

2025年 8月 1 4日

此文件在 收到・城市規劃委員會
只會在收到所有必要的資料及文件後才正式確認收到
申請的日期。

Appendix I of RNTPC
Paper No. A/NE-TKL/813A

This document is received on 2025-08-14
The Town Planning Board will formally acknowledge
the date of receipt of the application only upon receipt
of all the required information and documents.

Form No. S16-I
表格第 S16-I 號

**APPLICATION FOR PERMISSION
UNDER SECTION 16 OF
THE TOWN PLANNING ORDINANCE
(CAP. 131)**

根據《城市規劃條例》(第131章)
第16條遞交的許可申請

Applicable to proposals not involving or not only involving:
適用於建議不涉及或不祇涉及:

- (i) **Construction of "New Territories Exempted House(s)";**
興建「新界豁免管制屋宇」;
- (ii) **Temporary use/development of land and/or building not exceeding 3 years in rural areas or Regulated Areas; and**
位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展;及
- (iii) **Renewal of permission for temporary use or development in rural areas or Regulated Areas**
位於鄉郊地區或受規管地區的臨時用途或發展的許可續期

Applicant who would like to publish the notice of application in local newspapers to meet one of the Town Planning Board's requirements of taking reasonable steps to obtain consent of or give notification to the current land owner, please refer to the following link regarding publishing the notice in the designated newspapers:
https://www.tpb.gov.hk/en/plan_application/apply.html

申請人如欲在本地報章刊登申請通知,以採取城市規劃委員會就取得現行土地擁有人的同意或通知現行土地擁有人所指定的其中一項合理步驟,請瀏覽以下網址有關在指定的報章刊登通知:
https://www.tpb.gov.hk/tc/plan_application/apply.html

General Note and Annotation for the Form
填寫表格的一般指引及註解

"Current land owner" means any person whose name is registered in the Land Registry as that of an owner of the land to which the application relates, as at 6 weeks before the application is made
「現行土地擁有人」指在提出申請前六星期,其姓名或名稱已在土地註冊處註冊為該申請所關乎的土地的擁有人的人

& Please attach documentary proof 請夾附證明文件

^ Please insert number where appropriate 請在適當地方註明編號

Please fill "NA" for inapplicable item 請在不適用的項目填寫「不適用」

Please use separate sheets if the space provided is insufficient 如所提供的空間不足,請另頁說明

Please insert a 「✓」 at the appropriate box 請在適當的方格內上加上「✓」號

2501767

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By Hand

Form No. S16-I 表格第 S16-I 號

For Official Use Only 請勿填寫此欄	Application No. 申請編號	A/NE-7KL/813
	Date Received 收到日期	2025-08-14

- The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.
申請人須把填妥的申請表格及其他支持申請的文件(倘有), 送交香港北角渣華道 333 號北角政府合署 15 樓城市規劃委員會(下稱「委員會」)秘書收。
- Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.tpb.gov.hk/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).
請先細閱《申請須知》的資料單張, 然後填寫此表格。該份文件可從委員會的網頁下載(網址: <http://www.tpb.gov.hk/>), 亦可向委員會秘書處(香港北角渣華道 333 號北角政府合署 15 樓 - 電話: 2231 4810 或 2231 4835)及規劃署的規劃資料查詢處(熱線: 2231 5000)(香港北角渣華道 333 號北角政府合署 17 樓及新界沙田上禾輦路 1 號沙田政府合署 14 樓)索取。
- This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.
此表格可從委員會的網頁下載, 亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全, 委員會可拒絕處理有關申請。

1. Name of Applicant 申請人姓名/名稱

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構)

Wealth Generation International Limited (新世代國際有限公司)

2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱 (如適用)

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☒ Company 公司 / ☐ Organisation 機構)

Lawson David & Sung Surveyors Limited (羅迅測計師行有限公司)

3. Application Site 申請地點

(a) Full address / location / demarcation district and lot number (if applicable) 詳細地址/地點/丈量約份及地段號碼(如適用)	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.
(b) Site area and/or gross floor area involved 涉及的地盤面積及/或總樓面面積	<input checked="" type="checkbox"/> Site area 地盤面積 4,180 sq.m 平方米 <input checked="" type="checkbox"/> About 約 <input checked="" type="checkbox"/> Gross floor area 總樓面面積 2,377 sq.m 平方米 <input checked="" type="checkbox"/> About 約
(c) Area of Government land included (if any) 所包括的政府土地面積(倘有)	..... 680 sq.m 平方米 <input checked="" type="checkbox"/> About 約

(d) Name and number of the related statutory plan(s) 有關法定圖則的名稱及編號	Approved Ping Che and Ta Kwu Ling Outline Zoning Plan No. S/NE-TKL/14
(e) Land use zone(s) involved 涉及的土地用途地帶	"Open Storage" ("OS")
(f) Current use(s) 現時用途	Vehicle Repair Workshops, Warehouses and Temporary Structures (If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)

4. "Current Land Owner" of Application Site 申請地點的「現行土地擁有人」

The applicant 申請人 -

- ☐ is the sole "current land owner"^{#&} (please proceed to Part 6 and attach documentary proof of ownership).
是唯一的「現行土地擁有人」^{#&} (請繼續填寫第 6 部分，並夾附業權證明文件)。
- ☐ is one of the "current land owners"^{#&} (please attach documentary proof of ownership).
是其中一名「現行土地擁有人」^{#&} (請夾附業權證明文件)。
- ☒ is not a "current land owner"[#].
並不是「現行土地擁有人」[#]。
- ☐ The application site is entirely on Government land (please proceed to Part 6).
申請地點完全位於政府土地上 (請繼續填寫第 6 部分)。

5. Statement on Owner's Consent/Notification 就土地擁有人的同意/通知土地擁有人的陳述

- (a) According to the record(s) of the Land Registry as at (DD/MM/YYYY), this application involves a total of "current land owner(s)"[#].
根據土地註冊處截至 年 月 日的記錄，這宗申請共牽涉 名「現行土地擁有人」[#]。

(b) The applicant 申請人 -

- ☐ has obtained consent(s) of "current land owner(s)"[#].
已取得 名「現行土地擁有人」[#]的同意。

Details of consent of "current land owner(s)" [#] obtained 取得「現行土地擁有人」 [#] 同意的詳情		
No. of 'Current Land Owner(s)' 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where consent(s) has/have been obtained 根據土地註冊處記錄已獲得同意的地段號碼/處所地址	Date of consent obtained (DD/MM/YYYY) 取得同意的日期 (日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has notified “current land owner(s)”[#]
已通知 名「現行土地擁有人」[#]。

Details of the “current land owner(s)” [#] notified 已獲通知「現行土地擁有人」 [#] 的詳細資料		
No. of ‘Current Land Owner(s)’ 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where notification(s) has/have been given 根據土地註冊處記錄已發出通知的地段號碼／處所地址	Date of notification given (DD/MM/YYYY) 通知日期(日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☒ has taken reasonable steps to obtain consent of or give notification to owner(s):
已採取合理步驟以取得土地擁有人的同意或向該人發給通知。詳情如下：

Reasonable Steps to Obtain Consent of Owner(s) 取得土地擁有人的同意所採取的合理步驟

- ☐ sent request for consent to the “current land owner(s)” on _____ (DD/MM/YYYY)^{#&}
於_____ (日/月/年)向每一名「現行土地擁有人」[#]郵遞要求同意書[&]

Reasonable Steps to Give Notification to Owner(s) 向土地擁有人發出通知所採取的合理步驟

- ☐ published notices in local newspapers on _____ (DD/MM/YYYY)[&]
於_____ (日/月/年)在指定報章就申請刊登一次通知[&]

- ☒ posted notice in a prominent position on or near application site/premises on
15.5.2025 - 28.5.2025 (DD/MM/YYYY)[&]
於_____ (日/月/年)在申請地點／申請處所或附近的顯明位置貼出關於該申請的通知[&]

- ☒ sent notice to relevant owners’ corporation(s)/owners’ committee(s)/mutual aid committee(s)/management office(s) or rural committee on 22.4.2025 (DD/MM/YYYY)[&]
於_____ (日/月/年)把通知寄往相關的業主立案法團/業主委員會/互助委員會或管理處，或有關的鄉事委員會[&]

Others 其他

- ☐ others (please specify)
其他（請指明）

Note: May insert more than one 「✓」.

Information should be provided on the basis of each and every lot (if applicable) and premises (if any) in respect of the application.

註：可在多於一個方格內加上「✓」號

申請人須就申請涉及的每一地段（倘適用）及處所（倘有）分別提供資料

6. Type(s) of Application 申請類別

- ☐ Type (i) Change of use within existing building or part thereof
第(i)類 更改現有建築物或其部分內的用途
- ☐ Type (ii) Diversion of stream / excavation of land / filling of land / filling of pond as required under Notes of Statutory Plan(s)
第(ii)類 根據法定圖則《註釋》內所要求的河道改道／挖土／填土／填塘工程
- ☐ Type (iii) Public utility installation / Utility installation for private project
第(iii)類 公用事業設施裝置/私人發展計劃的公用設施裝置
- ☐ Type (iv) Minor relaxation of stated development restriction(s) as provided under Notes of Statutory Plan(s)
第(iv)類 略為放寬於法定圖則《註釋》內列明的發展限制
- ☒ Type (v) Use / development other than (i) to (iii) above
第(v)類 上述的(i)至(iii)項以外的用途／發展

Note 1: May insert more than one 「✓」.

註 1：可在多於一個方格內加上「✓」號

Note 2: For Development involving columbarium use, please complete the table in the Appendix.

註 2：如發展涉及靈灰安置所用途，請填妥於附件的表格。

(i) For Type (i) application 供第(i)類申請			
(a) Total floor area involved 涉及的總樓面面積	sq.m 平方米		
(b) Proposed use(s)/development 擬議用途/發展	(If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)		
(c) Number of storeys involved 涉及層數	Number of units involved 涉及單位數目		
(d) Proposed floor area 擬議樓面面積	Domestic part 住用部分		sq.m 平方米 <input type="checkbox"/> About 約
	Non-domestic part 非住用部分		sq.m 平方米 <input type="checkbox"/> About 約
	Total 總計		sq.m 平方米 <input type="checkbox"/> About 約
(e) Proposed uses of different floors (if applicable) 不同樓層的擬議用途(如適用) (Please use separate sheets if the space provided is insufficient) (如所提供的空間不足，請另頁說明)	Floor(s) 樓層	Current use(s) 現時用途	Proposed use(s) 擬議用途

(ii) For Type (ii) application 供第(ii)類申請

<p>(a) Operation involved 涉及工程</p>	<p><input type="checkbox"/> Diversion of stream 河道改道</p> <p><input type="checkbox"/> Filling of pond 填塘 Area of filling 填塘面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填塘深度 m 米 <input type="checkbox"/> About 約</p> <p><input type="checkbox"/> Filling of land 填土 Area of filling 填土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填土厚度 m 米 <input type="checkbox"/> About 約</p> <p><input type="checkbox"/> Excavation of land 挖土 Area of excavation 挖土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of excavation 挖土深度 m 米 <input type="checkbox"/> About 約</p> <p>(Please indicate on site plan the boundary of concerned land/pond(s), and particulars of stream diversion, the extent of filling of land/pond(s) and/or excavation of land) (請用圖則顯示有關土地/池塘界線, 以及河道改道、填塘、填土及/或挖土的細節及/或範圍))</p>
<p>(b) Intended use/development 有意進行的用途/發展</p>	

(iii) For Type (iii) application 供第(iii)類申請

<p>(a) Nature and scale 性質及規模</p>	<p><input type="checkbox"/> Public utility installation 公用事業設施裝置</p> <p><input type="checkbox"/> Utility installation for private project 私人發展計劃的公用設施裝置</p>											
	<p>Please specify the type and number of utility to be provided as well as the dimensions of each building/structure, where appropriate 請註明有關裝置的性質及數量, 包括每座建築物/構築物(倘有)的長度、高度和闊度</p>											
	<table border="1"> <thead> <tr> <th data-bbox="502 1373 794 1489">Name/type of installation 裝置名稱/種類</th> <th data-bbox="794 1373 962 1489">Number of provision 數量</th> <th data-bbox="962 1373 1457 1489">Dimension of each installation /building/structure (m) (LxWxH) 每個裝置/建築物/構築物的尺寸 (米) (長 x 闊 x 高)</th> </tr> </thead> <tbody> <tr> <td data-bbox="502 1489 794 1615"></td> <td data-bbox="794 1489 962 1615"></td> <td data-bbox="962 1489 1457 1615"></td> </tr> <tr> <td data-bbox="502 1615 794 1740"></td> <td data-bbox="794 1615 962 1740"></td> <td data-bbox="962 1615 1457 1740"></td> </tr> <tr> <td data-bbox="502 1740 794 1861"></td> <td data-bbox="794 1740 962 1861"></td> <td data-bbox="962 1740 1457 1861"></td> </tr> </tbody> </table>	Name/type of installation 裝置名稱/種類	Number of provision 數量	Dimension of each installation /building/structure (m) (LxWxH) 每個裝置/建築物/構築物的尺寸 (米) (長 x 闊 x 高)								
Name/type of installation 裝置名稱/種類	Number of provision 數量	Dimension of each installation /building/structure (m) (LxWxH) 每個裝置/建築物/構築物的尺寸 (米) (長 x 闊 x 高)										
<p>(Please illustrate on plan the layout of the installation 請用圖則顯示裝置的布局)</p>												

(iv) For Type (iv) application 供第(iv)類申請

- (a) Please specify the proposed minor relaxation of stated development restriction(s) and also fill in the proposed use/development and development particulars in part (v) below –
請列明擬議略為放寬的發展限制並填妥於第(v)部分的擬議用途/發展及發展細節 –

- ☐ Plot ratio restriction From 由 to 至
地積比率限制
- ☐ Gross floor area restriction From 由sq. m 平方米 to 至sq. m 平方米
總樓面面積限制
- ☐ Site coverage restriction From 由% to 至 %
上蓋面積限制
- ☐ Building height restriction From 由m 米 to 至 m 米
建築物高度限制
From 由 mPD 米 (主水平基準上) to 至mPD 米 (主水平基準上)
From 由 storeys 層 to 至 storeys 層
- ☐ Non-building area restriction From 由m to 至 m
非建築用地限制
- ☐ Others (please specify)
其他 (請註明)

(v) For Type (v) application 供第(v)類申請

(a) Proposed use(s)/development
擬議用途/發展

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office for a Period of 5 Years

(Please illustrate the details of the proposal on a layout plan 請用平面圖說明建議詳情)

(b) Development Schedule 發展細節表

Proposed gross floor area (GFA) 擬議總樓面面積	.....2,377..... sq.m 平方米	<input checked="" type="checkbox"/> About 約
Proposed plot ratio 擬議地積比率	.....0.57.....	<input checked="" type="checkbox"/> About 約
Proposed site coverage 擬議上蓋面積	.....33..... %	<input checked="" type="checkbox"/> About 約
Proposed no. of blocks 擬議座數	.....6.....	
Proposed no. of storeys of each block 每座建築物的擬議層數	.....1-2..... storeys 層	
	<input type="checkbox"/> include 包括.....storeys of basements 層地庫	
	<input type="checkbox"/> exclude 不包括.....storeys of basements 層地庫	
Proposed building height of each block 每座建築物的擬議高度	.....NA..... mPD 米(主水平基準上)	<input type="checkbox"/> About 約
	.....3-7..... m 米	<input checked="" type="checkbox"/> About 約

(iv) For Type (iv) application 供第(iv)類申請

- (a) Please specify the proposed minor relaxation of stated development restriction(s) and **also fill in the proposed use/development and development particulars in part (v) below** –
請列明擬議略為放寬的發展限制並填妥於第(v)部分的擬議用途/發展及發展細節 –

- ☐ Plot ratio restriction From 由 to 至
地積比率限制
- ☐ Gross floor area restriction From 由sq. m 平方米 to 至sq. m 平方米
總樓面面積限制
- ☐ Site coverage restriction From 由% to 至 %
上蓋面積限制
- ☐ Building height restriction From 由m 米 to 至 m 米
建築物高度限制
From 由 mPD 米 (主水平基準上) to 至mPD 米 (主水平基準上)
From 由 storeys 層 to 至 storeys 層
- ☐ Non-building area restriction From 由m to 至 m
非建築用地限制
- ☐ Others (please specify)
其他 (請註明)

(v) For Type (v) application 供第(v)類申請

(a) Proposed use(s)/development
擬議用途/發展

Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years

(Please illustrate the details of the proposal on a layout plan 請用平面圖說明建議詳情)

(b) Development Schedule 發展細節表

Proposed gross floor area (GFA) 擬議總樓面面積	..... 2,377 sq.m 平方米	<input checked="" type="checkbox"/> About 約
Proposed plot ratio 擬議地積比率	..... 0.57	<input checked="" type="checkbox"/> About 約
Proposed site coverage 擬議上蓋面積	..... 33 %	<input checked="" type="checkbox"/> About 約
Proposed no. of blocks 擬議座數	..... 6	
Proposed no. of storeys of each block 每座建築物的擬議層數	..... 1 - 2 storeys 層	
	<input type="checkbox"/> include 包括 storeys of basements 層地庫	
	<input type="checkbox"/> exclude 不包括 storeys of basements 層地庫	
Proposed building height of each block 每座建築物的擬議高度	..... NA mPD 米(主水平基準上)	<input type="checkbox"/> About 約
	..... 3 - 7 m 米	<input checked="" type="checkbox"/> About 約

7. Anticipated Completion Time of the Development Proposal 擬議發展計劃的預計完成時間

Anticipated completion time (in month and year) of the development proposal (by phase (if any)) (e.g. June 2023)
擬議發展計劃預期完成的年份及月份 (分期 (倘有)) (例: 2023 年 6 月)
(Separate anticipated completion times (in month and year) should be provided for the proposed public open space and Government, institution or community facilities (if any))
(申請人須就擬議的公眾休憩用地及政府、機構或社區設施 (倘有) 提供個別擬議完成的年份及月份)

January 2026

8. Vehicular Access Arrangement of the Development Proposal 擬議發展計劃的行车通道安排

<p>Any vehicular access to the site/subject building? 是否有車路通往地盤/有關建築物?</p>	<p>Yes 是</p> <p>No 否</p>	<p><input checked="" type="checkbox"/> There is an existing access. (please indicate the street name, where appropriate) 有一條現有車路。(請註明車路名稱(如適用))</p> <p>Sha Tau Kok Road - Lung Yeuk Tau.</p> <p><input type="checkbox"/> There is a proposed access. (please illustrate on plan and specify the width) 有一條擬議車路。(請在圖則顯示, 並註明車路的闊度)</p> <p><input type="checkbox"/></p>														
<p>Any provision of parking space for the proposed use(s)? 是否有為擬議用途提供停車位?</p>	<p>Yes 是</p> <p>No 否</p>	<p><input checked="" type="checkbox"/> (Please specify type(s) and number(s) and illustrate on plan) 請註明種類及數目並於圖則上顯示)</p> <table border="0"> <tr> <td>Private Car Parking Spaces 私家車車位</td> <td>21 (2.5m x 5m)</td> </tr> <tr> <td>Motorcycle Parking Spaces 電單車車位</td> <td>NA</td> </tr> <tr> <td>Light Goods Vehicle Parking Spaces 輕型貨車泊車位</td> <td>NA</td> </tr> <tr> <td>Medium Goods Vehicle Parking Spaces 中型貨車泊車位</td> <td>NA</td> </tr> <tr> <td>Heavy Goods Vehicle Parking Spaces 重型貨車泊車位</td> <td>NA</td> </tr> <tr> <td>Others (Please Specify) 其他 (請列明)</td> <td>33 (2.5m x 5m)</td> </tr> <tr> <td>Electric Taxi</td> <td></td> </tr> </table> <p><input type="checkbox"/></p>	Private Car Parking Spaces 私家車車位	21 (2.5m x 5m)	Motorcycle Parking Spaces 電單車車位	NA	Light Goods Vehicle Parking Spaces 輕型貨車泊車位	NA	Medium Goods Vehicle Parking Spaces 中型貨車泊車位	NA	Heavy Goods Vehicle Parking Spaces 重型貨車泊車位	NA	Others (Please Specify) 其他 (請列明)	33 (2.5m x 5m)	Electric Taxi	
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Others (Please Specify) 其他 (請列明)	33 (2.5m x 5m)															
Electric Taxi																
<p>Any provision of loading/unloading space for the proposed use(s)? 是否有為擬議用途提供上落客貨車位?</p>	<p>Yes 是</p> <p>No 否</p>	<p><input checked="" type="checkbox"/> (Please specify type(s) and number(s) and illustrate on plan) 請註明種類及數目並於圖則上顯示)</p> <table border="0"> <tr> <td>Taxi Spaces 的士車位</td> <td>NA</td> </tr> <tr> <td>Coach Spaces 旅遊巴車位</td> <td>NA</td> </tr> <tr> <td>Light Goods Vehicle Spaces 輕型貨車車位</td> <td>NA</td> </tr> <tr> <td>Medium Goods Vehicle Spaces 中型貨車車位</td> <td>1 (3.5m x 11m)</td> </tr> <tr> <td>Heavy Goods Vehicle Spaces 重型貨車車位</td> <td>NA</td> </tr> <tr> <td>Others (Please Specify) 其他 (請列明)</td> <td>NA</td> </tr> </table> <p><input type="checkbox"/></p>	Taxi Spaces 的士車位	NA	Coach Spaces 旅遊巴車位	NA	Light Goods Vehicle Spaces 輕型貨車車位	NA	Medium Goods Vehicle Spaces 中型貨車車位	1 (3.5m x 11m)	Heavy Goods Vehicle Spaces 重型貨車車位	NA	Others (Please Specify) 其他 (請列明)	NA		
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Others (Please Specify) 其他 (請列明)	NA															

9. Impacts of Development Proposal 擬議發展計劃的影響

If necessary, please use separate sheets to indicate the proposed measures to minimise possible adverse impacts or give justifications/reasons for not providing such measures.

如需要的話，請另頁註明可盡量減少可能出現不良影響的措施，否則請提供理據/理由。

<p>Does the development proposal involve alteration of existing building? 擬議發展計劃是否包括現有建築物的改動?</p>	<p>Yes 是</p> <p>No 否</p>	<p><input type="checkbox"/> Please provide details 請提供詳情</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p> <p><input checked="" type="checkbox"/></p>																														
<p>Does the development proposal involve the operation on the right? 擬議發展是否涉及右列的工程? (Note: where Type (ii) application is the subject of application, please skip this section. 註：如申請涉及第(ii)類申請，請跳至下一條問題。)</p>	<p>Yes 是</p> <p>No 否</p>	<p><input type="checkbox"/> (Please indicate on site plan the boundary of concerned land/pond(s), and particulars of stream diversion, the extent of filling of land/pond(s) and/or excavation of land) (請用地盤平面圖顯示有關土地／池塘界線，以及河道改道、填塘、填土及／或挖土的細節及／或範圍)</p> <p><input type="checkbox"/> Diversion of stream 河道改道</p> <p><input type="checkbox"/> Filling of pond 填塘 Area of filling 填塘面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填塘深度 m 米 <input type="checkbox"/> About 約</p> <p><input type="checkbox"/> Filling of land 填土 Area of filling 填土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of filling 填土厚度 m 米 <input type="checkbox"/> About 約</p> <p><input type="checkbox"/> Excavation of land 挖土 Area of excavation 挖土面積 sq.m 平方米 <input type="checkbox"/> About 約 Depth of excavation 挖土深度 m 米 <input type="checkbox"/> About 約</p> <p><input checked="" type="checkbox"/></p>																														
<p>Would the development proposal cause any adverse impacts? 擬議發展計劃會否造成不良影響？</p>		<table border="0"> <tr> <td>On environment 對環境</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On traffic 對交通</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On water supply 對供水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On drainage 對排水</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>On slopes 對斜坡</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Affected by slopes 受斜坡影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Landscape Impact 構成景觀影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Tree Felling 砍伐樹木</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Visual Impact 構成視覺影響</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> <tr> <td>Others (Please Specify) 其他 (請列明)</td> <td>Yes 會 <input type="checkbox"/></td> <td>No 不會 <input checked="" type="checkbox"/></td> </tr> </table> <p>.....</p> <p>.....</p> <p>Please state measure(s) to minimise the impact(s). For tree felling, please state the number, diameter at breast height and species of the affected trees (if possible) 請註明盡量減少影響的措施。如涉及砍伐樹木，請說明受影響樹木的數目、及胸高度的樹幹直徑及品種(倘可)</p> <p>NA</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p>	On environment 對環境	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On traffic 對交通	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On water supply 對供水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On drainage 對排水	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	On slopes 對斜坡	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Affected by slopes 受斜坡影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Landscape Impact 構成景觀影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Tree Felling 砍伐樹木	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Visual Impact 構成視覺影響	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>	Others (Please Specify) 其他 (請列明)	Yes 會 <input type="checkbox"/>	No 不會 <input checked="" type="checkbox"/>
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10. Justifications 理由

The applicant is invited to provide justifications in support of the application. Use separate sheets if necessary.
現請申請人提供申請理由及支持其申請的資料。如有需要，請另頁說明。

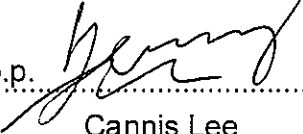
Please refer to the planning statement attached.

11. Declaration 聲明

I hereby declare that the particulars given in this application are correct and true to the best of my knowledge and belief.
本人謹此聲明，本人就這宗申請提交的資料，據本人所知及所信，均屬真實無誤。

I hereby grant a permission to the Board to copy all the materials submitted in this application and/or to upload such materials to the Board's website for browsing and downloading by the public free-of-charge at the Board's discretion. 本人現准許委員會酌情將本人就此申請所提交的所有資料複製及/或上載至委員會網站，供公眾免費瀏覽或下載。

Signature
簽署

p.p. 
.....
Cannis Lee

☐ Applicant 申請人 / ☒ Authorised Agent 獲授權代理人

Name in Block Letters
姓名（請以正楷填寫）

Associate Director (Planning)

Position (if applicable)
職位（如適用）

Professional Qualification(s)
專業資格

☒ Member 會員 / ☐ Fellow of 資深會員

☐ HKIP 香港規劃師學會 / ☐ HKIA 香港建築師學會 /

☐ HKIS 香港測量師學會 / ☐ HKIE 香港工程師學會 /

☐ HKILA 香港園境師學會 / ☐ HKIUD 香港城市設計學會

☐ RPP 註冊專業規劃師

Others 其他 MPIA

on behalf of
代表

Lawson David & Sung Surveyors Limited



☒ Company 公司 / ☐ Organisation Name and Chop (if applicable) 機構名稱及蓋章（如適用）

Date 日期

6/8/2025

(DD/MM/YYYY 日/月/年)

Remark 備註

The materials submitted in this application and the Board's decision on the application would be disclosed to the public. Such materials would also be uploaded to the Board's website for browsing and free downloading by the public where the Board considers appropriate.

委員會會向公眾披露申請人所遞交的申請資料和委員會對申請所作的決定。在委員會認為合適的情況下，有關申請資料亦會上載至委員會網頁供公眾免費瀏覽及下載。

Warning 警告

Any person who knowingly or wilfully makes any statement or furnish any information in connection with this application, which is false in any material particular, shall be liable to an offence under the Crimes Ordinance.

任何人在明知或故意的情況下，就這宗申請提出在任何要項上是虛假的陳述或資料，即屬違反《刑事罪行條例》。

Statement on Personal Data 個人資料的聲明

1. The personal data submitted to the Board in this application will be used by the Secretary of the Board and Government departments for the following purposes:

委員會就這宗申請所收到的個人資料會交給委員會秘書及政府部門，以根據《城市規劃條例》及相關的城市規劃委員會規劃指引的規定作以下用途：

(a) the processing of this application which includes making available the name of the applicant for public inspection when making available this application for public inspection; and

處理這宗申請，包括公布這宗申請供公眾查閱，同時公布申請人的姓名供公眾查閱；以及

(b) facilitating communication between the applicant and the Secretary of the Board/Government departments.

方便申請人與委員會秘書及政府部門之間進行聯絡。

2. The personal data provided by the applicant in this application may also be disclosed to other persons for the purposes mentioned in paragraph 1 above.

申請人就這宗申請提供的個人資料，或亦會向其他人士披露，以作上述第 1 段提及的用途。

3. An applicant has a right of access and correction with respect to his/her personal data as provided under the Personal Data (Privacy) Ordinance (Cap. 486). Request for personal data access and correction should be addressed to the Secretary of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.

根據《個人資料(私隱)條例》(第 486 章)的規定，申請人有權查閱及更正其個人資料。如欲查閱及更正個人資料，應向委員會秘書提出有關要求，其地址為香港北角渣華道 333 號北角政府合署 15 樓。

For Developments involving Columbarium Use, please also complete the following:
如發展涉及靈灰安置所用途，請另外填妥以下資料：

Ash interment capacity 骨灰安放容量^②

Maximum number of sets of ashes that may be interred in the niches

在龕位內最多可安放骨灰的數量

Maximum number of sets of ashes that may be interred other than in niches

在非龕位的範圍內最多可安放骨灰的數量

Total number of niches 龕位總數

Total number of single niches

單人龕位總數

Number of single niches (sold and occupied)

單人龕位數目 (已售並佔用)

Number of single niches (sold but unoccupied)

單人龕位數目 (已售但未佔用)

Number of single niches (residual for sale)

單人龕位數目 (待售)

Total number of double niches

雙人龕位總數

Number of double niches (sold and fully occupied)

雙人龕位數目 (已售並全部佔用)

Number of double niches (sold and partially occupied)

雙人龕位數目 (已售並部分佔用)

Number of double niches (sold but unoccupied)

雙人龕位數目 (已售但未佔用)

Number of double niches (residual for sale)

雙人龕位數目 (待售)

Total no. of niches other than single or double niches (please specify type)

除單人及雙人龕位外的其他龕位總數 (請列明類別)

Number of niches (sold and fully occupied)

龕位數目 (已售並全部佔用)

Number of niches (sold and partially occupied)

龕位數目 (已售並部分佔用)

Number of niches (sold but unoccupied)

龕位數目 (已售但未佔用)

Number of niches (residual for sale)

龕位數目 (待售)

Proposed operating hours 擬議營運時間

② Ash interment capacity in relation to a columbarium means –

就靈灰安置所而言，骨灰安放容量指：

- the maximum number of containers of ashes that may be interred in each niche in the columbarium;
每個龕位內可安放的骨灰容器的最高數目；
- the maximum number of sets of ashes that may be interred other than in niches in any area in the columbarium; and
在該靈灰安置所並非龕位的範圍內，總共最多可安放多少份骨灰；以及
- the total number of sets of ashes that may be interred in the columbarium.
在該靈灰安置所內，總共最多可安放多少份骨灰。

Gist of Application 申請摘要

(Please provide details in both English and Chinese as far as possible. This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information.)
(請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)

Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)		
Location/address 位置/地址	Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.		
Site area 地盤面積	4,180 sq. m 平方米 <input checked="" type="checkbox"/> About 約 (includes Government land of 包括政府土地 680 sq. m 平方米 <input checked="" type="checkbox"/> About 約)		
Plan 圖則	Approved Ping Che and Ta Kwu Ling Outline Zoning Plan No. S/NE-TKL/14		
Zoning 地帶	"Open Storage" ("OS")		
Applied use/ development 申請用途/發展	Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years		
(i) Gross floor area and/or plot ratio 總樓面面積及/或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	NA <input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	2,377 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.57 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用	NA	
	Non-domestic 非住用	6	
	Composite 綜合用途	NA	

(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	NA	m 米 <input type="checkbox"/> (Not more than 不多於)
		NA	mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		NA	Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於) (<input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
	Non-domestic 非住用	3 - 7	m 米 <input checked="" type="checkbox"/> (Not more than 不多於)
		NA	mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		1 - 2	Storeys(s) 層 <input checked="" type="checkbox"/> (Not more than 不多於) (<input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
	Composite 綜合用途	NA	m 米 <input type="checkbox"/> (Not more than 不多於)
		NA	mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		NA	Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於) (<input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
(iv) Site coverage 上蓋面積	33 % <input checked="" type="checkbox"/> About 約		
(v) No. of units 單位數目	NA		
(vi) Open space 休憩用地	Private 私人	NA	sq.m 平方米 <input type="checkbox"/> Not less than 不少於
	Public 公眾	NA	sq.m 平方米 <input type="checkbox"/> Not less than 不少於

(vii) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數	54
	Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) Electric Taxi	21 (2.5m x 5m) NA NA NA NA 33 (2.5m x 5m)
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數	1
	Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明)	NA NA NA 1 (3.5m x 11m) NA NA

Submitted Plans, Drawings and Documents 提交的圖則、繪圖及文件		
	Chinese 中文	English 英文
Plans and Drawings 圖則及繪圖		
Master layout plan(s)/Layout plan(s) 總綱發展藍圖／布局設計圖	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Block plan(s) 樓宇位置圖	<input type="checkbox"/>	<input type="checkbox"/>
Floor plan(s) 樓宇平面圖	<input type="checkbox"/>	<input type="checkbox"/>
Sectional plan(s) 截視圖	<input type="checkbox"/>	<input type="checkbox"/>
Elevation(s) 立視圖	<input type="checkbox"/>	<input type="checkbox"/>
Photomontage(s) showing the proposed development 顯示擬議發展的合成照片	<input type="checkbox"/>	<input type="checkbox"/>
Master landscape plan(s)/Landscape plan(s) 園境設計總圖／園境設計圖	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他 (請註明)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Location Plan, Lot Index Plan, Plan showing Vehicular Access to the Application Site, Photos of Electric Charging Device and Site Photos		
Reports 報告書		
Planning Statement/Justifications 規劃綱領/理據	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Environmental assessment (noise, air and/or water pollutions) 環境評估 (噪音、空氣及／或水的污染)	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on vehicles) 就車輛的交通影響評估	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Traffic impact assessment (on pedestrians) 就行人的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Visual impact assessment 視覺影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Landscape impact assessment 景觀影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Tree Survey 樹木調查	<input type="checkbox"/>	<input type="checkbox"/>
Geotechnical impact assessment 土力影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Drainage impact assessment 排水影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Sewerage impact assessment 排污影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Risk Assessment 風險評估	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他 (請註明)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Drainage Proposal		

Note: May insert more than one '✓', 註：可在多於一個方格內加上「✓」號

Note: The information in the Gist of Application above is provided by the applicant for easy reference of the general public. Under no circumstances will the Town Planning Board accept any liabilities for the use of the information nor any inaccuracies or discrepancies of the information provided. In case of doubt, reference should always be made to the submission of the applicant.

註：上述申請摘要的資料是由申請人提供以方便市民大眾參考。對於所載資料在使用上的問題及文義上的歧異，城市規劃委員會概不負責。若有任何疑問，應查閱申請人提交的文件。

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羅 迅 測 計 師 行
Lawson David & Sung
S U R V E Y O R S L I M I T E D
Property Consultants • Planning • Valuers • Auctioneers
Estate Agency Licence No. C-006328

Ref: LDS/PLAN/7139

Section 16 Planning Application

**Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office for a Period of 5 Years at
Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government Land,
Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T.**

Planning Statement

Applicant

Wealth Generation International Limited (新世代國際有限公司)

Prepared by

Lawson David & Sung Surveyors Limited

August 2025

Executive Summary

This planning statement is prepared in support of a planning application for proposed temporary electric vehicle charging station, vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office (“the proposed development”) for a period of 5 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (the “Application Site”).

The Application Site, covering an area of about 4,180 sq.m. (including Government Land of about 680 sq.m.), falls within an area zoned “Open Storage” (“OS”) on the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14 gazetted on 12.3.2010. According to the Notes of the OZP, the proposed EV charging station is a ‘Petrol Filling Station / Green Fuel Station’, which is a ‘Column 2’ use of the “OS” zone, along with the proposed ‘Shop and Services’ and ‘Eating Place’. Meanwhile, ‘Vehicle Repair Workshop’ is a Column 1 use, which is always permitted.

The Application Site is currently occupied by a cluster of vehicle repair workshops, warehouses and temporary structures. In consideration of the growing popularity of the electric vehicles (EVs) in Hong Kong and introduction of 3,000 electric taxis expected in 2026, the Applicant, in cooperation with one of the selected taxi fleet operators, intends to convert the Application Site into an one-stop EV service depot that includes charging station to provide EV charging facilities for electric taxis with supplementary services.

The proposed development will provide a total of 54 parking spaces (2.5m x 5m), with 33 for electric taxis, 15 for motor vehicle showroom and office use and 6 for staff/visitors. Among the proposed parking spaces for electric taxis, 18 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) quick chargers with an output power of 120kW. The remaining 15 parking spaces will be designated as parking / waiting spaces for the charging facilities and free of charge.

Three 1-storey (height: about 3m) containers for EV chargers, each with a floor area of about 29 sq.m., will be situated at the northern boundary of the Application Site. Each container will provide 6 EV charging devices for electric taxis. A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices. Meanwhile, a 2-storeys (height: about 7m) temporary structure (G/F: vehicle repair workshop; 1/F: eating place with ancillary office) with a total floor area of about 2,012 sq.m. will be erected at the western portion of the Application Site. A 1-storey (height: about 5m) motor vehicle showroom with a floor area of about 225 sq.m. will also be proposed next to the 2-storey structure.

The existing ingress/egress point of about 8m wide will be maintained at the southern boundary of the Application Site, connecting to Sha Tau Kok Road – Lung Yeuk Tau. The proposed hours of operation of the EV charging station at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays), while other supporting facilities will be operated from 7am to 8pm daily.

The justifications of this application are:

1. The proposed development is in line with Government policy to promote EVs usage;
2. The proposed development meets the demand for quick EV charging facilities and supplementary services for EVs;
3. The proposed development does not contravene the planning intention of “OS” zone;
4. The proposed development is considered not incompatible with surrounding land uses;
5. The proposed development will promote optimization of valuable land resources;
6. The proposed development will not hinder future development of New Territories North New Town; and
7. The proposed development will not generate adverse traffic, drainage and environmental impacts on the surrounding areas.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 5 years.

行政摘要

此規劃報告書旨在支持在新界粉嶺沙頭角公路 - 龍躍頭段丈量約份第 83 約補租地段第 464 號 A 分段餘段(部分)及毗連政府土地 (“申請地點”), 作為期五年的擬議臨時電動車充電站(電動的士)、汽車修理工場、商店及服務行業(汽車陳列室)及食肆連附屬辦公室用途(“擬議發展”)的規劃申請。

申請地點的面積約 4,180 平方米 (包括政府土地約 680 平方米), 座落於在 2010 年 3 月 12 日刊憲的坪輦及打鼓嶺分區計劃大綱核准圖 (圖則編號: S/NE-TKL/14) 上的「露天貯物」地帶。根據該大綱圖的註釋, 擬議電動車充電站為「加油站 / 潔淨能源站」, 跟擬議「商店及服務行業」及「食肆」均屬「露天貯物」地帶內的第二欄用途, 需先向城市規劃委員會(“城規會”)提出申請。同時, 擬議「汽車修理工場」屬「露天貯物」地帶內的第一欄用途, 為經常准許的用途。

申請地點現時用作多個汽車修理工場、貨倉及臨時構築物。考慮到本地電動車日趨普及以及政府將於 2026 年引入 3,000 輛電動的士, 申請人將與一支被政府選中的的士車隊合作, 擬將申請地點改成一站式電動車服務充電站, 為電動的士提供充電設施以及附屬服務。

擬議電動車充電站將提供 54 個的停車位(2.5 米 x 5 米), 其中 33 個供電動的士停泊, 15 個供汽車陳列室及辦公室使用, 以及 6 個供職員及訪客使用。在擬議的電動的士停車位中, 18 個停車位將會是配備電動車充電設備的收費停車位, 所有充電裝置均是輸出功率為 120kW 的直流快充裝置。剩餘的 15 個停車位將用作電動的士的停車 / 等候區, 並免費使用。

申請地點北面將設置三個一層高(高度約 3 米, 樓面面積各約 29 平方米)的改裝貨櫃, 以容納電動車充電裝置, 每個貨櫃可提供 6 個電動的士充電器。貨櫃之間將提供一個一層高(高度約 3.5 米, 樓面面積約 53 平方米)的電力變壓器房 / 掣房, 為所有充電設備提供足夠的電力。同時, 申請地點西面將提供一座兩層高(高度約 7 米, 總樓面面積約 2,012 平方米)的臨時構築物(地下為汽車維修工場; 一樓為食肆及附屬辦公室)。在擬議兩層高的構築物旁亦會提供一座一層高(高度約 5 米, 樓面面積約 225 平方米)的汽車陳列室。

申請地點將沿用現時設於南面(約 8 米闊)的出入口, 連接到沙頭角公路 - 龍躍頭段。擬議電動車充電站的營運時間為星期一至日(包括公眾假期) 24 小時運作, 至於其他附屬設施的運作時間為星期一至日上午 7 時至晚上 8 時。

本規劃申請的理據為:

1. 擬議發展支持政府推動電動車普及化的政策;
2. 擬議發展能滿足對電動車快速充電設施以及附屬服務的需求;
3. 擬議發展不會違背「露天貯物」地帶的規劃意向;
4. 擬議發展與周邊土地用途兼容;
5. 擬議發展可有效利用寶貴的土地資源;
6. 擬議發展不會阻礙未來新界北新市鎮的發展; 及
7. 擬議發展不會對附近的交通、排水及環境構成不良的影響。

基於本規劃報告書所述的理據, 敬希 各城規會委員及有關政府部門能批准此項申請, 作為期五年的臨時用途。

TABLE OF CONTENTS

Page

1. INTRODUCTION	1
1.1 Purpose	
1.2 Background of the Application	
1.3 Organization of the Planning Statement	
2. SITE CONTEXT	2
2.1 The Application Site and Its Existing Condition	
2.2 Surrounding Land Uses	
2.3 Accessibility	
2.4 Land Status	
3. PLANNING CONTEXT	3
3.1 Outline Zoning Plan	
3.2 Previous Application	
3.3 Similar Application	
4. DEVELOPMENT PROPOSAL	3
4.1 Site Configuration and Layout	
4.2 Site Operations	
4.3 Traffic Arrangement and Traffic Impact Assessment	
4.4 Drainage Proposal	
4.5 Landscape	
4.6 Fire Service Installations	
4.7 Sewage Treatment	
4.8 Environmental Consideration	
5. PLANNING JUSTIFICATIONS	6
5.1 In Line with Government Policy to Promote EVs Usage	
5.2 Meeting the Demand for Quick EV Charging Facilities and Supplementary Services for EVs	
5.3 Not Contravene with the Planning Intention of "OS" Zone	
5.4 Not Incompatible with Surrounding Land Uses	
5.5 Optimization of Valuable Land Resources	
5.6 Will not Hinder Future Development of New Territories North New Town	
5.7 No Adverse Impacts on the Surrounding Areas	
6. CONCLUSION	8

List of Figures

Figure 1	Location Plan
Figure 2	Lot Index Plan
Figure 3	Vehicular Access to the Site
Figure 4	Indicative Layout Plan
Figure 5	Photos of Electric Charging Device

Site Photos

List of Appendices

Appendix 1	Endorsement Letter from the Transport Department
Appendix 2	Traffic Impact Assessment
Appendix 3	Drainage Proposal

1. Introduction

1.1 Purpose

Pursuant to Section 16 of the Town Planning Ordinance, this Planning Statement Report is submitted to the Town Planning Board (“the Board”) in support of a planning application for proposed temporary electric vehicle charging station (for electric taxi), vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office (“the proposed development”) for a period of 5 years at Taxlord Lot 464 S.A RP (Part) in D.D.83 and Adjoining Government land, Sha Tau Kok Road - Lung Yeuk Tau, Fanling, N.T. (hereafter referred to as “the Application Site”). **Figure 1** shows the location of the Application Site and the Lot Index Plan indicating the concerned lot is shown per **Figure 2**.

The Application Site, covering an area of about 4,180 sq.m. (including Government land of about 680 sq.m.), falls within an area zoned “Open Storage” (“OS”) on the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14 gazetted on 12.3.2010. According to the Notes of the OZP, the proposed EV charging station is a ‘Petrol Filling Station / Green Fuel Station’, which is a ‘Column 2’ use of the “OS” zone, along with the proposed ‘Shop and Services’ and ‘Eating Place’. Meanwhile, ‘Vehicle Repair Workshop’ is a Column 1 use, which is always permitted.

The Application Site is currently occupied by a cluster of vehicle repair workshops, warehouses and temporary structures. In view of the growing demand for electric vehicles (EVs), the Applicant intends to convert the Application Site into an EV service depot, which includes an EV charging station to provide EV charging spaces for electric taxi, as well as supplementary uses for EV such as a vehicle repair workshop, a motor vehicle showroom, an eating place with ancillary office. The Application Site is considered a preferred location for the proposed development.

1.2 Background of the Application

In response of combating global climate change, electrification of transportation system has become a global trend. EVs are considered a preferable choice to conventional vehicles with no tailpipe emissions of air pollutants, which can help improve air quality and reduce greenhouse gas emissions.

To drive Hong Kong towards the vision of “Zero Carbon Emissions ● Clean Air ● Smart City”, the Government has announced the “Hong Kong Roadmap on Popularisation of EV” on 17 March 2021, which outlines the long-term objectives to achieve zero vehicular emissions before 2050 and promote wider or eventual full adoption of EVs in Hong Kong. The roadmap includes measures such as phasing out conventional fuel-propelled vehicles by 2035, expanding EV charging network in both private and public sectors, enhancing education and training programs for maintenance services and introducing trials of electric commercial vehicles for public transportations.

On 10 December 2024, the Government has also announced the “Green Transformation Roadmap of Public Buses and Taxis” to expand the adoption of EVs in public transportation with introduction of 3,000 electric taxis by mid-2026. The Government has also highlighted a multi-pronged approach in promoting the expansion of EV charging facilities, so as to gradually develop a territory-wide network for ensuring sufficient and convenient charging support for suiting the needs of various EVs.

As at February 2025, the number of EVs account for about 12.6% of the total number of vehicles in Hong Kong, and is expected to increase with the ongoing promotion and campaigns by the Government. In the Chief Executive’s 2024 Policy Address, the Government has also reiterated their efforts to expand the charging network for EVs with new scheme to encourage private sector to install quick-charging facilities. It is targeted to have a total of 3,000 quick chargers installed by 2030.

Additionally, the Government has announced 5 selected taxi fleet operators to enhance taxi service quality and reform the taxi trade on 3 March 2025, in which all selected operators will provide electric

taxis to promote green transport in Hong Kong.

In support of the Government's initiatives, the Applicant, in partnership with one of the selected taxi fleet operators (see endorsement letter from the Transport Department at **Appendix 1**), intends to convert the Application Site into an EV service depot that would include an EV charging station to provide charging spaces for electric taxi with other supplementary uses for EV. All chargers will be direct current (DC) chargers with output power of 120kW to increase the charging efficiency.

1.3 Organization of the Planning Statement

This planning statement is divided into 6 chapters. Chapter 1 is an introduction outlining the above background of the planning application. Chapter 2 will then illustrate the site context and land status followed by Chapter 3 which describes the planning context in details. Chapter 4 gives the particulars of the proposed development. Detailed accounts of planning justifications of the proposed development are presented in Chapter 5. The planning statement finally concludes with a summary in Chapter 6.

2. Site Context

2.1 The Application Site and Its Existing Condition

The Application Site, with an area of about 4,180 sq.m. (including Government land of about 680 sq.m.), is located next to Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T. The Application Site is a piece of flat land, hard-paved and currently occupied by a cluster of vehicle repair workshops, warehouses and temporary structures. The Application Site is fenced off by chain-link fence and metal sheets of about 2.5m high (see **Site Photos**). All the existing temporary structures will be demolished.

2.2 Surrounding Land Uses

The surrounding areas are predominantly rural in character and comprise of warehouses, logistics centre, open storage/storage yards, vehicle repair workshops, parking of vehicles and village houses. To the immediate west is a temporary logistics centre (Application No. A/NE-TKL/708) within the same "OS" zone approved by the Board on 17.3.2023. To the further north and northwest are clusters of warehouses, open storage/storage yards and industrial premises within the "OS" zone. To the south across Sha Tau Kok Road – Lung Yeuk Tau are a series of vehicle repair workshops and industrial premises, and the village settlements of Kwan Tei. To the east are Grade 2 historic buildings, Sam Tung Uk.

2.3 Accessibility

The Application Site is directly accessible via Sha Tak Kok Road – Lung Yeuk Tau. The existing vehicular access will be maintained by the Applicant. A plan showing the vehicular access to the Application Site is provided at **Figure 3**.

2.4 Land Status

According to the records of the Land Registry, the subject lot is held under Block Government Lease and is demised as agricultural use with lease term for 75 years, from 1.7.1898 and is renewable for a further term of 24 years.

The subject lot is an old schedule agricultural lot. The Applicant will apply for a Short Term Waiver (STW) for the proposed structures and a Short Term Tenancy (STT) to regularize the use of the Government land to the Lands Department upon obtaining planning approval for this application.

3. Planning Context

3.1 Outline Zoning Plan

The Application Site currently falls within an area zoned “Open Storage” (“OS”) on the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14 gazetted on 12.3.2010 (see **Figure 1**).

The planning intention of “OS” zone is “primarily for the provision of land for appropriate open storage uses and to regularize the already haphazard proliferation of open storage uses. It provides for the orderly development of land for open storage uses that cannot be accommodated in conventional godown premises”

According to the Notes of the OZP, the proposed EV charging station is a ‘Petrol Filling Station / Green Fuel Station’, which is a ‘Column 2’ use of the “OS” zone, along with the proposed ‘Shop and Services’ and ‘Eating Place’, and requires planning permission from the Board. Meanwhile, ‘Vehicle Repair Workshop’ is a Column 1 use, which is always permitted.

3.2 Previous Application

The Application Site is not subject to any previous planning application.

3.3 Similar Application

There is no similar application within the same “OS” zone under the Ta Kwu Ling and Ping Che OZP.

4. Development Proposal

4.1 Site Configuration and Layout

The Applicant proposes to use the Application Site for a temporary EV service depot (including EV charging station, vehicle repair workshop, motor vehicle showroom, eating place and ancillary office) for a period of 5 years. The proposed development intends to provide charging space for electric taxis of a taxi fleet and supplementary services for EV as an one-stop service depot.

The Application Site has an area of about 4,180 sq.m. The proposed development will provide a total of 54 parking spaces (2.5m x 5m), with 33 for electric taxis. Among the electric taxi parking space, 18 of them will be fee-paying parking spaces and equipped with EV charging device, with all being direct current (DC) ultra-fast chargers with output power of 120kW. The remaining 15 parking spaces will be designated as parking / waiting spaces for the charging facilities and will be free of charge (see Layout Plan at **Figure 4**).

Three 1-storey (height: about 3m) containers for placements of EV chargers, each with a floor area of about 29 sq.m., will be situated at the northern boundary of the Application Site. Each container will provide 6 EV chargers for electric taxis (see **Figure 5**). A 1-storey (height: about 3.5m) transformer room / switch room with a floor area of about 53 sq.m. will be placed in between the containers to provide sufficient electricity to all charging devices.

The proposed development will also provide supplementary services for EV. A 2-storeys (height: about 7m) temporary structure with a total floor area of about 2,012 sq.m. will be erected at the western portion of the Application Site. The ground floor of the proposed structure will be used as a vehicle repair workshop for EV while the first floor will be used as an eating place for taxi drivers with an ancillary office for staff. Meanwhile, a 1-storey (height: about 5m) motor vehicle showroom with a floor area of about 225 sq.m. will be proposed next to the 2-storey structure. Additional 21 parking

spaces (2.5m x 5m) for motor vehicle showroom and office use and staff/visitors, and 1 loading/unloading space (3.5m x 11m) for medium goods vehicle will also be provided for the aforementioned uses.

The Application Site will be fenced off by 2.5m high metal sheets on all sides. Only electric taxis and private vehicles with valid licenses issued under the Road Traffic (Registration and Licensing of Vehicles) Regulations are allowed to be parked on the Application Site.

The key development parameters of the application are shown below:-

Applied Use	Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Service (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years
Site Area	About 4,180 sq.m. (including 680 sq.m. of Government land)
Total Floor Area (Non-domestic)	About 2,377 sq.m.
No. of Structure	6 <ul style="list-style-type: none"> - 1 proposed 1-storey motor vehicle showroom - 1 proposed 2-storey vehicle repair workshop, eating place with ancillary office - 1 proposed transformer room / switch room - 3 proposed containers for EV chargers
Height of Structure	About 3 – 7m (1-2 storeys)
No. of Parking Spaces	54 Nos. (2.5m x 5m) <ul style="list-style-type: none"> - 18 EV charging spaces for electric taxis - 15 parking / waiting spaces for electric taxis - 15 parking spaces for motor vehicle showroom and office use - 6 parking spaces for staff/visitors
No. of Loading/Unloading Spaces	1 no. for medium goods vehicle (3.5m x 11m)

4.2 Site Operations

The proposed development will only provide parking and EV charging facilities for electric taxis, and supplementary services for EVs. All EV chargers onsite will be quick chargers that will provide electric taxis 600km of mileage when charged for about 30 minutes.

The Applicant will collaborate with Sino Development (International) Company Limited, one of the 5 selected taxi fleet operators by the Government, to provide various quick EV charging facilities in various locations in Hong Kong (see **Appendix 1**). The fee arrangement will be similar to other EV charging station in the market, in which users will be charged at time basis as they go. A mobile application catered for electric taxi drivers will be adopted, which will include the payment platform for EV charging and provide real-time availability of EV charging spaces at the EV charging stations managed by the Applicant.

The proposed hours of operation of the EV charging station at the Application Site are 24 hours daily from Mondays to Sundays (including public holidays). Meanwhile, shorter operating hours (Mondays to Sundays 7am to 8pm) will be proposed for the supplementary services (i.e. vehicle repair workshop, motor vehicle showroom, eating place and ancillary site office). To minimize any potential noise and environmental impacts to the surrounding area, all workshop activities will be contained within the proposed enclosed structure. No land filling work will be conducted at the Application Site.

4.3 Traffic Arrangement and Traffic Impact Assessment

The Application Site can be accessed via Sha Tau Kok Road – Lung Yeuk Tau (see **Figure 3**). The existing ingress/egress point at the southern boundary of about 8m wide will be maintained by the Applicant. A Traffic Impact Assessment (TIA) report (see **Appendix 2**) is submitted to assess the potential traffic impact on the existing and future road network.

To analyze the existing traffic conditions, a traffic count survey was conducted on 22 May 2025 at the nearby critical links and junctions. The peak hour was identified to be between 1630 - 1730. Based on the existing traffic flows, all concerned junctions were performing satisfactorily and all concerned road sections were operating with spare capacity during the peak hour.

A swept path analysis is also included in the TIA to demonstrate that satisfactory manoeuvring of vehicles entering to and existing from the Application Site and manoeuvring within the Application Site. There will be no difficulties in internal traffic circulation sense as sufficient spaces for vehicle manoeuvring have been reserved within the Application Site such that no vehicle has to queue back to or reverse onto/from Sha Tau Kok Road – Lung Yeuk Tau.

As identified in the TIA report, the peak traffic generation of the proposed development will be either between 1500 – 1700 or after midnight, before the shift changes of the electric taxi drivers. The proposed development is expected to generate a two-way traffic of 87 pcu/hour during the peak hour. By analyzing the Year 2030 Design Flows, which were derived from applying a nominal growth to the existing traffic flow plus the additional traffic generated and attracted by the proposed development, both the junction and link capacity assessments show that the concerned junction and road links would perform satisfactorily during the peak hour. Based on the assessment result, it can be concluded that the proposed development will not induce additional adverse traffic impact on the surrounding road network and is considered acceptable from traffic engineering point of view.

The mobile application adopted by the Applicant will provide real-time availability of EV charging space and facilitate electric taxi drivers to locate the available EV chargers. The drivers will be directed to the nearest alternative sites if all the EV chargers in the proposed EV charging station are in use. In addition, since quick chargers will be used in the Application Site, the charging time will be much shorter than standard chargers, which allow high turnover rate and cut down waiting time. 15 parking / waiting spaces have been reserved in the Application Site to allow drivers to wait for EV charging space. Therefore, no electric taxi will be queuing outside the Application Site along Sha Tau Kok Road – Lung Yeuk Tau.

4.4 Drainage Proposal

Drainage facilities including 525mm peripheral U-channels and catchpits with trap are proposed to collect the surface runoff and divert them to the existing surface channels at Ng Tung River to the north of the Application Site. A 100mm high gap will also be proposed at the bottom of the security hoarding/fence wall along the southern boundary to ensure no surface runoff from the surroundings flowing onto the Application Site to be obstructed. A drainage proposal (see **Appendix 3**) is submitted to show the proposed drainage design. The Applicant will provide the drainage facilities to the satisfaction of Drainage Services Department.

4.5 Landscape

Majority of the Application Site is occupied by the existing vehicle repairing workshops, warehouses and temporary structures while wild overgrown and some trees are found at the vacant land. No old and valuable trees are found within the site. Meanwhile, existing trees of various species are found immediate outside the eastern boundary. These existing trees outside the site boundary will not be disturbed by the Applicant and would act as the natural hedges bounding the edge of the development.

The Application Site is located within an area of rural character intermixed with warehouses, logistics centre, open storage/storage yards, vehicle repair workshops, parking of vehicles and village houses. Since the proposed development intends to provide an EV service depot for EVs with EV charging facilities for electric taxis only, it is considered not incompatible with the landscape character of the surrounding areas.

4.6 Fire Service Installations Proposal

To minimize the fire hazard, the Applicant will provide fire service installations (FSIs) with Firemen's Emergency Switch to the satisfaction of the Fire Services Department.

4.7 Sewage Treatment

The Applicant will provide septic tank and soakaway pits according to Professional Persons Environmental Consultative Committee Practice Notes ("ProPECC PN") No.1/23 for sewage treatment and dispose at the Application Site. All waste water collected from the kitchen, including that from basins, sinks and floor drains, will be discharged via a grease trap in accordance with this ProPECC PN No.1/23 to prevent any water pollution.

4.8 Environmental Consideration

The nature of the proposed use will merely involve provision of EV charging facilities with supplementary services for EV. To minimize the possible environmental nuisance, the Applicant will follow the 'Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites' and undertake the following mitigation measures:-

- (a) No more than 33 electric taxis and 21 private vehicles will be allowed at the Application Site at the same time;
- (b) All vehicle repairing and workshop activities will be conducted within the proposed enclosed structure at the Application Site;
- (c) The operating hours for the supplementary services of the proposed development will be restricted from 7am to 8pm on Mondays to Sundays;
- (d) No land filling works will be conducted at the Application Site; and
- (e) The Application Site will be fenced off by 2.5m high metal sheets on along the site boundary.

5. Planning Justifications

5.1 In Line with Government Policy to Promote EVs Usage

The proposed development would provide quick EV charging facilities for electric taxis. It aligns with the Government's initiative to expand the charging network for EVs in Hong Kong, especially in meeting the target of installing 3,000 quick chargers by 2030. With the introduction of 3,000 electric taxis expected in mid-2026, the proposed development can also meet the increasing demand for EV charging spaces catered for electric taxis in Hong Kong. Additionally, the Applicant has received support from the Transport Department to collaborate with one of the selected taxi fleet operators to provide EV charging facilities in Hong Kong (see **Appendix 1**). Therefore, the proposed development is considered in line with Government policy to promote the wider use of EVs and support the operations of electric commercial vehicles and achieve the aforesaid target.

5.2 Meeting the Demand for Quick EV Charging Facilities and Supplementary Services for EVs

In view of the emerging popularity of commercial EVs with the Government's initiative, the demand for EV charging space for commercial EVs will increase as well. At present, the nearest EV charging facilities in the locality is located within Queens Hill Estate, which only provide standard chargers for

private vehicles and are considered too slow for electric taxis. Since the proposed development will provide EV charging spaces with quick chargers, it can meet the demand of electric taxis for quick charging EV facilities.

Additionally, the proposed development will also provide supplementary services (i.e. vehicle repair workshop and motor vehicle showroom) for EVs. With the rising popularity of EVs, it can also satisfy the demand of supplementary services for EVs in the market.

5.3 Not Contravene with the Planning Intention of “OS” Zone

The Application Site falls within an area zoned “OS” on the Ping Che and Ta Kwu Ling OZP No. S/NE-TKL/14. The proposed development will provide EV charging station for electric taxi with supplementary uses, which will provide a more orderly development at the Application Site. On top of provision of EV charging facilities for electric taxis, the proposed development will also provide repair and maintenance services for EVs, which is a ‘Column 1’ use of the “OS” zone. Therefore, the proposed development will not contravene with the planning intention of the subject “OS” zone. Due to the temporary nature of the Application, the proposed development will also not jeopardize the long-term planning intention of the “OS” zone.

5.4 Not Incompatible with Surrounding Land Uses

The proposed development is considered not incompatible with the surrounding land uses. The surrounding areas comprise of warehouses, logistics centre, open storage/storage yards, vehicle repair workshops, parking of vehicles and village houses. Since the proposed development is intended to serve electric taxis, the proposed development could co-exist well with the existing surrounding land uses without inflicting any environmental impacts. As the proposed development is considered not incompatible with other developments/facilities in the adjacent areas in terms of nature and scale of use, approval of the application would therefore not result in any interface problems with the surrounding areas.

5.5 Optimization of Valuable Land Resources

The Application Site is considered a suitable and preferred venue for the proposed development. The Application Site is a piece of flat land with majority of the land being paved, in which the conversion into the proposed development will be smooth. The Applicant will demolish all the existing structures and convert the Application Site into an orderly managed development. The proposed development will also improve the currently under-utilized adjoining Government land, in which the Applicant will manage the site and therefore improve the existing condition.

In addition, the Application Site is directly accessible to Sha Tau Kok Road – Lung Yeuk Tau, which is connected to strategic road networks to different parts of New Territories, electric taxis could easily access and leave the site after charging. Meanwhile, the Application Site is situated close to densely populated areas such as Sheung Shui and Fanling and future new towns such as Kwu Tung and Ping Che. The provision of EV charging spaces for electric taxis and supplementary services for EVs, at the Application Site will be able to meet the growing demand for EV charging facilities and EV maintenance services in these areas. The proposed development is therefore considered fully commensurate with its local geographical settings and ideal to attain utmost land use maximization.

5.6 Will not Hinder Future Development of New Territories North New Town

The Application Site falls within the project boundary of New Territories North New Town. The Applicant understands that the proposed development will only be temporary and agrees to move out when the Application Site is resumed by the government for development of the New Town in the future. Due to the temporary nature of the Application, the proposed development will not hinder the future New Town development at the Application Site.

5.7 No Adverse Impacts on the Surrounding Areas

Owing to the nature and size of the proposed development, no adverse impacts are anticipated on the surrounding areas, as discussed below:

Traffic

The design of the proposed development has taken into account of vehicular circulation and manoeuvring. The Applicant will undertake traffic management measures to ensure pedestrian safety. Waiting spaces have been reserved within the Application Site to prevent any electric taxis from queuing outside the Application Site. Based on the submitted TIA (see **Appendix 2**), it is concluded that the proposed development will not induce significant traffic impact on the surrounding areas and is considered acceptable from traffic engineering point of view.

Drainage

The Applicant will provide drainage facilities within the Application Site for this application, in which surface runoff will be effectively collected from and discharged out of the Application Site. The Applicant has accordingly submitted a drainage proposal (see **Appendix 3**) to elaborate. Therefore, no adverse drainage impact on the surrounding areas is anticipated.

Environment

As the proposed development will only be used for EV charging station with provision of supplementary services for EVs, it would not generate adverse noise, air pollution or visual intrusion. In addition, all vehicle repairing and workshop activities will be contained within the proposed enclosed structure. The Applicant will also undertake relevant mitigation measures to minimize the possible environmental nuisance. Therefore, no adverse environmental impact on the surrounding areas is anticipated.

6. Conclusion

The Application Site falls within “OS” zone on the Ping Che and Ta Kwu Ling OZP No. S/NE-TKL/14. The Application Site is currently occupied by a cluster of vehicle repair workshops, warehouses and temporary structures and the Applicant intends to convert the Application Site into an EV service depot with EV charging facilities for electric taxis. In view of the site location, the existing site conditions and surrounding land uses, the proposed development is considered not incompatible with surrounding land uses and will not contravene with the planning intention of “OS” zone.

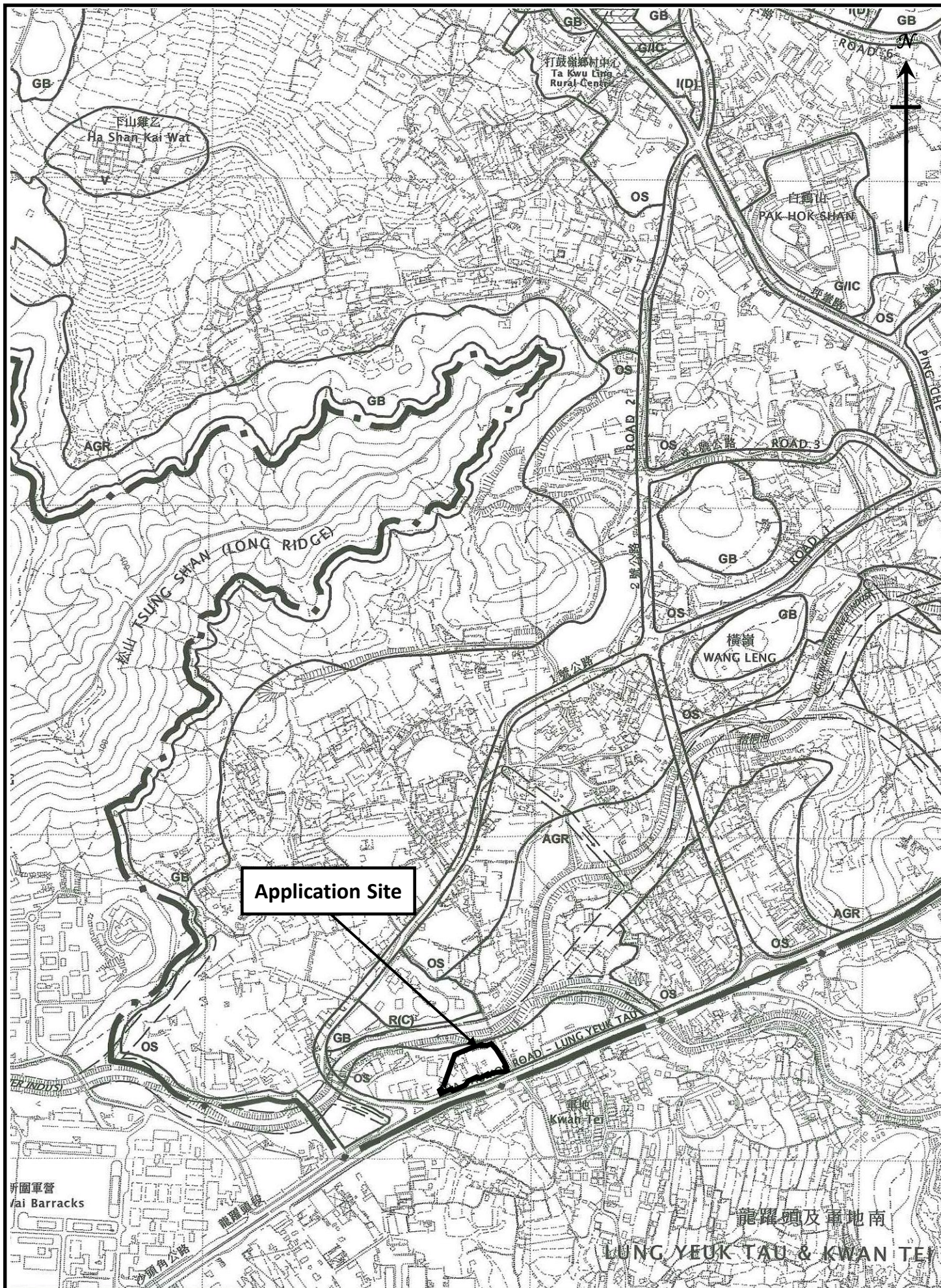
The Application Site is considered a suitable venue for the proposed development with its geographical settings and ideal to attain utmost land use maximization. Additionally, the proposed development will meet the demand for EV charging spaces and supplementary services for EVs, especially with provision of quick chargers for electric taxis. It therefore aligns with Government policy to expand EV charging networks and promote wider EVs usage in Hong Kong.

Meanwhile, the temporary nature of the proposed development will not hinder future development of New Territories North New Town. It is anticipated that the proposed development will not generate adverse impacts to the surrounding environment, and the technical concerns of relevant government departments could be addressed through the implementation of approval conditions.

In view of the justifications as presented in this planning statement, it is hoped that members of the Board and relevant Government departments will approve this application for a period of 5 years.

Figures

- Figure 1 Location Plan
- Figure 2 Lot Index Plan
- Figure 3 Vehicular Access to the Site
- Figure 4 Indicative Layout Plan
- Figure 5 Photos of Electric Charging Device



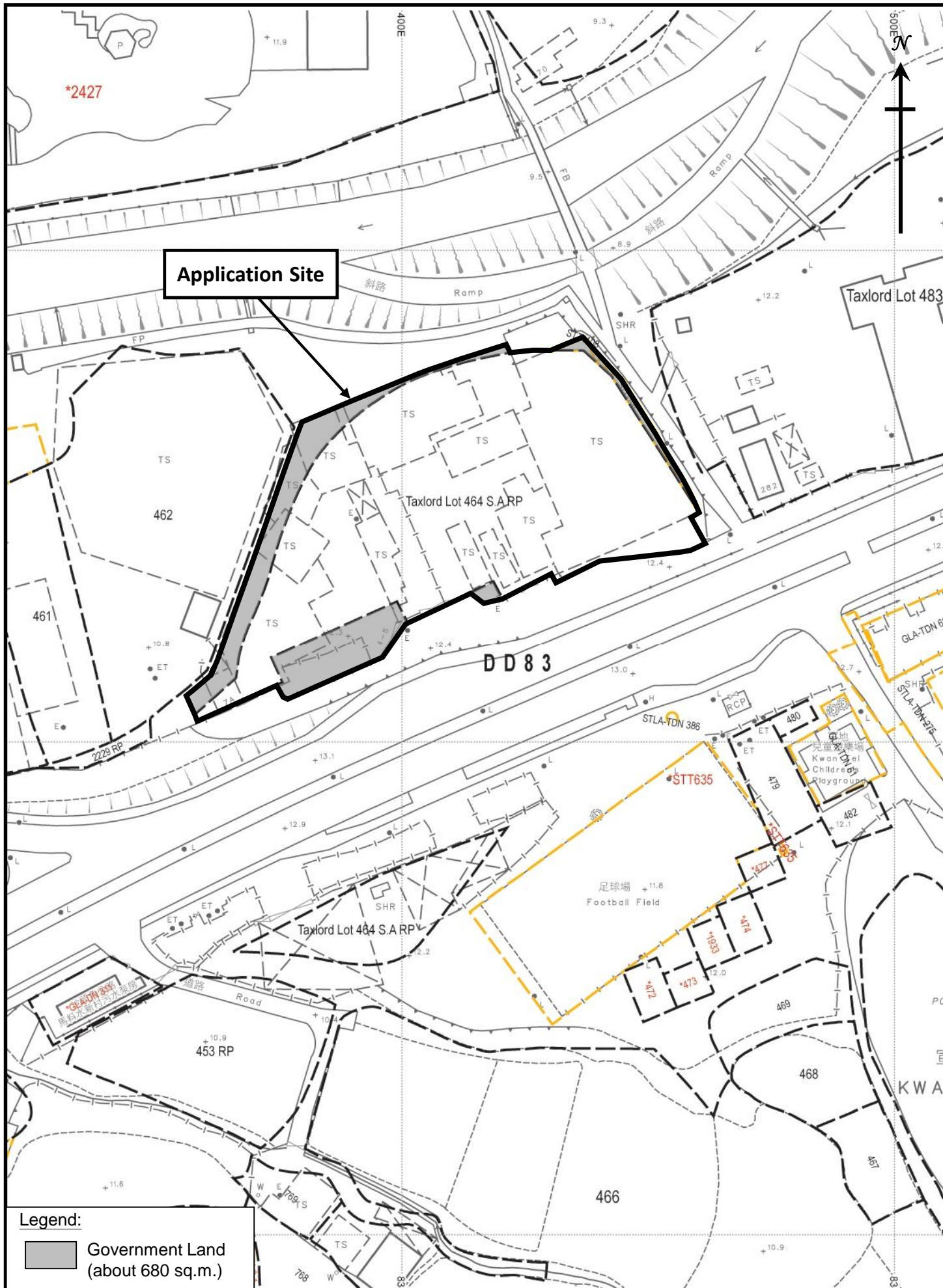
Location Plan
(Extract from Approved Ping Che and Ta Kwu Ling
Outline Zoning Plan No. S/NE-TKL/14)

Figure 1


1 : 7500



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Legend:

 Government Land
(about 680 sq.m.)

Lot Index Plan

Figure 2

1 : 1000



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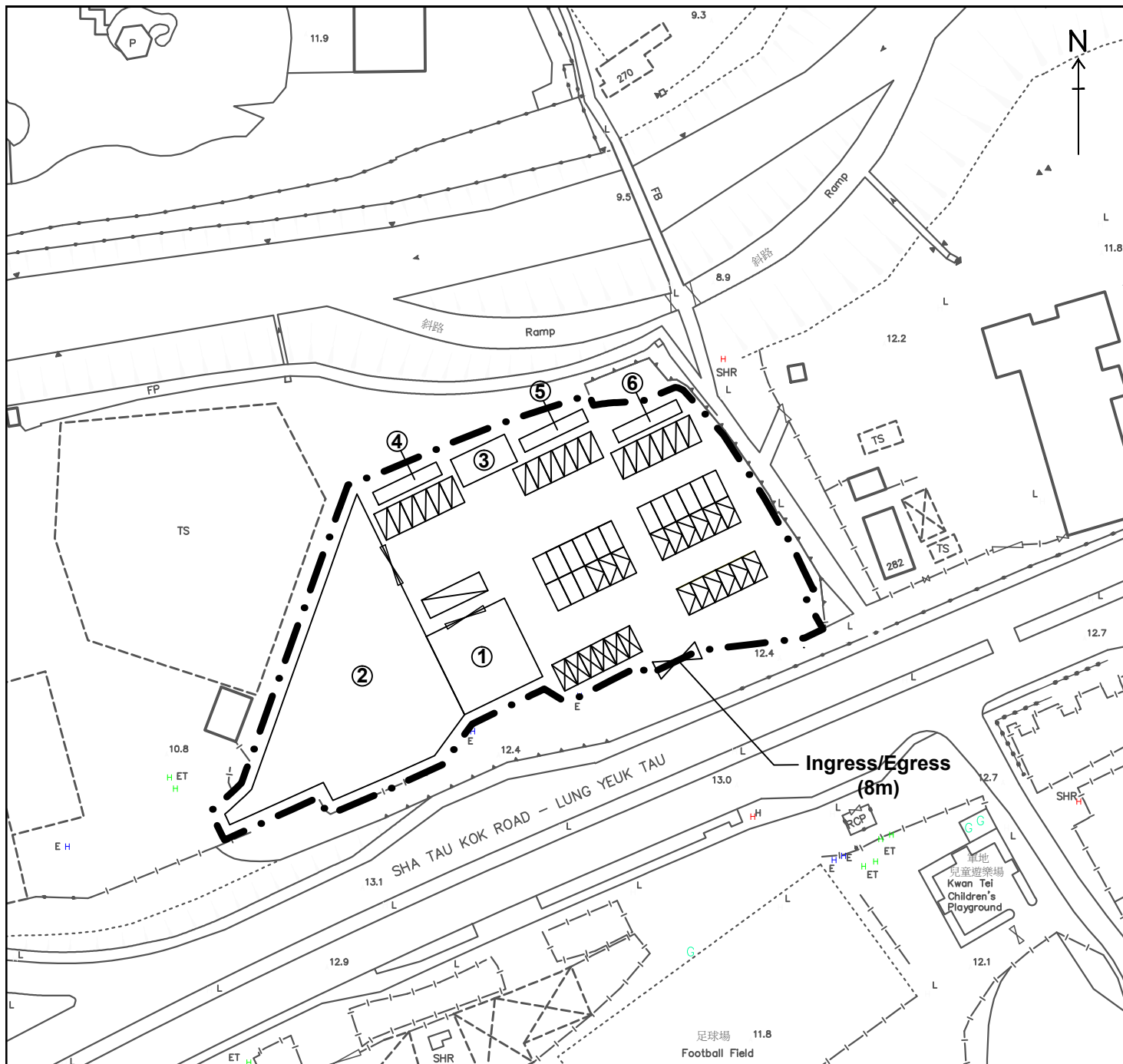
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1 : 5000





Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	1,006 806 + 200	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- EV Charing Spaces for Electric Taxi (18 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)

Layout Plan

Figure 4

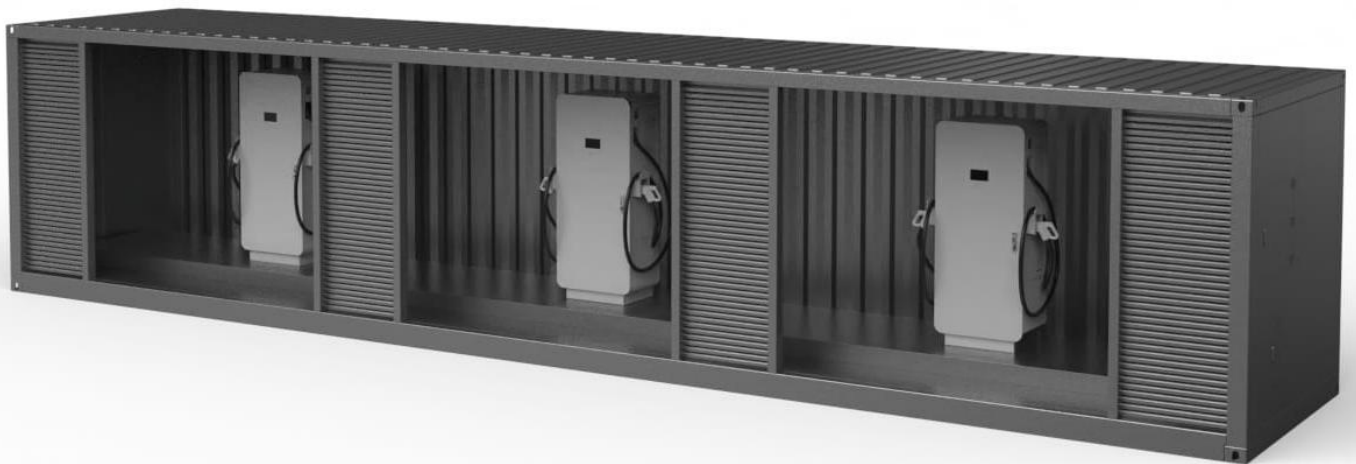
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Container for EV Chargers



EV Charging Device for Electric Taxis

For Reference Only

Photos of Electric Charging Device

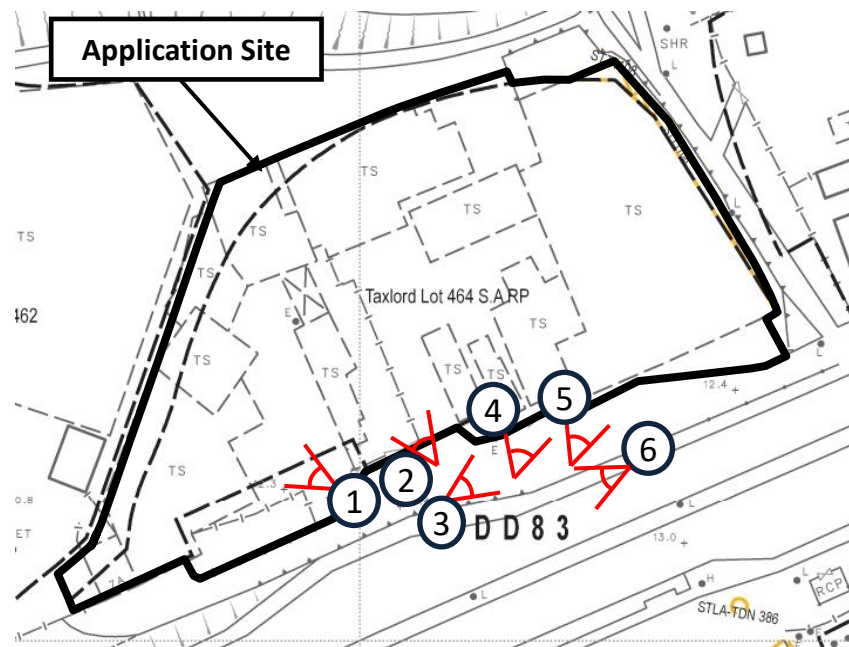
Figure 5

Not to Scale



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Site Photos



Legend:



Viewpoint of the Photo



Photo No.



Existing Condition of the Application Site

Site Photos

Appendix 1

Endorsement Letter from Transport Department



運輸署

Transport Department

本署檔號： 064-101-000-FP-006-005
電 話： 3842 6416
圖文傳真： 2824 2176

新世代國際有限公司

執事先生：

有關新科發展（國際）有限公司的電動車充電設施

香港政府早前推出一系列措施以提升個人化點對點交通服務的整體質素，當中包括引入的士車隊制度。政府希望藉着引入的士車隊制度，讓提供優質服務的的士車隊在行業中起示範和帶頭作用，透過市場演化逐漸改變行業生態，同時促進不同車隊，以及車隊和非車隊的士之間的良性競爭，鼓勵業界精益求精，提升服務水平和行業形象。

當中，新科發展（國際）有限公司（下稱「新科」）為其中一個獲選的的士車隊，並會分階段以1,000輛吉利幸福號電動車來營運。由於其車隊具一定規模，在投入服務後將對充電的配套有龐大的需求。本署支持新科與 貴公司合作，在全港不同位置發展電動車充電設施和配套，好讓其的士車隊能盡快投入服務。

如對上述事宜有任何查詢，請致電3842 6416與我聯絡。

運輸署署長

（李朝傑



代行）

2025 年 4 月 22 日

Appendix 2

Traffic Impact Assessment

Document Status Control Record

**Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office for a Period of 5 Years
at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.**

Traffic Impact Assessment Report

Originating Organisation: LLA Consultancy Limited Unit 610, 6/F Island Place Tower 510 King's Road North Point, Hong Kong	Prepared by: SKL	SKL	Date: 8 July 2025
	Approved by: SLN	SLN	Date: 8 July 2025
	Revision No.: -		Date of Issue: 8 July 2025

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1 INTRODUCTION

1.1 Background

- 1.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi), vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office for a period of 5 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T..
- 1.1.2 The Site is zoned as “Open Storage” use under the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14. A planning application is required for the proposed temporary use.
- 1.1.3 LLA Consultancy Limited was commissioned to undertake the TIA to support the planning application. This TIA report presents the findings of the study.

1.2 Objectives

- 1.2.1 The objectives of the study are as follows:
- to review the existing traffic conditions in the vicinity of the Site;
 - to estimate the traffic generation and attraction of the proposed temporary use;
 - to project the future traffic situations in the surrounding road network;
 - to appraise the potential traffic impact of the proposed temporary use; and
 - to recommend the internal transport facilities for the proposed temporary use.

2 THE PROPOSED DEVELOPMENT

2.1 The Site

- 2.1.1 As shown in **Figure 2.1**, the Site is located at Sha Tau Kok Road – Lung Yeuk Tau area. It has a total site area of about 4,180 m².

2.2 The Proposed Use and Internal Transport Facilities Provisions

- 2.2.1 The main function of the Site is to provide charging facilities for electric taxis with supporting facilities, including vehicle repair workshop, eating place and ancillary office. The key development parameters are listed out in **Table 2.1**.

Table 2.1 Key Development Parameters

Use	Quantity
Site Area	4,180 m ²
EV Charging Facilities for Electric Taxi only (2.5m x 5.0m)	18 nos.
Parking Spaces / Waiting Spaces for Electric Taxi only (2.5m x 5.0m)	15 spaces
Motor Vehicle Showroom, Vehicle Repair Workshop, Eating Place, Ancillary Office (Retail and Office Use)	2,237 m ²
Parking Space for Retail/Office Use @ 1 space per 150 m ² (2.5m x 5.0m)	15 spaces
Parking Space for Staff/Visitors (2.5m x 5.0m)	6 spaces
Loading/unloading Space for Medium Goods Vehicle (3.5m x 11.0m)	1 space

2.3 Swept Path Analysis

- 2.3.1 The Site is directly abutted to Sha Tau Kok Road – Lung Yeuk Tau, the development traffic is anticipated to enter and leave the Site with a left-in/left out movement onto Sha Tau Kok Road north-eastbound. In order to ensure the safety of pedestrians walking along the public footpath, a standard run-in/out in accordance with Highways Department's standards will be formed and flashing warning lights will be installed at both sides of the run-in/out as shown in **Figure 2.2**.
- 2.3.2 To ensure smooth manoeuvring of the parking area, swept path analysis was conducted to demonstrate that adequate space is provided for the vehicle's manoeuvring as shown in **Figure SP-01 – SP-02**.

3 EXISTING TRAFFIC SITUATION

3.1 Existing Road Network

- 3.1.1 The Site has a frontage along Sha Tau Kok Road – Lung Yeuk Tau and an existing run-in/out is provided for the current use of the Site.
- 3.1.2 Sha Tau Kok Road – Lung Yeuk Tau is a dual two lanes distributor road connecting Fanling and Sha Tau Kok. In 2023, it recorded an AADT of 22,810 vehicles.

3.2 Traffic Count Surveys

- 3.2.1 In order to appraise the existing traffic conditions, a traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30, the peak traffic generation period of the vehicle charging station. More details will be discussed in **Section 4.2**.
- 3.2.2 The locations of the surveyed junctions are shown in **Figure 3.1**.

J1 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road

J2 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road

- 3.2.3 The peak hour identified are 16:30 – 17:30. The observed 2025 traffic flows are presented in **Figure 3.2**.

3.3 Existing Junction Capacity Assessment

- 3.3.1 Based on the observed traffic flows, the performance of the junctions is assessed. It is calculated that all junctions are operating satisfactorily. The results are presented in **Table 3.1** and the detailed calculation sheets are attached in **Appendix A**.

Table 3.1 Existing Junction Capacity Assessment

Ref.	Junction Location	Type/Index ⁽¹⁾	Peak Hour of the Vehicle Charing Station
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.45
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.43

Note: (1) DFC = Design Flow to Capacity

3.4 Existing Link Capacity Assessment

- 3.4.1 The Volume to Capacity (V/C) Ratios of Sha Tau Kok Road – Lung Yeuk Tau is assessed and the results are presented in **Table 3.2**.

Table 3.2 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	Peak Hour Traffic Volume (pcu/hr)	Peak Hour V/C Ratio
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	980	0.96
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	294	0.29

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

- 3.4.2 As shown in **Table 3.2**, the concerned road sections are operating with spare capacity during the peak hour.

4 FUTURE TRAFFIC SITUATION

4.1 Design Year

- 4.1.1 The proposed use, if approved by the Town planning Board, will be valid for 5 years until 2030. Hence, Year 2030 is adopted as the design assessment year.

4.2 Traffic Generation of the Proposed Development

Traffic Generation for Electric Vehicle Charging Station

- 4.2.1 For the charging facilities, all EV chargers are direct current (DC) quick chargers with output power of 120kW which can provide 600km of mileage when an electric taxi charged for about 30 minutes.
- 4.2.2 From the taxi trade, the drivers are usually working on two shifts, i.e., the day shift and the night shift. In general, the day shift is 05:00 to 17:00 while the night shift is 17:00 to 05:00. The peak hour usage of charging facilities is expected to be the time periods before the shift such that a fully charged taxi can be passed to the driver in the next shift. As a result, the peak traffic generation of the proposed charging station will be either between 15:00 to 17:00 or after midnight. It is anticipated that minimal drivers will charge their taxis during the normal highway peak hours because these time periods will be the busiest hour in their business.
- 4.2.3 According to the Hong Kong Planning Standard and Guidelines, Section 3.9.4 of Chapter 12, a minimum of two waiting spaces should be provided in each EV charging station to avoid the awaiting vehicles to queue on public roads. In view of the size of the Site, 15 waiting spaces can be provided. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.
- 4.2.4 The estimated traffic generation and attraction for Electric Vehicle Charging Station during the peak operation hour will be a two-way traffic of 36 taxis (36 Nos. of vehicles = 18 EV charging stations x 2 because of 30 minutes charging/servicing time for each taxi).

Traffic Generation for Motor Vehicle Showroom, Vehicle Repair Workshop, Eating Place, Ancillary Office (Retail and Office Use)

- 4.2.5 For conservative assessment purpose, traffic generated/ attracted by these components of the proposed development would be estimated based on the PM trip rates for retail documented in TPDM Volume 1 Chapter 3 – Transport Considerations of Town Plans.
- 4.2.6 Based on the above, the traffic generation of the proposed development has been estimated and presented in **Table 4.1**.

Table 4.1 Development Traffic Generation

Proposed Use	Unit /Content	AM Peak Hour		
		Generation	Attraction	Total
Adopted Trip rates				
Retail ⁽¹⁾	pcu/hr/100m ²	0.3100	0.3563	-
Estimated Traffic Generation/Attraction				
EV Charging Station	18 stations	36 ⁽²⁾	36 ⁽²⁾	72 ⁽²⁾
Ancillary Office and Retail Use	2,235 m ²	7	8	15
Total		43	44	87

Notes: (1) TPDM mean trip rates for retail use are adopted.
(2) Traffic generation and attraction in Table 4.1 are adopted.

4.2.7 As shown in **Table 4.2**, the Site will generate a two-way traffic flows of 87 pcu/hour in the peak hour. The distribution of the development traffic is shown in **Figure 4.1**.

4.3 Traffic Growth

Annual Traffic Census (ATC) Data

4.3.1 Reference was made to the 2019 to 2023 Annual Traffic Census Reports, published by the Transport Department, to determine the natural traffic growth. The traffic data recorded at counting station in the vicinity of the Site is shown in **Table 4.2**.

Table 4.2 Annual Traffic Census Data

Stn. No.	Road Section			AADT (vehicle/day) ⁽¹⁾					Average Annual Growth (%)
	Road	From	To	2019	2020	2021	2022	2023	
5660	Sha Tau Kok Rd	On Kui St	Ping Che Rd	33,630	23,740 (-29.4%)	22,980 (-3.2%)	22,280 (-3%)	22,810 (2.4%)	-1.3% (Between 2020 and 2023)

Note: (1) Figures in bracket indicated the % increase between two successive years.

4.3.2 From **Table 4.2**, the recorded traffic flows in 2020 dropped significantly by almost 30% and was due to the opening of Lung Shan Tunnel in 2019. From 2020 to 2023, the recorded traffic flows have been quite steady with only -1.3% decrease annually.

Territorial Population and Employment Data Matrix (TPEDM)

4.3.3 Reference was also made to the 2021-based TPEDM published by Planning Department. The population and employment data of year 2026 and 2031 in Northeast New Territories are summarized in **Table 4.3**.

Table 4.3 Population and Employment Data in Northeast New Territories

Year	Population	Employment	Total
2026	1,361,150	421,350	1,782,500
2031	1,416,800	448,600	1,865,400
Average Annual Growth Rate			+0.9%

- 4.3.4 As shown in **Table 4.3**, the projected average annual growth rate of the population and employment total number under the TPEDM in Northeast New Territories is +0.9% between the years 2026 to 2031, which is larger than the annual growth rate derived from ATC. To be conservative, the larger growth rate of +0.9% is adopted for the subsequent assessments.

4.4 Reference and Design Flows

- 4.4.1 The 2030 Reference Flows, i.e. the future traffic flows in the local road network WITHOUT the development traffic, were estimated based on the following equation:

$$2030 \text{ Reference Flows} = 2025 \text{ Existing Flows} \times (1 + 0.9\%)^5$$

- 4.4.2 The 2030 Design Flows, i.e. the future traffic flows in the local road network WITH the development traffic, are estimated based on the following equations.

$$2030 \text{ Design Flows} = 2030 \text{ Reference Flows} + \text{Traffic generated and attracted by the proposed development}$$

- 4.4.3 The forecast traffic flows for the two scenarios are shown in **Figures 4.2 and 4.3** respectively.

4.5 Junction Capacity Assessment

- 4.5.1 Junction capacity analysis is carried out for the assessment year 2030. The assessment results are shown in **Table 4.4** and the detailed calculation sheets are attached in **Appendix B**.

Table 4.4 2030 Junction Capacity Assessment

Ref.	Junction Location	Type/ Index ⁽¹⁾	2030 Reference Peak Hour	2030 Design Peak Hour
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.47	0.49
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.45	0.46

Note: (1) DFC = Design Flow to Capacity.

- 4.5.2 The results in **Table 4.4** show that the assessed junctions will operate satisfactorily in both reference and design scenarios. Therefore, it is anticipated that the proposed temporary EV charging station will not induce significant traffic impact to the surrounding road network.

4.6 Link Capacity Assessment

4.6.1 The V/C Ratios of Sha Tau Kok Road were assessed and the results are presented in **Table 4.5**.

Table 4.5 2030 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	2030 Traffic Volume		2030 V/C Ratio	
		Reference Scenario	Design Scenario	Reference Scenario	Design Scenario
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	1025	0.33	1025	0.33
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	307	0.10	307	0.10

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

4.6.2 As shown in **Table 4.5**, all the concerned road sections will operate with capacity during AM and PM peak hours in all scenarios. Hence, it can be concluded that the traffic induced by the proposed temporary EV charging station will not induce significant traffic impact to the road network.

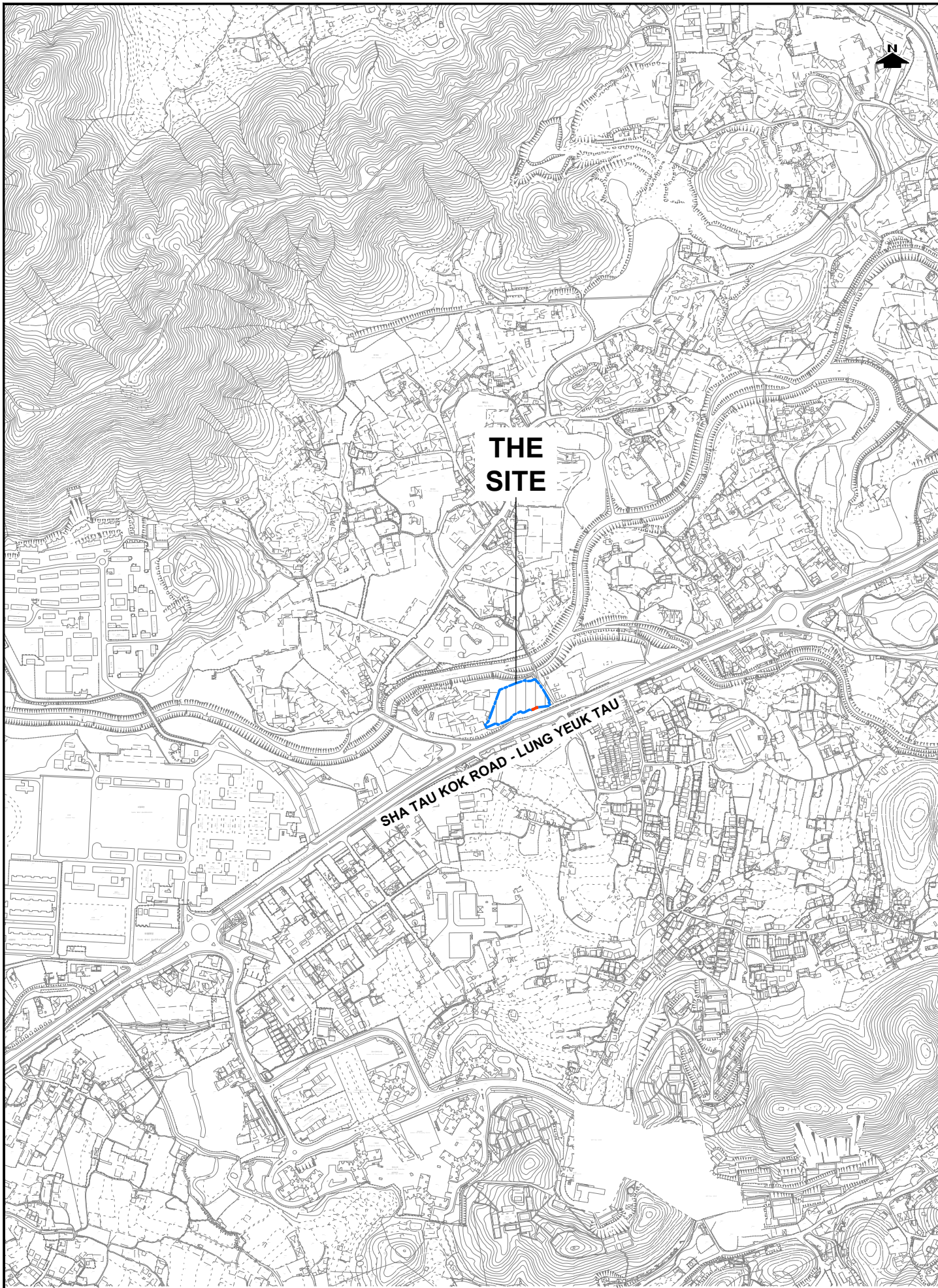
5 SUMMARY AND CONCLUSION

5.1 Summary

- 5.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi), vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office for a period of 5 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T. The main function of the Site is to provide charging facilities for electric taxis with supporting facilities, including vehicle repair workshop, motor vehicle showroom, eating place and ancillary office.
- 5.1.2 Traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30 which the time period will be the peak traffic generation period of the proposed development. Based on the existing traffic flows, the junction and link capacity assessments show that they are operating satisfactorily.
- 5.1.3 The proposed development is estimated to generate a two-way traffic of 87 pcu/hour in the operation peak hours. Junction and link capacity assessment was carried out for the reference and design scenarios in 2030 and the results revealed that the concerned junctions and road links would perform satisfactorily with spare capacity in 2030. Therefore, it is anticipated that the proposed development will not induce significant traffic impact to the surrounding road network.
- 5.1.4 For the 18 nos. of EV charging stations, 15 nos. of waiting spaces can be provided (minimum two spaces in each EV charging station) to avoid the awaiting vehicles to queue on public roads. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.

5.2 Conclusion

- 5.2.1 This planning application is to allow a temporary vehicle charging station with supporting facilities, including vehicle repair workshop, motor vehicle showroom, eating place and ancillary office for a period of 5 years. Based on the assessment result, it can be concluded that the proposed use will not induce additional adverse traffic impact on the surrounding road network and it is considered acceptable from traffic engineering point of view.



PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 2.1	REV. .
DESIGNED SLN	DATE MAY 2025	DRAWING TITLE LOCATION PLAN		LLA 顧問有限公司 Consultancy Limited	
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CHECKED SLN					

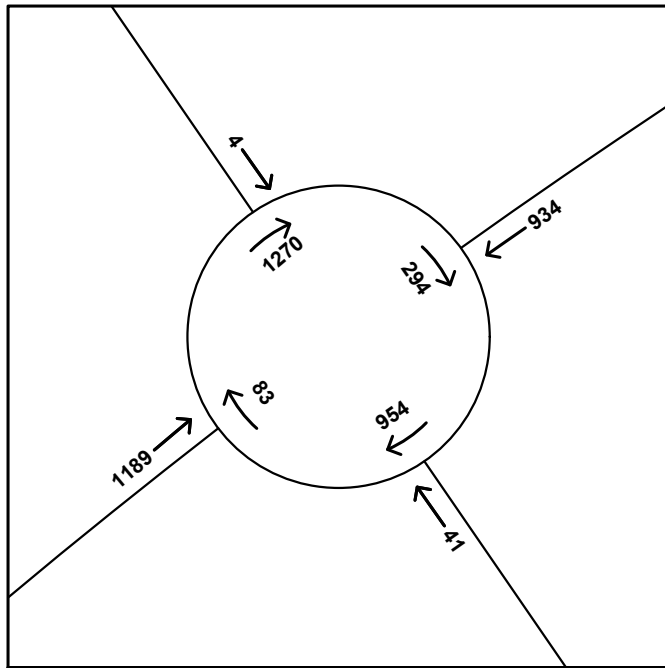


LEGEND:

- AREA OF INFLUENCE
- J1 SURVEYED JUNCTION

PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOR LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.	
DESIGNED SLN	DATE MAY 2025	DRAWING TITLE AREA OF INFLUENCE	
DRAWN CLL	SCALE 1:5000		
CHECKED SLN			

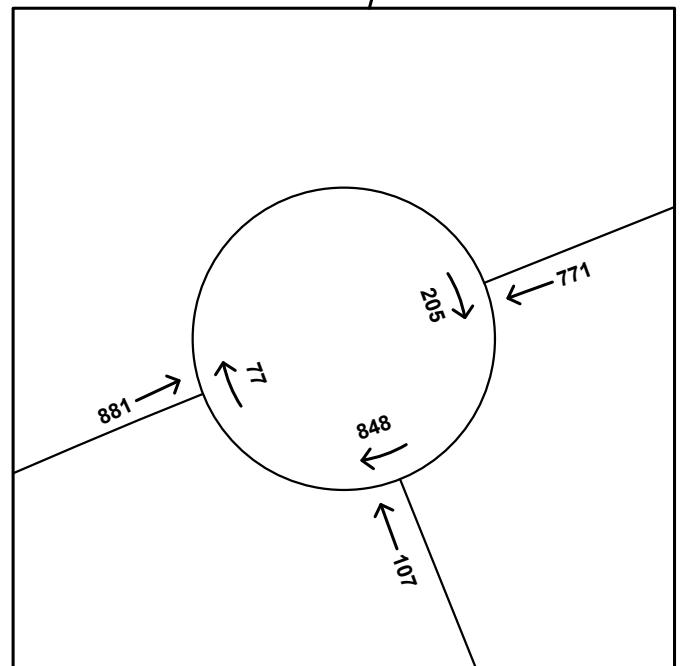
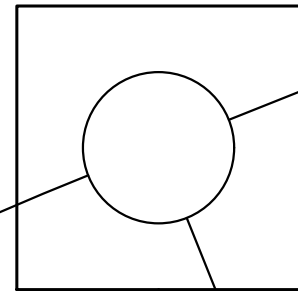
DRAWING NO. FIGURE 3.1	REV. .
LLA 顧問有限公司 Consultancy Limited	



THE SITE



SHA TAU KOK ROAD - LUNG YEUK TAU



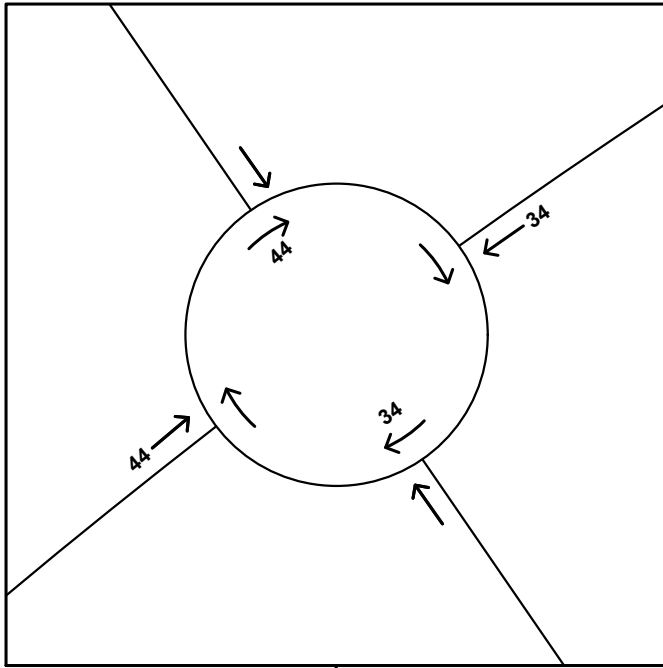
LEGEND:

123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

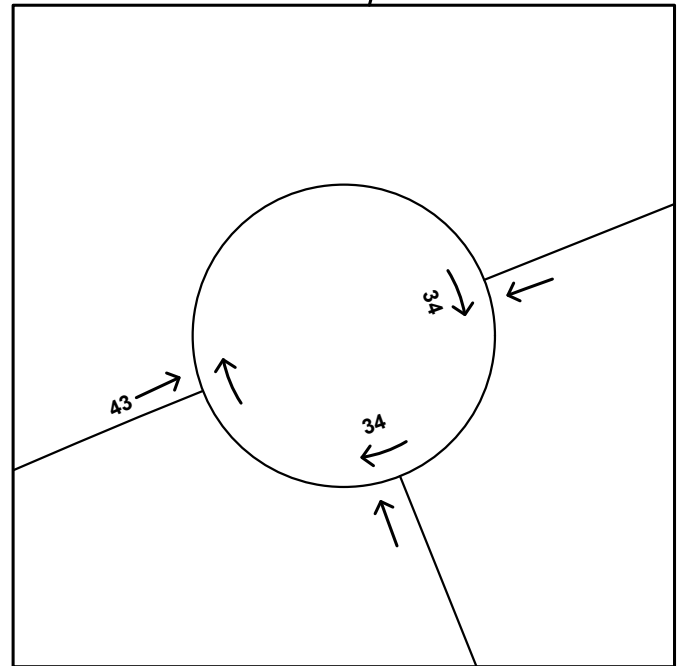
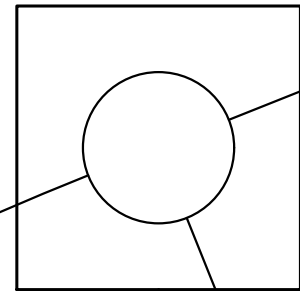
PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LORD LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 3.2	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2025 OBSERVED TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



THE SITE



SHA TAU KOK ROAD - LUNG YEUK TAU



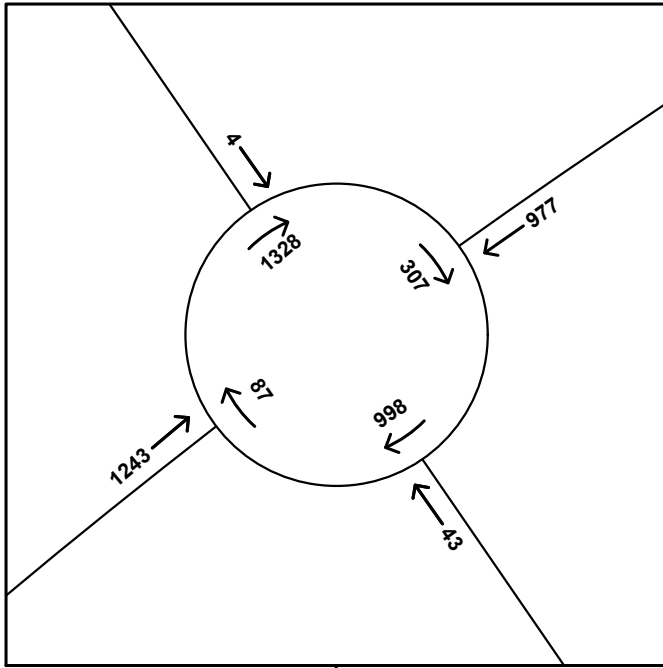
LEGEND:

123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

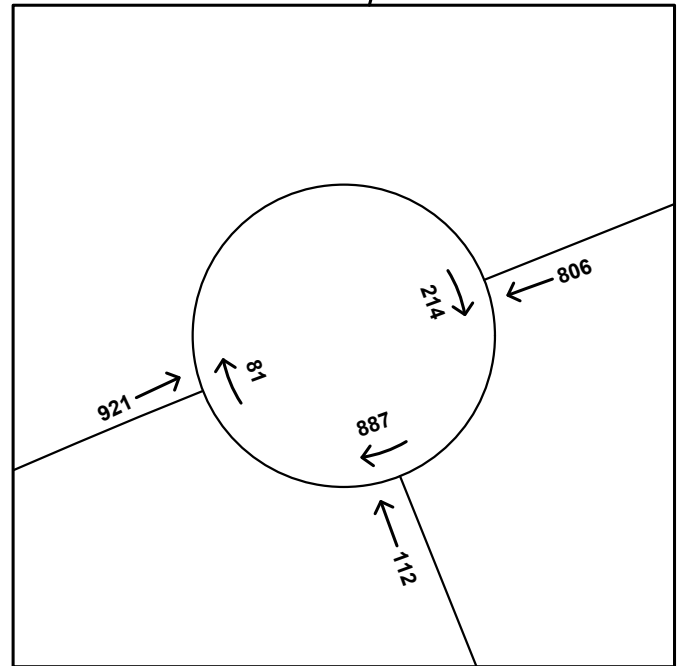
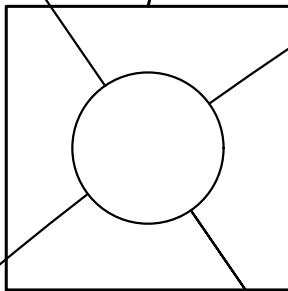
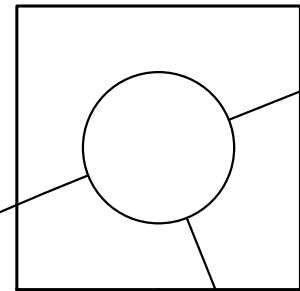
PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.1	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE DEVELOPMENT TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



THE SITE



SHA TAU KOK ROAD - LUNG YEUK TAU



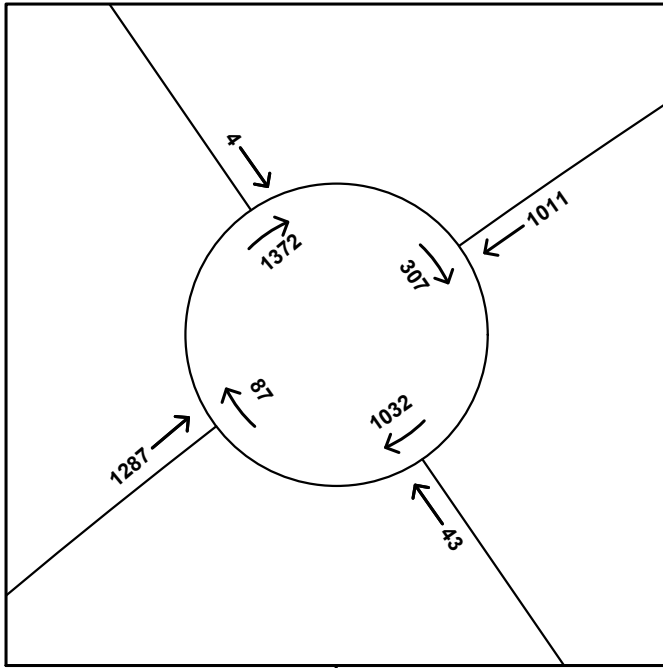
LEGEND:

123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

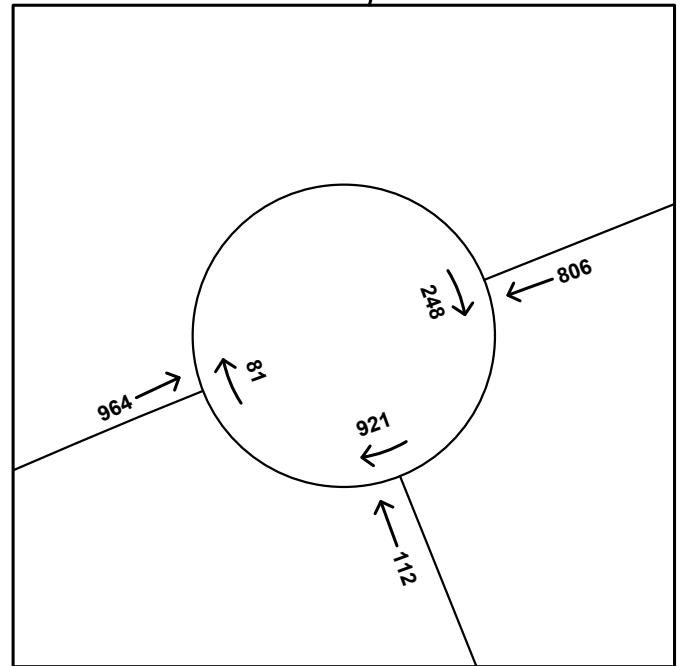
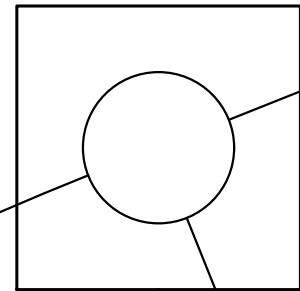
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DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2030 REFERENCE TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



THE SITE



SHA TAU KOK ROAD - LUNG YEUK TAU



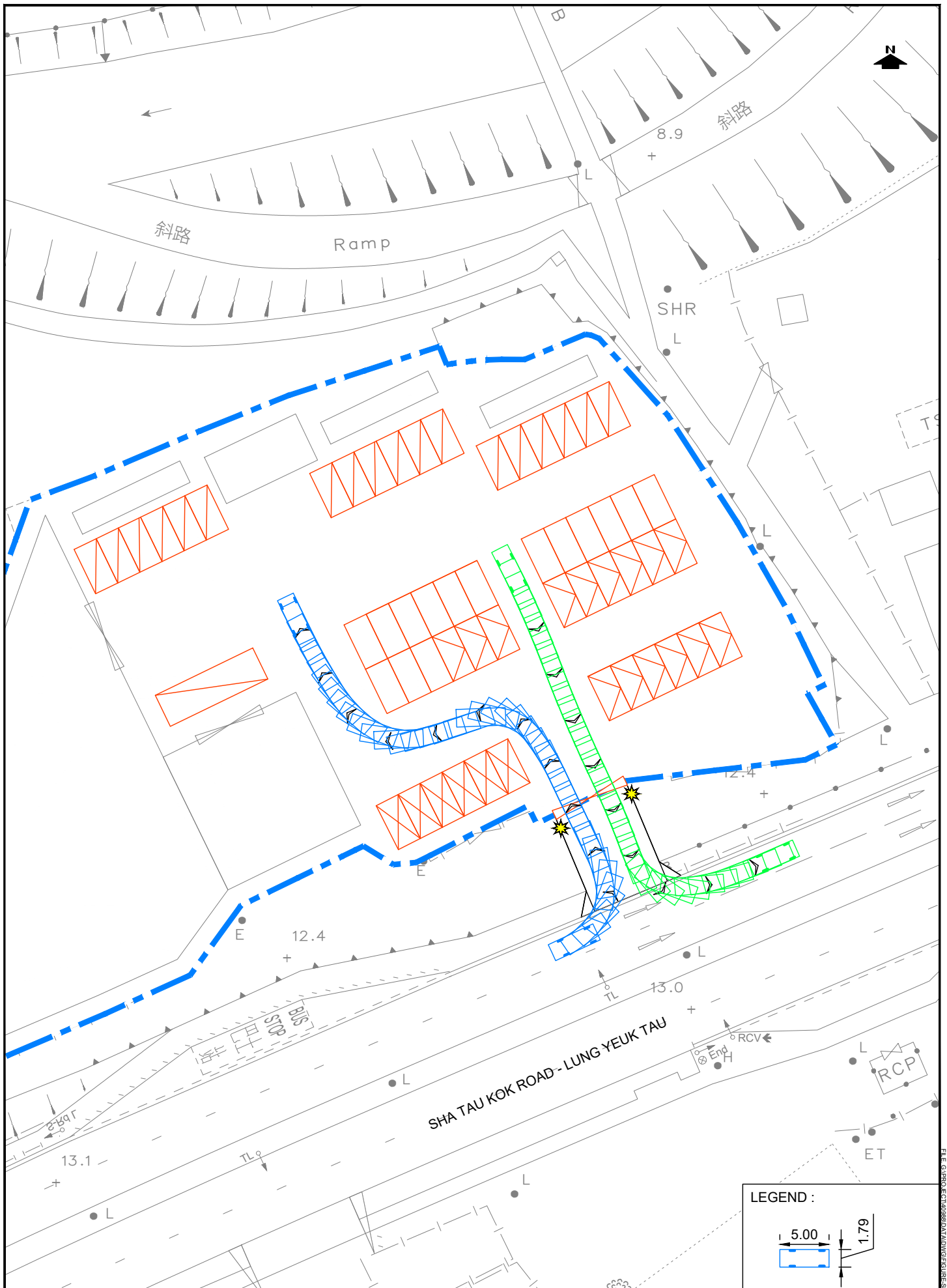
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123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

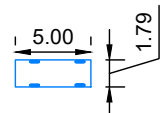
NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXLORD LOT 464 S.A.R.P. (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.3	REV. .
DESIGNED SLN	DATE JUN 2025	DRAWING TITLE 2030 DESIGN TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



LEGEND :



PROJECT NO.

40988

PROJECT TITLE

PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.

DRAWING NO.

SP-01

REV.

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DESIGNED

SLN

DATE

JUL 2025

DRAWING TITLE

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SCALE

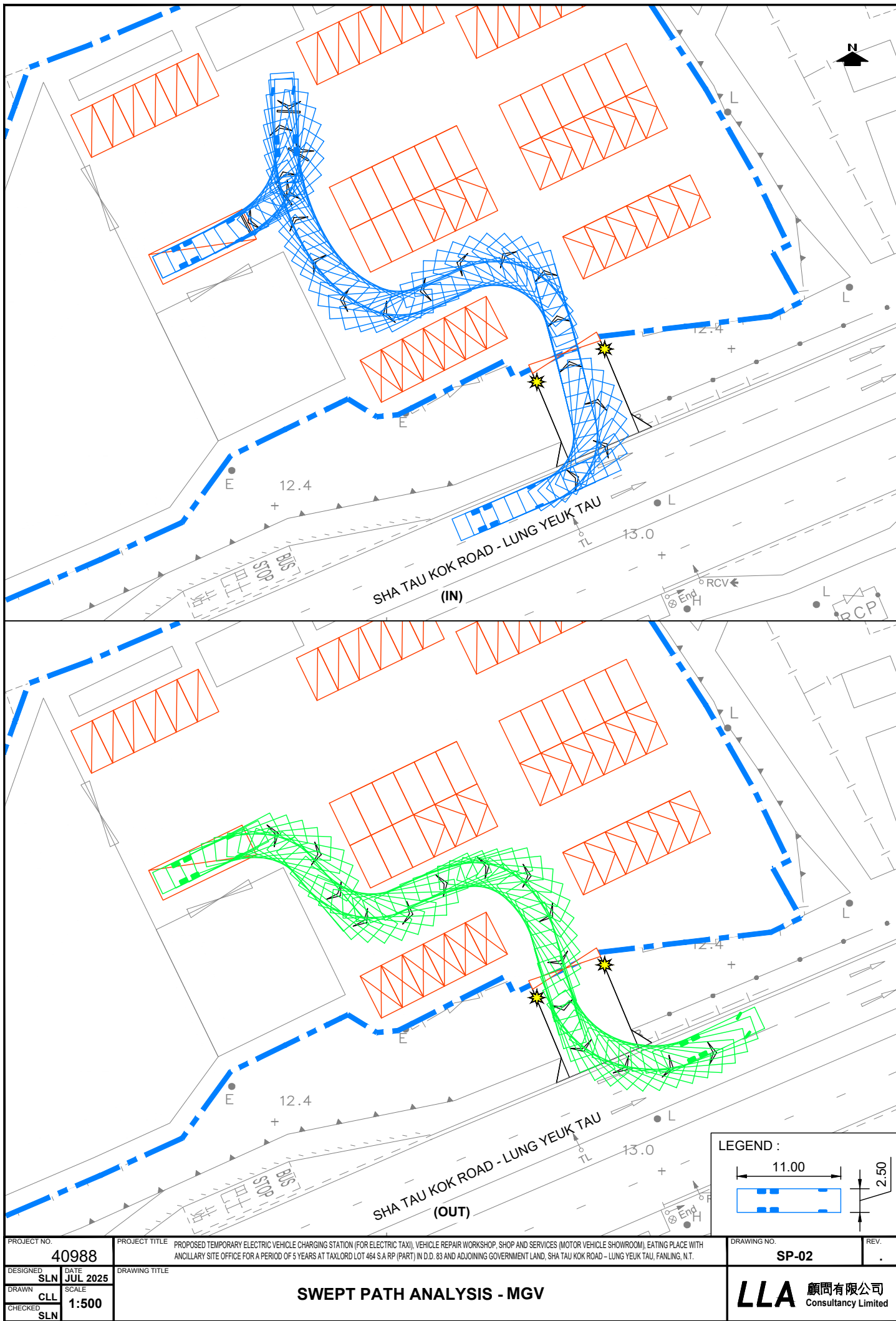
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SWEPT PATH ANALYSIS - PC

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Consultancy Limited



Appendix A
Junction Capacity Assessments
- Existing Scenario

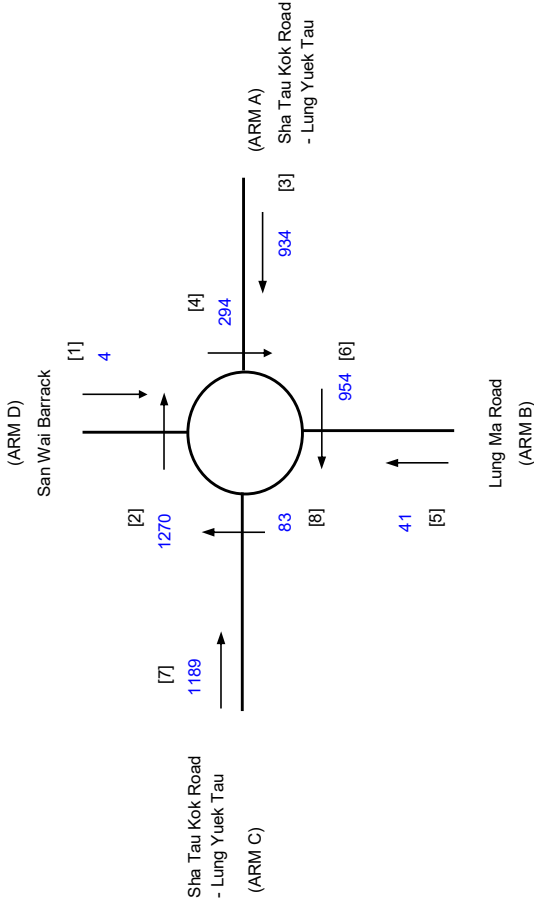
LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDBABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:		INITIALS	DATE
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:		SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:		SLN	Jun-25
				SLN	Jun-25



ARM

A B C D

INPUT PARAMETERS:

V	=	Approach half width (m)	7.10	3.50	7.10	4.20
E	=	Entry width (m)	7.30	5.20	9.50	4.70
L	=	Effective length of flare (m)	1.00	12.00	19.00	9.00
R	=	Entry radius (m)	58.00	69.00	31.00	42.00
D	=	Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A	=	Entry angle (degree)	21.00	10.00	32.00	18.00
Q	=	Entry flow (pcu/h)	934	41	1189	4
Qc	=	Circulating flow across entry (pcu/h)	294	954	83	1270

OUTPUT PARAMETERS:

S	=	Sharpness of flare = 1.6(E-V)/L	0.32	0.23	0.20	0.09
K	=	1-0.00347(A-30)-0.978(1/R-0.05)	1.06	1.10	1.01	1.07
X2	=	V + ((E-V)/(1+2S))	7.22	4.67	8.81	4.62
M	=	EXP((D-60)/10)	0.50	0.50	0.50	0.50
F	=	303*X2	2188	1415	2669	1401
Td	=	1+(0.5/(1+M))	1.33	1.33	1.33	1.33
Fc	=	0.21*Td*(1+0.2*X2)	0.68	0.54	0.77	0.54
Qe	=	K(F-Fc*Qc)	2113	992	2632	765

DFC = Design flow/Capacity = Q/Qe

Total In Sum = 2168 PCU

DFC of Critical Approach = 0.45

LLA CONSULTANCY LIMITED

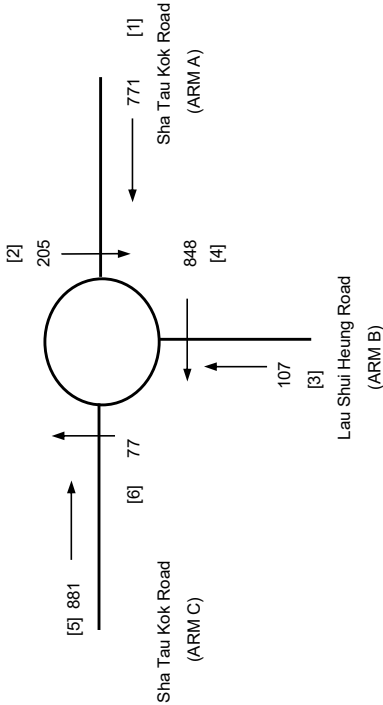
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:	SLN	Jun-25

2025 Existing 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	771	107	881
Qc = Circulating flow across entry (pcu/h)	205	848	77

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1790	870	2093

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.43

Total In Sum = 1759 PCU

Appendix B

Junction Capacity Assessments - Reference & Design Scenarios

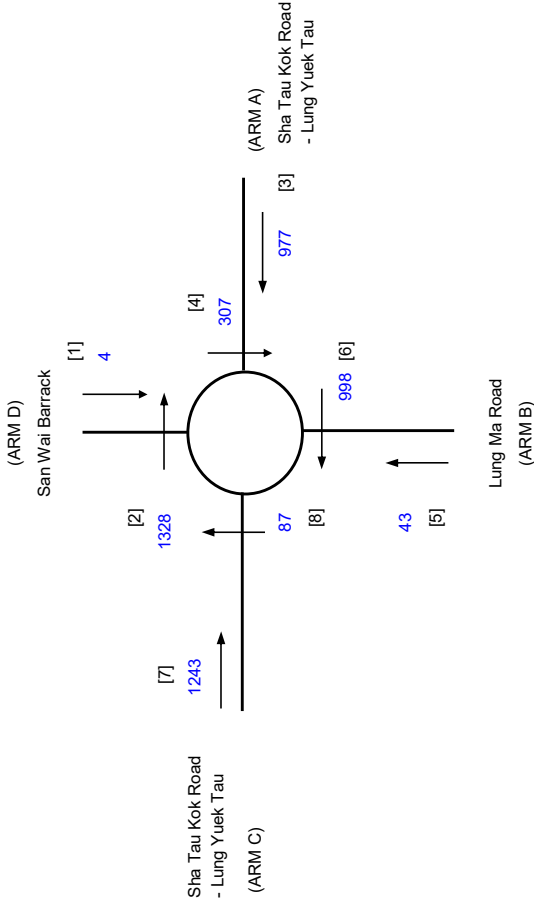
LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDAABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:		INITIALS	DATE
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:		SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:		SLN	Jun-25
				SLN	Jun-25



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	977	43	1243	4
Qc = Circulating flow across entry (pcu/h)	307	998	87	1328

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2 = $V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M = $EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F = $303 \times X2$	2188	1415	2669	1401
Td = $1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe = $K(F-Fc \times Qc)$	2103	965	2629	731

DFC = Design flow/Capacity = Q/Qe

Total In Sum =

2267 PCU

DFC of Critical Approach = 0.47

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Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDABOUT CALCULATION

2030 Design 1630-1730

PROJECT NO.: 40988

FILENAME : J1_STKR_LMR.xl

REFERENCE NO.:

PREPARED BY:

CHECKED BY:

REVIEWED BY:

INITIALS

SKL

SLN

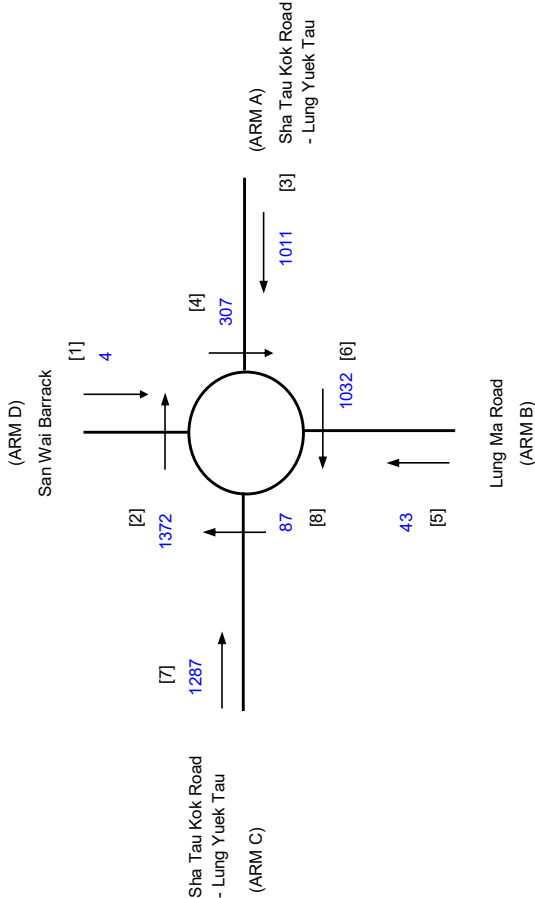
SLN

DATE

Jun-25

Jun-25

Jun-25



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	1011	43	1287	4
Qc = Circulating flow across entry (pcu/h)	307	1032	87	1372

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2 = $V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M = $EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F = $303 \times X2$	2188	1415	2669	1401
Td = $1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe = $K(F-Fc \times Qc)$	2103	945	2629	706

DFC = Design flow/Capacity = Q/Qe

Total In Sum =

2345 PCU

DFC of Critical Approach = 0.49

LLA CONSULTANCY LIMITED

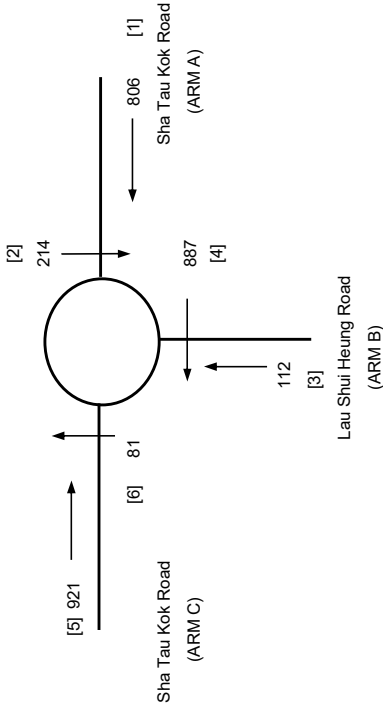
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:	SLN	Jun-25
			SLN	Jun-25

2030 Reference 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	806	112	921
Qc = Circulating flow across entry (pcu/h)	214	887	81

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1784	851	2090

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.45

Total In Sum = 1839 PCU

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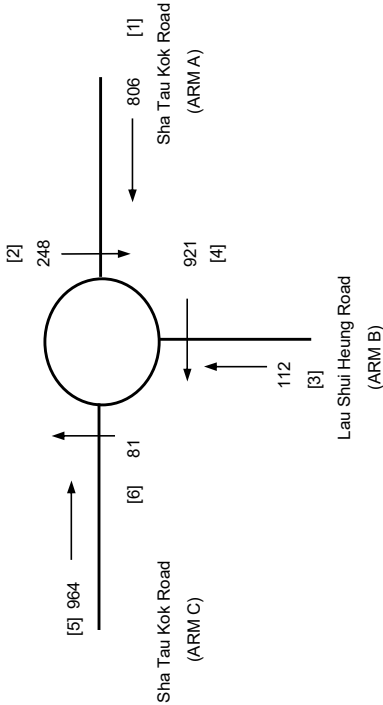
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDBABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	INITIALS	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SKL	Jun-25
REFERENCE NO.:		REVIEWED BY:	SLN	Jun-25

2030 Design 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	806	112	964
Qc = Circulating flow across entry (pcu/h)	248	921	81

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1763	833	2090

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.46

Total In Sum =

1882 PCU

Appendix 3

Drainage Proposal

Drainage Submission in support of

S16 Planning Application for

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office
for a Period of 5 Years in “Open Storage” zone

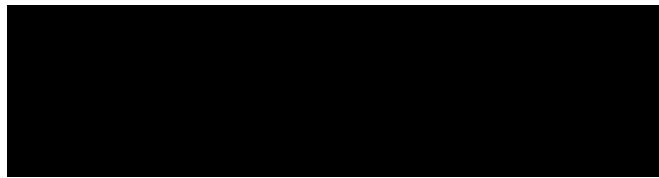
at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining
Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling,
New Territories

(HT25039)

July 2025

Drainage Consultant:

何田顧問工程師有限公司
HO TIN & ASSOCIATES



Prepared & approved by	LEE Kwok Cheung <i>RPE(Civil)</i>	
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1. Background

- 1.1 With respect to a S16 Planning Application for Proposed Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories, Messrs. Ho Tin & Associates Consulting Engineers Limited was appointed to prepare a drainage submission.

2. Approach to Prepare this Proposal

- 2.1 This Drainage Submission is prepared in line with the “Technical Note to prepare a Drainage Submission (Relating to applications for temporary change of land use such as temporary storage areas, car parks, workshops, small factories ... etc. under S.16 of the Town Planning Ordinance)” issued by Drainage Services Department in December 2024.

3. The Subject Site and Proposed Development

- 3.1 The subject site with a total site area of about 4,190m² comprises of Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land (about 680m²), Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories. The subject site is to the south of Ng Tung River and on the north side of Sha Tau Kok Road – Lung Yeuk Tau opposite to Kwan Tei Children’s Playground. A Site Location Plan is shown in **Figure D1**.
- 3.2 The subject development consists of one single-storey motor vehicle showroom structure (5m high, total floor area = 225m²), one two-storey vehicle repair workshop cum eating place and ancillary office structure (7m high, total floor area = 2,012m²), one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m²), and 3 numbers of container for EV chargers (3m high, total floor area = 29m² each) accompanying with 18 numbers of EV charging spaces for electric taxi, 30 numbers of parking spaces/waiting spaces for electric taxi, 6 numbers of parking spaces for staff/visitors, and 1 number of loading/unloading space for medium goods vehicle. A plan showing the proposed site layout is at **Figure D2**.

4. Existing Drainage Conditions of the Site

- 4.1 At present, the subject site is hard paved, partitioned into sub-units and partly used for storage use and car repairing workshop and partly vacant (refer to **Plate 1** to **4**).
- 4.2 The subject site is bounded by a footpath on the east and north sides (refer to **Plate 8** to **12**). There is existing 300mm U-channel running between the subject site and the aforementioned footpaths collecting surface runoff of the subject site and the adjacent areas. The existing 300mm U-channel discharges its collected flows via an existing 450 wide trapezoidal channel into Ng Tung River to the further north.
- 4.3 Outside the southern subject site boundary is currently used as access of the subject site and its nearby areas beside Sha Tau Kok Road – Lung Yeuk Tau. There is existing 450mm U-channel serving the access (refer to **Plate 5** to **7**).
- 4.4 To the west of the subject site is another development of which the surface runoff would be discharged via an existing 600 wide trapezoidal channel into Ng Tung River.
- 4.5 Current conditions of the subject site and its existing drainage conditions are shown in the following photos (photo taking locations are shown on **Figure D3**):



	
<p>Plate 1 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (1 of 4)</p>	<p>Plate 2 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (2 of 4)</p>



Plate 3 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (3 of 4)



Plate 4 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (4 of 4)



Plate 5 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (1 of 2)

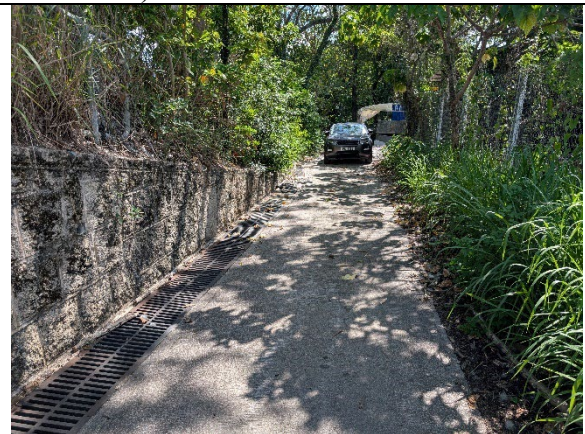


Plate 6 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (2 of 2)



Plate 7 – Existing 450mm U-channel running eastward along the existing access in front of the southern subject site boundary



Plate 8 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (1 of 3)

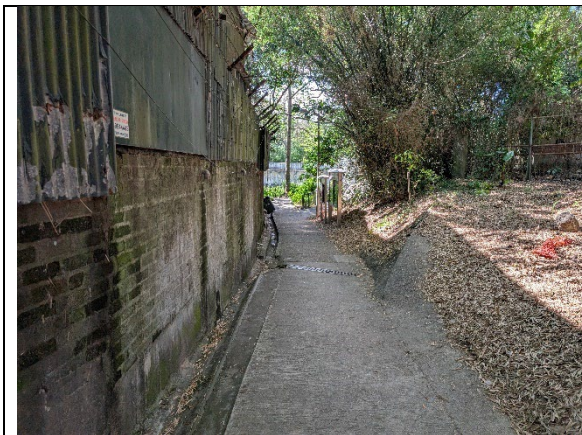


Plate 9 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (2 of 3)

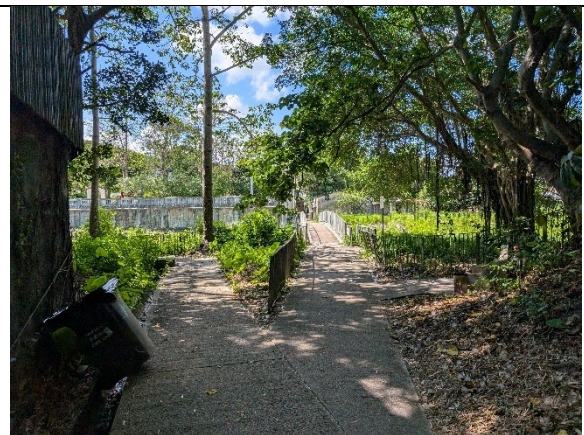


Plate 10 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (3 of 3)



Plate 11 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (1 of 2)



Plate 12 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (2 of 2)

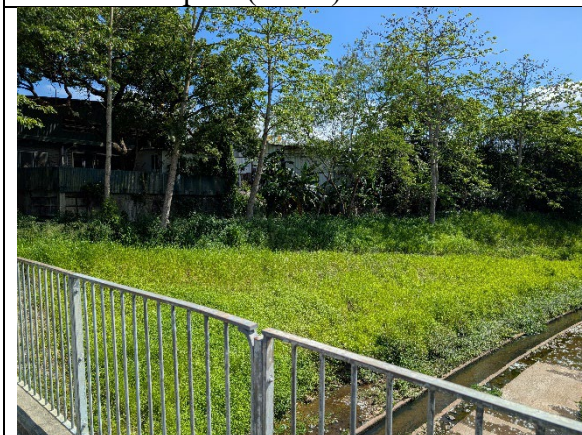


Plate 13 – The section of Ng Tung River outside the northern subject site boundary



Extract of the Drainage Records from LandsD's GeoInfo Map in May 2025

5. Drainage Assessment and Proposal

- 5.1 The subject site is a simple small site with a total site area of about 4,190m² (less than 1 ha in size). The existing subject site levels are generally same as those of the area close to Sha Tau Kok Road – Lung Yeuk Tau and relatively higher than the surroundings at the remaining three sides. There are existing surface channels intercepting surface runoff on the area between the subject site and Sha Tau Kok Road – Lung Yeuk Tau. In general, surface runoff flowing toward the subject site would be collected and conveyed away the subject site by the existing surrounding channels.
- 5.2 The existing site levels would be maintained and no site formation/leveling works would be carried out such that the subject proposed development would be maintained at levels relatively higher than its adjacent developments/areas to its east, west and north, and would be at similar levels of the existing access running along its southern boundary. In principle, surface runoff of the surrounding areas would be collected and conveyed away by the existing channels in the adjacent areas of the subject site, hence, any new solid fence wall of the subject site would not obstruct any existing overland flows (in addition, it should be noted that the existing levels (similar to the proposed finished levels) of the subject site are already relatively higher than those to the east, west and north). Nevertheless, for conservative, in order to ensure no surface runoff from the southern side (at levels close to the proposed finished levels of the subject development), due to any unexpected reasons, flowing onto the subject site to be obstructed, 100mm high gap will be formed at the bottom of the security hoarding/fence wall along the southern subject site boundary. Cross sections of the proposed subject development and typical details of the gap at the bottom of proposed security hoarding/fence are shown in **Figure D5**.
- 5.3 With respect to the "Technical Note to prepare a Drainage Submission" (the "TN") published by Drainage Services Department, for the subject site area = 4,190m² ≤ 5,000m², peripheral 525mm U channel at 1 in 200 gradient would be appropriate. It is therefore proposed to construct new peripheral 525mm U channel at 1 in 200 gradient for the subject development. A new catchpit with trap will be constructed at the end of the proposed 525 U channels near the northwest corner of the subject site before discharging into the existing 450 wide trapezoidal channel at Ng Tung River. A Proposed Stormwater Drainage Layout Plan is shown in **Figure D4**.

5.4 Besides, the Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the drainage system to the satisfaction of relevant Government departments.

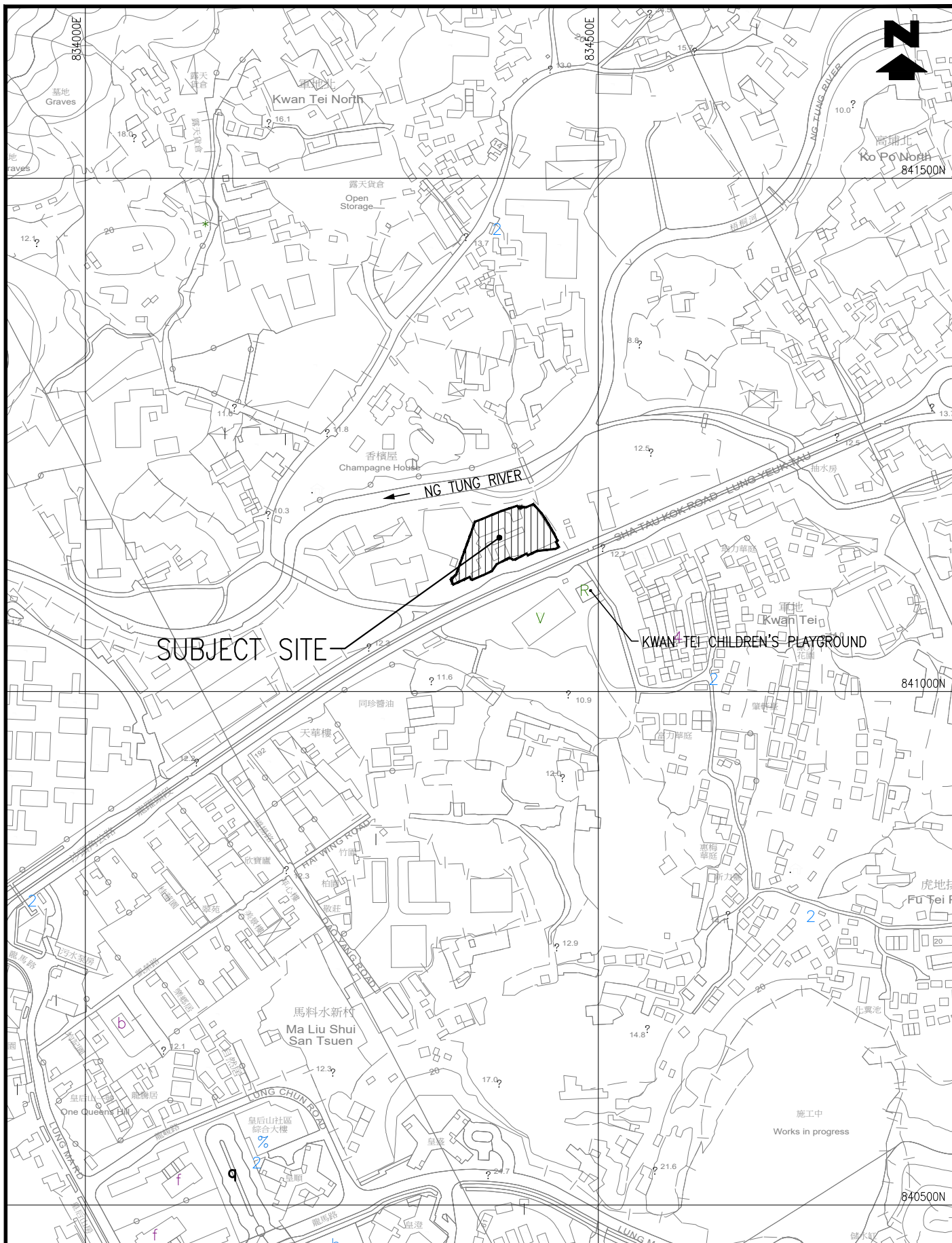
5.5 Details of proposed drainage provisions shall follow relevant details shown in Government departments' Standard Drawings as follows:

<i>Proposed Drainage Provisions</i>	<i>Standard Drawings</i>	<i>Drawing No. & Title</i>
Catchpit	CEDD Standard Drawings	C 2405/1 to /5 – Standard Catchpit Details
Catchpit with trap		C 2406/1 to /2A – Catchpit with Trap
Catchpit precast concrete cover		C 2407B – Precast Concrete Covers for Catchpit and Sand Trap
U-channel		C 2409J – Details of Half-round and U-channels
Channel cover		C 2412E – Cover Slab and Cast Iron Grating for Channels

6. Conclusion and Recommendations

6.1 The subject development as Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office will be for temporary use for a period of 5 years. The subject site area has been hard paved for a very long period without complaints on drainage conditions. The existing site levels would be maintained and no site formation/leveling works would be carried out.

- 6.2 The subject site is at present served by surrounding surface channels and its levels are relatively higher than those to its east, west and north. In principle, no surface runoff would flow onto the subject site from its surroundings. 525mm U channel at 1 in 200 gradient will be constructed at the peripheral of the subject site to intercept all crossing surface runoff. For conservative, 100mm high gap will be formed at the bottom of the security hoarding/fence along the southern subject site boundary to ensure no surface runoff from the adjacent, due to any unexpected incidents, to be obstructed. A new catchpit with trap will be constructed at the ends of the 525mm U channel before discharging into the existing 450 wide trapezoidal channel at Ng Tung River.
- 6.3 The Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the system to the satisfaction of relevant Government departments.
- 6.4 The subject development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works, i.e. debris clearance and damage repair. Therefore, in conclusion, the subject development would not cause any adverse drainage impact onto the area.



TITLE

SITE LOCATION PLAN

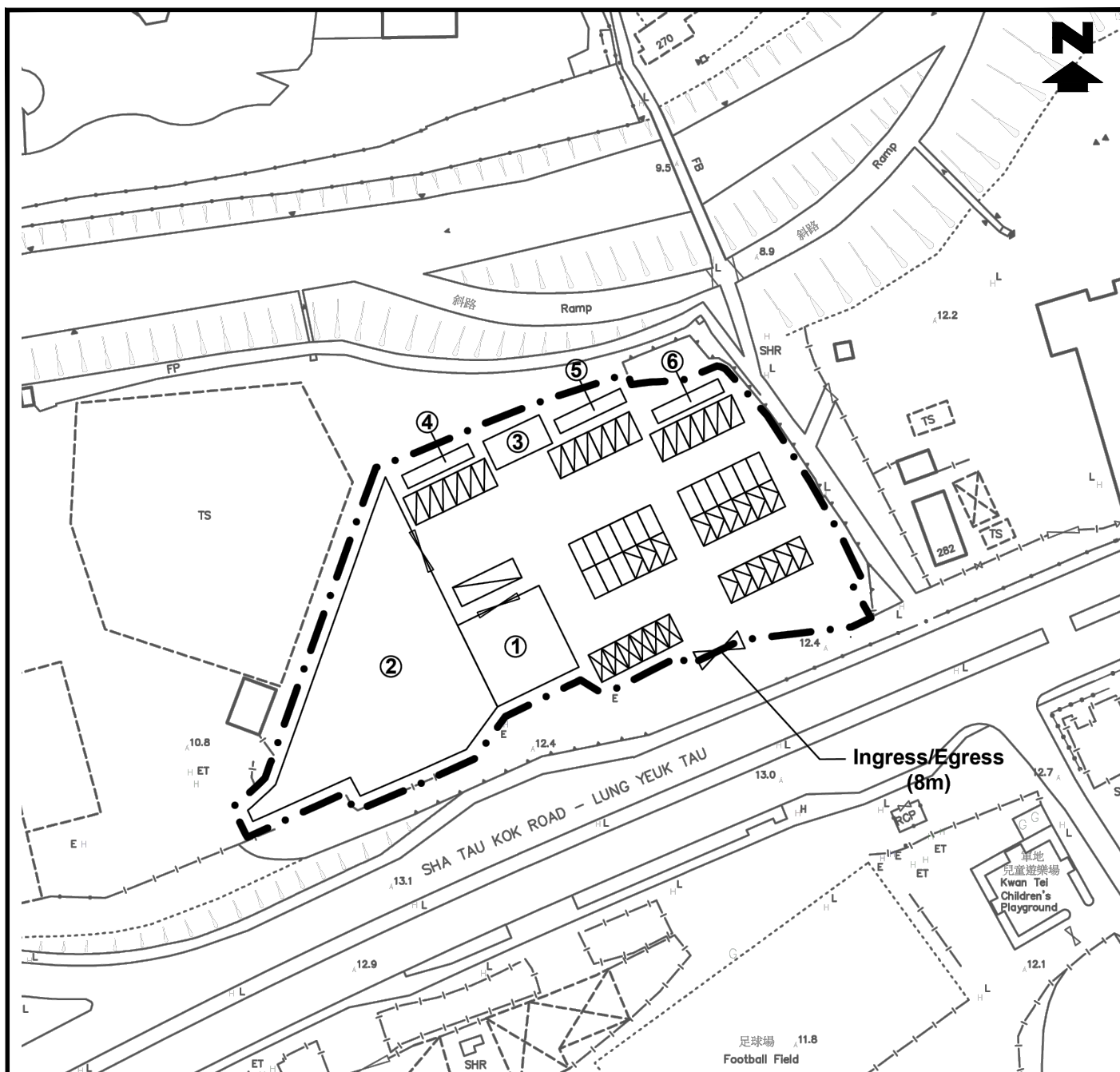
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 5000 - A4

DRAWING No.

FIGURE D1



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	2,012	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- [Pattern] EV Charing Spaces for Electric Taxi (18 nos.)
- [Pattern] Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- [Pattern] Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- [Pattern] Parking Spaces for Staff/Visitors (6 nos.)
- [Pattern] Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)

TITLE

LAYOUT PLAN

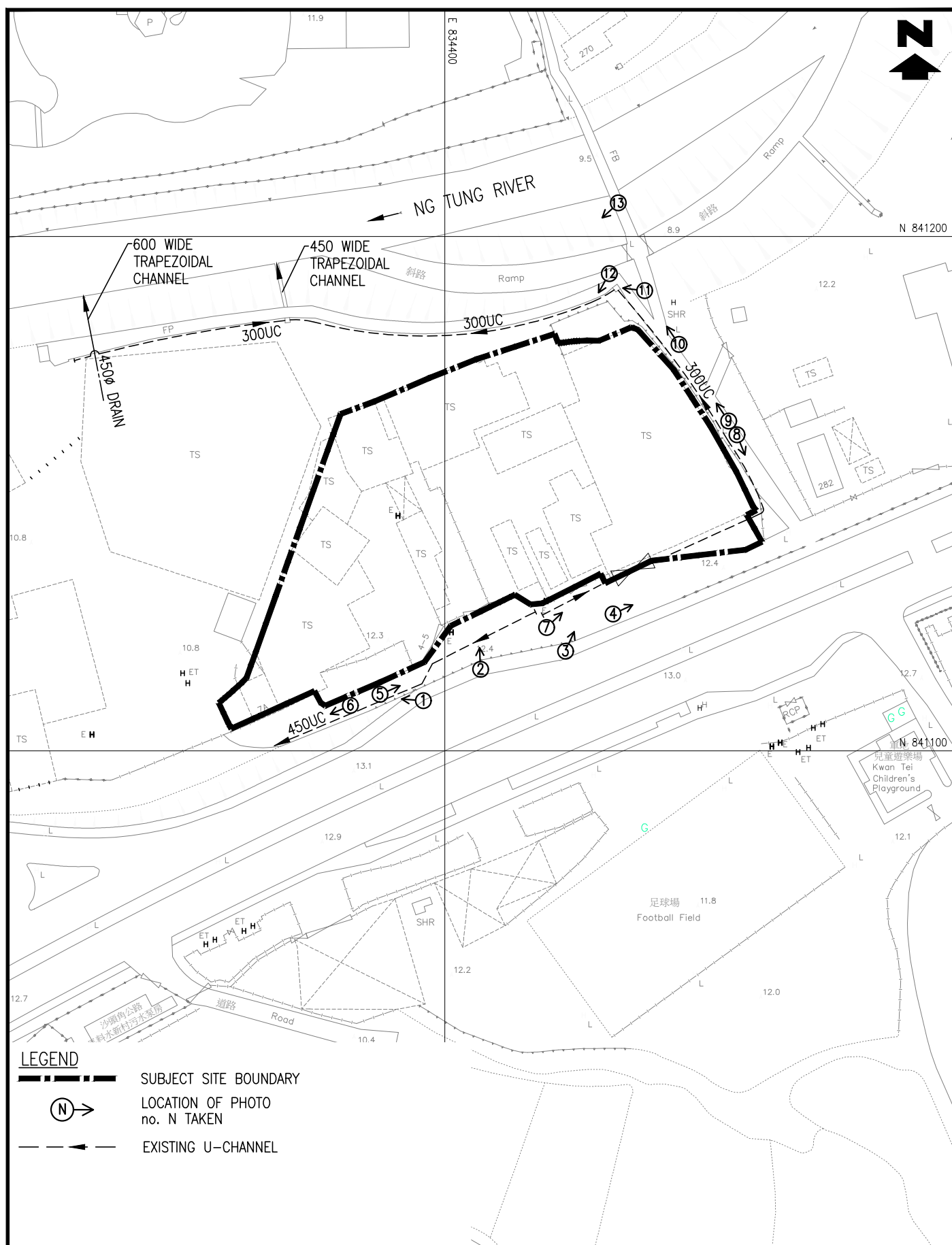
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 1000 - A4

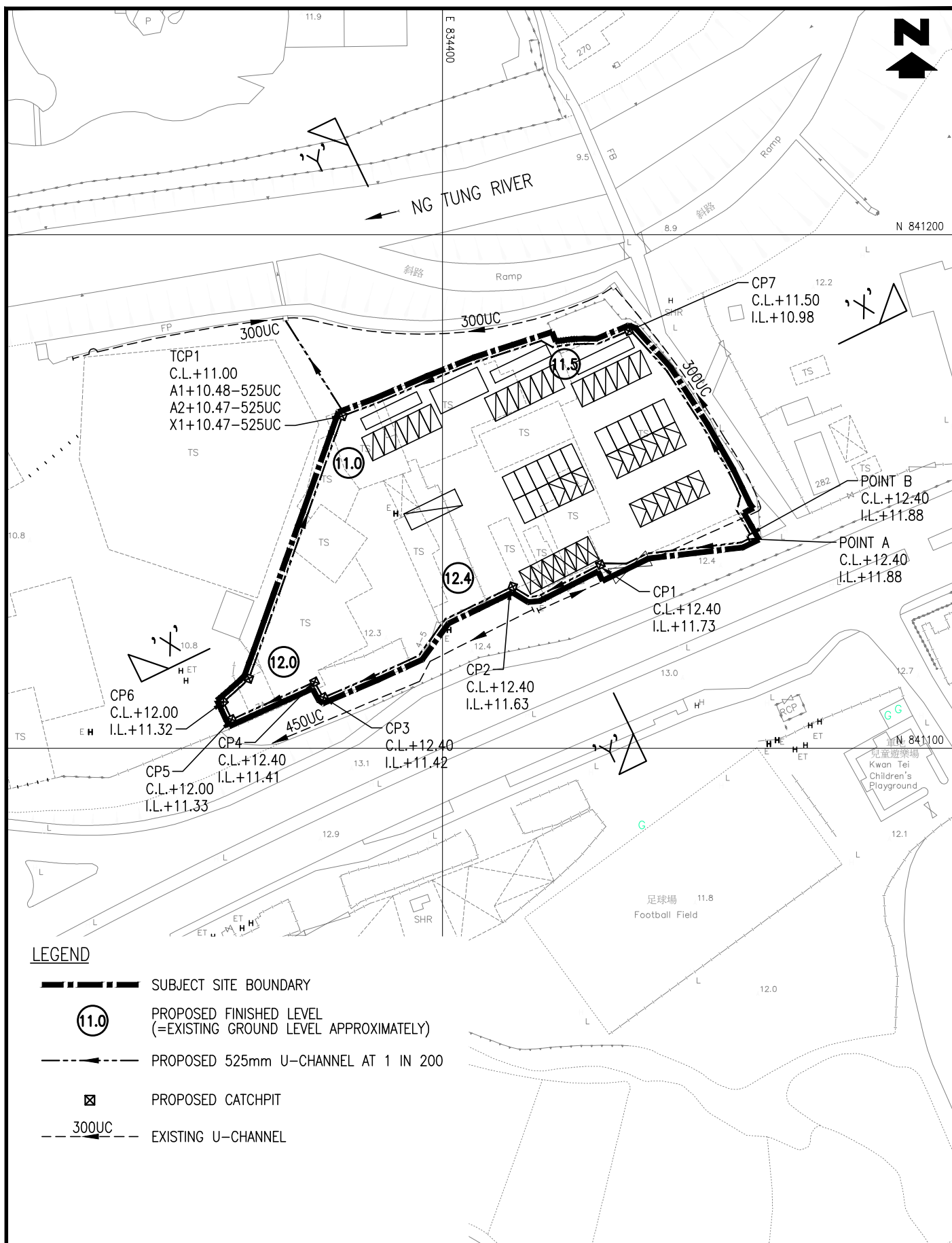
DRAWING No.

FIGURE D2



TITLE

H:\25039_LungYeukTau_Site\DRAWING\25039_FIGD2_01 FIGD3_00 FIGD4_01 & FIGD5_00.dwg, 10/7/2025 11:09:20



TITLE

PROPOSED STORMWATER DRAINAGE LAYOUT PLAN

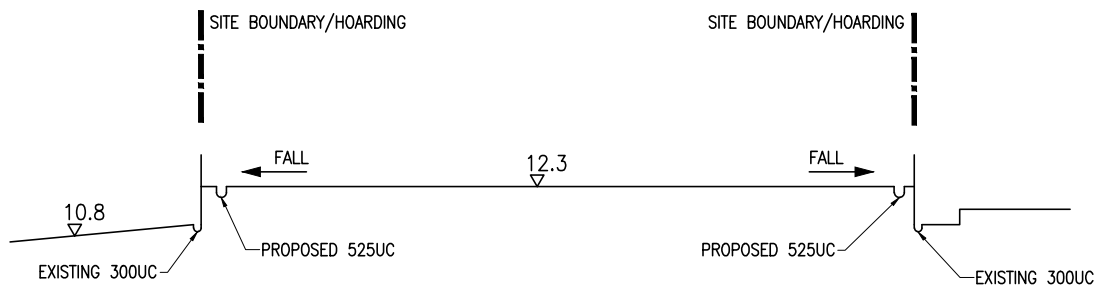
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

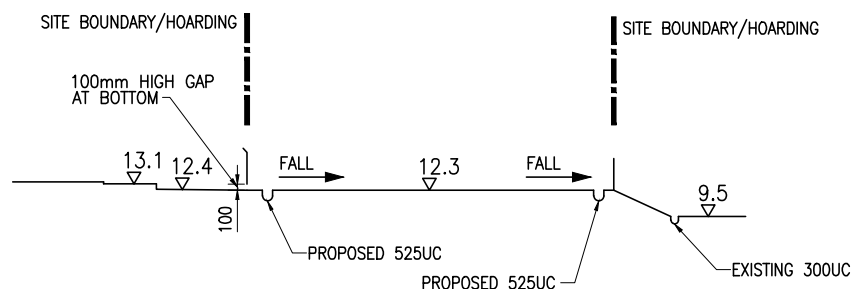
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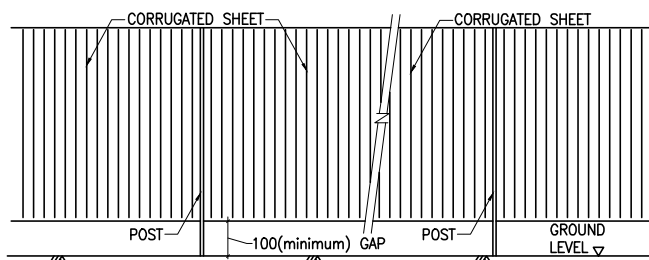
FIGURE D4



SECTION 'X'-'X'



SECTION 'Y'-'Y'



TYPICAL ELEVATION OF THE HOARDING N.T.S.
WITH 100mm HIGH GAP AT BOTTOM

TITLE

SITE CROSS SECTIONS

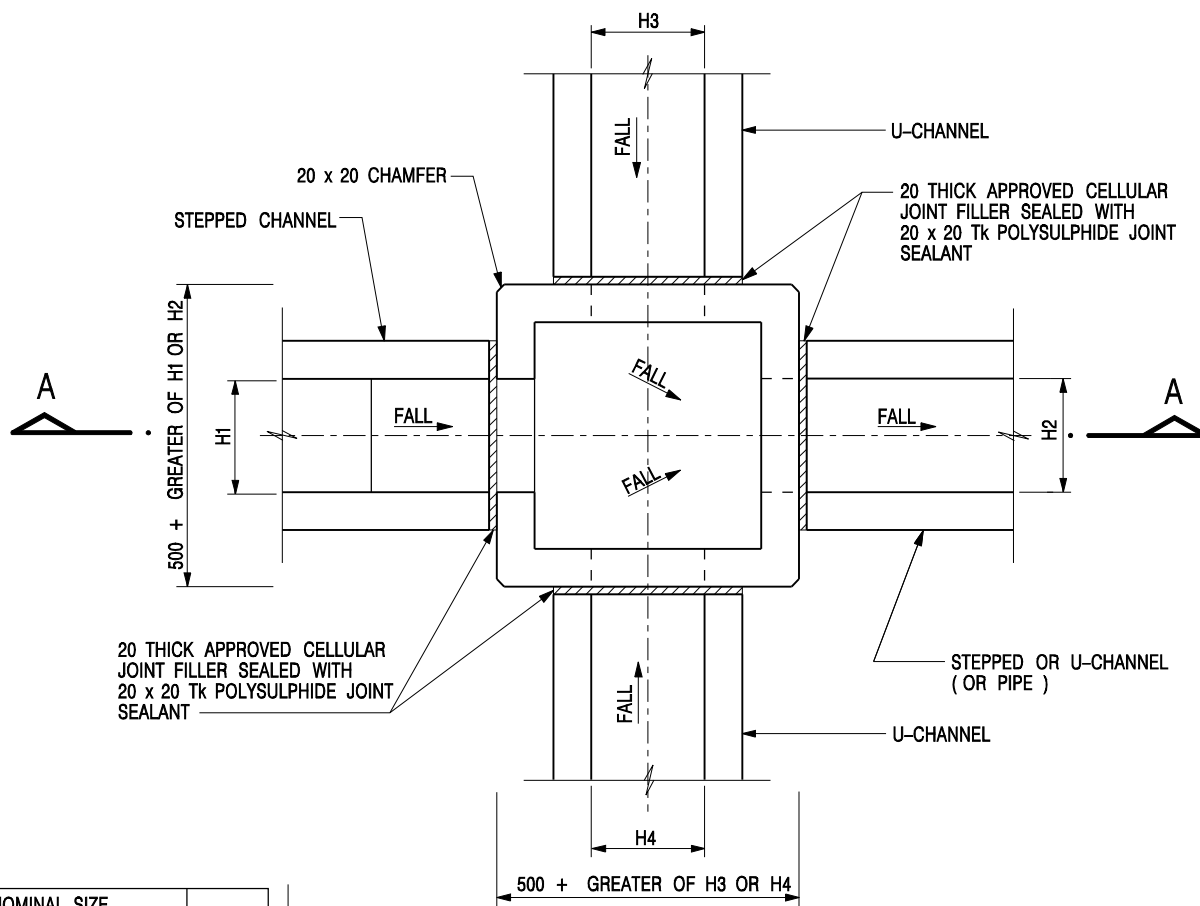
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SCALE

1 : 500 - A4

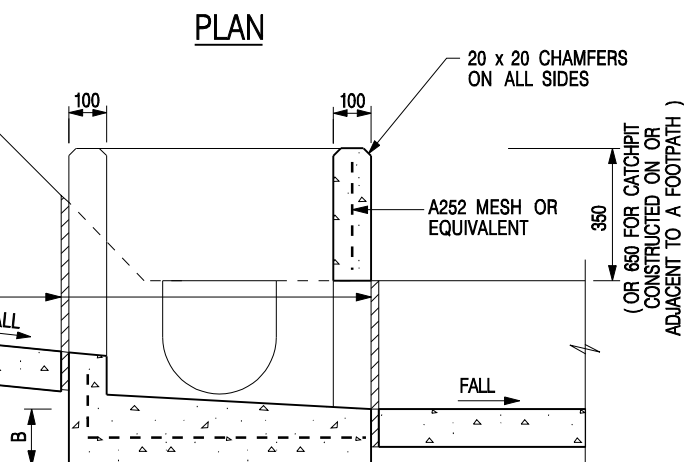
DRAWING No.

FIGURE D5



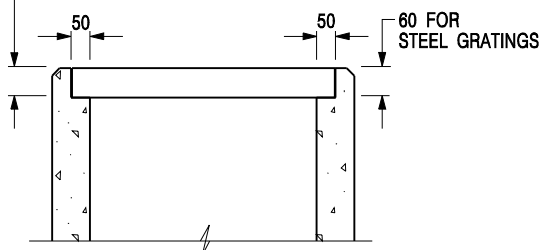
NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT



SECTION A - A

DEPTH OF RECESS AND DETAILS OF PRECAST CONCRETE COVERS (SEE STD. DRG. NO. C2407)




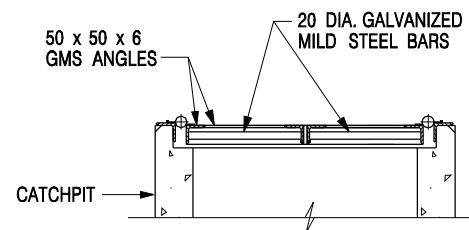
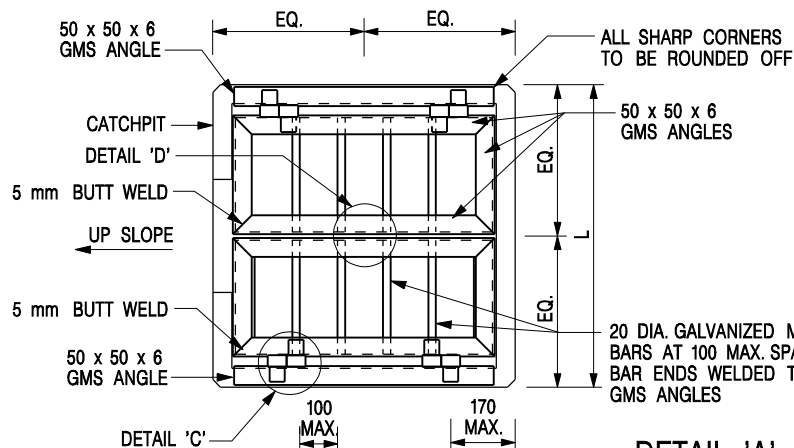
NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

STANDARD CATCHPIT DETAILS (SHEET 1 OF 5)

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REF.	REVISION	SIGNATURE	DATE
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		DRAWING NO. C2405 /1	

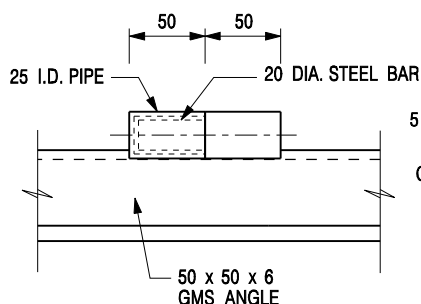


SECTIONAL ELEVATION

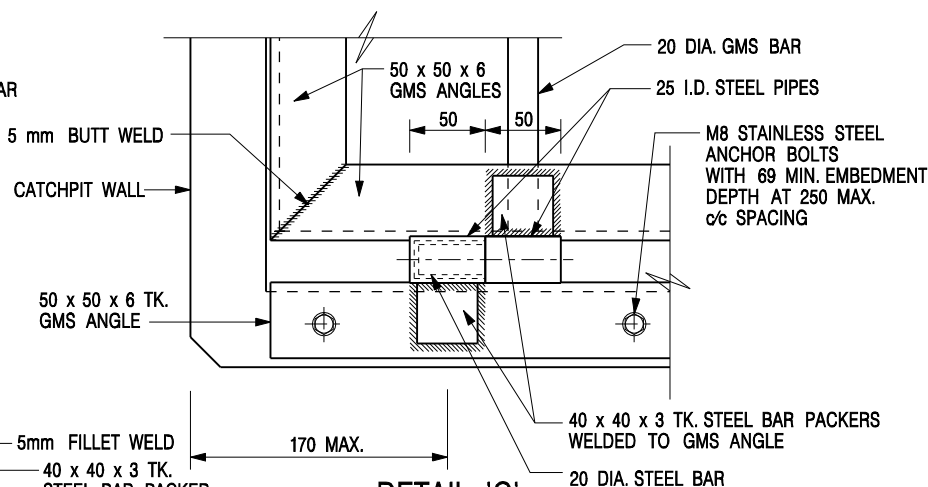
DETAIL 'A'

(DETAILS OF DOUBLE SIDE OPENING STEEL GRATING FOR L > 900mm)

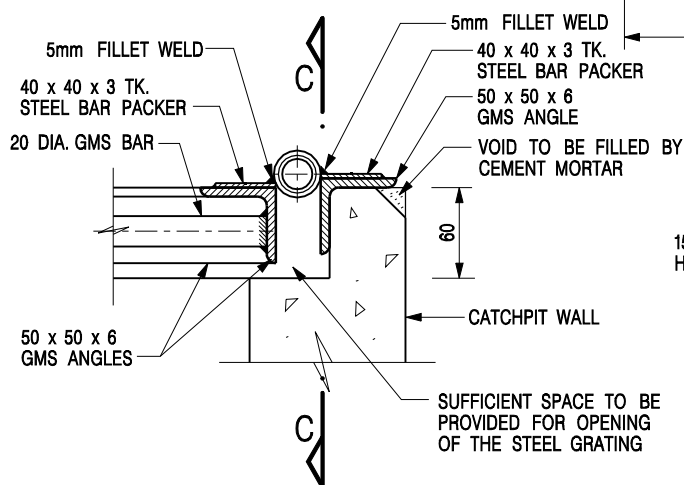
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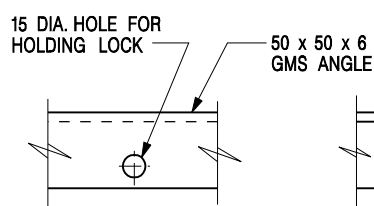
SECTION C - C



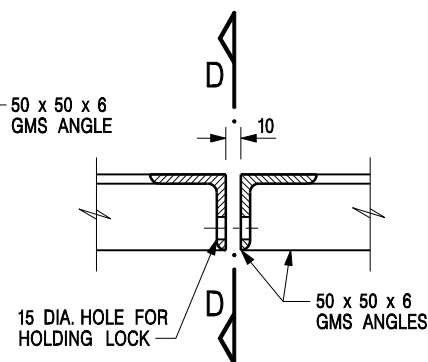
DETAIL 'C'
(DETAILS OF HINGE)
SCALE 1 : 5



SECTIONAL ELEVATION
(DETAIL 'C')



SECTION D - D




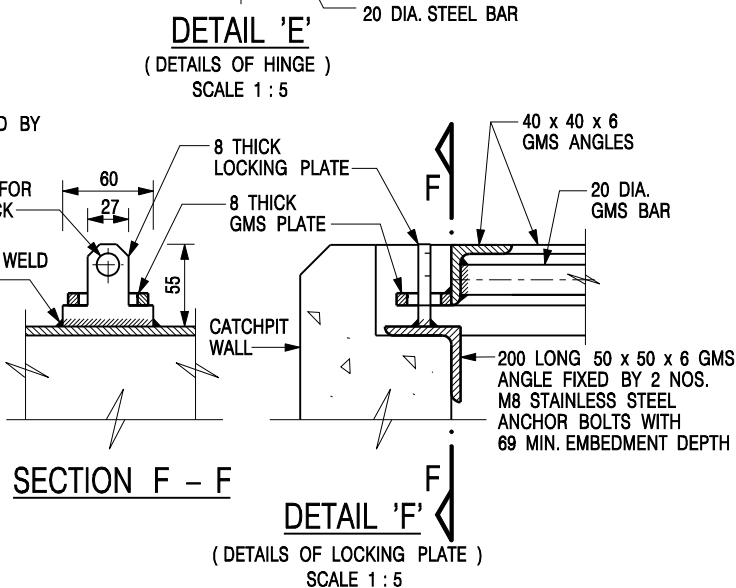
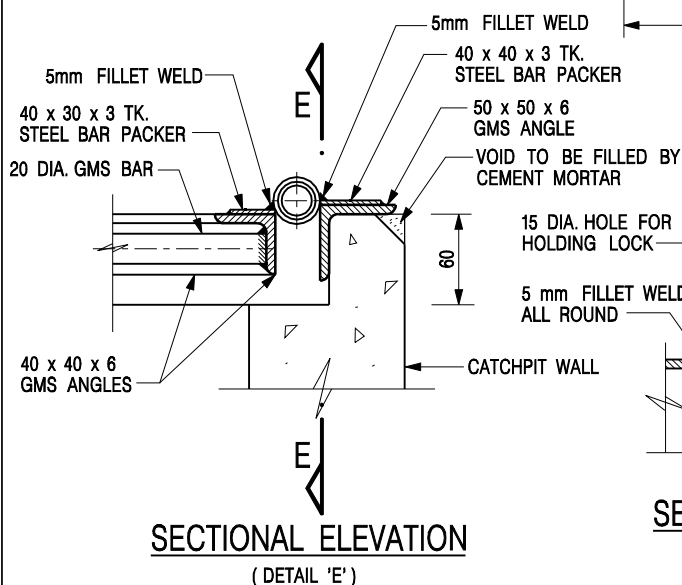
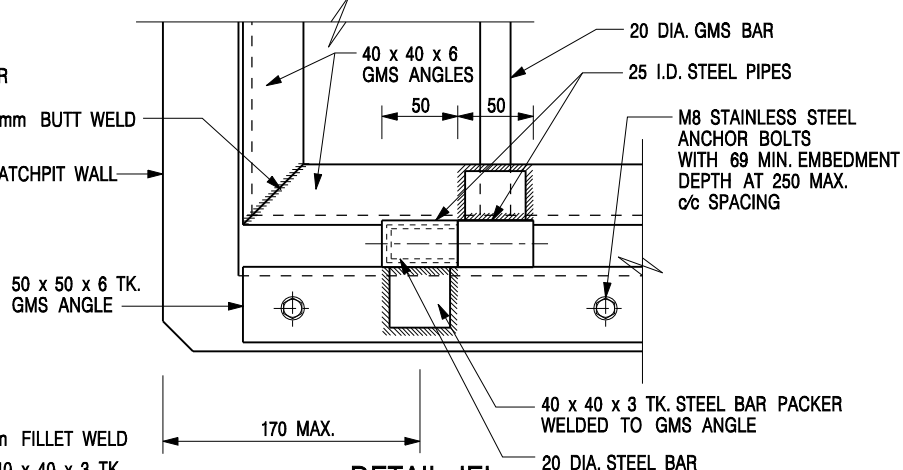
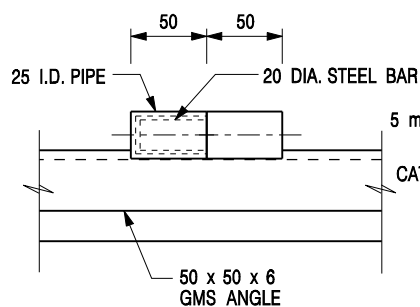
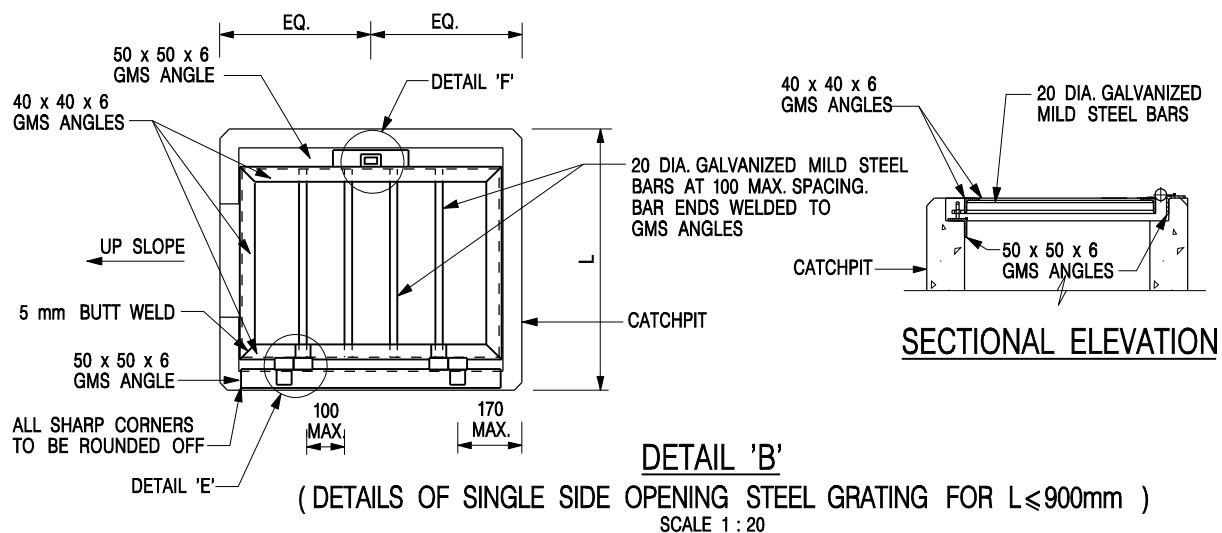
DETAIL 'D'
(DETAILS OF HOLE FOR LOCK)
SCALE 1 : 5

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 2 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
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		DRAWING NO. C2405 /2	




NOTES:

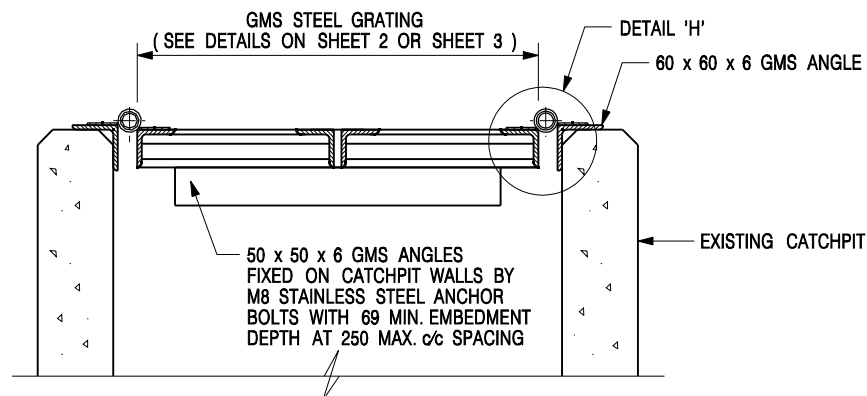
1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS (SHEET 3 OF 5)

卓越工程 建設香港

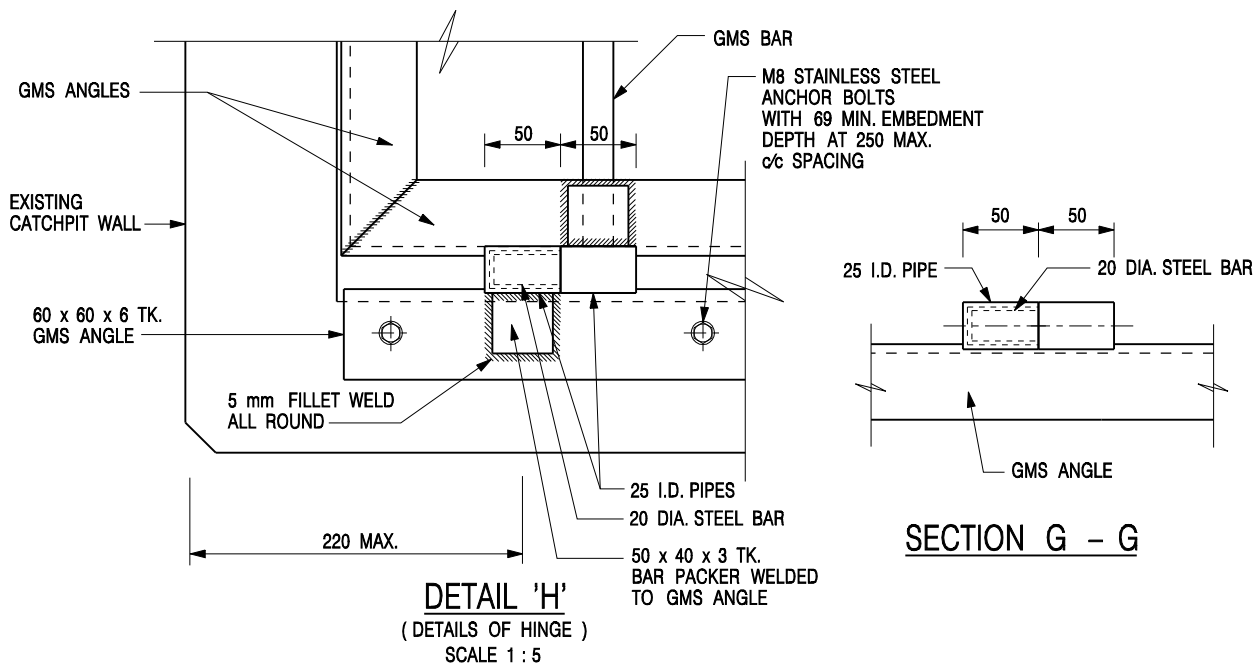
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REF.	REVISION	SIGNATURE	DATE
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		DRAWING NO. C2405 /3	

We Engineer Hong Kong's Development

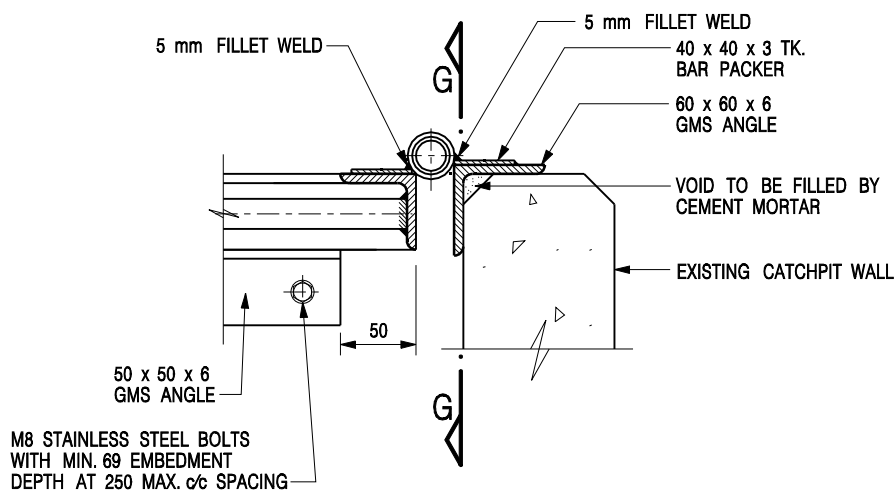


DETAIL 'G' - DETAILS OF STEEL GRATING CONSTRUCTED ON EXISTING CATCHPIT

SCALE 1 : 10



SECTION G - G




SECTIONAL ELEVATION

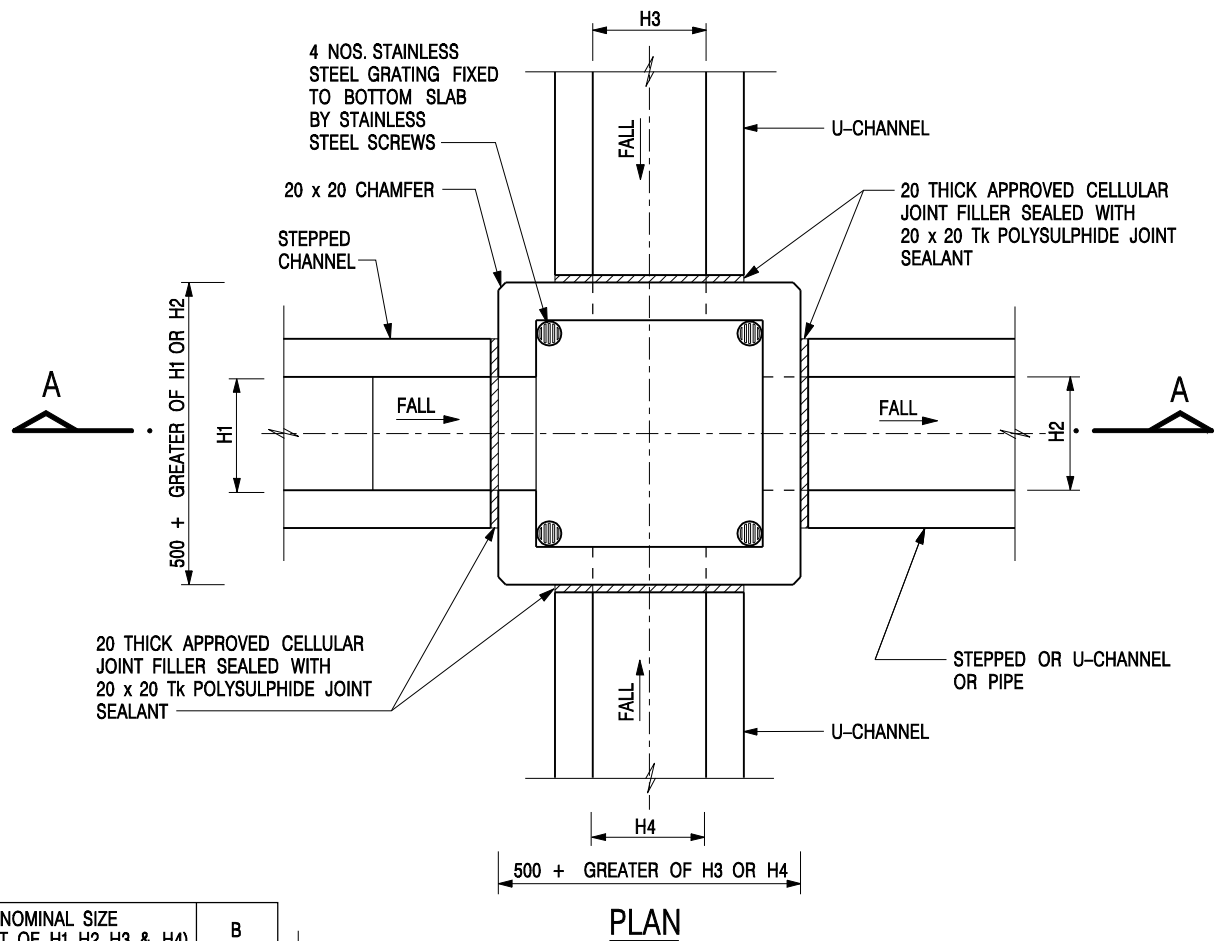
(DETAIL 'H')

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

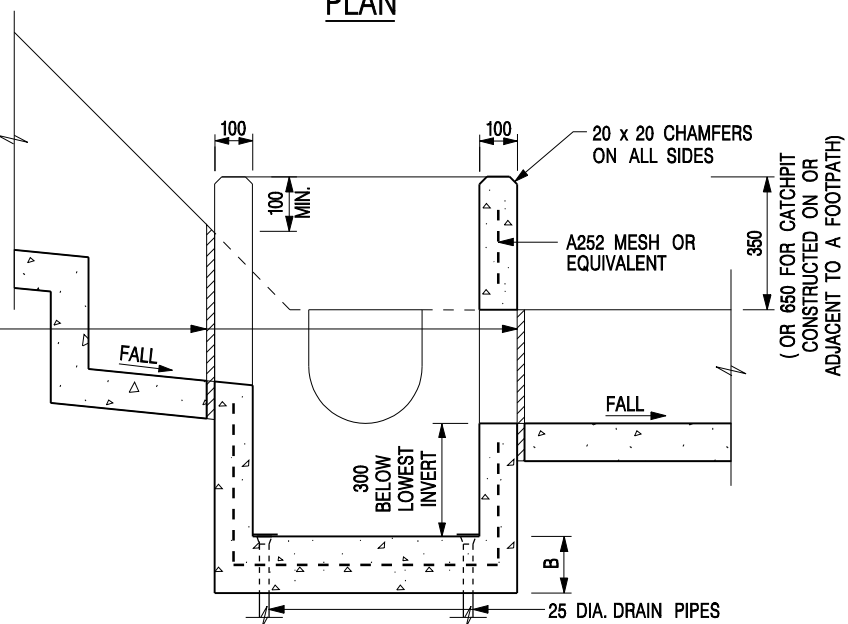
STANDARD CATCHPIT DETAILS
(SHEET 4 OF 5)

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REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE AS SHOWN		DRAWING NO. C2405 /4	
DATE JAN 1991			



NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175


20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT

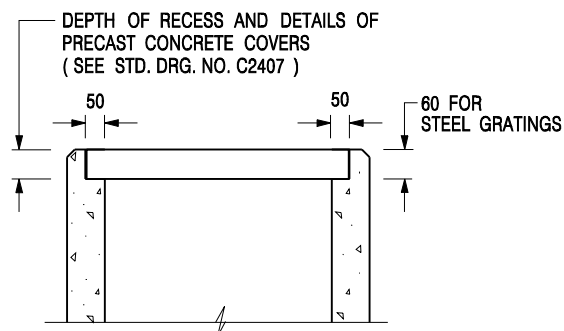


NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 2 FOR OTHER NOTES.

CATCHPIT WITH TRAP
(SHEET 1 OF 2)

-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE 1 : 20 DATE JAN 1991	
		DRAWING NO. C2406 /1	



ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAIL 'A' ON STD. DRG. NO. C2405 /2) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON STD. DRG. NO. C2405 /5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON STD. DRG. NO. C2405 /4.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

A	MINOR AMENDMENT.	Original Signed	04.2016
-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

CATCHPIT WITH TRAP
(SHEET 2 OF 2)



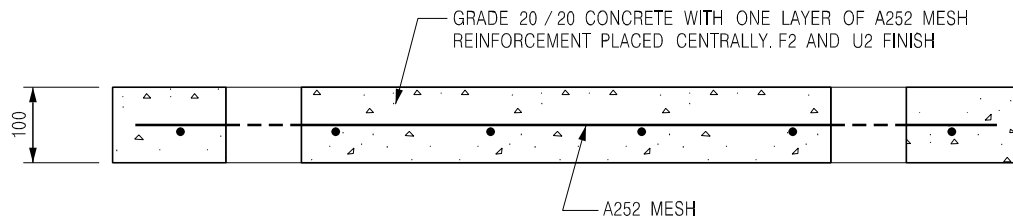
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

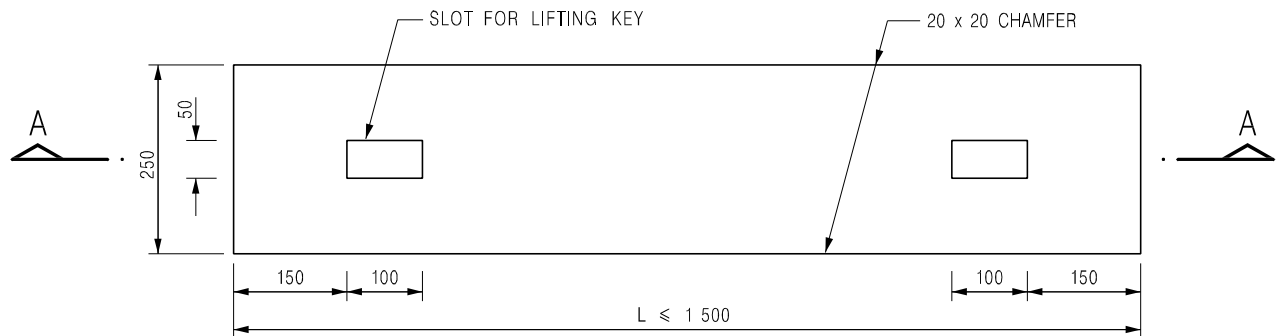
DATE JAN 1991

DRAWING NO.

C2406 /2A

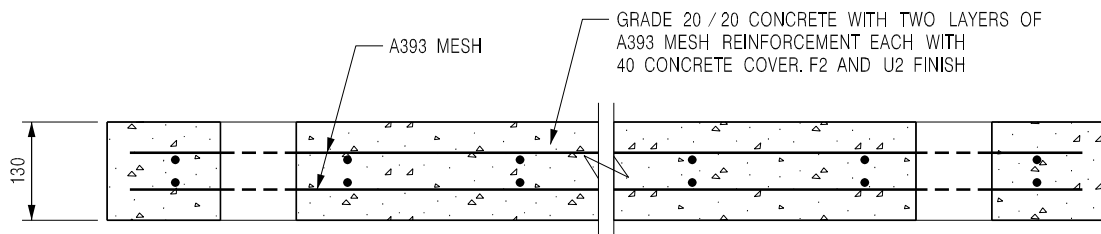


SECTION A - A

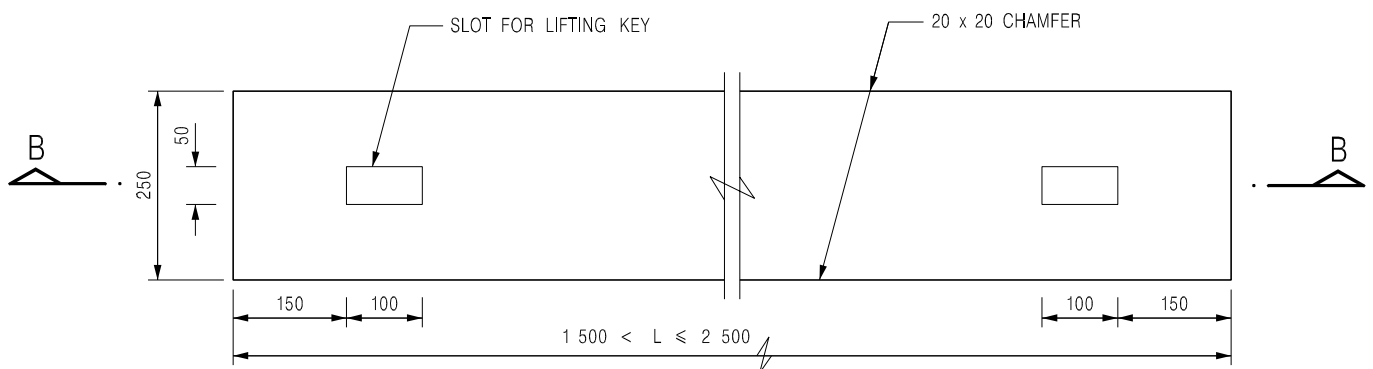


PLAN

TYPE 1 - FOR SPAN UP TO 1.5 m



SECTION B - B



PLAN

TYPE 2 - FOR SPANS 1.5 m TO 2.5 m

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL EXTERNAL EDGES OF THE COVERS SHALL BE 20mm CHAMFERED.

B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	GENERAL REVISION	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**PRECAST CONCRETE COVERS
FOR CATCHPIT AND SAND TRAP**

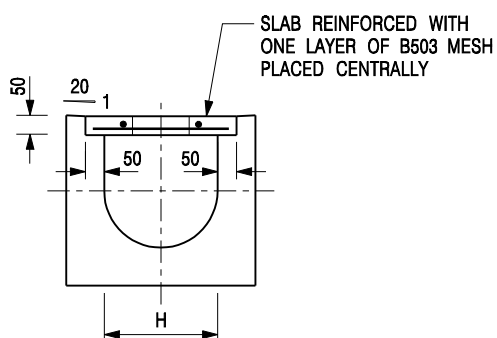


**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

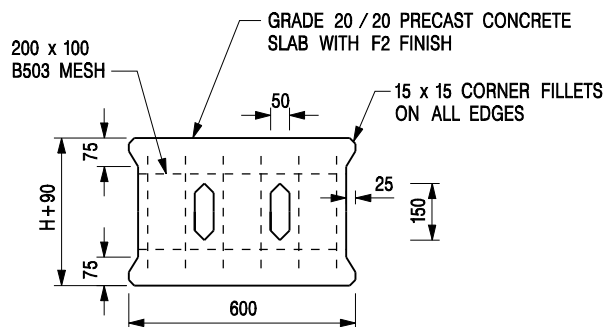
SCALE 1 : 10

DATE JAN 1991

DRAWING NO.
C2407B



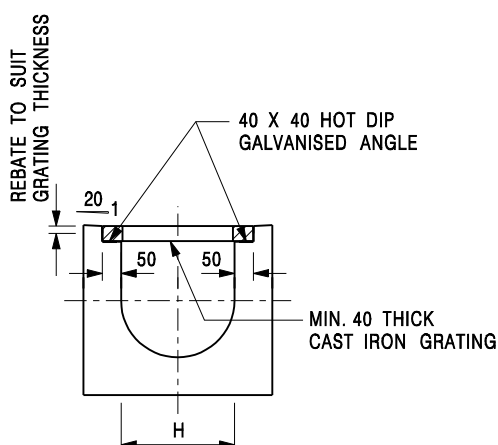
TYPICAL SECTION



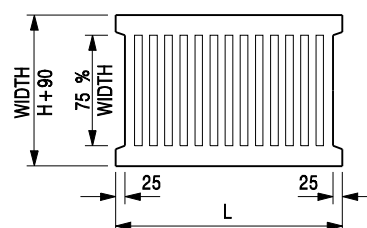
PLAN OF SLAB

U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION



L = 600mm FOR H ≤ 375mm
L = 400mm FOR H > 375mm

CAST IRON GRATING

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

COVER SLAB AND CAST IRON
GRATING FOR CHANNELS



**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

DATE JAN 1991

DRAWING NO.
C2412E



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Lawson David & Sung
SURVEYORS LIMITED



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Appendix Ia of RNTPC
Paper No. A/NE-TKL/813A



宋梓華
Sung Tze Wah
FRICS FHKIS MSISV MCIREA ACIarb R.P.S.(GP)

李霧儀
Lee Mo Yi
MPIA RPP MUDD BA (Hons)

吳恆廣
Ng Hang Kwong, BBS
FRICS FHKIS R.P.S.(GP)
Honorary World Valuer (WAVO)

林桂金
Daniel K.K. Lam
MRICS MHKIS MCIREA R.P.S.(GP) BSc

宋樹鴻
Sung Shu Hung
FRICS MHKIS R.P.S.(GP)(PD) MCIREA
MHIREA BSc (Hons)

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MRICS MHKIS MSc BBus (Mktg)

Consultant :

陳志凌
Elwyn C. Chan
RPE PMgr CEnv FIHE FCIOB MICE
MHKIE MSOE FCMI MCIArb MSc

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Lau Chi Kwong
FRICS FHKIS ALS MHKIS
R.P.S.(LS)(PFM) MSc

潘孝維
Pun How Wai
B.Arch.HK RIBA

By Email and Post

Date : 19 September 2025
Your Ref.: TPB/A/NE-TKL/813
Our Ref. : LDS/PLAN/7139

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road
North Point, Hong Kong

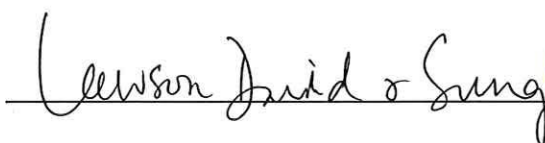

Dear Sir/Madam,

Application for Planning Permission for Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.
(Application No. A/NE-TKL/813)

We refer to the comments from various Government departments on the captioned application and submit herewith our response in **Table 1** for your consideration.

Should there be any queries, please contact our Ms. Cannis Lee or Mr. Jeremy Poon at [REDACTED]

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited

Encl.

c.c. DPO/STN (Attn.: Ms. Sheren Lee) – By Email
Client

Your Assets for Growth


Table 1: Responses to Comments from Government Departments on Planning Application No. A/NE-TKL/813 (19 September, 2025)

COMMENTS	RESPONSES
1. Agriculture, Fisheries and Conservation Department (AFCD)	
(a) We have no adverse comment on the application. Nevertheless, since the applicant site is near a night roost of egrets, the applicant should preserve existing trees within the application site as far as possible, avoid disturbing wild birds, and avoid directing lighting toward the night roost. The night roost is located at approximately 22.509670, 114.157288 (GPS coordinates).	(a) Noted. The Applicant will preserve the existing trees within the Application Site as far as possible. All the existing trees outside the Application Site will not be disturbed by the Applicant. There will be no directing lighting toward the night roost to the further west of the Application Site within the same “Open Storage” zone.
2. Environmental Protection Department (EPD)	
(a) Please advise whether cooking activities will be involved in the proposed eating place and if involved, please provide details including the floor space for the eating place.	(a) Cooking activities will be involved in the proposed eating place. The floor space of the eating place is about 806 sq.m. Proper licence/permit will be obtained from Food and Environmental Hygiene Department. The Applicant will also implement the necessary measures to control the emission of oily fumes and cooking odours in accordance with EPD’s guidelines “Control of Oily Fume and Cooking Odour from Restaurants and Food Business”, and obtain the necessary licence for its wastewater discharge under the Water Pollution Control Ordinance.
(b) Please confirm if the vehicle repair activities be carried out in enclosed structure?	(b) The vehicle repair activities will only be conducted in the enclosed structure. Please note that ‘vehicle repair workshop’ is an always permitted use within the “Open Storage” zone.
(c) The type of goods vehicles for delivery of materials	(c) The Applicant confirms that only light goods vehicles will be used for delivery of materials/food (see revised Figure 4).

COMMENTS	RESPONSES
3. Water Supplies Department (WSD)	
(a) <u>Major Comments on the Application/Main Reasons of Objection:</u> Nil	(a) Noted.
<p>(b) <u>Other Detailed Comments (if applicable):</u> Existing water mains inside the proposed site as shown in the MRP may be affected. The applicant is required to either divert or protect the water mains found on site.</p> <p>If diversion is required, existing water mains inside the proposed site areas are needed to be diverted outside the site boundary of the proposed site to lie in Government land. A strip of land of minimum 1.5m in width should be provided for the diversion of existing water mains. The cost of diversion of existing water mains upon request will have to be borne by the applicant; and the applicant shall submit all the relevant proposal to WSD for consideration and agreement before the works commence.</p> <p>If diversion is not required, the following conditions shall apply:</p> <p>(i) Existing water mains are affected as indicated on the site plan and no development which requires resiting of water mains will be allowed.</p> <p>(ii) Details of site formation works shall be submitted to the Director of Water Supplies for approval prior to commencement of works.</p> <p>(iii) No structures shall be built or materials stored within 1.5 metres from the centre line(s) of water main(s) shown on the plan. Free access shall be made available at all times</p>	<p>(b) The existing water mains is located at the eastern boundary of the Application Site (see Figure 4A). The Applicant will protect the water mains found on site. No structures will be erected on the water main and a minimum 1.5m buffer from the centre line of the water mains will be provided. As no diversion of water mains is required, the Applicant will follow the conditions (i) – (vi) as requested by WSD.</p>

COMMENTS	RESPONSES
<p>for staff of the Director of Water Supplies or their contractor to carry out construction, inspection, operation, maintenance and repair works.</p> <p>(iv) No trees or shrubs with penetrating roots may be planted within the Water Works Reserve or in the vicinity of the water main(s) shown on the plan. No change of existing site condition may be undertaken within the aforesaid area without the prior agreement of the Director of Water Supplies. Right root barriers may be required if the clear distance between the proposed tree and the pipe is 2.5m or less, and the barrier must extend below the invert level of the pipe.</p> <p>(v) No planting or obstruction of any kind except turfing shall be permitted within the space of 1.5 metres around the cover of any valve or within a distance of 1 metre from any hydrant outlet.</p> <p>(vi) Tree planting may be prohibited in the event that the Director of Water Supplies considers that there is any likelihood of damage being caused to water mains.</p>	
4. Secretary for Environment and Ecology (SEE)	
<p>(a) To echo with the latest version of Chapter 8 of Hong Kong Planning Standards and Guidelines (HKPSG) about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the applicant is suggested to comply with the relevant requirement of HKPSG, i.e. EV chargers with output power of not less than 7kW (i.e. medium chargers) should be installed in all parking spaces for private cars, light goods vehicles and motorcycles of the Site.</p>	<p>(a) Noted. The Applicant will only provide 18 charging spaces for electric taxis which are all equipped with EV chargers with output power of 120kW (i.e. not less than 7kW). The no. of charging spaces provided is determined by the available power supply. Other spaces within the Application Site are designated as waiting spaces for electric taxis and parking spaces to serve the supplementary services of the proposed EV service depot, so as to provide adequate parking spaces and avoid illegal parking/queuing of vehicles outside the Application Site.</p>

COMMENTS	RESPONSES
5. Drainage Services Department (DSD)	
(i) No objection to the application from the public drainage viewpoint;	(i) Noted.
(ii) the Site is in an area where no public sewerage connection is available. Environmental Protection Department should be consulted regarding the sewage treatment/disposal facilities for the proposed use;	(ii) Noted.
(iii) her comments on the submitted drainage proposal are as follows:	(iii) A revised drainage proposal (see Annex 1) is submitted and our responses are as follows:
<ul style="list-style-type: none"> - the applicant should review if the proposed discharge point, i.e. 450mm UC, has adequate capacity and satisfactory condition to cater for the additional discharge from the 525mm UC of the Site. Alternative/multiple discharge points should be considered. The applicant should ensure that the flow from the Site will not overload the existing drainage system; 	<ul style="list-style-type: none"> - A new 525mm stepped channel is proposed to carry the flow of the Subject Site directly into Ng Tung River as shown in the attached Figure D4 (revision A).
<ul style="list-style-type: none"> - the applicant should provide photos to the proposed discharge point and further drainage downstream; 	<ul style="list-style-type: none"> - Photos to the proposed discharge point and further drainage downstream are provided below (please refer to the attached Figure D3 (revision A) for the locations of photo taken):

COMMENTS	RESPONSES
	<div data-bbox="1182 220 2103 758"></div> <div data-bbox="1182 758 2103 837"><p>Plate 14 – Existing 450mm stepped channel discharging into Ng Tung River close to the northern boundary of the Subject Site</p></div> <div data-bbox="1182 837 2103 1375"></div> <div data-bbox="1182 1375 2103 1457"><p>Plate 15 – Existing 600mm stepped channel discharging into Ng Tung River at the downstream of the Subject Site</p></div>

COMMENTS	RESPONSES
<ul style="list-style-type: none"> - the applicant should indicate the catchpit with sand trap on the proposed drainage plan; - the cover levels of proposed channels should be flush with the existing adjoining ground level; - should the applicant choose reconnect his proposed drainage systems to DSD's networks, they shall furnish her with their connection proposal for agreement. After obtaining her agreement, the applicant shall submit a duly completed Form HBPI with a cross cheque covering the technical audit fee and a plan showing the details of the proposed connection works to the division for formal application for the required connection. Upon her 	<div data-bbox="1182 220 2101 758" data-label="Image"> </div> <p>Plate 16 – View toward the further downstream of the Subject Site along Ng Tung River</p> <ul style="list-style-type: none"> - The catchpit with trap is shown in the attached Figure D4 (revision A) – ‘Proposed Stormwater Drainage Layout Plan’. - Cover levels of all the proposed channels would be flush with the existing adjoining ground level. - Noted and will follow if considered necessary by the Government departments.

COMMENTS	RESPONSES
<p>acceptance of the connection application, the applicant shall carry out the proposed connection works in accordance with DSD Standard Drawings at the resources of the applicant;</p> <ul style="list-style-type: none"> - the proposed drainage works, whether within or outside the site boundary, should be constructed and maintained properly by the applicant and rectify the system if it is found to be inadequate or ineffective during operation at his/her own expense; - the applicant shall allow all time free access for the Government and its agent to conduct site inspection on his completed drainage works; - the applicant and the successive lot owners shall allow connections from the adjacent lots to the completed drainage works on Government Land when so required; - for works to be undertaken outside the lot boundary, the applicant should obtain prior consent and agreement from DLO/N and/or relevant private lot owners; and - the applicant should make good all the adjacent affected areas upon the completion of the drainage works. 	<ul style="list-style-type: none"> - Noted and agreed. - Noted and agreed. - Noted and agreed. - Noted and agreed. - Noted and agreed.
6. Planning Department (PlanD)	
(a) It is understood that the applicant is collaborating with one of the selected taxi fleet operators. Please advise if only	(a) The Applicant is in partnership with one of the selected taxi fleet operators, Sino Development (International) Company Limited, to convert the Application Site into an EV service depot. Only taxis

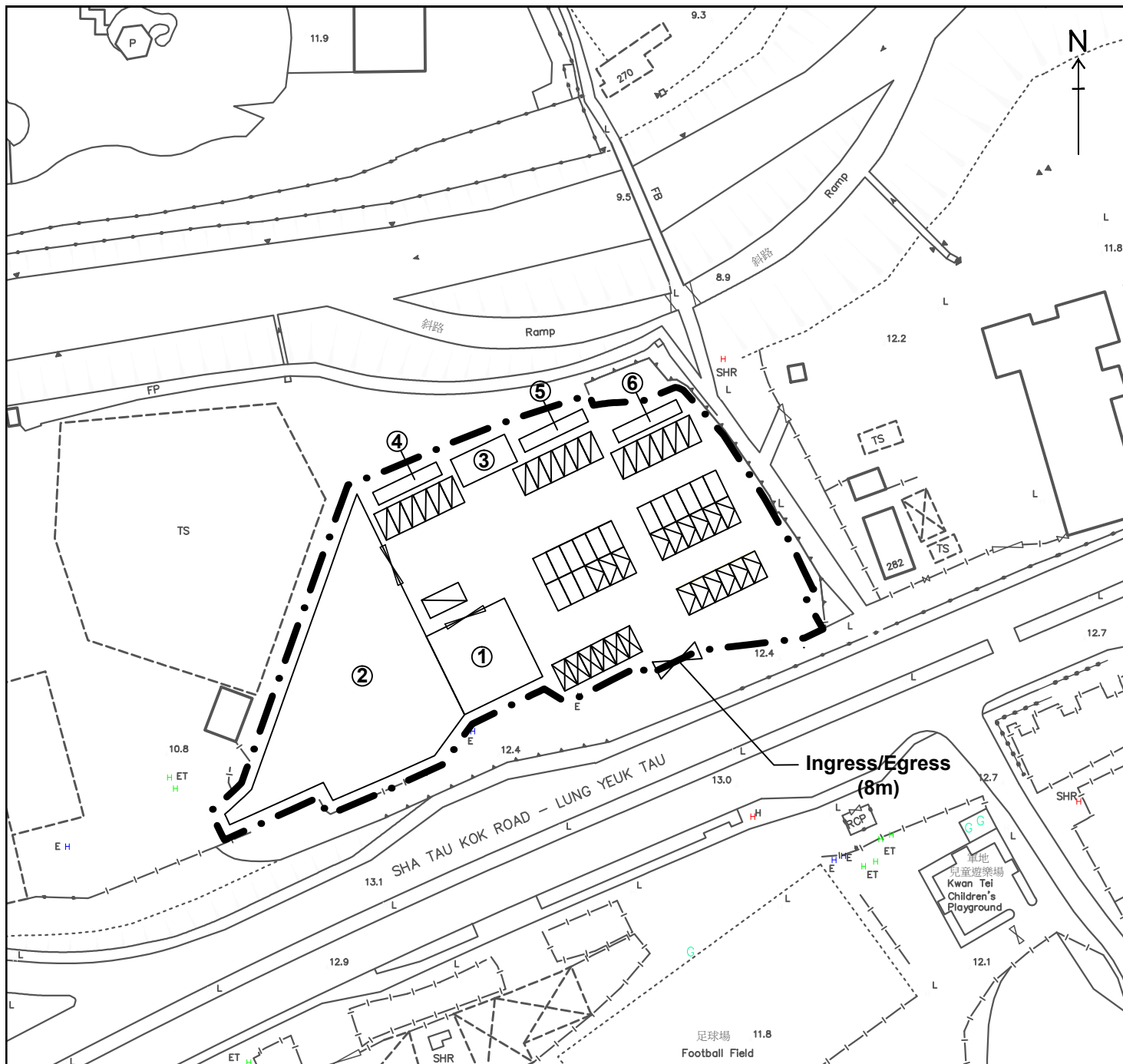
COMMENTS	RESPONSES
the taxis under that taxi fleet will be allowed to charge at the site.	belonging to that fleet will be permitted to charge at the Application Site. The charging facilities will not be open for public use.
(b) Are there are any measures to prevent the taxis from parking at the site after charging?	(b) The on-site staff will request electric taxis to depart promptly after charging. Under normal circumstances, electric taxis will depart from the Application Site to collect passengers once they are fully charged. Should taxi drivers require other services (such as catering), which prevent them from leaving the Application Site, they may park in designated waiting spaces. As different types of parking spaces will be provided at the Application Site, staff will also allocate parking spaces flexibly according to prevailing conditions.
(c) Do the proposed hours for supplementary services, i.e. 7 a.m. to 8 p.m. from Mondays to Sundays, include public holidays as well?	(c) The operation hours for supplementary services is from 7 a.m. to 8 p.m. from Mondays to Sundays, include public holidays. The proposed eating place will mainly serve the taxi drivers and staff.
(d) What kind of cars will be shown in the vehicle showroom?	(d) The Application Site is an EV service depot and therefore the motor vehicle showroom will only display private vehicles intended for use as electric taxis. The vehicles for sale will arrive the Application Site with T-plates and be stored in the motor vehicle showroom.
(e) Does the proposed vehicle repair workshop serve all EVs or just electric taxis?	(e) The proposed vehicle repair workshop will only serve electric taxis belonging to that taxi fleet.
(f) The applied use	(f) It is confirmed that the applied use should be “Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with <u>Ancillary Office</u> for a Period of 5 Years”.

COMMENTS	RESPONSES
(g) The intended use of different parking spaces	<p>(g) We would like to clarify the intended use of different parking spaces as follows:</p> <ul style="list-style-type: none"> (i) 18 EV charging spaces for electric taxis: these will be used by electric taxis for charging purpose; (ii) 15 parking spaces/waiting spaces for electric taxis: these will be used by taxi drivers while they wait for their vehicles to charge or after they have finished charging; (iii) 15 parking spaces for private vehicles (for motor vehicle showroom and office use): these will be used by visitors to the motor vehicle showroom and office staff. Electric taxis to be displayed will be stored in the motor vehicle showroom; (iv) 6 parking spaces for staff/visitors: these will be used by staff and visitors to the eating place.
7. Public Comments	
(a) One comment was received from a private individual expressing concerns on environmental, traffic, drainage, sewerage and fire hazard aspects.	<p>(a) <u>Environmental Concern</u></p> <p>The EV charging station will equip the charging equipment with small cooling fans inside to ventilate the produced heat out during their operations. The small cooling fans will also be placed inside the containers and no noise nuisance is expected. The Applicant will provide good air circulation system within the Application Site so that no air pollution nuisance will be created. Since the Application Site has been hard-paved, limited dust nuisance is foreseen from vehicle manoeuvring from the proposed development.</p> <p>The Application Site is currently occupied by a cluster of vehicle repair workshops which is always permitted within the subject “OS” zone. Compared with existing use on the Application Site, the proposed vehicle repair workshop will only serve electric taxis and all workshop</p>

COMMENTS	RESPONSES
	<p>activities will only be undertaken within the enclosed structure. The proposed use is a more clean and environmental friendly use.</p> <p>The no. of vehicles visiting the Application Site can be under control as only 18 charging spaces for electric taxis will be provided within the Application Site. Various measures will be adopted by the Applicant to control the traffic flow. A Traffic Impact Assessment has been submitted and confirmed that the proposed development will not induce additional adverse traffic impact on the surrounding road network.</p> <p>The Applicant will follow the environmental measures recommended in the latest “Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites” issued by EPD. The Applicant will also obtain the necessary licence for its wastewater discharge under the Water Pollution Control Ordinance. Good site practice will be implemented to further minimize environmental nuisance to the surrounding areas.</p> <p><u>Traffic Concern</u></p> <p>Since the Applicant is in partnership with one of the selected tax fleet operators, only taxis belonging to that fleet will be permitted to charge at the Application Site. The Applicant will also implement the following traffic management measures to ensure no queuing of electric taxis outside the Application Site:</p> <p>(i) On-site staff will be deployed to manage the in and out traffic flow. Vehicles entering the site would be directed to the charging/waiting spaces to prevent blocking of the ingress/egress;</p>

COMMENTS	RESPONSES
	<p>(ii) On-site staff will request electric taxis to depart promptly after charging or park in designated waiting spaces if they require other services within the proposed development;</p> <p>(iii) The mobile application adopted by the Applicant will provide real-time availability of EV charging spaces and facilitate the electric taxi drivers to locate the available EV chargers. The drivers will be directed to the nearest alternative sites if all the EV chargers at the Application Site are in use. The on-site staff will also assist and direct the drivers to other alternative sites to avoid drivers waiting outside the Application Site;</p> <p>(iv) Prior appointment will be arranged for light goods vehicles to deliver the materials; and</p> <p>(v) All goods vehicles will leave the Application Site after loading/unloading activities.</p> <p><u>Drainage and Sewerage Concerns</u></p> <p>Further to the comments from the DSD, the Applicant has revised the drainage proposal (see Annex 1) and will provide the drainage facilities to the satisfaction of the DSD, in which no adverse drainage impact on the surrounding areas from the proposed development are foreseen.</p> <p>In regards of sewerage treatment, the Applicant will provide septic tank and soakaway pits according to Professional Persons Environmental Consultative Committee Practice Notes (“ProPECC PN”) No.1/23 for sewage treatment and dispose at the Application Site. All waste water collected from the kitchen, including that from basins, sinks and floor drains, will be discharged via a grease trap in accordance with this ProPECC PN No.1/23 to prevent any water</p>

COMMENTS	RESPONSES
	<p>pollution. As such, no sewerage impact on the surrounding areas is anticipated.</p> <p><u>Fire Hazard Concern</u></p> <p>Upon approval of this application, the Applicant will submit a fire service installations (FSIs) proposal to elaborate the FSIs provision with Firemen's Emergency Switch within the proposed development, which will be in compliance with the requirements of the Fire Services Department (FSD) and relevant regulations. The Applicant will also provide the FSIs to the satisfaction of FSD. Therefore, no fire hazard concerns from the proposed development will be foreseen.</p>
<p>(b) Two comments were received from private individuals expressing concerns on pedestrian safety, environmental and destroy the home of villagers residing there.</p>	<p>(b) It is noted that the tenancy agreement between the owners of the Application Site and the two objectors, who are the tenants ended in July 2021, but the tenants refused to move out. The owners will take further action to prevent illegal trespass.</p>



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	1,006 806 + 200	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- EV Charing Spaces for Electric Taxi (18 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Light Goods Vehicle (3.5m x 7m) (1 no.)

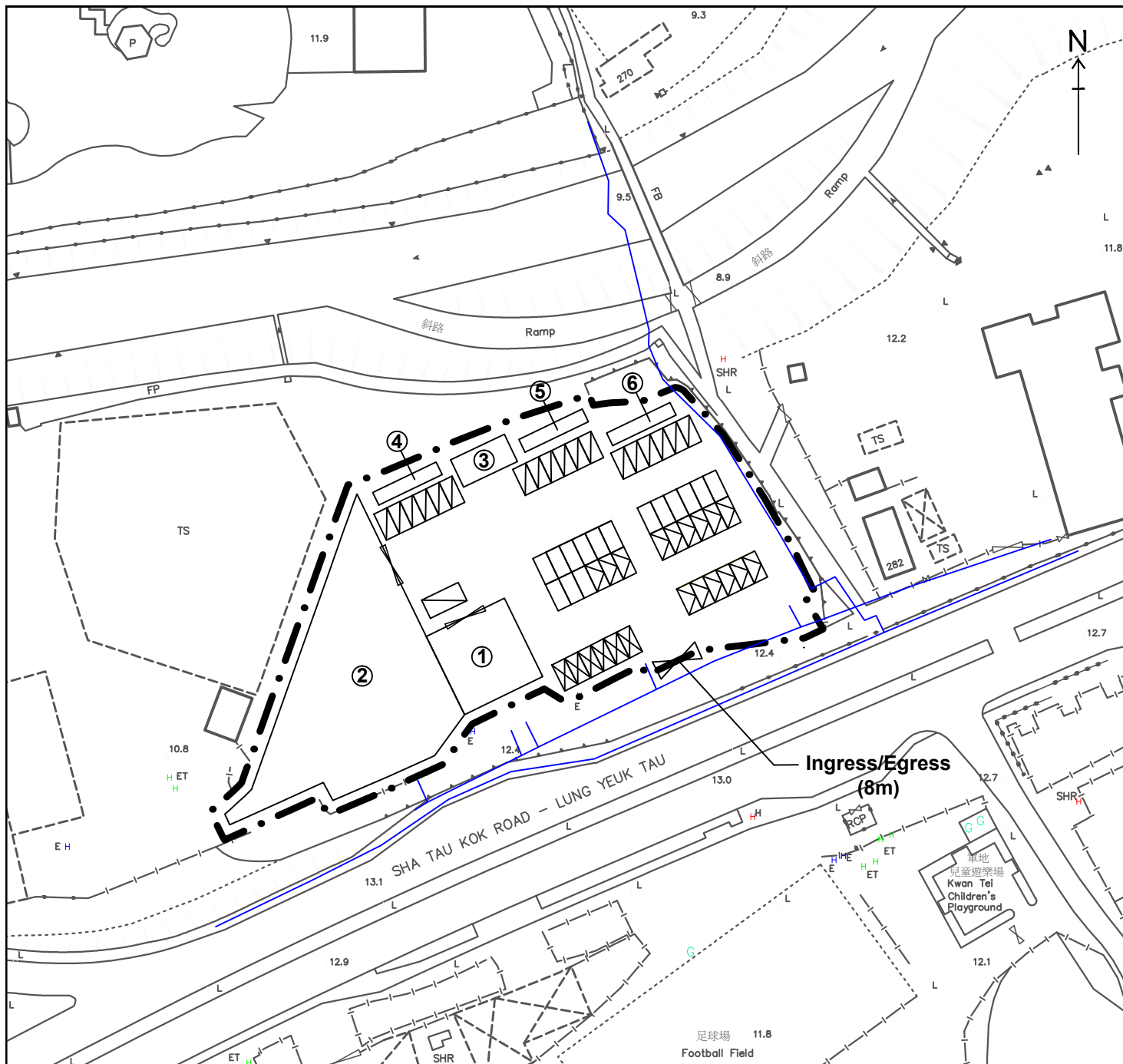
Layout Plan

Figure 4

Scale 1:1000



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 Estate Agency Licence No. C-006328



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
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Legend:

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- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Light Goods Vehicle (3.5m x 7m) (1 no.)
- Water Mains

Layout Plan

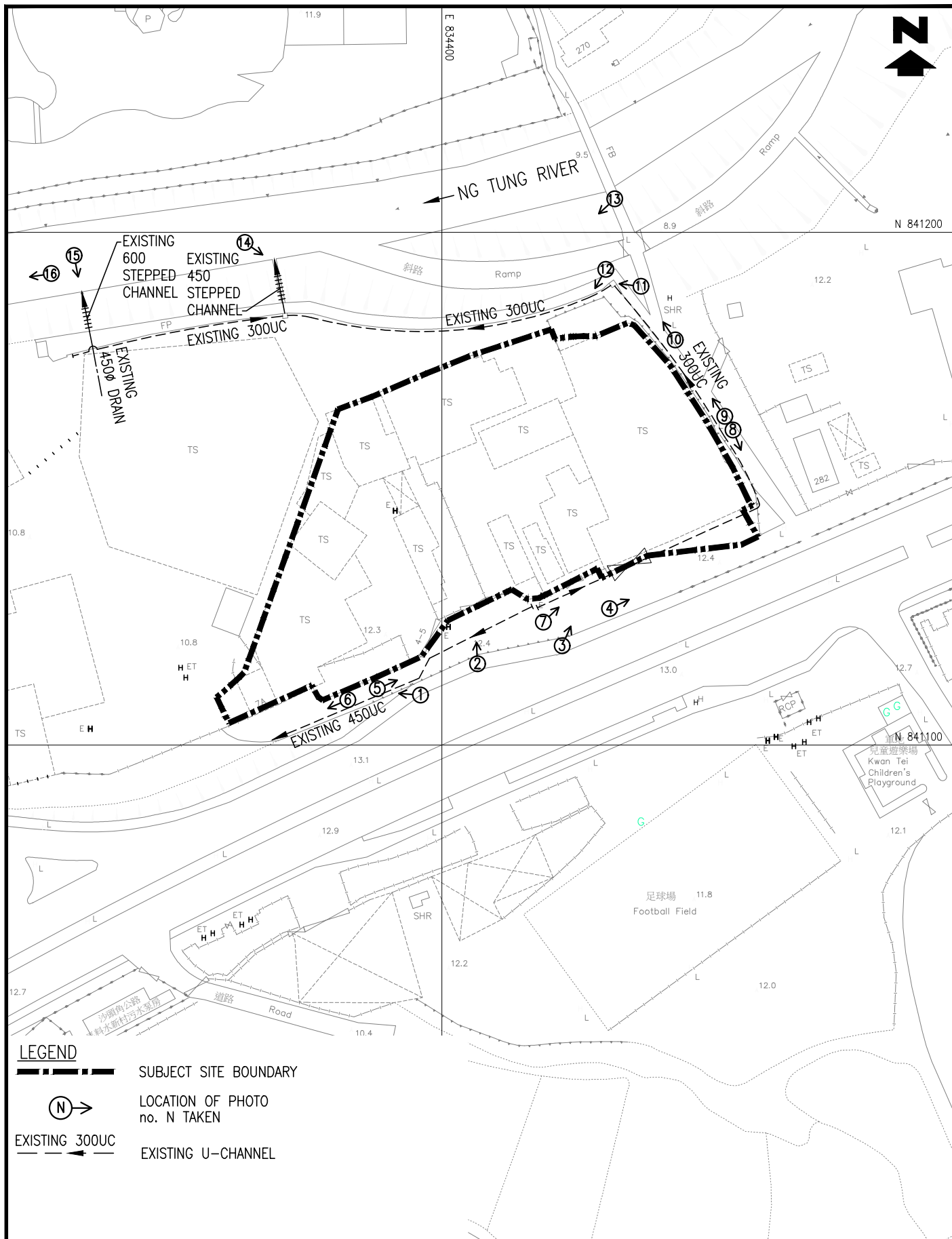
Figure 4A

Scale 1:1000



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H:\25039_LungYeukTau_Site\DRAWING\25039_FIGD2_01 FIGD3A_00 FIGD4A_00 & FIGD5_00.dwg, 18/9/2025 16:30:00



TITLE

EXISTING DRAINAGE AND LOCATIONS OF PHOTO TAKEN

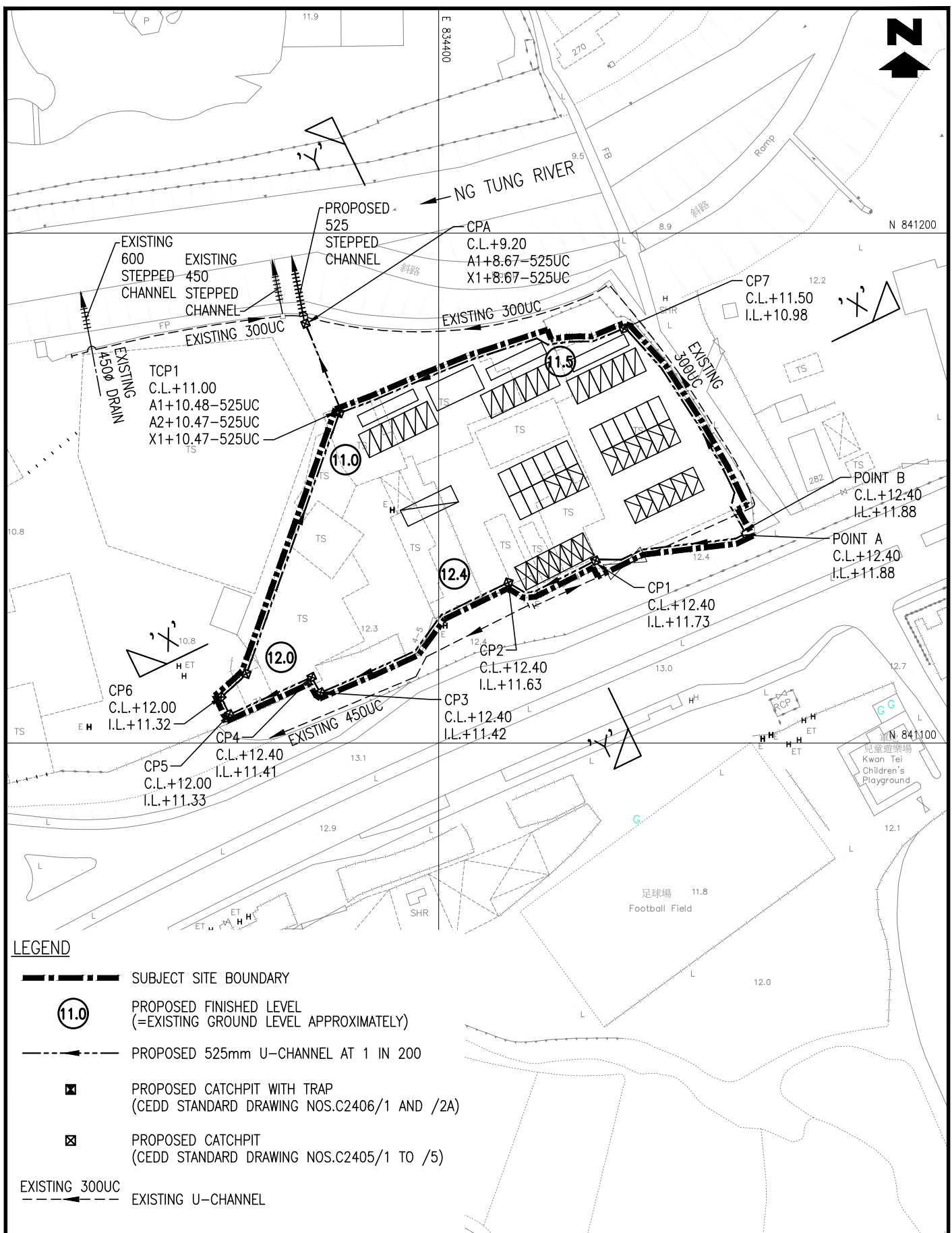
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED







SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D3
(REVISION A)



 SUBJECT SITE BOUNDARY
 PROPOSED FINISHED LEVEL
 (=EXISTING GROUND LEVEL APPROXIMATELY)
 PROPOSED 525mm U-CHANNEL AT 1 IN 200
 PROPOSED CATCHPIT WITH TRAP
 (CEDD STANDARD DRAWING NOS.C2406/1 AND /2A)
 PROPOSED CATCHPIT
 (CEDD STANDARD DRAWING NOS.C2405/1 TO /5)
 EXISTING 300UC
 EXISTING U-CHANNEL

PROPOSED STORMWATER DRAINAGE LAYOUT PLAN

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

1 : 1000 - A4

FIGURE D4
(REVISION A)

Drainage Submission in support of

S16 Planning Application for

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office
for a Period of 5 Years in “Open Storage” zone

at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining
Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling,
New Territories

(HT25039)

September 2025

(Revision A)

Drainage Consultant:

何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED



Prepared & approved by	LEE Kwok Cheung <i>RPE(Civil)</i>	
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1. Background

- 1.1 With respect to a S16 Planning Application for Proposed Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories, Messrs. Ho Tin & Associates Consulting Engineers Limited was appointed to prepare a drainage submission.

2. Approach to Prepare this Proposal

- 2.1 This Drainage Submission is prepared in line with the “Technical Note to prepare a Drainage Submission (Relating to applications for temporary change of land use such as temporary storage areas, car parks, workshops, small factories ... etc. under S.16 of the Town Planning Ordinance)” issued by Drainage Services Department in December 2024.

3. The Subject Site and Proposed Development

- 3.1 The subject site with a total site area of about 4,190m² comprises of Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land (about 680m²), Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories. The subject site is to the south of Ng Tung River and on the north side of Sha Tau Kok Road – Lung Yeuk Tau opposite to Kwan Tei Children’s Playground. A Site Location Plan is shown in **Figure D1**.
- 3.2 The subject development consists of one single-storey motor vehicle showroom structure (5m high, total floor area = 225m²), one two-storey vehicle repair workshop cum eating place and ancillary office structure (7m high, total floor area = 2,012m²), one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m²), and 3 numbers of container for EV chargers (3m high, total floor area = 29m² each) accompanying with 18 numbers of EV charging spaces for electric taxi, 30 numbers of parking spaces/waiting spaces for electric taxi, 6 numbers of parking spaces for staff/visitors, and 1 number of loading/unloading space for medium goods vehicle. A plan showing the proposed site layout is in **Figure D2**.

4. Existing Drainage Conditions of the Site

- 4.1 At present, the subject site is hard paved, partitioned into sub-units and partly used for storage use and car repairing workshop and partly vacant (refer to **Plate 1** to **4**).
- 4.2 The subject site is bounded by a footpath on the east and north sides (refer to **Plate 8** to **12**). There is existing 300mm U-channel running between the subject site and the aforementioned footpaths collecting surface runoff of the subject site and the adjacent areas. The existing 300mm U-channel discharges its collected flows via an existing 450mm stepped channel into Ng Tung River to the further north (refer to **Plate 13** and **14**).
- 4.3 Outside the southern subject site boundary is currently used as access of the subject site and its nearby areas beside Sha Tau Kok Road – Lung Yeuk Tau. There is existing 450mm U-channel serving the access (refer to **Plate 5** to **7**).
- 4.4 To the west of the subject site is another development of which the surface runoff would be discharged via an existing 600mm stepped channel into Ng Tung River (refer to **Plate 15** and **16**).
- 4.5 Current conditions of the subject site and its existing drainage conditions are shown in the following photos (photo taking locations are shown on **Figure D3**):



	
<p>Plate 1 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (1 of 4)</p>	<p>Plate 2 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (2 of 4)</p>



Plate 3 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (3 of 4)



Plate 4 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (4 of 4)



Plate 5 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (1 of 2)



Plate 6 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (2 of 2)



Plate 7 – Existing 450mm U-channel running eastward along the existing access in front of the southern subject site boundary



Plate 8 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (1 of 3)







	
<p>Plate 9 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (2 of 3)</p>	<p>Plate 10 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (3 of 3)</p>
	
<p>Plate 11 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (1 of 2)</p>	<p>Plate 12 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (2 of 2)</p>
	
<p>Plate 13 – The section of Ng Tung River outside the northern subject site boundary</p>	<p>Photo 14 – Existing 450mm stepped channel discharging into Ng Tung River close to the northern boundary of the Application Site</p>



Plate 15 – Existing 600mm stepped channel discharging into Ng Tung River at the downstream of the Application Site



Plate 16 – View toward the further downstream of the Application Site along Ng Tung River



Extract of the Drainage Records from LandsD's GeoInfo Map in May 2025

5. Drainage Assessment and Proposal

- 5.1 The subject site is a simple small site with a total site area of about 4,190m² (less than 1 ha in size). The existing subject site levels are generally same as those of the area close to Sha Tau Kok Road – Lung Yeuk Tau and relatively higher than the surroundings at the remaining three sides. There are existing surface channels intercepting surface runoff on the area between the subject site and Sha Tau Kok Road – Lung Yeuk Tau. In general, surface runoff flowing toward the subject site would be collected and conveyed away the subject site by the existing surrounding channels.

- 5.2 The existing site levels would be maintained and no site formation/leveling works would be carried out such that the subject proposed development would be maintained at levels relatively higher than its adjacent developments/areas to its east, west and north, and would be at similar levels of the existing access running along its southern boundary. In principle, surface runoff of the surrounding areas would be collected and conveyed away by the existing channels in the adjacent areas of the subject site, hence, any new solid fence wall of the subject site would not obstruct any existing overland flows (in addition, it should be noted that the existing levels (similar to the proposed finished levels) of the subject site are already relatively higher than those to the east, west and north). Nevertheless, for conservative, in order to ensure no surface runoff from the southern side (at levels close to the proposed finished levels of the subject development), due to any unexpected reasons, flowing onto the subject site to be obstructed, 100mm high gap will be formed at the bottom of the security hoarding/fence wall along the southern subject site boundary. Cross sections of the proposed subject development and typical details of the gap at the bottom of proposed security hoarding/fence are shown in **Figure D5**.
- 5.3 With respect to the “Technical Note to prepare a Drainage Submission” (the “TN”) published by Drainage Services Department, for the subject site area = $4,190\text{m}^2 \leq 5,000\text{m}^2$, peripheral 525mm U channel at 1 in 200 gradient would be appropriate. It is therefore proposed to construct new peripheral 525mm U channel at 1 in 200 gradient for the subject development. A new catchpit with trap will be constructed at the end of the proposed 525 U channels near the northwest corner of the subject site before discharging into a proposed 525mm stepped channel at Ng Tung River. A Proposed Stormwater Drainage Layout Plan is shown in **Figure D4**.
- 5.4 Besides, the Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the drainage system to the satisfaction of relevant Government departments.
- 5.5 Details of proposed drainage provisions shall follow relevant details shown in Government Departments’ Standard Drawings as follows:

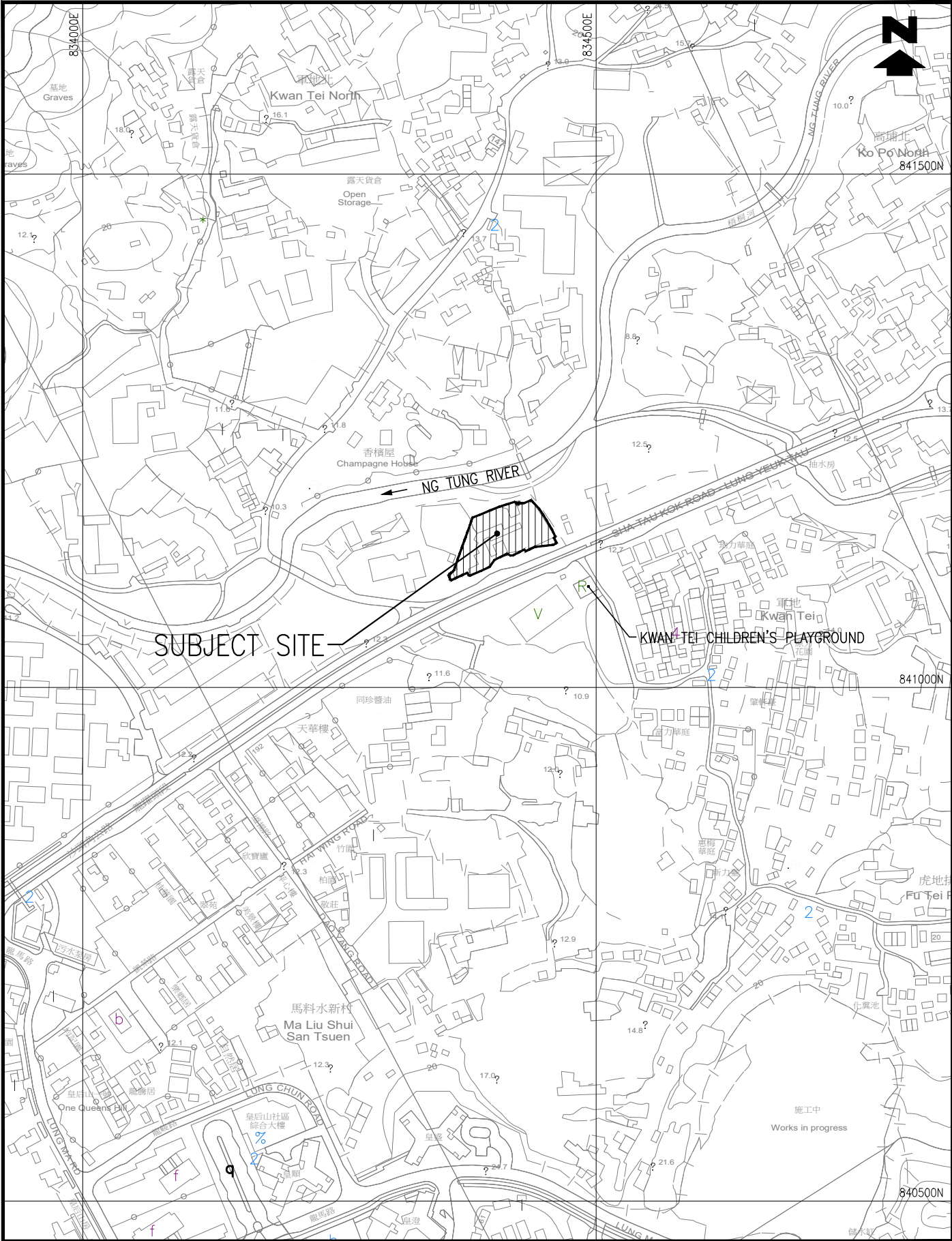
<i>Proposed Drainage Provisions</i>	<i>Standard Drawings</i>	<i>Drawing No. & Title</i>
Catchpit	CEDD Standard Drawings	C 2405/1 to /5 – Standard Catchpit Details
Catchpit with trap		C 2406/1 to /2A – Catchpit with Trap
Catchpit precast concrete cover		C 2407B – Precast Concrete Covers for Catchpit and Sand Trap
U-channel		C 2409J – Details of Half-round and U-channels
Channel cover		C 2412E – Cover Slab and Cast Iron Grating for Channels

6. Conclusion and Recommendations

- 6.1 The subject development as Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office will be for temporary use for a period of 5 years. The subject site area has been hard paved for a very long period without complaints on drainage conditions. The existing site levels would be maintained and no site formation/leveling works would be carried out.
- 6.2 The subject site is at present served by surrounding surface channels and its levels are relatively higher than those to its east, west and north. In principle, no surface runoff would flow onto the subject site from its surroundings. 525mm U channel at 1 in 200 gradient will be constructed at the peripheral of the subject site to intercept all crossing surface runoff. For conservative, 100mm high gap will be formed at the bottom of the security hoarding/fence along the southern subject site boundary to ensure no surface runoff from the adjacent, due to any unexpected incidents, to be obstructed. A new catchpit with trap will be constructed at the ends of the 525mm U channel before discharging into the proposed new 525mm stepped channel at Ng Tung River.

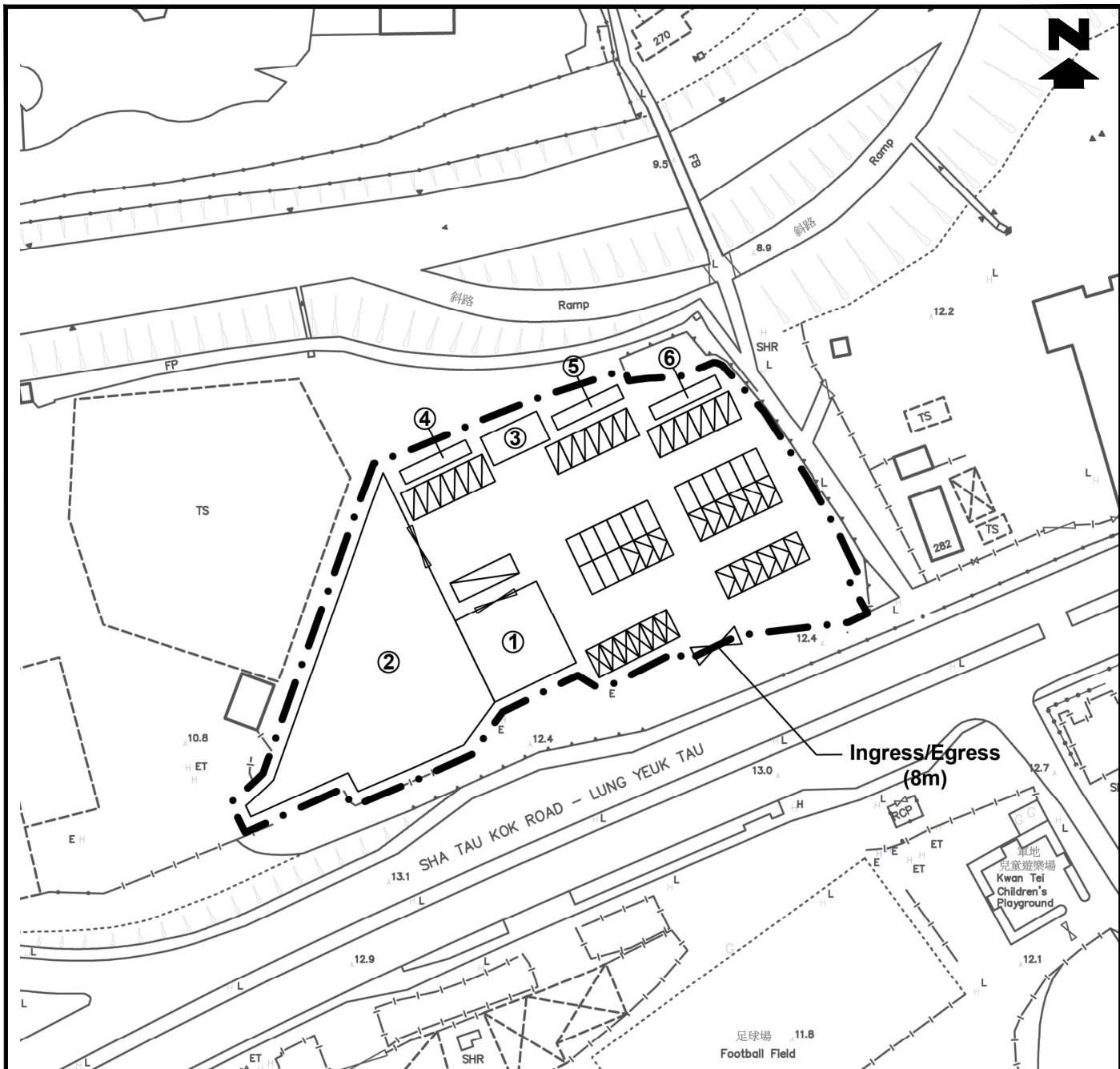
- 6.3 The Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the system to the satisfaction of relevant Government departments.
- 6.4 The subject development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works, i.e. debris clearance and damage repair. Therefore, in conclusion, the subject development would not cause any adverse drainage impact onto the area.

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TITLE	何 田 顧 問 工 程 師 有 限 公 司 HO TIN & ASSOCIATES CONSULTING ENGINEERS LIMITED	
	SCALE 1 : 5000 – A4	DRAWING No. FIGURE D1
SITE LOCATION PLAN		

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Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	2,012	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- EV Charing Spaces for Electric Taxi (18 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Medium Goods Vehicle (3.5m x 11m) (1 no.)

TITLE

LAYOUT PLAN

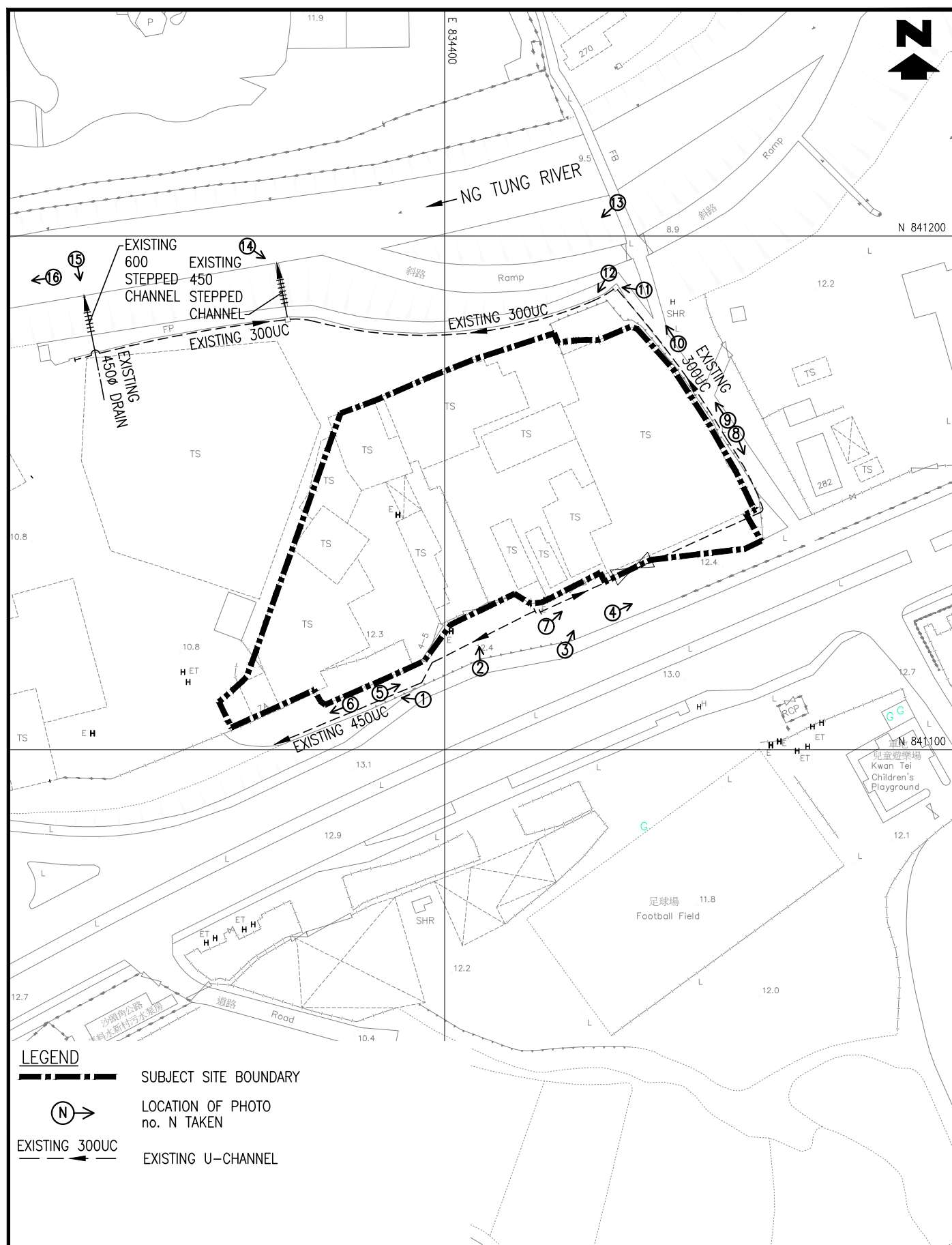
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CONSULTING ENGINEERS LIMITED

SCALE

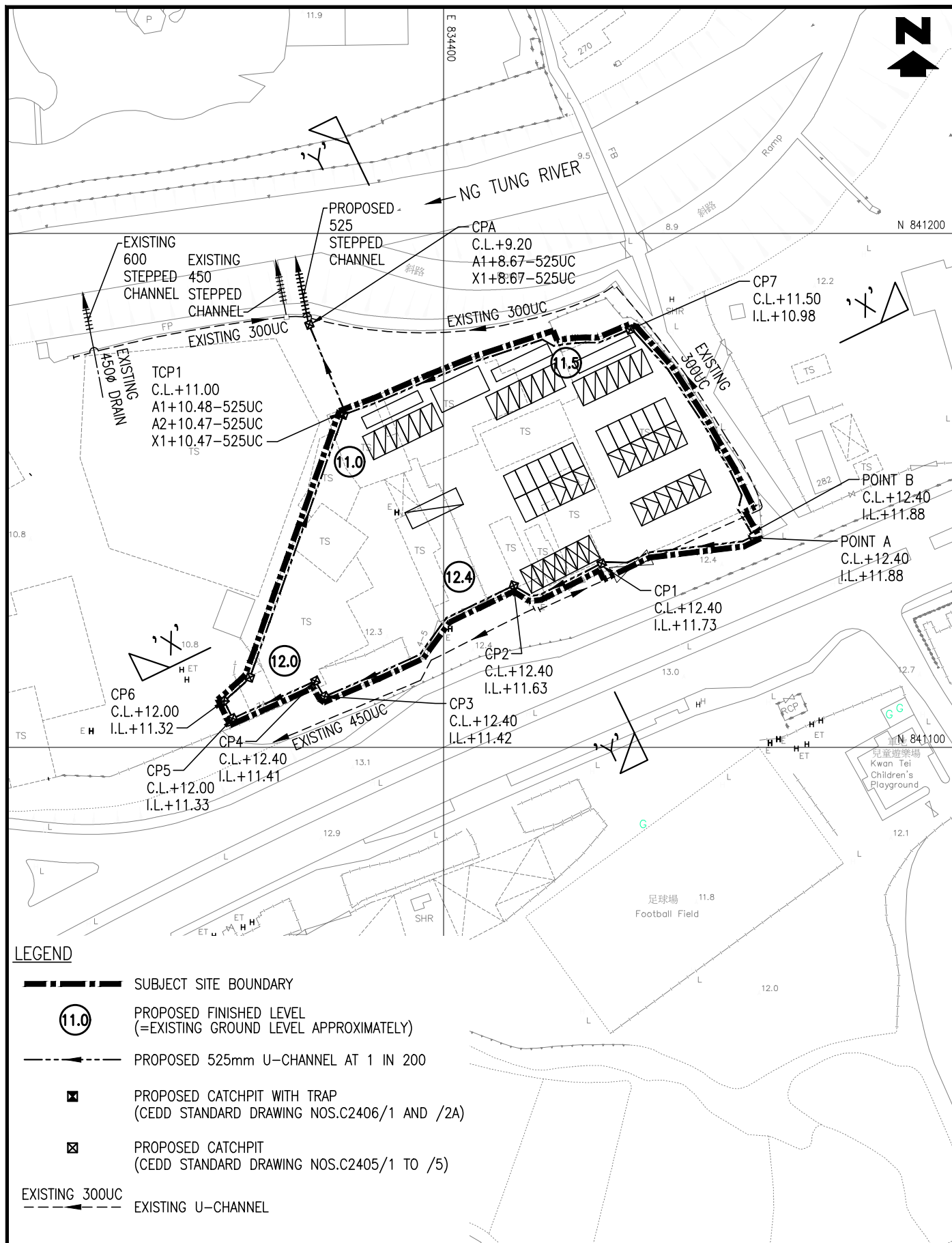
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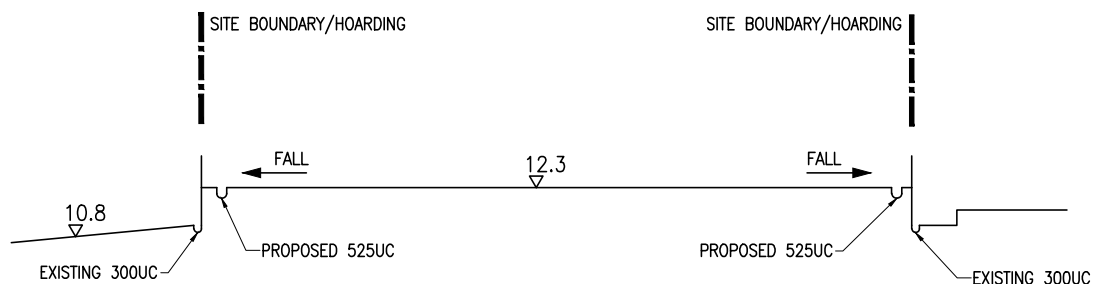
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FIGURE D2

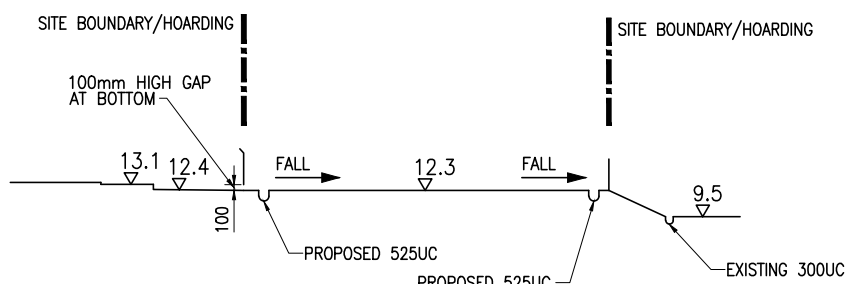


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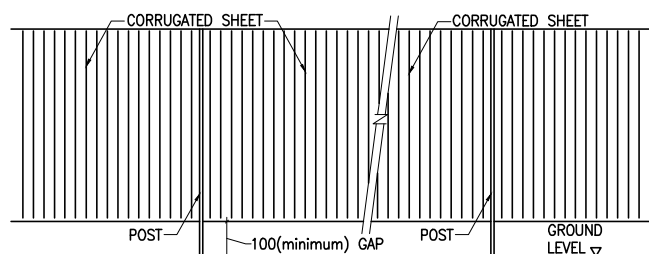




SECTION 'X'-'X'



SECTION 'Y'-'Y'



TYPICAL ELEVATION OF THE HOARDING N.T.S.
WITH 100mm HIGH GAP AT BOTTOM

TITLE

SITE CROSS SECTIONS

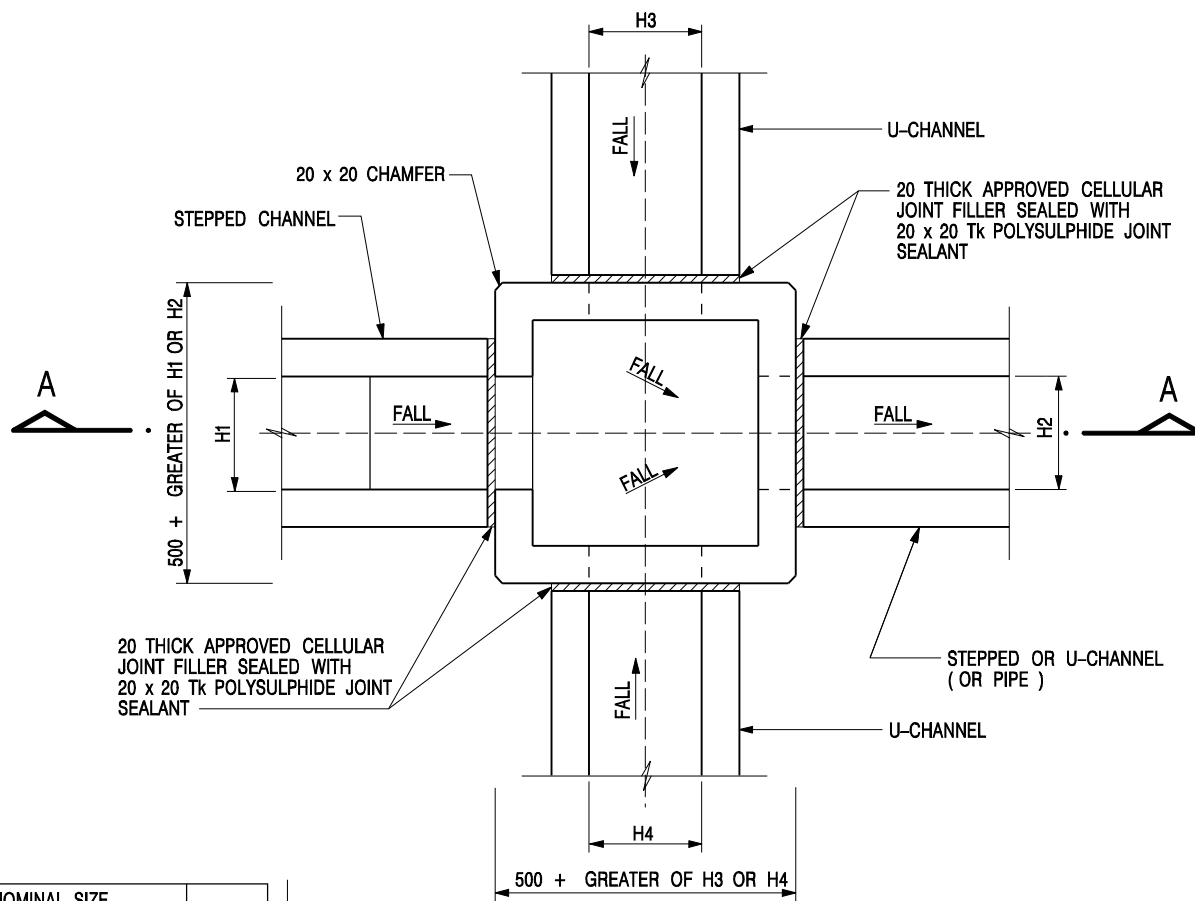
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SCALE

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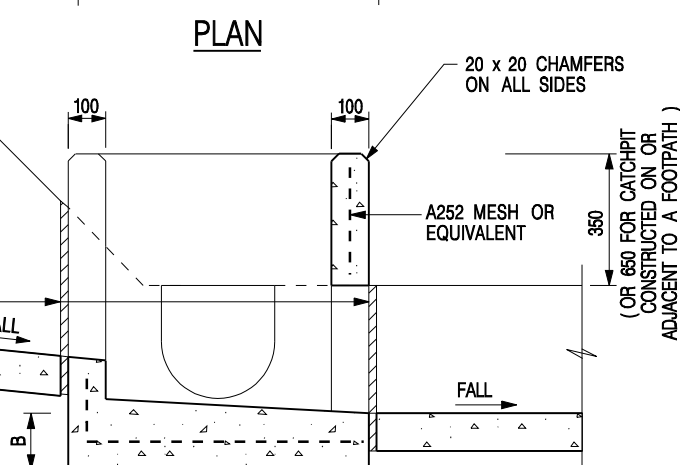
DRAWING No.

FIGURE D5

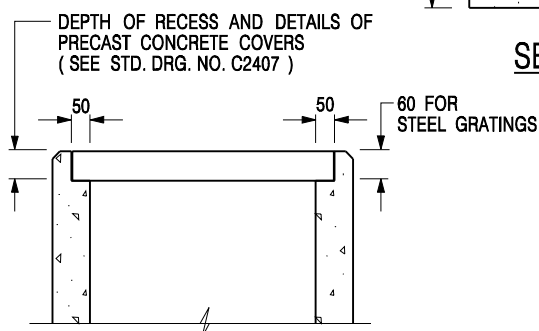


NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT



SECTION A - A




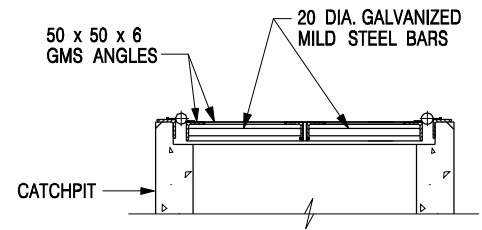
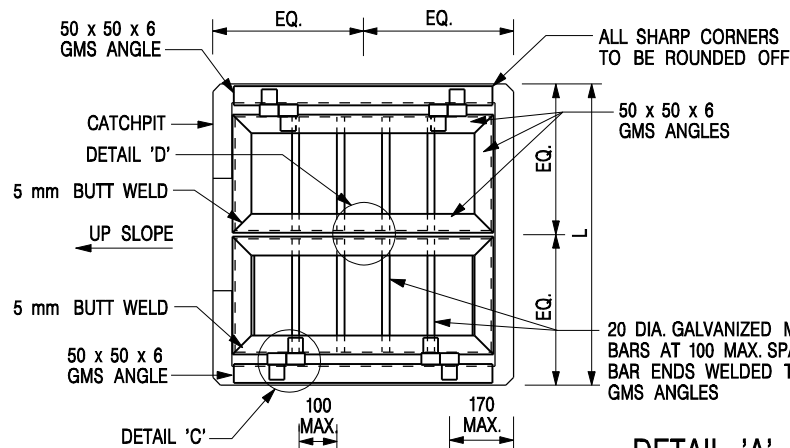
NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

ALTERNATIVE TOP SECTION FOR
PRECAST CONCRETE COVERS / GRATINGS

STANDARD CATCHPIT DETAILS
(SHEET 1 OF 5)

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REF.	REVISION	SIGNATURE	DATE
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DATE JAN 1991			

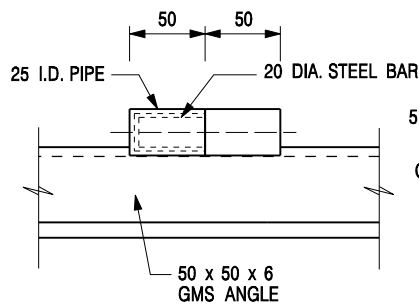


SECTIONAL ELEVATION

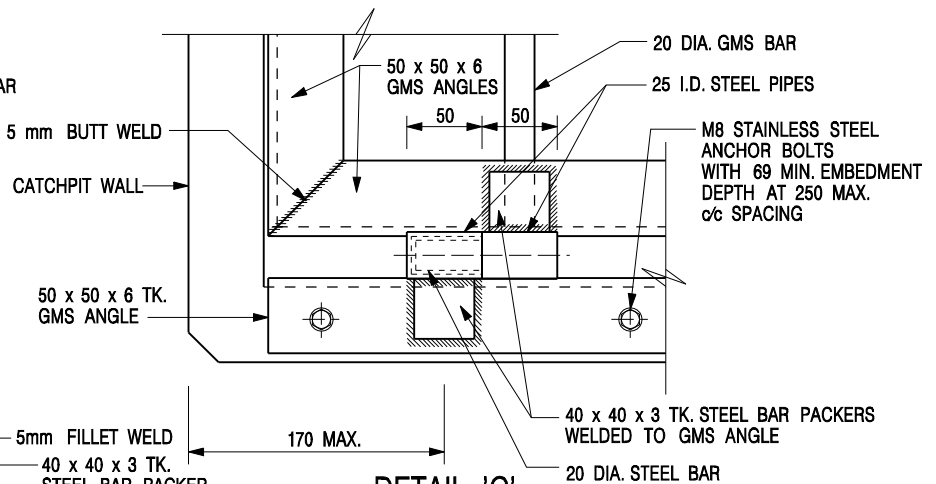
DETAIL 'A'

(DETAILS OF DOUBLE SIDE OPENING STEEL GRATING FOR $L > 900\text{mm}$)

SCALE 1 : 20

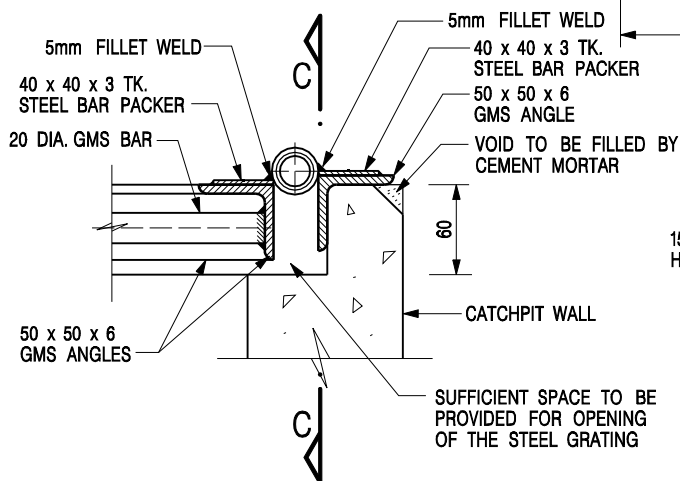


SECTION C - C



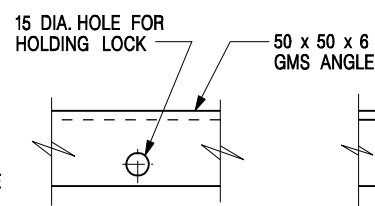
DETAIL 'C'
(DETAILS OF HINGE)

SCALE 1 : 5

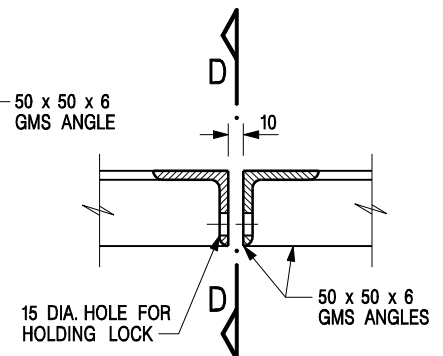


SECTIONAL ELEVATION

(DETAIL 'C')



SECTION D - D




DETAIL 'D'

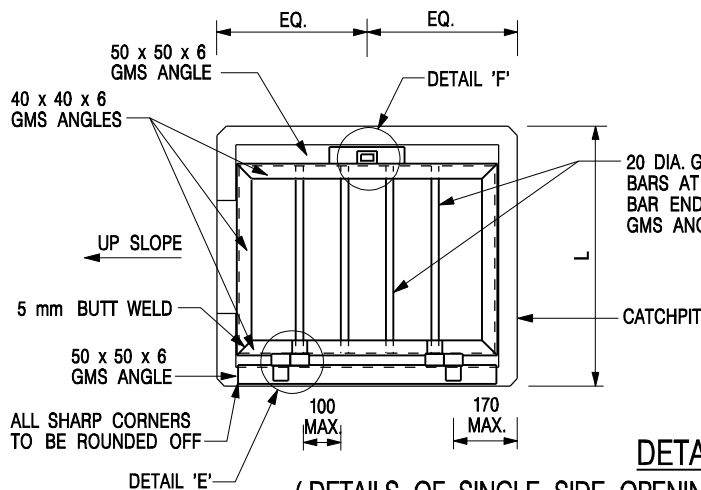
(DETAILS OF HOLE FOR LOCK)
SCALE 1 : 5

NOTES:

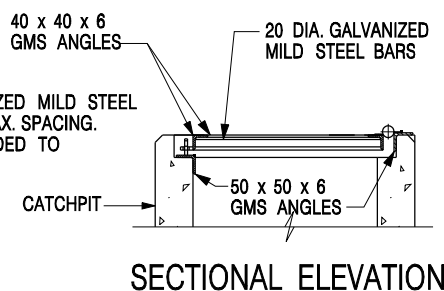
- ALL DIMENSIONS ARE IN MILLIMETRES.
- REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 2 OF 5)

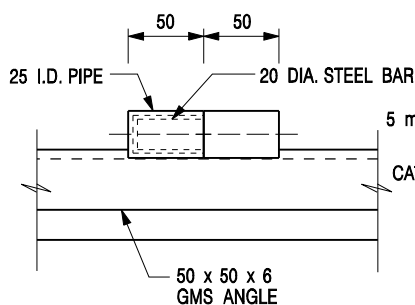
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REF.	REVISION	SIGNATURE	DATE
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		DRAWING NO. C2405 /2	



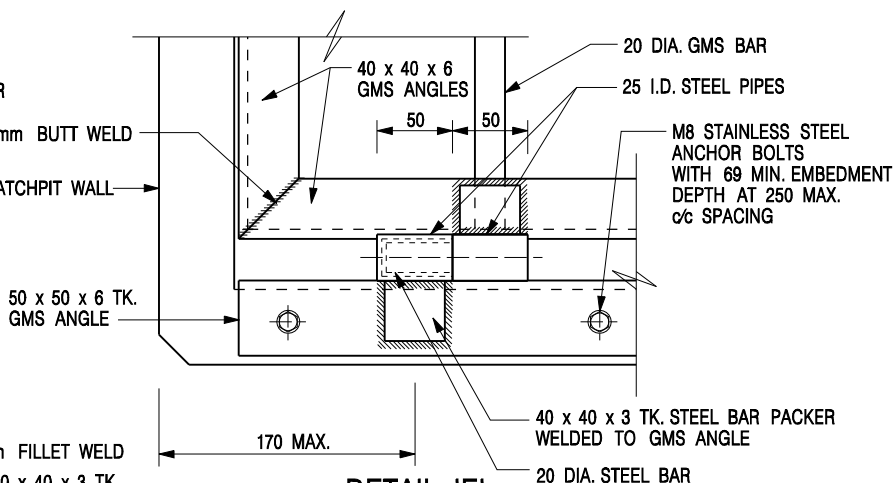
DETAIL 'B'
(DETAILS OF SINGLE SIDE OPENING STEEL GRATING FOR $L \leq 900\text{mm}$)
SCALE 1 : 20



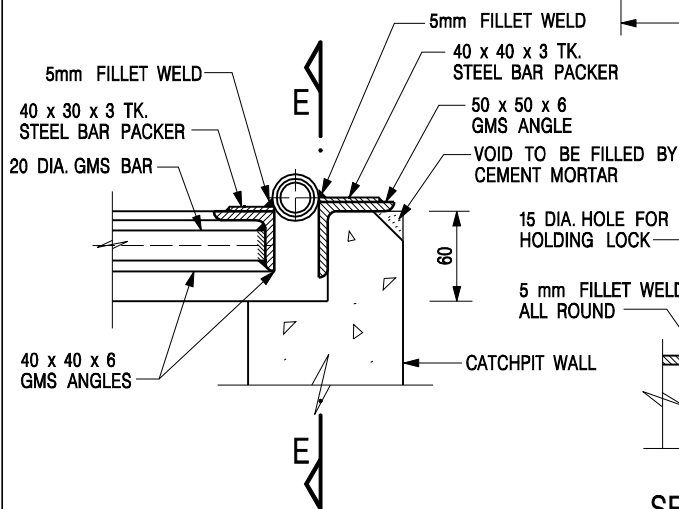
SECTIONAL ELEVATION



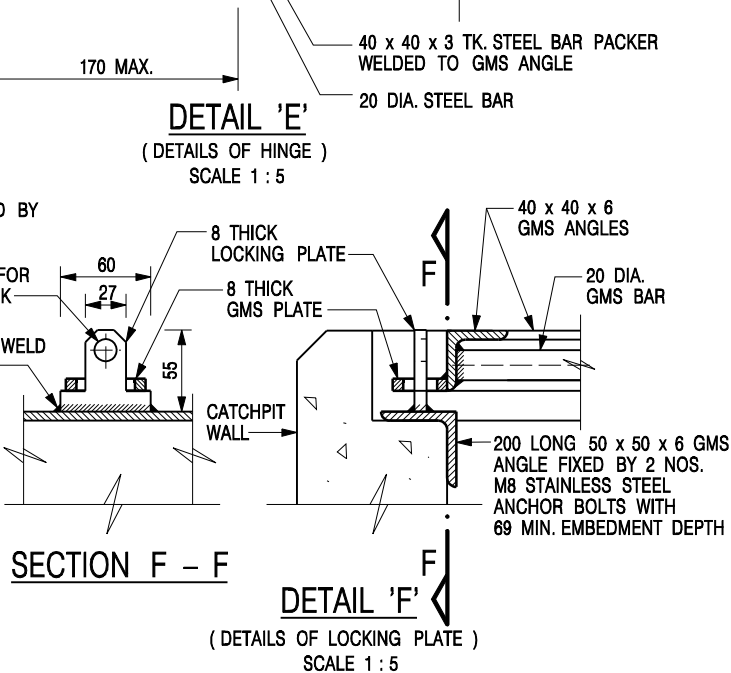
SECTION E - E



DETAIL 'E'
(DETAILS OF HINGE)
SCALE 1 : 5



SECTIONAL ELEVATION
(DETAIL 'E')




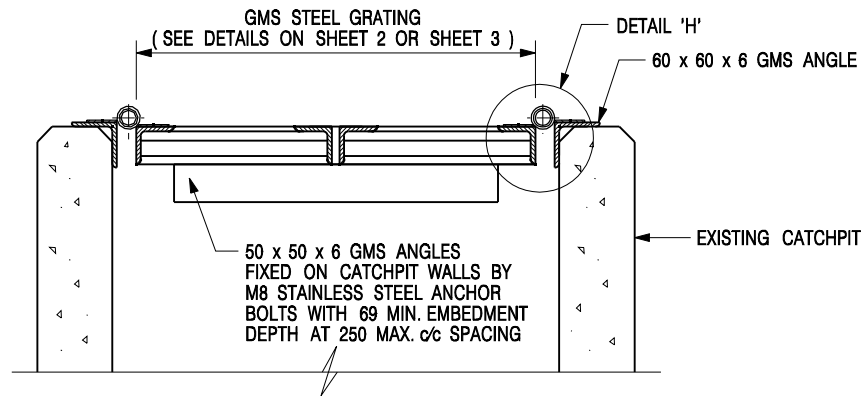
DETAIL 'F'
(DETAILS OF LOCKING PLATE)
SCALE 1 : 5

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- REFER TO SHEET 5 FOR OTHER NOTES.

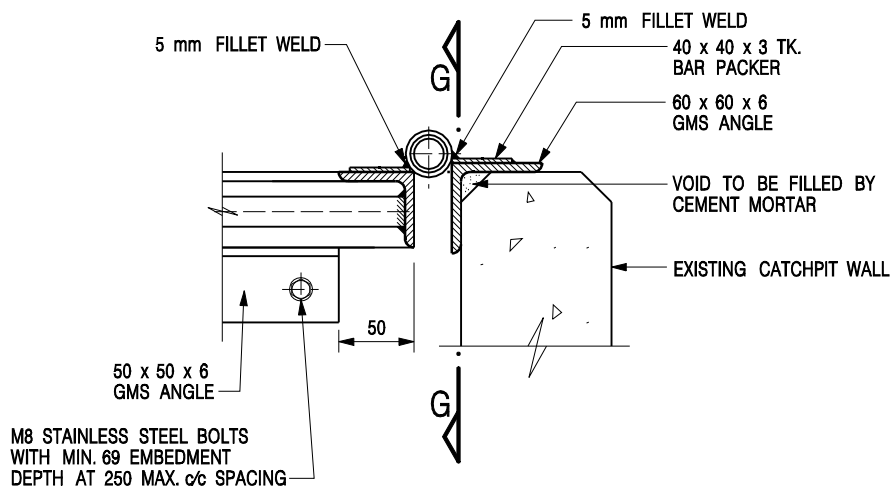
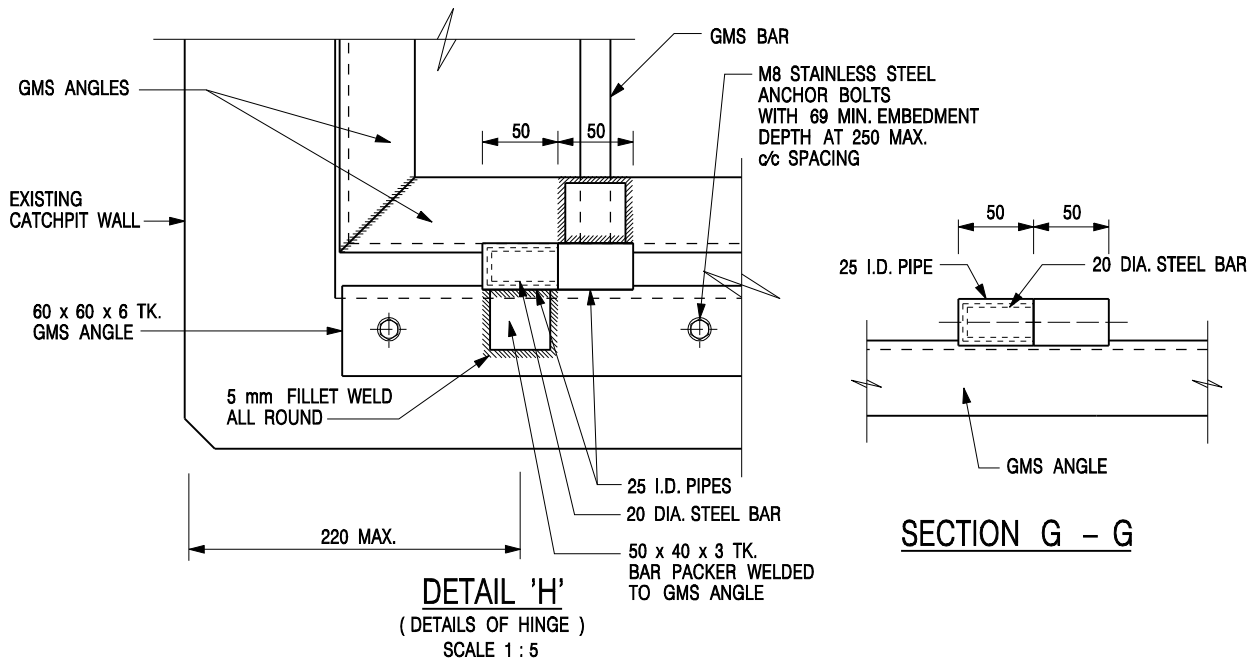
STANDARD CATCHPIT DETAILS
(SHEET 3 OF 5)

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REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /3	



DETAIL 'G' - DETAILS OF STEEL GRATING CONSTRUCTED ON EXISTING CATCHPIT


SCALE 1 : 10

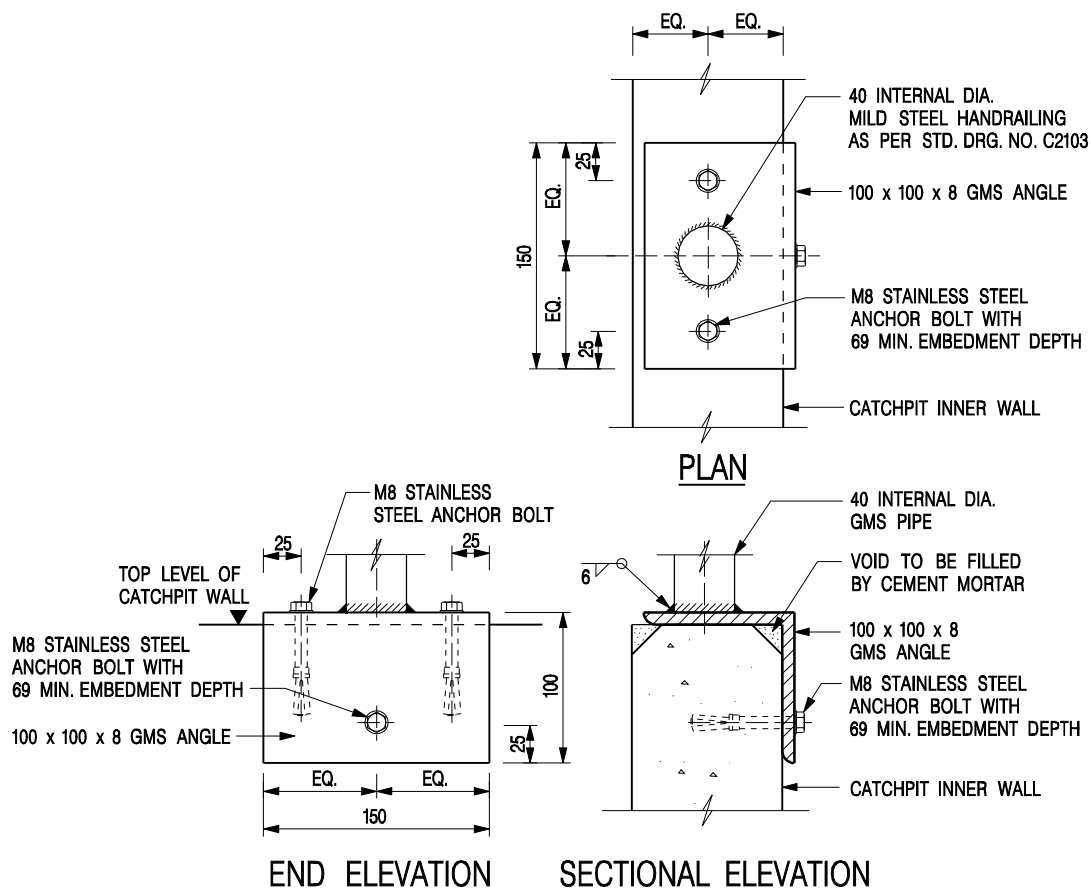


NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 4 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE AS SHOWN		DRAWING NO. C2405 /4	
DATE JAN 1991			




DETAIL 'J' – FIXING DETAILS FOR HANDRAILING ON TOP OF CATCHPIT WALL

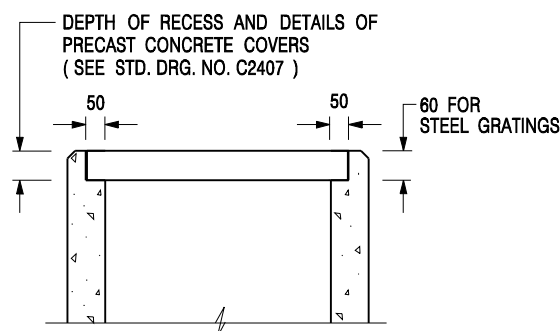
SCALE 1 : 5

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- ALL CONCRETE SHALL BE GRADE 20 /20.
- CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
- FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
- CONCRETE TO BE COLOURED AS SPECIFIED.
- FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAILS ON SHEET 2 OR SHEET 3) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
- IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON SHEET 5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
- MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 mm c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
- FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON SHEET 4.
- ALL STEEL ANGLES SHALL COMPLY WITH BS EN 10025 AND BS EN 10056.
- UNLESS OTHERWISE SPECIFIED, ALL WELDS SHALL BE 5 mm CONTINUOUS FILLET WELDS.
- ALL WELDS SHALL BE CHIPPED, GROUND SMOOTH, BRUSHED TO REMOVE SLAG PRIOR TO HOT-DIP GALVANIZATION.
- ALL STEELWORK SHALL BE HOT-DIP GALVANIZED TO BS EN ISO 1461. ALL EXPOSED STEELWORK SURFACES SHALL BE TREATED AND PAINTED IN ACCORDANCE WITH THE GENERAL SPECIFICATION.
- SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

STANDARD CATCHPIT DETAILS
(SHEET 5 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /5	



ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAIL 'A' ON STD. DRG. NO. C2405 /2) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON STD. DRG. NO. C2405 /5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON STD. DRG. NO. C2405 /4.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

A	MINOR AMENDMENT.	Original Signed	04.2016
-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP
(SHEET 2 OF 2)**



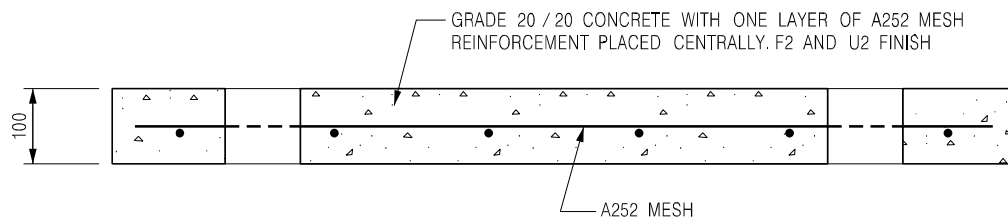
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

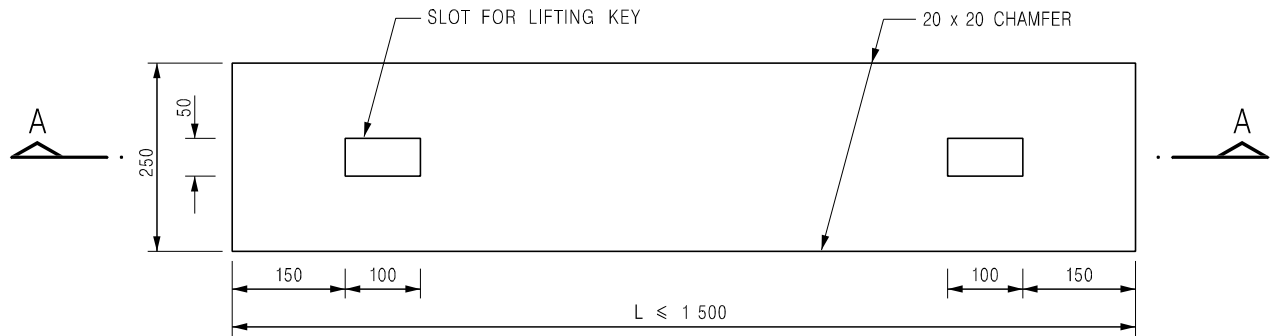
DATE JAN 1991

DRAWING NO.

C2406 /2A

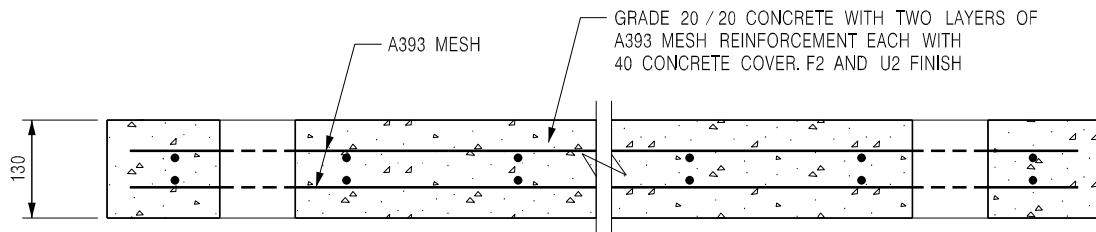


SECTION A - A

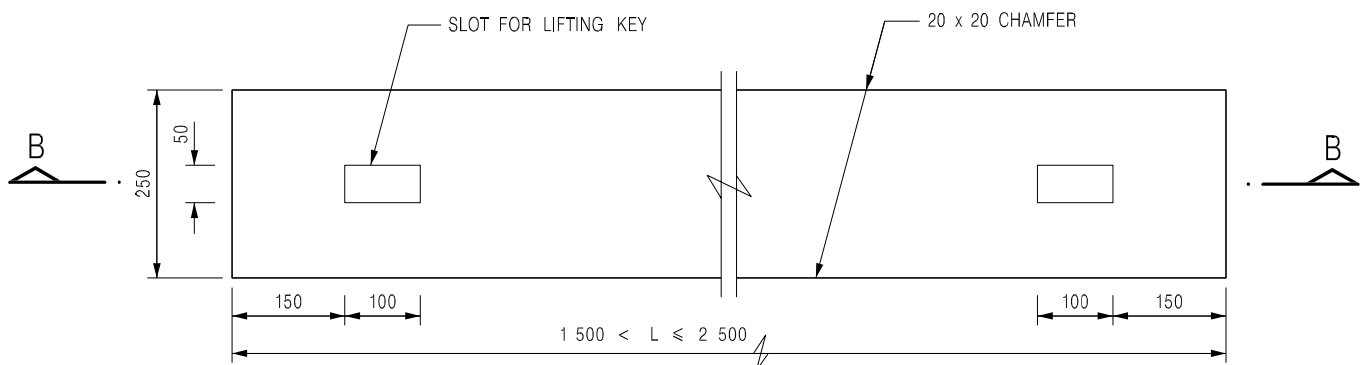


PLAN

TYPE 1 - FOR SPAN UP TO 1.5 m



SECTION B - B



PLAN

TYPE 2 - FOR SPANS 1.5 m TO 2.5 m

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL EXTERNAL EDGES OF THE COVERS SHALL BE 20mm CHAMFERED.

B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	GENERAL REVISION	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

PRECAST CONCRETE COVERS
FOR CATCHPIT AND SAND TRAP

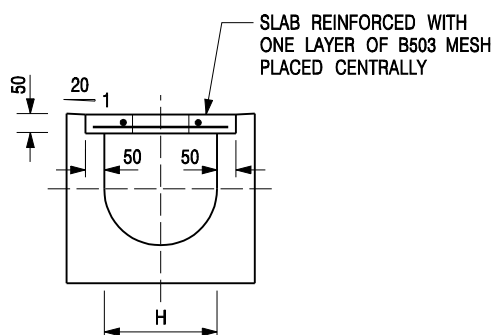


CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT

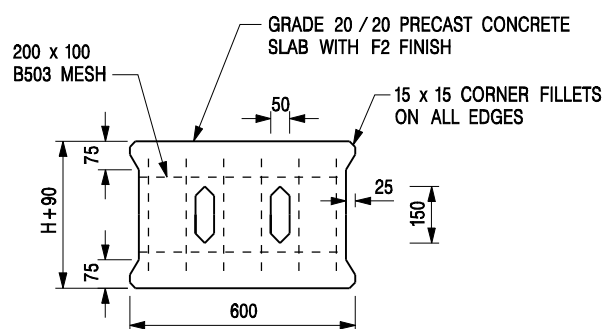
SCALE 1 : 10

DATE JAN 1991

DRAWING NO.
C2407B



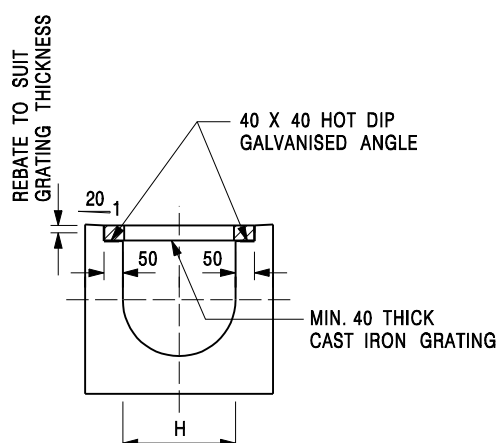
TYPICAL SECTION



PLAN OF SLAB

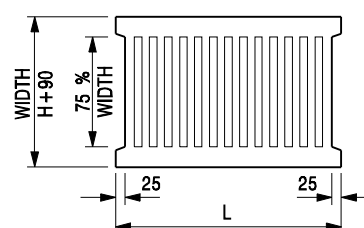
U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)



L = 600mm FOR H ≤ 375mm
L = 400mm FOR H > 375mm

CAST IRON GRATING

U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**COVER SLAB AND CAST IRON
GRATING FOR CHANNELS**



**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

DATE JAN 1991

DRAWING NO.
C2412E



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Appendix Ib of RNTPC
Paper No. A/NE-TKL/813A



宋梓華
Sung Tze Wah
FRICS FHKIS MSISV MCIREA ACI Arb R.P.S.(GP)

李霧儀
Lee Mo Yi
MPIA RPP MUDD BA (Hons)

吳恆廣
Ng Hang Kwong, BBS
FRICS FHKIS R.P.S.(GP)
Honorary World Valuer (WAVO)

林桂金
Daniel K.K. Lam
MRICS MHKIS MCIREA R.P.S.(GP) BSc

宋樹鴻
Sung Shu Hung
FRICS MHKIS R.P.S.(GP)(PD) MCIREA
MHIREA BSc (Hons)

趙慧姿
Chiu Wai Chi
MRICS MHKIS MSc BBus (MKIlg)

Consultant :

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RPE PMgr CEnv FIHE FCIQB MICE
MHKIE MSOE FCI MI MCIArb MSc

劉志光
Lau Chi Kwong
FRICS FHKIS ALS MHKIS
R.P.S.(LS)(PFM) MSc

潘孝維
Pun How Wai
B.Arch.HK RIBA

By Email and Post

Date : 7 November 2025
Your Ref.: TPB/A/NE-TKL/813
Our Ref. : LDS/PLAN/7139

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road, North Point, Hong Kong

Dear Sir/Madam,

Application for Planning Permission for Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

(Application No. A/NE-TKL/813)

We refer to the comments from Transport Department on the captioned application and submit herewith our response in **Table 1** for your consideration.

Due to the minor adjustment of the vehicular access and layout, we also submit herewith the following updated figures/report for your perusal:

- (1) Figure 4 Layout Plan
- (2) Figure 4A Layout Plan showing the nearby water mains
- (3) The updated Drainage Proposal (see **Annex A**)

Should there be any queries, please contact our Ms. Cannis Lee at [REDACTED]

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited

Lawson David & Sung



Encl.

c.c. DPO/STN (Attn.: Ms. Sheren Lee) – By Email
Client

Your Assets for Growth

Table 1: Responses to Comments from Government Departments on Planning Application No. A/NE-TKL/813 (7 November, 2025)

COMMENTS	RESPONSES
1. Transport Department (TD)	
<p><u>Traffic impact assessment from traffic engineering point of view</u></p> <p>(a) Table 3.2: The peak hour V/C ratio seem to be arithmetically incorrect. The applicant should review the calculation;</p>	<p>(a) Noted. Table 3.2 is reviewed and updated accordingly. Please refer to the updated TIA report (see Annex B) for details.</p>
<p>(b) Table 4.5: The figures for “2030 Traffic Volume Design Scenario” and “2030 VIC Ratio Reference Scenario” are not in order. The applicant should review accordingly;</p>	<p>(b) Noted. Table 4.5 is reviewed and updated accordingly. Please refer to the updated TIA report for details.</p>
<p>(c) Drawing No. FIGURE 2.2: The applicant should advise the dimensions of the driveway in the site;</p>	<p>(c) Noted. The dimensions of the driveway are marked on FIGURE 2.2 accordingly. Please refer to the updated TIA report for details.</p>
<p>(d) Drawing Nos. FIGURE 3.2, FIGURE 4.1, FIGURE 4.2 and FIGURE 4.3: The applicant should provide the breakdown of flows for each traffic direction at each arm of the junctions;</p>	<p>(d) Noted. FIGURES 3.2, 4.1, 4.2 and 4.3 are updated to show the breakdown of flows for each traffic direction at each arm of the junctions. Please refer to the updated TIA report for details.</p>
<p>(e) Drawing No. SP-02: Vehicle should not encroach onto the adjacent lane when entering the run-in/out from the near side lane. The applicant should review accordingly;</p>	<p>(e) Noted. According to the latest layout, it is proposed to provide one LGV loading/unloading bay (7.0m x 3.5m) instead of one HGV loading/unloading bay (11.0m x 3.5m). Please refer to FIGURE 2.2 of the updated TIA report for the latest layout plan.</p> <p>Swept path is conducted to demonstrate that the LGV will not encroach onto the adjacent lane when entering the run-in/out. Please refer to the updated FIGURE SP-02 for details.</p>

COMMENTS	RESPONSES
<p>(a) <u>Other comments from traffic engineering point of view</u> the applicant shall advise the provision and management of pedestrian facilities to ensure pedestrian safety; and</p> <p>(b) the vehicular access between the subject site and Sha Tau Kok Road is not managed by TD. The applicant should seek comment from the responsible party.</p>	<p>(a) Please note that flashing warning lights will be installed at both sides of the run-in/out to warn pedestrians on the public footpath whenever vehicles leave the Site. Please refer to Section 2.3.1 and FIGURE 2.2 of the TIA report for details.</p> <p>(b) Noted. Comments from responsible party will be sought.</p>
2. Lands Department (LandsD)	
<p>My comments/observations based on the applicant's information are as follow:</p> <p>(a) The application site comprises Old Schedule Agricultural Lot held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through Government Land (GL) but no right of access via GL is granted to the application site at the proposed ingress/egress point.</p> <p>(b) Part of the private lot is covered by Short Term Waiver (STW) No. 1226 for the purpose of storage of rattan furniture. Part of GL within the application site is covered by Short Term Tenancy (STT) No. 1108 for the purposes of storage of rattan furniture. The proposed sizes and uses</p>	<p>(a) The existing ingress/egress point, which has been used by the occupiers for over 35 years, will be retained in the proposed development.</p> <p>(b) Noted. Should this application be approved, the Applicant will apply for STW and STT for the proposed use.</p>

COMMENTS	RESPONSES
<p>of the structures and GL involved in the application site do not tally with those permitted under the said STW and STT.</p> <p>(c) The application lot is partly covered by Letter of Approval (LoA) No. 2713 and Modification of Tenancy (MoT) No. 35316 for erection of temporary structures for dwelling, kitchen, pigsty and shed. This office reserve the rights to take enforcement action for any irregularities and cancel the LoA/MoT as appropriate.</p> <p>(d) I must point out that the following irregularities covered by the subject planning application have been detected by this office.</p> <p><u>Unauthorised structure within the said private lot covered by the planning application</u></p> <p>LandsD has reservation on the planning application since there are unauthorised structures within the said private lot which are already subject to lease enforcement actions according to case priority. The lot owner should rectify the lease breaches as demanded by LandsD,</p> <p><u>Unlawful occupation of GL with unauthorised structures covered by the planning application</u></p>	<p>(c) Noted.</p> <p>(d)</p> <p>Noted. Should this application be approved, the Applicant will apply for STW for the proposed use. All unauthorized structures on the private lot will be removed by the Applicant.</p>

COMMENTS	RESPONSES
<p>Part of the GL within the application site (about 680m² as mentioned in the application form) has been illegally occupied with unauthorised structures without any permission. Any occupation of GL without Government's prior approval is an offence under Cap. 28. This office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice.</p> <p>(e) The following irregularities <u>not</u> covered by the subject planning application have been detected by this office:</p> <p><u>Unauthorised structures within the said private lot not covered by the planning application</u></p> <p>There are unauthorised structures within the said private lot not covered by the subject planning application. The lot owners should immediately rectify the lease breaches and this office reserves the rights to take necessary lease enforcement action against the breaches without further notice.</p> <p><u>Unlawful occupation of GL not covered by the planning application</u></p> <p>The GL adjoining the said private lot has been illegally occupied with unauthorised structure without permission.</p>	<p>Noted. Should this application be approved, the Applicant will apply for STT for the proposed use.</p> <p>(e)</p> <p>Noted. The unauthorised structures will be removed by the Applicant.</p> <p>Noted. The unauthorized structures on GL will be removed by the Applicant.</p>

COMMENTS	RESPONSES
<p>The GL being illegally occupied is not included in the application. Any occupation of GL without Government's prior approval is an offence under Cap. 28. This office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice.</p> <p>(f) The lot owners/applicant shall <u>either</u> (i) remove the unauthorised structures and cease the illegal occupation of the GL not covered by the subject planning application immediately; <u>or</u> (ii) include the unauthorised structures and the adjoining GL being illegal occupied in the subject planning application for further consideration by the relevant departments <u>and</u>, subject to the approval of the Town Planning Board to the planning application which shall have reflected the rectification or amendment as aforesaid required, apply to this office for modification/new STW and STT to permit the structures erected/to be erected and the occupation of GL. The applications for modification/ new STW and STT will be considered by the Government in its capacity as a landlord and there is no guarantee that they will be approved. Besides, the STW would be processed on whole lot basis. And the STW and the STT, if approved, will be subject to such terms and conditions including the payment of back-dated waiver fee/rent from the first date when the unauthorised structures were erected and the occupation of</p>	<p>(f) Noted. The Applicant will remove the unauthorized structures and cease the illegal occupation of the GL not covered by the planning application.</p>

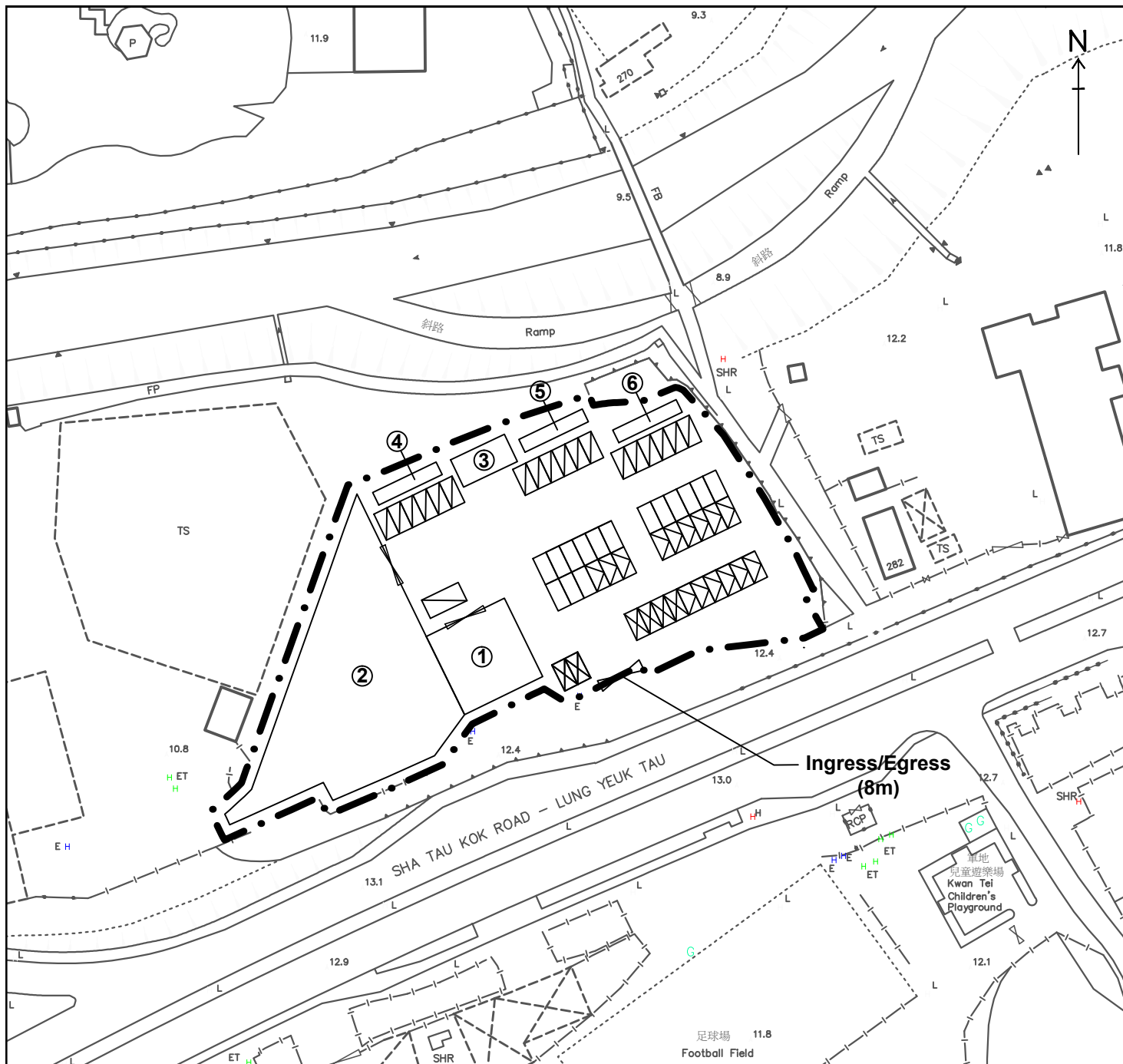
COMMENTS	RESPONSES
<p>GL as well as administrative fee as considered appropriate to be imposed by LandsD. In addition, LandsD reserves the right to take enforcement action against the lot owners for any breach of the lease conditions, including the breach(es) already in existence or to be detected at any point of time in future and land control action for any unlawful occupation of GL. Besides, given the proposed use is temporary in nature, only erection of temporary structure(s) will be considered.</p> <p>(g) Unless and until the unauthorised structures and the unlawful occupation of GL are duly rectified by the lot owners/applicant or entirely included in the subject planning application, please take it as this office's objection to the application which must be brought to the attention of the Town Planning Board when they consider the application.</p>	<p>(g) Noted.</p>
3. APD/ Environment and Ecological Bureau	
<p>(a) It is noted that this application will provide 54 spaces for parking of vehicles, and only 18 spaces will be provided with EV charging facilities.</p> <p>(b) To echo with the latest version of Ch.8 of HKPSG about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the</p>	<p>(a) Noted.</p> <p>(b) The application complies with the latest version of Ch. 8 of HKPSG that all EV charging spaces are equipped with EV chargers with an output power of 120kW (i.e. not less than 7kW).</p>

COMMENTS	RESPONSES
<p>applicant is suggested to comply with the relevant requirement of HKPSG, i.e., EV chargers with output power of not less than 7kW (i.e. medium chargers) should be installed in all parking spaces for private cars, light goods vehicles and motorcycles of the subject site.</p>	

Figures

Figure 4 Updated Layout Plan

Figure 4A Updated Layout Plan showing the Nearby Water
Mains



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	1,006 806 + 200	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- EV Charing Spaces for Electric Taxi (18 nos.)
- Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Light Goods Vehicle (3.5m x 7m) (1 no.)

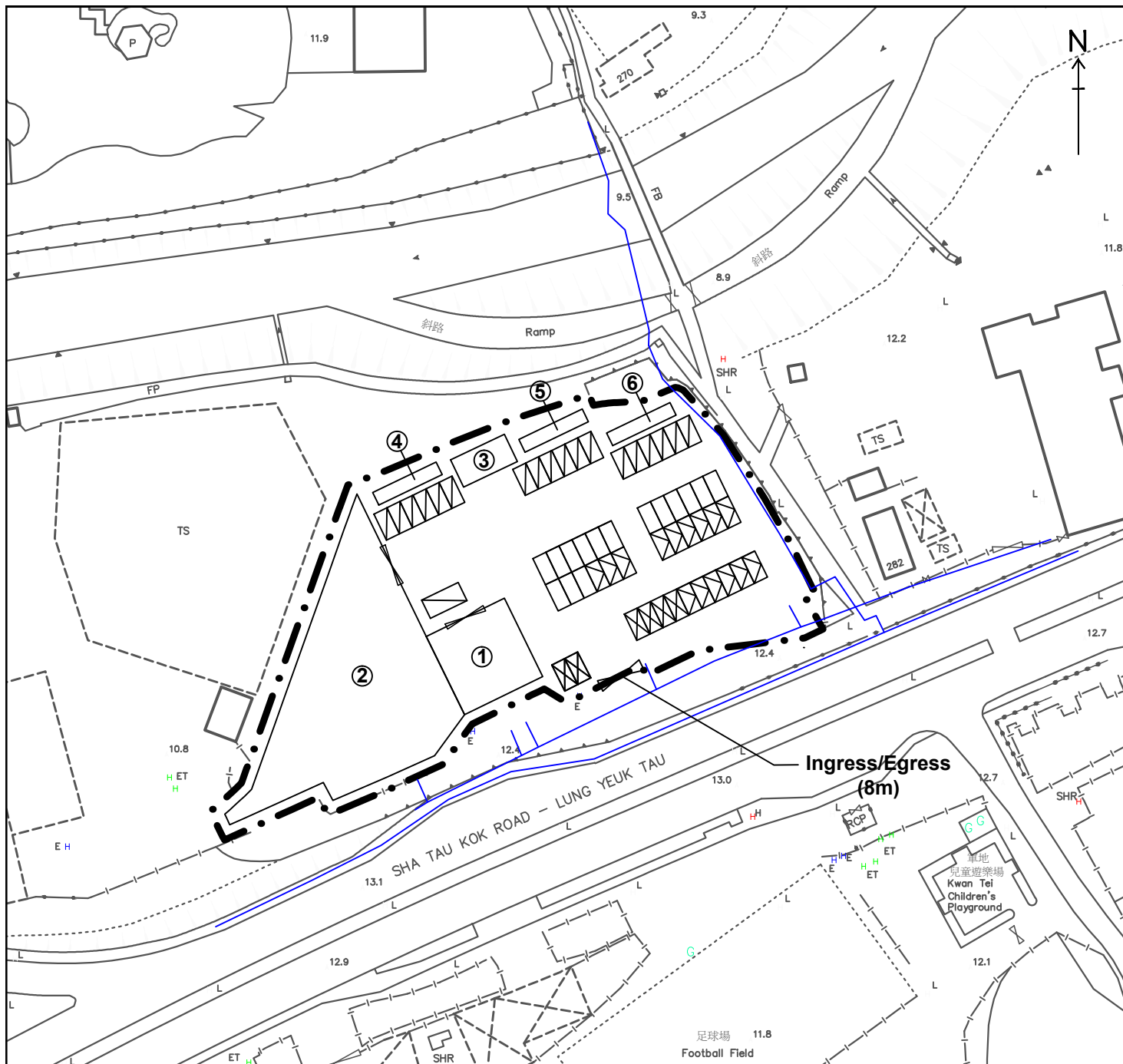
Layout Plan

Figure 4

Scale 1:1000



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 Estate Agency Licence No. C-006328



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	1,006 806 + 200	7.0
3	Transformer Room and Switch Room	1	53	3.5
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Legend:

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- Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- Parking Spaces for Staff/Visitors (6 nos.)
- Loading/Unloading Space for Light Goods Vehicle (3.5m x 7m) (1 no.)
- Water Mains

Layout Plan

Figure 4A

Scale 1:1000



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Estate Agency Licence No. C-006328

Annex A

Updated Drainage Proposal

Drainage Submission in support of

S16 Planning Application for

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office
for a Period of 5 Years in “Open Storage” zone

at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining
Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling,
New Territories

(HT25039)

October 2025

(Revision A)

Drainage Consultant:

何田顧問工程師有限公司

HO TIN & ASSOCIATES

CONSULTING ENGINEERS LIMITED

香港九龍官塘鴻圖道26號威登中心12樓1201-3室

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Rooms 1201-3, Westin Centre, 26 Hung To Road, Kwun Tong, Kowloon, Hong Kong

Tel: 2895 2238 Fax: 2890 8872 E-mail: admin@hotin.com.hk

Prepared & approved by

LEE Kwok Cheung
RPE(Civil)



1. Background

- 1.1 With respect to a S16 Planning Application for Proposed Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories, Messrs. Ho Tin & Associates Consulting Engineers Limited was appointed to prepare a drainage submission.

2. Approach to Prepare this Proposal

- 2.1 This Drainage Submission is prepared in line with the “Technical Note to prepare a Drainage Submission (Relating to applications for temporary change of land use such as temporary storage areas, car parks, workshops, small factories ... etc. under S.16 of the Town Planning Ordinance)” issued by Drainage Services Department in December 2024.

3. The Subject Site and Proposed Development

- 3.1 The subject site with a total site area of about 4,190m² comprises of Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land (about 680m²), Sha Tau Kok Road – Lung Yeuk Tau, Fanling, New Territories. The subject site is to the south of Ng Tung River and on the north side of Sha Tau Kok Road – Lung Yeuk Tau opposite to Kwan Tei Children’s Playground. A Site Location Plan is shown in **Figure D1**.
- 3.2 The subject development consists of one single-storey motor vehicle showroom structure (5m high, total floor area = 225m²), one two-storey vehicle repair workshop cum eating place and ancillary office structure (7m high, total floor area = 2,012m²), one single-storey transformer room and switch room structure (3.5m high, total floor area = 53m²), and 3 numbers of container for EV chargers (3m high, total floor area = 29m² each) accompanying with 18 numbers of EV charging spaces for electric taxi, 30 numbers of parking spaces/waiting spaces for electric taxi, 6 numbers of parking spaces for staff/visitors, and 1 number of loading/unloading space for medium goods vehicle. A plan showing the proposed site layout is in **Figure D2**.

4. Existing Drainage Conditions of the Site

- 4.1 At present, the subject site is hard paved, partitioned into sub-units and partly used for storage use and car repairing workshop and partly vacant (refer to **Plate 1** to **4**).
- 4.2 The subject site is bounded by a footpath on the east and north sides (refer to **Plate 8** to **12**). There is existing 300mm U-channel running between the subject site and the aforementioned footpaths collecting surface runoff of the subject site and the adjacent areas. The existing 300mm U-channel discharges its collected flows via an existing 450mm stepped channel into Ng Tung River to the further north (refer to **Plate 13** and **14**).
- 4.3 Outside the southern subject site boundary is currently used as access of the subject site and its nearby areas beside Sha Tau Kok Road – Lung Yeuk Tau. There is existing 450mm U-channel serving the access (refer to **Plate 5** to **7**).
- 4.4 To the west of the subject site is another development of which the surface runoff would be discharged via an existing 600mm stepped channel into Ng Tung River (refer to **Plate 15** and **16**).
- 4.5 Current conditions of the subject site and its existing drainage conditions are shown in the following photos (photo taking locations are shown on **Figure D3**):



	
<p>Plate 1 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (1 of 4)</p>	<p>Plate 2 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (2 of 4)</p>



Plate 3 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (3 of 4)



Plate 4 – Photo of the southern side of the subject site taken from Sha Tau Kok Road – Lung Yeuk Tau (4 of 4)



Plate 5 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (1 of 2)



Plate 6 – Existing 450mm U-channel running westward along the existing access in front of the southern subject site boundary (2 of 2)



Plate 7 – Existing 450mm U-channel running eastward along the existing access in front of the southern subject site boundary



Plate 8 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (1 of 3)

	
<p>Plate 9 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (2 of 3)</p>	<p>Plate 10 – Existing 300mm U-channel running between the eastern subject site boundary and an existing external footpath (3 of 3)</p>
	
<p>Plate 11 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (1 of 2)</p>	<p>Plate 12 – Existing 300mm U-channel (currently covered up by overgrown with weeds) running between the northern subject site boundary and an existing external footpath (2 of 2)</p>
	
<p>Plate 13 – The section of Ng Tung River outside the northern subject site boundary</p>	<p>Photo 14 – Existing 450mm stepped channel discharging into Ng Tung River close to the northern boundary of the Application Site</p>



Plate 15 – Existing 600mm stepped channel discharging into Ng Tung River at the downstream of the Application Site



Plate 16 – View toward the further downstream of the Application Site along Ng Tung River



Extract of the Drainage Records from LandsD's GeoInfo Map in May 2025

5. Drainage Assessment and Proposal

- 5.1 The subject site is a simple small site with a total site area of about 4,190m² (less than 1 ha in size). The existing subject site levels are generally same as those of the area close to Sha Tau Kok Road – Lung Yeuk Tau and relatively higher than the surroundings at the remaining three sides. There are existing surface channels intercepting surface runoff on the area between the subject site and Sha Tau Kok Road – Lung Yeuk Tau. In general, surface runoff flowing toward the subject site would be collected and conveyed away the subject site by the existing surrounding channels.

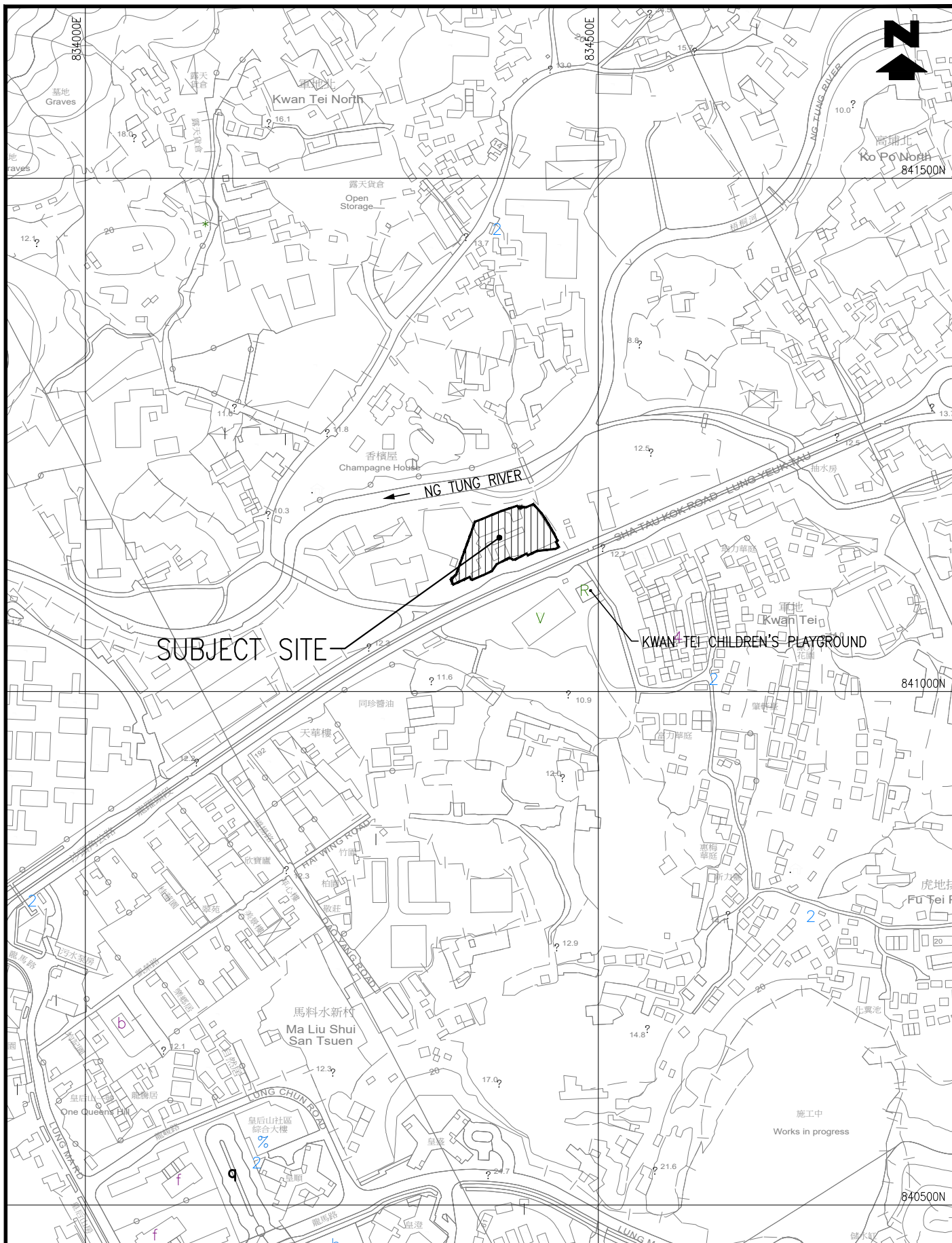
- 5.2 The existing site levels would be maintained and no site formation/leveling works would be carried out such that the subject proposed development would be maintained at levels relatively higher than its adjacent developments/areas to its east, west and north, and would be at similar levels of the existing access running along its southern boundary. In principle, surface runoff of the surrounding areas would be collected and conveyed away by the existing channels in the adjacent areas of the subject site, hence, any new solid fence wall of the subject site would not obstruct any existing overland flows (in addition, it should be noted that the existing levels (similar to the proposed finished levels) of the subject site are already relatively higher than those to the east, west and north). Nevertheless, for conservative, in order to ensure no surface runoff from the southern side (at levels close to the proposed finished levels of the subject development), due to any unexpected reasons, flowing onto the subject site to be obstructed, 100mm high gap will be formed at the bottom of the security hoarding/fence wall along the southern subject site boundary. Cross sections of the proposed subject development and typical details of the gap at the bottom of proposed security hoarding/fence are shown in **Figure D5**.
- 5.3 With respect to the "Technical Note to prepare a Drainage Submission" (the "TN") published by Drainage Services Department, for the subject site area = $4,190\text{m}^2 \leq 5,000\text{m}^2$, peripheral 525mm U channel at 1 in 200 gradient would be appropriate. It is therefore proposed to construct new peripheral 525mm U channel at 1 in 200 gradient for the subject development. A new catchpit with trap will be constructed at the end of the proposed 525 U channels near the northwest corner of the subject site before discharging into a proposed 525mm stepped channel at Ng Tung River. A Proposed Stormwater Drainage Layout Plan is shown in **Figure D4**.
- 5.4 Besides, the Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the drainage system to the satisfaction of relevant Government departments.
- 5.5 Details of proposed drainage provisions shall follow relevant details shown in Government Departments' Standard Drawings as follows:

<i>Proposed Drainage Provisions</i>	<i>Standard Drawings</i>	<i>Drawing No. & Title</i>
Catchpit	CEDD Standard Drawings	C 2405/1 to /5 – Standard Catchpit Details
Catchpit with trap		C 2406/1 to /2A – Catchpit with Trap
Catchpit precast concrete cover		C 2407B – Precast Concrete Covers for Catchpit and Sand Trap
U-channel		C 2409J – Details of Half-round and U-channels
Channel cover		C 2412E – Cover Slab and Cast Iron Grating for Channels

6. Conclusion and Recommendations

- 6.1 The subject development as Temporary Electric Vehicle (EV) Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office will be for temporary use for a period of 5 years. The subject site area has been hard paved for a very long period without complaints on drainage conditions. The existing site levels would be maintained and no site formation/leveling works would be carried out.
- 6.2 The subject site is at present served by surrounding surface channels and its levels are relatively higher than those to its east, west and north. In principle, no surface runoff would flow onto the subject site from its surroundings. 525mm U channel at 1 in 200 gradient will be constructed at the peripheral of the subject site to intercept all crossing surface runoff. For conservative, 100mm high gap will be formed at the bottom of the security hoarding/fence along the southern subject site boundary to ensure no surface runoff from the adjacent, due to any unexpected incidents, to be obstructed. A new catchpit with trap will be constructed at the ends of the 525mm U channel before discharging into the proposed new 525mm stepped channel at Ng Tung River.

- 6.3 The Applicant is committed to obtain consents from owners of adjacent relevant land/lots prior to commencement of the proposed drainage works outside the subject site and to maintain regularly to avoid blockage of the system to the satisfaction of relevant Government departments.
- 6.4 The subject development would not alter the existing drainage conditions and pattern of the area and the proposed drainage system would be maintained with appropriate drainage clearance and repair works, i.e. debris clearance and damage repair. Therefore, in conclusion, the subject development would not cause any adverse drainage impact onto the area.



TITLE

SITE LOCATION PLAN

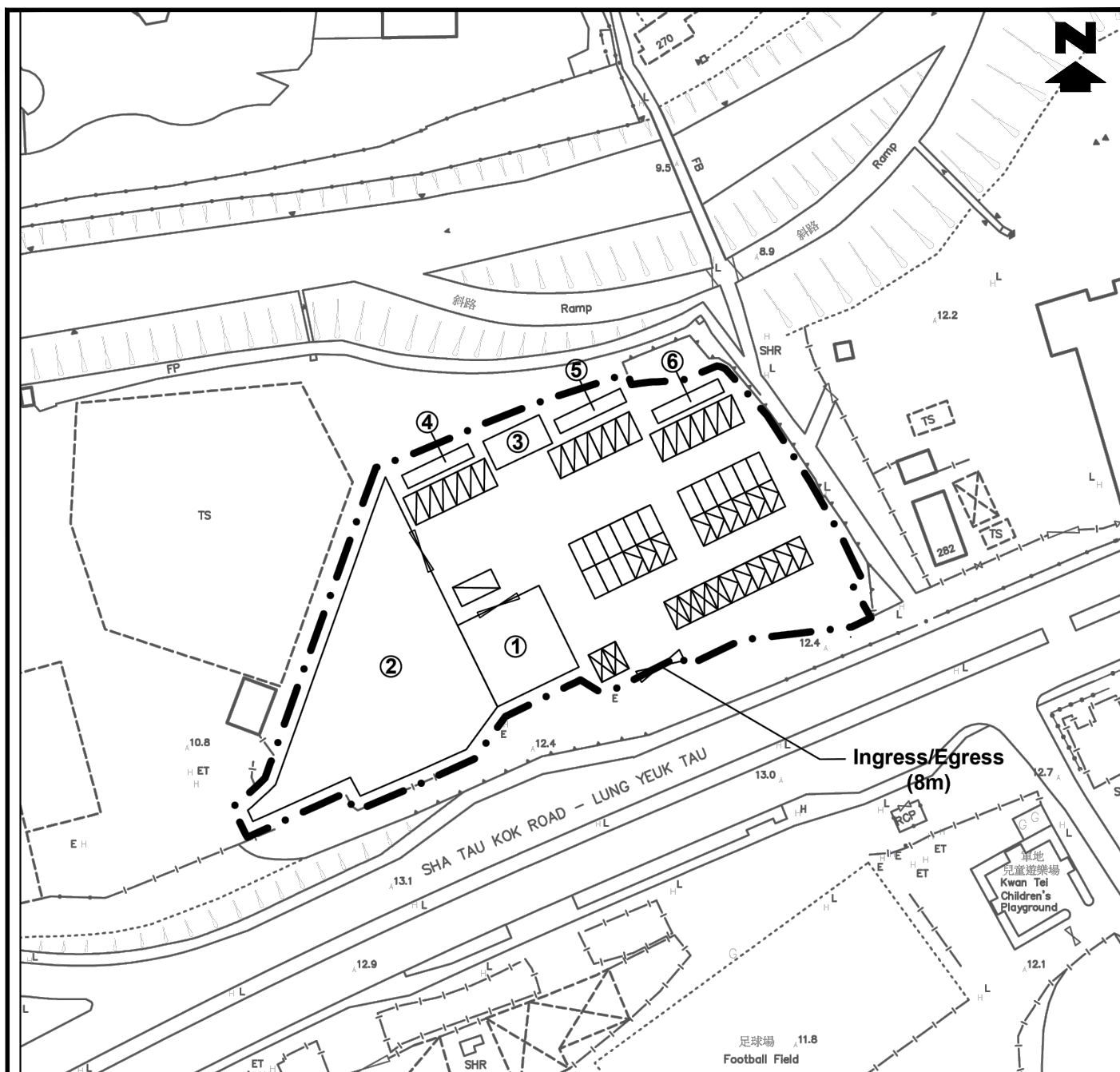
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 5000 - A4

DRAWING No.

FIGURE D1



Structure No.	Uses	No. of Storey	Floor Area (m ²)	Height (m)
1	Motor Vehicle Showroom	1	225	5.0
2	G/F: Vehicle Repair Workshop 1/F: Eating Place, Ancillary Office	2	1,006 806 + 200	7.0
3	Transformer Room and Switch Room	1	53	3.5
4	Container for EV Chargers	1	29	3.0
5	Container for EV Chargers	1	29	3.0
6	Container for EV Chargers	1	29	3.0

Legend:

- Application Site (Area: about 4,180m²)
- ▨ EV Charing Spaces for Electric Taxi (18 nos.)
- ▨ Parking Spaces/Waiting Spaces for Electric Taxi (15 nos.)
- ▨ Parking Spaces for Private Vehicles (for Motor Vehicle Showroom and Office Use) (15 nos.)
- ▨ Parking Spaces for Staff/Visitors (6 nos.)
- ▨ Loading/Unloading Space for Light Goods Vehicle (3.5m x 7m) (1 no.)

TITLE

LAYOUT PLAN

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HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

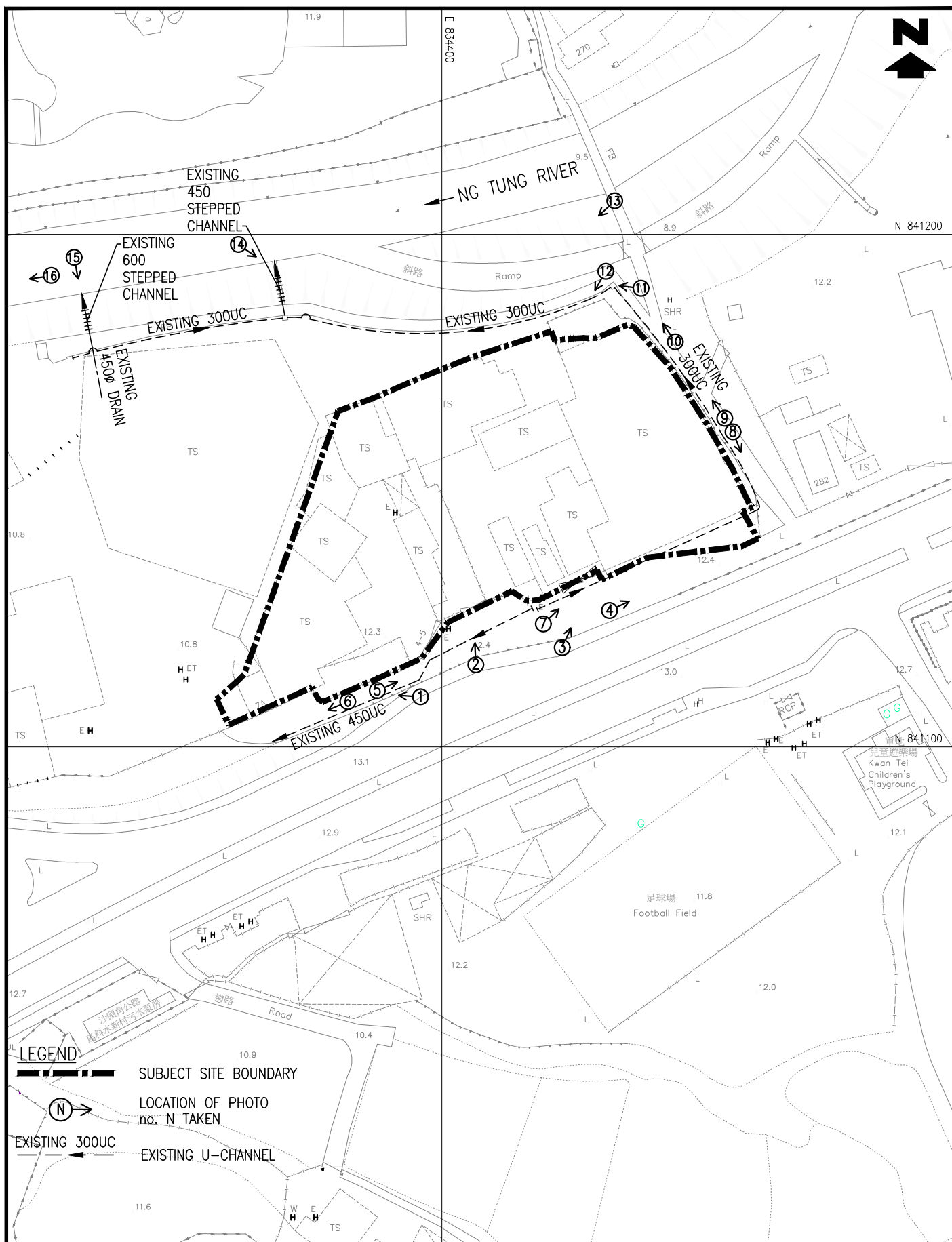
SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D2

H:\25039_LungYeukTau_Site\DRAWING\25039_FIGD2_03 FIGD3A_01 FIGD4A_02 & FIGD5_00.dwg, 8/10/2025 10:24:30



TITLE

EXISTING DRAINAGE AND LOCATIONS OF PHOTO TAKEN

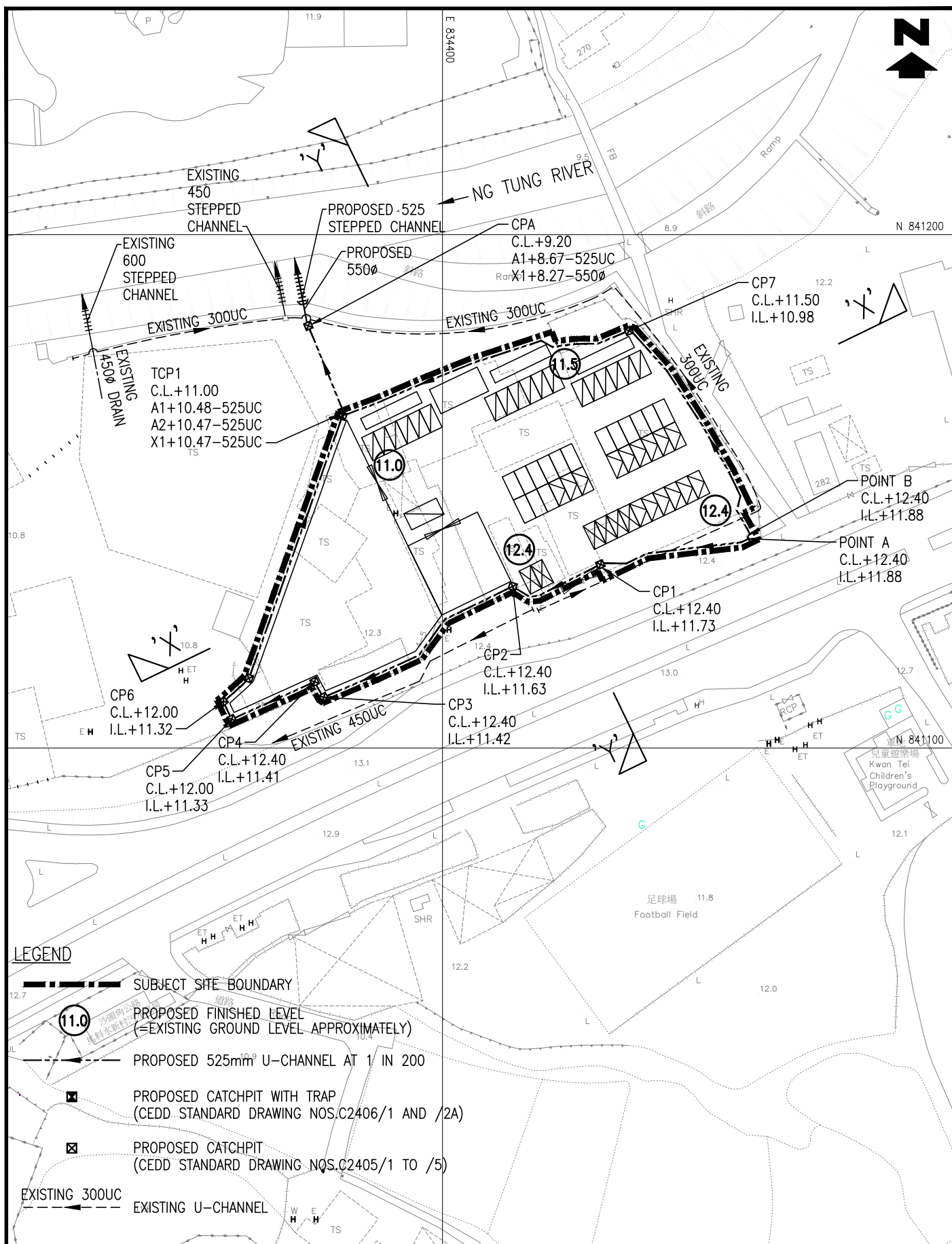
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 1000 - A4

DRAWING No.

FIGURE D3
(REVISION A)



TITLE

PROPOSED STORMWATER DRAINAGE LAYOUT PLAN

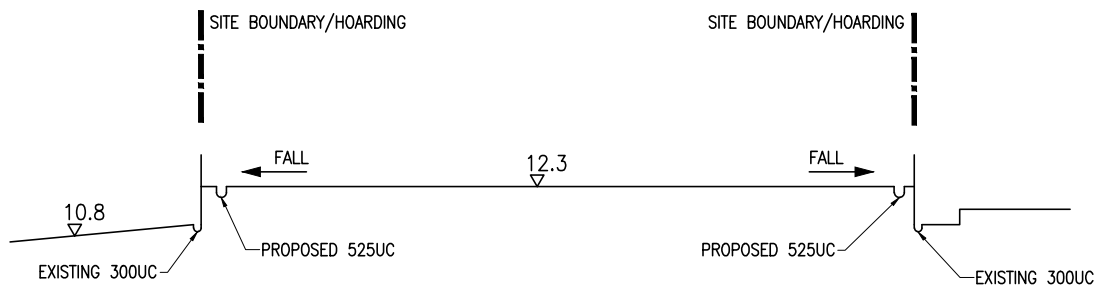
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

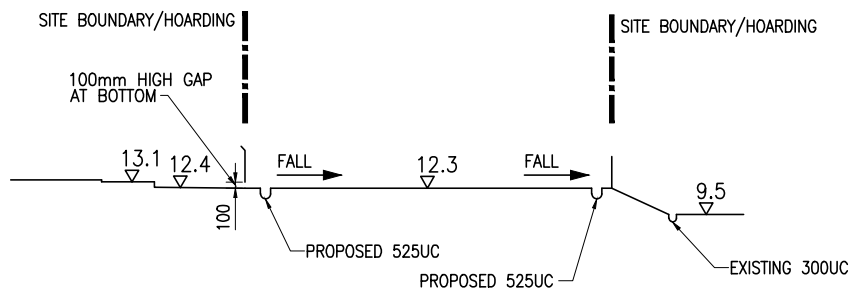
1 : 1000 - A4

DRAWING No.

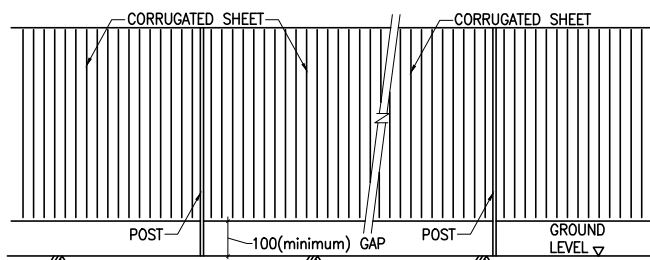
FIGURE D4
(REVISION A)



SECTION 'X'-'X'



SECTION 'Y'-'Y'



TYPICAL ELEVATION OF THE HOARDING N.T.S.
WITH 100mm HIGH GAP AT BOTTOM

TITLE

SITE CROSS SECTIONS

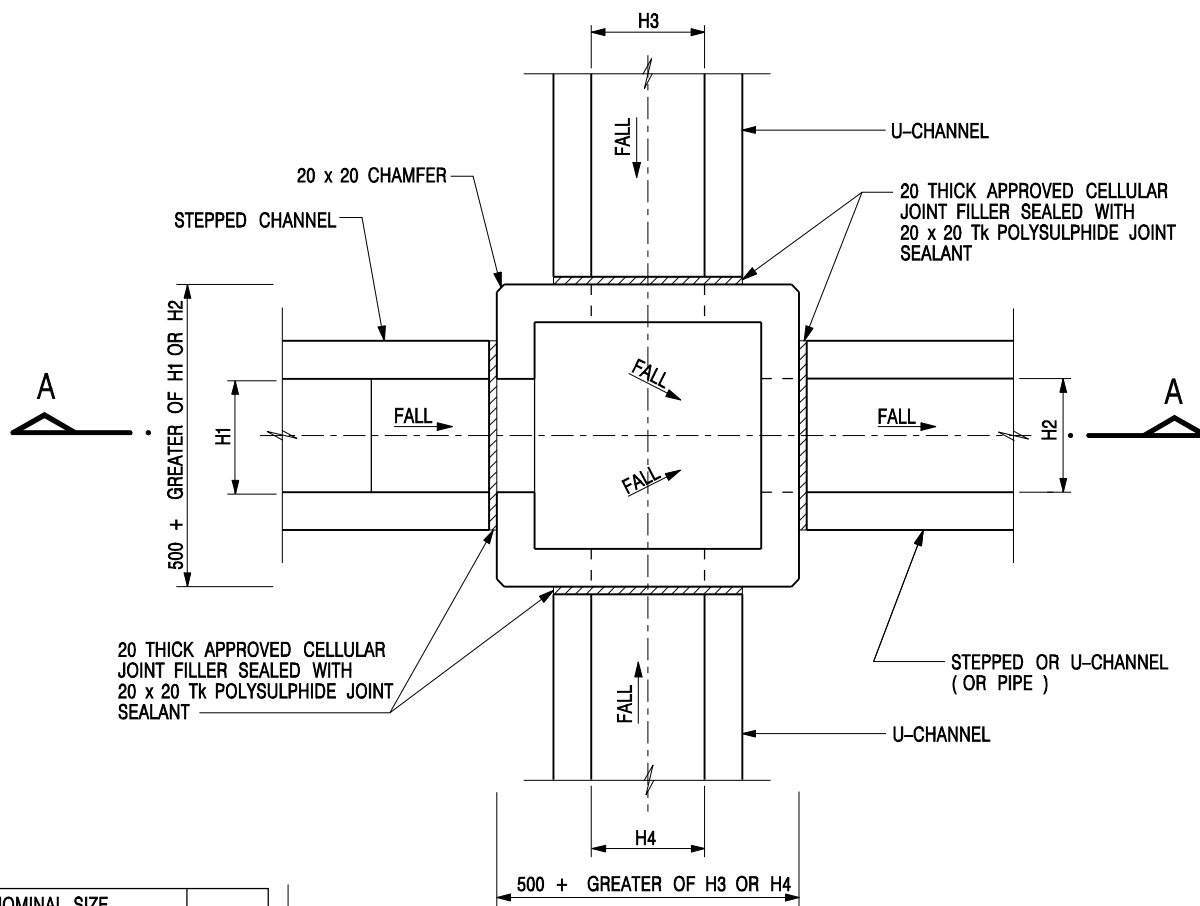
何田顧問工程師有限公司
HO TIN & ASSOCIATES
CONSULTING ENGINEERS LIMITED

SCALE

1 : 500 - A4

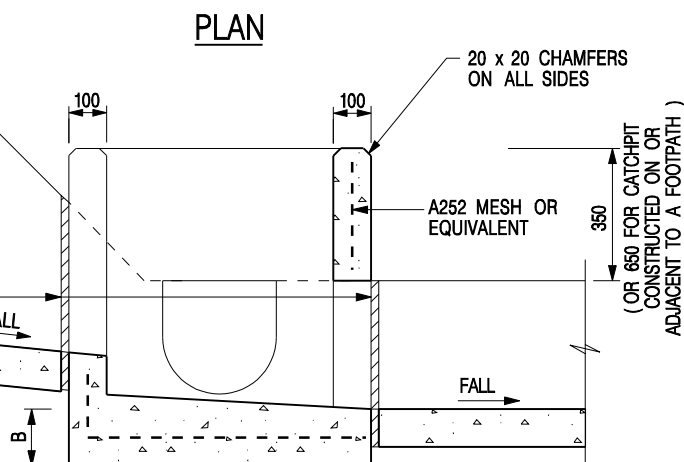
DRAWING No.

FIGURE D5



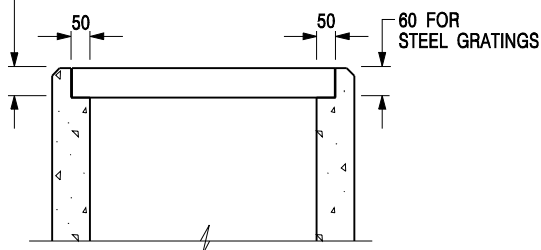
NOMINAL SIZE (LARGEST OF H1, H2, H3 & H4)	B
300 - 600	150
675 - 900	175

20 THICK APPROVED CELLULAR JOINT FILLER SEALED WITH 20 x 20 Tk POLYSULPHIDE JOINT SEALANT



SECTION A - A

DEPTH OF RECESS AND DETAILS OF PRECAST CONCRETE COVERS (SEE STD. DRG. NO. C2407)




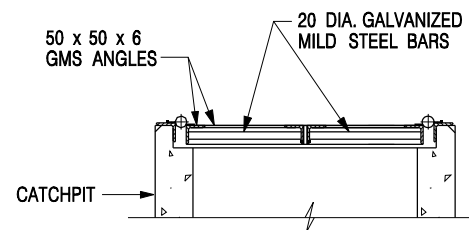
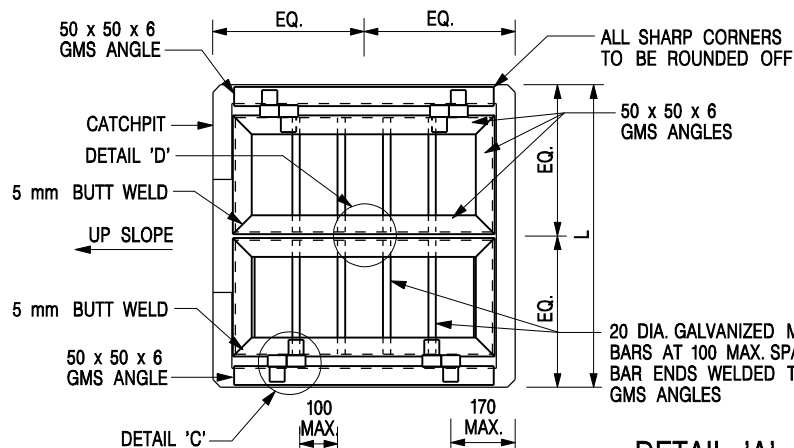
NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

STANDARD CATCHPIT DETAILS (SHEET 1 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE 1 : 20		DRAWING NO. C2405 /1	
DATE JAN 1991			

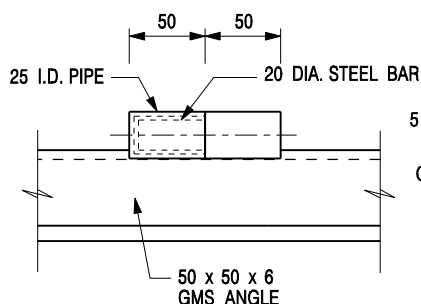


SECTIONAL ELEVATION

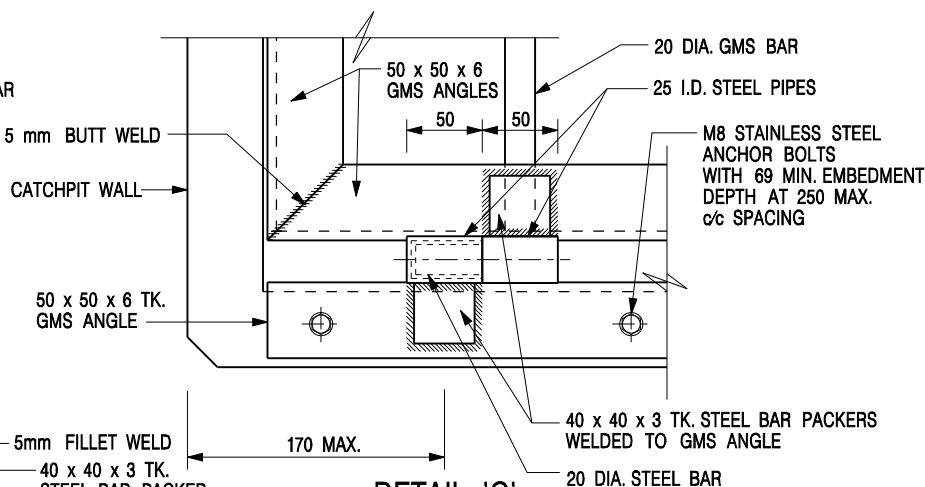
DETAIL 'A'

(DETAILS OF DOUBLE SIDE OPENING STEEL GRATING FOR L > 900mm)

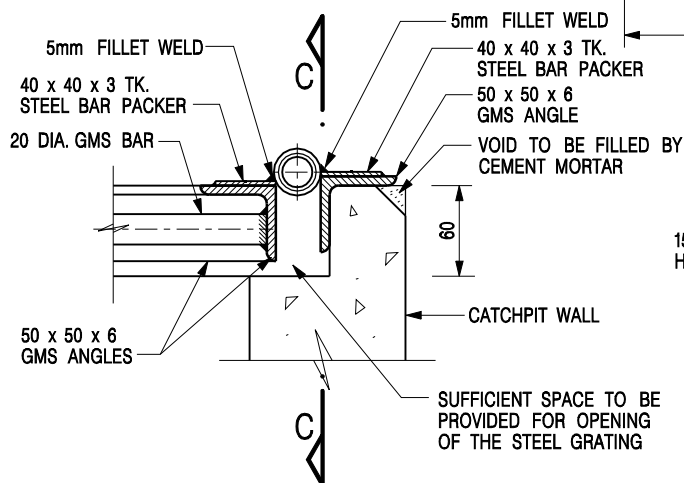
SCALE 1 : 20



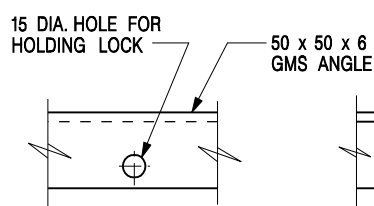
SECTION C - C



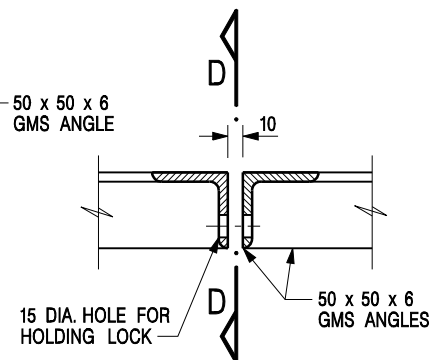
DETAIL 'C'
(DETAILS OF HINGE)
SCALE 1 : 5



SECTIONAL ELEVATION
(DETAIL 'C')



SECTION D - D




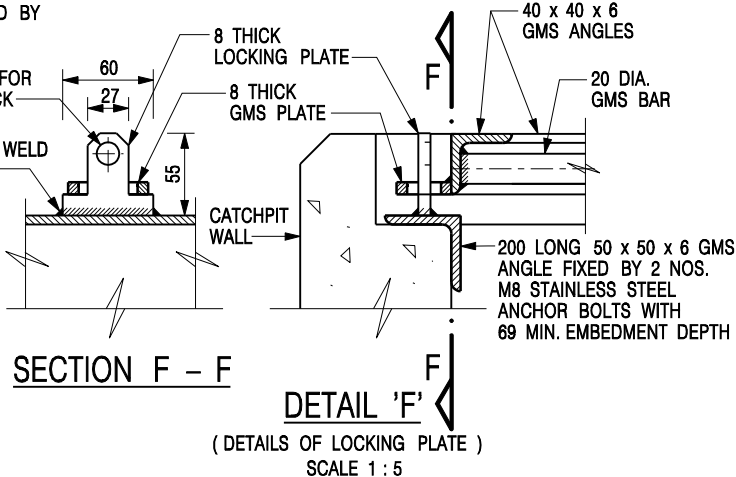
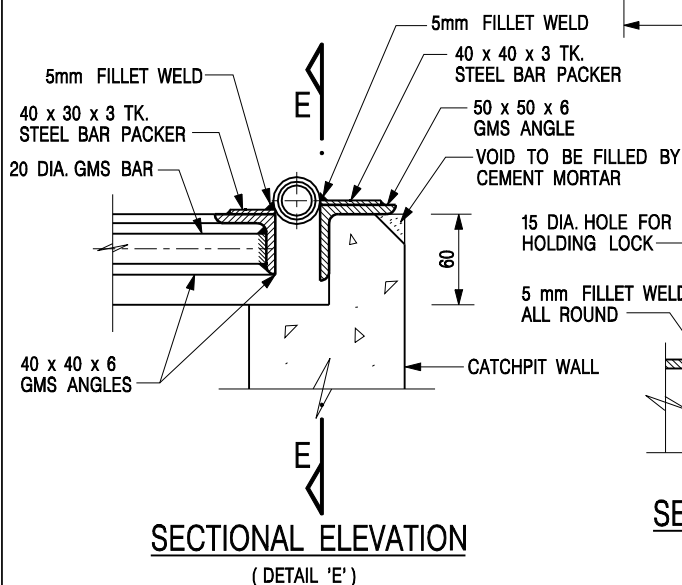
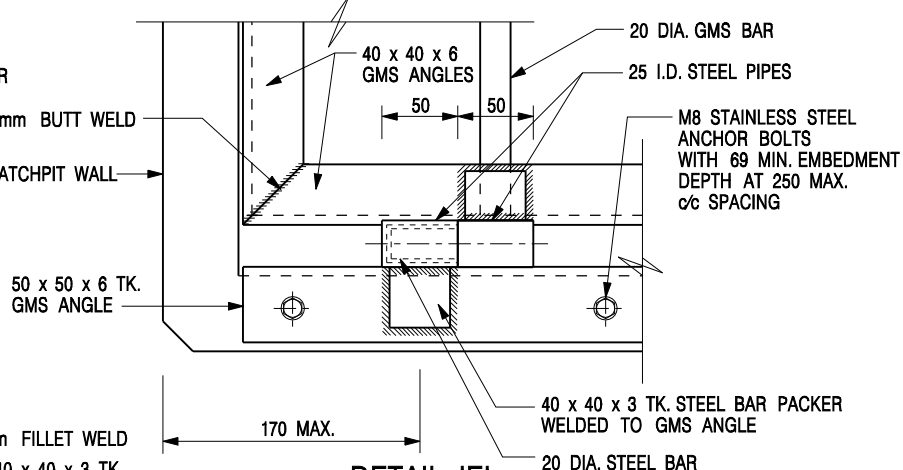
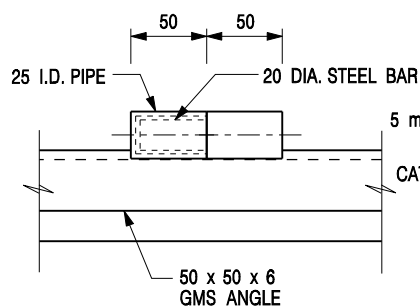
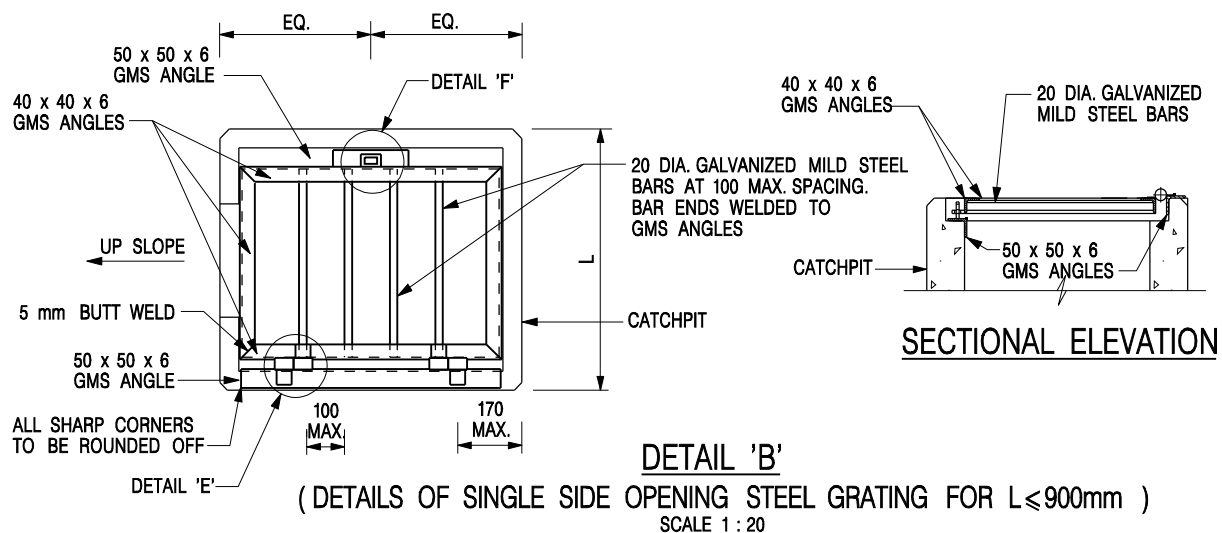
DETAIL 'D'
(DETAILS OF HOLE FOR LOCK)
SCALE 1 : 5

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 2 OF 5)


-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /2	

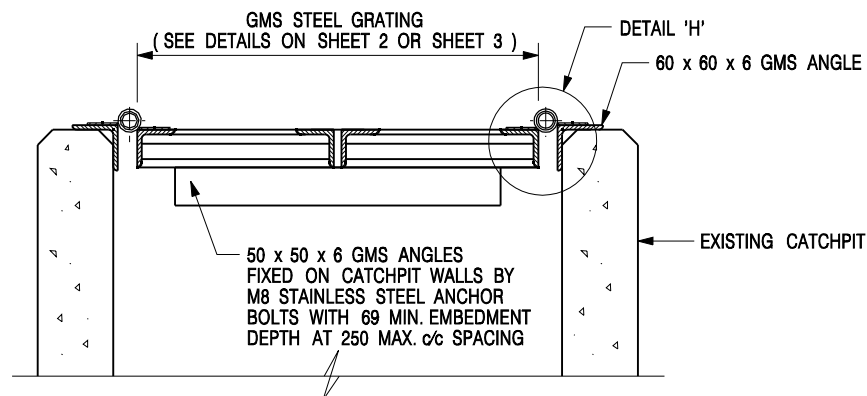


NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

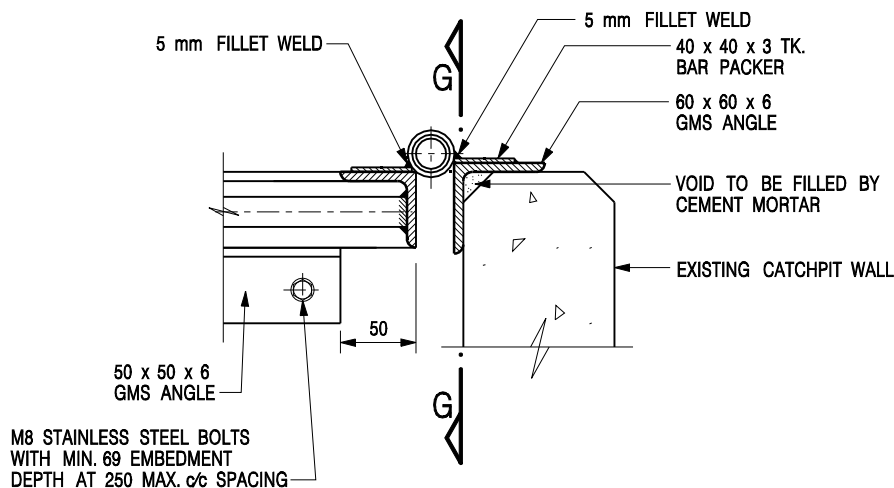
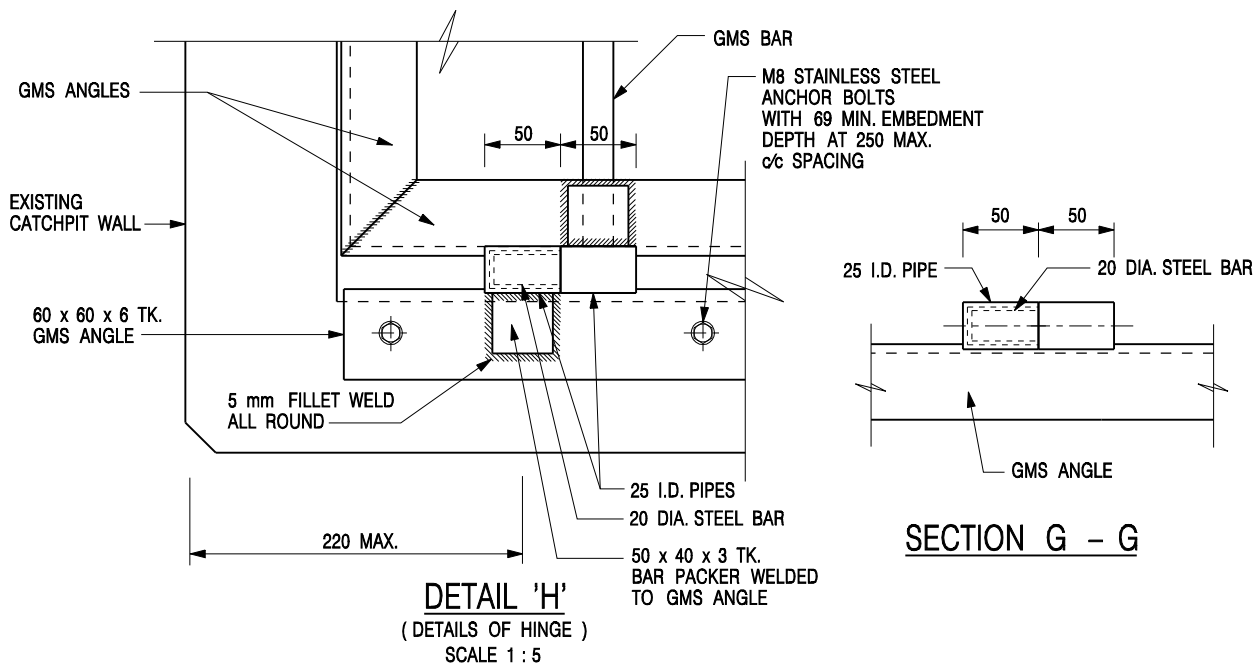
STANDARD CATCHPIT DETAILS
(SHEET 3 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
 CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT		SCALE AS SHOWN DATE JAN 1991	
		DRAWING NO. C2405 /3	



DETAIL 'G' - DETAILS OF STEEL GRATING CONSTRUCTED ON EXISTING CATCHPIT

SCALE 1 : 10




SECTIONAL ELEVATION

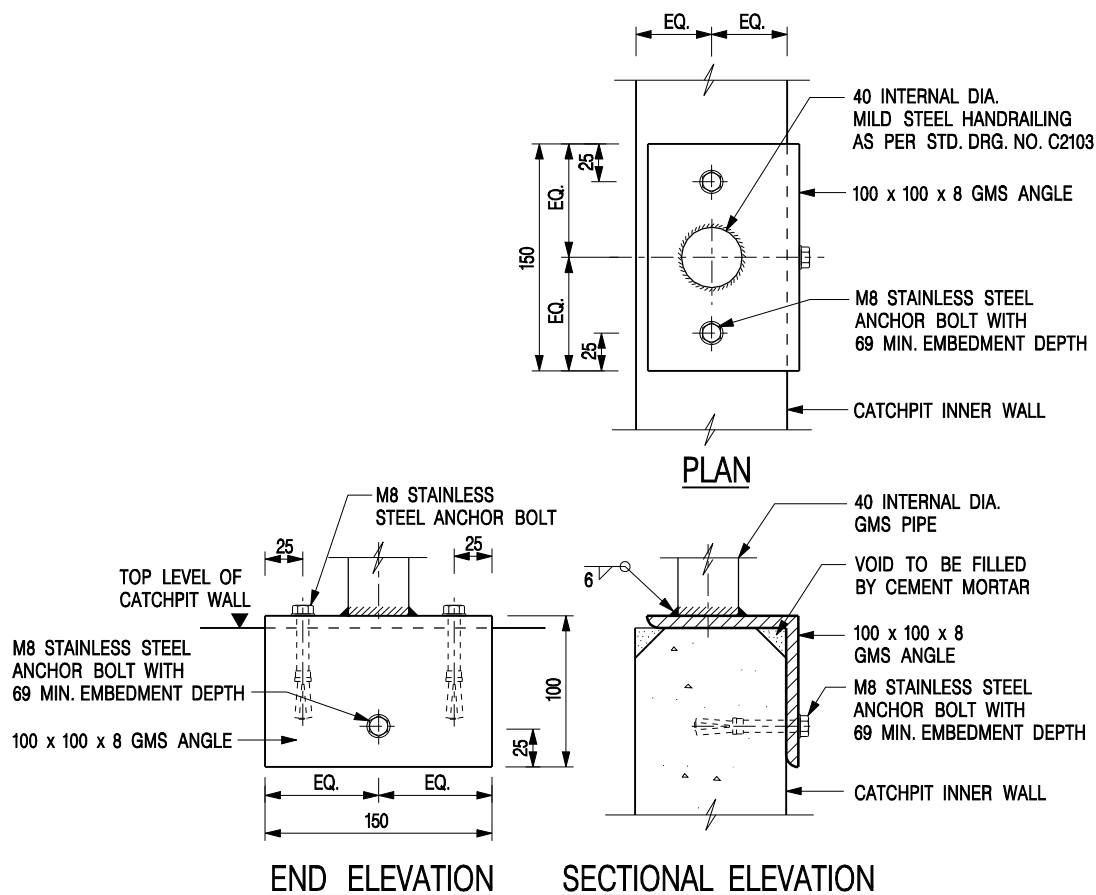
(DETAIL 'H')

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. REFER TO SHEET 5 FOR OTHER NOTES.

STANDARD CATCHPIT DETAILS
(SHEET 4 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE
<div><div>CIVIL ENGINEERING AND DEVELOPMENT DEPARTMENT</div></div>			
SCALE AS SHOWN		DRAWING NO. C2405 /4	
DATE JAN 1991			



DETAIL 'J' – FIXING DETAILS FOR HANDRAILING ON TOP OF CATCHPIT WALL

SCALE 1 : 5

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- ALL CONCRETE SHALL BE GRADE 20 /20.
- CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
- FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
- CONCRETE TO BE COLOURED AS SPECIFIED.
- FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAILS ON SHEET 2 OR SHEET 3) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
- IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON SHEET 5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
- MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 mm c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
- FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON SHEET 4.
- ALL STEEL ANGLES SHALL COMPLY WITH BS EN 10025 AND BS EN 10056.
- UNLESS OTHERWISE SPECIFIED, ALL WELDS SHALL BE 5 mm CONTINUOUS FILLET WELDS.
- ALL WELDS SHALL BE CHIPPED, GROUND SMOOTH, BRUSHED TO REMOVE SLAG PRIOR TO HOT-DIP GALVANIZATION.
- ALL STEELWORK SHALL BE HOT-DIP GALVANIZED TO BS EN ISO 1461. ALL EXPOSED STEELWORK SURFACES SHALL BE TREATED AND PAINTED IN ACCORDANCE WITH THE GENERAL SPECIFICATION.
- SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

STANDARD CATCHPIT DETAILS
(SHEET 5 OF 5)

-	FORMER DRG. NO. C2405J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

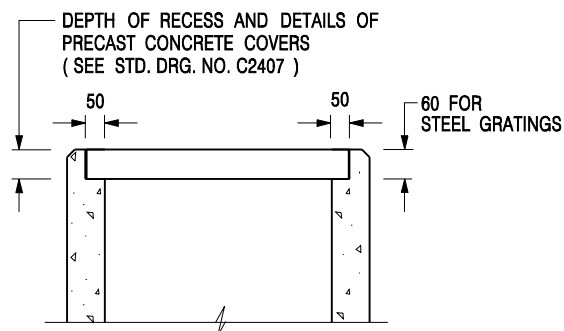


**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE AS SHOWN

DATE JAN 1991

DRAWING NO.
C2405 /5



ALTERNATIVE TOP SECTION FOR PRECAST CONCRETE COVERS / GRATINGS

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL CONCRETE SHALL BE GRADE 20 /20.
3. CONCRETE SURFACE FINISH SHALL BE CLASS U2 OR F2 AS APPROPRIATE.
4. FOR DETAILS OF JOINT, REFER TO STD. DRG. NO. C2413.
5. CONCRETE TO BE COLOURED AS SPECIFIED.
6. UNLESS REQUESTED BY THE MAINTENANCE PARTY AND AS DIRECTED BY THE ENGINEER, CATCHPIT WITH TRAP IS NORMALLY NOT PREFERRED DUE TO PONDING PROBLEM.
7. UPON THE REQUEST FROM MAINTENANCE PARTY, DRAIN PIPES AT CATCHPIT BASE CAN BE USED BUT THIS IS FOR CATCHPITS LOCATED AT SLOPE TOE ONLY AND AS DIRECTED BY THE ENGINEER.
8. FOR CATCHPITS CONSTRUCTED ON OR ADJACENT TO A FOOTPATH, STEEL GRATINGS (SEE DETAIL 'A' ON STD. DRG. NO. C2405 /2) OR CONCRETE COVERS (SEE STD. DRG. NO. C2407) SHALL BE PROVIDED AS DIRECTED BY THE ENGINEER.
9. IF INSTRUCTED BY THE ENGINEER, HANDRAILING (SEE DETAIL 'J' ON STD. DRG. NO. C2405 /5; EXCEPT ON THE UPSLOPE SIDE) IN LIEU OF STEEL GRATINGS OR CONCRETE COVERS CAN BE ACCEPTED AS AN ALTERNATIVE SAFETY MEASURE FOR CATCHPITS NOT ON A FOOTPATH NOR ADJACENT TO IT. TOP OF THE HANDRAILING SHALL BE 1 000 mm MIN. MEASURED FROM THE ADJACENT GROUND LEVEL.
10. MINIMUM INTERNAL CATCHPIT WIDTH SHALL BE 1 000 mm FOR CATCHPITS WITH A HEIGHT EXCEEDING 1 000 mm MEASURED FROM THE INVERT LEVEL TO THE ADJACENT GROUND LEVEL. AND, STEP IRONS (SEE DSD STD. DRG. NO. DS1043) AT 300 c/c STAGGERED SHALL BE PROVIDED. THICKNESS OF CATCHPIT WALL FOR INSTALLATION OF STEP IRONS SHALL BE INCREASED TO 150 mm.
11. FOR RETROFITTING AN EXISTING CATCHPIT WITH STEEL GRATING, SEE DETAIL 'G' ON STD. DRG. NO. C2405 /4.
12. SUBJECT TO THE APPROVAL OF THE ENGINEER, OTHER MATERIALS CAN ALSO BE USED AS COVERS / GRATINGS.

A	MINOR AMENDMENT.	Original Signed	04.2016
-	FORMER DRG. NO. C2406J.	Original Signed	03.2015
REF.	REVISION	SIGNATURE	DATE

**CATCHPIT WITH TRAP
(SHEET 2 OF 2)**



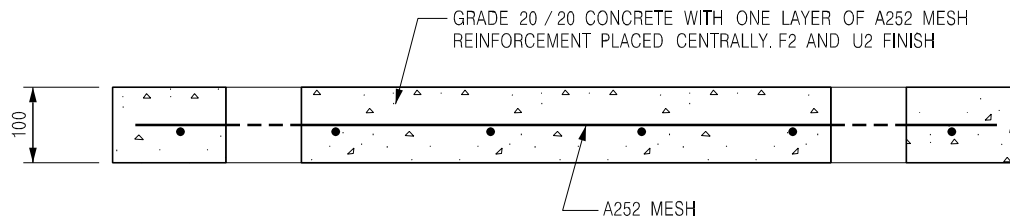
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

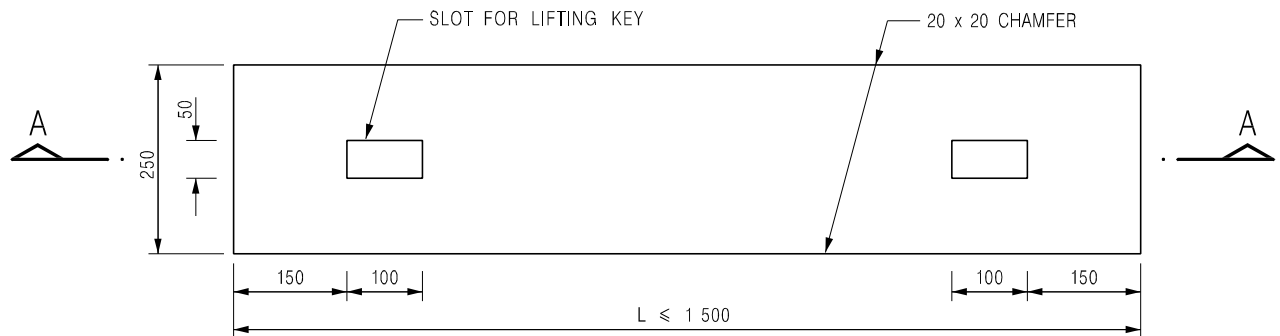
DATE JAN 1991

DRAWING NO.

C2406 /2A

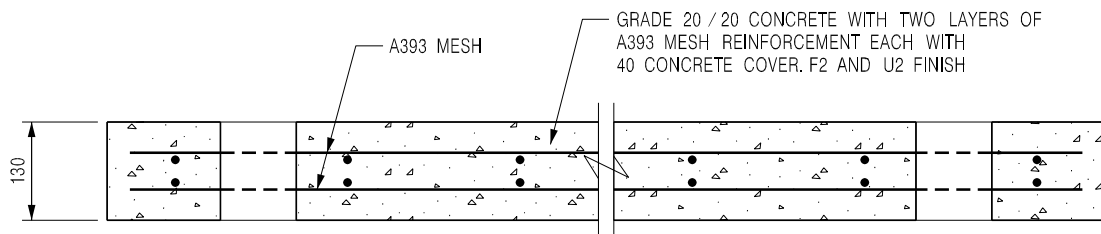


SECTION A - A

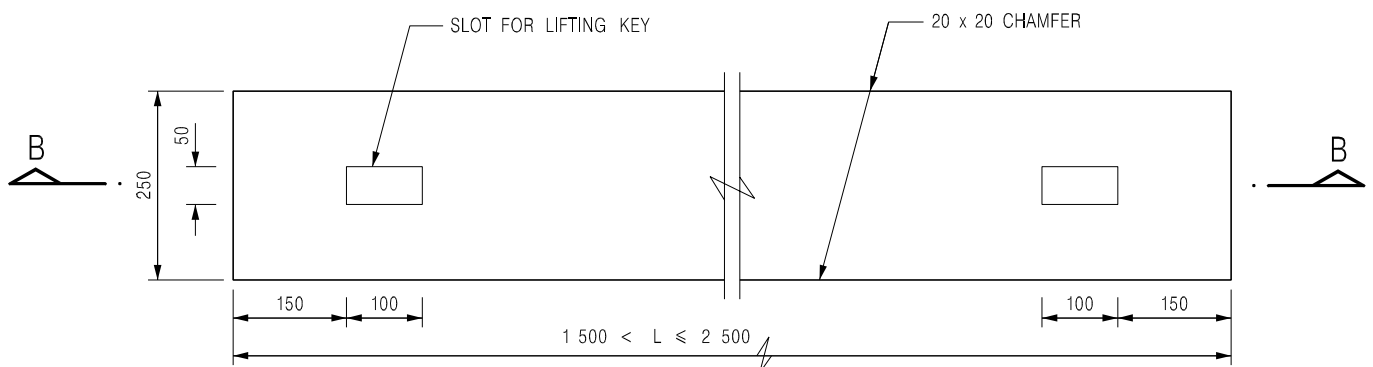


PLAN

TYPE 1 - FOR SPAN UP TO 1.5 m



SECTION B - B



PLAN

TYPE 2 - FOR SPANS 1.5 m TO 2.5 m

NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES.
2. ALL EXTERNAL EDGES OF THE COVERS SHALL BE 20mm CHAMFERED.

B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	GENERAL REVISION	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**PRECAST CONCRETE COVERS
FOR CATCHPIT AND SAND TRAP**

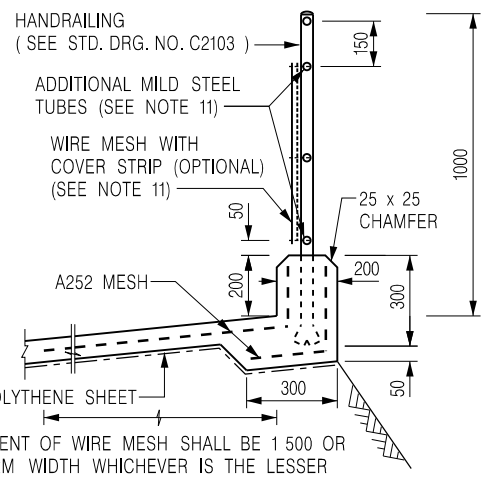


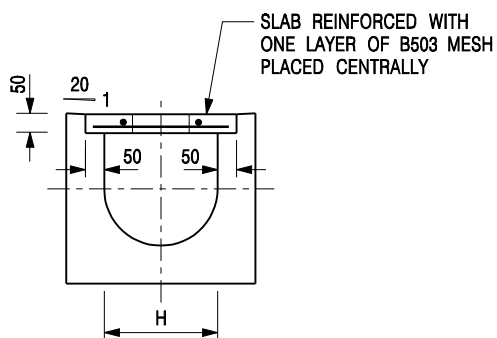
**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 10

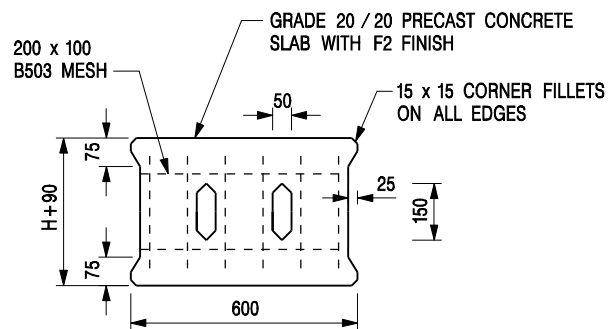
DATE JAN 1991

DRAWING NO.
C2407B





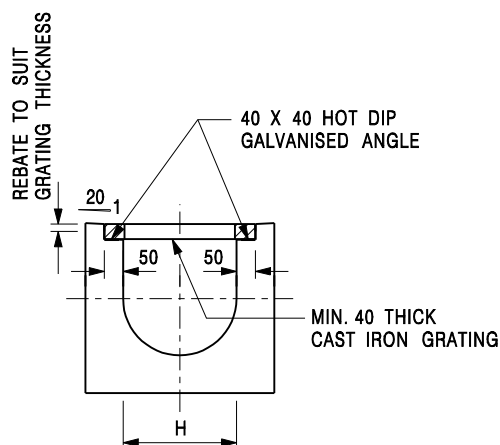
TYPICAL SECTION



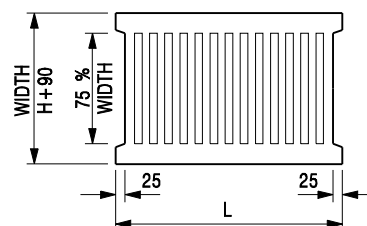
PLAN OF SLAB

U-CHANNELS WITH PRECAST CONCRETE SLABS

(UP TO H OF 525)



TYPICAL SECTION



L = 600mm FOR H ≤ 375mm
L = 400mm FOR H > 375mm

CAST IRON GRATING

(DIMENSIONS ARE FOR GUIDANCE ONLY, CONTRACTOR MAY SUBMIT EQUIVALENT TYPE)

U-CHANNEL WITH CAST IRON GRATING

(UP TO H OF 525)

NOTES:

- ALL DIMENSIONS ARE IN MILLIMETRES.
- H=NOMINAL CHANNEL SIZE.
- ALL CAST IRON FOR GRATINGS SHALL BE GRADE EN-GJL-150 COMPLYING WITH BS EN 1561.
- FOR COVERED CHANNELS TO BE HANDED OVER TO HIGHWAYS DEPARTMENT FOR MAINTENANCE, THE GRATING DETAILS SHALL FOLLOW THOSE AS SHOWN ON HyD STD. DRG. NO. H3156.

E	NOTES 3 & 4 AMENDED.	Original Signed	12.2014
D	NOTE 4 ADDED.	Original Signed	06.2008
C	MINOR AMENDMENT. NOTE 3 ADDED.	Original Signed	12.2005
B	NAME OF DEPARTMENT AMENDED.	Original Signed	01.2005
A	CAST IRON GRATING AMENDED.	Original Signed	12.2002
REF.	REVISION	SIGNATURE	DATE

**COVER SLAB AND CAST IRON
GRATING FOR CHANNELS**



**CIVIL ENGINEERING AND
DEVELOPMENT DEPARTMENT**

SCALE 1 : 20

DATE JAN 1991

DRAWING NO.
C2412E

Annex B

Revised Traffic Impact Assessment

Document Status Control Record

**Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi),
Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom),
Eating Place with Ancillary Site Office for a Period of 5 Years
at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land,
Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.**

Traffic Impact Assessment Report

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	Approved by: SLN	SLN	Date: 4 November 2025
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1 INTRODUCTION

1.1 Background

- 1.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi), vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office for a period of 5 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T..
- 1.1.2 The Site is zoned as “Open Storage” use under the Approved Ping Che and Ta Kwu Ling Outline Zoning Plan (OZP) No. S/NE-TKL/14. A planning application is required for the proposed temporary use.
- 1.1.3 LLA Consultancy Limited was commissioned to undertake the TIA to support the planning application. This TIA report presents the findings of the study.

1.2 Objectives

- 1.2.1 The objectives of the study are as follows:
- to review the existing traffic conditions in the vicinity of the Site;
 - to estimate the traffic generation and attraction of the proposed temporary use;
 - to project the future traffic situations in the surrounding road network;
 - to appraise the potential traffic impact of the proposed temporary use; and
 - to recommend the internal transport facilities for the proposed temporary use.

2 THE PROPOSED DEVELOPMENT

2.1 The Site

- 2.1.1 As shown in **Figure 2.1**, the Site is located at Sha Tau Kok Road – Lung Yeuk Tau area. It has a total site area of about 4,180 m².

2.2 The Proposed Use and Internal Transport Facilities Provisions

- 2.2.1 The main function of the Site is to provide charging facilities for electric taxis with supporting facilities, including vehicle repair workshop, eating place and ancillary office. The key development parameters are listed out in **Table 2.1**.

Table 2.1 Key Development Parameters

Use	Quantity
Site Area	4,180 m ²
EV Charging Facilities for Electric Taxi only (2.5m x 5.0m)	18 nos.
Parking Spaces / Waiting Spaces for Electric Taxi only (2.5m x 5.0m)	15 spaces
Motor Vehicle Showroom, Vehicle Repair Workshop, Eating Place, Ancillary Office (Retail and Office Use)	2,237 m ²
Parking Space for Retail/Office Use @ 1 space per 150 m ² (2.5m x 5.0m)	15 spaces
Parking Space for Staff/Visitors (2.5m x 5.0m)	6 spaces
Loading/unloading Space for Goods Vehicle (3.5m x 7.0m)	1 space

2.3 Swept Path Analysis

- 2.3.1 The Site is directly abutted to Sha Tau Kok Road – Lung Yeuk Tau, the development traffic is anticipated to enter and leave the Site with a left-in/left out movement onto Sha Tau Kok Road north-eastbound. In order to ensure the safety of pedestrians walking along the public footpath, a standard run-in/out in accordance with Highways Department's standards will be formed and flashing warning lights will be installed at both sides of the run-in/out as shown in **Figure 2.2**.
- 2.3.2 To ensure smooth manoeuvring of the parking area, swept path analysis was conducted to demonstrate that adequate space is provided for the vehicle's manoeuvring as shown in **Figure SP-01 – SP-02**.

3 EXISTING TRAFFIC SITUATION

3.1 Existing Road Network

- 3.1.1 The Site has a frontage along Sha Tau Kok Road – Lung Yeuk Tau and an existing run-in/out is provided for the current use of the Site.
- 3.1.2 Sha Tau Kok Road – Lung Yeuk Tau is a dual two lanes distributor road connecting Fanling and Sha Tau Kok. In 2023, it recorded an AADT of 22,810 vehicles.

3.2 Traffic Count Surveys

- 3.2.1 In order to appraise the existing traffic conditions, a traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30, the peak traffic generation period of the vehicle charging station. More details will be discussed in **Section 4.2**.
- 3.2.2 The locations of the surveyed junctions are shown in **Figure 3.1**.
- J1 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road
- J2 – J/O Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road
- 3.2.3 The peak hour identified are 16:30 – 17:30. The observed 2025 traffic flows are presented in **Figure 3.2**.

3.3 Existing Junction Capacity Assessment

- 3.3.1 Based on the observed traffic flows, the performance of the junctions is assessed. It is calculated that all junctions are operating satisfactorily. The results are presented in **Table 3.1** and the detailed calculation sheets are attached in **Appendix A**.

Table 3.1 Existing Junction Capacity Assessment

Ref.	Junction Location	Type/Index ⁽¹⁾	Peak Hour of the Vehicle Charing Station
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.46
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.43

Note: (1) DFC = Design Flow to Capacity

3.4 Existing Link Capacity Assessment

- 3.4.1 The Volume to Capacity (V/C) Ratios of Sha Tau Kok Road – Lung Yeuk Tau is assessed and the results are presented in **Table 3.2**.

Table 3.2 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	Peak Hour Traffic Volume (pcu/hr)	Peak Hour V/C Ratio
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	811	0.26
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	935	0.30

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

- 3.4.2 As shown in **Table 3.2**, the concerned road sections are operating with spare capacity during the peak hour.

4 FUTURE TRAFFIC SITUATION

4.1 Design Year

- 4.1.1 The proposed use, if approved by the Town planning Board, will be valid for 5 years until 2030. Hence, Year 2030 is adopted as the design assessment year.

4.2 Traffic Generation of the Proposed Development

Traffic Generation for Electric Vehicle Charging Station

- 4.2.1 For the charging facilities, all EV chargers are direct current (DC) quick chargers with output power of 120kW which can provide 600km of mileage when an electric taxi charged for about 30 minutes.
- 4.2.2 From the taxi trade, the drivers are usually working on two shifts, i.e., the day shift and the night shift. In general, the day shift is 05:00 to 17:00 while the night shift is 17:00 to 05:00. The peak hour usage of charging facilities is expected to be the time periods before the shift such that a fully charged taxi can be passed to the driver in the next shift. As a result, the peak traffic generation of the proposed charging station will be either between 15:00 to 17:00 or after midnight. It is anticipated that minimal drivers will charge their taxis during the normal highway peak hours because these time periods will be the busiest hour in their business.
- 4.2.3 According to the Hong Kong Planning Standard and Guidelines, Section 3.9.4 of Chapter 12, a minimum of two waiting spaces should be provided in each EV charging station to avoid the awaiting vehicles to queue on public roads. In view of the size of the Site, 15 waiting spaces can be provided. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.
- 4.2.4 The estimated traffic generation and attraction for Electric Vehicle Charging Station during the peak operation hour will be a two-way traffic of 36 taxis (36 Nos. of vehicles = 18 EV charging stations x 2 because of 30 minutes charging/servicing time for each taxi).

Traffic Generation for Motor Vehicle Showroom, Vehicle Repair Workshop, Eating Place, Ancillary Office (Retail and Office Use)

- 4.2.5 For conservative assessment purpose, traffic generated/ attracted by these components of the proposed development would be estimated based on the PM trip rates for retail documented in TPDM Volume 1 Chapter 3 – Transport Considerations of Town Plans.
- 4.2.6 Based on the above, the traffic generation of the proposed development has been estimated and presented in **Table 4.1**.

Table 4.1 Development Traffic Generation

Proposed Use	Unit /Content	Operation Peak Hour		
		Generation	Attraction	Total
Adopted Trip rates				
Retail ⁽¹⁾	pcu/hr/100m ²	0.3100	0.3563	-
Estimated Traffic Generation/Attraction				
EV Charging Station	18 stations	36 ⁽²⁾	36 ⁽²⁾	72 ⁽²⁾
Ancillary Office and Retail Use	2,235 m ²	7	8	15
Total		43	44	87

Notes: (1) TPDM mean trip rates for retail use (AM peak) are adopted.
(2) Traffic generation and attraction discussed in **Section 4.2** are adopted.

4.2.7 As shown in **Table 4.2**, the Site will generate a two-way traffic flows of 87 pcu/hour in the peak hour. The distribution of the development traffic is shown in **Figure 4.1**.

4.3 Traffic Generation of the Adjacent New Planning Application

4.3.1 One new application (Planning Application No. A/NE-LYT/860) at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, is identified in the vicinity of the Site and it is a temporary electric vehicle charging station. Having considered this application would have similar operation hours and patterns as the proposed temporary use, the traffic generation and attraction of this application are incorporated in the subsequent assessment and shown in **Table 4.2**.

Table 4.2 Traffic Generation of the Adjacent New Planning Application

Proposed Use	Unit /Content	Operation Peak Hour ⁽¹⁾		
		Generation	Attraction	Total
Estimated Traffic Generation/Attraction				
EV Charging Station	12 charging station	24	24	48

Note: (1) Traffic generation and attraction numbers are abstracted from the TIA report of the application.

4.3.2 As shown in **Table 4.2**, this development would generate two-way traffic flows of 48 pcu/hr in the operation peak hour.

4.4 Traffic Growth

Annual Traffic Census (ATC) Data

4.4.1 Reference was made to the 2019 to 2023 Annual Traffic Census Reports, published by the Transport Department, to determine the natural traffic growth. The traffic data recorded at counting station in the vicinity of the Site is shown in **Table 4.3**.

Table 4.3 Annual Traffic Census Data

Stn. No.	Road Section			AADT (vehicle/day) ⁽¹⁾					Average Annual Growth (%)
	Road	From	To	2019	2020	2021	2022	2023	
5660	Sha Tau Kok Rd	On Kui St	Ping Che Rd	33,630	23,740 (-29.4%)	22,980 (-3.2%)	22,280 (-3%)	22,810 (2.4%)	-1.3% (Between 2020 and 2023)

Note: (1) Figures in bracket indicated the % increase between two successive years.

- 4.4.2 From **Table 4.3**, the recorded traffic flows in 2020 dropped significantly by almost 30% and was due to the opening of Lung Shan Tunnel in 2019. From 2020 to 2023, the recorded traffic flows have been quite steady with only -1.3% decrease annually.

Territorial Population and Employment Data Matrix (TPEDM)

- 4.4.3 Reference was also made to the 2021-based TPEDM published by Planning Department. The population and employment data of year 2026 and 2031 in Northeast New Territories are summarized in **Table 4.4**.

Table 4.4 Population and Employment Data in Northeast New Territories

Year	Population	Employment	Total
2026	1,361,150	421,350	1,782,500
2031	1,416,800	448,600	1,865,400
Average Annual Growth Rate			+0.9%

- 4.4.4 As shown in **Table 4.4**, the projected average annual growth rate of the population and employment total number under the TPEDM in Northeast New Territories is +0.9% between the years 2026 to 2031, which is larger than the annual growth rate derived from ATC. To be conservative, the larger growth rate of +0.9% is adopted for the subsequent assessments.

4.5 Reference and Design Flows

- 4.5.1 The 2030 Reference Flows, i.e. the future traffic flows in the local road network WITHOUT the development traffic, were estimated based on the following equation:

$$\text{2030 Reference Flows} = \text{2025 Existing Flows} \times (1 + 0.9\%)^5 + \text{Traffic generated and attracted by adjacent new planning application}$$

- 4.5.2 The 2030 Design Flows, i.e. the future traffic flows in the local road network WITH the development traffic, are estimated based on the following equations.

$$\text{2030 Design Flows} = \text{2030 Reference Flows} + \text{Traffic generated and attracted by the proposed development}$$

- 4.5.3 The forecast traffic flows for the two scenarios are shown in **Figures 4.2 and 4.3** respectively.

4.6 Junction Capacity Assessment

4.6.1 Junction capacity analysis is carried out for the assessment year 2030. The assessment results are shown in **Table 4.5** and the detailed calculation sheets are attached in **Appendix B**.

Table 4.5 2030 Junction Capacity Assessment

Ref.	Junction Location	Type/ Index ⁽¹⁾	2030 Reference Peak Hour	2030 Design Peak Hour
J1	Sha Tau Kok Road – Lung Yeuk Tau / Lung Ma Road	Roundabout /DFC	0.49	0.51
J2	Sha Tau Kok Road – Lung Yeuk Tau / Lau Shui Heung Road	Roundabout /DFC	0.46	0.47

Note: (1) DFC = Design Flow to Capacity.

4.6.2 The results in **Table 4.5** show that the assessed junctions will operate satisfactorily in both reference and design scenarios. Therefore, it is anticipated that the proposed temporary EV charging station will not induce significant traffic impact to the surrounding road network.

4.7 Link Capacity Assessment

4.7.1 The V/C Ratios of Sha Tau Kok Road were assessed and the results are presented in **Table 4.6**.

Table 4.6 2030 Link Capacity Assessments

Road	Capacity ⁽¹⁾ (pcu/hr)	2030 Traffic Volume		2030 V/C Ratio	
		Reference Scenario	Design Scenario	Reference Scenario	Design Scenario
Sha Tau Kok Road E/B (between Lung Ma Road and Lau Shui Heung Road)	3,120	853	897	0.27	0.29
Sha Tau Kok Road W/B (between Lau Shui Heung Road and Lung Ma Road)	3,120	1,002	1,036	0.32	0.33

Note: (1) Capacity refers to TPDM Vol.2 Ch. 2.4. A factor of 1.2 (based on the traffic count survey result) is adopted to convert the capacity from veh/hr to pcu/hr.

4.7.2 As shown in **Table 4.5**, all the concerned road sections will operate with capacity during AM and PM peak hours in all scenarios. Hence, it can be concluded that the traffic induced by the proposed temporary EV charging station will not induce significant traffic impact to the road network.

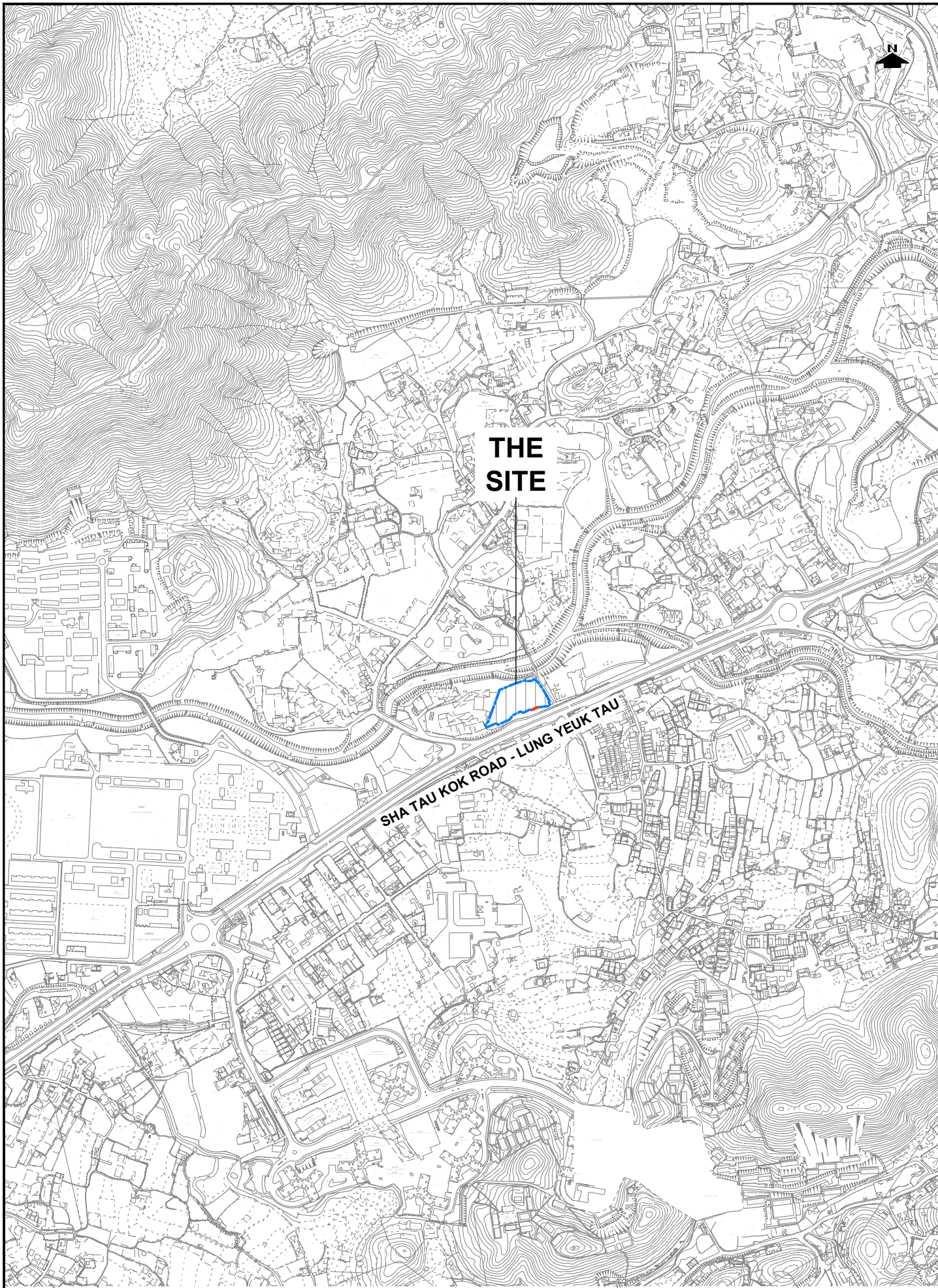
5 SUMMARY AND CONCLUSION

5.1 Summary

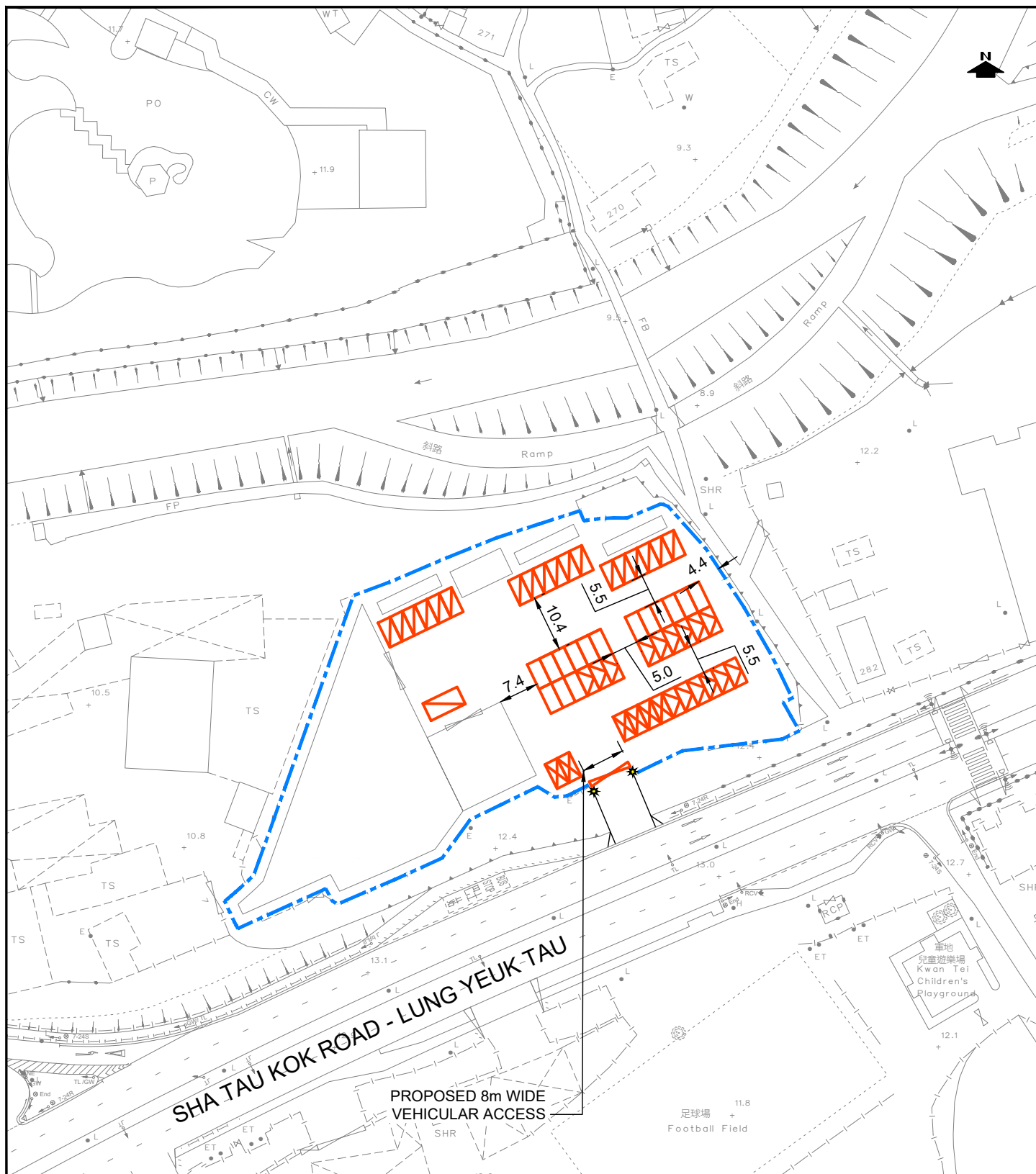
- 5.1.1 The Applicant proposes to use a piece of land for temporary electric vehicle charging station (for electric taxi), vehicle repair workshop, shop and services (motor vehicle showroom), eating place with ancillary site office for a period of 5 Years. The piece of land (hereinafter referred to “the Site”) is located at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road–Lung Yeuk Tau, Fanling, N.T. The main function of the Site is to provide charging facilities for electric taxis with supporting facilities, including vehicle repair workshop, motor vehicle showroom, eating place and ancillary office.
- 5.1.2 Traffic count survey was carried out on 22 May 2025 (Thursday) for the time periods of 14:30 – 17:30 which the time period will be the peak traffic generation period of the proposed development. Based on the existing traffic flows, the junction and link capacity assessments show that they are operating satisfactorily.
- 5.1.3 The proposed development is estimated to generate a two-way traffic of 87 pcu/hour in the operation peak hours. Junction and link capacity assessment was carried out for the reference and design scenarios in 2030 and the results revealed that the concerned junctions and road links would perform satisfactorily with spare capacity in 2030. Therefore, it is anticipated that the proposed development will not induce significant traffic impact to the surrounding road network.
- 5.1.4 For the 18 nos. of EV charging stations, 15 nos. of waiting spaces can be provided (minimum two spaces in each EV charging station) to avoid the awaiting vehicles to queue on public roads. In addition, to minimize the waiting time of the taxi drivers and hence maximize the available operation hours, a mobile application catered for the taxi drivers will be introduced which will include the payment platform for EV charging and provide real-time availability of EV charging stations at different locations managed by the Applicant.

5.2 Conclusion

- 5.2.1 This planning application is to allow a temporary vehicle charging station with supporting facilities, including vehicle repair workshop, motor vehicle showroom, eating place and ancillary office for a period of 5 years. Based on the assessment result, it can be concluded that the proposed use will not induce additional adverse traffic impact on the surrounding road network and it is considered acceptable from traffic engineering point of view.



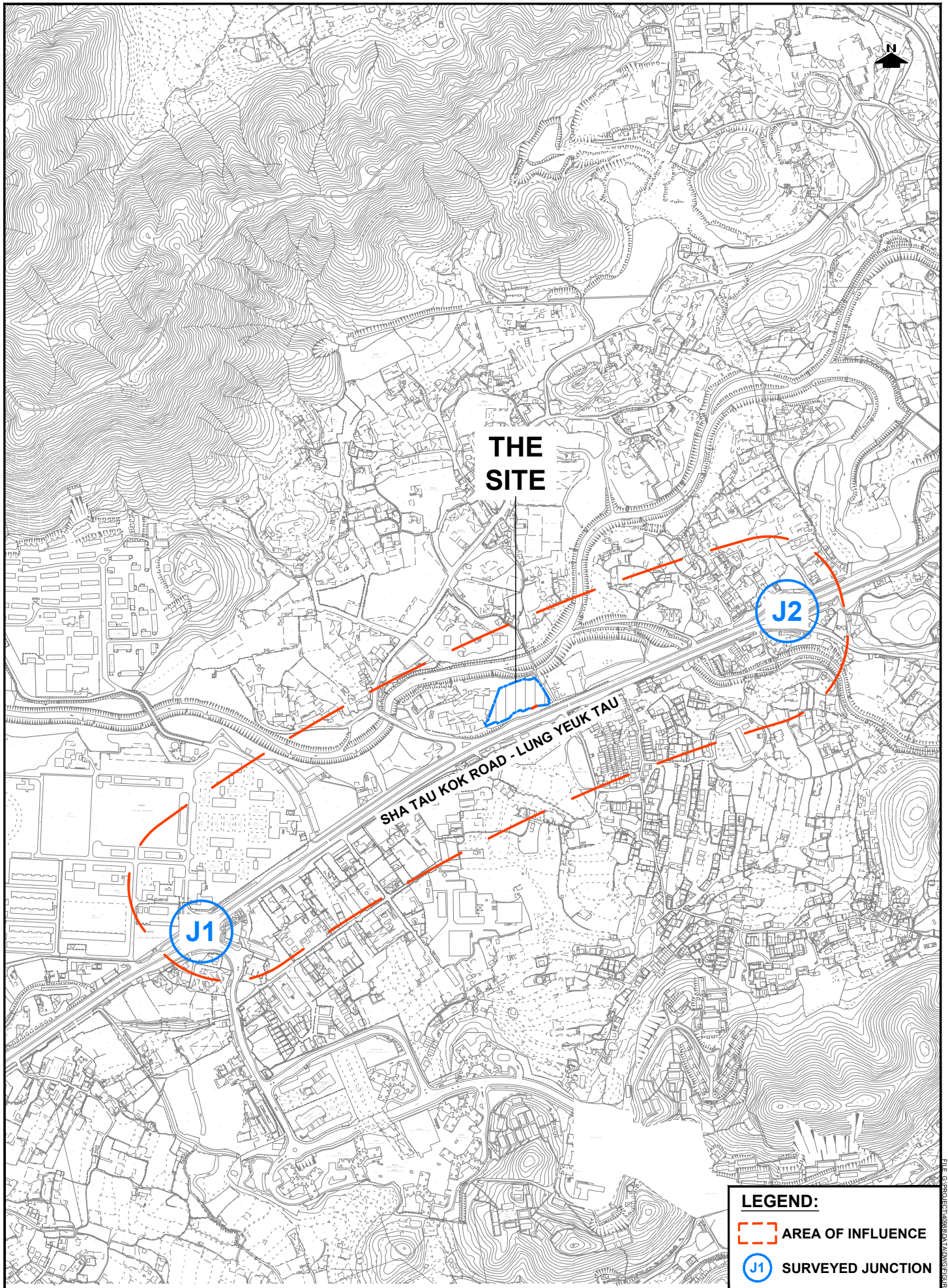
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DESIGNED SLN	DATE MAY 2025	DRAWING TITLE LOCATION PLAN		LLA 顧問有限公司 Consultancy Limited	
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LEGEND:

- - - - - APPLICATION SITE (AREA: ABOUT 4,180M²)
- EV CHARGING SPACES FOR ELECTRIC TAXI (18 NOS.)
- PARKING SPACES/WAITING SPACES FOR ELECTRIC TAXI (15 NOS.)
- PARKING SPACES FOR PRIVATE VEHICLES (FOR MOTOR VEHICLE SHOWROOM AND OFFICE USE) (15 NOS.)
- PARKING SPACES FOR STAFF/VISITORS (6 NOS.)
- LOADING/UNLOADING SPACE FOR LIGHT GOODS VEHICLE (3.5M X 7M) (1 NO.)
- ★ REVOLVING LAMPS

PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXLORD LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 2.2	REV. B
DESIGNED SLN	DATE JUL 2025	DRAWING TITLE PROPOSED LAYOUT PLAN		LLA 顧問有限公司 Consultancy Limited	
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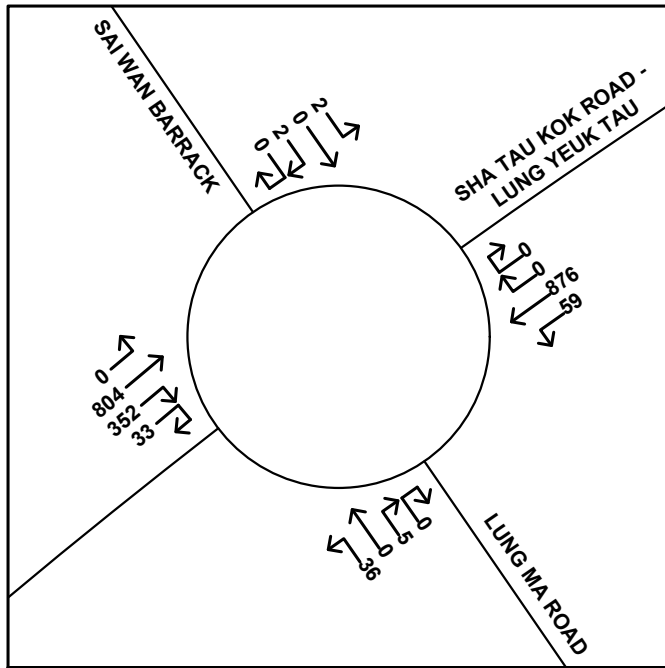


LEGEND:

-  **AREA OF INFLUENCE**
-  **J1 SURVEYED JUNCTION**

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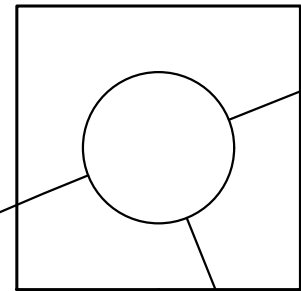
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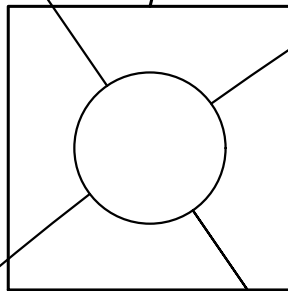
THE SITE



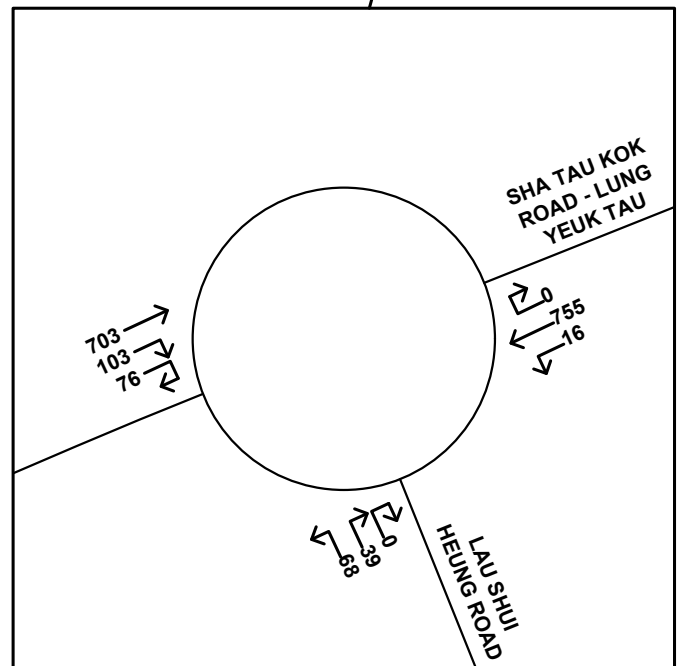
SHA TAU KOK ROAD - LUNG YEUK TAU



LAU SHUI
HEUNG ROAD



LUNG MA ROAD



SHA TAU KOK
ROAD - LUNG
YEUK TAU

LAU SHUI
HEUNG ROAD

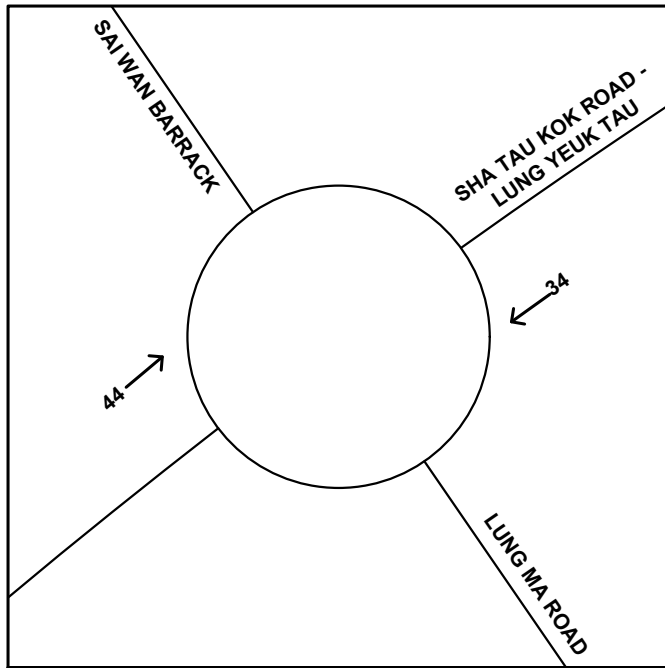
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123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

- 1, ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

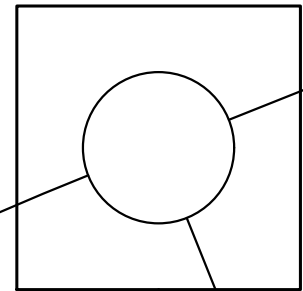
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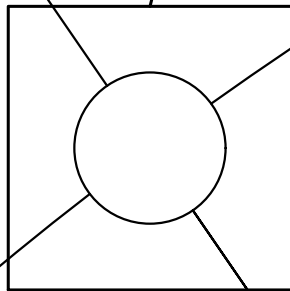
THE SITE



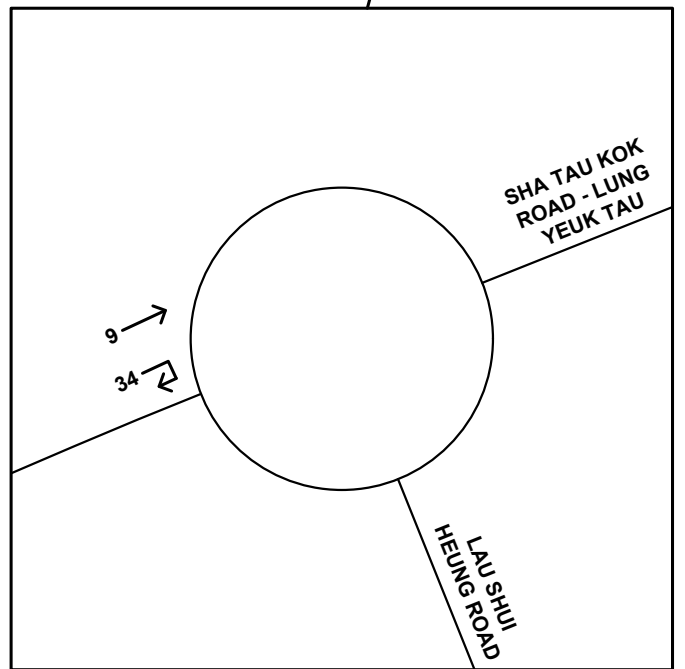
SHA TAU KOK ROAD - LUNG YEUK TAU



LAU SHUI
HEUNG ROAD



LUNG MA ROAD



SHA TAU KOK
ROAD - LUNG
YEUK TAU

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LAU SHUI
HEUNG ROAD

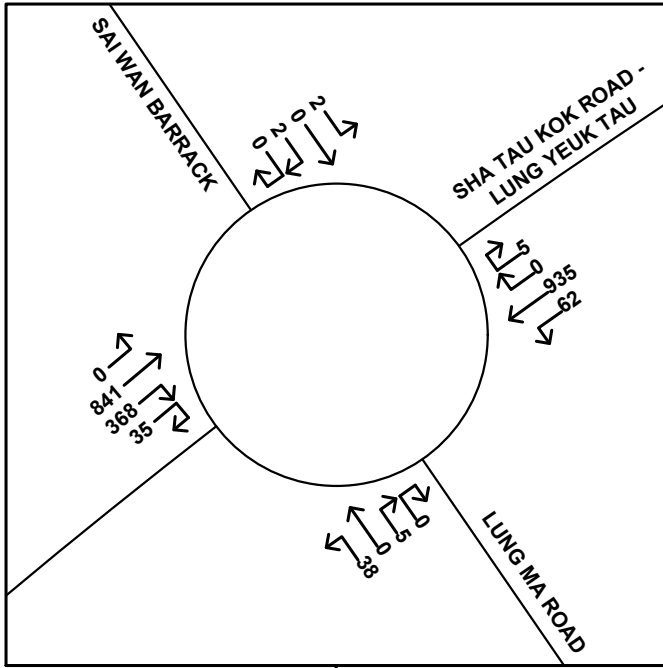
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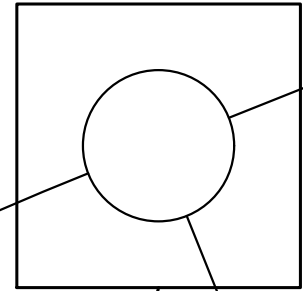
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DESIGNED SLN	DATE SEP 2025	DRAWING TITLE DEVELOPMENT TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
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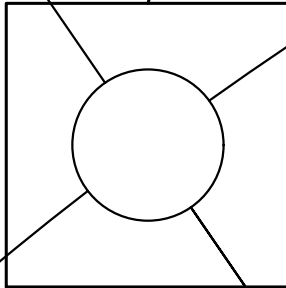
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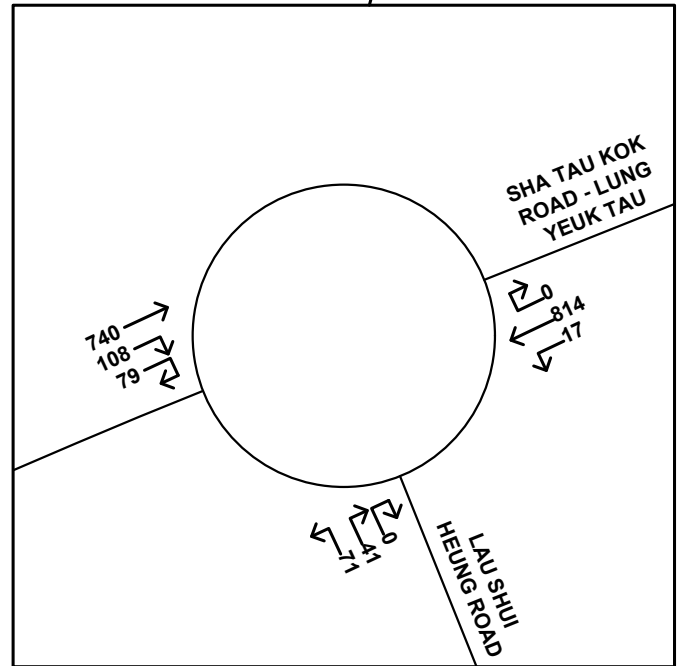
SHA TAU KOK ROAD - LUNG YEUK TAU



LAU SHUI
HEUNG ROAD



LUNG MA ROAD



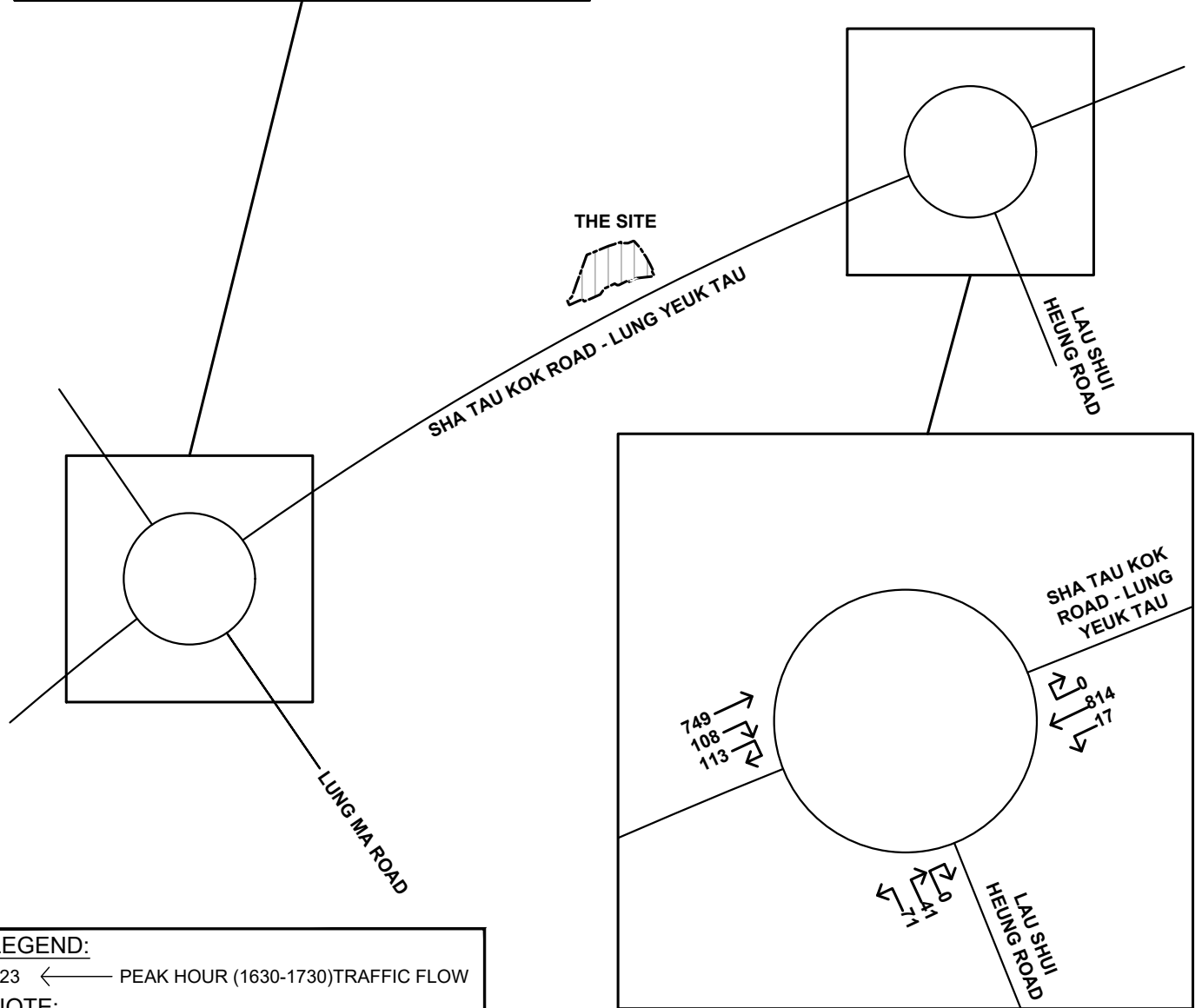
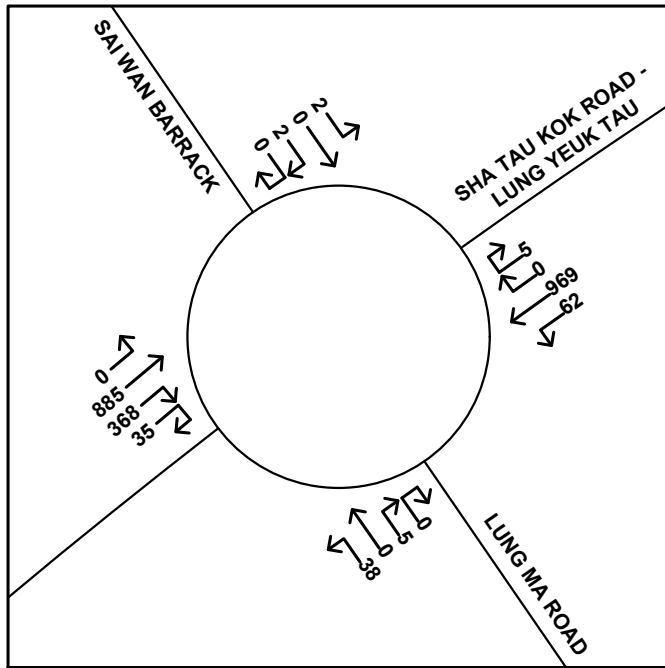
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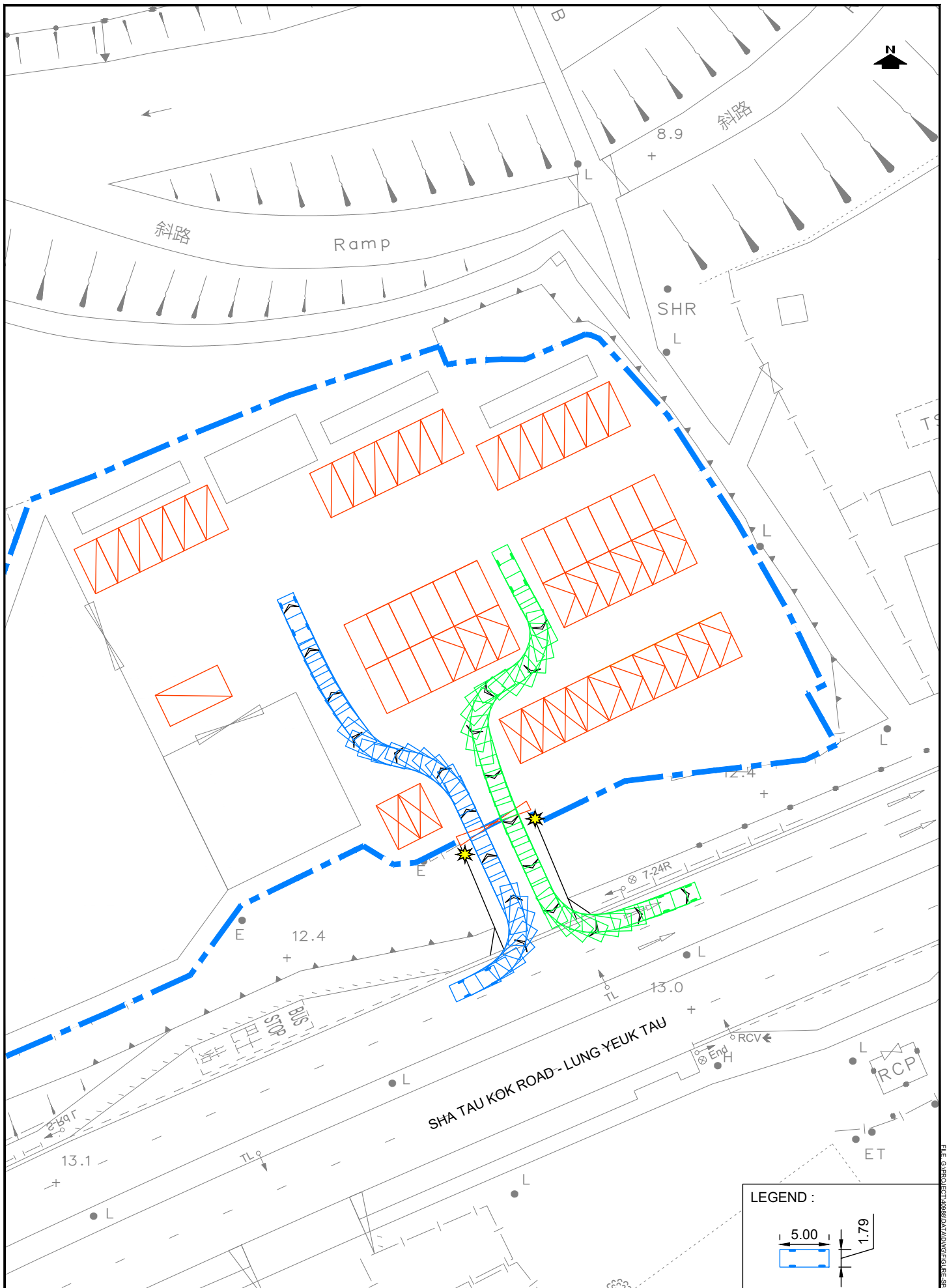
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123 ← PEAK HOUR (1630-1730) TRAFFIC FLOW

NOTE:

1. ALL TRAFFIC FLOWS ARE IN PCU/HOUR
2. MINOR ROADS ARE NOT SHOWN FOR CLARITY

PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOR LOT 464 S.A. RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. FIGURE 4.3	REV. B
DESIGNED SLN	DATE OCT 2025	DRAWING TITLE 2030 DESIGN TRAFFIC FLOWS		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE N.T.S.				
CHECKED SLN					



PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXLORD LOT 464 S A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. SP-01	REV. A
DESIGNED SLN	DATE JUL 2025	DRAWING TITLE SWEPT PATH ANALYSIS - PC		LLA 顧問有限公司 Consultancy Limited	
DRAWN CLL	SCALE 1:500				
CHECKED SLN					



PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOR LOT 464 S A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. SP-02	REV. B
DESIGNED SLN	DATE SEP 2025	DRAWING TITLE		<div><div>7.00</div><div>2.19</div></div> <div>LEGEND :</div> <div><div>7.00</div><div>2.19</div></div>	
DRAWN CLL	SCALE 1:500	SWEPT PATH ANALYSIS - LGV			
CHECKED SLN					

Appendix A
Junction Capacity Assessments
- Existing Scenario

LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDBABOUT CALCULATION

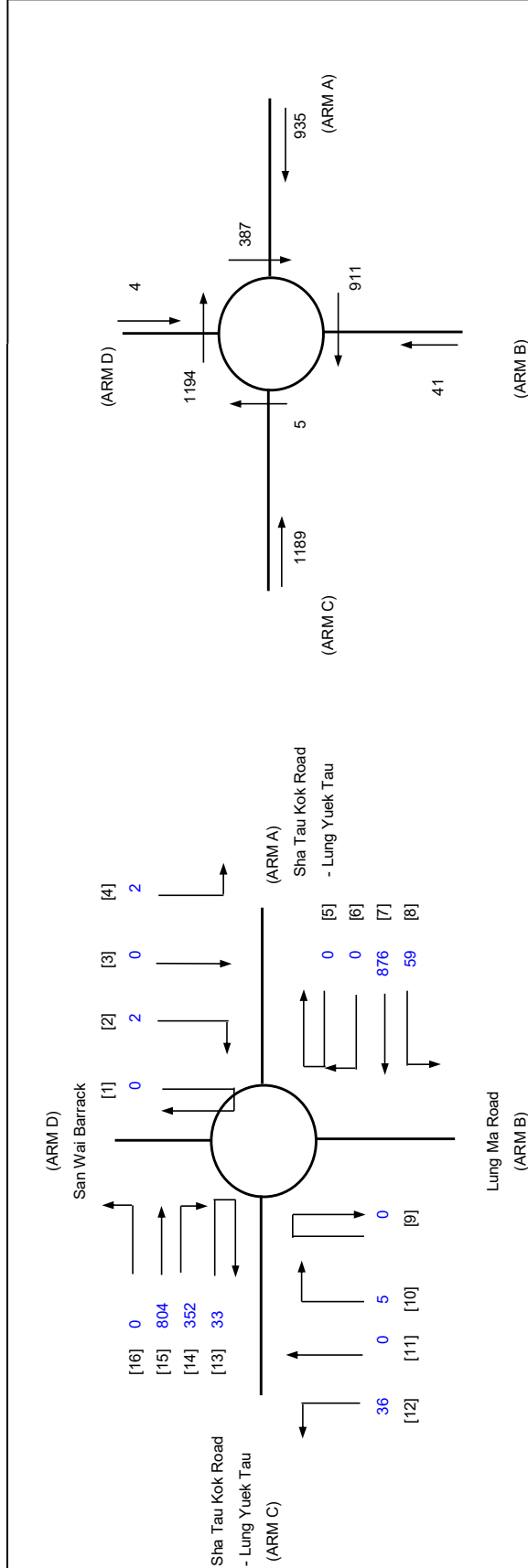
2025 Existing 1630-1730

PROJECT NO.: 40988
 FILENAME : J1_STKR_LMR.xl
 REFERENCE NO.:

PREPARED BY:
 CHECKED BY:
 REVIEWED BY:

INITIALS
 SKL
 SLN
 SLN

DATE
 Oct-25
 Oct-25
 Oct-25



ARM

INPUT PARAMETERS:

	A	B	C	D
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	935	41	1189	4
Qc = Circulating flow across entry (pcu/h)	387	911	5	1194

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.32	0.23	0.20	0.09
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	1.06	1.10	1.01	1.07
X2 = $V + ((E-V)/(1+2S))$	7.22	4.67	8.81	4.62
M = $EXP((D-60)/10)$	0.50	0.50	0.50	0.50
F = $303 \times X2$	2188	1415	2669	1401
Td = $1+(0.5/(1+M))$	1.33	1.33	1.33	1.33
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.68	0.54	0.77	0.54
Qe = $K(F-Fc \times Qc)$	2045	1017	2693	808

DFC = Design flow/Capacity = Q/Qe

Total In Sum = 1292 PCU

DFC of Critical Approach = 0.46

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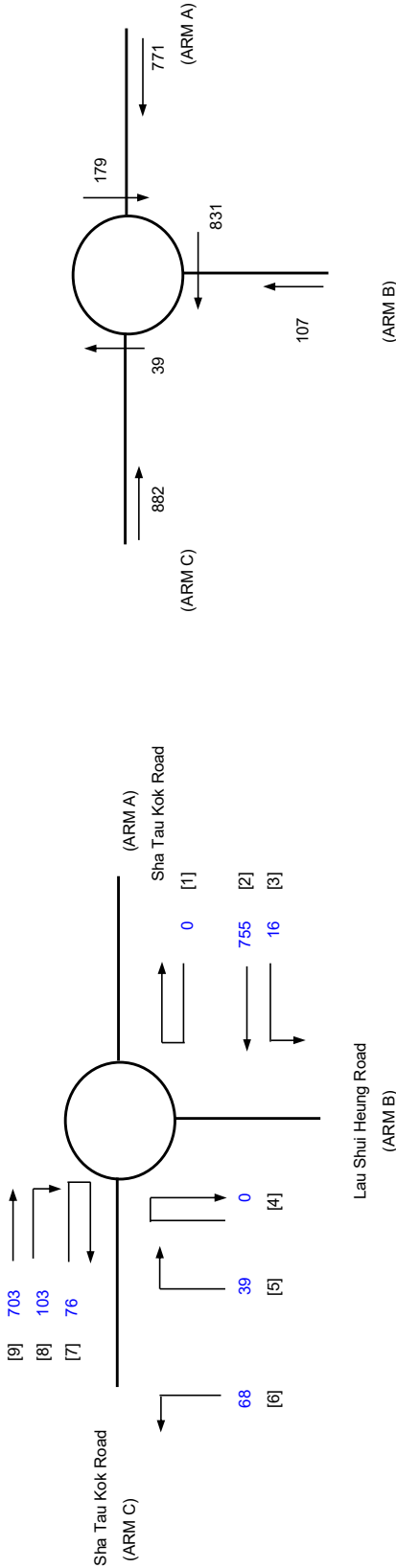
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDAABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	SKL	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SLN	Oct-25
REFERENCE NO.:		REVIEWED BY:	SLN	Oct-25

2025 Existing 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	771	107	882
Qc = Circulating flow across entry (pcu/h)	179	831	39

OUTPUT PARAMETERS:

S = Sharpness of flare = 1.6(E-V)/L	0.16	0.24	0.16
K = 1-0.00347(A-30)-0.978(1/R-0.05)	0.92	0.93	1.09
X2 = V + ((E-V)/(1+2S))	6.88	4.61	6.48
M = EXP((D-60)/10)	0.45	0.45	0.45
F = 303*X2	2083	1398	1962
Td = 1+(0.5/(1+M))	1.34	1.34	1.34
Fc = 0.21*Td(1+0.2*X2)	0.67	0.54	0.65
Qe = K(F-Fc*Qc)	1806	879	2120

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.43

Total In Sum = 1760 PCU

Appendix B

Junction Capacity Assessments - Reference & Design Scenarios

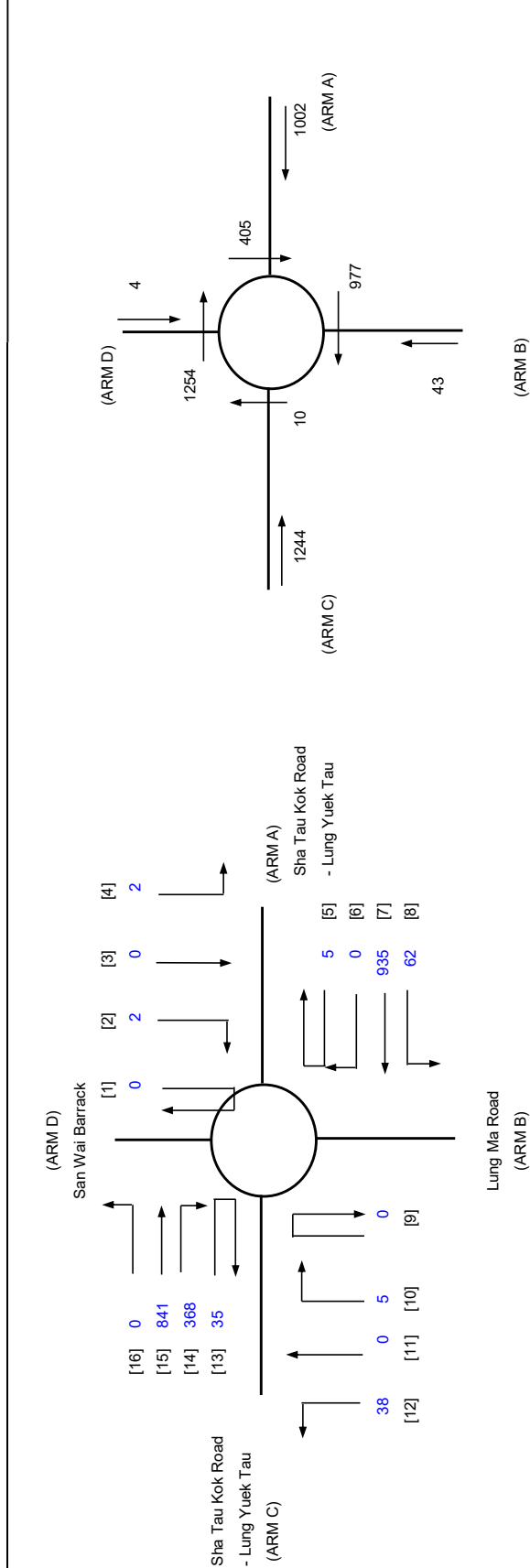
LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	SKL	DATE
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:	SLN	Oct-25
REFERENCE NO.:		REVIEWED BY:	SLN	Oct-25



ARM	A	B	C	D
INPUT PARAMETERS:				
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	1002	43	1244	4
Qc = Circulating flow across entry (pcu/h)	405	977	10	1254
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.32	0.23	0.20	0.09
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.06	1.10	1.01	1.07
X2 = V + ((E-V)/(1+2S))	7.22	4.67	8.81	4.62
M = EXP((D-60)/10)	0.50	0.50	0.50	0.50
F = 303*X2	2188	1415	2669	1401
Td = 1+(0.5/(1+M))	1.33	1.33	1.33	1.33
Fc = 0.21*Td(1+0.2*X2)	0.68	0.54	0.77	0.54
Qe = K(F-Fc*Qc)	2032	978	2689	774
Total In Sum =				1375 PCU
DFC = Design flow/Capacity = Q/Qe	0.49	0.04	0.46	0.01
DFC of Critical Approach =				0.49

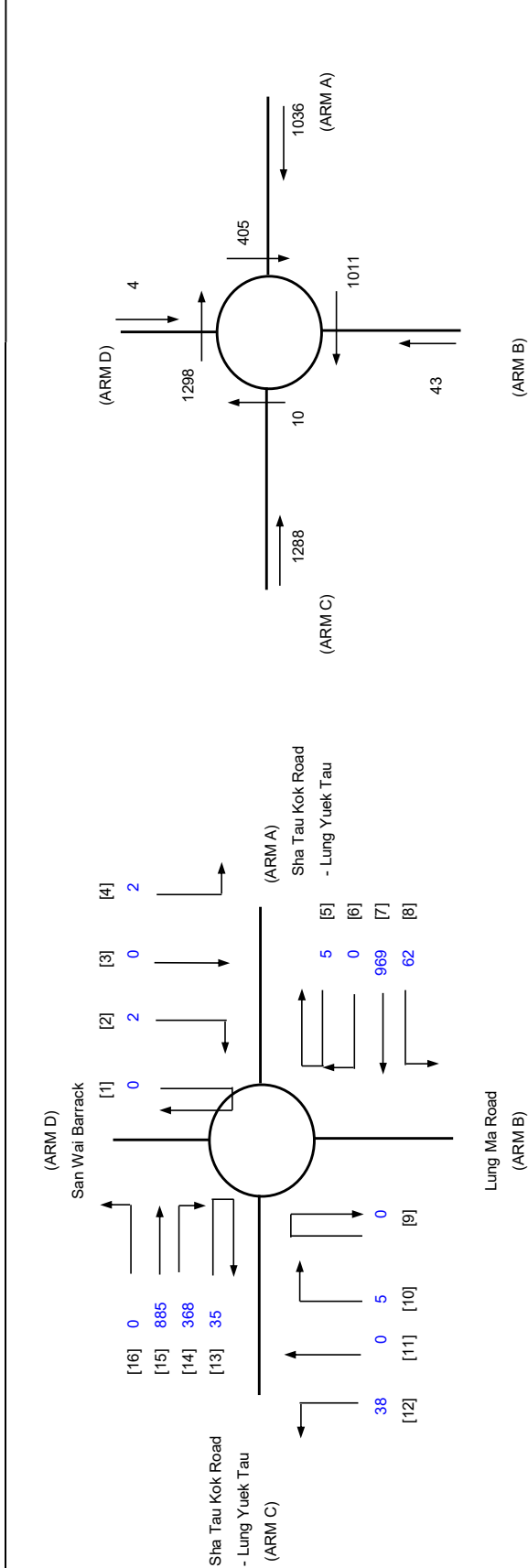
LLA CONSULTANCY LIMITED

Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yuek Tau, Fanling, N.T.

J1 Sha Tau Kok Road - Lung Yuek Tau / Lung Ma Road

ROUNDBABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	SKL	DATE
FILENAME :	J1_STKR_LMR.xl	CHECKED BY:	SLN	Oct-25
REFERENCE NO.:		REVIEWED BY:	SLN	Oct-25



ARM	A	B	C	D
INPUT PARAMETERS:				
V = Approach half width (m)	7.10	3.50	7.10	4.20
E = Entry width (m)	7.30	5.20	9.50	4.70
L = Effective length of flare (m)	1.00	12.00	19.00	9.00
R = Entry radius (m)	58.00	69.00	31.00	42.00
D = Inscribed circle diameter (m)	53.00	53.00	53.00	53.00
A = Entry angle (degree)	21.00	10.00	32.00	18.00
Q = Entry flow (pcu/h)	1036	43	1288	4
Qc = Circulating flow across entry (pcu/h)	405	1011	10	1298
OUTPUT PARAMETERS:				
S = Sharpness of flare = 1.6(E-V)/L	0.32	0.23	0.20	0.09
K = 1-0.00347(A-30)-0.978(1/R-0.05)	1.06	1.10	1.01	1.07
X2 = V + ((E-V)/(1+2S))	7.22	4.67	8.81	4.62
M = EXP((D-60)/10)	0.50	0.50	0.50	0.50
F = 303*X2	2188	1415	2669	1401
Td = 1+(0.5/(1+M))	1.33	1.33	1.33	1.33
Fc = 0.21*Td(1+0.2*X2)	0.68	0.54	0.77	0.54
Qe = K(F-Fc*Qc)	2032	957	2689	748
DFC = Design flow/Capacity = Q/Qe	0.51	0.04	0.48	0.01
Total In Sum =				1409 PCU
DFC of Critical Approach =				0.51

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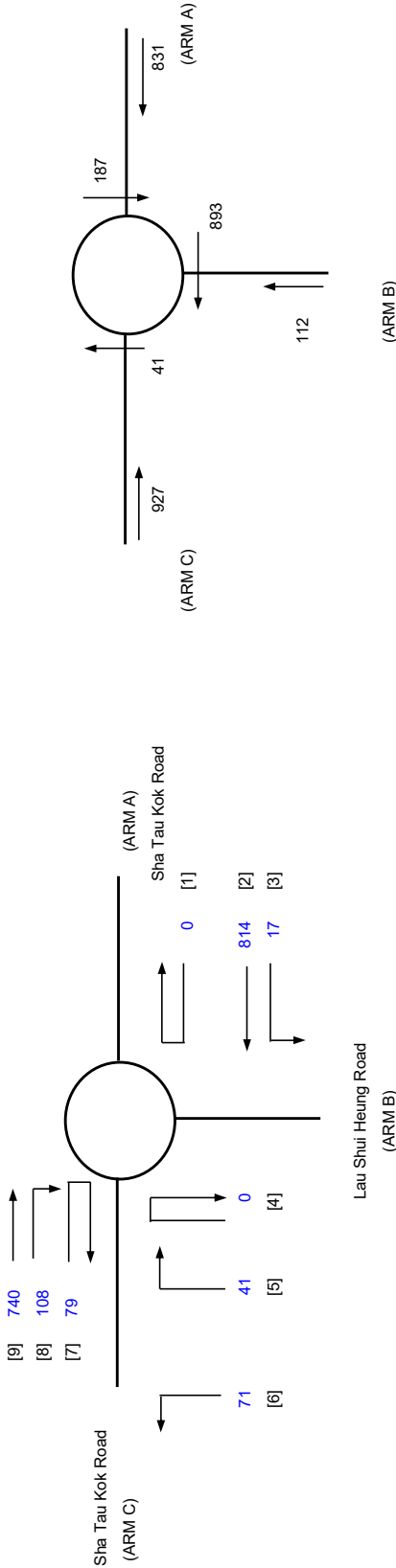
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDBABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	SKL	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SLN	Oct-25
REFERENCE NO.:		REVIEWED BY:	SLN	Oct-25

2030 Reference 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	831	112	927
Qc = Circulating flow across entry (pcu/h)	187	893	41

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.16	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.92	0.93	1.09
X2 = $V + ((E-V)/(1+2S))$	6.88	4.61	6.48
M = $EXP((D-60)/10)$	0.45	0.45	0.45
F = $303 \times X2$	2083	1398	1962
Td = $1+(0.5/(1+M))$	1.34	1.34	1.34
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.67	0.54	0.65
Qe = $K(F-Fc \times Qc)$	1801	848	2119

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.46

Total In Sum =

1870 PCU

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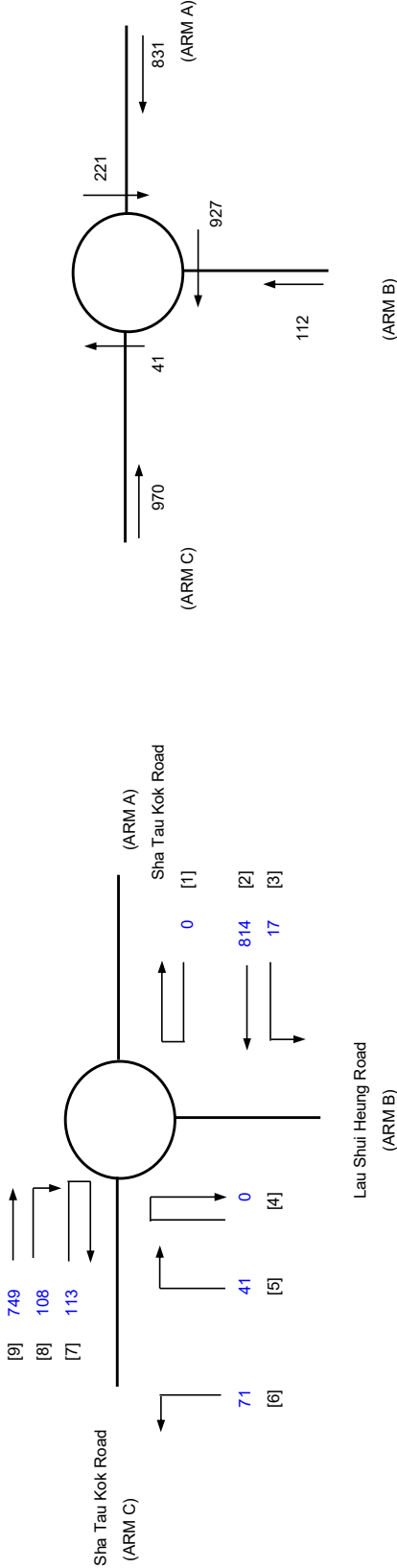
Proposed Temporary Electric Vehicle Charging Station (for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Site Office for a Period of 5 Years at Taxford Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling.

J2 Sha Tau Kok Road - Ma Mei Ha / Lau Shui Heung Road

ROUNDAABOUT CALCULATION

PROJECT NO.:	40988	PREPARED BY:	SKL	DATE
FILENAME :	J2_STKR_LSHR.x	CHECKED BY:	SLN	Oct-25
REFERENCE NO.:		REVIEWED BY:	SLN	Oct-25

2030 Design 1630-1730



ARM

INPUT PARAMETERS:

	A	B	C
V = Approach half width (m)	6.80	3.40	6.40
E = Entry width (m)	6.90	5.20	6.50
L = Effective length of flare (m)	1.00	11.90	1.00
R = Entry radius (m)	10.00	60.00	22.00
D = Inscribed circle diameter (m)	52.00	52.00	52.00
A = Entry angle (degree)	39.00	60.00	4.00
Q = Entry flow (pcu/h)	831	112	970
Qc = Circulating flow across entry (pcu/h)	221	927	41

OUTPUT PARAMETERS:

S = Sharpness of flare = $1.6(E-V)/L$	0.16	0.24	0.16
K = $1-0.00347(A-30)-0.978(1/R-0.05)$	0.92	0.93	1.09
X2 = $V + ((E-V)/(1+2S))$	6.88	4.61	6.48
M = $EXP((D-60)/10)$	0.45	0.45	0.45
F = $303 \times X2$	2083	1398	1962
Td = $1+(0.5/(1+M))$	1.34	1.34	1.34
Fc = $0.21 \times Td(1+0.2 \times X2)$	0.67	0.54	0.65
Qe = $K(F-Fc \times Qc)$	1780	830	2119

DFC = Design flow/Capacity = Q/Qe

DFC of Critical Approach = 0.47

Total In Sum = 1913 PCU



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Appendix Ic of RNTPC
Paper No. A/NE-TKL/813A

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19811231

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FRICS FHKIS MSISV MCIREA ACIarb R.P.S.(GP)

李霧儀
Lee Mo Yi
MPIA RPP MUDD BA (Hons)

吳恆廣
Ng Hang Kwong, BBS
FRICS FHKIS R.P.S.(GP)
Honorary World Valuer (WAVO)

林桂金
Daniel K.K. Lam
MRICS MHKIS MCIREA R.P.S.(GP) BSc

宋樹鴻
Sung Shu Hung
FRICS MHKIS R.P.S.(GP)(PD) MCIREA
MHIREA BSc (Hons)

趙慧姿
Chiu Wai Chi
MRICS MHKIS MSc BBus (Mktg)

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MHKIE MSOE FCI MCIarb MSc

劉志光
Lau Chi Kwong
FRICS FHKIS ALS MHKIS
R.P.S.(LS)(PFM) MSc
潘孝維
Pun How Wai
B.Arch.HK RIBA

By Email and Post

Date : 3 December 2025
Your Ref.: TPB/A/NE-TKL/813
Our Ref. : LDS/PLAN/7139

Secretary
Town Planning Board
15/F, North Point Government Offices
333 Java Road, North Point, Hong Kong

Dear Sir/Madam,

Application for Planning Permission for Proposed Temporary Green Fuel Station (Electric Vehicle Charging Station for Electric Taxi), Vehicle Repair Workshop, Shop and Services (Motor Vehicle Showroom), Eating Place with Ancillary Office for a Period of 5 Years at Taxlord Lot 464 S.A RP (Part) in D.D. 83 and Adjoining Government Land, Sha Tau Kok Road – Lung Yeuk Tau, Fanling, N.T.

(Application No. A/NE-TKL/813)

We refer to the comments from Transport Department and the public on the captioned application and submit herewith our response in **Table 1** for your consideration.

Should there be any queries, please contact our Ms. Cannis Lee at [REDACTED]

Yours faithfully,
For and on behalf of
Lawson David & Sung Surveyors Limited



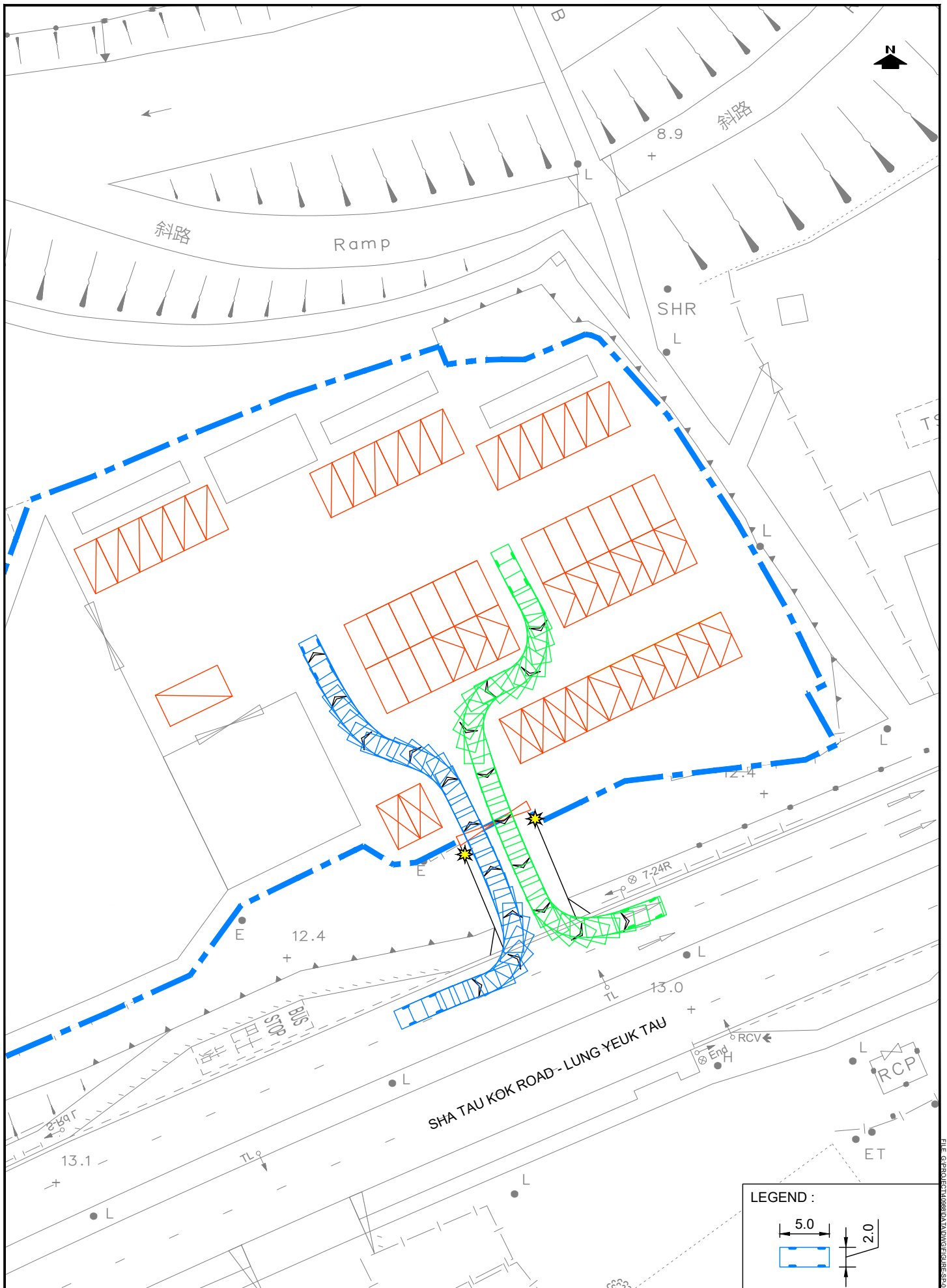

Encl.

c.c. DPO/STN (Attn.: Ms. Sheren Lee) – By Email
Client

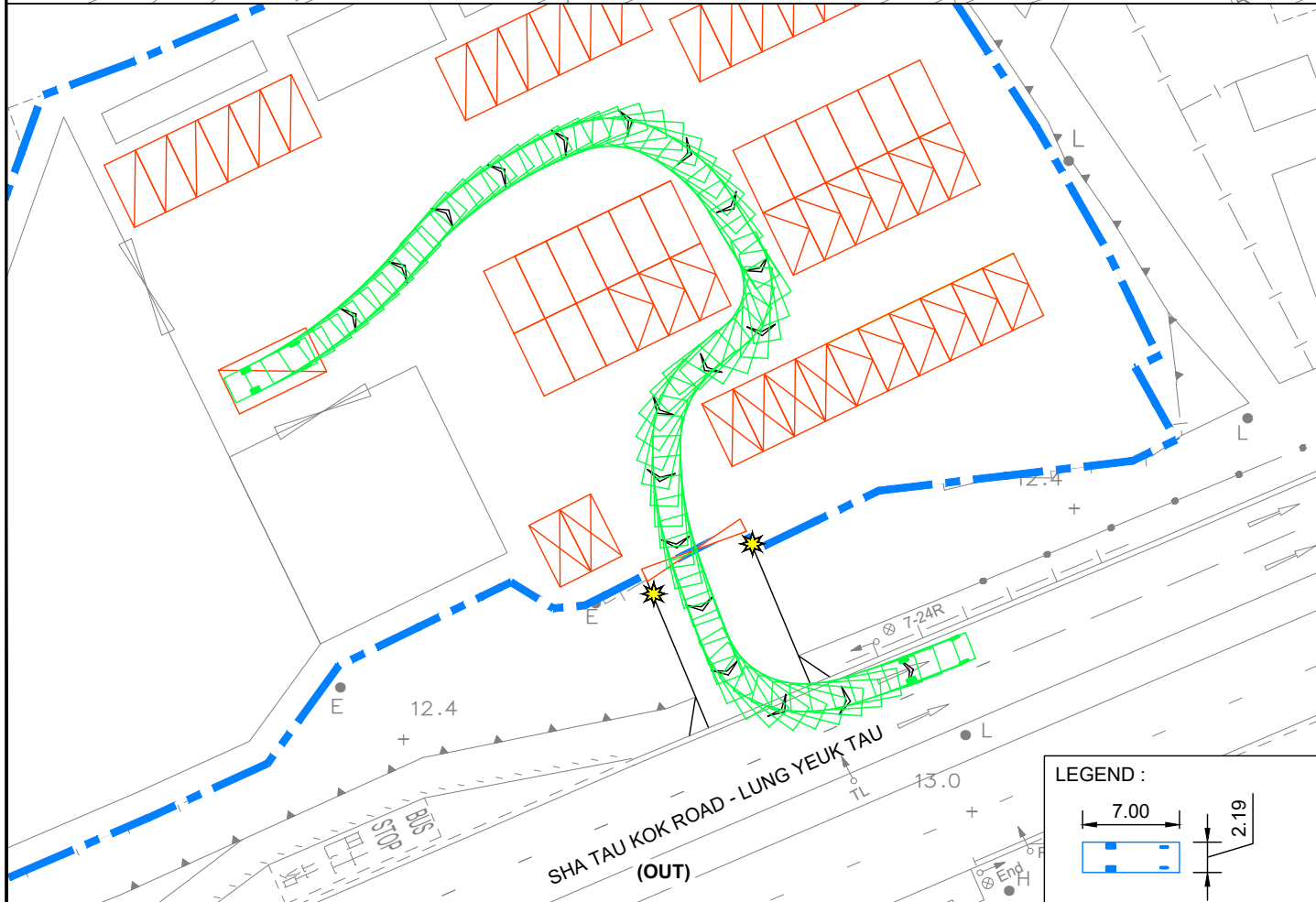
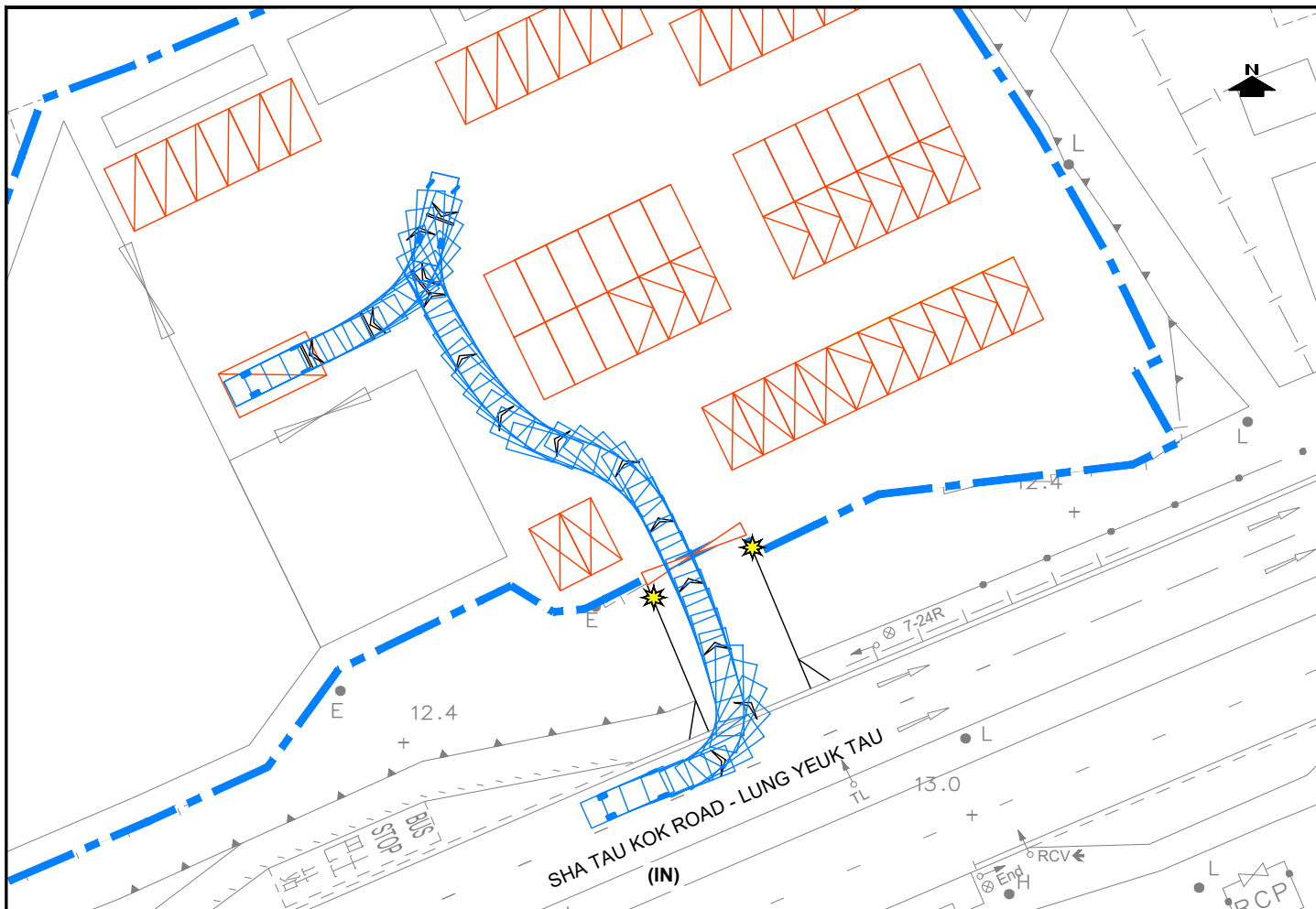
Table 1: Responses to Comments from Government Departments on Planning Application No. A/NE-TKL/813 (3 December, 2025)

COMMENTS	RESPONSES
1. Transport Department (TD)	
(a) Drawing No. FIGURE 4.2: The total entry traffic flows on Sha Tau Kok Road-Lung Yeuk Tau southbound arms in both junctions do not tally with those in the previous version of TIA. Please review.	(a) Please note that the development traffic of a nearby planned development (Planning Application No. A/NE-LYT/860) was incorporated into the updated traffic impact assessment report. The traffic flows of the planned development were distributed onto the road network in the reference and design scenarios which lead to an increase in the total entry traffic flows at the concerned location.
(b) Drawing No. FIGURE 4.3: The total entry traffic flow on Sha Tau Kok Road-Lung Yeuk Tau southbound arm in the junction of Sha Tau Kok Road-Lung Yeuk Tau/Lau Shui Heung Road does not tally with that in the previous version of TIA. Please review.	(b) Please note that the development traffic of a nearby planned development (Planning Application No. A/NE-LYT/860) was incorporated into the updated traffic impact assessment report. The traffic flows of the planned development were distributed onto the road network in the reference and design scenarios which lead to an increase in the total entry traffic flows at the concerned location.
(c) Drawing Nos. SP-01 and SP-02: Vehicles shall not encroach onto the lateral side of the proposed run-in/out. Please revise the swept paths.	(c) Please note that the swept paths were revised and please refer to the attached Figures (Drawing Nos. SP-01 and SP-02) for details.
2. Public Comments	
(a) The concern on air quality arisen from the proposed use and the potential noise and light nuisance caused by the traffic induced by the proposed use.	(a) <u>Concerns on Air Quality</u> Using electric vehicles (EVs) instead of petrol vehicles significantly reduces emissions and improves air quality. With the adoption of advanced technology for EV charging devices, the temperature generated by the devices will not exceed 60°C, and small cooling fans will be used to ventilate the produced heat during operation. These fans will be placed inside the containers, and the fluid-dynamic design of

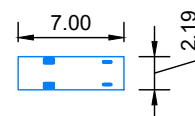
COMMENTS	RESPONSES
	<p>the cooling fans will reduce operating noise. The Applicant will also provide a good air circulation system at the Application Site to prevent air pollution. Since the Application Site is hard-paved, limited dust nuisance is anticipated from vehicles manoeuvring during the proposed development.</p> <p><u>Concerns on Potential Noise and Light Nuisance</u></p> <p>The number of vehicles visiting the Application Site can be controlled, as only 18 charging spaces for electric taxis will be provided. The EV charging station at the Application Site will be open 24 hours a day, 7 days a week (including public holidays), while other supporting facilities will be open from 7 am to 8 pm daily. Therefore, the potential noise impact at night is minimal. Other supporting facilities are located within structures and no noise impact is anticipated.</p> <p>The Application Site is located within the “Open Storage” (“OS”) zone. Operating the proposed EV charging station is similar to running a public car park, which is always permitted in the “OS” zone. The potential noise and light nuisance generated by the Application Site is expected to be insignificant.</p> <p>The lighting system is primarily intended to illuminate the Application Site. During the evening and night, it provides basic lighting for electric taxis only and does not cause light pollution issues for the surrounding environment.</p>



PROJECT NO. 40988		PROJECT TITLE PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXI LOR LOT 464 S A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.		DRAWING NO. SP-01	REV. B
DESIGNED SLN	DATE JUL 2025	DRAWING TITLE SWEPT PATH ANALYSIS - PC			LLA 顧問有限公司 Consultancy Limited
DRAWN CLL	SCALE 1:500				
CHECKED SLN					



LEGEND :



PROJECT NO.	40988
DESIGNED	SLN
DRAWN	CLL
CHECKED	SLN
DATE	SEP 2025
SCALE	1:500

PROJECT TITLE	PROPOSED TEMPORARY ELECTRIC VEHICLE CHARGING STATION (FOR ELECTRIC TAXI), VEHICLE REPAIR WORKSHOP, SHOP AND SERVICES (MOTOR VEHICLE SHOWROOM), EATING PLACE WITH ANCILLARY SITE OFFICE FOR A PERIOD OF 5 YEARS AT TAXILORD LOT 464 S.A RP (PART) IN D.D. 83 AND ADJOINING GOVERNMENT LAND, SHA TAU KOK ROAD - LUNG YEUK TAU, FANLING, N.T.
DRAWING TITLE	SWEPT PATH ANALYSIS - LGV

DRAWING NO.	SP-02	REV.	C
LLA 顧問有限公司 Consultancy Limited			

General Comments from Government Bureau/Departments

1. Traffic

Comments of the Commissioner for Transport (C for T):

- no comment on the application; and
- her advisory comments are at **Appendix III**.

Comments of Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD):

- no comment on the application from highways maintenance point of view; and
- his advisory comment are at **Appendix III**.

2. Drainage

Comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD):

- no objection to the application from the public drainage viewpoint;
- no comment on the submitted drainage proposal;
- should the application be approved, conditions should be included to request the applicant to implement the accepted drainage proposal for the application site (the Site) to ensure that it will not cause adverse drainage impact on the adjacent area, and the implemented drainage facilities at the Site shall be maintained at all times during the planning approval period;
- the Site is in an area where no public sewerage connection is available. Environmental Protection Department should be consulted regarding the sewage treatment/disposal facilities for the proposed uses; and
- her advisory comment are at **Appendix III**.

3. Environment

Comments of the Secretary for Environment and Ecology (SEE):

- it is noted that the application will provide 54 spaces for parking of vehicles, and only 18 spaces will be provided with electric vehicle (EV) charging facilities; and
- to echo with the latest version of Chapter 8 of Hong Kong Planning Standards and Guidelines (HKPSG) about EV charging facilities and to support the Government's policies in promoting the wider adoption of EVs, the applicant is suggested to comply with the relevant requirement of HKPSG, i.e. EV chargers with output power of not less than 7kW (i.e. medium chargers) should be installed in all parking spaces for private cars, light goods vehicles and motorcycles of the Site.

Comments of the Director of Environmental Protection (DEP):

- no objection to the application from the environmental planning perspective since no heavy vehicle or dusty operation is involved;
- from the information provided in the application, the EV charging stations will be housed within semi-enclosed container structures and the Site will be fenced-off with 2.5m tall metal sheets in all sides. Since EV charging facilities normally do not emit noise at high level, with the above set up, he does not envisage adverse noise impact from the EV charging facilities as proposed in the application;
- no substantiated environmental complaints against the Site was received in the past three years; and
- his advisory comment are at **Appendix III**.

4. Nature Conservation

Comments of the Director of Agriculture, Fisheries and Conservation (DAFC):

- no adverse comment on the application; and
- his advisory comment are at **Appendix III**.

5. Fire Safety

Comments of the Director of Fire Services (D of FS):

- no in-principle objection to the application subject to fire service installations and water supplies for firefighting being provided to his satisfaction; and
- his advisory comment are at **Appendix III**.

6. Town Gas Safety

Comments of the Director of Electrical and Mechanical Services (DEMS):

- there are underground intermediate pressure town gas pipes running along Sha Tau Kok Road that are close to the Site;
- if there is any works to be involved in the process, in particular any works involving excavation, the relevant parties shall liaise with The Hong Kong and China Gas Company Limited in respect of the exact locations of existing or planned gas pipes/gas installations in the vicinity of the works site and any required minimum set back distance away from them during any works; and
- his advisory comment are at **Appendix III**.

7. Food and Environmental Hygiene

Comments of the Director of Food and Environmental Hygiene (DFEH):

- no adverse comment on the application; and
- his advisory comment are at **Appendix III**.

8. Water Supplies

Comments of the Chief Engineer/ Construction, Water Supplies Department (CE/C, WSD):

- no objection to the application; and
- his advisory comment are at **Appendix III**.

9. Building Matters

Comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):

- no objection to the application;
- it is noted that six structures are proposed in the application, before any new building works (including containers / open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on Site, prior approval and consent of the Building Authority should be obtained, otherwise they are unauthorised building works under the Buildings Ordinance (BO). An Authorised Person should be appointed as the co-ordinator for the proposed building works in accordance with the BO; and
- his advisory comment are at **Appendix III**.

10. Other Departments

The following government departments have no objection to/no comment on the application:

- (a) Project Manager (North), Civil Engineering and Development Department (PM(N), CEDD);
- (b) Head of Geotechnical Engineering Office, CEDD (H(GEO), CEDD); and
- (c) District Officer (North), Home Affairs Department (DO(N), HAD).

Recommended Advisory Clauses

- (a) to resolve any land issues relating to the proposed uses with the concerned owner(s) of the application site (the Site);
- (b) to resolve any issues relating to the proposed uses with the concerned resident(s) at the Site;
- (c) to note the comments of the District Lands Officer/North, Lands Department (DLO/N, LandsD) that:
 - (i) the Site comprises Old Schedule Agricultural Lot held under the Block Government Lease which contains the restriction that no structures are allowed to be erected without the prior approval of the Government. The proposed ingress/egress of the Site is required to pass through Government land (GL) but no right of access via GL is granted to the Site at the proposed ingress/egress point;
 - (ii) part of the private lot is covered by Short Term Wavier (STW) No. 1226 for the purpose of storage of rattan furniture. Part of GL within the Site is covered by Short Term Tenancy (STT) No. 1108 for the purposes of storage of rattan furniture. The proposed sizes and uses of the structures and GL involved in the Site do not tally with those permitted under the said STW and STT;
 - (iii) the application lot is partly covered by Letter of Approval (LoA) No. 2713 and Modification of Tenancy (MOT) No. 35316 for erection of temporary structures for dwelling, kitchen, pigsty and shed. His office reserves the rights to take enforcement action for any irregularities and cancel the LoA\MoT as appropriate;
 - (iv) the following irregularities covered by the planning application have been detected by his office:
 - unauthorised structures within the said private lot covered by the planning application
there are unauthorised structures within the said private lot which are already subject to lease enforcement actions according to case priority. The lot owners should rectify the lease breaches as demanded by LandsD;
 - unlawful occupation of GL with unauthorised structures covered by the planning application
part of the GL within the Site (about 680m² as mentioned in the applicant's submission) has been illegally occupied with unauthorised structures without any permission. Any occupation of GL without Government's prior approval is an offence under Cap. 28. His office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice;

- (v) the following irregularities not covered by the planning application have been detected by his office:

unauthorised structures within the said private lot not covered by the planning application

there are unauthorised structures within the said private lot not covered by the subject planning application. The lot owners should immediately rectify the lease breaches and his office reserves the rights to take necessary lease enforcement action against the breaches without further notice;

unlawful occupation of GL not covered by the planning application

the GL adjoining the said private lot has been illegally occupied with unauthorised structures without permission. The GL being illegally occupied is not included in the application. Any occupation of GL without Government's prior approval is an offence under Cap. 28. His office reserves the rights to take necessary land control action against the illegal occupation of GL without further notice; and

- (vi) the lot owners shall either (i) remove the unauthorised structures and cease the illegal occupation of the GL not covered by the planning application immediately; or (ii) include the unauthorised structures and the adjoining GL being illegal occupied in the planning application for further consideration by the relevant departments and, subject to the approval of the Town Planning Board to the planning application which shall have reflected the rectification or amendment as aforesaid required, apply to his office for modification/new STW and STT to permit the structures erected/to be erected and the occupation of GL. The applications for modification/new STW and STT will be considered by the Government in its capacity as a landlord and there is no guarantee that they will be approved. Besides, the STW would be processed on whole lot basis. And the STW and the STT, if approved, will be subject to such terms and conditions including the payment of back-dated waiver fee/rent from the first date when the unauthorised structures were erected and the occupation of GL as well as administrative fee as considered appropriate to be imposed by LandsD. In addition, LandsD reserves the right to take enforcement action against the lot owners for any breach of the lease conditions, including the breach(es) already in existence or to be detected at any point of time in future and land control action for any unlawful occupation of GL. Besides, given the proposed uses are temporary in nature, only erection of temporary structure(s) will be considered;

- (d) to note the comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD) that:

- (i) the Site is in an area where no public sewerage connection is available. Environmental Protection Department should be consulted regarding the sewage treatment/disposal facilities for the proposed uses;
- (ii) the applicant should note the following general comments/requirements:

- for proposed U-channel and stepped channel on GL, the applicant should follow the standard drawing and the works shall conform to the requirements stipulated in Stormwater Drainage Manual, DSD Standard Drawings, DSD Technical Circulars, Practice Notes and Guidelines, etc.;
 - the applicant is required to construct and maintain the proposed drainage works properly and rectify the drainage systems if they are found to be inadequate or ineffective during operation. The applicant shall also be liable for and shall indemnify claims and demands arising out of damage or nuisance caused by a failure of the systems. For works undertaken outside the lot boundary, prior consent and agreement from DLO/N, LandsD and/or relevant private lot owners should be sought;
 - the applicant is reminded that all existing flow paths as well as the run-off falling onto and passing through the Site should be intercepted and disposed of via proper discharge points. The applicant shall also ensure that no works, including any site formation works, shall be carried out as may adversely interfere with the free flow condition of the existing drain, channels and watercourses on or in the vicinity of the Site any time during or after the works;
 - the applicant shall take all precautionary measures to prevent any disturbance, damage and pollution from the development to any parts of the existing drainage facilities in the vicinity of the lots. In the event of any damage to the existing drainage facilities, the applicant shall be held responsible for the cost of all necessary repair works, compensation and any other consequences arising there from;
 - the applicant shall allow all time free access for the Government and its agent to conduct site inspection on his completed drainage works, if necessary; and
 - the applicant should also be advised that the limited desk-top checking by Government on the drainage proposal covers only the fundamental aspects of the drainage design which will by no means relieve his obligations to ensure that (i) the proposed drainage works will not cause any adverse drainage or environmental impacts in the vicinity; and (ii) the proposed drainage works and the downstream drainage systems have the adequate capacity and are in good conditions to receive the flows collected from his lot and all upstream catchments;
- (e) to note the comments of the Director of Environmental Protection (DEP) that the applicant should follow the requirements of (i) the ‘Code of Practice on Handling Environmental Aspects of Temporary Uses and Open Storage Sites’; (ii) Professional Persons Environmental Consultative Committee Practice Notes 1/23 ‘Drainage Plans subject to Comment by the Environmental Protection Department, Building (Standards of Sanitary Fitments, Plumbing, Drainage Works and Latrines) Regulations (Cap. 123I) Section 40(1), 40(2), 41(1) and 90’ for the proposed septic tank and soakaway pit system; (iii) ‘Control of Oily Fume and Cooking Odour from Restaurants and Food Business’ for cooking fumes from the eating place; (iv)

‘Grease Traps for Restaurants and Food Processors’; (v) relevant requirements under the Hong Kong Planning Standards and Guidelines when designing the proposed temporary uses to avoid causing environmental impacts on the surroundings; and (vi) other relevant environmental legislation;

- (f) to note the comments of the Commissioner for Transport (C for T) that the vehicular access between the Site and Sha Tau Kok Road is not managed by Transport Department (TD). The applicant should seek comment from the responsible party;
- (g) to note the comments of the Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD) that:
 - (i) the proposed access arrangement, Traffic Impact Assessment and swept path analysis should be commented by TD;
 - (ii) HyD is not/shall not be responsible for the maintenance of any access connecting the Site and Sha Tau Kok Road - Lung Yeuk Tau;
 - (iii) adequate drainage measures should be provided to prevent surface water running from the Site to the nearby public roads and drains; and
 - (iv) the applicant should take adequate precautionary measures to avoid damaging roads, street furniture, drainage and slopes etc. maintained by his office. Damage caused to roads, street furniture, drainage and slopes etc. maintained by his office due to the proposed work shall be repaired to his satisfaction at the applicant’s own costs;
- (h) to note the comments of the Director of Agriculture, Fisheries and Conservation (DAFC) that since the Site is near a night roost of egrets, the applicant should preserve existing trees within the Site as far as possible, avoid disturbing wild birds, and avoid directing lighting toward the night roost. The night roost is located at approximately 22.509670, 114.157288 (GPS coordinates);
- (i) to note the comments of the Director of Electrical and Mechanical Services (DEMS) that:
 - (i) there are underground intermediate pressure town gas pipes running along Sha Tau Kok Road that are close to the Site;
 - (ii) if there is any works to be involved in the process, in particular any works involving excavation, the relevant parties shall liaise with The Hong Kong and China Gas Company Limited in respect of the exact locations of existing or planned gas pipes/gas installations in the vicinity of the works site and any required minimum set back distance away from them during any works; and
 - (iii) the relevant parties are required to observe the Electrical and Mechanical Services Department’s requirements on the “Avoidance of Damage to Gas Pipes 2nd Edition” for reference;

- (j) to note the comments of the Director of Food and Environmental Hygiene (DFEH) that:
- (i) no Food and Environmental Hygiene Department's (FEHD) facilities shall be affected;
 - (ii) proper licence/ permit issued by his Department is required if there is any food business/ catering service/ activities regulated by the DFEH under the Public Health and Municipal Services Ordinance (Cap. 132) and other relevant legislation for the public:
 - under the Food Business Regulation, Cap. 132X, a food business licence is required for the operation of the relevant type of food business listed in the Regulation. For any premises intended to be used for food business (e.g. a restaurant, a food factory, a fresh provision shop), a food business licence from the FEHD in accordance with the Public Health and Municipal Services Ordinance (Cap. 132) shall be obtained. The application for licence, if acceptable by the FEHD, will be referred to relevant government departments such as the Buildings Department (BD), Fire Services Department and Planning Department for comment. If there is no objection from the departments concerned, a letter of requirements will be issued to the applicant for compliance and the licence will be issued upon compliance of all the requirements;
 - depending on the mode of operation, generally there are several types of food business licence/permits that the operator may apply for under the Food Business Regulation:
 - (i) if food is sold to customers for consumption on the premises, a restaurant licence should be obtained;
 - (ii) if food is only prepared for sale for consumption off the premises, a food factory licence should be obtained;
 - (iii) if fresh, chilled or frozen beef, mutton, pork, reptiles (including live snake), fish (including live fish) and poultry is sold, a fresh provision shop licence should be obtained;
 - (iv) if restricted foods like milk, frozen confections, non-bottled drinks, cut fruit etc. are to be sold, relevant individual restricted food permit(s) or a composite restricted foods permit should be obtained;
 - when choosing a premises for food business licences, the applicant must ensure that the operation of food business at the subject premises is in compliance with the requirements imposed under the legislation administered by his Department, other government departments and the relevant authorities. Applicants are strongly advised to check well in advance the following documents: (a) the Government Lease, (b) the Occupation Permit of the building, and (c) the statutory plan. No part of a food premises shall be located in, under or over any structures built without the approval and consent of the Building Authority (BA);

- (iii) proper licence issued by his Department is required if related place of entertainment is involved. Any person who desires to keep or use any place of public entertainment for example a theatre and cinema or a place, building, erection or structure, whether temporary or permanent, on one occasion or more, capable of accommodating the public presenting or carrying on public entertainment within Places of Public Entertainment (PPE) Ordinance (Cap. 172) and its subsidiary legislation, such as a concert, opera, ballet, stage performance or other musical, dramatic or theatrical entertainment, cinematograph or laser projection display, a circus, a lecture or story-telling, an exhibition of any 1 or more of the following, namely, pictures, photographs, books, manuscripts or other documents or other things, a sporting exhibition or contest, a bazaar, a dance party or an amusement ride and mechanical device which is designed for amusement, a PPE Licence (or Temporary PPE Licence) should be obtained from FEHD whatever the general public is admitted with or without payment; and
 - (iv) there should be no encroachment on the public place and no environmental nuisance should be generated to the surroundings. Its state should not be a nuisance or injurious or dangerous to health and surrounding environment. Also, for any waste generated from such activities/ operation, the applicant should arrange disposal properly at their own expenses.
- (k) to note the comments of the Chief Engineer/ Construction, Water Supplies Department (CE/C, WSD) that:
- (i) existing water mains inside the Site as shown on the Mains Record Plan (**Attachment**) may be affected. The applicant is required to either divert or protect the water mains found on site;
 - (ii) if diversion is required, existing water mains inside the Site are needed to be diverted outside the site boundary of the Site to lie in GL. A strip of land of minimum 1.5m in width should be provided for the diversion of existing water mains. The cost of diversion of existing water mains upon request will have to be borne by the applicant; and the applicant shall submit all the relevant proposal to WSD for consideration and agreement before the works commence;
 - (iii) if diversion is not required, the following conditions shall apply:
 - existing water mains are affected as indicated on the site plan and no development which requires resiting of water mains will be allowed;
 - details of site formation works shall be submitted to the Director of Water Supplies (D of WS) for approval prior to commencement of works;
 - no structures shall be built or materials stored within 1.5m from the centre line(s) of water main(s). Free access shall be made available at all times for staff of the D of

WS or their contractor to carry out construction, inspection, operation, maintenance and repair works;

- no trees or shrubs with penetrating roots may be planted within the Water Works Reserve or in the vicinity of the water main(s). No change of existing site condition may be undertaken within the aforesaid area without the prior agreement of the D of WS. Rigid root barriers may be required if the clear distance between the proposed tree and the pipe is 2.5m or less, and the barrier must extend below the invert level of the pipe;
- no planting or obstruction of any kind except turfing shall be permitted within the space of 1.5m around the cover of any valve or within a distance of 1m from any hydrant outlet; and
- tree planting may be prohibited in the event that the D of WS considers that there is any likelihood of damage being caused to water mains;

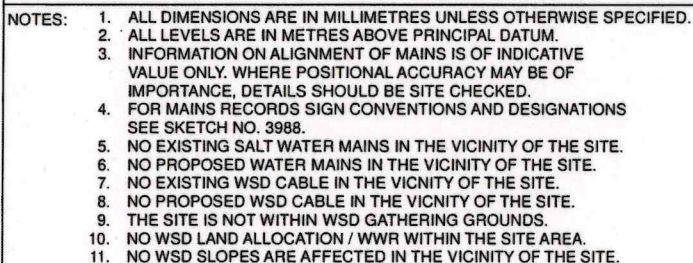
(l) note the comments of the Director of Fire Services (D of FS) that:

- (i) in consideration of the design/nature of the proposal, fire service installations (FSIs) are anticipated to be required. The applicant is advised to submit relevant layout plans incorporated with the proposed FSIs to his department for approval;
- (ii) the applicant should be advised that the layout plans should be drawn to scale and depicted with dimensions and nature of occupancy and the location of the proposed FSIs to be installed should be clearly marked on the layout plans; and
- (iii) if the proposed structures are required to comply with the Buildings Ordinance (BO) (Cap. 123), detailed fire service requirements will be formulated upon receipt of formal submission of general building plans or upon referral from the relevant licensing authority;

(m) to note the comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD) that:

- (i) it is noted that six structures are proposed in the application, before any new building works (including containers / open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on Site, prior approval and consent of the Building Authority should be obtained, otherwise they are unauthorized building works (UBW) under the BO. An Authorised Person should be appointed as the co-ordinator for the proposed building works in accordance with the BO;
- (ii) the applicant's attention is drawn to the following points:
 - the Site shall be provided with means of obtaining access thereto from a street and emergency vehicular access in accordance with Regulations 5 and 41D of the Building (Planning) Regulations (B(P)R) respectively;

- if the Site does not abut on a specified street of not less than 4.5m wide and its permitted development intensity shall be determined under Regulation 19(3) of the B(P)R at building plan submission stage;
 - for UBW erected on leased land, enforcement action may be taken by the BD to effect their removal in accordance with the prevailing enforcement policy against UBW as and when necessary. The granting of any planning approval should not be construed as an acceptance of any existing building works or UBW on the Site under the BO;
 - any temporary shelters or converted containers for office, storage, washroom or other uses are considered as temporary buildings are subject to the control of Part VII of the B(P)R;
 - the applicant's attention is also drawn to the provision under Regulations 40 and 41 of the Building (Standards of Sanitary Fitments, Plumbing, Drainage Works and Latrines) Regulations in respect of disposal of foul water and surface water respectively;
 - the headroom (5m) of structure no. 1 (i.e. Motor Vehicle Showroom) is considered excessive. It should be justified upon formal plan submission to BD; and
 - detailed checking under the BO will be carried out at building plan submission stage; and
- (n) to note the comments of the Project Manager (North), Civil Engineering and Development Department (PM(N), CEDD) that the proposed uses are located within the proposed New Territories North (NTN) New Town under the Planning and Engineering (P&E) Study for NTN New Town and Man Kam To. The preliminary development proposal for NTN New Town was released in December 2024. While the implementation programme of NTN New Town is being formulated under the P&E Study, the site formation works will likely commence soon after the completion of detailed design in next stage. Hence, subject to the land use planning in the P&E Study, the proposed uses may need to be vacated for the site formation works.



W67880/3-SW-3D & 4C

FILE REF: (5) IN WSD/M/SP 3051/437/80S/98 PT.1

REF. CODE: 35W25M

SHEET 1 OF 1

SCALE 1:1200



水 務 署
Water Supplies Department

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

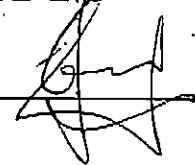
有關的規劃申請編號 The application no. to which the comment relates
A/NE-TKL/813

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

「提意見人」姓名/名稱 Name of person/company making this comment 侯志強議員

簽署 Signature



日期 Date

2025.8.26

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

250907-100619-57241

提交限期

Deadline for submission:

12/09/2025

提交日期及時間

Date and time of submission:

07/09/2025 10:06:19

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TKL/813

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. Mr. Chan

意見詳情

Details of the Comment :

本人就有關規劃申請A/NE-TKL/813提出反對意見理據如下懇請常規會仔細考慮及傳閱予有關政府部門, 審慎考慮有關規劃申請及增加附加條款,保障公眾安全及利益:

1, 環境評估 / 空氣流通評估

A, 雖然電動車對空氣污染影響遠低於燃油車, 但快充站直流電源櫃冷卻風扇會揚起輪胎、煞車及塵土微粒, 造成空氣污染問題。加州大學洛杉磯分校研究團隊在洛杉磯縣 50 個快充站進行測試, 發現微粒濃度比一般城市環境高出一倍, 近半數充電站超出世衛空氣品質標準。申請人的規劃資料並沒有提到有關影響的緩解措施減低對附近社區的影響

B, 有關申請人建議興建的充電設施比對現時市場上同類型裝置運作時會排出大量廢熱空氣, 發出低頻噪音以及風扇運作噪音會造成滋擾影響附近人士。申請人的規劃資料並沒有提到有關影響的緩解措施減低附近社區的影響

C, 有關建議興建的充電設施有關充電設施運作時大量車輛進出車流會構成噪音以及燈光滋擾附近社區人士。申請人的規劃資料並沒有提到有關影響的緩解措施減低附近社區的影響

D, 申請人提到有關設施將會附屬汽車修理工場, 眾所周知汽車修理工場於日常運作會排出各類型化學廢料及污染物申請人並未有提到該等污染處理物如何妥善處理以及防止污染週圍的環境

2, 交通影響評估 (車輛/行人)

A) 有關建議興建的充電設施有關充電設施運作時大量車輛進出車流, 有關充電設施並有指明是否供特定車隊使用懷疑會用作開放與公眾使用, 有關充電設施會有機會造成大量人流及車流, 特別是有關設施的等候位置使人懷疑會否導致有關使用設施的人士罔顧營運者的使用指引於有關設施的閘外排隊造成車龍及擠塞而又有關營運者又未能處理此等於閘外等候的交通擠塞問題

3, 排水及排污影響評估

A, 有關充電設施並有指明是否供特定車隊使用懷疑會用作開放與公眾使用, 有關人士使用設施時會有機會造成大量人流及車流, 而申請人規劃文件中並有提到會否有關建立廁所等設施另外有關排水的報告中只描述咗填土後帶來的雨水收集情況並有提及有關廁所排污問題, 就典型規劃申請中指出的化糞池亦都似乎難以令人信服可以處理因充電設施而引致大量的人流所造成排污問題

B, 另外申請人提到有關設施將會附屬食肆, 有關食肆運作帶來的污水更加似乎難以使用普通化糞池作出恰當處理造成排污問題

4, 消防風險評估

A, 申請人提到有關設施將會附屬汽車修理工場, 根據機電工程署的指引以及編制的電動車

維修工作指引,有關維修電動車的工場必須為合資格嘅註冊車輛維修工場並具有關消防設施場地配置等特定要求,申請人有指出有關工場將會進行什麼類型的工作,根據相關電動車維修服務嘅危險程度相應嘅消防設施亦有唔同,就申請人提出嘅簡單陳述難以理解佢嘅消防配套可以進行如此複雜及高風險嘅電動車維修工作,另外電動車維修工場屬於工業設施根據消防處嘅指引提出嘅消防安全設施配套並未能於申請人提供的資料中找到:

低層工業/貨倉建築物 須裝設的系統/裝置/設備: (i) (ii) (iii) (iv) (v) (vi) 自動啟動裝置 不含水的滅火劑自動固定裝置 應急發電機 應急照明系統 出口指示牌 火警警報系統 (vii) 消防控制中心 (viii) 火警偵測系統 (ix) 消防栓/喉轆系統 (x) (xi) 消防員升降機或消防和救援樓梯間 認可的人手操作手提器具 (xii) 花灑系統 (xiii) 靜態式或機械式排煙系統 (xiv) 通風/空調控制系統

B, 據地政總署審批同類型規劃發展的短期租約指引其中消防處提出一般的消防規定,本人研究有關規劃申請的資料似乎有欠周詳,有關申請人提出的消防設施與及批核圖並沒有包含到有關申請人提出的換電設施根據消防處提出嘅指引[有關換電設施受消防處出版‘最低限度之消防裝置及設備守則 與 裝置及設備之檢查、測試及保養守則’的 電池房和充電設施

須裝設的系統/裝置/設備:

- (i) 不含水的滅火劑自動固定裝置
- (ii) 出口指示牌
- (iii) 火警警報系統
- (iv) 火警偵測系統
- (v) 氣體排放系統
- (vi) 認可的人手操作手提器具
- (vii) 通風/空調控制系統

申請人的規劃資料並沒有提到有關影響嘅緩解消防風險措施減低附近社區嘅影響

B, 眾所週知當電動車發生火警時需要使用大量水源撲滅,有關申請人提供的規劃資料並無指示標示有關發展項目的100米範圍內是否設有消防龍頭,根據規劃署出版的香港規劃標準及準則相類型的電動車充電站需要以加油站標準設計有關消防設施,如申請人的規劃申請人面亦都有提出建造相關適當容量的消防水缸消防泵以及應急電源等設施令人難以信服數量眾多嘅電動車喺此等設施發生火警時有關消防裝置能否應付以及阻止火勢蔓延危及社區安全

From:

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Subject:

反對規劃申請 A/NE-TKL/813

致城市規劃委員會：

本人反對規劃申請 A/NE-TKL/813 理據如下：

1：在申請地點附近是村民必經之地，經常出入，現址已有小型維修車行，如再增加大型停車場，另至增加車輛使用通路，造成人車爭路，增加意外禍及村民生命危險。

2.破壞周邊生態環境。

3.破壞住在上址村民的家，失去居所。

希望委員會能夠慎重考慮規劃此用地，顧及軍地居民的安全及日常生活。期待收到你的答覆，謝謝。

謹啟

居民梁小姐

致城市規劃委員會

本人反對規劃申請 A/NE-TKL/813 改變
土地用途 理據如下 =

1. 上址是村民經常出入，上落車必經之地，現址已有小型維修車行，如再增加大型停車場，造成人車爭路，增加意外發生，禍害村民，有生命之危。
2. 破壞周邊生態環境。
3. 破壞住在上址村民家園，失去溫暖家庭。



村民：

梁玉玲

2025年9月6日。