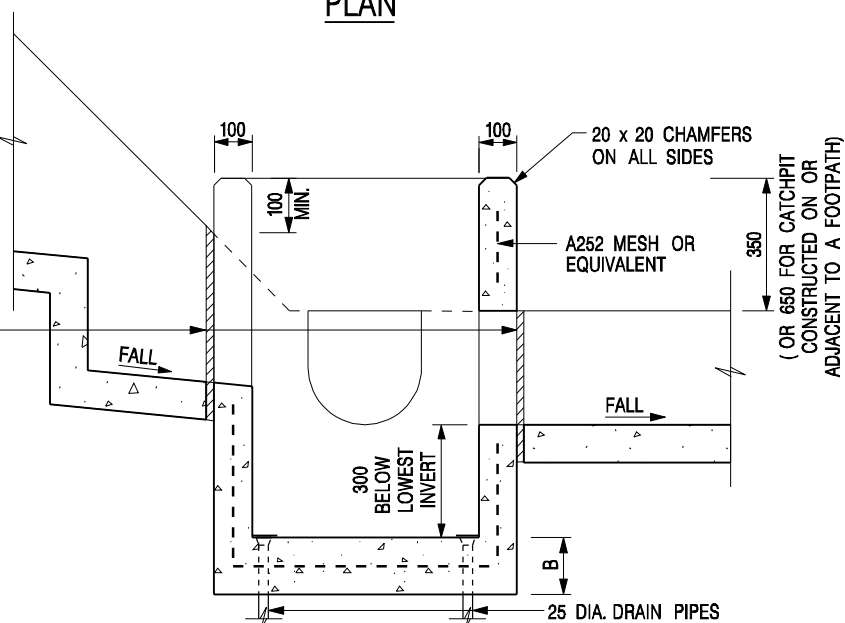






NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175


20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT

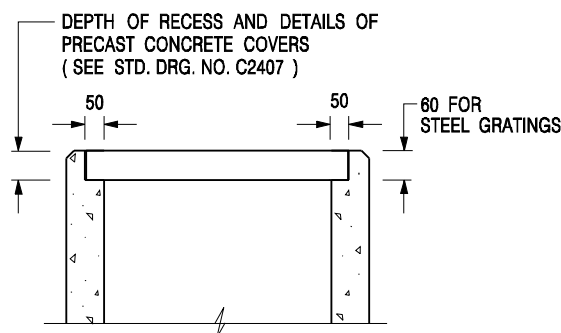


#### NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- REFER TO SHEET 2 FOR OTHER NOTES.

CATCHPIT WITH TRAP  
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE 1 : 20		DRAWING NO. C2406 /1	
DATE JAN 1991			




### ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

#### NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS ( SEE DETAIL 'A' ON STD. DRG. NO. C2405 ) OR CONCRETE COVERS ( SEE STD. DRG. NO. C2407 ) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING ( SEE DETAIL 'G' ON STD. DRG. NO. C2405; EXCEPT ON THE UPSLOPE SIDE ) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS ( SEE DSD STD. DRG. NO. DS1043 ) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'F' ON STD. DRG. NO. C2405.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

**CATCHPIT WITH TRAP  
(SHEET 2 OF 2)**

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
<b>REF.</b>	<b>REVISION</b>	<b>SIGNATURE</b>	<b>DATE</b>
 <b>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</b>			
<b>SCALE</b> 1 : 20		<b>DRAWING NO.</b>	
<b>DATE</b> JAN 1991		<b>C2406 /2</b>	

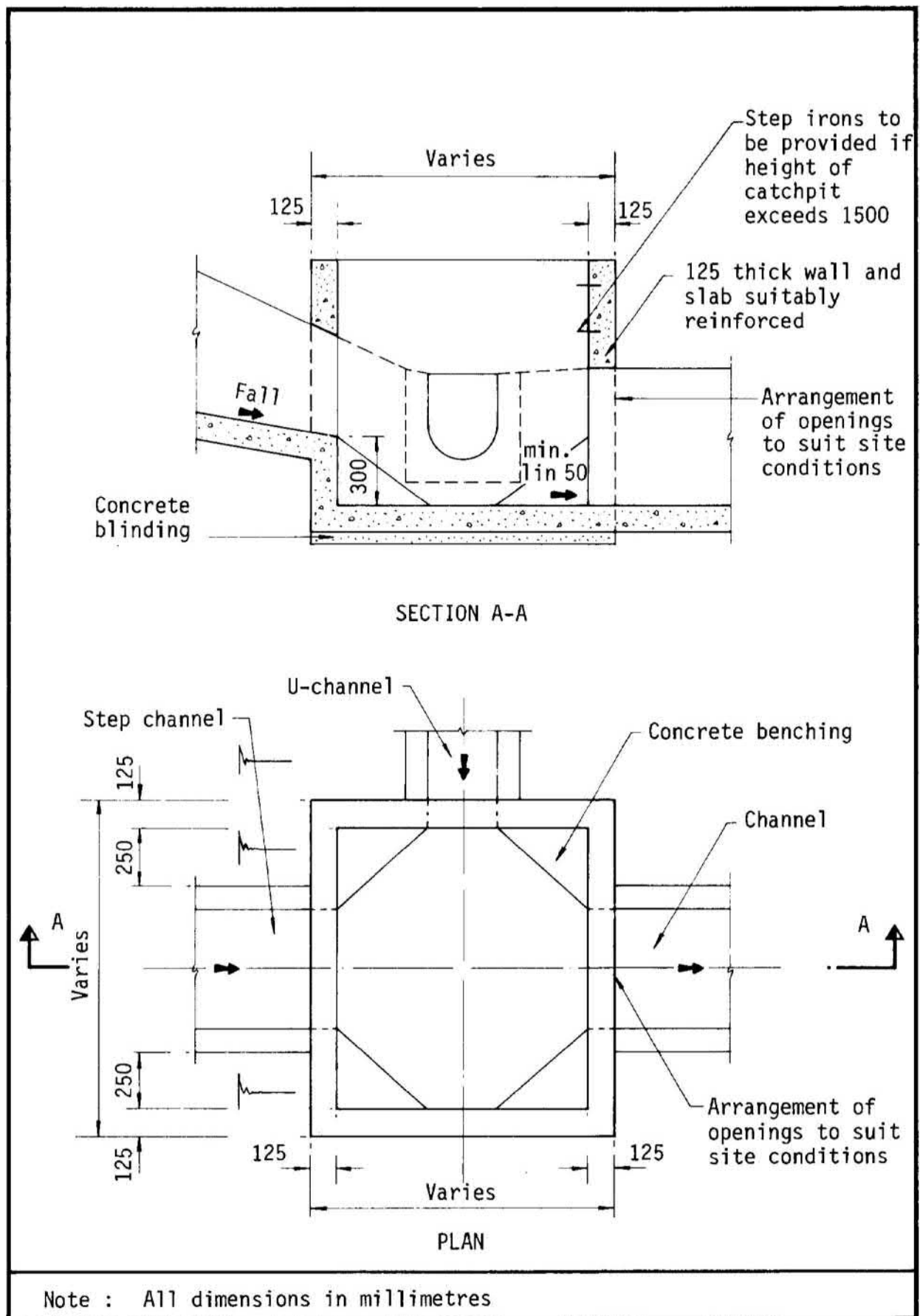
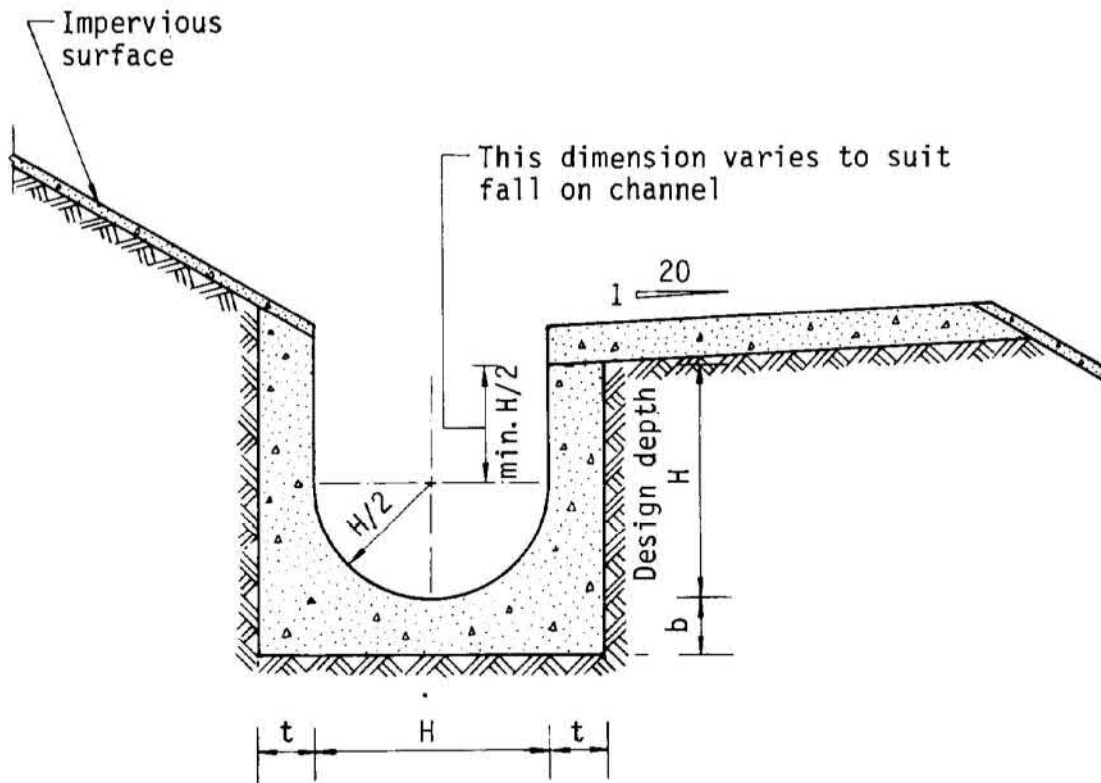


Figure 8.10 - Typical Details of Catchpits

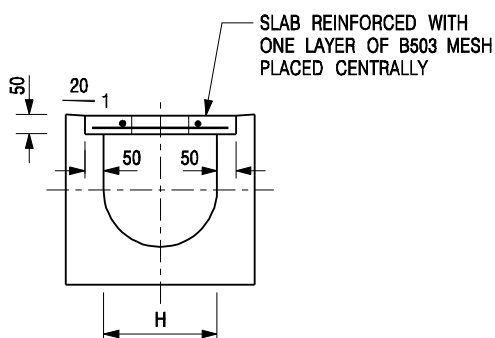


Dimensions of U - channel

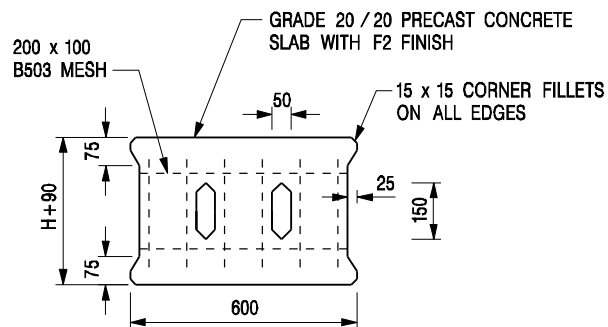
Nominal size of channel H (mm)	Thickness t (mm)	Thickness b (mm)
225 to 600	150	150
675 to 1200	175	225

Figure 8.11 - Typical U-channel Details





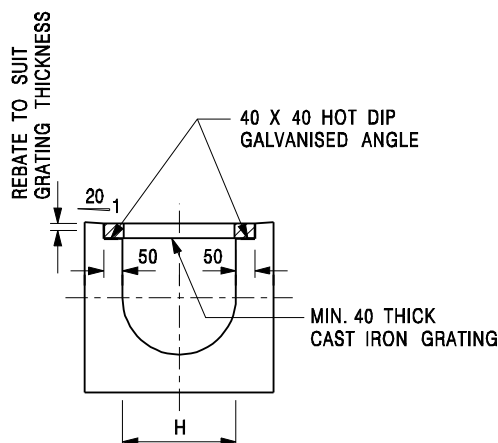
**TYPICAL SECTION**



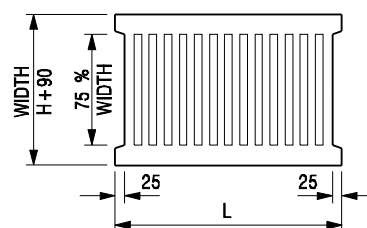
**PLAN OF SLAB**

## **U-CHANNELS WITH PRECAST CONCRETE SLABS**

(UP TO H OF 525)



**TYPICAL SECTION**



L = 600mm FOR H ≤ 375mm  
L = 400mm FOR H > 375mm

**CAST IRON GRATING**

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

## **U-CHANNEL WITH CAST IRON GRATING**

(UP TO H OF 525)

### **NOTES:**

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**COVER SLAB AND CAST IRON  
GRATING FOR CHANNELS**



**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

**DRAWING NO.**  
**C2412E**

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Timothy Wai Pui WU/PLAND

---

寄件者: Junior <[REDACTED]>  
寄件日期: 2025年07月23日星期三 21:28  
收件者: tpbpd/PLAND  
副本: Timothy Wai Pui WU/PLAND; 'Cherie Lee'  
主旨: RE: Regarding Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: Ltr\_to TPB\_20250723.pdf  
類別: Internet Email

Dear Sir/ Madam,

Please find attached our submission.

Regards,  
Junior Ho

[REDACTED]  
[REDACTED]  
[REDACTED]

-----  
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**LCH Planning and Development  
Consultants Limited**

Our Ref.: PD2504003/04

Your Ref.:

23 July 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION  
UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

We refer to the email enclosing the departmental comments regarding the captioned application.

Please find attached our responses to departmental comments.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee at [REDACTED]

Yours faithfully,  
For and on behalf of  
**LCH Planning & Development Consultants Limited**

**Junior Ho**  
Director

Encl.

c.c. the Applicant  
- Response to Comments Table



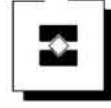


Section 16 Application No. A/NE-TKLN/102

Annex 1 - Response to Comments Table

No.	Comments Received	Our Responses
1.	<i>Comments from Planning Department received on 23 July 2025</i>	
a	It is noted that the Site is subject to two revoked planning applications (No. A/NE-TKLN/57 and 58). Please advise the reason for not complying with the approval conditions under the said planning applications. Please also advise how the applicant would ensure the approval conditions can be complied with if the current application is approved by TPB.	<p>We have different views on the implementation of the accepted drainage proposal dated 28 February 2024, and we do not have sufficient time to further discuss the issue with the Drainage Services Department (DSD) on the revised drainage proposal. Hence, the Applicant could not meet the target date to comply with the concerned approval conditions regarding the provision of drainage facilities. As emergency measures accepted by DSD, the Applicant provided water pump and sand bags in the site. In case of flooding, the water pump can quickly pump the water away while the sand bags can prevent water flowing on to Lin Ma Hung Road.</p> <p>In this application, the Applicant has submitted a revised drainage proposal to support the application for satisfaction of DSD, and the Applicant will try its best to implement the accepted drainage proposal should this application be approved.</p>
b	Please advise the proposed operating hours for the public vehicle park and shop and services as some discrepancies between the application form and the submitted planning statement (e.g. 24 hours is mentioned in the application form but 9:00 a.m. to 8:00 p.m. in para 2.10.3 of the submitted planning statement).	<p>Noted.</p> <p>Please be clarified that the operation hours for the proposed development will 24 hours.</p>
c	It is noted that "Fencing will be provided around the boundary of the carpark" in para 2.10.3 of the submitted planning statement. Please further advise if the boundary of the carpark refers to the application site?	<p>Yes, fencing is proposed to be provided around the boundary of the Application Site.</p>





d	<p>Thank you for your clarification of superseding previous planning statement report via your FI dated 21.7.2025. However, we cannot ascertain the submitted planning statement report via the dates of your letters. To make it simple, we have received i) the planning statement report together with the application form dated 10.6.2025; and ii) the planning statement report submitted on 13.6.2025 at 12:12 via the designated one drive link. Please advise which version should be superseded</p>	<p>Please noted that both versions shall be the same. While planning statement report submitted with application form dated 10.6.2025 is the hard copy, the planning statement report submitted on 13.6.2025 is the soft copy.</p> <p>We would advise to refer to the planning statement report submitted on 13.6.2025 which will supersede the previous version sent on 10.6.2025.</p> <p>Thank you.</p>
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☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy ☐Confidential

Timothy Wai Pui WU/PLAND

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寄件者: Cherie Lee <[REDACTED]>  
寄件日期: 2025年07月25日星期五 14:06  
收件者: tpbpd/PLAND  
副本: Timothy Wai Pui WU/PLAND; Junior  
主旨: RE: Regarding Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: FI\_to TPB\_20250725.pdf  
類別: Internet Email

Dear Sir/Madam,

Attached please find our letter and replacement pages of the planning statement.

Thank you so much.

Best regards,  
Cherie

[REDACTED]  
[REDACTED]  
[REDACTED]

[REDACTED]  
[REDACTED]

-----  
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**LCH Planning and Development  
Consultants Limited**

Our Ref.: PD2504003/05

Your Ref.:

25 July 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION  
UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

Referring to the departmental comments regarding the captioned application, we would like to make further clarification on three items as shown in the following:

1. The operation hours for the proposed vehicle park would be 24 hours daily, including public holidays, while for the proposed convenience store, the operation hour is from 9a.m. to 8p.m., daily, including public holidays.
2. Regarding the compliance of approval condition (j) – the implementation of the Fire Service Installation (FSIs) proposal of Planning Application No. A/NE-TKLN/57, the Applicant have already found a qualified fire service engineer for the provision of FSIs. However, the Applicant could not meet the target date to comply with the concerned approval condition, even when the implementation was nearly completed. Thus, in this application, the Applicant will try its best to implement the accepted FSI proposal on time should this application be approved.
3. The proposed convenience store would be mainly serving car parking user.

Besides, we have made some minor updates on the planning statement. Attached please find the relevant replacement pages.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee at [REDACTED].

Yours faithfully,  
For and on behalf of

**LCH Planning & Development Consultants Limited**

**Junior Ho**  
Director

Encl.

c.c. the Applicant  
[REDACTED]



2.10.2 According to Chapter 8 of the Hong Kong Planning Standards and Guidelines, the dimension of parking spaces and loading/ unloading bays are set. All the proposed parking spaces and loading/ unloading bays comply with the relevant standards.

Types of Parking Space and Loading/Unloading Bay	Length (m)	Width (m)
Private Cars, Van-type Light Goods Vehicles and Taxis	5	2.5
Light Goods Vehicles (LGV)	7	3.5

2.10.3 The convenience store will operate from 9a.m. to 8p.m. daily including public holidays, catering the needs of car parking users. This is similar to the previously approved scheme. Fencing will be provided around the boundary of the carpark.

2.10.4 Enter and exit of the car park will be through the existing gate opening along the Lin Ma Hang Road with a width of 8m.

2.10.5 The following traffic management measures are proposed to follow:

- No vehicle without valid licence issued under the Road Traffic Ordinance will be allowed to be parked/stored on or enter/exit the Site;
- Only private car as defined in the Road Traffic Ordinance will be allowed to be parked/stored on or enter/exit the Site;
- No medium or heavy goods vehicle exceeding 5.5 tonnes, including container tractor/ trailer will be permitted to park at the Site;
- A notice should be posted at the ingress/ egress point of the site to indicate that no medium or heavy goods vehicle (i.e. exceeding 5.5 tonnes) including container trailers/tractors, as defined in the Road Traffic Ordinance, will be allowed to be parked/stored on the Site; and
- No car washing, vehicle repair or other workshop activities will be allowed on the Site.





<b>Application No.</b>	A/NE-TKLN/53	A/NE-TKLN/70
<b>Decision Date</b>	23/06/2023	27/10/2023
<b>Applied Use</b>	Temporary Car Park (Private Cars and Light Goods Vehicles) for a Period of 3 Years	Proposed Temporary Public Vehicle Park (Private Cars Only) for a Period of 3 Years
<b>Expire Date</b>	23/06/2026	27/10/2026
<b>Total no. of vehicle spaces</b>	69	11
<b>Private Car Parking Spaces</b>	54	11
<b>LGV Parking Spaces</b>	15	0

- 4.1.4 There are also other similar planning applications for public vehicle park and shop and services uses in the vicinity. Hence, the proposed development can also help to meet the increasing demand of the parking lots in the vicinity.

<u>Application No.</u>	A/NE-TKLN/75	A/NE-TKLN/90	A/NE-TKLN/68
<u>Decision Date</u>	15/03/2024 Approved	28/02/2025 Approved	05/04/2024 Approved
<u>Applied Use</u>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) and Shop and Services for a Period of 3 Years	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle), Eating Place and Shop and Services (Local Provision Store with Ancillary Office and Store Room) for a Period of 3 Years	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) and Shop and Services for a Period of Five Years
<u>Expire Date</u>	15/03/2027	28/02/2028	05/04/2029

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**Timothy Wai Pui WU/PLAND**

---

寄件者: Cherie Lee <[REDACTED]>  
寄件日期: 2025年09月10日星期三 17:41  
收件者: Timothy Wai Pui WU/PLAND  
副本: Junior  
主旨: FW: Regarding Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: 20250910\_A\_NE-TKLN\_102\_Combined FI.pdf  
  
類別: Internet Email

Dear Timothy,

Attached please find our submission.

Thank you.

Best regards,  
Cherie

LCH (Asia-Pacific) Surveyors Limited | LCH Planning & Development Consultants Limited  
17th Floor, Champion Building  
Nos. 287-291 Des Voeux Road Central, Hong Kong

[REDACTED]

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**LCH Planning and Development**  
**Consultants Limited**

Our Ref.: PD2504003/07

Your Ref.:

10 September 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION**  
**UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

We refer to the emails enclosing the departmental comments regarding the captioned application.

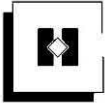
Please find attached our responses to departmental comments and replacement pages of Planning Statement.

In view of Lands Department's comment, we have further updated the proposed layout. The following table summarised the minor changes of the proposed development parameters:

Development Parameter	Original Proposal Submitted under this Application	Latest Proposal under this Further Information	Changes
Site Area (sq.m.)	About 4,628	About 4,628	No change
Covered Area (sq.m.)	About 192	About 204	+3.1%
No. of Structure(s)	2	5	+3
Max. Height of Structures (m)	6	6	No change
Site Coverage (%)	About 4.1	About 4.4	+0.3%
Plot Ratio	About 0.083	About 0.086	+3.6%

This Further Information contains the responses to comments of relevant Government departments and minor changes in proposed plot ratio, covered area and site coverage, with changes not exceeding 10% of the original development parameters. Thus, according to TPB Planning Guideline No. 32B, this Further Information does not result in a material change of the nature of the application and should be accepted by the TPB for inclusion into the application.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee at [REDACTED].



Yours faithfully,  
For and on behalf of  
**LCH Planning & Development Consultants Limited**

**Junior Ho**  
Director

Encl.

c.c. the Applicant

- Response to Comments Table
- Annex 1 – Revised Indicative Layout Plan
- Annex 4 – Revised Traffic Consideration
- Annex 5 – Revised Drainage Proposal
- Annex 6 – Letter to Relevant Government Department
- Annex 7 – Supporting Letters from TKLN Rural Committee and Local Residents
- Replacement Pages of Planning Statement





**Section 16 Application No. A/NE-TKLN/102**

**Annex 1 - Response to Comments Table**

No.	Comments Received	Our Responses
<b>1.</b>	<b><i>Comments from Drainage Services Department received on 25 July 2025</i></b>	
a	1. I have the following comments on the FI: <u>Drawing no. D01 refers.</u> The applicant should review if an u-channel along the northern boundary of lot 62 should be provided to intercept the overland flow from adjacent land.	The U-channel is provided along the northern boundary of Lot 62.
b	The applicant should update the legend for CP7.	It has been revised accordingly.
c	It was indicated that water will fall from +15.2mPD to +15.25mPD at the northern boundary of lot 65 S.B RP. The applicant should review.	It has been revised accordingly.
d	It is not feasible to construct a 450UC with CL=+15.25mPD and IL=+14.90mPD. The applicant should review and check if similar issues occur at other locations.	Noted. Invert level has been reviewed and revised accordingly in <b>Annex 5</b> .
e	The applicant should clarify if the diameter of the proposed pipe at the outlet is 675mm or 750mm, and update the drawing accordingly.	The diameter shall be 750mm. It has been revised accordingly.
f	<u>Drawing no. D02 refers.</u> The sections provided in this drawing do not match with the plan. The applicant should review.	The sections have been reviewed and revised accordingly in <b>Annex 5</b> .
g	Section B-B: The applicant should clarify if lot 62 forms part of the site.	Yes, Lot 62 is part of the Site in this application, the drainage proposal has been revised accordingly.
h	Unless the applicant could submit a revised drainage proposal to my satisfaction, I do not support the application from drainage perspective.	Noted with thanks.
<b>2.</b>	<b><i>Comments from Transport Department received on 28 July 2025</i></b>	
i	Please find below our major comments on the subject application:	It is believed that the required journey time delay analysis study is unnecessary to be done because of three reasons:



	<p>The applicant shall justify and demonstrate the use of 600 veh/hr is adequate for the design flow capacity of Lin Ma Hang Road, including a journey time delay analysis study should be conducted to demonstrate the journey time in connection with different v/c ratios.</p>	<ol style="list-style-type: none"><li>1. It is important to recognise that the current high traffic flow and parking demand in the area are not attributable to the Applicant but stem directly from the operations of the nearby Heung Yuen Wai Boundary Control Point. This control point generates a substantial volume of vehicular traffic, including cross-border commuters, tourists and commercial vehicles, which overwhelms the existing road infrastructure. In reality, even in the absence of the proposed development, the potential congestion issues would escalate, as the checkpoint's traffic continues unabated. Introducing the temporary vehicle park would, in fact, serve as a mitigating factor by providing organized parking options that divert vehicles off the roads, thereby reducing on-street queuing and haphazard parking that exacerbates delays. Requiring a journey time delay analysis under these circumstances would overlook the exogenous nature of the problem, focusing instead on a symptom rather than the underlying cause, and could delay the implementation of a much-needed facility.</li><li>2. The current operator, known as Skye Parking Limited, brings over two decades of specialised experience in managing open and covered vehicle parks, establishing itself as one of Hong Kong's premier providers of integrated parking solutions. This extensive history includes the existing public vehicle park operated at the Heung Yuen Wai Boundary Control Point as well as the vehicle parks at the Application Site which have gained support from various government departments, including Ta Kwu Ling Police Office and the Transport Department, as well as positive feedback from local residents, tourists and rural committees of Ta Kwu Ling North District (see <b>Annexes 6 and 7</b>). These existing and proposed temporary vehicle parks, supported by letters from local residents and rural committees, have demonstrated how they alleviated traffic congestion by offering reliable parking alternatives, which in turn satisfy the acute demand in the vicinity and prevent spillover effects such as illegal roadside parking since their operation in 2023. Historically, no such journey time delay analysis has</li></ol>
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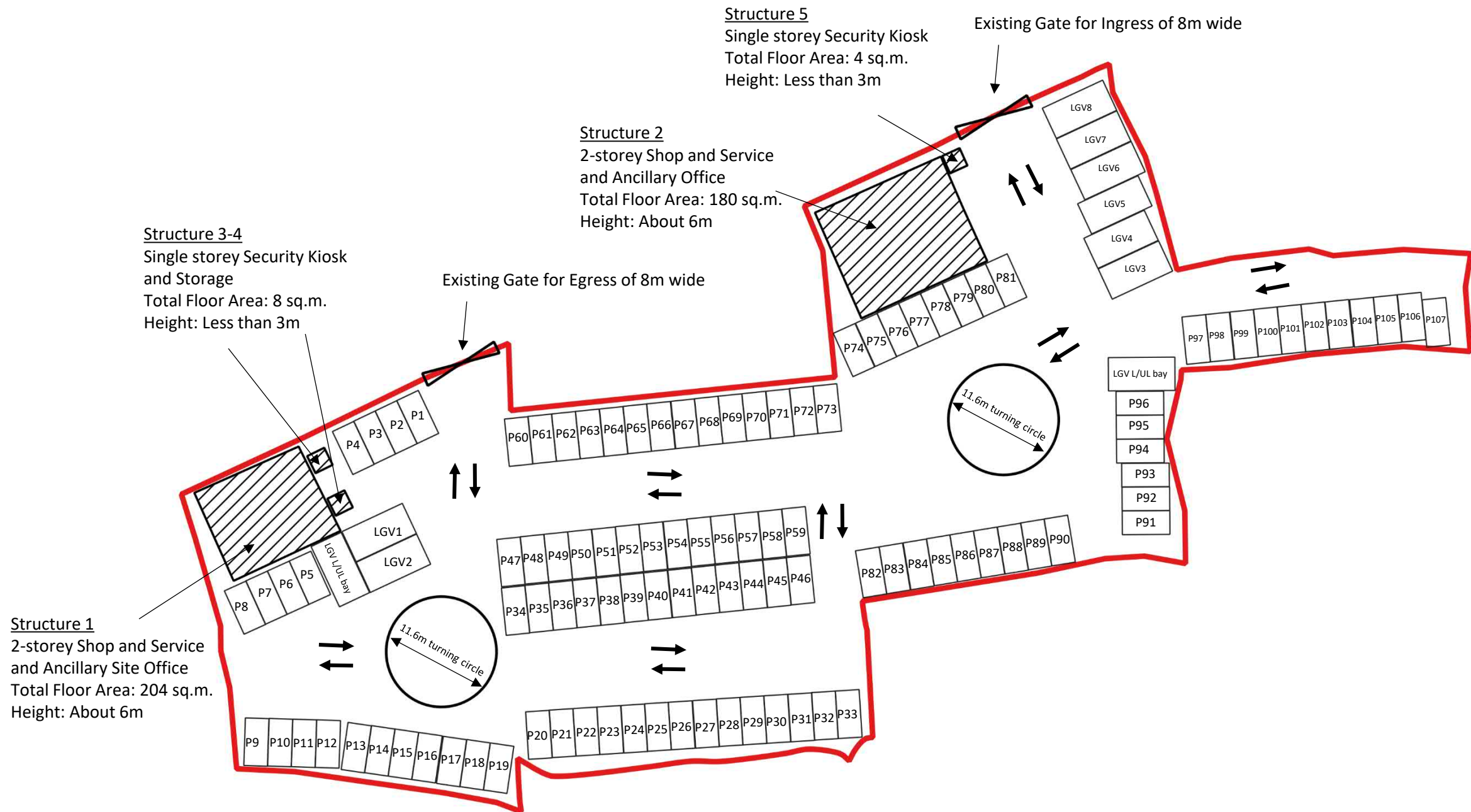


		<p>been required by relevant departments for prior approvals. The unnecessary analysis would impose undue time and financial burdens on the Applicant, yielding no substantive benefits given the proposed development's established positive impact. Moreover, any complaints about traffic congestion in the area are not linked to the Applicant's operations but, as previously mentioned, arise from the lack of parking space in the Boundary Control Point, driving desire of the travelers and the inadequate public transport capacity. Ultimately, the proposed vehicle park is not just beneficial but essential for assisting the government in fulfilling community demands, promoting smoother cross-boundary movements and enhancing overall public convenience—outcomes that align with broader urban planning goals and the positioning of the site as part of the boundary commerce and industry zone as outlined in the Northern Metropolis Action Agenda 2023.</p> <p>3. The Applicant fully acknowledges the potential for traffic congestion at the site and has proactively implemented traffic management measures to mitigate any adverse effects. For example, when the existing vehicle parks reach the capacity, clear signage is displayed to inform drivers and trained security guards are stationed to direct traffic and advise users to seek alternatives, thereby preventing unnecessary queuing or backups on the concerned roads. These measures have been refined over years of operation at the existing vehicle parks, resulting in a commendable track record: no formal complaints related to traffic congestion have been received from the community. This success demonstrates the Applicant's commitment to responsible stewardship and operational efficiency, ensuring that the facility integrates seamlessly into the local traffic ecosystem without contributing to delays.</p>
--	--	---



		Hence, requiring a journey time delay analysis would be redundant, as empirical evidence from ongoing operations already confirms the effectiveness of these controls in maintaining flow and safety.
j	b) The applicant shall justify the adoption of a growth rate of 14.3% (the growth rate of projected population over 3 years) is appropriate for traffic flow calculation.	The adoption of a growth rate has been revised in <b>Annex 4</b> based on the population and employment data obtained from 2021-based Territorial Population and Employment Data Matrices (TPEDM) planning data in North District published by Planning Department. An annual growth factor of 1.81% from 2025 to 2028 is adopted.
k	c) Our previous comment on "the applicant shall advise the management / control measures to be implemented to ensure no queueing of vehicles outside the subject site" has not been addressed. The applicant shall further supplement and propose additional measures to prevent queueing of traffic e.g. provision of parking information to the public.	As adopted in the existing practice, when the proposed public vehicle park is fully occupied, the staff will erect a "Full" sign at the entrance to notify and direct drivers to other nearby parking lots, and the staff will be stationed entrance to assist the drivers, ensuring there will be no queueing of vehicles outside the proposed development.
l	We might offer further comments after received the above requested information.	Noted.
<b>3. Comments from Lands Department received on 30 July 2025</b>		
m	Site inspection conducted by staff of this office on 14.7.2025 revealed that there were a total of 5 structures erected on the application site. The Built-over Area (BOA) of the 1 existing structures erected on Lot No. 61 SB RP in DD. 80 has exceeded the BOA permitted under the Short Term Waiver No. 1666. The size and height of the remaining 2 existing structures erected on Lot No. 65 SB RP in DD. 80 do not tally with the proposal under the subject planning application (i.e. the indicative layout plan at Annex I of the Supporting Planning Statement).	<p>Noted.</p> <p>Previously submitted planning application has been revised to reflect the actual condition of the Application Site. Upon planning approval, the Applicant will apply for a new Short Term Waiver for the concerned structure on Lot No. 61 SB RP in DD. 80 to ensure everything is tally with the permitted BOA.</p> <p>The size and height of the remaining two existing structures erected on Lot No. 65 SB RP in DD. 80 are also reflected in the revised layout plan in <b>Annex 1</b>, and revised planning statement.</p>
n	LandsD's previous comment sent to you on 17.7.2025 remain valid.	Noted with thanks.





*All parking spaces and loading/ unloading bay satisfy the dimension requirement under the Hong Kong Planning Standard and Guidelines*



**LCH Planning and Development  
Consultants Limited**

Annex 1 : Indicative Layout Plan

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)

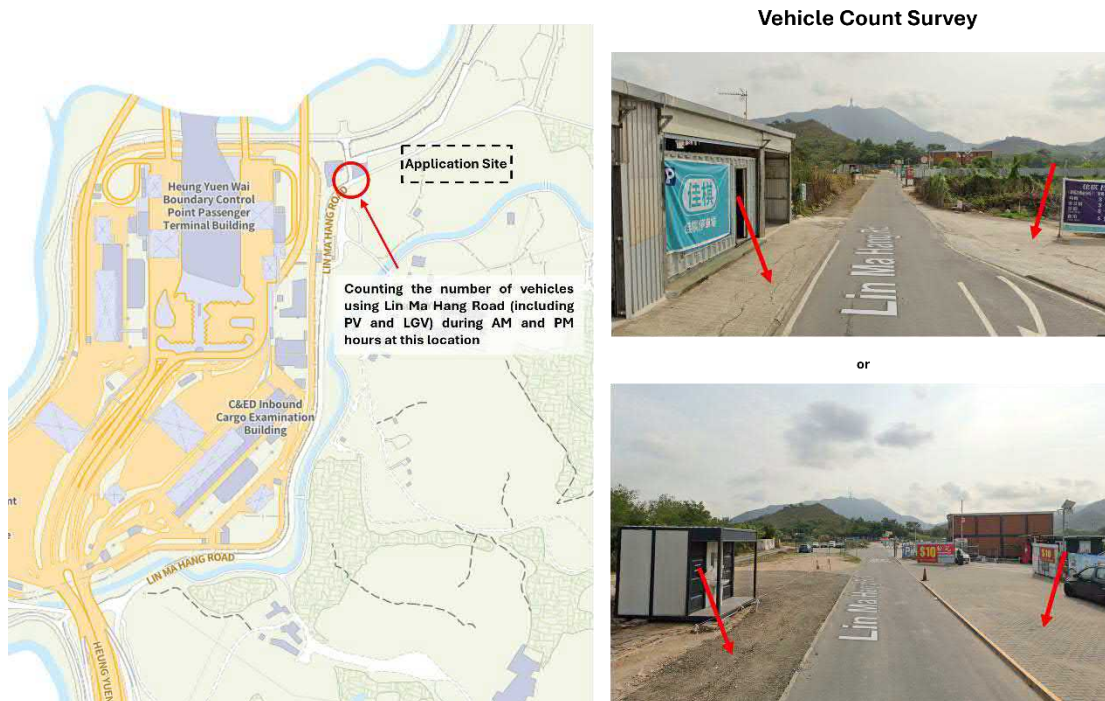
## **Annex 4**

# **Traffic Consideration**

## 1.1 Traffic Consideration

### *Operation Mode*

- 1.1.1 While the subject carpark is proposing a flexible mode, it is anticipated that the proposed temporary vehicle park will not incur adverse traffic impact. With most of the end user would be cross-boundary travellers, they may park their cars in the Application site and stay in the Mainland for one day or more.
- 1.1.2 In order to assess the existing traffic conditions, a vehicle count survey was conducted at the Lin Ma Hang Road from 8:00 am to 7:00 pm on 16 July (WED), 2025. Flow counts are recorded at 60-minute intervals; and converted to Passenger Car Unit (pcu) values. The highest 60-minute traffic volume is used as the peak hour traffic volume. Location of conducting the vehicle count survey is shown in **Diagram 1**.



**Diagram 1.** Location for Traffic Estimation

- 1.1.3 The morning and afternoon peak times for the network are determined to be 11:00am to 12:00pm and 5:00pm to 6:00pm respectively. The traffic flow observed in the study area is demonstrated in the following table.

### *Development Traffic Generation and Attraction*

- 1.1.4 A vehicle count survey was conducted at two existing public vehicle parks (Application No. A/NE-TKLN/57 and 58) in the vicinity from 8:00 am to 7:00 pm on 10 July (THURS), 2025. The traffic flow of the existing public vehicle parks at the peak hours is recorded and shown below:

	No. of Parking Space	AM (11 am - 12 pm) (pcu/hr)		PM (5 pm - 6 pm) (pcu/hr)	
		Generation	Attraction	Generation	Attraction
A/NE-TKLN/57	24 CPS	4	3	12	3
A/NE-TKLN/58	26 CPS	3	8	7	7

1.1.5 Flow counts are recorded at 60-minute intervals; and converted to Passenger Car Unit (pcu) values. The highest 60-minute traffic volume is used as the peak hour traffic volume. A summary of the result is as follow:

Public Vehicle Park		Generation		Attraction	
		AM	PM	AM	PM
	No. of Spaces	Traffic flow (pcu)			
A/NE-TKLN/57	24 CPS	4+3	5+8	12+7	3+7
A/NE-TKLN/58	26 CPS				
Total	50 CPS				
Trip rate (pcu/hr/parking space)					
		= 7/50 = 0.14	=13/50 = 0.26	= 19/50 =0.38	= 10/50 =0.2
Trips in veh/hr (pcu/hr)					
Proposed Parking Facilities		= 0.14 x 115 <sup>(3)</sup> = 16	= 0.26 x 115 <sup>(3)</sup> = 30	=0.38 x 115 <sup>(3)</sup> = 44	=0.2 x 115 <sup>(3)</sup> = 23
Proposed Shop and Services		2	2	2	2
Total		18	32	46	25

- (1) It is noted from vehicle count survey that the two sites have been operated as a whole, with its ingress and egress marked in Figure 2.
- (2) The number of estimated traffic generation (10) and number of estimated traffic attraction (13) are derived from the TIA submitted with planning application no. A/NE-TKLN/75.
- (3) The total number of parking spaces for this application is 115.

#### Existing Traffic Capacity

1.1.6 The Volume to Capacity (V/C) ratio represents the proportion of road capacity used by traffic flow during peak hours. Higher V/C ratios for roads indicate greater use of road connection problems. A V/C ratio of 0.85 or less indicates that there is sufficient capacity available and vehicles are not expected to experience significant queues and delays.

1.1.7 To assess the current traffic conditions at the Application Site, traffic count surveys were conducted during both AM and PM peak hours on a typical weekday in 2025.

1.1.8 Based on the observed peak hour traffic flows, the performances on the Lin Ma Hang Road could be assessed. The results are summarized in the following table:

No.	Road Link (Direction)	Observed Flow		V/C Ratio	
		AM	PM	AM	PM
L1	Lin Ma Hang Road (Eastbound)	110	69	0.31	0.19
L2	Lin Ma Hang Road (Westbound)	95	94	0.26	0.26

(1) The Capacity Index for Road Links is Peak Hourly Traffic Flow/Design Flow Capacity

(2) Design Capacity of the Link according to TPDM, reduction was considered due to high proportion of Heavy Goods Vehicles. Thus, the design capacity of Lin Ma Hang Road (Rural road with 6.3m 2-lane single carriageway) = 720 veh/hr (2-way)

- 1.1.9 As demonstrated in the above table, that the V/C ratio of Lin Ma Hang Road is less than 0.85, which means the Lin Ma Hang Road operates satisfactorily during the peak hour of weekday.

#### *Future Year Reference Traffic Flows*

- 1.1.10 Reference is made to the 2021-based Territorial Population and Employment Data Matrix (TPEDM) planning data published by Planning Department. The following table presents the population and employment data in North District for 2021, 2026 and 2031.

Category	2021	2025 <sup>(1)</sup>	2026	2028 <sup>(2)</sup>	2031	% Growth p.a.
						2025 - 2028
Population	309,650	330,825	352,000	393,775	435,550	1.69%
Employment	84,150	94,130	104,050	124,450	144,850	2.31%
Total	393,800	424,955	456,050	518,225	580,400	1.81%

Source: 2021-based TPEDM published by Planning Department.

- (1) 2025 population and employment places are calculated by interpolation between 2021 – 2026  
 (2) 2028 population and employment places are calculated by interpolation between 2026 – 2031

- 1.1.11 It is anticipated that the population and employment places in North District would be increased by 1.69% and 2.31% p.a. respectively, with an overall annual growth rate of 1.81% between 2025 and 2028.

- 1.1.12 For the traffic growth of year between Year 2025 to 2028, reference is made to the estimation from “Hong Kong Population Projection 2022-2046” published by Census and Statistics Department (C&SD). The following table presents the projected population data in 2025 and 2028.

Category	2025	2028	% Growth p.a.
			2025 - 2028
Projected Population	7,559,800	7,684,500	+0.5%

- 1.1.13 As illustrated in the above tables, the predicted growth of population from 2025 to 2028 is approximately +0.5% per annum. Taking into account the above factors, it is proposed to adopt an average growth rate of 1.81% per annum from 2025 to 2028 to forecast the 2028 background traffic flows.

#### *Traffic Assessment*

- 1.1.14 This application is seeking temporary permission for a period of 3 years only, which targets to commence by the end of 2025. Thus, to estimate the worst-case scenario, a growth factor will be applied to the end of this application, i.e., 2028 to demonstrate the traffic impact.



$$2028 \text{ Reference Flow} = 2025 \text{ Observed Flow} \times 1.81\%^1$$

$$2028 \text{ Design Flow} = 2028 \text{ Reference Flow} + \text{Development Flow of this Application}$$

1.1.15 The link capacity assessment results under the 2028 reference and design scenarios are summarised in table below:

**2028 Traffic Assessment**

No.	Road Link (Direction)	Reference Flow (veh/hr)		Reference V/C Ratio		Design Flow (veh/hr)		Design V/C Ratio	
		AM	PM	AM	PM	AM	PM	AM	PM
L1	Lin Ma Hang Road (Eastbound)	96 x 1.81 = 174	55 x 1.81 = 100	0.48	0.28	174 + 4 + 3 = 181	100 + 5 + 8 = 113	0.50	0.31
L2	Lin Ma Hang Road (Westbound)	81 x 1.81 = 147	80 x 1.81 = 145	0.41	0.40	147 + 12 + 7 = 166	145 + 3 + 7 = 155	0.46	0.43

(1) The Capacity Index for Road Links is Peak Hourly Traffic Flow/Design Flow Capacity

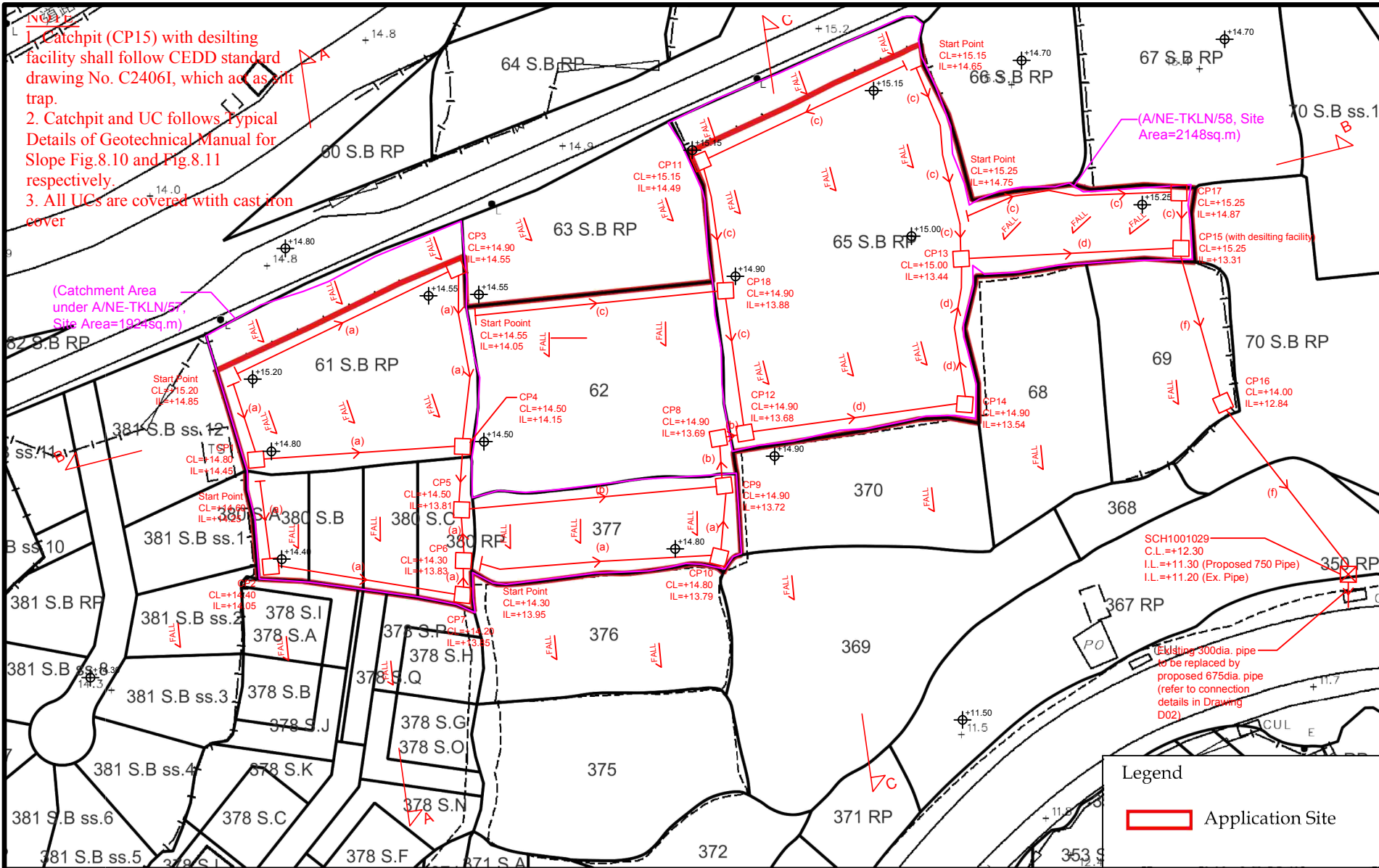
(2) Design Capacity of the Link according to TPDM, reduction was considered due to high proportion of Heavy Goods Vehicles. Thus, the design capacity of Lin Ma Hang Road (Rural road with 6.3m 2-lane single carriageway) = 720 veh/hr (2-way)

Notes: The design flow has also taken into account the traffic flow of adjacent site at Lot Nos. 388 S.A, 388 S.B, 388 RP (Part) and 390 RP (Part) in D.D. 78 and Adjoining Government Land.

1.1.16 As shown in table above, the V/C ratio of Lin Ma Hang Road is less than 0.85 which means the Lin Ma Hang Road would be performing satisfactorily during the peak periods for both Reference and Design Scenarios. No adverse traffic impact is anticipated.

1.1.17 Traffic management measures proposed in Section 2.9 in the supporting planning statement would be fully implemented. With only 8 to 9 trips per hour generated and attracted from the application, it is expected that there will not be significant negative impacts regarding the safety of road users and the traffic network of the area concerned.

<sup>1</sup> According to the Highways Department Agreement No. CE 51/2013 (HY) Widening of Western Section and Eastern Section of Lin Ma Hang Road – Design and Construction Note of Traffic Forecast Review (Western Section), the steady traffic growth rate of 0.6% p.a. is anticipated. However, an annual growth factor of 1.81% from 2025 to 2028 has been adopted by making reference to the population and employment data obtained from 2021-based Territorial Population and Employment Data Matrices (TPEDM) planning data in North District published by Planning Department.



LEGEND	
(a)	Proposed 300UC (1:200) with Cast Iron cover
(b)/(c)	Proposed 450UC (1:200) with Cast Iron cover
(d)	Proposed 525UC (1:200) with Cast Iron cover
(e)	Proposed 675UC (1:200) with Cast Iron cover
(f)	Proposed 750 underground concrete pipe (1:175)
☒	Existing Catchpit
□	Proposed Catchpit
⊕	Existing Level

Company:  
正宏工程顧問公司  
Ching Wan Engineering Consultants Company

PROJECT:  
Section 16  
Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

CH Planning and Development Consultants Limited

Status

only. Not to scale.)

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

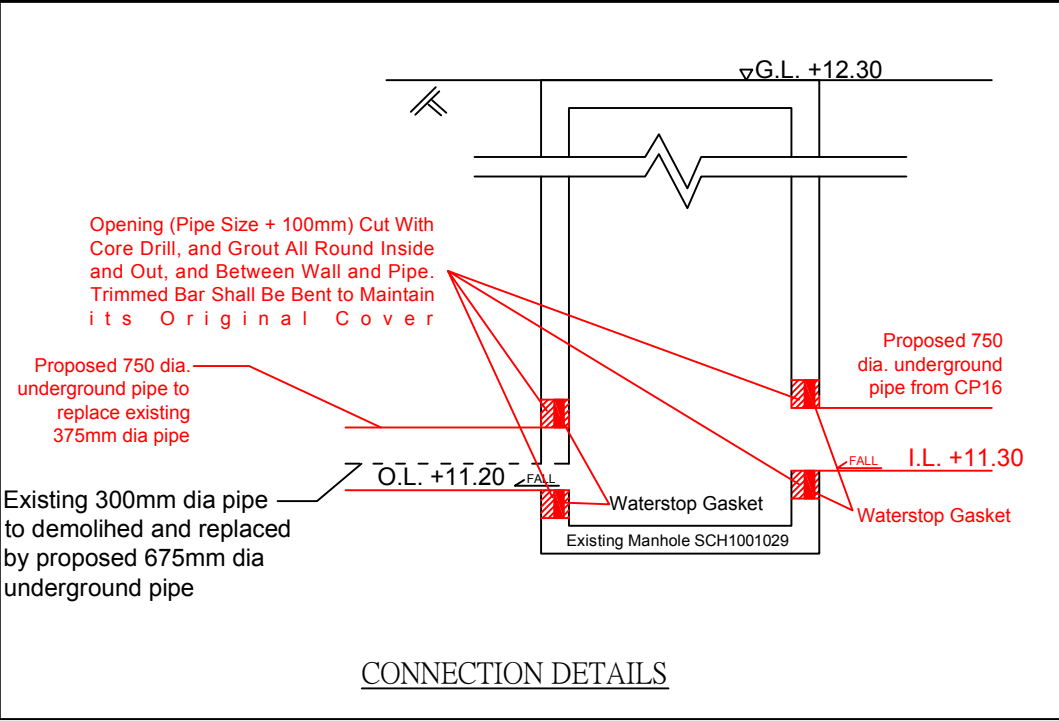
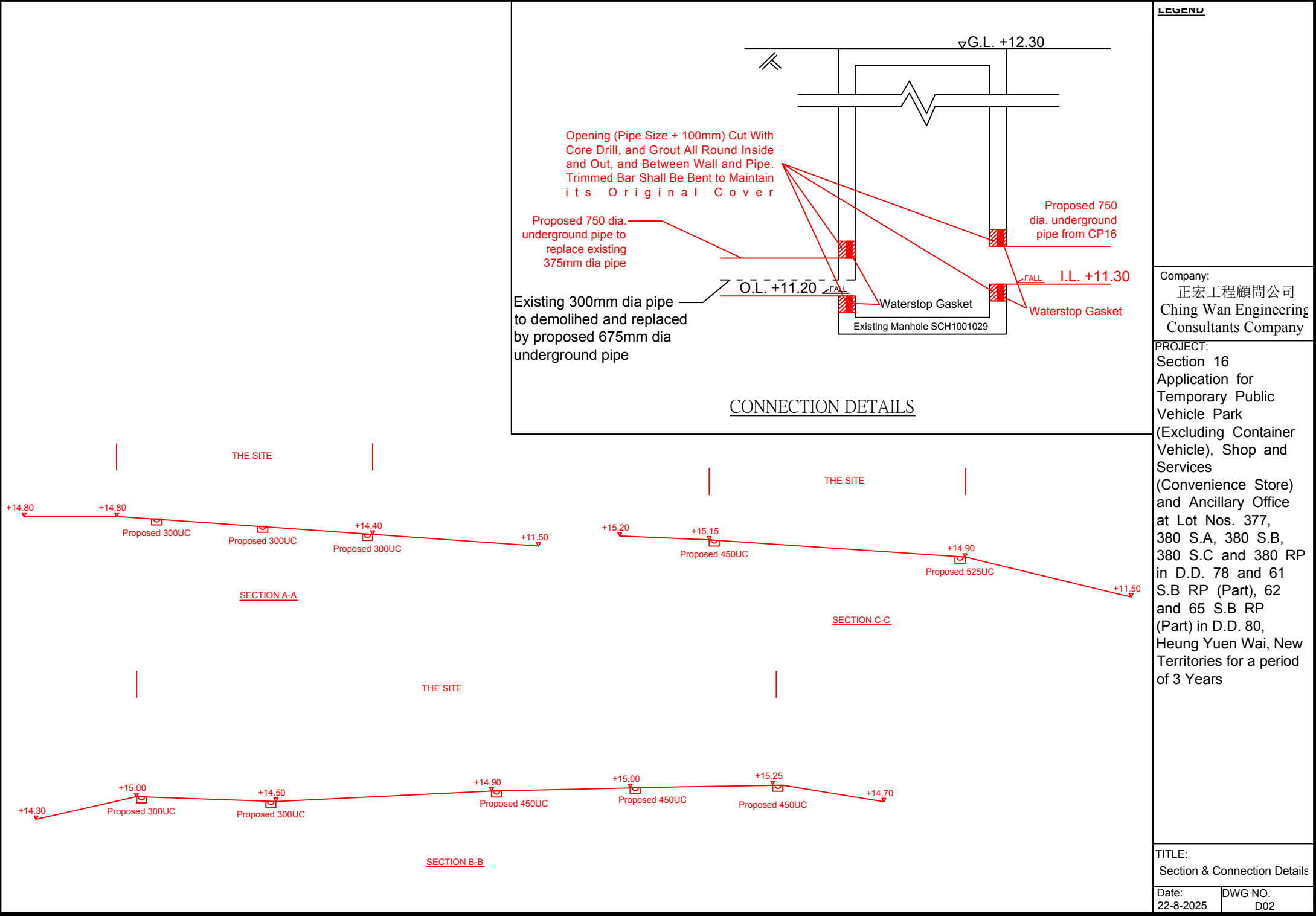
(Source: HK GEODATA STORE, HKSAR Government)

Legend  
Application Site

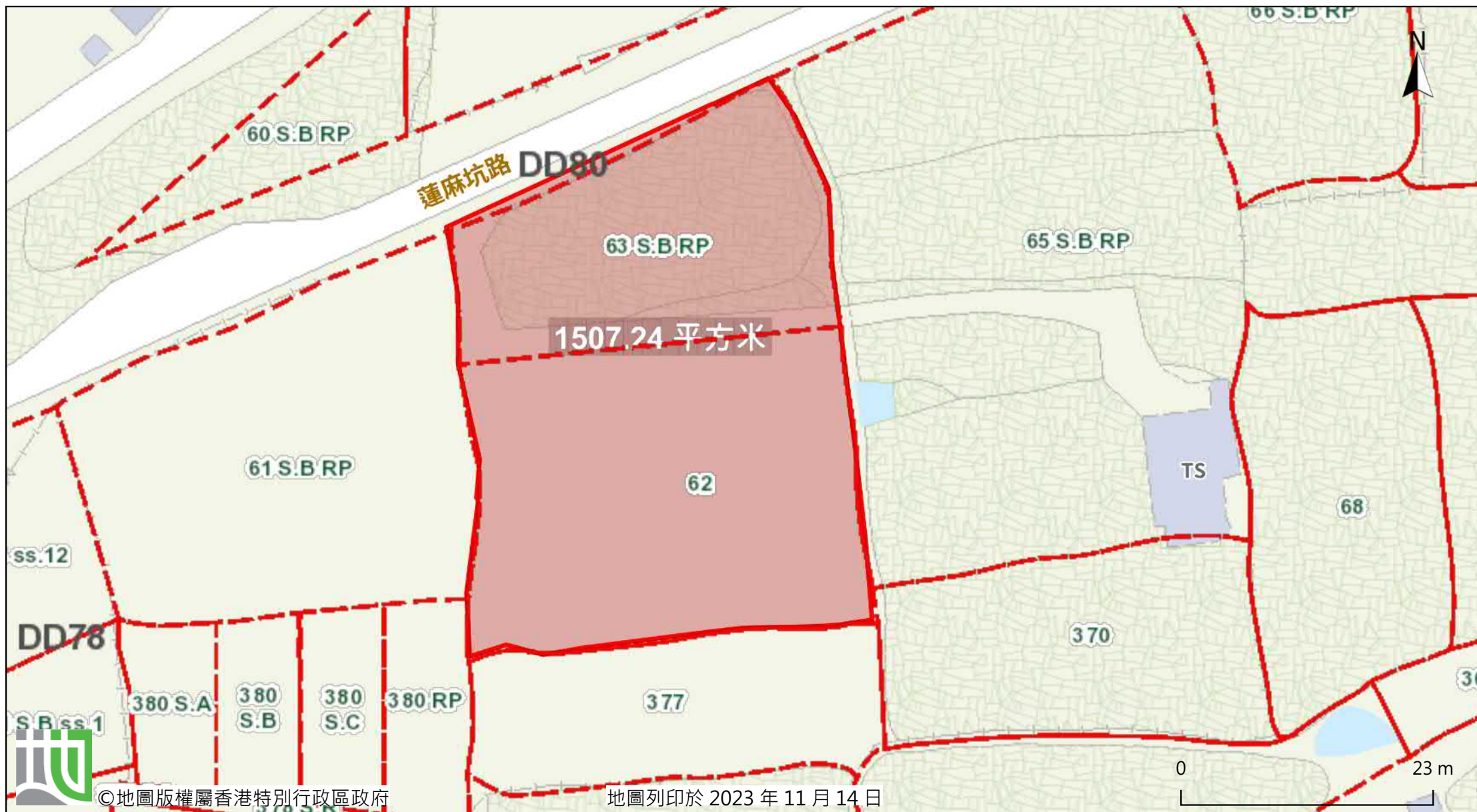
Annex 5

TITLE:  
Drainage Proposal

Date: 22-8-2025	DWG NO. D01
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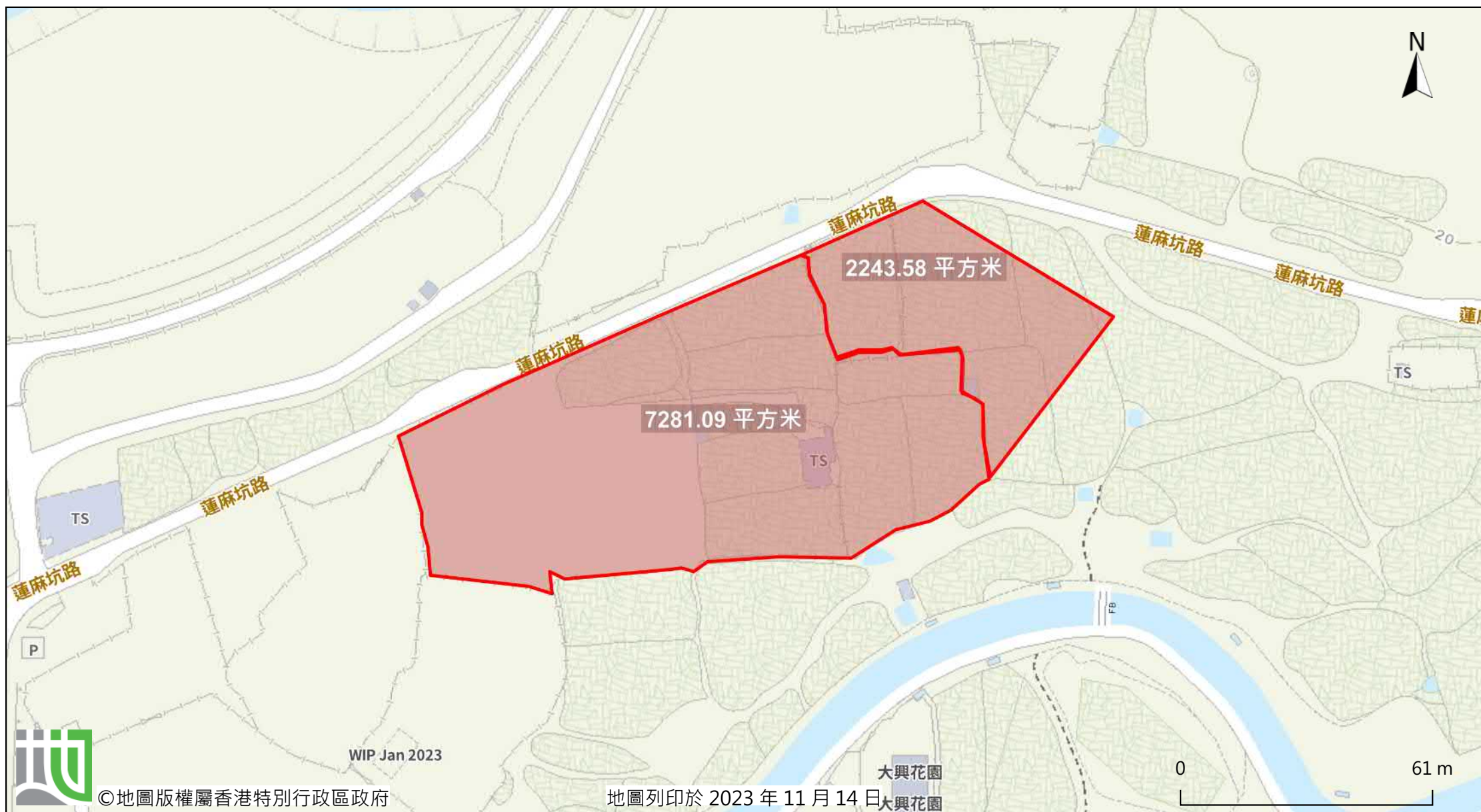






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**Catchment Area under A/NE-TKLN/57**

Site Area = 962 m<sup>2</sup> (1924/2)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 962 \quad \text{m}^2$$
$$= 0.000962 \quad \text{km}^2$$

$$t = 0.14465 L / H^{0.2} A^{0.1}$$
$$= 0.14465 * 10 / 1^{0.2} * 962^{0.1}$$
$$= 0.728 \quad \text{min}$$

$$i = 1.111 * a / (t + b)^c \quad (\text{50 yrs return period, Table 3d, Corrigendum 2024, SDM and (11.1\% increase due to climate change)})$$
$$= 1.111 * 474.6 / (0.728 + 2.60)^{0.371}$$
$$= 326.9 \quad \text{mm/hr}$$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001924$

$$= 0.083 \quad \text{m}^3/\text{sec}$$
$$= 4983 \quad \text{lit/min}$$

**Provide 300UC (1:200) is OK**

**Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP**

Lot 62 & 63BRP = 1507 m<sup>2</sup> (C=0.95)

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 1507 \quad \text{m}^2$$
$$= 0.001507 \quad \text{km}^2$$

take  $i = 326.9 \quad \text{mm/hr}$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001507$

$$= 0.130 \quad \text{m}^3/\text{sec}$$
$$= 7806 \quad \text{lit/min}$$

For Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP

$$Q = 4983 + 7806$$
$$= 12790 \quad \text{lit/min}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/58+Outside Catchment Area**

Site Area = 2148 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278*0.95*326.9*0.002148+0.278*0.25*326.9*0.002244 \\ &= 0.236 \text{ m}^3/\text{sec} \\ &= 14186 \text{ lit/min}\end{aligned}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/57+Lot 62 & 63BRP+A/NE-TKLN/58+Outside Catchment Area**

$$Q = 12790 + 14186$$

$$= 26975 \text{ lit/min}$$

**Provide 525UC (1:200) is OK**

**Outfall (all catchment area**

Site Area = 7281 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278*0.95*326.9*0.007281+0.278*0.25*326.9*0.002244 \\ &= 0.680 \text{ m}^3/\text{sec} \\ &= 40775 \text{ lit/min}\end{aligned}$$

**Provide 750UC (1:200) is OK**

Check 750mm dia. Pipes by Colebrook-White Equation

$$V = -\sqrt{(8gDs)} \log\left(\frac{ks}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}}\right)$$

where :

V	=		mean velocity (m/s)	
g	=	9.81	m/s <sup>2</sup> gravitational acceleration (m/s <sup>2</sup> )	
D	=	0.75	m internal pipe diameter (m)	
ks	=	0.00015	m hydraulic pipeline roughness (m)	(Table14, from DSD SDM 2018, concrete pipe)
v	=	1.14E-06	m <sup>2</sup> /s kinematic viscosity of fluid (m <sup>2</sup> /s)	
s	=	0.005	hydraulic gradient	

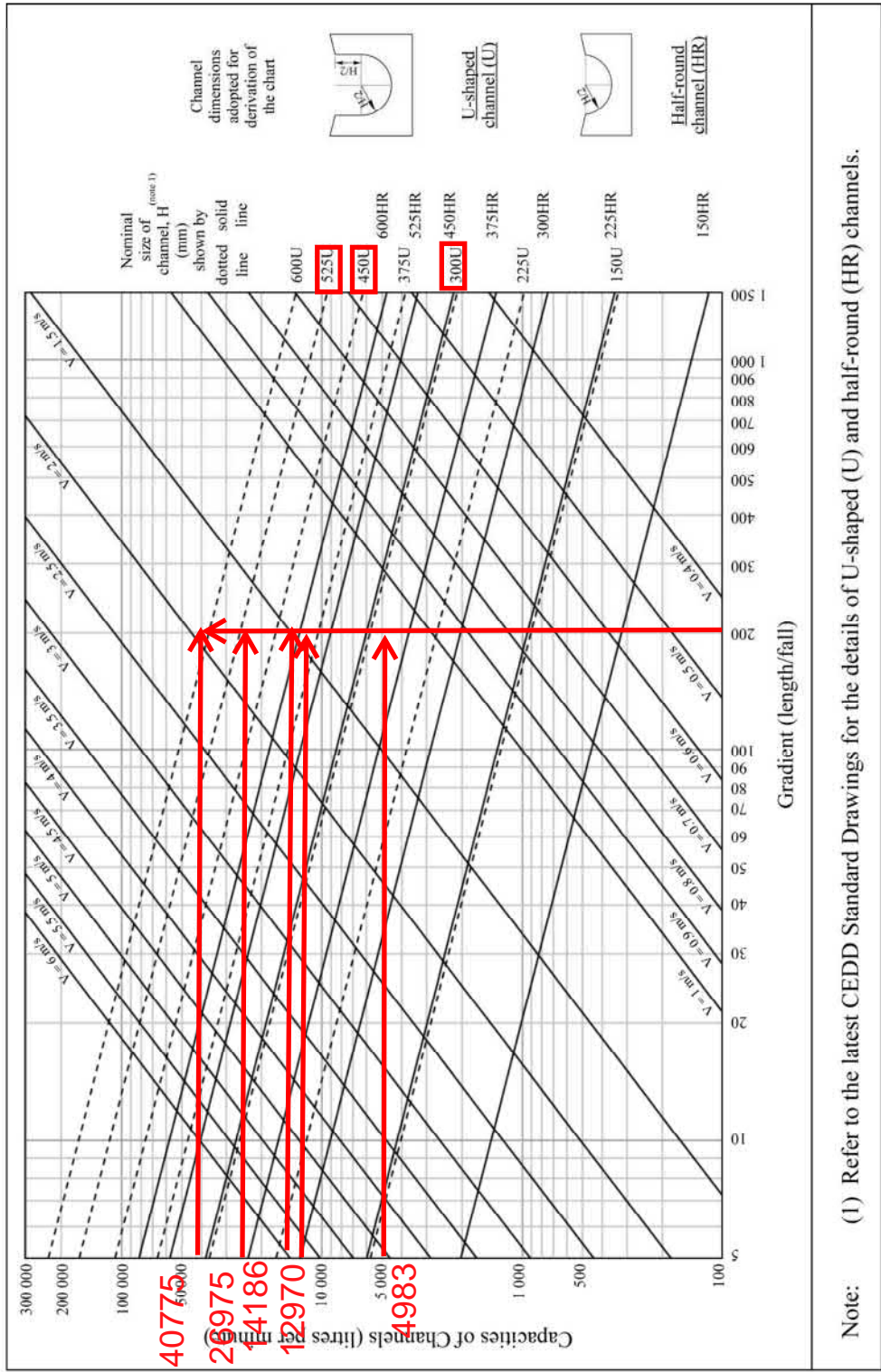
Therefore, design V of pipe capacity = 2.2604 m/s

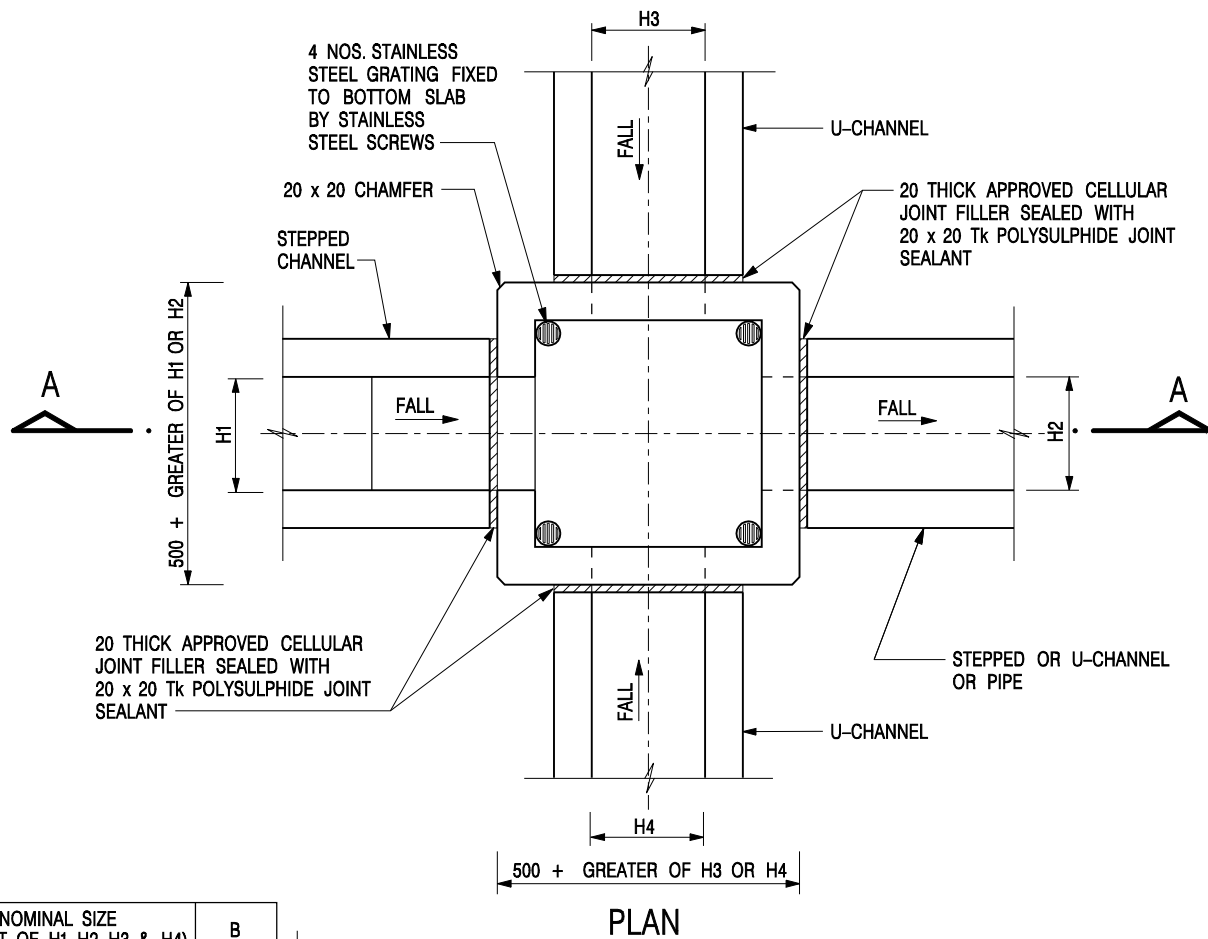
Q= 0.8VA		(0.8 factor for sedimentation)
= 0.799	m <sup>3</sup> /s	
= 47934	lit/min	
> 40775	lit/min	Ok

**GEO Technical Guidance Note No. 43 (TGN 43)**  
**Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes**

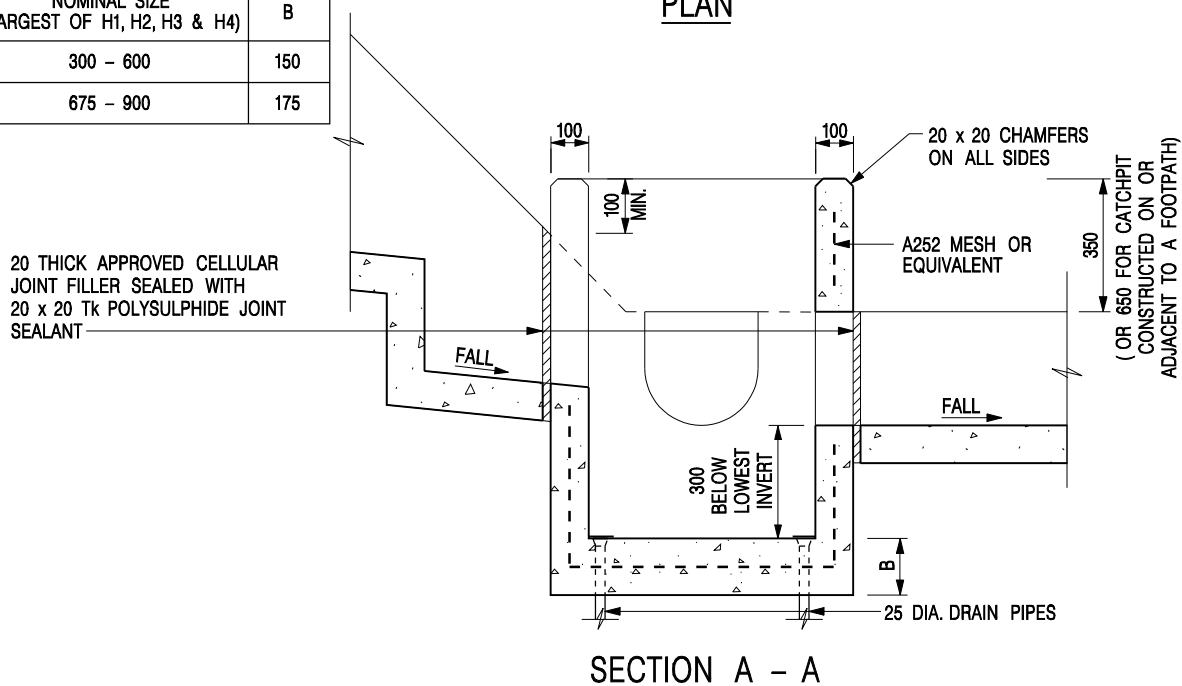
Issue No.: 1      Revision: -      Date: 05.06.2014      Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm






NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175



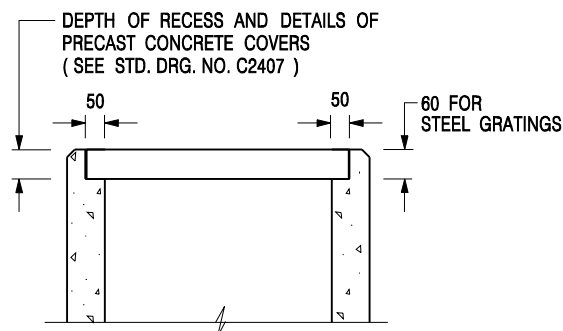
**NOTES:**

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

**CATCHPIT WITH TRAP**  
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 <b>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</b>		<b>SCALE</b> 1 : 20	
		<b>DATE</b> JAN 1991	
		<b>DRAWING NO.</b> C2406 /1	





### ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

#### NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS ( SEE DETAIL 'A' ON STD. DRG. NO. C2405 ) OR CONCRETE COVERS ( SEE STD. DRG. NO. C2407 ) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING ( SEE DETAIL 'G' ON STD. DRG. NO. C2405; EXCEPT ON THE UPSLOPE SIDE ) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS ( SEE DSD STD. DRG. NO. DS1043 ) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'F' ON STD. DRG. NO. C2405.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
<b>REF.</b>	<b>REVISION</b>	<b>SIGNATURE</b>	<b>DATE</b>

**CATCHPIT WITH TRAP**  
**(SHEET 2 OF 2)**



**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

**DRAWING NO.**

**C2406 /2**

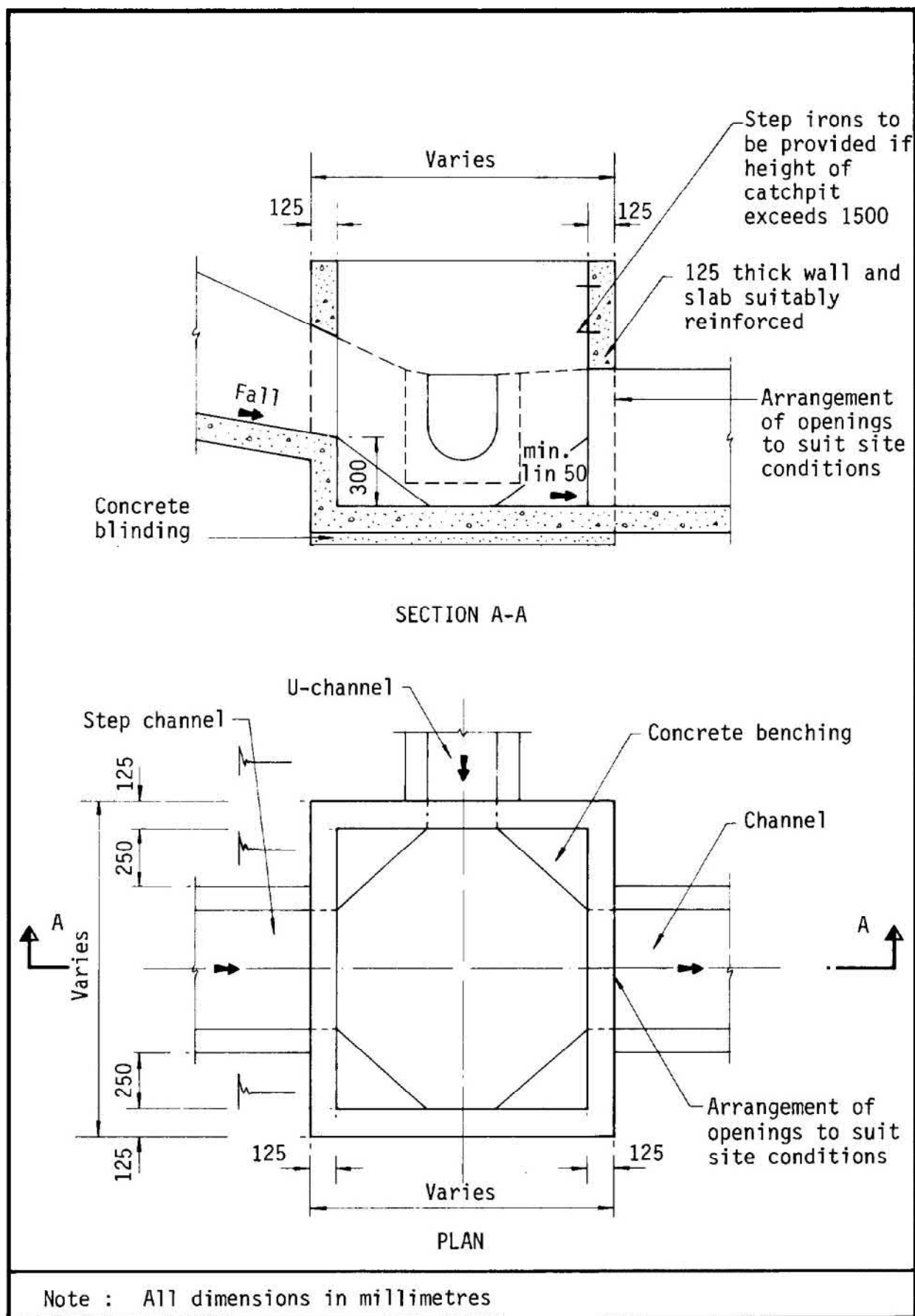
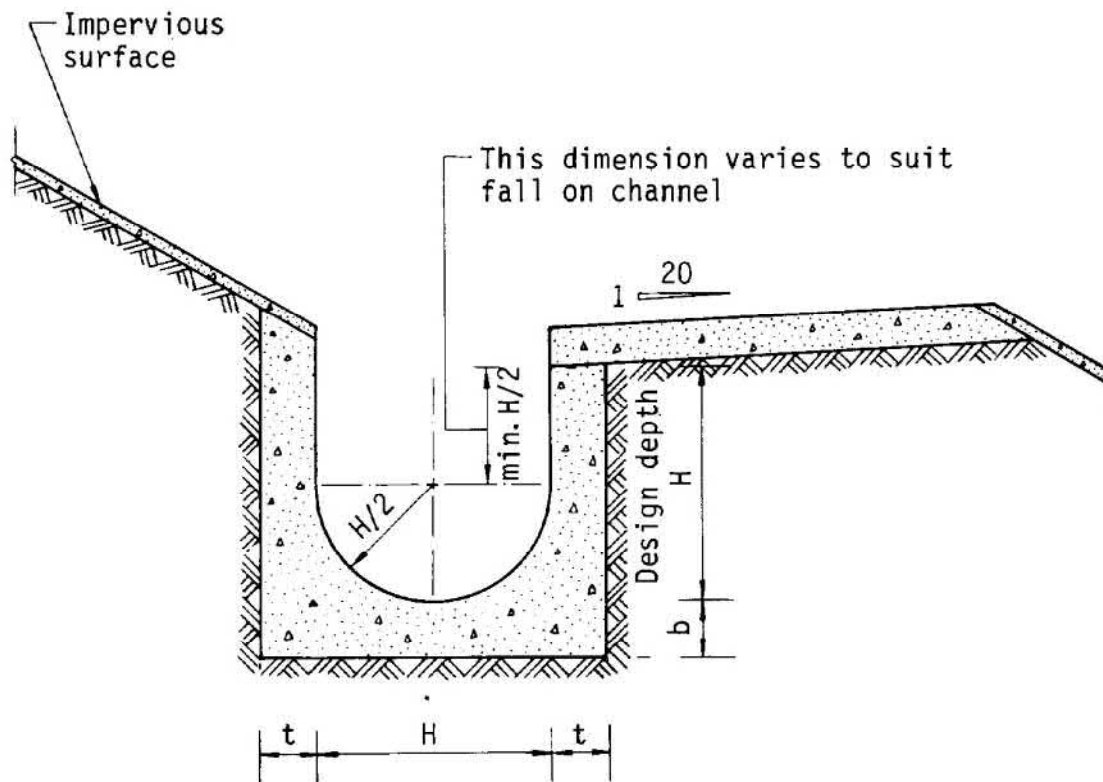


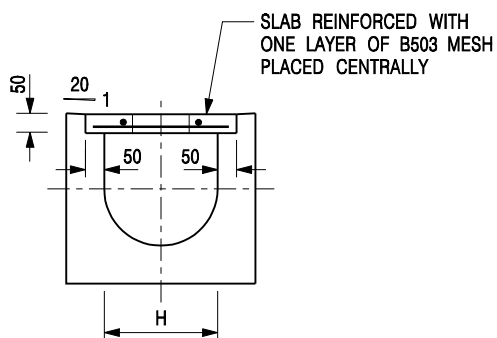
Figure 8.10 - Typical Details of Catchpits



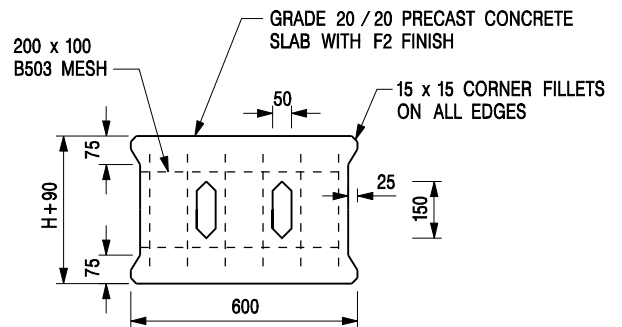
Dimensions of U - channel

Nominal size of channel H (mm)	Thickness t (mm)	Thickness b (mm)
225 to 600	150	150
675 to 1200	175	225

Figure 8.11 - Typical U-channel Details



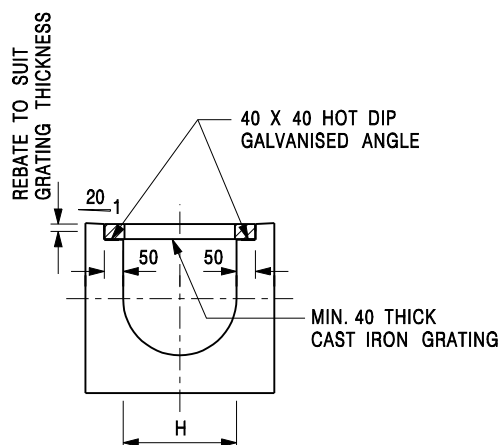
**TYPICAL SECTION**



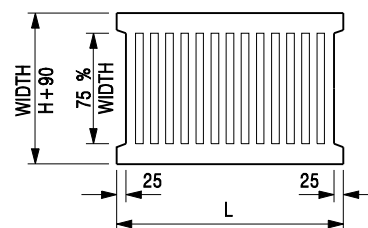
**PLAN OF SLAB**

## **U-CHANNELS WITH PRECAST CONCRETE SLABS**

(UP TO H OF 525)



**TYPICAL SECTION**



L = 600mm FOR H ≤ 375mm  
L = 400mm FOR H > 375mm

**CAST IRON GRATING**

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

## **U-CHANNEL WITH CAST IRON GRATING**

(UP TO H OF 525)

### **NOTES:**

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**COVER SLAB AND CAST IRON  
GRATING FOR CHANNELS**



**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

**DRAWING NO.**  
**C2412E**



Skye Parking Limited

緯德停車場有限公司

打鼓嶺警署交通部

新界打鼓嶺坪輦路

尊敬的負責人：

有關新界打鼓嶺蓮麻坑路地段 61 S.B RP 外貴部門於私人地方放置路障的事宜

我司謹此函達，貴署最近於上述地段的部分土地上放置路障，這導致本公司的業務受到損失，並直接影響車主對本公司的印象。我們希望了解貴署放置路障的原因。(附圖)

該私人土地是本公司向業主租用的部分土地，作為停車場用途。

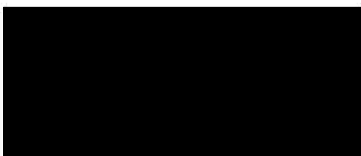
貴署所放置路障的範圍，本公司僅於交通繁忙時用作臨時停車，以便疏導交通，防止附近交通混亂，若非必要時並不會使用該範圍。現場警員曾提及因該部分土地上畫有黃線，故不能停車，而本公司現已清除所有黃線及鐵柱。

此外，附近其他停車場的管理公司亦在私人土地外停泊車輛，煩請貴署跟進此事。(附圖 1 及 2)

我們懇請貴部門盡快回覆，告知我們相關的處理結果及進一步的措施。

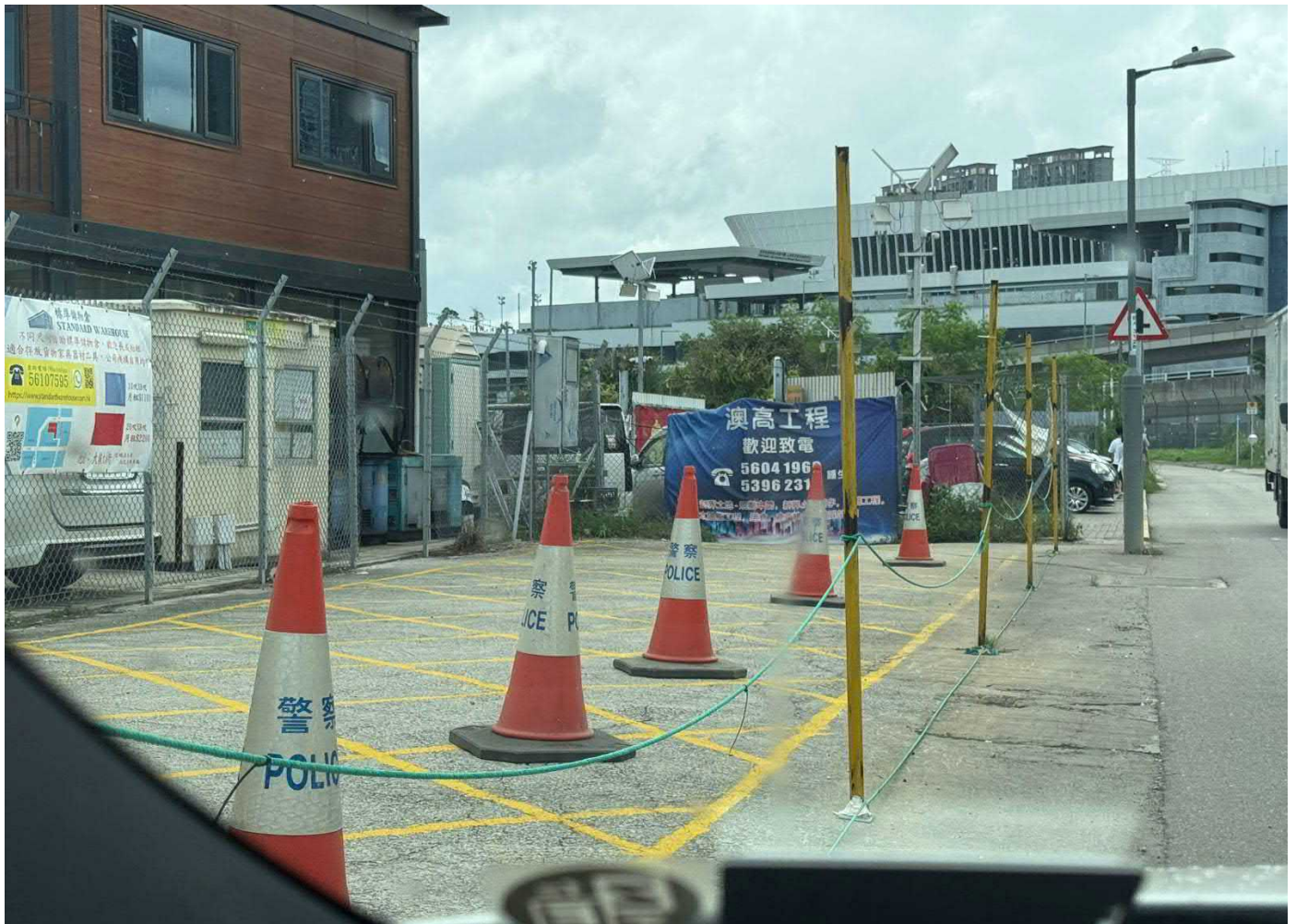
謝謝您的關注。

此致，



A handwritten signature in blue ink, appearing to be a stylized name.









致政府產業署

敬啟者：

有關香園圍邊境管制站公眾收費停車場

我謹代表打鼓嶺區鄉事委員會，特此致函以表達我們支持緯德停車場有限公司參與今年7月4日的重新招標。緯德停車場有限公司自2023年起接手管理及營運位於蓮麻坑路的三個露天停車場，我們深感欣慰。

緯德停車場有限公司在管理方面展現了卓越的專業能力。他們不僅有效解決了蓮麻坑一帶因泊車問題而引發的交通阻塞情況，還通過合理的車位規劃和流量管理，顯著提高了停車場的運營效率。此外，他們積極與社區居民溝通，了解民眾的需求，並針對性地優化服務，這一點得到了村民和旅客的一致好評。

自緯德停車場有限公司開始營運以來，村民和旅客均表示滿意，反映出該公司的服務不僅提升了停車便利性，更改善了周邊的交通環境。我們相信，持續支持他們的運營將進一步促進本區的發展與繁榮。

因此，我們懇請貴署考慮將香園圍邊境管制站公眾停車場的經營權繼續交由緯德停車場有限公司負責。相信在他們的專業管理下，我們的社區將獲得更多便利，並持續改善交通流暢性。

此致！



張天送

打鼓嶺區鄉事委員會首副主席

2025年6月25日



致：政府產業署

敬啟者：

有關香園圍邊境管制站公眾收費停車場

本人（萬新財）是香園圍村原居民代表，本人對緯德停車場有限公司在停車場管理方面的努力表示感謝。今年7月4日將會重新招標，緯德停車場有限公司自2023年起接手管理及營運位於蓮麻坑路的三個露天停車場，我們深感欣慰。

緯德停車場有限公司以其專業的管理方式，有效解決了蓮麻坑一帶因泊車問題而引發的交通阻塞情況，極大地改善了我們的出行環境。自該公司開始營運本區停車場以來，村民和旅客均對其服務表示高度讚賞與支持。

因此，我們希望貴署能考慮將香園圍邊境管制站的公眾停車場的經營權繼續交由緯德停車場有限公司負責管理，以便他們能夠以同樣的專業精神，為我們的社區帶來更多的便利與改善蓮麻坑路的交通暢順。

簽署：



（香園圍村原居民代表：萬新財）

日期：2025年6月27日



## **Executive Summary**

*(in case of discrepancy between English and Chinese versions, English version shall prevail)*

This Application is submitted to the Town Planning Board (“**the Board**”) under Section 16 of the Town Planning Ordinance (“**the Ordinance**”) for a proposed temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Service (convenience store) and Ancillary Office for a period of 3 years (“**the Proposed Development**”) at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78, and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80 in Heung Yuen Wai, New Territories (“**the Application Site**”).

The Application Site falls within an area of “Recreation” (“**REC**”) zone and “Village Type Development” (“**V**”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 (“**the OZP**”). Upon completion of the Proposed Development, it comprises 107 parking spaces for private cars (at least 2.5m x 5m each) and 8 parking spaces for Light Goods Vehicle (LGV) (at least 3.5m x 7m each), as well as two two-storey structures for convenience store and ancillary office uses with building height of 6 m, and three structures for security kiosk and storage with building height less than 3 m. The total floor area is about 396 sq.m. There will be two various loading and unloading bays for LGV to support the Proposed Development.

The Application echoes government’s direction to provide more parking spaces to serve the Heung Yuen Wai Boundary Control Point, which aims to accommodate the high demand of the local villagers and the cross-boundary travellers for car parking spaces. It is situated at a convenient location that is within 5 minutes walking distance from the Boundary Control Point.

The Proposed Development does not hinder the long-term planning intention of “REC” and “V” zone. Similar applications in the “REC” and “V” zone in the Ta Kwu Ling North area have also been approved and considered. The Proposed Development will not cause adverse traffic, visual, landscape, drainage and environmental impacts.

In view of the above justifications, we would sincerely seek for favourable consideration of the Board to approve this Application.





## 內容摘要

(如內文與其英文版本有差異，則以英文版本為準)

本規劃申請根據《城市規劃條例》第 16 條，就新界香園圍丈量約份第 78 約地段第 377 號、第 380 號 A 分段、第 380 號 B 分段、第 380 號 C 分段及第 380 號餘段及丈量約份第 80 約地段第 61 號 B 分段餘段（部分）、第 62 號，以及第 65 號 B 分段餘段（部分）（下稱「**申請地點**」）的用地，向城市規劃委員會（下稱「**城規會**」）申請作擬議臨時公眾停車場（貨櫃車除外）及臨時商店及服務行業（便利店）連附屬辦公室用途，為期 3 年（下稱「**擬議發展**」）。

申請地點現時於《打鼓嶺北分區計劃大綱圖編號 S/NE-TKLN/2》（下稱「**大綱圖**」）劃作「康樂」和「鄉村式發展」地帶。擬議發展包括 107 個私家車專用停車位（每個最少 2.5 米 x 5 米）和 8 個輕型貨車專用停車位，以及兩個兩層高的構築物作為臨時商店及服務行業和附屬辦公室用途，高度約 6 米。另外，三個少於三米高的擬議構築物則作為停車場更亭和儲物室。總樓面面積約為 396 平方米。擬議發展將會有兩個輕型貨車的上落客貨車位。

申請響應政府提供更多泊車位服務香園圍口岸的方向，旨在滿足當地村民和過境旅客對泊車位的殷切需求。申請地點坐落在一個方便的位置，從蓮塘/香園圍口岸步行 5 分鐘即可到達。

擬議發展不會妨礙打鼓嶺北「康樂」和「鄉村式發展」地帶的長遠規劃意向。而且於打鼓嶺北的「康樂」和「鄉村式發展」地帶亦已有不少相關申請曾被批准。擬議發展不會對交通、視覺、景觀、排水和環境造成不利影響。

鑒於以上提出的依據，我們真誠地尋求城規會批准該申請。



## 2.9 Temporary Public Vehicle Park for Private Cars, Shop and Services and Ancillary Office

2.9.1 The current Application Site covers an area of about 4,628 sq.m. The Proposed Development will provide a total of 107 parking spaces for private cars (at least 2.5m x 5m each) and 8 parking spaces for LGV (at least 3.5m x 7m each) within the Application Site.

2.9.2 Following the previously approved applications No. A/NE-TKLN/57 and 58, there will be two two-storey structures as temporary shop and services (convenience store) and ancillary office, each situated at the north-western portion and north-eastern portion of the Application Site. Together, with a total floor area of 396 sq.m. and a height of about 6 meters. There will be two loading & unloading bays at the north-western portion and north-eastern portion of the Application Site to serve the commercial use.

Parameters	Proposed Development
Site Area	About 4,628 sq.m.
No. of Structure	5
Covered Area	About 204 sq.m.
Maximum Height of Structure	Two structures with height about 6 metres, two-storey; and three structures with height less than 3 metres
Maximum Total Site Coverage	About 4.4%
Maximum Plot Ratio	About 0.086
No. of Parking Spaces for Private Cars	107
No. of Parking Spaces for LGV	8
Loading/ Unloading Space for LGV	2

2.9.3 Following the previously approved applications, a setback of 6 m from the road kerb side of Lin Ma Hang Road has been reserved for the future widening works of the eastern section of Lin Ma Hang Road. **These areas shall not form part of the application site**, hence no interfacing issue is anticipated from this Development.

2.9.4 The indicative layout plan of the Development is shown in **Annex 1**.

## 2.10 Operation Arrangement

2.10.1 The car park will operate 24 hours a day. Daily, weekly and monthly rental basis will be allowed for local residents and visitors. A longer operation hour and a varied charging scheme will provide greater flexibility to the cross-boundary travellers, so as to relieve the pressure from the existing Heung Yuen Wai Car Park.



intentions of the “REC” and “V” zones.<sup>5</sup>

- 3.2.3 The building form and building bulk follows the approved scheme. The only difference is the increase of car parking spaces as well as site area. A table summarizing the differences between Applications No. A/NE-TKLN/57 and 58 and the current applicant is as follows:

<u>Parameters</u>	<u>Application No.</u> <u>A/NE-TKLN/57</u>	<u>Application No.</u> <u>A/NE-TKLN/58</u>	<u>Current</u> <u>Application</u>	<u>Difference</u> <u>between</u> <u>current and</u> <u>two approved</u> <u>applications</u>
Site Area	About 1,924 sq.m.	About 2,228 sq.m.	About 4,628 sq.m.	+ 476 sq.m.
Floor Area	About 204 sq.m.	About 180 sq.m.	About 396 sq.m.	+ 12 sq.m.
No. of Structure(s)	1	1	2	Same
Max. Height of Structures	6 meters	6 meters	6 meters	Same
No. of Private Car Parking Spaces	24	26	107	+ 57
No. of LGV Parking Spaces	0	0	8	+ 8
No. of Loading/ Unloading Bay for Light Goods Vehicle	1	1	2	Same

#### 4.1 Similar Approved Applications for Public Vehicle Parks

- 4.1.1 There are several similar applications for temporary vehicle park use in the vicinity of the Application Site, which falls within “REC” zone and “V” zone.
- 4.1.2 There is a similar approved application of similar uses within the same “REC” zone and “V” zone in Heung Yuen Wai area, involving different sites. The adjacent application No. A/NE-TKLN/53 for temporary public vehicle park for private cars and LGV was approved on 2023, with its access point also abuts Lin Ma Hang Road.
- 4.1.3 Planning approval for the adjacent site (No. A/NE-TKLN/53) is going to be expired in mid-2026, with another approved site (No. A/NE-TKLN/70) that will be lapsed

<sup>5</sup> [https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt\\_e.pdf](https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt_e.pdf)

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy ☐Confidential

**Timothy Wai Pui WU/PLAND**

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寄件者: Cherie Lee <[REDACTED]>  
寄件日期: 2025年10月27日星期一 15:31  
收件者: Timothy Wai Pui WU/PLAND  
副本: Junior; Brian Ching Hong CHAN/PLAND  
主旨: FW: Response to Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: 20251027\_A\_NE-TKLN\_102\_Combined FI.pdf  
  
類別: Internet Email

Dear Timothy and Brian,

Attached please find our submission.

Thank you.

Best regards,  
Cherie

LCH (Asia-Pacific) Surveyors Limited | LCH Planning & Development Consultants Limited

[REDACTED]

[REDACTED]

[REDACTED]

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This e-mail and attachments (if any) is intended only for the addressee(s) and is subject to copyright. This e-mail contains information which may be confidential or privileged. If you are not the addressee, you may not read, copy, forward, disclose or use any part of the message or its attachments and if you have received this message in error, please notify the sender immediately by return e-mail and delete it from your system. Unless specifically stated, this e-mail does not constitute formal advice or commitment by the sender or LCH (Asia-Pacific) Surveyors Ltd. or any of its subsidiaries.  
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**LCH Planning and Development  
Consultants Limited**

Our Ref.: PD2504003/09

Your Ref.:

27 October 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION  
UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

We refer to the emails enclosing the departmental comments regarding the captioned application.

Please find attached our responses to departmental comments, revised drainage proposal and replacement pages of Application Form and Planning Statement.

This Further Information contains the responses to comments of relevant Government departments and reduction on number of parking spaces. Thus, according to TPB Planning Guideline No. 32B, this Further Information does not result in a material change of the nature of the application and should be accepted by the TPB for inclusion into the application.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee at [REDACTED].

Yours faithfully,

For and on behalf of

**LCH Planning & Development Consultants Limited**

**Junior Ho**

Director

Encl.

c.c. the Applicant

- Annex 1 – Response to Comments Table
- Attachment 1 – Revised Drainage Proposal
- Attachment 2 – Revised Layout Plan
- Attachment 3 – Revised Swept Path Analysis
- Attachment 4 – Replacement Pages of Application Form
- Attachment 5 – Replacement Pages of Planning Statement





**Annex 1**

**Response to Comments Table**



**Section 16 Application No. A/NE-TKLN/102**

**Response to Comments Table**

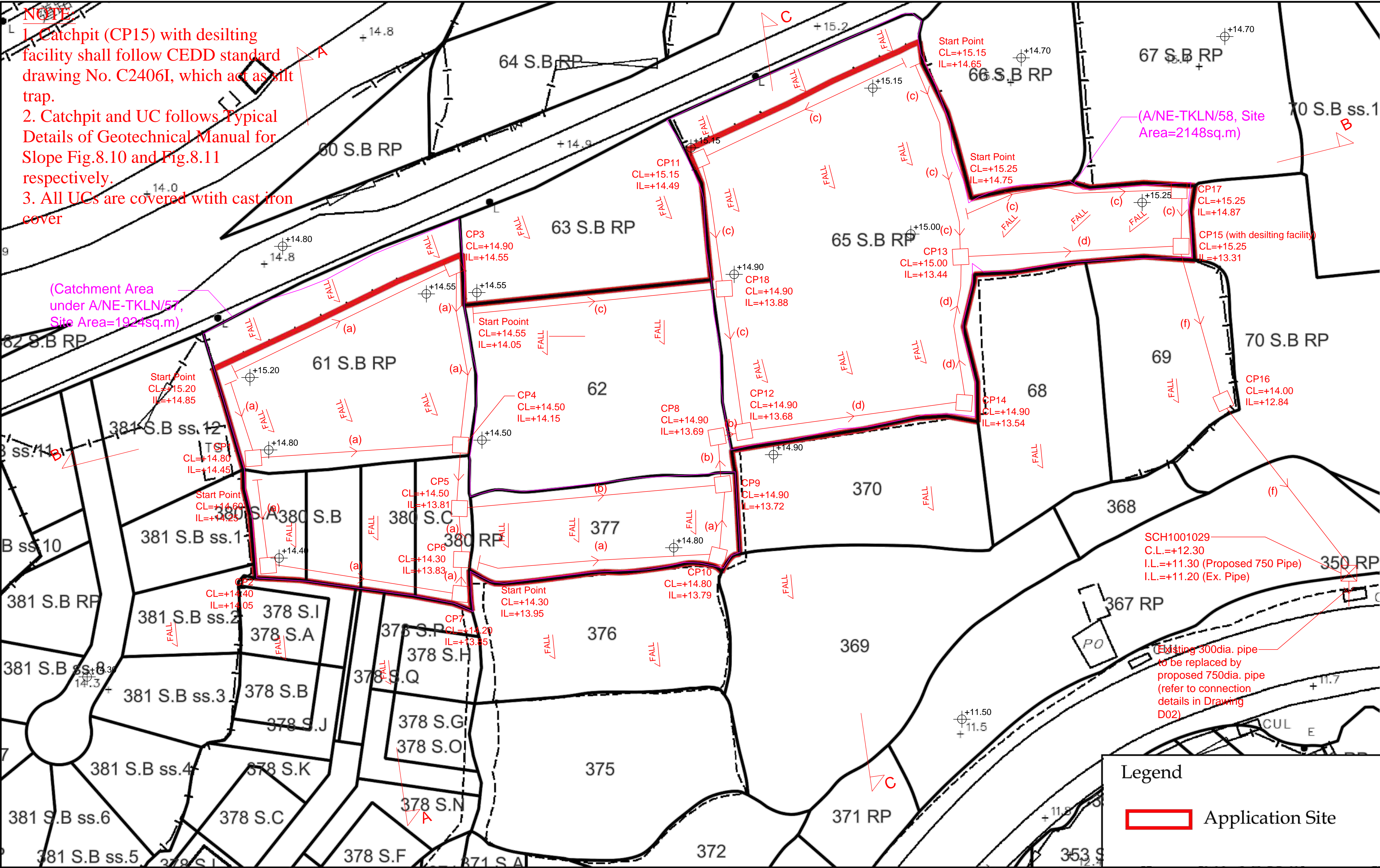
No.	Comments Received	Our Responses
<b>1.</b>	<b><i>Comments from Drainage Services Department received on 25 September 2025</i></b>	
a	Drawing nos. D01 and D02: The applicant should clarify if the diameter of the pipe downstream to the existing catch pit SCH1001029 will be upgraded to 675mm or 750mm, and update the drawings accordingly.	The diameter of the pipe downstream to the existing catchpit SCH1001029 shall be upgraded to 750mm, the drawings are revised accordingly ( <b>Attachment 1</b> ).
b	Unless the applicant could submit a revised drainage proposal to my satisfaction, I do not support the application from drainage perspective.	Noted. A revised drainage proposal is at <b>Attachment 1</b> .
<b>2.</b>	<b><i>Comments from Transport Department received on 25 September 2025</i></b>	
c	Our previous comment "The applicant shall justify and demonstrate the use of 600 veh/hr is adequate for the design flow capacity of Lin Ma Hang Road, including a journey time delay analysis study should be conducted to demonstrate the journey time in connection with different v/c ratios." has not been addressed.	<p>Taking into account the current traffic situation, we have further updated the car parking spaces in the latest proposal. The Proposed Development is now reduced to have 50 nos. of car parking spaces only, which is the same as the total car parking space numbers in the previously approved application nos. A/NE-TKLN/57 and A/NE-TKLN/58. Revised layout plan is at <b>Attachment 2</b>.</p> <p>It is anticipated that the traffic impact brought by this Proposed Development will be similar to the previously approved applications and there will be no material change in the traffic impact to the surrounding road network. Therefore, a journey time delay analysis is unnecessary in this case.</p>
d	Our previous comment "The applicant shall advise the management / control measures to be implemented to ensure no queueing of vehicles outside the subject site" has not been addressed. The applicant shall further supplement and propose additional measures to prevent queueing of traffic e.g. provision of parking information to the public." has not been addressed. It would be considered too late to inform the public that the carpark is full outside the subject site. The applicant should provide	<p>Well noted. The operator will provide real-time parking information to the Transport Department, allowing the public to access up-to-date parking vacancy information on the HKeMobility.</p> <p>The operator will also provide such real time information of the vacant car parking space number on an online platform</p>



	<p>the parking information well in advance and make the public aware the real time information of the parking vacancy.</p>	<p>to inform the public (i.e. parking space availability updates will be provided hourly and parking space availability forecast will be provided every two hours) so to avoid vehicle entering the proposed car park when it is expected to be fully occupied. Besides, the operator will design an online booking system for the car parking spaces for the concerned car park to avoid congestion.</p> <p>Besides, the setback areas of the Application Site will serve as passing place for drivers to safely stop in and wait, allowing oncoming vehicle from the opposite direction to pass without obstruction or congestion. This ensures that drivers have a clear and secure location to pause, reducing the risk of collisions and improving overall accessibility within the site. Such measure will also have concrete benefit to smooth the traffic flow on the existing Lin Ma Hang Road.</p> <p>It is illustrated in the revised swept path analysis in <b>Attachment 3</b>.</p>
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**Attachment 1**  
**Revised Drainage Proposal**





**CH Planning and Development Consultants Limited**

and Status

only. Not to scale.)

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)

LEGEND	
(a)	Proposed 300UC (1:200) with Cast Iron cover
(b)/(c)	Proposed 450UC (1:200) with Cast Iron cover
(d)	Proposed 525UC (1:200) with Cast Iron cover
(e)	Proposed 675UC (1:200) with Cast Iron cover
(f)	Proposed 750 underground concrete pipe (1:200)
☒	Existing Catchpit
□	Proposed Catchpit
⊕	Existing Level

Company:

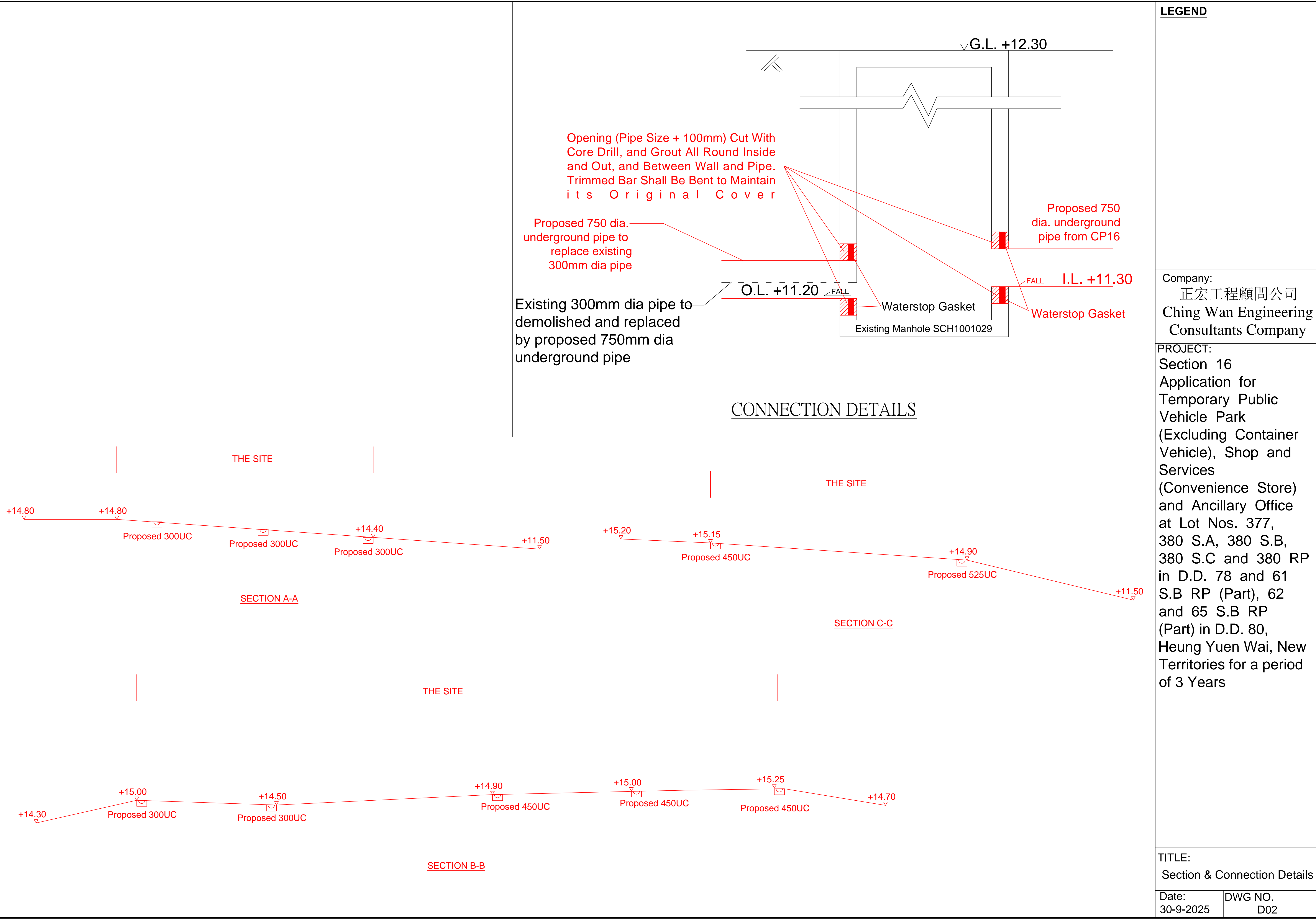
正宏工程顧問公司  
Ching Wan Engineering Consultants Company

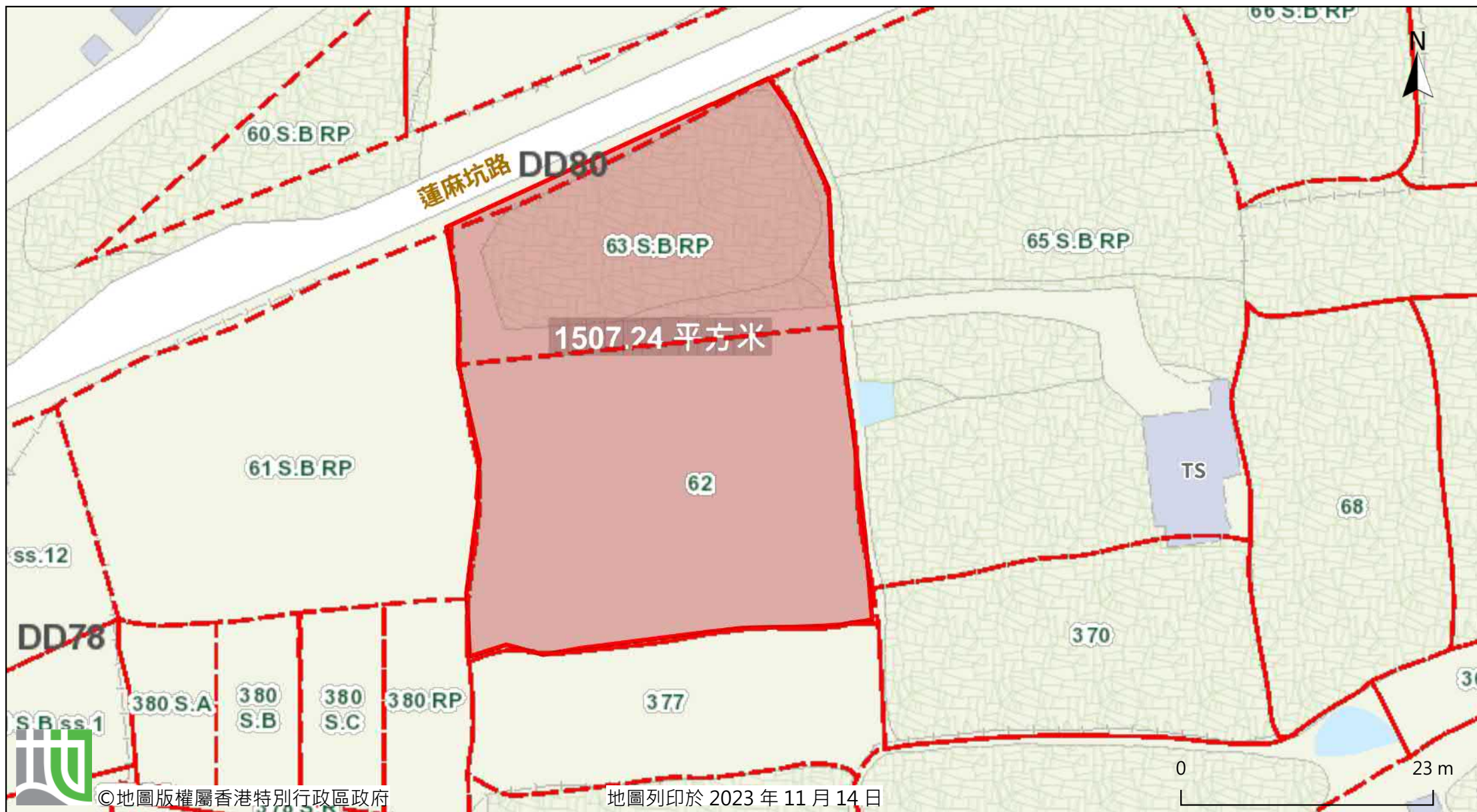
PROJECT:

Section 16  
Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a period of 3 Years

TITLE:	
Drainage Proposal	
Date: 30-9-2025	DWG NO. D01



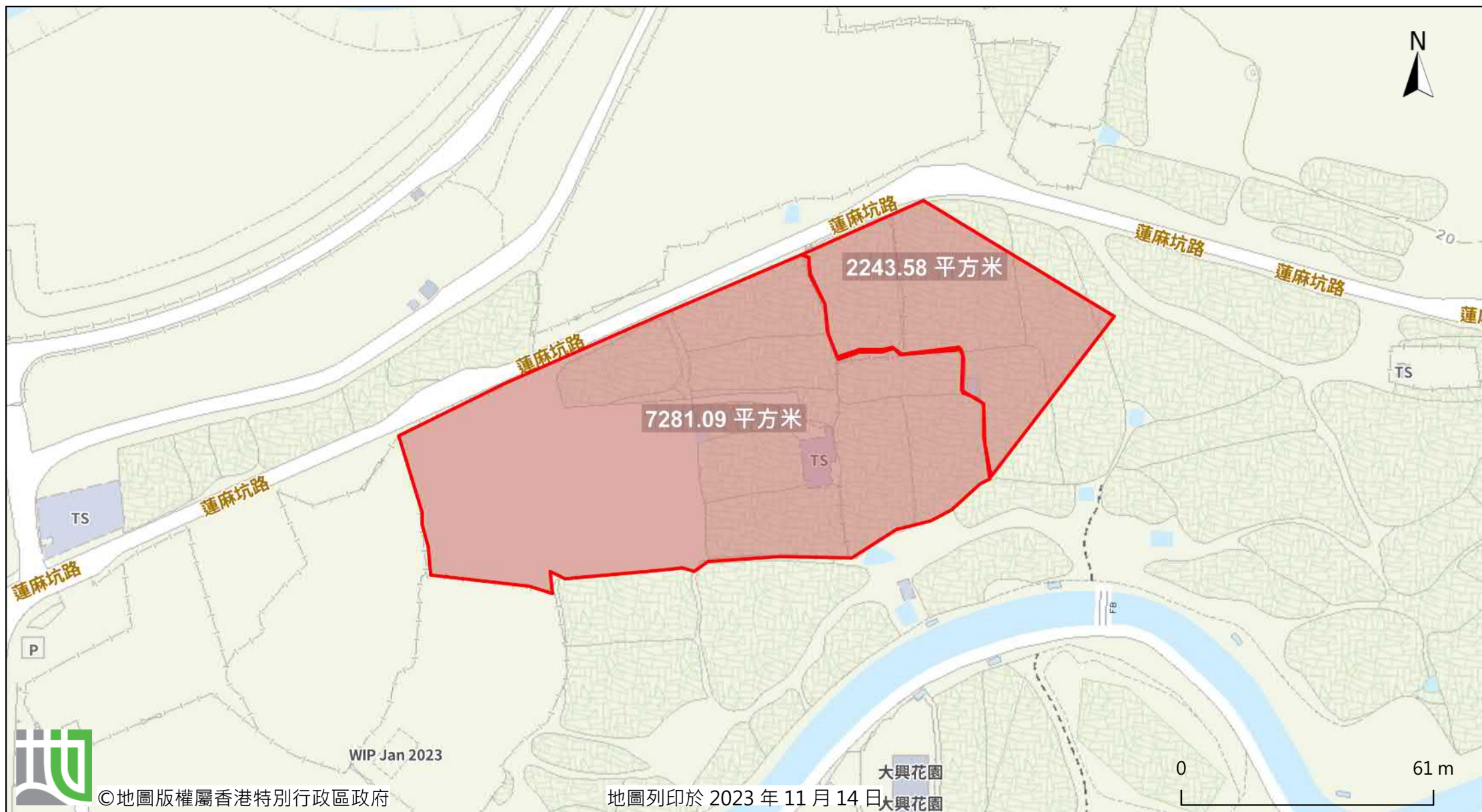




由「地理資訊地圖」網站提供: <https://www.map.gov.hk>

注意: 使用此地圖受「地理資訊地圖」的使用條款及條件以及知識產權告示約束。





**Catchment Area under A/NE-TKLN/57**

Site Area = 962 m<sup>2</sup> (1924/2)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 962 \quad \text{m}^2$$
$$= 0.000962 \quad \text{km}^2$$

$$t = 0.14465 L / H^{0.2} A^{0.1}$$
$$= 0.14465 * 10 / 1^{0.2} * 962^{0.1}$$
$$= 0.728 \quad \text{min}$$

$$i = 1.111 * a / (t + b)^c \quad (50 \text{ yrs return period, Table 3d, Corrigendum 2024, SDM and (11.1\% increase due to climate change)})$$
$$= 1.111 * 474.6 / (0.728 + 2.60)^{0.371}$$
$$= 326.9 \quad \text{mm/hr}$$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001924$

$$= 0.083 \quad \text{m}^3/\text{sec}$$
$$= 4983 \quad \text{lit/min}$$

**Provide 300UC (1:200) is OK**

**Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP**

Lot 62 & 63BRP = 1507 m<sup>2</sup> (C=0.95)

$$Q = 0.278 C i A$$

$$C = 0.95 \quad (\text{P.42 of Stormwater Drainage Manual})$$

$$A = 1507 \quad \text{m}^2$$
$$= 0.001507 \quad \text{km}^2$$

take  $i = 326.9 \quad \text{mm/hr}$

Therefore,  $Q = 0.278 * 0.95 * 326.9 * 0.001507$

$$= 0.130 \quad \text{m}^3/\text{sec}$$
$$= 7806 \quad \text{lit/min}$$

For Catchment Area under A/NE-TKLN/57+Lot 62 & 63BRP

$$Q = 4983 + 7806$$
$$= 12790 \quad \text{lit/min}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/58+Outside Catchment Area**

Site Area = 2148 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278*0.95*326.9*0.002148+0.278*0.25*326.9*0.002244 \\ &= 0.236 \text{ m}^3/\text{sec} \\ &= 14186 \text{ lit/min}\end{aligned}$$

**Provide 450UC (1:200) is OK**

**A/NE-TKLN/57+Lot 62 & 63BRP+A/NE-TKLN/58+Outside Catchment Area**

$$Q = 12790 + 14186$$

$$= 26975 \text{ lit/min}$$

**Provide 525UC (1:200) is OK**

**Outfall (all catchment area)**

Site Area = 7281 m<sup>2</sup> (C=0.95)

Outside Catchment Area = 2244 m<sup>2</sup> (C=0.25)

Calculation of Runoff from the Proposed Development,

$$Q = 0.278 C i A$$

$$\text{take } i = 326.9 \text{ mm/hr}$$

$$\begin{aligned}\text{Therefore, } Q &= 0.278*0.95*326.9*0.007281+0.278*0.25*326.9*0.002244 \\ &= 0.680 \text{ m}^3/\text{sec} \\ &= 40775 \text{ lit/min}\end{aligned}$$



Check 750mm dia. Pipes by Colebrook-White Equation

$$V = -\sqrt{(8gDs)} \log\left(\frac{ks}{3.7D} + \frac{2.51v}{D\sqrt{(2gDs)}}\right)$$

where :

V	=		mean velocity (m/s)	
g	=	9.81	m/s <sup>2</sup> gravitational acceleration (m/s <sup>2</sup> )	
D	=	0.75	m internal pipe diameter (m)	
ks	=	0.00015	m hydraulic pipeline roughness (m)	(Table14, from DSD SDM 2018, concrete pipe)
v	=	1.14E-06	m <sup>2</sup> /s kinematic viscosity of fluid (m <sup>2</sup> /s)	
s	=	0.005	hydraulic gradient	

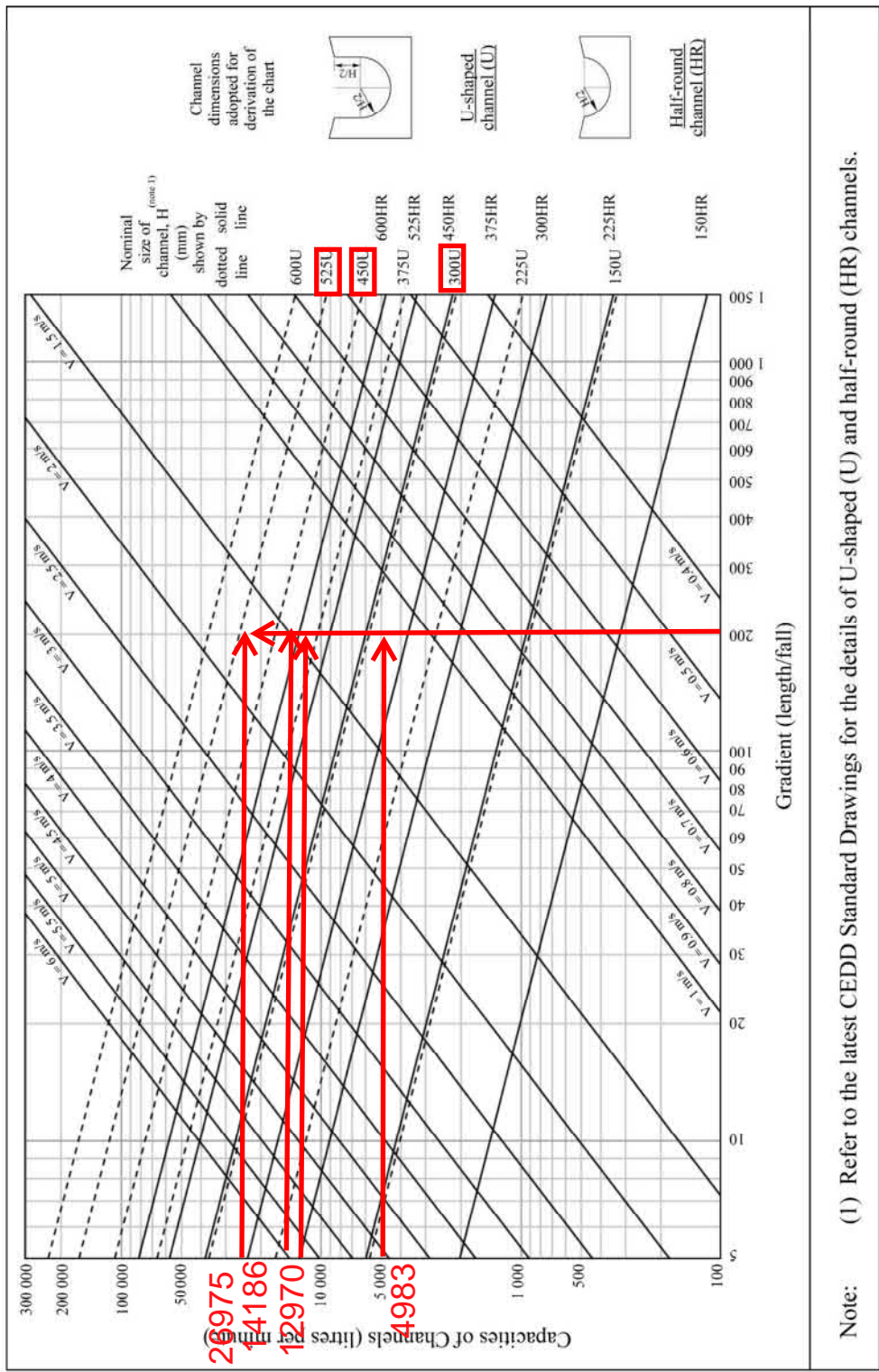
Therefore, design V of pipe capacity = 2.2604 m/s

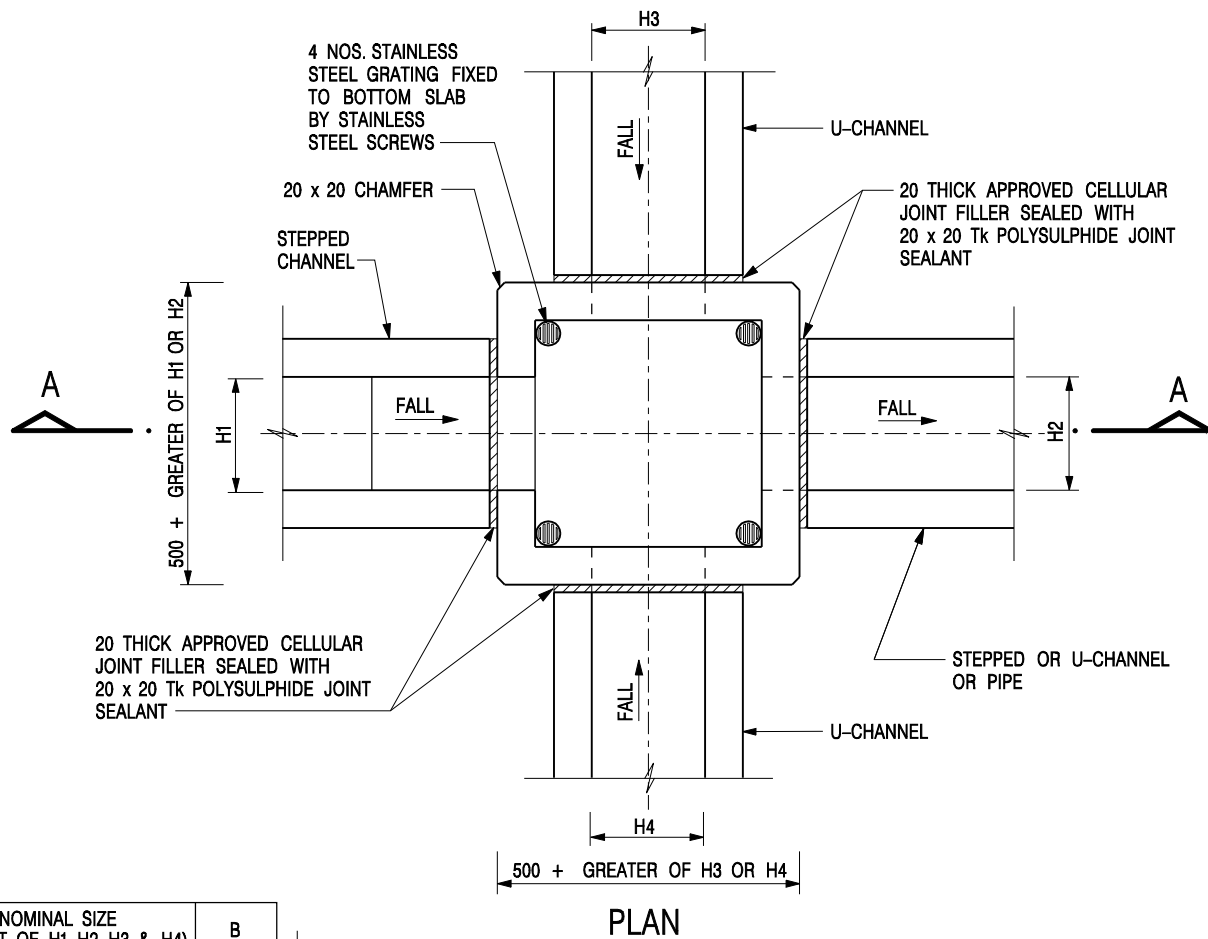
Q= 0.8VA		(0.8 factor for sedimentation)
= 0.799	m <sup>3</sup> /s	
= 47934	lit/min	
> 40775	lit/min	Ok

**GEO Technical Guidance Note No. 43 (TGN 43)**  
**Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes**

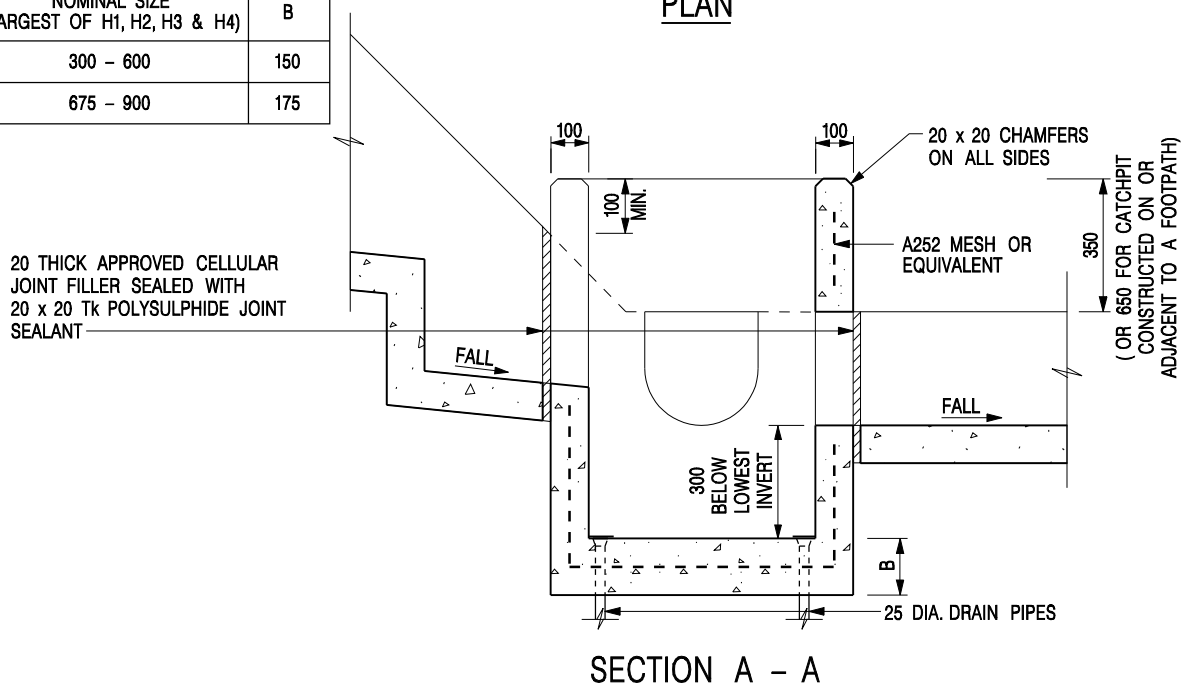
Issue No.: 1      Revision: -      Date: 05.06.2014      Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm






NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

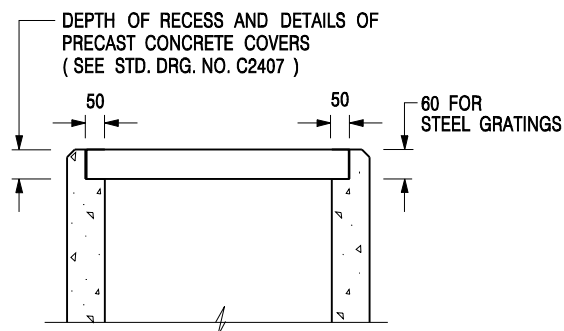


**NOTES:**

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

**CATCHPIT WITH TRAP**  
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 <b>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</b>		<b>SCALE</b> 1 : 20	
		<b>DRAWING NO.</b> C2406 /1	
<b>DATE</b> JAN 1991			



### ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

#### NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS ( SEE DETAIL 'A' ON STD. DRG. NO. C2405 ) OR CONCRETE COVERS ( SEE STD. DRG. NO. C2407 ) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING ( SEE DETAIL 'G' ON STD. DRG. NO. C2405; EXCEPT ON THE UPSLOPE SIDE ) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS ( SEE DSD STD. DRG. NO. DS1043 ) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'F' ON STD. DRG. NO. C2405.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP**  
**(SHEET 2 OF 2)**



**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

**DRAWING NO.**

**C2406 /2**

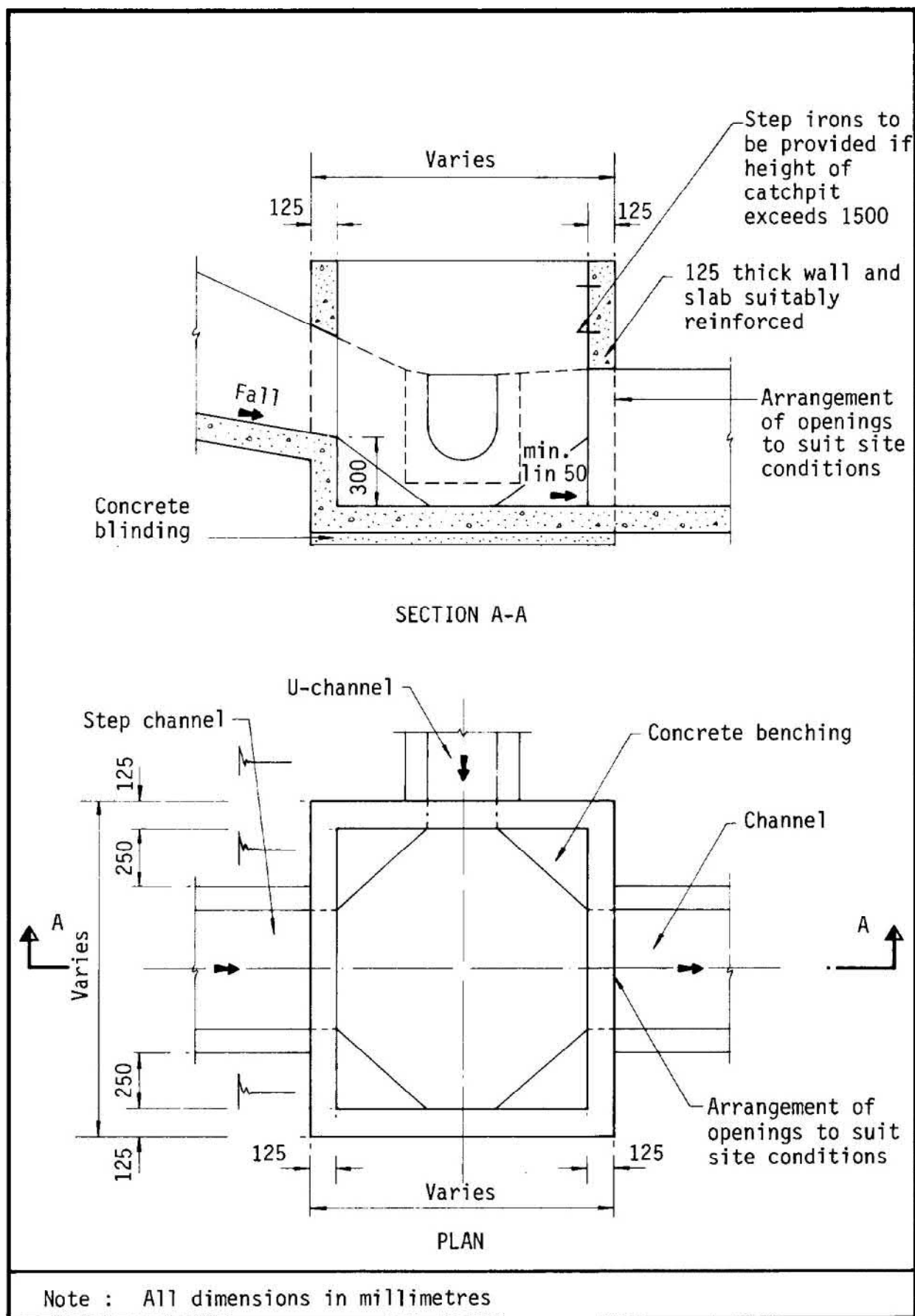
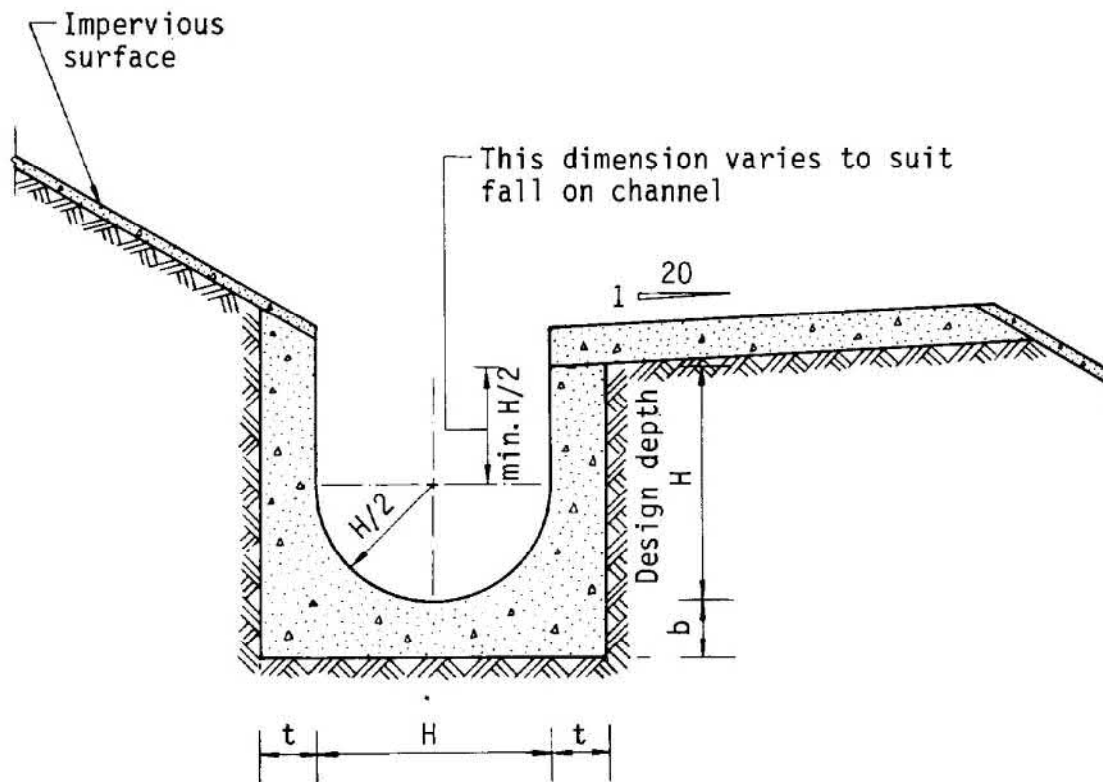


Figure 8.10 - Typical Details of Catchpits

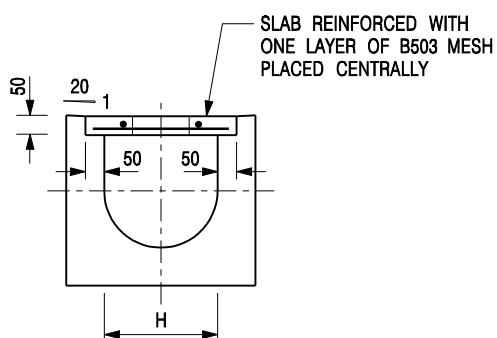




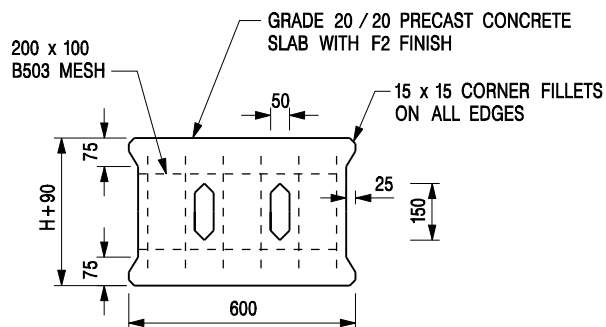
Dimensions of U - channel

Nominal size of channel H (mm)	Thickness t (mm)	Thickness b (mm)
225 to 600	150	150
675 to 1200	175	225

Figure 8.11 - Typical U-channel Details



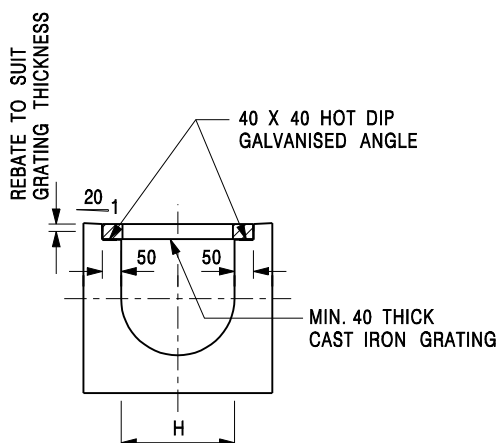
TYPICAL SECTION



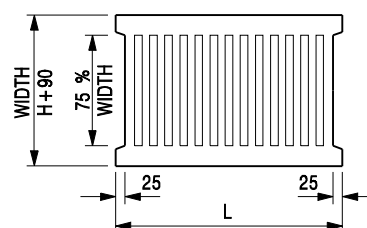
PLAN OF SLAB

### U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION



L = 600mm FOR H ≤ 375mm  
L = 400mm FOR H > 375mm

CAST IRON GRATING

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

### U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

#### NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

COVER SLAB AND CAST IRON  
GRATING FOR CHANNELS



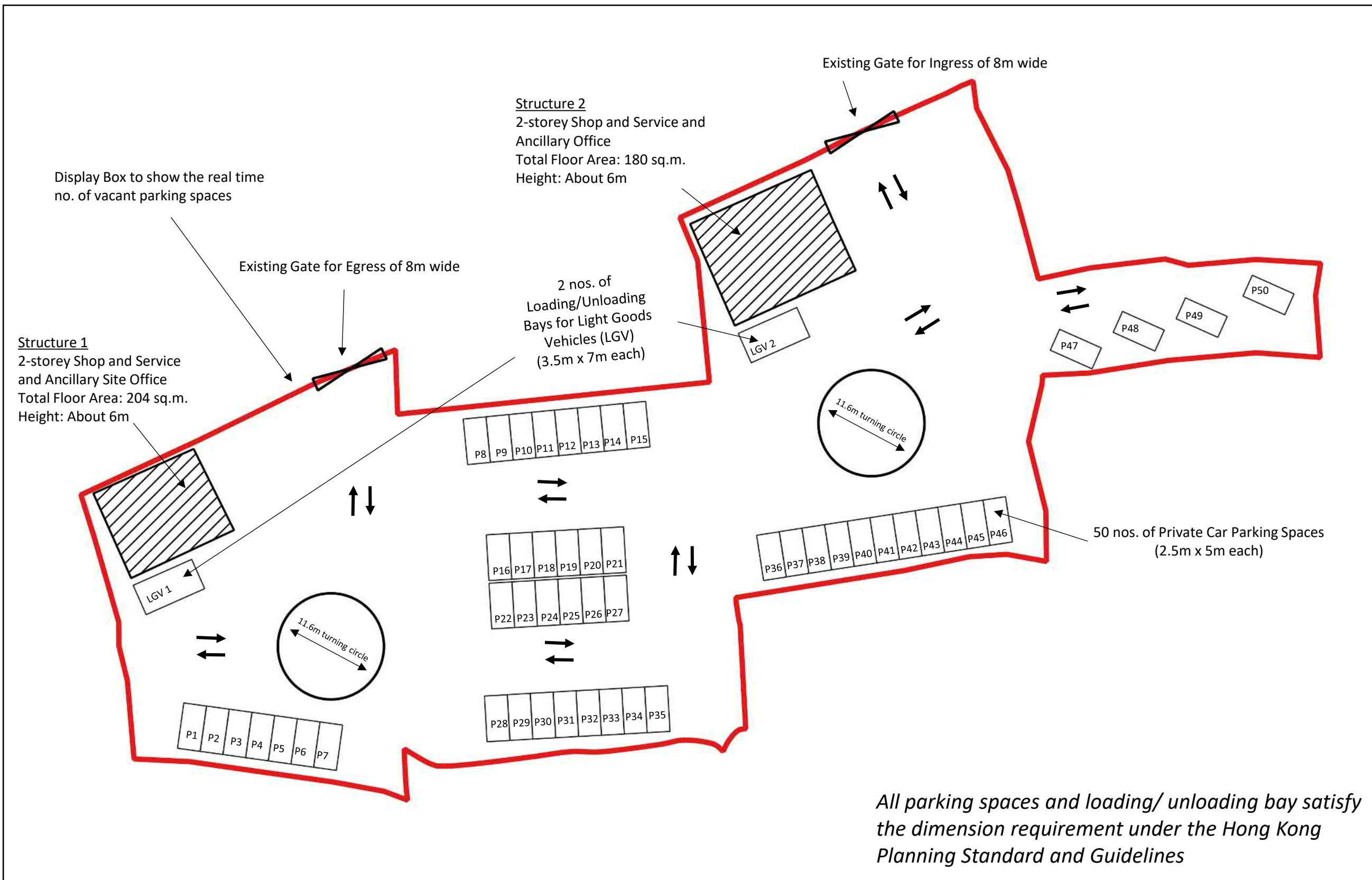
**CIVIL ENGINEERING AND  
DEVELOPMENT DEPARTMENT**

**SCALE** 1 : 20

**DATE** JAN 1991

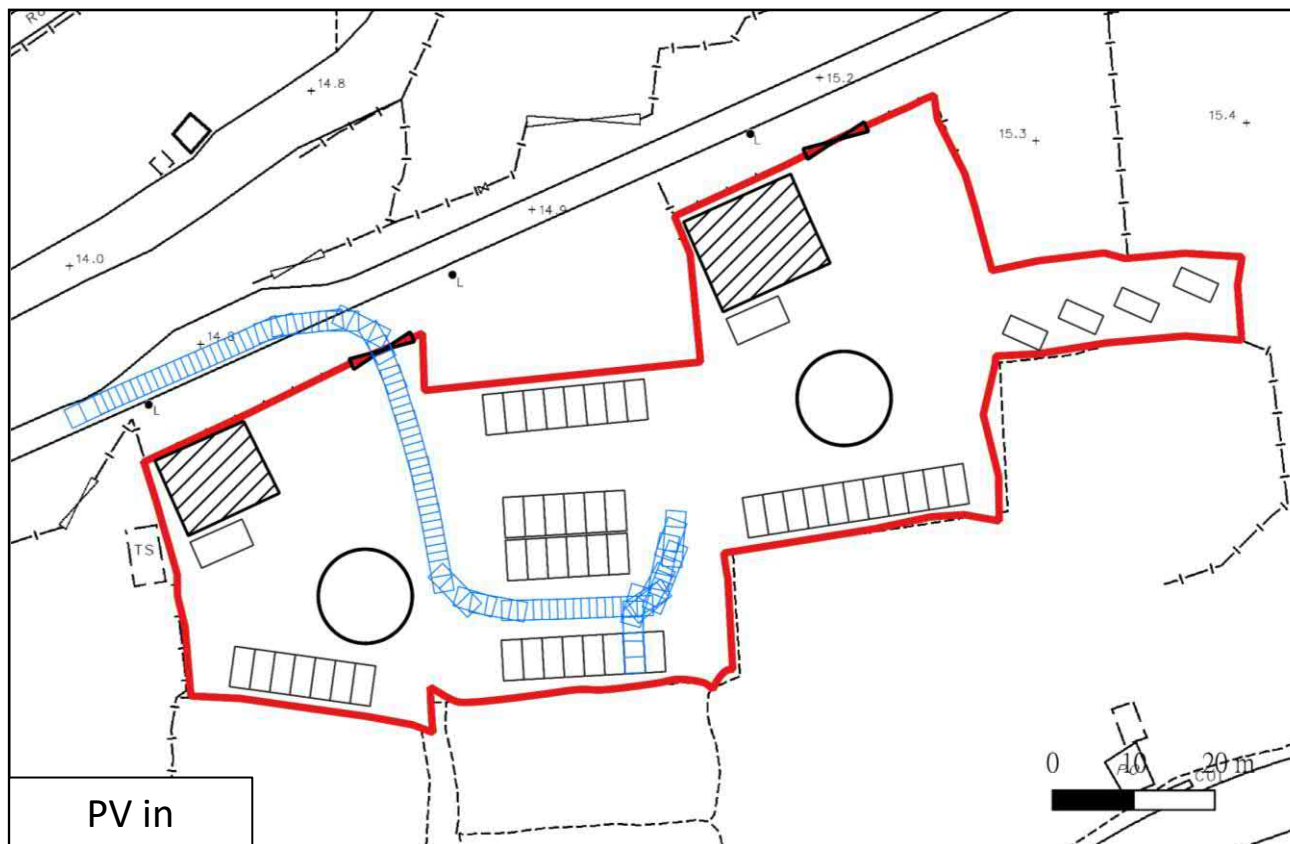
**DRAWING NO.**  
**C2412E**

**Attachment 2**  
**Revised Layout Plan**

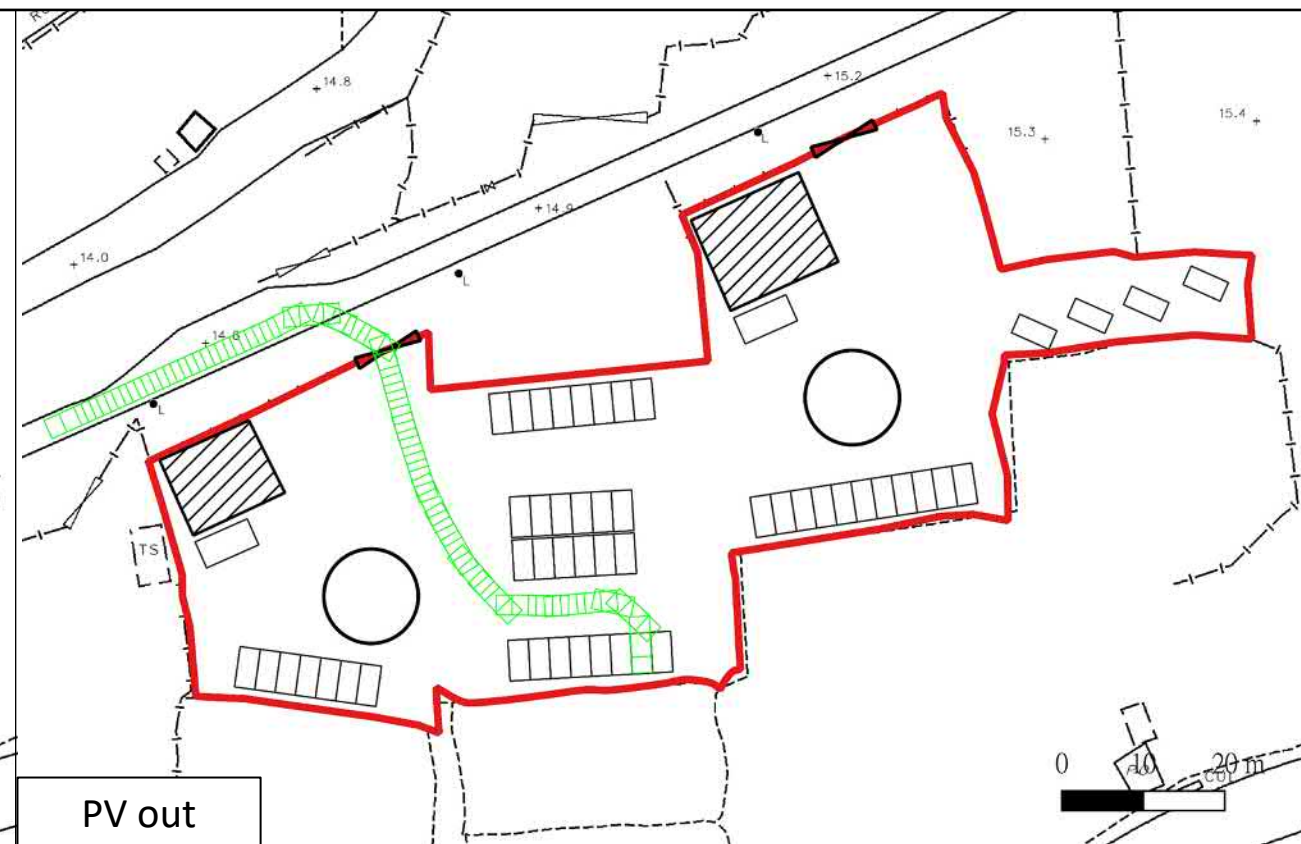


**Attachment 3**  
**Revised Swept Path Analysis**





PV in



PV out



LGV in



LGV out



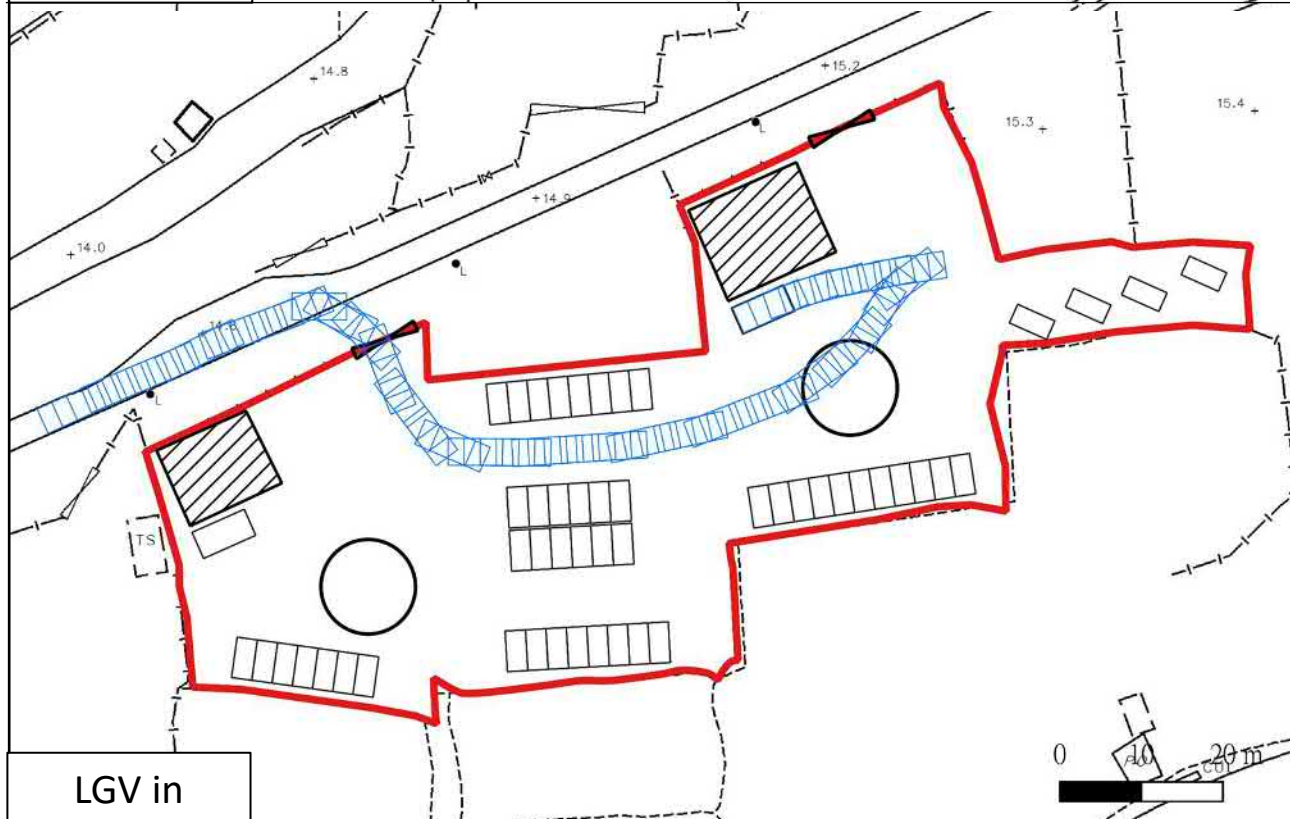
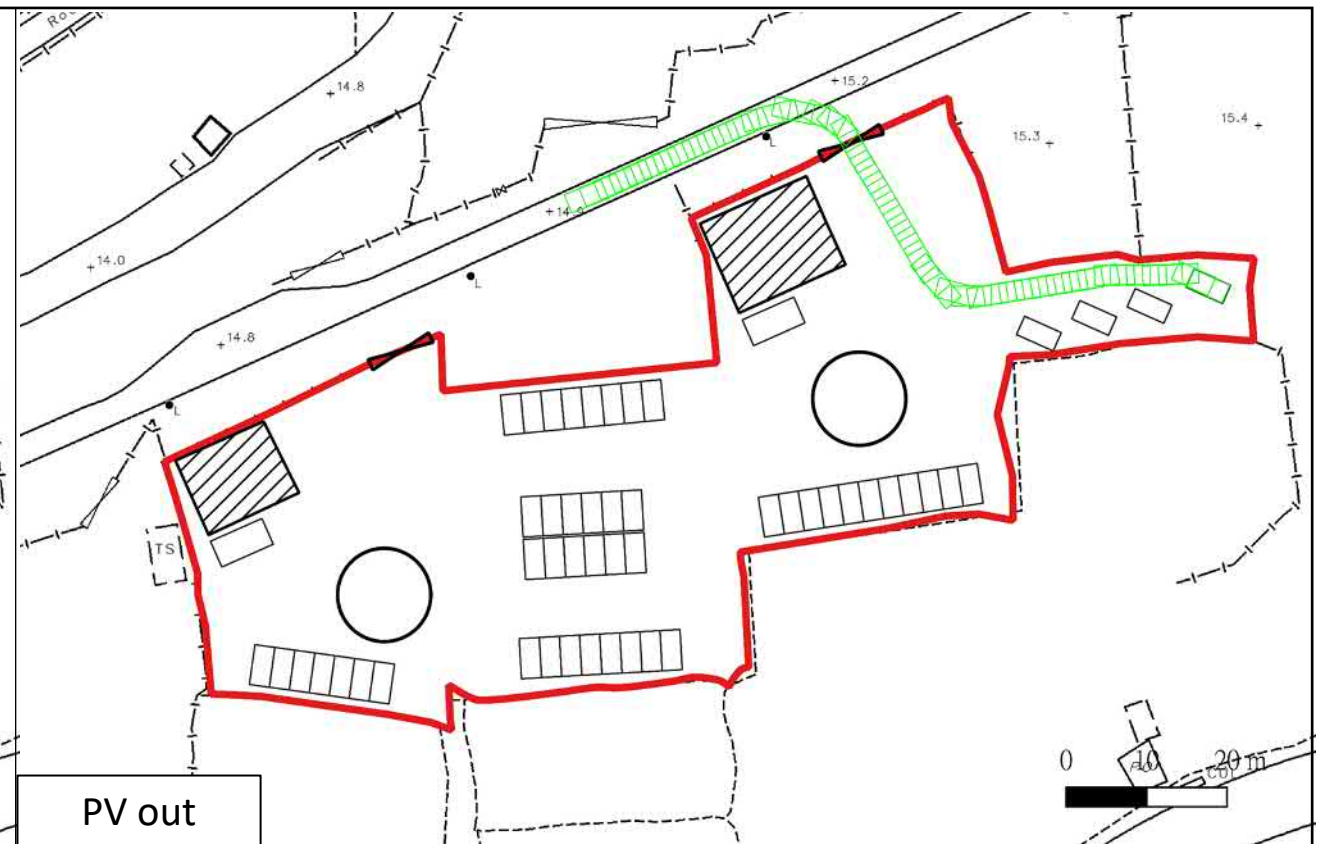
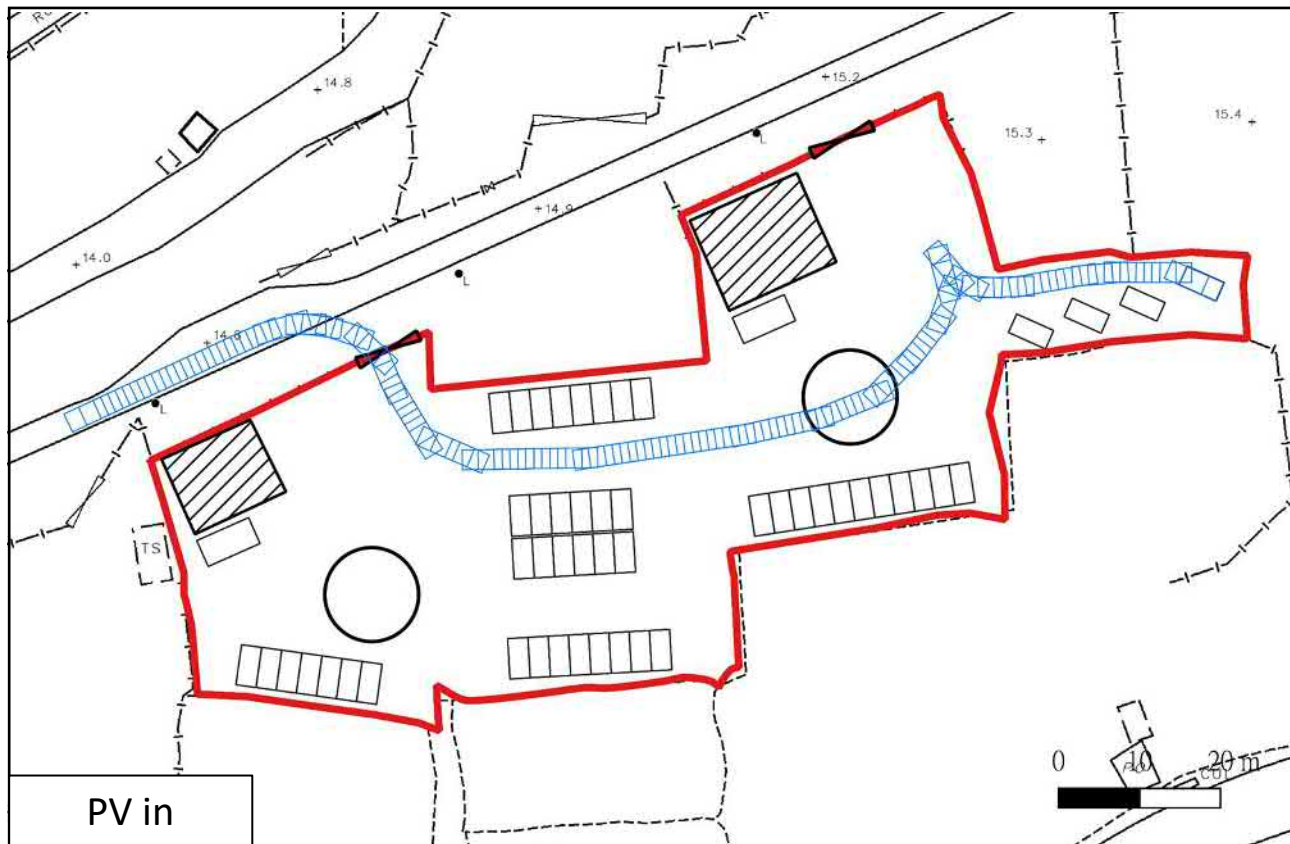
**LCH Planning and Development  
Consultants Limited**

Annex 3 : Swept Path Analysis

Section 16 Application for Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lot Nos. 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Heung Yuen Wai, New Territories for a Period of 3 Years

(Source: HK GEODATA STORE, HKSAR Government)









## **Attachment 4**

# **Replacement Pages of Application Form**

<b>6. Type(s) of Application 申請類別</b>	
<b>(A) Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas</b> 位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展 <b>(For Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas, please proceed to Part (B))</b> (如屬位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期，請填寫(B)部分)	
(a) Proposed use(s)/development 擬議用途/發展	Temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Services (Convenience Store) and Ancillary Office  (Please illustrate the details of the proposal on a layout plan) (請用平面圖說明擬議詳情)
(b) Effective period of permission applied for 申請的許可有效期	<input checked="" type="checkbox"/> year(s) 年 ..... <b>Three</b> ..... <input type="checkbox"/> month(s) 個月 .....
<b>(c) Development Schedule 發展細節表</b>	
Proposed uncovered land area 擬議露天土地面積	..... <b>4436</b> .....sq.m <input checked="" type="checkbox"/> About 約
Proposed covered land area 擬議有上蓋土地面積	..... <b>192</b> .....sq.m <input checked="" type="checkbox"/> About 約
Proposed number of buildings/structures 擬議建築物／構築物數目	..... <b>2</b> .....
Proposed domestic floor area 擬議住用樓面面積	.....sq.m <input type="checkbox"/> About 約
Proposed non-domestic floor area 擬議非住用樓面面積	..... <b>384</b> .....sq.m <input checked="" type="checkbox"/> About 約
Proposed gross floor area 擬議總樓面面積	..... <b>384</b> .....sq.m <input checked="" type="checkbox"/> About 約
Proposed height and use(s) of different floors of buildings/structures (if applicable) 建築物/構築物的擬議高度及不同樓層的擬議用途 (如適用) (Please use separate sheets if the space below is insufficient) (如以下空間不足，請另頁說明) Please refer to the planning statement ..... ..... .....	
Proposed number of car parking spaces by types 不同種類停車位的擬議數目	
Private Car Parking Spaces 私家車車位	..... <b>50</b> .....
Motorcycle Parking Spaces 電單車車位	.....
Light Goods Vehicle Parking Spaces 輕型貨車泊車位	.....
Medium Goods Vehicle Parking Spaces 中型貨車泊車位	.....
Heavy Goods Vehicle Parking Spaces 重型貨車泊車位	.....
Others (Please Specify) 其他 (請列明)	.....
Proposed number of loading/unloading spaces 上落客貨車位的擬議數目	
Taxi Spaces 的士車位	.....
Coach Spaces 旅遊巴車位	.....
Light Goods Vehicle Spaces 輕型貨車車位	..... <b>2</b> .....
Medium Goods Vehicle Spaces 中型貨車車位	.....
Heavy Goods Vehicle Spaces 重型貨車車位	.....
Others (Please Specify) 其他 (請列明)	.....



(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	384 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.083 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用		
	Non-domestic 非住用	2	
(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	m 米 <input type="checkbox"/> (Not more than 不多於)	
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於)	
	Non-domestic 非住用	6 m 米 <input checked="" type="checkbox"/> (Not more than 不多於)	
		2 Storeys(s) 層 <input checked="" type="checkbox"/> (Not more than 不多於)	
(iv) Site coverage 上蓋面積	4.1 % <input checked="" type="checkbox"/> About 約		
(v) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數		50
	Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) _____ _____		50
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數		2
	Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) _____ _____		2

## **Attachment 5**

# **Replacement Pages of Planning Statement**



## **Executive Summary**

*(in case of discrepancy between English and Chinese versions, English version shall prevail)*

This Application is submitted to the Town Planning Board (“**the Board**”) under Section 16 of the Town Planning Ordinance (“**the Ordinance**”) for a proposed temporary Public Vehicle Park (Excluding Container Vehicle), Shop and Service (convenience store) and Ancillary Office for a period of 3 years (“**the Proposed Development**”) at Lot Nos. 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78, and 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80 in Heung Yuen Wai, New Territories (“**the Application Site**”).

The Application Site falls within an area of “Recreation” (“**REC**”) zone and “Village Type Development” (“**V**”) zone on the Approved Ta Kwu Ling North Outline Zoning Plan No. S/NE-TKLN/2 (“**the OZP**”). Upon completion of the Proposed Development, it comprises 50 parking spaces for private cars (at least 2.5m x 5m each), as well as two two- storey structures for convenience store and ancillary office uses. The proposed building height of 6 m and the total floor area is about 384 sq.m. There will be two various loading and unloading bays for Light Goods Vehicle (LGV) (at least 3.5m x 7m each) to support the Proposed Development.

The Application echoes government’s direction to provide more parking spaces to serve the Heung Yuen Wai Boundary Control Point, which aims to accommodate the high demand of the local villagers and the cross-boundary travellers for car parking spaces. It is situated at a convenient location that is within 5 minutes walking distance from the Boundary Control Point.

The Proposed Development does not hinder the long-term planning intention of “REC” and “V” zone. Similar applications in the “REC” and “V” zone in the Ta Kwu Ling North area have also been approved and considered. The Proposed Development will not cause adverse traffic, visual, landscape, drainage and environmental impacts.

In view of the above justifications, we would sincerely seek for favourable consideration of the Board to approve this Application.



## 內容摘要

(如內文與其英文版本有差異，則以英文版本為準)

本規劃申請根據《城市規劃條例》第16條，就新界香園圍丈量約份第78約地段第377號、第380號A分段、第380號B分段、第380號C分段及第380號餘段及丈量約份第80約地段第61號B分段餘段（部分）、第62號，以及第65號B分段餘段（部分）（下稱「**申請地點**」）的用地，向城市規劃委員會（下稱「**城規會**」）申請作擬議臨時公眾停車場（貨櫃車除外）及臨時商店及服務行業（便利店）連附屬辦公室用途，為期3年（下稱「**擬議發展**」）。

申請地點現時於《打鼓嶺北分區計劃大綱圖編號S/NE-TKLN/2》（下稱「**大綱圖**」）劃作「康樂」和「鄉村式發展」地帶。擬議發展包括50個私家車專用停車位（每個最少2.5米x5米），以及兩個兩層高的構築物作為臨時商店及服務行業和附屬辦公室用途。擬議構築物的高度約6米，總樓面面積約為384平方米。擬議發展將會有兩個輕型貨車的上落客貨車位（每個最少3.5米x7米）。

申請響應政府提供更多泊車位服務香園圍口岸的方向，旨在滿足當地村民和過境旅客對泊車位的殷切需求。申請地點坐落在一個方便的位置，從蓮塘/香園圍口岸步行5分鐘即可到達。

擬議發展不會妨礙打鼓嶺北「康樂」和「鄉村式發展」地帶的長遠規劃意向。而且於打鼓嶺北的「康樂」和「鄉村式發展」地帶亦已有不少相關申請曾被批准。擬議發展不會對交通、視覺、景觀、排水和環境造成不利影響。

鑒於以上提出的依據，我們真誠地尋求城規會批准該申請。



## 2.9 Temporary Public Vehicle Park for Private Cars, Shop and Services and Ancillary Office

2.9.1 The current Application Site covers an area of about 4,628 sq.m. The Proposed Development will provide a total of 50 parking spaces for private cars (at least 2.5m x 5m each) within the Application Site.

2.9.2 Following the previously approved applications No. A/NE-TKLN/57 and 58, there will be two two-storey structures as temporary shop and services (convenience store) and ancillary office, each situated at the north-western portion and north-eastern portion of the Application Site. Together, with a total floor area of 384 sq.m. and a height of about 6 meters. There will be two loading & unloading bays at the north-western portion and north-eastern portion of the Application Site to serve the commercial use.

Parameters	Proposed Development
Site Area	About 4,628 sq.m.
No. of Structure	2
Covered Area	About 192 sq.m.
Maximum Height of Structure	About 6 metres, two-storey
Maximum Total Site Coverage	About 4.1%
Maximum Plot Ratio	About 0.083
No. of Parking Spaces for Private Cars	50
Loading/ Unloading Space for LGV	2

2.9.3 Following the previously approved applications, a setback of 6 m from the road kerb side of Lin Ma Hang Road has been reserved for the future widening works of the eastern section of Lin Ma Hang Road. **These areas shall not form part of the application site**, hence no interfacing issue is anticipated from this Development.

2.9.4 The indicative layout plan of the Development is shown in **Annex 1**.

## 2.10 Operation Arrangement

2.10.1 The car park will operate 24 hours a day. Daily, weekly and monthly rental basis will be allowed for local residents and visitors. A longer operation hour and a varied charging scheme will provide greater flexibility to the cross- boundary travellers, so as to relieve the pressure from the existing Heung Yuen Wai Car Park.

2.10.2 According to Chapter 8 of the Hong Kong Planning Standards and Guidelines, the dimension of parking spaces and loading/ unloading bays are set. All the proposed parking spaces and loading/ unloading bays comply with the relevant standards.

Types of Parking Space and Loading/Unloading Bay	Length (m)	Width (m)
Private Cars, Van-type Light Goods Vehicles and Taxis	5	2.5
Light Goods Vehicles (LGV)	7	3.5





intentions of the “REC” and “V” zones.<sup>5</sup>

- 3.2.3 The building form and building bulk follows the approved scheme. The only difference is the increase of car parking spaces as well as site area. A table summarizing the differences between Applications No. A/NE-TKLN/57 and 58 and the current applicant is as follows:

<u>Parameters</u>	<u>Application No.</u> <u>A/NE-TKLN/57</u>	<u>Application No.</u> <u>A/NE-TKLN/58</u>	<u>Current</u> <u>Application</u>	<u>Difference</u> <u>between</u> <u>current and</u> <u>two approved</u> <u>applications</u>
Site Area	About 1,924 sq.m.	About 2,228 sq.m.	About 4,628 sq.m.	+ 476 sq.m.
Floor Area	About 204 sq.m.	About 180 sq.m.	About 384 sq.m.	Same
No. of Structure(s)	1	1	2	Same
Max. Height of Structures	6 meters	6 meters	6 meters	Same
No. of Private Car Parking Spaces	24	26	50	Same
No. of Loading/ Unloading Bay for Light Goods Vehicle	1	1	2	Same

#### 4.1 Similar Approved Applications for Public Vehicle Parks

- 4.1.1 There are several similar applications for temporary vehicle park use in the vicinity of the Application Site, which falls within “REC” zone and “V” zone.
- 4.1.2 There is a similar approved application of similar uses within the same “REC” zone and “V” zone in Heung Yuen Wai area, involving different sites. The adjacent application No. A/NE-TKLN/53 for temporary public vehicle park for private cars and LGV was approved on 2023, with its access point also abuts Lin Ma Hang Road.
- 4.1.3 Planning approval for the adjacent site (No. A/NE-TKLN/53) is going to be expired in mid-2026, with another approved site (No. A/NE-TKLN/70) that will be lapsed in late-2026. The shortage of car parking spaces in Heung Yuen Wai will become more serious with the lapse of application No. A/NE-TKLN/53.

<sup>5</sup> [https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt\\_e.pdf](https://www.tpb.gov.hk/en/meetings/RNTPC/Minutes/m727rnt_e.pdf)

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**Timothy Wai Pui WU/PLAND**

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寄件者: stndp/PLAND  
寄件日期: 2025年12月11日星期四 17:04  
收件者: Timothy Wai Pui WU/PLAND; Ivy Cho Wa WONG/PLAND  
主旨: Fw: Response to Departmental Comments on Planning Application No. A/NE-TKLN/102  
附件: 20251211\_A\_NE-TKLN\_102\_Combined FI.pdf

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**From:** tpbbpd/PLAND <tpbbpd@pland.gov.hk>  
**Sent:** Thursday, December 11, 2025 5:01 PM  
**To:** stndp/PLAND <stndpo@pland.gov.hk>  
**Cc:** Annie Siu Ling CHAN/PLAND <[REDACTED]>  
**Subject:** Fw: Response to Departmental Comments on Planning Application No. A/NE-TKLN/102

---

**From:** Cherie Lee <[REDACTED]>  
**Sent:** Thursday, December 11, 2025 4:58 PM  
**To:** tpbbpd/PLAND <[tpbbpd@pland.gov.hk](mailto:tpbbpd@pland.gov.hk)>  
**Cc:** Junior <[REDACTED]>  
**Subject:** Response to Departmental Comments on Planning Application No. A/NE-TKLN/102

Dear Sir/Madam,

Attached please find our letter and responses to departmental comments for your review.

Thank you.

Yours faithfully,  
Cherie

LCH (Asia-Pacific) Surveyors Limited | LCH Planning & Development Consultants Limited

[REDACTED]

[REDACTED]

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**LCH Planning and Development  
Consultants Limited**

Our Ref.: PD2504003/10

Your Ref.:

11 December 2025

By Email

Town Planning Board Secretariat  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong

Dear Sir/Madam,

**APPLICATION NO. A/NE-TKLN/102 FOR PERMISSION  
UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE**

**FURTHER INFORMATION**

We refer to the emails enclosing the departmental comments regarding the captioned application.

Please find attached our responses to departmental comments.

This Further Information contains the responses to comments of relevant Government departments. Thus, according to TPB Planning Guideline No. 32B, this Further Information does not result in a material change of the nature of the application and should be accepted by the TPB for inclusion into the application.

Should you require further information or have any query, please feel free to contact the undersigned or Cherie Lee at [REDACTED].

Yours faithfully,  
For and on behalf of  
**LCH Planning & Development Consultants Limited**

**Junior Ho**  
Director

Encl.

c.c. the Applicant  
- Annex 1 - Response to Comments Table

[REDACTED]



**Section 16 Application No. A/NE-TKLN/102**

**Response to Comments Table**

No.	Comments Received	Our Responses
1.	<i>Comments from Lands Department received on 26 November 2025</i>	
a	It is noted that the built-over area (BOA) and disposition of the existing structures erected on site do not tally with the revised proposal, the BOA permitted under STW No. 1666 and the proposed STW of Lot No. 65 S.B RP in D.D. 80. The applicant is required to clarify.	<p>Noted.</p> <p>Upon planning approval, the Applicant will remove all structures which do not conform to the BOA permitted under STW No. 1666. The Applicant will apply for a new Short Term Waiver for the concerned structures on Lot No. 61 SB RP and 65 SB RP in DD. 80 to ensure that all proposed structures are fully aligned and compliant with the permitted BOA and the terms of the proposal.</p> <p>The Applicant is committed to rectifying any inconsistencies to meet the requirements by Lands Department.</p>
b	Apart from the above, our comments dated 17.7.2025 remain valid.	Noted with thanks.

**Previous S.16 Applications**

**Approved Applications**

	<b>Application No.</b>	<b>Uses/Developments</b>	<b>Date of Consideration</b>
1.	A/NE-TKLN/40	Proposed Temporary Shop and Services (Convenience Store) with Ancillary Office for a Period of Three Years	25.6.2021
2.	A/NE-TKLN/57	Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) for a Period of Three Years	22.9.2023 (Revoked on 22.6.2025)
3.	A/NE-TKLN/58	Proposed Temporary Public Vehicle Park (Private Cars Only) and Shop and Services (Convenience Store) for a Period of Three Years	22.9.2023 (Revoked on 22.6.2025)



**Similar S.16 Applications for Temporary Public Vehicle Park and/or Shop and Services,  
in the Vicinity of the Application Site within “Recreation” and “Village Type Development”  
Zones in the Past Five Years**

**Approved Applications**

	<b>Application No.</b>	<b>Uses/Developments</b>	<b>Date of Consideration</b>
1.	A/NE-TKLN/23 <sup>1</sup>	Proposed Temporary Place of Recreation, Sports or Culture (Parent-Child Play Area), Eating Place and Shop and Services for a Period of Five Years	5.2.2021 (Revoked on 5.5.2024)
2.	A/NE-TKLN/35 <sup>%</sup>	Proposed Temporary Eating Place and Shop and Services with Ancillary Office/Store Room for a Period of Three Years	8.1.2021
3.	A/NE-TKLN/37 <sup>#</sup>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) for a Period of Three Years	28.5.2021
4.	A/NE-TKLN/39 <sup>@</sup>	Temporary Retail Shop, Eating Place and Ancillary Office for a Period of Three Years	28.5.2021
5.	A/NE-TKLN/53	Temporary Car Park (Private Cars and Light Goods Vehicles) for a Period of Three Years	23.6.2023
6.	A/NE-TKLN/55	Proposed Temporary Shop and Services (Real Estate Agency) for a Period of Three Years	22.9.2023 (Revoked on 22.6.2025)
7.	A/NE-TKLN/67	Proposed Temporary Public Vehicle Park (PVP) (Private Car Only) for a Period of Three Years	27.10.2023 (Revoked on 27.4.2025)
8.	A/NE-TKLN/68 <sup>1</sup>	Proposed Temporary Public Vehicle Park (PVP) (Excluding Container Vehicle) and Shop and Services for a Period of Five Years	5.4.2024 (Revoked on 1.9.2025)
9.	A/NE-TKLN/70	Proposed Temporary Public Vehicle Park (PVP) (Private Cars Only) for a Period of Three Years	27.10.2023 (Revoked on 27.4.2025)
10.	A/NE-TKLN/75 <sup>#</sup>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) and Shop and Services for a Period of Three Years	15.3.2024
11.	A/NE-TKLN/76	Proposed Temporary Shop and Services (Real Estate Agency) for a Period of Three Years	5.4.2024

12.	A/NE-TKLN/80 <sup>\$</sup>	Proposed Temporary Public Vehicle Park (Private Car Only) for a Period of Three Years and Associated Filling of Land	15.3.2024 (Revoked on 15.12.2025)
13.	A/NE-TKLN/87 <sup>@</sup>	Proposed Temporary Retail Shop, Canteen and Ancillary Office for a Period of Three Years	20.9.2024
14.	A/NE-TKLN/90 <sup>%</sup>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle), Eating Place and Shop and Services (Local Provision Store) with Ancillary Office and Store Room for a Period of Three Years	28.2.2025 (Revoked on 1.9.2025)

Remarks

- <sup>!</sup> : Application Nos. A/NE-TKLN/23 and A/NE-TKLN/68 involve the same site.
- <sup>%</sup> : Application No. A/NE-TKLN/35 forms part of application No. A/NE-TKLN/90.
- <sup>#</sup> : Application Nos. A/NE-TKLN/37 and A/NE-TKLN/75 involve the same site.
- <sup>@</sup> : Application Nos. A/NE-TKLN/39 and A/NE-TKLN/87 involve the same site.
- <sup>\$</sup> : Application Nos. A/NE-TKLN/45 (rejected application) and A/NE-TKLN/80 involve the same site

### **Rejected Application**

	<b>Application No.</b>	<b>Uses/Developments</b>	<b>Date of Consideration</b>	<b>Rejection Reasons</b>
1.	A/NE-TKLN/45 <sup>\$</sup>	Proposed Temporary Public Vehicle Park (Excluding Container Vehicle) for a Period of Three Years	29.7.2022	R1, R2

### **Remarks**

<sup>\$</sup> : Application Nos. A/NE-TKLN/45 and A/NE-TKLN/80 (approved application) involve the same site

### **Rejection Reasons**

- R1 The proposed use was not in line with the planning intentions of the “Agriculture” and “Village Type Development” zones which were primarily to retain and safeguard good quality agricultural land/farm/fish ponds for agricultural purposes and to retain fallow arable land with good potential for rehabilitation for cultivation and other agricultural purposes, and designate both existing recognized villages and areas of land considered suitable for village expansion. Land within this zone was primarily intended for the development of Small Houses by indigenous villagers respectively. There was no strong planning justification in the submission for a departure from such planning intentions, even on a temporary basis.
- R2 The applicant failed to provide sufficient information in the submission to demonstrate that the proposed development would not cause adverse traffic impact on the surrounding areas.

**Government Departments' General Comments**

**1. Land Administration**

Comments of the District Lands Officer/North, Lands Department (DLO/N, LandsD):

- no objection to the application;
- there is no Small House application received or being processed at the application site (the Site);
- the Site comprises Old Schedule Agricultural Lots held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through Government land (GL) but no right of access via GL is granted to the Site;
- Lot No. 61 S.B RP in D.D. 80 is covered by Short Term Waiver No. 1666 (the STW) for the purposes of shop and services and ancillary office, it is noted that the total Built-over Area (BOA) of the existing structures erected on site has exceeded the permitted BOA of the STW. STW application for the purpose of shop and services and ancillary office on the Lot No. 65 S.B RP in D.D. 80 is under processing. While there is no restriction on the number of buildings/structures to be erected under the said STWs, it is noted that there may be more than two existing buildings/structures erected on the Site which contravene to the information stipulated in the Application Form. It is also noted that the BOA and disposition of the existing structures erected on the Site do not tally with the latest proposal, the BOA permitted under STW No. 1666 and the proposed STW of Lot No. 65 S.B RP in D.D. 80. The applicant is required to clarify; and
- if the planning application is approved, the STW holder will need to apply to his office for modification of the conditions of the STW No. 1666 where appropriate to permit the structures erected/to be erected within Lot No. 61 S.B RP in D.D. 80. The application will be considered by the Government in its capacity as a landlord and there is no guarantee that it will be approved. The application, if approved, will be subject to such terms and conditions including the payment of backdated waiver fee and administrative fee as considered appropriate by LandsD. Besides, given the applied uses are temporary in nature, only erection of temporary structure(s) will be considered.

**2. Traffic**

Comments of the Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD):

- no comment from highways maintenance point of view; and
- his advisory comments are at **Appendix V**.

### 3. **Drainage**

Comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD):

- no objection to the application from public drainage viewpoint;
- the latest submitted drainage proposal is considered acceptable;
- taking into account that the area is subject to flooding issue and previous flooding cases during inclement weather, should the application be approved, an approval condition should be included to request the applicant to implement the accepted drainage proposal within three months upon approval of the planning application. The drainage facilities should be properly maintained at all times during the planning approval period and rectified if they are found inadequate/ineffective during operation;
- the Site is in an area where public sewerage connection is not available; and
- her advisory comments are at **Appendix V**.

### 4. **Fire Safety**

Comments of the Director of Fire Services (D of FS):

- no objection in principle to the proposal subject to fire service installations and water supplies for firefighting being provided to his satisfaction; and
- his advisory comments are at **Appendix V**.

### 5. **Environment**

Comments of the Director of Environmental Protection (DEP):

- no objection to the application from environmental perspective noting that heavy vehicles and dusty operation will not be involved in the applied uses; and
- no substantiated environmental complaints against the Site has been received over the past three years; and
- his advisory comments are at **Appendix V**.

### 6. **Building Matters**

Comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):

- it is noted that two structures are proposed in the application, before any new building works (including containers/open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on site, prior approval and consent of the Building Authority should be obtained, otherwise they are unauthorized building works (UBW) under the Buildings Ordinance (BO). An Authorized Person should be appointed as the co-ordinator for the proposed building works in accordance with the BO; and
- his advisory comments are at **Appendix V**.



## 7. **Landscape**

Comments of the Chief Town Planner/Urban Design and Landscape, Planning Department (CTP/UD&L, PlanD):

- no adverse comment from landscape planning perspective;
- based on the aerial photo of 2023, the Site is located in an area of rural inland plains landscape character comprising clusters of tree groups, vegetated areas, Heung Yuen Wai Boundary Control Point to the west, and small houses within the “Village Type Development” zone to the south. With reference to the site photos taken on 19.6.2025, the Site is mostly occupied by vehicles and associated car park facilities, with no significant landscape resources within it. Significant adverse impact on the landscape character and existing landscape resources within the Site is not anticipated; and
- her advisory comments are at **Appendix V**.

## 8. **Other Departments**

The following government departments have no objection to/no comments on the application:

- (a) Director of Agriculture, Fisheries and Conservation (DAFC);
- (b) Chief Engineer/Construction, Water Supplies Department (CE/C, WSD);
- (c) Head of the Geotechnical Engineering Office, Civil Engineering and Development Department (H(GEO), CEDD);
- (d) Project Manager (North), CEDD (PM(N), CEDD);
- (e) Commissioner of Police (C of P); and
- (f) District Officer (North), Home Affairs Department (DO(N), HAD).

**Recommended Advisory Clauses**

- (a) the application site (the Site) falls within the boundary of the Priority Development Area (PDA) of the proposed New Territories North (NTN) New Town and will partly overlap with the proposed Heung Yuen Wai Food Control Facilities (HYW FCF) of the Food and Environmental Hygiene Department (FEHD). The Site may be resumed by the Government and the applied uses at the Site may be terminated at any time during the planning approval period for implementation of Government projects;
- (b) prior planning permission should have been obtained before commencing the applied uses at the Site;
- (c) to resolve any land issue relating to the applied uses with the concerned owner(s) of the Site;
- (d) should the applicant fail to comply with the approval conditions again resulting in the revocation of the planning permission, sympathetic consideration may not be given to any further application;
- (e) to note the comments of the Director of Food and Environmental Hygiene (DFEH) that:
  - (i) according to the Development Proposals of NTN New Town as announced in December 2024, and the proposed location of HYW FCF as announced in February 2025, the Site for the applied uses is within the PDA of NTN New Town and would overlap with the FCF location. The Site would have to be vacated for the site formation/construction works relating to the PDA of NTN New Town and FCF;
  - (ii) the applicant should take into account the potential impact of NTN New Town development, including the relocation of FCF to HYW. It should also be noted that FCF falls within the PDA of NTN New Town. The proposed FCF relocation exercise will involve diversion of a section of Lin Ma Hang Road, site formation and construction of FCF. Subject to the finalisation of the project boundary, the aforesaid works cover an area of about 5ha, of which about 3ha of private land will be acquired. North District Council was consulted for the relocation project at the meeting on 18.2.2025. To take forward the FCF relocation exercise, his Department would submit a section 16 planning application under the Town Planning Ordinance (TPO) (Cap. 131). The statutory procedures for land resumption are planned to commence after obtaining the Town Planning Board (the Board)'s permission. Subject to such procedures, land reversion and site clearance would commence in due course. In general, ex-gratia land compensation may be offered to the owners of private land in the New Territories being affected by land resumption. For business undertakings affected by land resumption/clearance exercises, ex-gratia allowances may be offered to eligible business undertakings operating for at least two years immediately preceding the Pre-clearance Survey (PCS) and not in breach of the lease or conditions of relevant land instruments;
  - (iii) no FEHD's facilities should be affected;
  - (iv) proper licence/permit issued by his Department is required if there is any food business/catering service/activities regulated by DFEH under the Public Health and Municipal Services Ordinance (Cap. 132) and other relevant legislation for the public. In accordance with Cap 132 and the Food Business Regulation (Cap. 132X), a food business licence shall be obtained for any premises intended to operate the relevant type of food business (e.g. restaurant, food factory, fresh provision shop, etc.) listed in the Regulation. The application for licence, if acceptable by FEHD, will be referred to relevant government departments such as the Buildings Department (BD), Fire Services

Department (FSD) and Planning Department (PlanD) for comment. If there is no objection from the departments concerned, a letter of requirements will be issued to the applicant for compliance and the licence will be issued upon compliance of all the requirements;

- (v) depending on the mode of operation, generally there are several types of food business licence/permits that the operator of a convenience store may apply for under the Food Business Regulation:
  - if food is sold to customers for consumption on the premises, a restaurant licence should be obtained;
  - if food is only prepared for sale for consumption off the premises, a food factory licence should be obtained;
  - if fresh, chilled or frozen meat is sold, a fresh provision shop licence should be obtained; and
  - if restricted foods like milk, frozen confections, non-bottled drinks, cut fruit, etc. are to be sold, relevant restricted food permits should be obtained;
- (vi) when choosing a premises, the applicant must ensure that the operation of food business at the subject premises is in compliance with the requirements imposed under the legislation administered by his Department, other government departments and the relevant authorities. The applicant is strongly advised to check well in advance the following documents: (a) the Government Lease, (b) the Occupation Permit of the building, and (c) the statutory plan. No part of a food premises shall be located in, under or over any structures built without the approval and consent of the Building Authority (BA);
- (vii) proper licence issued by his Department is required if related place of entertainment is involved. Any person who desires to keep or use any place of public entertainment for example a theatre and cinema or a place, building, erection or structure, whether temporary or permanent, on one occasion or more, capable of accommodating the public presenting or carrying on public entertainment within the Places of Public Entertainment (PPE) Ordinance (Cap. 172) and its subsidiary legislation, such as a concert, opera, ballet, stage performance or other musical, dramatic or theatrical entertainment, cinematograph or laser projection display, a circus, a lecture or story-telling, an exhibition of any one or more of the following, namely, pictures, photographs, books, manuscripts or other documents or other things, a sporting exhibition or contest, a bazaar, a dance party or an amusement ride and mechanical device which is designed for amusement, a PPE Licence (or Temporary PPE Licence) should be obtain from FEHD whatever the general public is admitted with or without payment; and
- (viii) there should be no encroachment on the public place and no environmental nuisance should be generated to the surroundings. Its state should not be a nuisance or injurious or dangerous to health and surrounding environment. For any waste generated from the such activity/operation, the applicant should arrange disposal properly at their own expense;
- (f) to note the comments of Project Manager (North), Civil Engineering and Development Department (PM(N), CEDD) that:
  - (i) the applied uses are located within the proposed NTN New Town under the Planning and Engineering (P&E) Study for NTN New Town and Man Kam To. The preliminary development proposal for NTN New Town was released in December 2024; and

- (ii) the applied uses fall within the proposed boundary of the PDA in NTN New Town. Government-initiated works for the PDA may commence in 2028/29 the earliest. Subject to the land use planning in the P&E Study, the applied uses would need to be vacated for the site formation works. The applicant should take account of the above if the applied uses are pursued;
- (g) to note the comments of the District Lands Officer/North, Lands Department (DLO/N, LandsD) that:
  - (i) the Site comprises Old Schedule Agricultural Lots held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through Government land (GL) but no right of access via GL is granted to the Site;
  - (ii) Lot No. 61 S.B RP in D.D. 80 is covered by Short Term Waiver No. 1666 (the STW) for the purposes of shop and services and ancillary office, it is noted that the total Built-over Area (BOA) of the existing structures erected on site has exceeded the permitted BOA of the STW. STW application for the purpose of shop and services and ancillary office on the Lot No. 65 S.B RP in D.D. 80 is under processing. While there is no restriction on the number of buildings/structures to be erected under the said STWs, it is noted that there may be more than two existing buildings/structures erected on the Site which contravene to the information stipulated in the Application Form. It is also noted that the BOA and disposition of the existing structures erected on the Site do not tally with the latest proposal, the BOA permitted under STW No. 1666 and the proposed STW of Lot No. 65 S.B RP in D.D. 80. The applicant is required to clarify; and
  - (iii) the STW holder will need to apply to his office for modification of the conditions of the STW No. 1666 where appropriate to permit the structures erected/to be erected within Lot No. 61 S.B RP in D.D. 80. The application will be considered by the Government in its capacity as a landlord and there is no guarantee that it will be approved. The application, if approved, will be subject to such terms and conditions including the payment of backdated waiver fee and administrative fee as considered appropriate by LandsD. Besides, given the applied uses are temporary in nature, only erection of temporary structure(s) will be considered;
- (h) to note the comments of the Commissioner for Transport (C for T) that:
  - (i) sufficient manoeuvring space shall be provided within the Site. No vehicle is allowed to queue-back to or reverse onto/from public road at any time during the planning approval period; and
  - (ii) provision of parking information including the availability of parking spaces at the Site should be included as one of the traffic management measures. The applicant should make reference to the 'Guidelines for Provision of Parking Information' issued by the Transport Department (TD) when providing parking information;
- (i) to note the comments of the Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD) that:
  - (i) the proposed access arrangement should be commented by TD;
  - (ii) HyD is not/shall not be responsible for the maintenance of any access connecting the Site and Lin Ma Hang Road;
  - (iii) adequate drainage measures should be provided to prevent surface water running from the Site to the nearby public roads and drains; and

- (iv) the applicant should take adequate precautionary measures to avoid damaging roads, street furniture, drainage and slopes etc. maintained by his Office. Damage caused to roads, street furniture, drainage and slopes etc. maintained by his office due to the proposed works shall be repaired to his satisfaction at the applicant's own costs;
- (j) to note the comments of the Director of Fire Services (D of FS) that:
  - (i) in consideration of the design/nature of the proposal, fire service installations (FSIs) are anticipated to be required. The applicant should submit relevant layout plans incorporated with the proposed FSIs to his Department for approval. In addition, the applicant should be advised on the following points:
    - the layout plans should be drawn to scale and depicted with dimensions and nature of occupancy and;
    - the location of the proposed FSI to be installed should be clearly marked on the layout plans; and
  - (ii) the applicant is reminded that if the proposed structures are required to comply with the Buildings Ordinance (BO) (Cap. 123), detailed fire service requirements will be formulated upon receipt of formal submission of general building plans;
- (k) to note the comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD) that:
  - (i) the drainage facilities should be rectified if they are found inadequate/ineffective during operation;
  - (ii) the applicant should construct and maintain the proposed drainage facilities whether within or outside the Site at his own expense; and
  - (iii) the Site is in an area where public sewerage connection is not available. Environmental Protection Department (EPD) should be consulted regarding the sewage impact assessment and sewage treatment/disposal facilities for the applied uses;
- (l) to note the comments of the Chief Town Planner/Urban Design and Landscape, PlanD (CTP/UD&L, PlanD) that approval of the application does not imply approval of tree works such as pruning, transplanting, felling and compensation/new tree planting. The applicant should seek approval for any proposed tree works from relevant departments prior to commencement of the works;
- (m) to note the comments of the Director of Environmental Protection (DEP) that the applicant should follow relevant mitigation measures and requirements in the 'Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites' and to meet the statutory requirements under relevant pollution control ordinances; and
- (n) to note the comments of Chief Building Surveyor/New Territories West, BD (CBS/NTW, BD) that:
  - (i) it is noted that two structures are proposed in the application, before any new building works (including containers/open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on site, prior approval and consent of the Building Authority (BA) should be obtained, otherwise they are unauthorized building works (UBW) under the BO. An Authorized Person should be appointed as the co-ordinator for the proposed building works in accordance with the BO; and



(ii) the applicant's attention is drawn to the following points:

- the Site shall be provided with means of obtaining access thereto from a street and emergency vehicular access in accordance with Regulations 5 and 41D of the Building (Planning) Regulations (B(P)R) respectively;
- the Site does not abut on a specified street of not less than 4.5m wide and its permitted development intensity shall be determined under Regulation 19(3) of the B(P)R at building plan submission stage;
- if the existing structures (not being a New Territories Exempted House) are erected on leased land without the approval of the BA, they are UBW under the BO and should not be designated for any applied uses under the application;
- for UBW erected on leased land, enforcement action may be taken by the BD to effect their removal in accordance with the prevailing enforcement policy against UBW as and when necessary. The granting of any planning approval should not be construed as an acceptance of any existing building works or UBW on the Site under the BO;
- any temporary shelters or converted containers for office, storage, washroom or other uses are considered as temporary buildings are subject to the control of Part VII of the B(P)R;
- the applicant's attention is also drawn to the provision under regulations 41 of the Building (Standards of Sanitary Fitments, Plumbing, Drainage Works and Latrines) Regulations in respect of disposal of surface water; and
- detailed checking under the BO will be carried out at building plan submission stage.

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

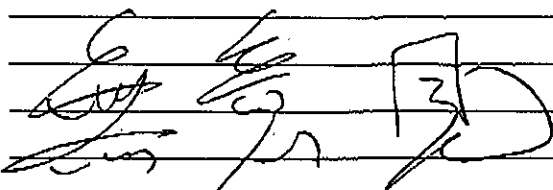
By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

A/NE-TKLN/102

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)



「提意見人」姓名/名稱 Name of person/company making this comment 侯志強議員

簽署 Signature



日期 Date

2025.6.19

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

**Reference Number:**

250620-171900-60104

提交限期

**Deadline for submission:**

08/07/2025

提交日期及時間

**Date and time of submission:**

20/06/2025 17:19:00

有關的規劃申請編號

**The application no. to which the comment relates:**

A/NE-TKLN/102

「提意見人」姓名/名稱

**Name of person making this comment:**

女士 Ms. 譚慧思

意見詳情

**Details of the Comment :**

該地塊已被納入食物管制設施收地規劃內的一部份，若此申請獲批，政府將被迫重啟收地流程，面臨巨額賠償成本。如此反常狀況，不禁令人質疑：是否存在官商勾結、利益輸送？相關部門亟待介入，徹查申請背後是否暗藏不正當交易，還原事件真相，以維護公共利益與行政公信力。

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

**Reference Number:**

250701-214532-62694

提交限期

**Deadline for submission:**

08/07/2025

提交日期及時間

**Date and time of submission:**

01/07/2025 21:45:32

有關的規劃申請編號

**The application no. to which the comment relates:**

A/NE-TKLN/102

「提意見人」姓名/名稱

**Name of person making this comment:**

女士 Ms. 譚慧思

意見詳情

**Details of the Comment :****關於強烈反對土地申請的陳述**

近期，我對某土地申請案深感憂慮與反對，這一立場基於多項土地開發違規事實及相關部門處理失當的情況。

據我了解，編號A/NE-TKLN/40、A/NE-TKLN/57及A/NE-TKLN/58的土地，在申請開發前均已作為停車場使用。然而，自從申請獲批後，這些土地卻未按要求落實附帶條件，尤其是在排水渠工程方面存在嚴重缺失。以A/NE-TKLN/40為例，原本規劃中明確要求改善的排水系統至今仍未完善，每逢雨天，停車場便積水嚴重，不僅影響車輛通行，更存在安全隱患。A/NE-TKLN/57及A/NE-TKLN/58的情況同樣令人擔憂，排水設施形同虛設，相關問題長期未得到解決。

更令人震驚的是，位於中間的土地DD 80 Lot62，在未獲得任何許可的情況下，於2024年10月擅自合併並改建為停車場，屬於明顯的違規發展行為。針對上述所有問題，我已多次向規劃署投訴，詳細列舉各項違規事實，並提供相關佐證材料，希望規劃署能依法處理，督促整改並追究責任。

但遺憾的是，儘管我的投訴有理有據，規劃署卻始終未對這些違規行為採取任何懲處措施。如今，涉事土地竟仍在推進新的申請，若此申請最終獲批，無疑是對規劃法規的公然踐踏，也難免讓公眾質疑其中是否存在利益勾連。我堅信，土地開發應以公眾利益和城市規劃為重，任何違規行為都不應被忽視或縱容。希望相關部門能重新審視此案，徹查違規事實，嚴肅處理失職行為，還市民一個公平、公正的城市發展環境。

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

**Reference Number:**

250706-215446-89812

提交限期

**Deadline for submission:**

08/07/2025

提交日期及時間

**Date and time of submission:**

06/07/2025 21:54:46

有關的規劃申請編號

**The application no. to which the comment relates:**

A/NE-TKLN/102

「提意見人」姓名/名稱

**Name of person making this comment:**

女士 Ms. Tam wai sze

意見詳情

**Details of the Comment :****嚴懲違例用地，堅守規劃底線**

其中DD80 62用地存在「先破壞、後建設」行為，嚴重違反2023年4月14日頒布的《城市規劃條例》，屬於明確界定的違例發展。城市規劃是引導區域有序發展、保障公共利益的根本準則，任何繞開法定程序的建設行為，均屬公然挑戰規劃權威。

根據條例規定，規劃監督部門將立即啟動執管程序，對該違例用地依法採取停工整頓、限期拆除等強制措施，並追究相關主體責任。同時，基於其違法性質，此類用地申請明顯不符合條例要求，應堅決駁回，杜絕「先違法、後補證」的僥倖空間。

後續，規劃監督部門將強化全流程監管，對疑似違例項目提前介入、嚴格審查，以「零容忍」態度維護城市規劃的嚴肅性，確保區域發展合法、有序。



# 就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

250704-000747-12489

Reference Number:

提交限期

08/07/2025

Deadline for submission:

提交日期及時間

04/07/2025 00:07:47

Date and time of submission:

有關的規劃申請編號

A/NE-TKLN/102

The application no. to which the comment relates:

「提意見人」姓名/名稱

先生 Mr. Henry Cheung

Name of person making this comment:

意見詳情

Details of the Comment :

本人反對上過申請原因.第一點.交通安全.在蓮麻坑路這一段.街燈號碼GD4635.你們知道從這街燈編號起.往松園下村方向.有多少家.經營中但沒有許可申請或正在申請及未有落實許可證中附帶條款的違規停車場嗎?單單是綽德.就有四個.加上79k巴士站對面一個.在非法擴展和沒有落實執行規劃署條例及霸佔政府土地情況下、(綽德)合共有五個在經營中的停車場.再加上(佳祺)在經營着三個旗鼓相當的停車場.(松園餐廳)經營中的一個停車場.以上八個停車場通通是沒有申請和正在申請中及違規經營.霸佔政府土地及違例發展的停車場.加上領有許可證的(松記)一個停車場和(迅領)一個停車場.合法及非法停車場數目達到11個之多.亂象橫生.交通意外風險大.在松園下村.有多家貨倉.及工廠.因此在沒有行人路的這段蓮麻坑路上單程但雙向行車的路上.常有重型貨櫃車出入.加上未被規劃的停車場帶來的私家車.及泊車後的駕駛者變為途人.加上外來遊客.拖男大女.有老有少.已常有擁堵情況.需要警方到場幫忙疏導交通.就在六月份已有不少於一宗交通意外發生.主因是這一段蓮麻坑路.已嚴重超出負荷.根源是違規經營的停車場太多.這正是本人反對申請停車場的主因.第二點.渠務系統.沒有落實執行渠務條例.讓水浸風險大大增加.來一場百年一遇的大暴雨.雖不知道什麼時候會遇上.但可以肯定的說.必定會遇上.在沒有任何預防措施情況下.舉足無措.帶來的後果可以是毀滅性的.在沒有洗手間設施及排污系統情況下.長年累月下的污水.污染社區把原來已脆弱的綠色環境推向萬劫不復的深淵.這些都是極度自私的行為.以破壞環境.糟蹋社區不顧及他人安全.換取金錢和利益.雖兵不血刃.但其惡極大.禍害極深.面對城市的發展先改變土地用途.把完來的農地變為停車場將在發展過程中為他們帶來比農地更大的利益.在以上種種問題上.本人對規劃署.期望極大.杜絕這一幫惡棍的停車場申請.為我們規劃更美好的未來.對規劃署在社區的發展居民是肯定的.歡迎的.只要更多的讓居民了解到未來規劃所帶來的好處.相信沒有居民會反對.但對這些目無皇法.自私自利的申請.我們必定會堅定反對到底.直至沒有這些不合理的申請為止.完.謝謝.

3 "附加"

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

Reference Number:

250704-025306-74155

提交限期

Deadline for submission:

08/07/2025

提交日期及時間

Date and time of submission:

04/07/2025 02:53:06

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TKLN/102

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. Henry cheung

意見詳情

Details of the Comment :

A/NE/TKLN/102。A/NE/TKLN/100。A/NE/TKLN/97.的申請我們非常懷疑申請人對落實許可證附例，的能力，因申請人，在蓮麻坑路，綽德有五個停車場之多，當中有申請許可證的，但沒有落實去個正附例，拒絕申請後再擴展申請，落實附例，拖得就拖，意圖瞞天過海，不緊這樣，還尋吞國家土地，改建為私人停車場謀取利益，在這兩個月當中，不管係地政署，還是規劃署，都分別掛上侵佔政府土地告示，及違例擴展告示，但卓德停車場，目無王法，通告掛上第二天就給他們銷毀了，正在申請的停車場，原來由規劃署發出的公眾通告，亦被銷毀，剝奪居民之情權，嘗試改變土地用途，希望能在城市發展中使政府補貼提高收購價，行為讓人齒冷。現實問題，交通非常擠塞，因他在蓮麻坑路單程路段增設了兩個出入口，讓原來的單線雙程行車，的蓮娜坑路不堪重負，常常擠塞，需要警方到場疏導汽車，及蓮麻坑路本身沒有行人路，途人，私家車，重型車輛，常常出現對頭情況，對交通安全轉取極大隱患，

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**From:** [REDACTED]  
**Sent:** 2025-07-06 星期日 03:59:54  
**To:** tpbpd/PLAND <tpbpd@pland.gov.hk>  
**Subject:** A/NE-TKLN/102 DD 78 Lin Ma Hang Road

A/NE-TKLN/102

Lots 377, 380 S.A, 380 S.B, 380 S.C and 380 RP in D.D. 78 and Lots 61 S.B RP (Part), 62 and 65 S.B RP (Part) in D.D. 80, Lin Ma Hang Road, Ta Kwu Ling North

Site area: About 4,628sq.m

Zoning: "Recreation" and "VTD"

Applied use: 117 Public Vehicle Park / Convenience Store

Dear TPB Members,

This is an amalgamation of applications A/NE-TKLN/57 and 58 approved 22/9/23 but conditions never fulfilled.

So back with a fresh application and a doubling in number of the parking spaces.

The Applicant stresses the need for more parking spaces.

That there is a need would indicate that the government depts involved in the planning of the Heung Yuen Wai Boundary Control Point were negligent in both their forecasts for demand for parking and in providing adequate facilities.

Why has there been no inquiry into this matter? With so much money spent on consultants, impact assessments, etc, the persons involved in the planning should be brought to task for failure to provide a brand new facility built at great cost but with inadequate provision of services.

It is unacceptable that the tax payer is burdened with the cost of infrastructure that fails to fulfil its purpose.

Operators take advantage of the lack of accountability to extend the blight of brownfield operations secure in the knowledge that failure to fulfil conditions is treated with the same laxity as failure to ensure that infrastructure projects are fit for their purpose.

Mary Mulvihill

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

**Reference Number:**

250705-134838-84184

提交限期

**Deadline for submission:**

08/07/2025

提交日期及時間

**Date and time of submission:**

05/07/2025 13:48:38

有關的規劃申請編號

**The application no. to which the comment relates:**

A/NE-TKLN/102

「提意見人」姓名/名稱

**Name of person making this comment:**

先生 Mr. MR,simon NG

意見詳情

**Details of the Comment :**

Regarding the above application site,it has already been damaged and used as a parking lot.Why apply for it again!!!

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

**tpbpd/PLAND**

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寄件者: [REDACTED]  
寄件日期: 2025年07月05日星期六 20:11  
收件者: tpbpd/PLAND  
主旨: A/NE-TKLN/102關於土地用途變更未履行公示義務的投訴  
  
類別: [REDACTED]

致城規會及規劃署：

根據《城市規劃條例》相關規定，申請改變土地用途需在場地內張貼通知，向公眾公示詳情，以保障鄰近居民及利害關係人知情權。但經現場查核，[具體地點：新界打鼓嶺北蓮麻坑路丈量約份第 78 約地段第 377 號、第 380 號 A 分段、第 380 號 B 分段、第 380 號 C 分段、第 380 號餘段和第 80 約地段第 61 號 B 分段餘段（部分）、第 62 號及第 65 號 B 分段餘段（部分）的土地用途變更申請人未按要求在場內張貼公示文件，完全忽視法定程序。

此舉不僅違反規劃公示流程，更剝奪了公眾參與意見的機會，可能導致後續規劃與實際需求脫節，甚至引發爭議。為維護規劃程序的嚴肅性及公眾權益，懇請貴單位立即介入調查，要求申請人補正公示義務，並依法追究其未履行政程序的責任。

盼復。