

2024年 7月 2 6日

此文件在 收到・城市規劃委員會
只會在收到所有必要的資料及文件後才正式確認收到
申請的日期。

26 JUL 2024

This document is received on
The Town Planning Board will formally acknowledge
the date of receipt of the application only upon receipt
of all the required information and documents.

Appendix I of RNTPC
Paper No. A/NE-TK/800B

Form No. S16-III
表格第 S16-III 號

APPLICATION FOR PERMISSION UNDER SECTION 16 OF THE TOWN PLANNING ORDINANCE (CAP. 131)

根據《城市規劃條例》(第131章)
第16條遞交的許可申請

Applicable to Proposal Only Involving Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas, or Renewal of Permission for such Temporary Use or Development*

適用於祇涉及位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展或該等臨時用途/發展的許可續期的建議*

**Form No. S16-I should be used for other Temporary Use/Development of Land and/or Building (e.g. temporary use/developments in the Urban Area) and Renewal of Permission for such Temporary Use or Development.*

**其他土地上及/或建築物內的臨時用途/發展(例如位於市區內的臨時用途或發展)及有關該等臨時用途/發展的許可續期，應使用表格第S16-I號。*

Applicant who would like to publish the notice of application in local newspapers to meet one of the Town Planning Board's requirements of taking reasonable steps to obtain consent of or give notification to the current land owner, please refer to the following link regarding publishing the notice in the designated newspapers:
https://www.tpb.gov.hk/en/plan_application/apply.html

申請人如欲在本地報章刊登申請通知，以採取城市規劃委員會就取得現行土地擁有人的同意或通知現行土地擁有人所指定的其中一項合理步驟，請瀏覽以下網址有關在指定的報章刊登通知：
https://www.tpb.gov.hk/tc/plan_application/apply.html

General Note and Annotation for the Form

填寫表格的一般指引及註解

"Current land owner" means any person whose name is registered in the Land Registry as that of an owner of the land to which the application relates, as at 6 weeks before the application is made

「現行土地擁有人」指在提出申請前六星期，其姓名或名稱已在土地註冊處註冊為該申請所關乎的土地的擁有人的人

& Please attach documentary proof 請夾附證明文件

^ Please insert number where appropriate 請在適當地方註明編號

Please fill "NA" for inapplicable item 請在不適用的項目填寫「不適用」

Please use separate sheets if the space provided is insufficient 如所提供的空間不足，請另頁說明

Please insert a 「✓」 at the appropriate box 請在適當的方格內上加上「✓」號

2401657

10/7

By hand

Form No. S16-III 表格第 S16-III 號

For Official Use Only 請勿填寫此欄	Application No. 申請編號	A/NE-TK/800
	Date Received 收到日期	26 JUL 2024

- The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.
申請人須把填妥的申請表格及其他支持申請的文件(倘有),送交香港北角渣華道333號北角政府合署15樓城市規劃委員會(下稱「委員會」)秘書收。
- Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.tpb.gov.hk/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).
請先細閱《申請須知》的資料單張,然後填寫此表格。該份文件可從委員會的網頁下載(網址:<http://www.tpb.gov.hk/>),亦可向委員會秘書處(香港北角渣華道333號北角政府合署15樓-電話:2231 4810或2231 4835)及規劃署的規劃資料查詢處(熱線:2231 5000)(香港北角渣華道333號北角政府合署17樓及新界沙田上禾輦路1號沙田政府合署14樓)索取。
- This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.
此表格可從委員會的網頁下載,亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全,委員會可拒絕處理有關申請。

1. Name of Applicant 申請人姓名/名稱

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☒ Ms. 女士 / ☐ Company 公司 / ☐ Organisation 機構)

CHEUNG SUK FAN FENNIE

2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱 (如適用)

(☐ Mr. 先生 / ☐ Mrs. 夫人 / ☐ Miss 小姐 / ☐ Ms. 女士 / ☐ Company 公司 / ☐ Organisation 機構)

3. Application Site 申請地點

(a) Full address / location / demarcation district and lot number (if applicable) 詳細地址/地點/丈量約份及地段號碼(如適用)	Various Lots In D.D.26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T. 地段號碼請看附件一
(b) Site area and/or gross floor area involved 涉及的地盤面積及/或總樓面面積	<input checked="" type="checkbox"/> Site area 地盤面積 1,525 sq.m 平方米 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Gross floor area 總樓面面積 sq.m 平方米 <input type="checkbox"/> About 約
(c) Area of Government land included (if any) 所包括的政府土地面積(倘有)	不適用 sq.m 平方米 <input type="checkbox"/> About 約

(d) Name and number of the related statutory plan(s) 有關法定圖則的名稱及編號	S/NE-TK/19汀角分區計劃大綱核准圖
(e) Land use zone(s) involved 涉及的土地用途地帶	GB, V, ROAD
(f) Current use(s) 現時用途	私人泊車 / 行車通道 (If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)

4. "Current Land Owner" of Application Site 申請地點的「現行土地擁有人」

The applicant 申請人 -

- ☐ is the sole "current land owner"^{#&} (please proceed to Part 6 and attach documentary proof of ownership).
是唯一的「現行土地擁有人」^{#&} (請繼續填寫第 6 部分，並夾附業權證明文件)。
- ☒ is one of the "current land owners"^{#&} (please attach documentary proof of ownership).
是其中一名「現行土地擁有人」^{#&} (請夾附業權證明文件)。
- ☐ is not a "current land owner"[#].
並不是「現行土地擁有人」[#]。
- ☐ The application site is entirely on Government land (please proceed to Part 6).
申請地點完全位於政府土地上 (請繼續填寫第 6 部分)。

5. Statement on Owner's Consent/Notification 就土地擁有人的同意/通知土地擁有人的陳述

- (a) According to the record(s) of the Land Registry as at (DD/MM/YYYY), this application involves a total of "current land owner(s)"[#].
根據土地註冊處截至 2024 年 7 月 8 日的記錄，這宗申請共牽涉 33 名「現行土地擁有人」[#]。

(b) The applicant 申請人 -

- ☒ has obtained consent(s) of "current land owner(s)"[#].
已取得 33 名「現行土地擁有人」[#]的同意。

Details of consent of "current land owner(s)" [#] obtained 取得「現行土地擁有人」 [#] 同意的詳情		
No. of 'Current Land Owner(s)' 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where consent(s) has/have been obtained 根據土地註冊處記錄已獲得同意的地段號碼/處所地址	Date of consent obtained (DD/MM/YYYY) 取得同意的日期 (日/月/年)
33	請看附件一	07/07/2024

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has notified “current land owner(s)”[#]
已通知 名「現行土地擁有人」[#]。

Details of the “current land owner(s)” [#] notified 已獲通知「現行土地擁有人」 [#] 的詳細資料		
No. of ‘Current Land Owner(s)’ 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where notification(s) has/have been given 根據土地註冊處記錄已發出通知的地段號碼／處所地址	Date of notification given (DD/MM/YYYY) 通知日期(日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has taken reasonable steps to obtain consent of or give notification to owner(s):
已採取合理步驟以取得土地擁有人的同意或向該人發給通知。詳情如下：

Reasonable Steps to Obtain Consent of Owner(s) 取得土地擁有人的同意所採取的合理步驟

- ☐ sent request for consent to the “current land owner(s)” on _____ (DD/MM/YYYY)[&]
於 _____ (日/月/年)向每一名「現行土地擁有人」[#]郵遞要求同意書[&]

Reasonable Steps to Give Notification to Owner(s) 向土地擁有人發出通知所採取的合理步驟

- ☐ published notices in local newspapers on _____ (DD/MM/YYYY)[&]
於 _____ (日/月/年)在指定報章就申請刊登一次通知[&]
- ☐ posted notice in a prominent position on or near application site/premises on _____ (DD/MM/YYYY)[&]
於 _____ (日/月/年)在申請地點／申請處所或附近的顯明位置貼出關於該申請的通知[&]
- ☐ sent notice to relevant owners’ corporation(s)/owners’ committee(s)/mutual aid committee(s)/management office(s) or rural committee on _____ (DD/MM/YYYY)[&]
於 _____ (日/月/年)把通知寄往相關的業主立案法團／業主委員會／互助委員會或管理處，或有關的鄉事委員會[&]

Others 其他

- ☐ others (please specify)
其他（請指明）

Note: May insert more than one 「✓」.

Information should be provided on the basis of each and every lot (if applicable) and premises (if any) in respect of the application.

註：可在多於一個方格內加上「✓」號

申請人須就申請涉及的每一地段（倘適用）及處所（倘有）分別提供資料

6. Type(s) of Application 申請類別	
(A) Temporary Use/Development of Land and/or Building Not Exceeding 3 Years in Rural Areas or Regulated Areas 位於鄉郊地區或受規管地區土地上及/或建築物內進行為期不超過三年的臨時用途/發展 (For Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas, please proceed to Part (B)) (如屬位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期，請填寫(B)部分)	
(a) Proposed use(s)/development 擬議用途/發展	臨時私人停車場(私家車及輕型貨車)(為期三年) (Please illustrate the details of the proposal on a layout plan) (請用平面圖說明擬議詳情)
(b) Effective period of permission applied for 申請的許可有效期	<input checked="" type="checkbox"/> year(s) 年 3 <input type="checkbox"/> month(s) 個月
(c) Development Schedule 發展細節表	
Proposed uncovered land area 擬議露天土地面積	..... 1,525 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed covered land area 擬議有上蓋土地面積	..... 438 .....sq.m <input checked="" type="checkbox"/> About 約
Proposed number of buildings/structures 擬議建築物／構築物數目	.....
Proposed domestic floor area 擬議住用樓面面積	.....sq.m <input type="checkbox"/> About 約
Proposed non-domestic floor area 擬議非住用樓面面積	.....sq.m <input type="checkbox"/> About 約
Proposed gross floor area 擬議總樓面面積	.....sq.m <input type="checkbox"/> About 約
Proposed height and use(s) of different floors of buildings/structures (if applicable) 建築物/構築物的擬議高度及不同樓層的擬議用途 (如適用) (Please use separate sheets if the space below is insufficient) (如以下空間不足，請另頁說明)	
Proposed number of car parking spaces by types 不同種類停車位的擬議數目	
Private Car Parking Spaces 私家車車位	..... 38
Motorcycle Parking Spaces 電單車車位	.....
Light Goods Vehicle Parking Spaces 輕型貨車泊車位	..... 1
Medium Goods Vehicle Parking Spaces 中型貨車泊車位	.....
Heavy Goods Vehicle Parking Spaces 重型貨車泊車位	.....
Others (Please Specify) 其他 (請列明)	.....
Proposed number of loading/unloading spaces 上落客貨車位的擬議數目	
Taxi Spaces 的士車位	.....
Coach Spaces 旅遊巴車位	.....
Light Goods Vehicle Spaces 輕型貨車車位	.....
Medium Goods Vehicle Spaces 中型貨車車位	.....
Heavy Goods Vehicle Spaces 重型貨車車位	.....
Others (Please Specify) 其他 (請列明)	.....

[illegible]

	<p>Please state measure(s) to minimise the impact(s). For tree felling, please state the number, diameter at breast height and species of the affected trees (if possible)</p> <p>請註明盡量減少影響的措施。如涉及砍伐樹木，請說明受影響樹木的數目、及胸高度的樹幹直徑及品種(倘可)</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>.....</p>
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(B) Renewal of Permission for Temporary Use or Development in Rural Areas or Regulated Areas

位於鄉郊地區或受規管地區臨時用途/發展的許可續期

(a) Application number to which the permission relates 與許可有關的申請編號	A/ _____ / _____
(b) Date of approval 獲批給許可的日期	..... (DD 日/MM 月/YYYY 年)
(c) Date of expiry 許可屆滿日期	..... (DD 日/MM 月/YYYY 年)
(d) Approved use/development 已批給許可的用途/發展	
(e) Approval conditions 附帶條件	<p><input type="checkbox"/> The permission does not have any approval condition 許可並沒有任何附帶條件</p> <p><input type="checkbox"/> Applicant has complied with all the approval conditions 申請人已履行全部附帶條件</p> <p><input type="checkbox"/> Applicant has not yet complied with the following approval condition(s): 申請人仍未履行下列附帶條件：</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>Reason(s) for non-compliance: 仍未履行的原因：</p> <p>.....</p> <p>.....</p> <p>.....</p> <p>(Please use separate sheets if the space above is insufficient) (如以上空間不足，請另頁說明)</p>
(f) Renewal period sought 要求的續期期間	<p><input type="checkbox"/> year(s) 年</p> <p><input type="checkbox"/> month(s) 個月</p>

7. Justifications 理由

The applicant is invited to provide justifications in support of the application. Use separate sheets if necessary.
 現請申請人提供申請理由及支持其申請的資料。如有需要，請另頁說明）。

我們居民慾申請地址 Various Lots In D.D. 26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.
 地段編號: 請看附件一，申請改變土地用途，由船灣詹屋門牌號361-365及371-379相關居民使用私人停車場，地點位於大埔汀角路中段，往來市中心、巴士總站、港鐵等公共交通，是必需乘搭接駁交通工具才能到達。

每天上下班繁忙時間，公共交通非常繁忙，經常滿座，導致每天花在交通的時間很多，只好自行駕駛車輛出入，增加效率。

其實大部份時間同樣也很難搭到車的，小巴基本上是滿座，巴士班次疏落及行走時間又長，居民花在出入候車的時間需要很多，構成生活壓力和浪費時間等問題，所以居民只好駕駛車輛方便出入，解決日常上下班、接送放學、購物、看病等生活所需。

考慮到居民大多是老少共住，有私家車便可接送孩童上學，老少若有病痛時也方便迅速地前往診所/醫院看病。

加上毗鄰樂善村過渡性房屋，已於今天四月份入伙，令該地段居住人數急劇增加多2,000人出入，而公共運輸亦未有追加班次疏導居民，可想像現時已不勝負荷的公共交通配套就令居民出入更難。基於居民對私家車在日常生活便捷的需求，故停車場位置亦屬殷切需要。

僅此，希望規劃署批准對該地段的申請改為臨時私人停車場用途，盼望批准。

敬希垂注！

8. Declaration 聲明

I hereby declare that the particulars given in this application are correct and true to the best of my knowledge and belief.
本人謹此聲明，本人就這宗申請提交的資料，據本人所知及所信，均屬真實無誤。

I hereby grant a permission to the Board to copy all the materials submitted in this application and/or to upload such materials to the Board's website for browsing and downloading by the public free-of-charge at the Board's discretion.

本人現准許委員會酌情將本人就此申請所提交的所有資料複製及/或上載至委員會網站，供公眾免費瀏覽或下載。

Signature
簽署



☒ Applicant 申請人 / ☐ Authorised Agent 獲授權代理人

CHEUNG SUK FAN FENNIE

Name in Block Letters
姓名（請以正楷填寫）

Position (if applicable)
職位（如適用）

Professional Qualification(s)
專業資格

☐ Member 會員 / ☐ Fellow of 資深會員

☐ HKIP 香港規劃師學會 / ☐ HKIA 香港建築師學會 /

☐ HKIS 香港測量師學會 / ☐ HKIE 香港工程師學會 /

☐ HKILA 香港園境師學會 / ☐ HKIUD 香港城市設計學會

☐ RPP 註冊專業規劃師

Others 其他

on behalf of
代表

☐ Company 公司 / ☐ Organisation Name and Chop (if applicable) 機構名稱及蓋章（如適用）

Date 日期

08/07/2024

..... (DD/MM/YYYY 日/月/年)

Remark 備註

The materials submitted in this application and the Board's decision on the application would be disclosed to the public. Such materials would also be uploaded to the Board's website for browsing and free downloading by the public where the Board considers appropriate.

委員會會向公眾披露申請人所遞交的申請資料和委員會對申請所作的決定。在委員會認為合適的情況下，有關申請資料亦會上載至委員會網頁供公眾免費瀏覽及下載。

Warning 警告

Any person who knowingly or wilfully makes any statement or furnish any information in connection with this application, which is false in any material particular, shall be liable to an offence under the Crimes Ordinance.

任何人在明知或故意的情況下，就這宗申請提出在任何要項上是虛假的陳述或資料，即屬違反《刑事罪行條例》。

Statement on Personal Data 個人資料的聲明

1. The personal data submitted to the Board in this application will be used by the Secretary of the Board and Government departments for the following purposes:

委員會就這宗申請所收到的個人資料會交給委員會秘書及政府部門，以根據《城市規劃條例》及相關的城市規劃委員會規劃指引的規定作以下用途：

(a) the processing of this application which includes making available the name of the applicant for public inspection when making available this application for public inspection; and

處理這宗申請，包括公布這宗申請供公眾查閱，同時公布申請人的姓名供公眾查閱；以及

(b) facilitating communication between the applicant and the Secretary of the Board/Government departments.

方便申請人與委員會秘書及政府部門之間進行聯絡。

2. The personal data provided by the applicant in this application may also be disclosed to other persons for the purposes mentioned in paragraph 1 above.

申請人就這宗申請提供的個人資料，或亦會向其他人士披露，以作上述第 1 段提及的用途。

3. An applicant has a right of access and correction with respect to his/her personal data as provided under the Personal Data (Privacy) Ordinance (Cap. 486). Request for personal data access and correction should be addressed to the Secretary of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.

根據《個人資料(私隱)條例》(第 486 章)的規定，申請人有權查閱及更正其個人資料。如欲查閱及更正個人資料，應向委員會秘書提出有關要求，其地址為香港北角渣打道 333 號北角政府合署 15 樓。

Gist of Application 申請摘要	
(Please provide details in both English and Chinese as far as possible. This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information.) (請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)	
Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)
Location/address 位置/地址	Various Lots In D.D.26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.
Site area 地盤面積	1,525 sq. m 平方米 <input checked="" type="checkbox"/> About 約 (includes Government land of 包括政府土地 sq. m 平方米 <input type="checkbox"/> About 約)
Plan 圖則	S/NE-TK/19汀角分區計劃大綱核准圖
Zoning 地帶	GB, V, ROAD
Type of Application 申請類別	<input checked="" type="checkbox"/> Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區的臨時用途/發展為期 <input checked="" type="checkbox"/> Year(s) 年 <u>3</u> <input type="checkbox"/> Month(s) 月 _____ <input type="checkbox"/> Renewal of Planning Approval for Temporary Use/Development in Rural Areas or Regulated Areas for a Period of 位於鄉郊地區或受規管地區臨時用途/發展的規劃許可續期為期 <input type="checkbox"/> Year(s) 年 _____ <input type="checkbox"/> Month(s) 月 _____
Applied use/ development 申請用途/發展	臨時私人停車場(私家車及輕型貨車)(為期三年)

(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of blocks 幢數	Domestic 住用		
	Non-domestic 非住用		
(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	m 米 <input type="checkbox"/> (Not more than 不多於)	
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於)	
	Non-domestic 非住用	m 米 <input type="checkbox"/> (Not more than 不多於)	
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於)	
(iv) Site coverage 上蓋面積	% <input type="checkbox"/> About 約		
(v) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數		39
	Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) _____ _____		38 1
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數		
	Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) _____ _____		

Submitted Plans, Drawings and Documents 提交的圖則、繪圖及文件		
	<u>Chinese</u> 中文	<u>English</u> 英文
<u>Plans and Drawings 圖則及繪圖</u>		
Master layout plan(s)/Layout plan(s) 總綱發展藍圖／布局設計圖	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Block plan(s) 樓宇位置圖	<input type="checkbox"/>	<input type="checkbox"/>
Floor plan(s) 樓宇平面圖	<input type="checkbox"/>	<input type="checkbox"/>
Sectional plan(s) 截視圖	<input type="checkbox"/>	<input type="checkbox"/>
Elevation(s) 立視圖	<input type="checkbox"/>	<input type="checkbox"/>
Photomontage(s) showing the proposed development 顯示擬議發展的合成照片	<input type="checkbox"/>	<input type="checkbox"/>
Master landscape plan(s)/Landscape plan(s) 園境設計總圖／園境設計圖	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input type="checkbox"/>
<hr/>		
<u>Reports 報告書</u>		
Planning Statement/Justifications 規劃綱領/理據	<input type="checkbox"/>	<input type="checkbox"/>
Environmental assessment (noise, air and/or water pollutions) 環境評估（噪音、空氣及／或水的污染）	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on vehicles) 就車輛的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on pedestrians) 就行人的交通影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Visual impact assessment 視覺影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Landscape impact assessment 景觀影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Tree Survey 樹木調查	<input type="checkbox"/>	<input type="checkbox"/>
Geotechnical impact assessment 土力影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Drainage impact assessment 排水影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Sewerage impact assessment 排污影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Risk Assessment 風險評估	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他（請註明）	<input type="checkbox"/>	<input type="checkbox"/>
<hr/>		
<hr/>		
Note: May insert more than one 「✓」. 註：可在多於一個方格內加上「✓」號		

Note: The information in the Gist of Application above is provided by the applicant for easy reference of the general public. Under no circumstances will the Town Planning Board accept any liabilities for the use of the information nor any inaccuracies or discrepancies of the information provided. In case of doubt, reference should always be made to the submission of the applicant.

註：上述申請摘要的資料是由申請人提供以方便市民大眾參考。對於所載資料在使用上的問題及文義上的歧異，城市規劃委員會概不負責。若有任何疑問，應查閱申請人提交的文件。

附件一

Various Lots In D.D. 26, SHUEN WAN CHIM UK, TING KOK ROAD, TAI PO, N.T.

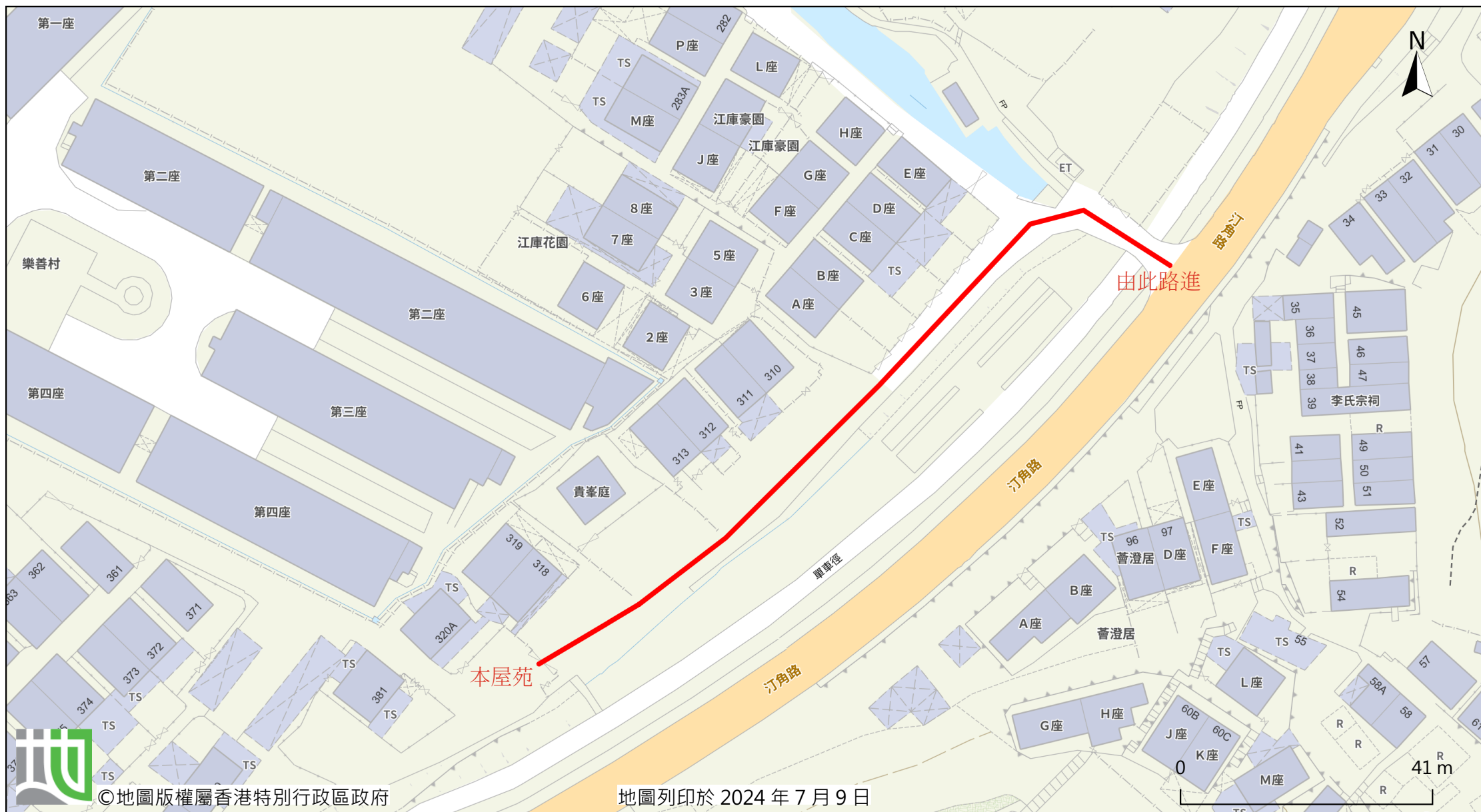
私家車位 1	D.D. 26 Lot 254 S.W ss.1	私家車位 23	D.D. 26 Lot 254 S.Y ss.7 & S.Y ss.8
私家車位 2	D.D. 26 Lot 254 S.W ss.2	私家車位 24	D.D. 26 Lot 254 S.Y ss.9 & S.Y ss.10 & Lot 255 S.R ss.12 S.A
私家車位 3	D.D. 26 Lot 254 S.W ss.3	私家車位 25	D.D. 26 Lot 254 S.Y. ss.11 & Lot 255 S.R ss.12 S.B
私家車位 4	D.D. 26 Lot 254 S.W ss.4	私家車位 26	D.D. 26 Lot 255 S.R ss.12 S.C & Lot 255 S.R ss.12 S.D
私家車位 5	D.D. 26 Lot 254 S.W ss.5	私家車位 27	D.D. 26 Lot 255 S.R ss.11 RP
私家車位 6	D.D. 26 Lot 254 S.W. ss.6	私家車位 28	D.D. 26 Lot 255 S.R. ss.11 S.B & Lot 254 S.Z RP
私家車位 7	D.D. 26 Lot 254 S.W ss.7	私家車位 29	D.D. 26 Lot 254 S.Z ss.11 & Lot 255 S.R ss.11 S.A
私家車位 8	D.D. 26 Lot 254 S.W RP	私家車位 30	D.D. 26 Lot 254 S.Z ss.10
私家車位 9	D.D. 26 Lot 254 S.X RP	私家車位 31	D.D. 26 Lot 254 S.Z ss.9
私家車位 10	D.D. 26 Lot 254 S.X ss.6	私家車位 32	D.D. 26 Lot 254 S.Z ss.8
私家車位 11	D.D. 26 Lot 254 S.X ss.5	私家車位 33	D.D. 26 Lot 254 S.Z ss.7
私家車位 12	D.D. 26 Lot 254 S.X ss.4	私家車位 34	D.D. 26 Lot 254 S.Z ss.6
私家車位 13	D.D. 26 Lot 254 S.X ss.3	私家車位 35	D.D. 26 Lot 254 S.Z ss.5
私家車位 14	D.D. 26 Lot 254 S.X ss.2	私家車位 36	D.D. 26 Lot 254 S.Z ss.4
私家車位 15	D.D. 26 Lot 254 S.X ss.1	私家車位 37	D.D. 26 Lot 254 S.Z ss.3
私家車位 16	D.D. 26 Lot 254 S.T	私家車位 38	D.D. 26 Lot 254 S.Z ss.2
私家車位 17	D.D. 26 Lot 254 S.U	私家車位 39	D.D. 26 Lot 254 S.Z ss.1
私家車位 18	D.D. 26 Lot 254 S.V	行車通道	D.D. 26 Lot 291 S.A RP
私家車位 19	D.D. 26 Lot 254 S.Y ss.1	行車通道	D.D. 26 Lot 291 S.B ss.1 (部份)
私家車位 20	D.D. 26 Lot 254 S.Y ss.2 & S.Y ss.3 RP	行車通道	D.D. 26 Lot 254 RP (部份)
私家車位 21	D.D. 26 Lot 254 S.Y ss.3 S.A & S.Y ss.4	行車通道	D.D. 26 Lot 254 S.AB
私家車位 22	D.D. 26 Lot 254 S.Y ss.5 & S.Y ss.6	行車通道	D.D. 26 Lot 255 S.R RP (部份)

車位及行車通道布局設計圖



Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.





☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

John Michael AUSTIN/PLAND

寄件者: cheung fennie [REDACTED]
寄件日期: 2024年09月02日星期一 16:52
收件者: John Michael AUSTIN/PLAND
主旨: 回覆: A/NE-TK/800 Application Form & layout plan
附件: A_NE-TK_800 申請位置圖.pdf

類別: Internet Email

Dear Mr. Ho

回答問題如下:

混凝土
帶有轆可移動上蓋將會移走

Best regards,
Fennie Cheung

寄件者: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
寄件日期: 2024 年 9 月 2 日 16:11
收件者: cheung fennie [REDACTED]
副本: Charlotte Tsz Wing WUN/PLAND <ctwwun@pland.gov.hk>
主旨: Re: A/NE-TK/800 Application Form & layout plan

Dear Ms. CHEUNG ,

As spoken, I refer to the captioned planning application.
Please find below comments from Lands Department for your follow up aciton.

Lands Department (LandsD):

- There are unauthorized uses on the private lots which are already subject to lease enforcement actions according to case priority. The Lot owner(s) **should rectify the lease breaches as demanded by LandsD.**
- We would also like you to address these issues for further processing.
- Please advise on the filling of land materials.
- Please identify the location of the car park for Light Goods Vehicles on the plan.

please feel free to contact the undersigned for other enquires.

車位及行車通道布局設計圖



Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

John Michael AUSTIN/PLAND

主旨: Fw: [A/NE-TK/800] - Comments from the Draiang Services Department

From: cheung fennie [REDACTED]
Sent: Wednesday, November 13, 2024 3:18 PM
To: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
Subject: Re: 回覆: [A/NE-TK/800] - Comments from the Draiang Services Department

Dear Mr. Ho

現在申請範圍大約是1512平方米，填土共845平方米。

謝謝！

Best regards,
Fennié Cheung

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

John Michael AUSTIN/PLAND

寄件者: John Michael AUSTIN/PLAND
寄件日期: 2024年11月12日星期二 12:03
收件者: tpbpd/PLAND
主旨: Fw: [A/NE-TK/800] - Comments from the Drainage Services Department
附件: A_NE-TK_800 申請位置圖.pdf; 附件一.xlsx; drainage proposal report 20241106-1.pdf

Dear TPB ,

Applicant of A/NE-TK/800 submitted FI on 12.11.2024 for your information please.

Best regards,

John AUSTIN

*Sha Tin, Tai Po & North District Planning Office
Planning Department
TPG/TP2
2158 6037*

From: cheung fennie [REDACTED]
Sent: Friday, November 8, 2024 9:10 AM
To: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
Subject: 回覆: [A/NE-TK/800] - Comments from the Drainage Services Department

Dear Mr. Ng

(更新通知)

現通知減小一個車位的申請，現在合共38個車位申請及附上渠務報告，詳細請看附件，謝謝！

Thanks and regards,
Fennié Cheung

車位及行車通道布局設計圖



Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.

添比建設有限公司

Ratio Architecture & Construction Limited



SUBMISSION REPORT
FOR
DRAINAGE PROPOSAL DESIGN
FOR
TEMPORARY PRIVATE VEHICLE PARK
(PRIVATE CARS AND LIGHT GOODS VEHICLES ONLY)
FOR A PERIOD OF 3 YEARS
AT
VARIOUS LOTS IN D.D.26, SHUEN WAN CHIM UK
TING KOK, TAI PO, NEW TERRITORIES



Ratio Architecture & Construction Limited

Date : November 2024

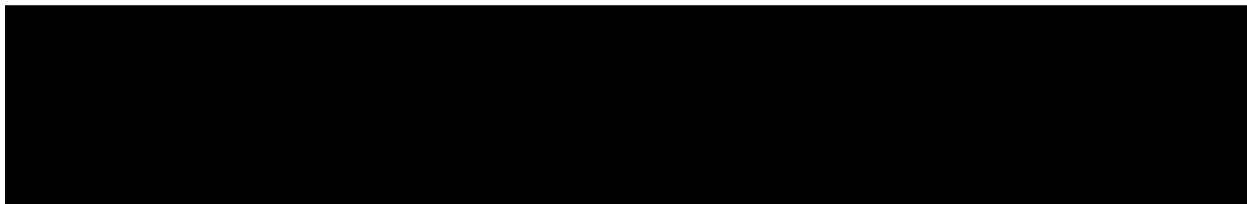


TABLE OF CONTENTS

1. Introduction
2. Existing Drainage Condition
3. Design parameters & assumptions
4. Proposed Stormwater Drainage
5. Effect on Drainage Characteristics and potential Drainage Impacts
6. Conclusions

APPENDIX

- | | |
|------------|-----------------------------------|
| Appendix A | Stormwater Drainage Proposal Plan |
| Appendix B | Surface Drainage Design |

REFERENCES

1. Stormwater Drainage Manual, Planning Design and Management by DSD
2. Geotechnical Manual for Slopes by GEO
3. Standard Drawings by DSD

1. Introduction

This proposal is prepared for the proposed stormwater drainage works for the temporary private vehicle park (private cars and light goods vehicles only) for a period of 3 years at various lots in D.D.26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories.

2. Existing Drainage Condition

A plan showing the existing catchments is enclosed in **Appendix B**. Currently, the surface runoff collected from the site is collected by the existing 225mm and 375mm u-channel as shown in **Appendix A**. A drainage proposal is required to be carried out for the assessment on the effect on the existing drainage system from the proposed development.

3. Design Parameters & Assumptions

The design criteria to be used for the modeling assessment are based on the standards set out in the Stormwater Drainage Manual, Third Edition (SDM). According to Section 6.6.1 of the SDM, the existing village drainage system in the vicinity of the development is classified as main rural catchment drainage system. Table 10 of the SDM recommends to be adopted a 50 year design return period storm event for the urban drainage branch system.

Stormwater Runoff (Q)

The rate of stormwater runoff used in this assessment report is estimated by the “Rational method” in which the peak runoff is calculated from the formula:

$$Q = K \times i \times A / 3600$$

where	Q	=	maximum runoff (L/s)
	i	=	design mean intensity of rainfall (mm/hr)
	A	=	area of catchment (m ²)
	K	=	runoff coefficient

Time of Concentration (tc)

The time of concentration is defined as the time required for stormwater runoff to flow from the most remote part of the catchment area to the point in the drainage system under consideration. Based on the assumptions adopted in the Rational Method, this is the time taken for the peak runoff to become established at the considered section.

The time of concentration comprises the time for water flowing within natural catchments and along the man-made drainage pipes/channels. For natural catchments, the time of concentration is estimated by the modified form of the Brandsby William's equation.

$$t_o = \frac{0.14465L}{H^{0.2} A^{0.1}}$$

Where t_o = time of concentration of a natural catchment (min.)

A = catchment area (m^2)

H = average slope (m per 100m), measured along the line of natural flow, from the summit of the catchment to the point under consideration

L = distance (on plan) measured on the line of natural flow between the summit and the point under consideration (m)

Mean Rainfall Intensity (i)

Mean rainfall intensity-duration curves attached in this report are based on the Statistical analysis of long term rainfall records from the Hong Kong Observatory. A return period of 50 years is adopted.

Runoff Coefficient (K)

The value of K is taken as 0.95 for developed area. For vegetated ground, the value of K is taken as 0.3.

4. Assessment on Existing Stormwater Drainage

The existing stormwater drainage works include surface U-channels at the peripheral of the site collecting the runoff from catchments within the site. As per the existing site condition, an existing drainage system of the site is considered adequate for the development and no additional surface channel is required. As stated by the local villagers, no flooding was occurred so far and even during the “once-in-500-years: rainstorm in September 2023.

In addition, regarding the concern of drainage impact arising from the newly-built “Lok Sin Village” development, a joint site visit was held in March 2024 with the presence of representatives from Drainage Services Department (DSD), building contractor from Lok Sin Village and various lots owners of the application. During the site visit, the representatives from DSD made some comments to the building contractor of Lok Sin Village. After that, the building contractor had improved the existing stormwater drainage works which was accepted by the DSD’s representatives.

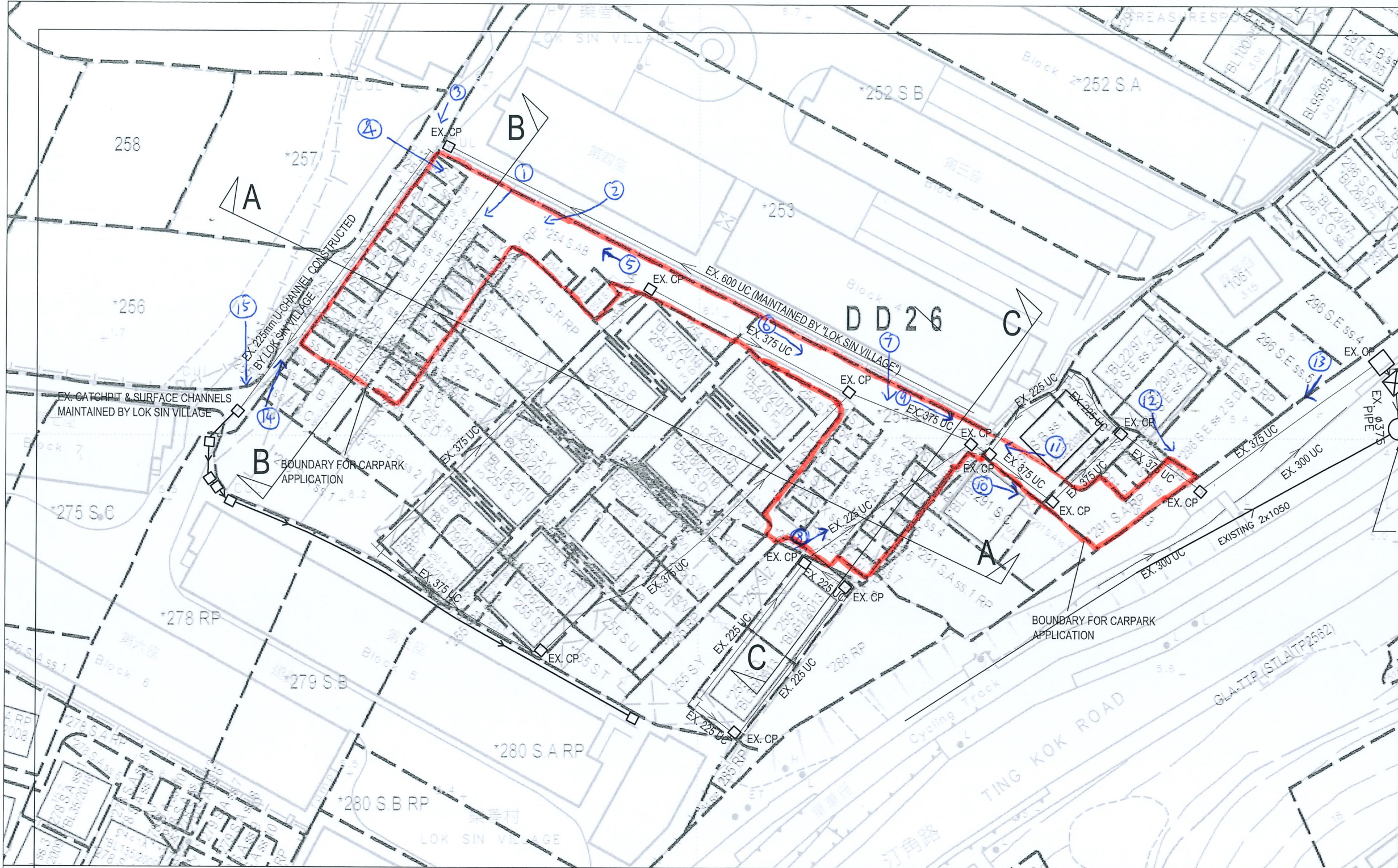
The existing stormwater drainage layout plan is shown in **Appendix A**.

5. Conclusion

Since there is no additional hard paving and no additional landfilling works proposed for the development and the existing surface system has enough capacity to collect the runoff from the application site and its adjacent area, it is considered that the drainage conditions of adjacent areas will not be adversely affected.

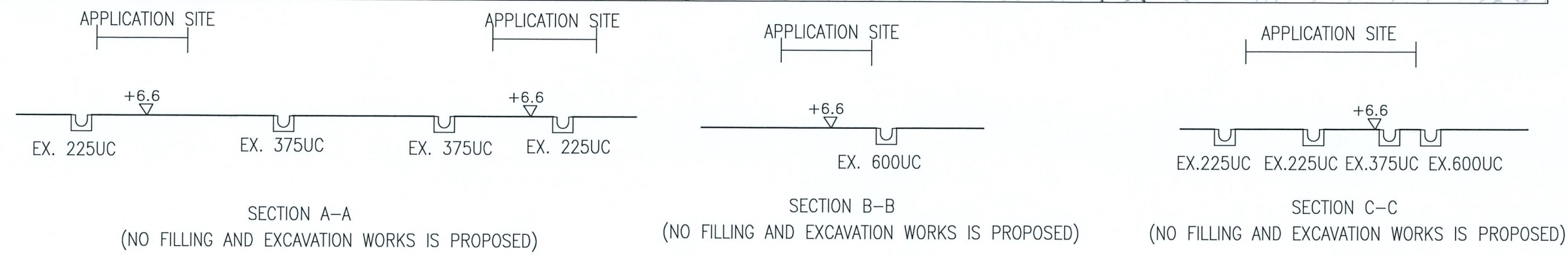
Appendix A

Stormwater Drainage Proposal Plan



- LEGEND:
- EX. 225UC EXISTING 225mm U-CHANNEL
 - EX. 375UC EXISTING 375mm U-CHANNEL
 - EX. CP EXISTING CATCHPIT
 - ① → PHOTO TAKING DIRECTION
 - APPLICATION SITE

Ø 250 PVC PIPE
EXISTING 2 x 1050
SMH1009338
I.L. 1.25



	DLO SUBMIT	RC	AY	RY	NOV 24
REV	DESCRIPTION	CHECKED	APPROVED	DWN	DATE
ENGINEERING CONSULTANT					
RATIO ARCHITECTURE & CONSTRUCTION COMPANY					
2/F, NO.73 KWONG FUK ROAD, TAI PO, N.T.					
PROJECT TITLE:					
STORMWATER DRAINAGE PROPOSAL FOR TEMPORARY PRIVATE VEHICLE PARK (PRIVATE CARS AND LIGHT GOODS VEHICLES ONLY) FOR A PERIOD OF 3 YEARS AT VARIOUS LOT IN D.D. 26 AT SHUEN WAN CHIM UK, TAI PO					
DRAWING TITLE:					
DRAINAGE PROPOSAL PLAN AND TYPICAL DETAILS					
SCALE : N.T.S.		CAD FILE: CAD_REF			
DRAWN	RY	DRAWING NO. SDP001			
S.D	RY				
DESIGNED	RC				
CHECKED	AY				
		B.D. REF. NO.:			

Photo 1



Photo 2



Photo 3

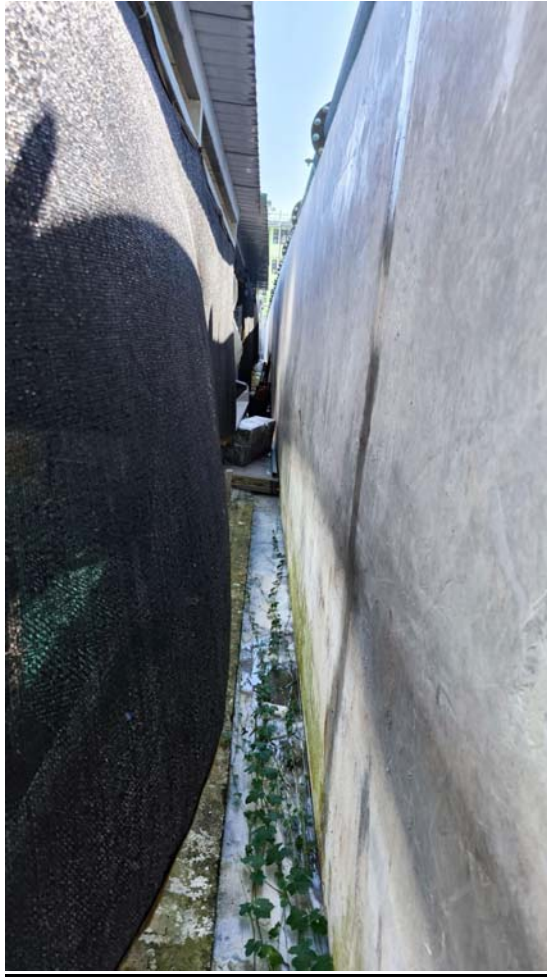


Photo 4



Photo 5



Photo 6



Photo 7



Photo 8



Photo 9



Photo 10



Photo 11



Photo 12

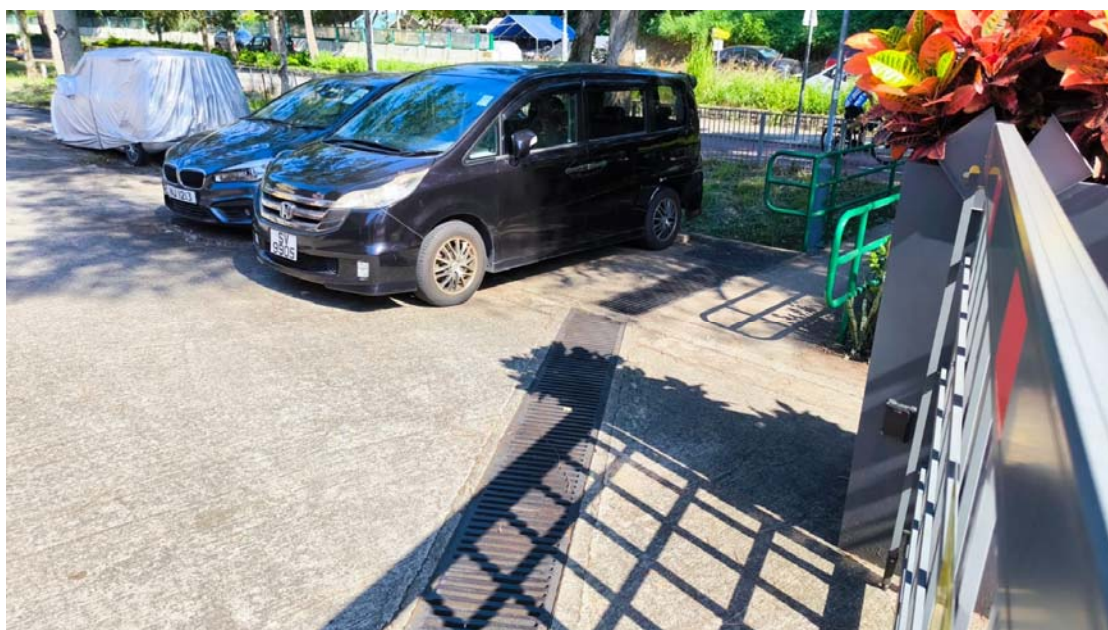


Photo 13



Photo 14

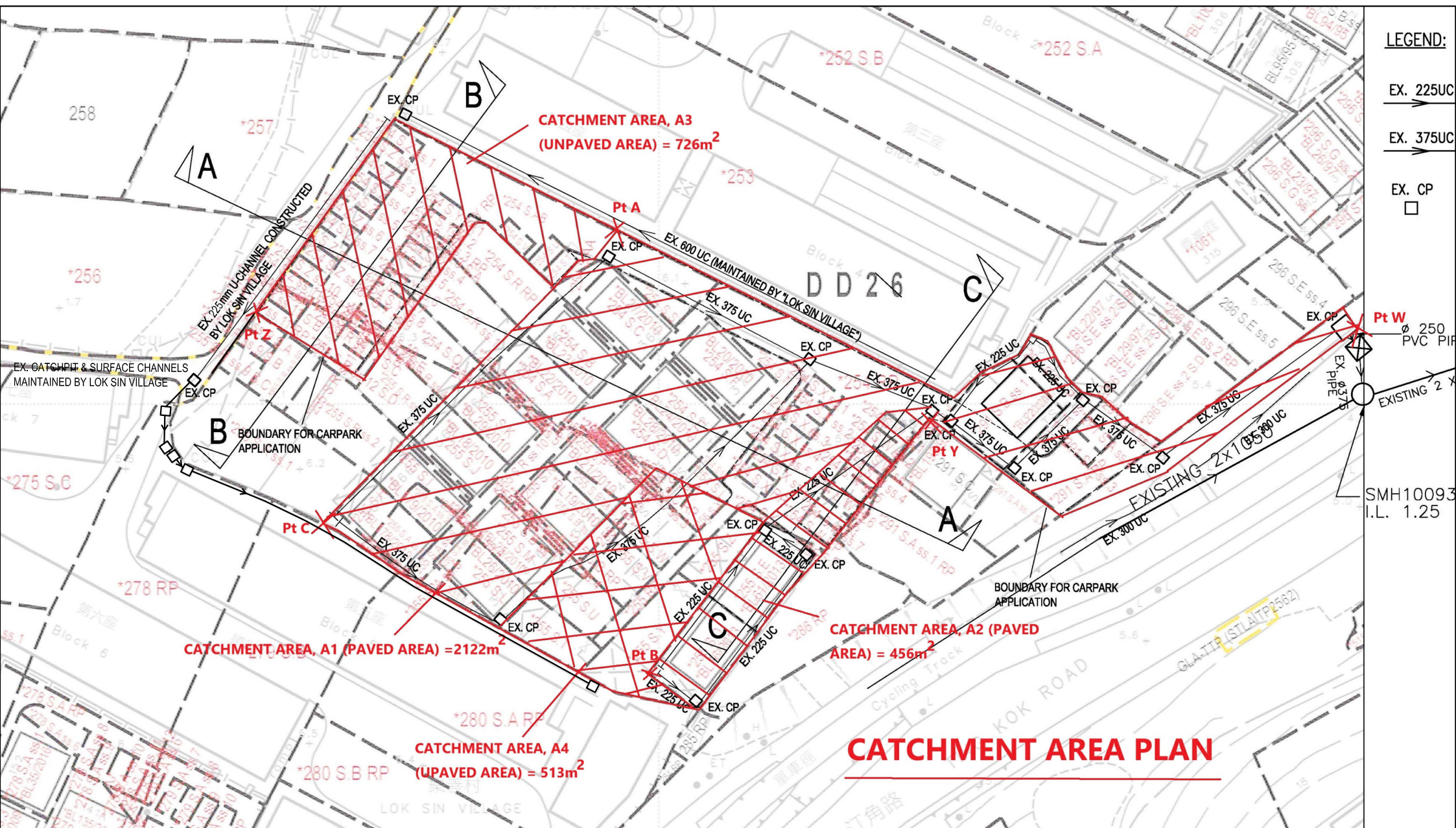


Photo 15



Appendix B

Surface Drainage Design



Project No.: Drainage Design at Chim Uk, Tai Po Date: 6-Nov-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 375UC

Catchment area,	A1	=	2122	m ²	Assume k = 0.95 for paved surface
	A2	=	456	m ²	
	A4	=	513	m ²	Assume k = 0.3 for unpaved surface
Total Catchment Area, A = (A1+A2)x0.95 + A4x0.3 =			2603	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt W	(Ld) =	161.00	m
Shortest distance from summit point to outlet, Pt W	(Ls) =	140.00	m

Elevation of remote point (Pt C)	=	6.60	mPD
Elevation of outlet point, Pt W	=	4.20	mPD

Average fall, H	=	$(z_1 - z_2)/L_s \times 100$	
	=	1.71	m per 100m

From TGN30

$$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$$

$$= 9.52 \quad \text{min}$$

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$$i = 270 \quad \text{mm/hr}$$

$$Q = \frac{kiA}{60} \times 1.138$$

$$= 13330 \quad \text{lit/min}$$

From TGN 43A1

For existing 375 UC with 1 in 100 gradient

Maximum capacity	=	15000	lit/min	>	13330	o.k.
The corresponding velocity	=	2.00	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po
Prepared by: Ray Cheng

Date: 6-Nov-24

Check for the drainage capacity of existing 225UC

Catchment area,	A2	=	456	m ²	Assume k = 0.95 for paved surface
Total Catchment, A=	A2 x 0.95	=	433.2	m ³	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Y	(Ld) =	70.00	m
Shortest distance from summit point to outlet, Pt Y	(Ls) =	53.00	m

Elevation of remote point (Pt B)	=	6.60	mPD
Elevation of outlet point, Pt Y	=	4.35	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100
	=	4.25 m per 100m

From TGN30

T _c	=	0.14465 x L _d / (H ^{0.2} x A ^{0.1})
	=	4.13 min

Assume a 1 in 50 year design rainfall return period for rural area
From Geo-Manual (Fig 8.2)

i	=	340	mm/hr
Q	=	kiA/60	x 1.138
		2794	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3900	lit/min	>	2794	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po
Prepared by: Ray Cheng

Date: 6-Nov-24

Check for the drainage capacity of existing 225UC

Catchment area,	A3	=	726	m ²	Assume k = 0.3 for unpaved surface
		=	726 x 0.3	m ²	
		=	217.8	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Z	(Ld) =	69.00	m
Shortest distance from summit point to outlet, Pt Z	(Ls) =	55.00	m

Elevation of remote point (Pt A)	=	6.60	mPD
Elevation of outlet point, Pt Z	=	5.92	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100
	=	1.24 m per 100m

From TGN30

T _c	=	0.14465 x L _d / (H ^{0.2} x A ^{0.1})	
	=	5.58	min

Assume a 1 in 50 year design rainfall return period for rural area
From Geo-Manual (Fig 8.2)

i	=	315	mm/hr
Q	=	kiA/60	x 1.138
		1301	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3900	lit/min	>	1301	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

GEO Technical Guidance Note No. 30 (TGN 30)
Updated Intensity-Duration-Frequency Curves with Provision for
Climate Change for Slope Drainage Design

Issue No.: 2	Revision: -	Date: 23.10.2018	Page: 3 of 4
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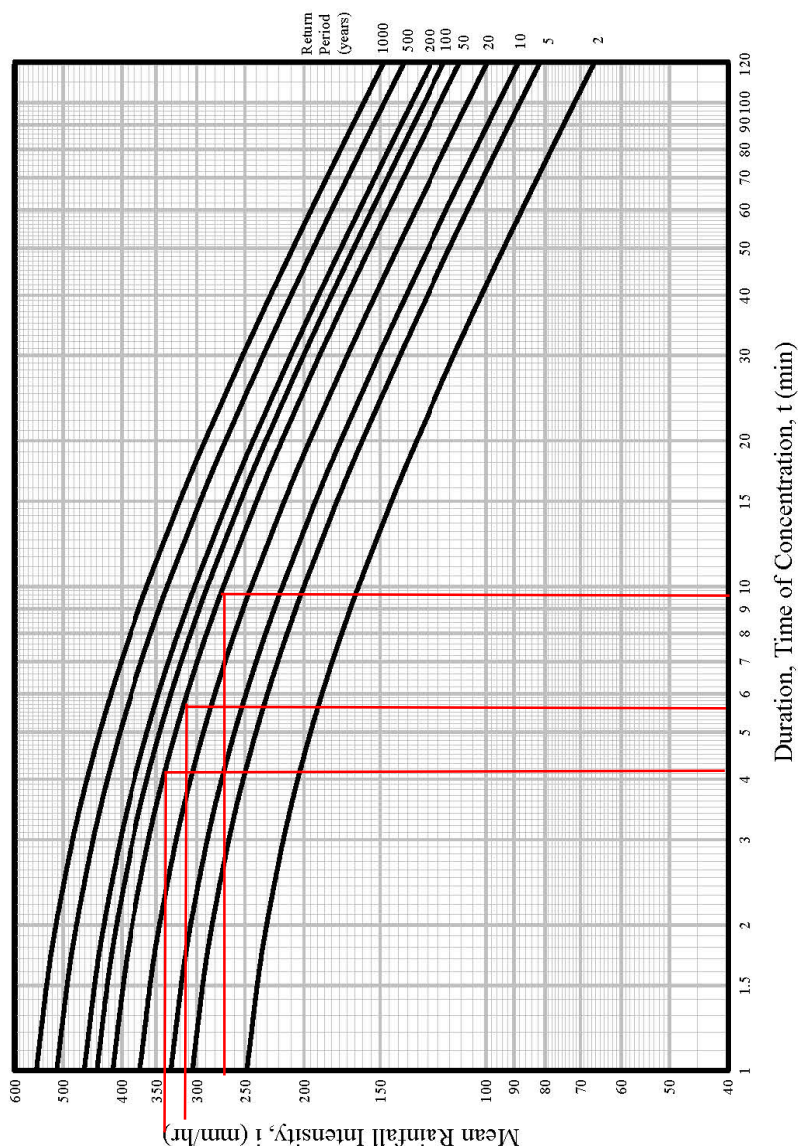


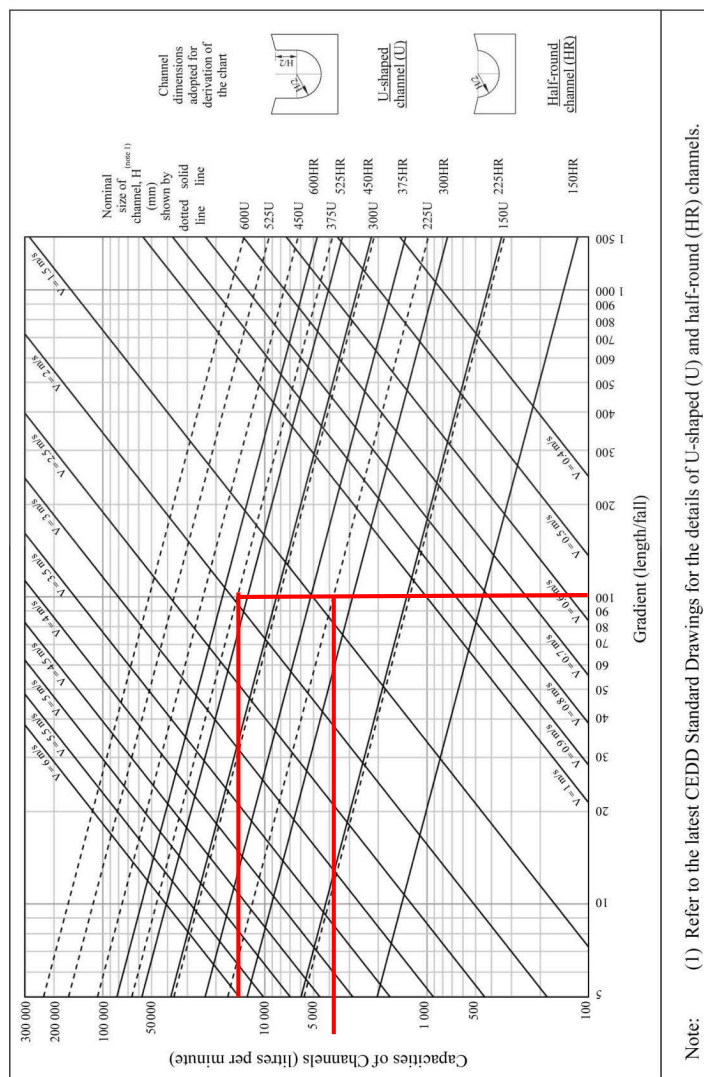
Figure 1 – Updated Intensity-Duration-Frequency Curves

- Notes:
1. These IDF curves are to supersede those given in Figure 8.2 of the Geotechnical Manual for Slopes (GCO, 1984).
 2. These IDF curves have not incorporated any projected climate change effects. Except for temporary slope drainage design, the mean rainfall intensity given by these IDF curves shall be increased by 13.8% for incorporating climate change effects.
 3. The mathematical formulae of these IDF curves are shown in Table 1 of Annex TGN 30 A1.

GEO Technical Guidance Note No. 43 (TGN 43)
Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes

Issue No.: 1 Revision: - Date: 05.06.2014 Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm



☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy ☐Confidential

Charlotte Tsz Wing WUN/PLAND

寄件者: cheung fennie <[REDACTED]>
寄件日期: 2025年01月13日星期一 16:32
收件者: Charlotte Tsz Wing WUN/PLAND
主旨: 回覆: [A/NE-TK/800] - Comments from the Drainage Services Department
附件: stormwater proposal 2nd submission to TPB (A_NE-TK_800).pdf
類別: Internet Email

Dear Miss Wun,

現附上第二次渠務報告，請看附見，謝謝！

Best regards,
Fennie Cheung

寄件者: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
寄件日期: 2024年12月23日 16:34
收件者: cheung fennie <[REDACTED]>
副本: Charlotte Tsz Wing WUN/PLAND <ctwwun@pland.gov.hk>; Ching Hoi Ching NG/PLAND <chcng@pland.gov.hk>
主旨: Re: [A/NE-TK/800] - Comments from the Drainage Services Department

張小姐:

如電話所述，本署已收到你就規劃申請No. A/NE-TK/800提交的延期申請。如日後有任何有關題述規劃申請的疑問，請與助理城市規劃師溫小姐 (電郵: ctwwun@pland.gov.hk ;電話: 2158 6018) 聯絡，謝謝。

祝好
沙田，大埔及北區規劃處
見習城市規劃師/大埔(2)
何曉暉

From: cheung fennie <[REDACTED]>
Sent: Thursday, December 19, 2024 3:52 PM
To: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
Subject: Re: [A/NE-TK/800] - Comments from the Drainage Services Department

Dear Mr.Ho

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy ☐Confidential

我現申請延期兩個月，原因是為了回應渠務署的建議，謝謝

Best regards
Fennie Cheung

John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>於2024年12月5日 下午2:51寫道：

Dear Ms Cheung ,

please find the below comments from the Drainage Services Department for your follow up action please:

Comments from the CE/MN, DSD (Contact Person: Karen HO, Tel: 2300 1364)

<image001.jpg>

If you intend to make response to the comments or provide further information to supplement your application, please make reference to the Town Planning Board Guidelines (TPB PG-No. 32) which is available for public viewing at the website of the TPB (http://www.info.gov.hk/tpb/en/forms/Guidelines/TPB_PG_32.pdf).

- Should you have any questions related to the comments from DSD, please contact the relevant department, please feel free to contact the undersigned for other enquires.

Best regards,
John AUSTIN
Sha Tin, Tai Po & North District Planning Office
Planning Department
TPG/TP2
2158 6037

From: cheung fennie [REDACTED]
Sent: Wednesday, November 13, 2024 3:18 PM
To: John Michael AUSTIN/PLAND <jmaustin@pland.gov.hk>
Subject: Re: 回覆: [A/NE-TK/800] - Comments from the Drainage Services Department

Dear Mr. Ho

現在申請範圍大約是1512平方米，填土共845平方米。

謝謝！

Best regards,
Fennie Cheung

Your Ref.: TPB/A/NE-TK/800

24 December, 2024

Secretary, Town Planning Board,
15/F., North Point Government Offices,
333 Java Road, North Point,
Hong Kong

Dear Sir/Madam,

**Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only)
for a Period of 3 Years and Associated Filling of Land in “Green Belt”, “Village
Type Development” and area shown as “Road”
Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories
(Application No. A/NE-TK/800)
Submission of Revised Stormwater Drainage Proposal**

Further to your email with comments from Drainage Services Department (DSD), we are pleased to tabulate the following responses to your comments for your easy reference:

Item	DSD comments	Our Responses
(a)	Please verify there is no landfilling works carried out at the application site as “no filling and excavation works is proposed: is specified on the drainage proposal plan.	As stated in the stormwater drainage proposal, it is confirmed that there will be no more landfilling works at the application site.
(b)	Cover and invert levels of the existing catchpits should be provided	The cover and invert levels of the existing catchpits is provided in this resubmission.
(c)	The rainfall intensity should be increased by 16% according to Table 28 of SDM Corrigendum No. 1/2022.	The rainfall intensity is increased by 16% in the drainage design in this resubmission.
(d)	Reduction in flow area should be taken in account in accordance with Section 9.3 of Stormwater Drainage Manual (SDM) (Fifth Edition, January 2018).	The flow area of the surface channel is considered in the drainage design in this resubmission.
(e)	Please demonstrate the hydraulic	Please note that the 250mm dia.

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	adequacy of the 250mm dia. and 375mm dia. storm drains at upstream of the manhole no. SMH1009338.	stormwater drain is no longer existed. The hydraulic calculation of 375mm dia. storm drains is carried out and found that its capacity is adequate to collect and discharge the runoff collected from site and adjacent area.
(f)	Please clarify if the existing 375mm dia. storm drain shown on the drainage proposal plan is feature no. SWD1043960.	The existing 375mm dia. storm drain is feature no. SWD1043960 and indicated on the drainage proposal in this resubmission.
(g)	Please provide photo showing the ground condition of catchment area A4 to justify the use of runoff coefficient of 0.3.	The previous catchment area A4 is assumed as paved area in the drainage design in this resubmission.
(h)	The catchpit with trap should be provided before connecting to the public stormwater drainage system.	The catchpit with trap (CP19) is provided before connecting to the public drainage system in this resubmission.

We are pleased to submit herewith revised stormwater drainage plan and revised catchment area plan of the captioned development for DSD's approval.

Yours faithfully,
On behalf of



Ratio Architecture & Construction Limited

Mr. Dennis TSE

Director

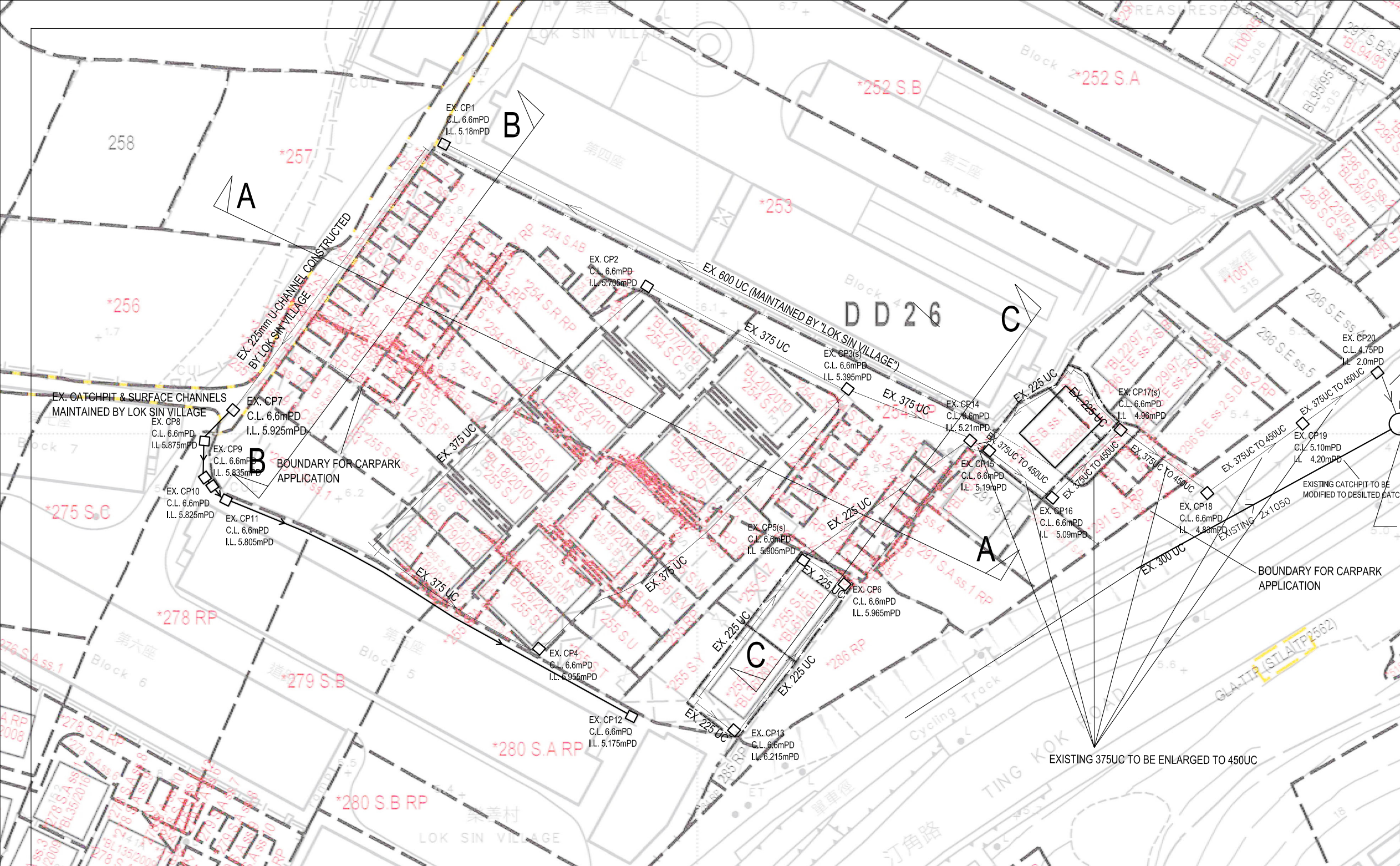
Encl. As stated

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Ratio Architecture & Construction Limited

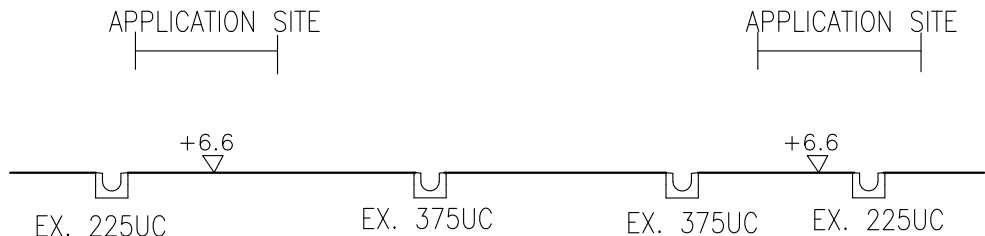
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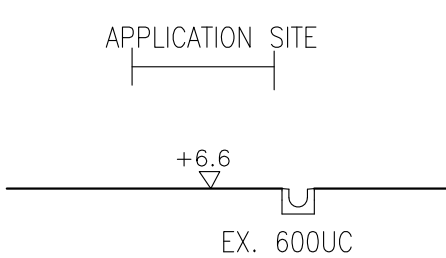
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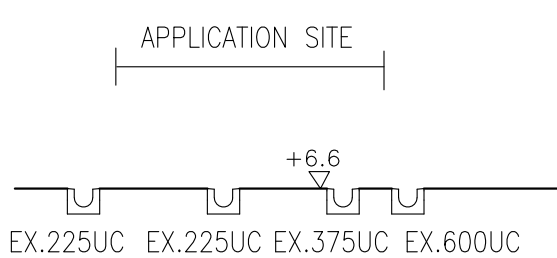
- LEGEND:**
- EX. 225UC
→ EXISTING 225mm U-CHANNEL
 - EX. 375UC
→ EXISTING 375mm U-CHANNEL
 - EX. CP
□ EXISTING CATCHPIT
 - EX. CP3(s)
□ EXISTING DESILTED CATCHPIT



SECTION A-A
(NO FILLING AND EXCAVATION WORKS IS PROPOSED)



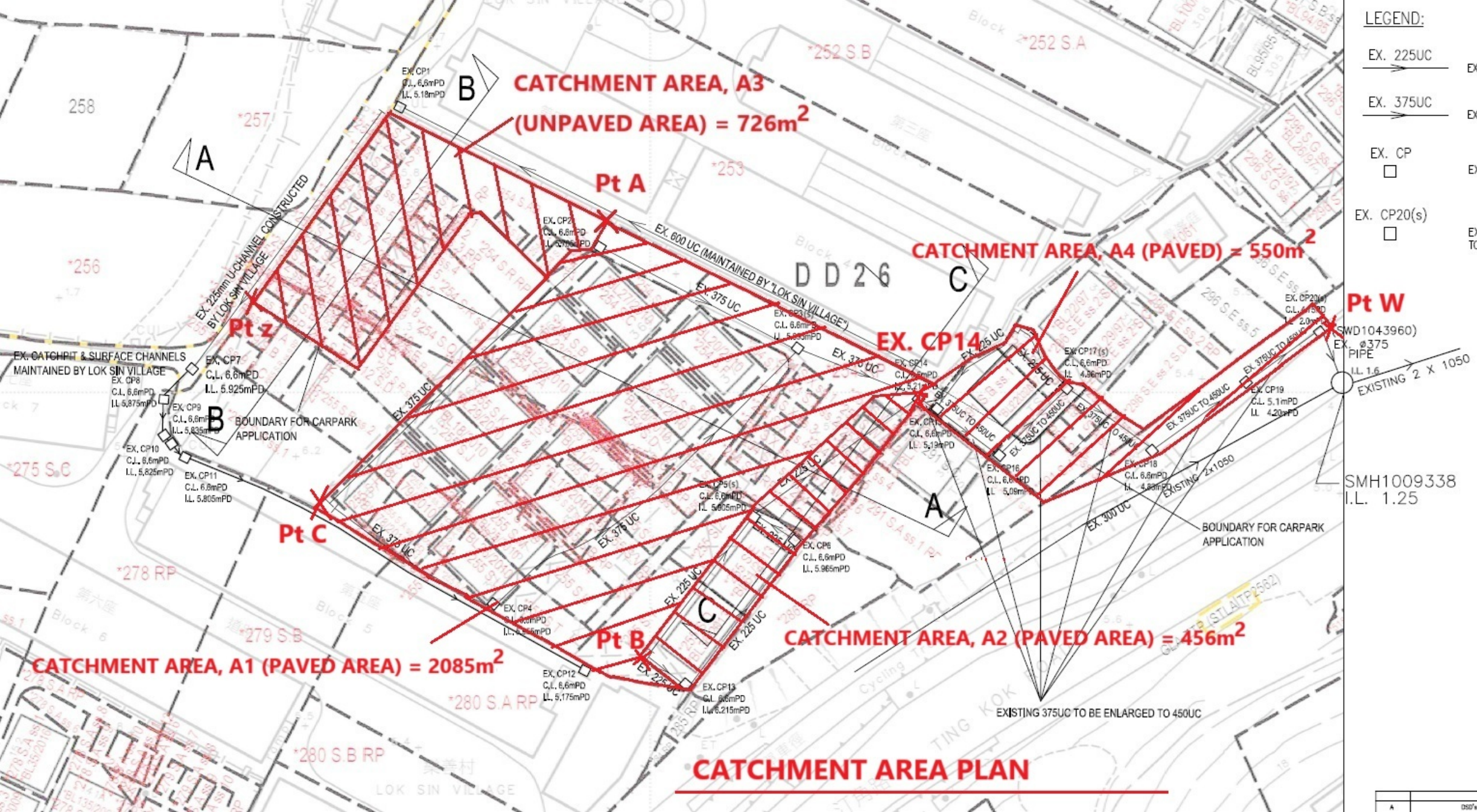
SECTION B-B
(NO FILLING AND EXCAVATION WORKS IS PROPOSED)



SECTION C-C
(NO FILLING AND EXCAVATION WORKS IS PROPOSED)

A	DSD's COMMENT	RC	AY	RY	DEC 24
	DLO SUBMIT	RC	AY	RY	NOV 24
REV	DESCRIPTION	CHECKED	APPROVED	DWN	DATE
ENGINEERING CONSULTANT					
RATIO ARCHITECTURE & CONSTRUCTION COMPANY					
2/F, NO.73 KWONG FUK ROAD, TAI PO, N.T.					
PROJECT TITLE:					
STORMWATER DRAINAGE PROPOSAL FOR TEMPORARY PRIVATE VEHICLE PARK (PRIVATE CARS AND LIGHT GOODS VEHICLES ONLY) FOR A PERIOD OF 3 YEARS AT VARIOUS LOT IN D.D. 26 AT SHUEN WAN CHIM UK, TAI PO					
DRAWING TITLE:					
DRAINAGE PROPOSAL PLAN AND TYPICAL DETAILS					
SCALE :		N.T.S.		CAD FILE: CAD_REF	
DRAWN	RY			DRAWING NO. SDP001A	
S.D	RY				
DESIGNED	RC				
CHECKED	AY				
				B.D. REF. NO.:	

EX. 225UC	→	EX
EX. 375UC	→	EX
EX. CP	<input type="checkbox"/>	EX
EX. CP20(s)	<input type="checkbox"/>	EX



Project No.: Drainage Design at Chim Uk, Tai Po Date: 14-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 375UC

Catchment area, A_1 = 2085 m^2 Assume $k = 0.95$ for paved surface

Total Catchment Area, $A = A_1 \times 0.95 = 1980.75 \text{ } m^2$

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m^2)

Longest distance from summit point to outlet, Ex. CP14

(L_d) = 102.00 m

Shortest distance from summit point to outlet, Ex. CP14

(L_s) = 84.00 m

Elevation of remote point (Pt C) = 6.60 mPD

Elevation of outlet point, Ex. CP14 = 4.80 mPD

Average fall, $H = (z_1 - z_2)/L_s \times 100$

= 2.14 m per 100m

From TGN30

$$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$$

= 5.93 min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$i = 310 \text{ mm/hr}$

$Q = \frac{kiA}{60} \times 1.16$
 11871 lit/min

From TGN 43A1

For existing 375 UC with 1 in 100 gradient

Maximum capacity = 13500 lit/min > 11871 o.k.

The corresponding velocity = 2.00 m/s < 4 o.k.

Project No.: Drainage Design at Chim Uk,Tai Po Date: 14-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 375UC

Catchment area, A_1 = 2085 m^2 Assume $k = 0.95$ for paved surface

Total Catchment Area, $A = A_1 \times 0.95 = 1980.75 \text{ } m^2$

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m^2)

Longest distance from summit point to outlet, Ex. CP14

(L_d) = 102.00 m

Shortest distance from summit point to outlet, Ex. CP14

(L_s) = 84.00 m

Elevation of remote point (Pt C) = 6.60 mPD

Elevation of outlet point, Ex. CP14 = 4.80 mPD

Average fall, $H = (z_1 - z_2)/L_s \times 100$

= 2.14 m per 100m

From TGN30

$$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$$

= 5.93 min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$i = 310 \text{ mm/hr}$

$Q = \frac{kiA}{60} \times 1.16$
 11871 lit/min

From TGN 43A1

For existing 375 UC with 1 in 100 gradient

Maximum capacity = 13500 lit/min > 11871 o.k.

The corresponding velocity = 2.00 m/s < 4 o.k.

Project No.: Drainage Design at Chim Uk, Tai Po Date: 14-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 225UC

Catchment area,	A2	=	456	m ²	Assume k = 0.95 for paved surface
Total Catchment, A=	A2 x 0.95	=	433.2	m ³	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)
 k = Runoff coefficient
 i = Design mean intensity of rainfall (mm/hr)
 A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Y	(Ld) =	70.00	m
Shortest distance from summit point to outlet, Pt Y	(Ls) =	53.00	m

Elevation of remote point (Pt B)	=	6.60	mPD
Elevation of outlet point, Pt Y	=	4.35	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100
	=	4.25 m per 100m

From TGN30

T _c	=	0.14465 x L _d / (H ^{0.2} x A ^{0.1})	
	=	4.13	min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

i	=	340	mm/hr
Q	=	kiA/60	x 1.16
		2848	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3510	lit/min	>	2848	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po Date: 14-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 225UC

Catchment area,	A3	=	726	m ²	Assume k = 0.3 for unpaved surface
		=	726 x 0.3	m ²	
		=	217.8	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Z	(Ld) =	69.00	m
Shortest distance from summit point to outlet, Pt Z	(Ls) =	55.00	m

Elevation of remote point (Pt A)	=	6.60	mPD
Elevation of outlet point, Pt Z	=	5.92	mPD

Average fall, H	=	$(z_1 - z_2)/L_s \times 100$
	=	1.24 m per 100m

From TGN30

T_c	=	$0.14465 \times L_d / (H^{0.2} \times A^{0.1})$	
	=	5.58	min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

i	=	315	mm/hr
Q	=	$kiA/60$	x 1.16
		1326	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3510	lit/min	>	1326	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po Date: 16-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of proposed 450UC

Catchment area,	A1	=	2085	m ²	Assume k = 0.95 for paved surface
	A2	=	456	m ²	
	A4	=	550	m ²	
Total Catchment Area, A = (A1+A2+A4) x 0.95 =			2936.45	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt W	(Ld) =	161.00	m
Shortest distance from summit point to outlet, Pt W	(Ls) =	140.00	m

Elevation of remote point (Pt C)	=	6.60	mPD
Elevation of outlet point, Pt W	=	4.20	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100	
	=	1.71	m per 100m

From TGN30

$$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$$

$$= 9.41 \quad \text{min}$$

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$$i = 270 \quad \text{mm/hr}$$

$$Q = \frac{kiA}{60} \times 1.16$$

$$= 15038 \quad \text{lit/min}$$

From TGN 43A1

For proposed 450 UC with 1 in 100 gradient

Maximum capacity	=	22500	lit/min	>	15038	o.k.
The corresponding velocity	=	2.00	m/s	<	4	o.k.

The capacity of the existing 375mm dia. Underground pipe = 28,728 lit/min > 15038 lit/min O.K.

GEO Technical Guidance Note No. 30 (TGN 30)
Updated Intensity-Duration-Frequency Curves with Provision for
Climate Change for Slope Drainage Design

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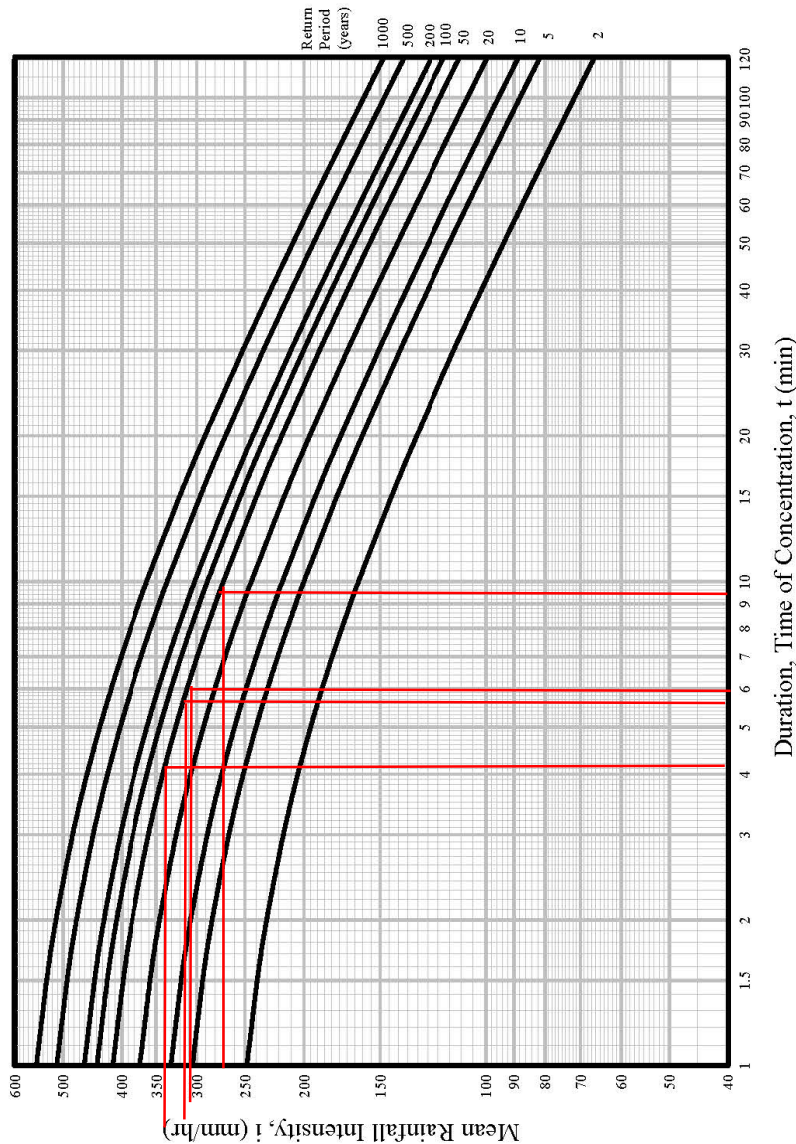


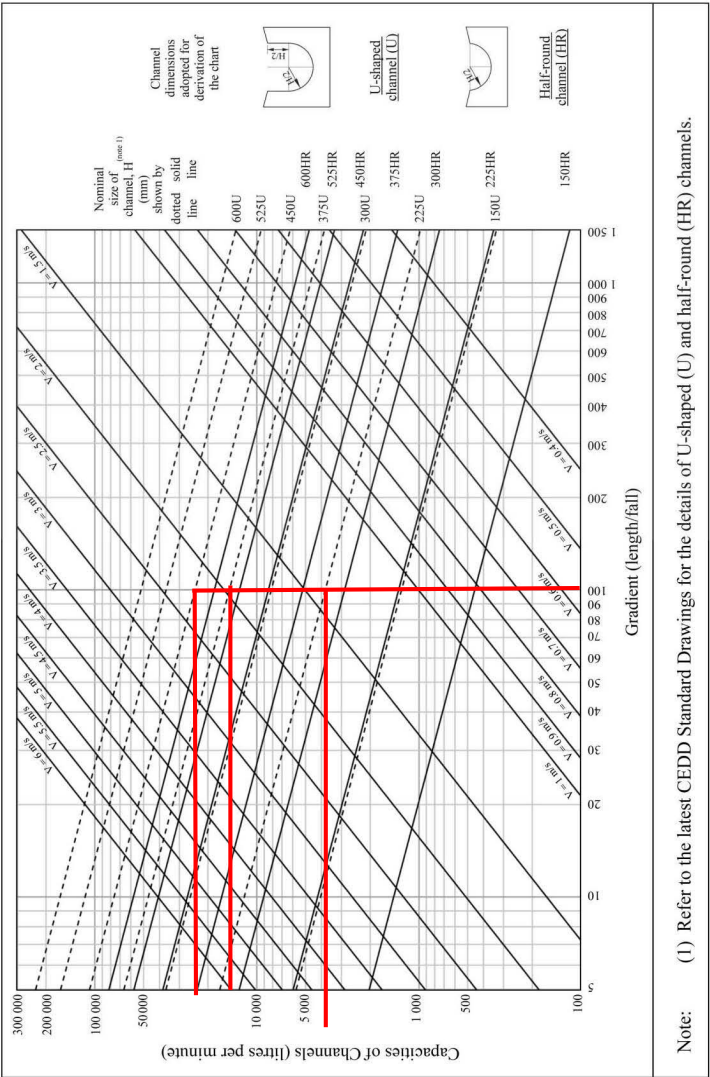
Figure 1 – Updated Intensity-Duration-Frequency Curves

- Notes:
1. These IDF curves are to supersede those given in Figure 8.2 of the Geotechnical Manual for Slopes (GCO, 1984).
 2. These IDF curves have not incorporated any projected climate change effects. Except for temporary slope drainage design, the mean rainfall intensity given by these IDF curves shall be increased by 13.8% for incorporating climate change effects.
 3. The mathematical formulae of these IDF curves are shown in Table 1 of Annex TGN 30 A1.

GEO Technical Guidance Note No. 43 (TGN 43)
Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes

Issue No.: 1 Revision: - Date: 05.06.2014 Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm



Since 10% reduction would be considered for deposition of sediment, the capacity of the proposed 225UC should be $3,900 \times 0.9 = 3,510 \text{ lit/min}$, 375UC should be $15,000 \times 0.9 = 13,500 \text{ lit/min}$, 450UC should be $25,000 \times 0.9 = 22,500 \text{ lit/min}$

Upstream level of SWD1043960 is +2.0mPD
 Downstream level of SWD1043960 is +1.6mPD
 Length of SWD1043960 is 5.4m
 Gradient of SWD1043960 = $5.4/(2-1.6) = 1:13.5$

ks = 0.600mm
 i = 0.004 to 0.1
 ie hydraulic gradient =
 1 in 250 to 1 in 10

Water (or sewage) at 15°C
 full bore conditions.
 velocities in m/s
 discharges in m³/s

18
 continued

The Capacity of SWD1043960 = $0.532 \times 1000 \times 60 \times 0.9 = 28,728 \text{ l/min} > 15,038 \text{ l/min}$

Gradient	Pipe diameters in mm:											
	350	375	400	450	500	525	600	675	700	750	800	825
0.02000 1/ 50	2.456 0.236	2.566 0.283	2.673 0.336	2.879 0.458	3.076 0.604	3.171 0.687	3.444 0.975	3.710 1.328	3.795 1.461	3.962 1.750	4.123 2.073	4.203 2.247
0.02200 1/ 45	2.577 0.248	2.692 0.297	2.804 0.352	3.020 0.480	3.227 0.634	3.327 0.720	3.617 1.023	3.892 1.393	3.981 1.532	4.156 1.836	4.325 2.174	4.409 2.357
0.02400 1/ 42	2.692 0.259	2.812 0.311	2.929 0.368	3.155 0.502	3.371 0.662	3.476 0.752	3.778 1.068	4.066 1.455	4.159 1.601	4.341 1.918	4.518 2.271	4.605 2.462
0.02600 1/ 38	2.803 0.270	2.928 0.323	3.050 0.383	3.284 0.522	3.509 0.689	3.618 0.783	3.933 1.112	4.233 1.515	4.329 1.666	4.519 1.996	4.703 2.364	4.794 2.563
0.02800 1/ 36	2.909 0.280	3.039 0.336	3.165 0.398	3.409 0.542	3.642 0.715	3.755 0.813	4.082 1.154	4.393 1.572	4.493 1.729	4.690 2.072	4.882 2.454	4.975 2.660
0.03000 1/ 33	3.012 0.290	3.146 0.347	3.277 0.412	3.529 0.561	3.770 0.740	3.888 0.842	4.225 1.195	4.548 1.627	4.652 1.790	4.855 2.145	5.053 2.540	5.151 2.753
0.03200 1/ 31	3.111 0.299	3.250 0.359	3.385 0.425	3.645 0.580	3.895 0.765	4.015 0.869	4.365 1.234	4.697 1.681	4.805 1.849	5.015 2.216	5.220 2.624	5.320 2.844
0.03400 1/ 29	3.207 0.309	3.350 0.370	3.489 0.438	3.758 0.598	4.015 0.788	4.140 0.896	4.500 1.272	4.842 1.733	4.953 1.906	5.170 2.284	5.381 2.705	5.484 2.932
0.03600 1/ 28	3.300 0.318	3.448 0.381	3.591 0.451	3.867 0.615	4.132 0.811	4.260 0.922	4.631 1.309	4.983 1.783	5.097 1.962	5.320 2.350	5.537 2.783	5.644 3.017
0.03800 1/ 26	3.391 0.326	3.542 0.391	3.690 0.464	3.974 0.632	4.245 0.834	4.377 0.948	4.758 1.345	5.120 1.832	5.237 2.015	5.466 2.415	5.689 2.860	5.799 3.100
0.04000 1/ 25	3.480 0.335	3.635 0.401	3.786 0.476	4.077 0.648	4.356 0.855	4.491 0.972	4.882 1.380	5.253 1.880	5.374 2.068	5.609 2.478	5.837 2.934	5.950 3.180
0.04200 1/ 24	3.566 0.343	3.725 0.411	3.880 0.488	4.178 0.665	4.464 0.877	4.602 0.996	5.005 1.415	5.384 1.926	5.507 2.119	5.747 2.539	5.982 3.007	6.097 3.259
0.04400 1/ 23	3.650 0.351	3.813 0.421	3.972 0.499	4.277 0.680	4.569 0.897	4.711 1.020	5.121 1.448	5.511 1.972	5.637 2.169	5.883 2.599	6.123 3.078	6.241 3.336
0.04600 1/ 22	3.733 0.359	3.899 0.431	4.061 0.510	4.374 0.696	4.672 0.917	4.817 1.043	5.236 1.481	5.635 2.016	5.764 2.218	6.016 2.658	6.261 3.147	6.381 3.411
0.04800 1/ 21	3.813 0.367	3.983 0.440	4.149 0.521	4.468 0.711	4.773 0.937	4.921 1.065	5.349 1.512	5.756 2.060	5.888 2.266	6.145 2.715	6.396 3.215	6.519 3.485
0.05000 1/ 20	3.892 0.374	4.066 0.449	4.235 0.532	4.560 0.725	4.872 0.957	5.023 1.087	5.460 1.544	5.875 2.102	6.010 2.313	6.272 2.771	6.528 3.281	6.654 3.557
0.05500 1/ 18	4.083 0.393	4.265 0.471	4.442 0.558	4.784 0.761	5.111 1.003	5.269 1.141	5.727 1.619	6.163 2.205	6.304 2.426	6.579 2.907	6.848 3.442	6.979 3.731
0.06000 1/ 17	4.265 0.410	4.455 0.492	4.640 0.583	4.997 0.795	5.338 1.048	5.504 1.191	5.982 1.692	6.437 2.304	6.585 2.534	6.872 3.036	7.153 3.595	7.290 3.897
0.06500 1/ 15	4.440 0.427	4.638 0.512	4.830 0.607	5.202 0.827	5.557 1.091	5.729 1.240	6.227 1.761	6.701 2.398	6.854 2.638	7.154 3.160	7.445 3.742	7.589 4.057
0.07000 1/ 14	4.608 0.443	4.813 0.532	5.013 0.630	5.399 0.859	5.767 1.132	5.946 1.287	6.463 1.827	6.954 2.489	7.113 2.738	7.424 3.280	7.727 3.884	7.876 4.210
0.07500 1/ 13	4.770 0.459	4.983 0.550	5.190 0.652	5.589 0.889	5.970 1.172	6.155 1.332	6.690 1.892	7.199 2.576	7.364 2.834	7.685 3.395	7.999 4.021	8.153 4.358
0.08000 1/ 13	4.927 0.474	5.147 0.568	5.360 0.674	5.772 0.918	6.167 1.211	6.358 1.376	6.910 1.954	7.436 2.661	7.606 2.927	7.938 3.507	8.262 4.153	8.420 4.501
0.08500 1/ 12	5.079 0.489	5.306 0.586	5.526 0.694	5.951 0.946	6.357 1.248	6.554 1.419	7.123 2.614	7.665 2.743	7.840 3.017	8.183 3.615	8.516 4.281	8.680 4.640
							7.320	7.888	8.068	8.420	8.764	8.932

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Charlotte Tsz Wing WUN/PLAND

寄件者: cheung fennie [REDACTED]
寄件日期: 2025年02月06日星期四 17:34
收件者: Charlotte Tsz Wing WUN/PLAND
主旨: 回覆: Planning Application No. A/NE-TK/800 - Comments from the Drainage Services Department
類別: Internet Email

Dear Miss Wun,

現回覆以下問題，謝謝！

1. 是
2. 規劃申請獲批准後會處理

Best regards,
Fennie Cheung

寄件者: Charlotte Tsz Wing WUN/PLAND <ctwwun@pland.gov.hk>
寄件日期: 2025 年 2 月 3 日 11:20
收件者: cheung fennie [REDACTED]
副本: Shing Fung CHAIR/PLAND [REDACTED]; Ching Hoi Ching NG/PLAND <chcng@pland.gov.hk>
主旨: Re: Planning Application No. A/NE-TK/800 - Comments from the Drainage Services Department

Dear Ms. CHEUNG,

I refer to your planning application (No. A/NE-TK/800) for temporary private vehicle park (private cars and light goods vehicles only) for a period of 3 years and associated filling of land at various lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories.

Please find the following items for your clarification:

- Whether the existing canopies at the application site is covered by the current planning application.
- As the Lands Department (LandsD) advised that there are unauthorized uses on the private lots subject to lease enforcement actions according to case priority and the lot owners should rectify

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the lease breaches as demanded by LandsD, please advise if the lands matters have been resolved or will be resolved should the planning application be approved.

If you intend to make response to the comments or provide further information to supplement your application, please make reference to the Town Planning Board Guidelines (TPB PG-No. 32B) which is available for public viewing at the website of the TPB (https://www.tpb.gov.hk/en/forms/Guidelines/TPB_PG_32B.pdf).

Should you have any questions, please feel free to contact the undersigned.

Thank you.

Regards,
Charlotte WUN
ATP/TP5
Sha Tin, Tai Po and North District Planning Office
Planning Department
Tel: 2158 6018

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Charlotte Tsz Wing WUN/PLAND

寄件者: cheung fennie [REDACTED]
寄件日期: 2025年02月10日星期一 10:48
收件者: Charlotte Tsz Wing WUN/PLAND
主旨: 回覆: Planning Application No. A/NE-TK/800 - Comments from the Drainage Services Department

類別: Internet Email

Dear Miss Wun,

填土面積約 625m2

謝謝!

Regards,
Fennie Cheung

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Charlotte Tsz Wing WUN/PLAND

寄件者: cheung fennie [REDACTED]
寄件日期: 2025年02月10日星期一 9:36
收件者: Charlotte Tsz Wing WUN/PLAND
主旨: 回覆: Planning Application No. A/NE-TK/800 - Comments from the Drainage Services Department
附件: A_NE-TK800申請位置圖.pdf; stormwater proposal 3rd submission cover to TPB dated 7.2.2025 (A_NE-TK_800).pdf
類別: Internet Email

Dear Miss Wun,

現附上第三次渠報告及更新申請位置圖，請看附件，謝謝！

Best Regards,
Fennie Cheung

車位及行車通道布局設計圖



Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok Road, Tai Po, N.T.

Your Ref.: TPB/A/NE-TK/800

7 February, 2025

Secretary, Town Planning Board,
15/F., North Point Government Offices,
333 Java Road, North Point,
Hong Kong

Dear Sir/Madam,

**Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only)
for a Period of 3 Years and Associated Filling of Land in “Green Belt”, “Village
Type Development” and area shown as “Road”
Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories
(Application No. A/NE-TK/800)
Submission of Revised Stormwater Drainage Proposal**

Further to your email with comments from Drainage Services Department (DSD), we are pleased to tabulate the following responses to your comments for your easy reference:

Item	DSD comments	Our Responses
(a)	R to C Item H refers. Please indicate on the drainage proposal that catchpit with trap (CP19) will be provided. Details of the catchpit with trap should also be provided.	The "existing catchpit CP19 to be modified to desilted catchpit" is already marked on drainage plan and the details of desilted catchpit is provided in this resubmission.
(b)	It is noted that the existing 375 U-channels between catchpits CP15 and CP20 will be upgraded to 450 U-channels. Please advise if the invert levels of the proposed 450 U-channels will be same as the invert levels of the existing 375 U-channels. Details of the proposed u-channel and its grating cover should also be provided.	The invert level of the catchpit CP15 to CP20 and proposed 450UC is already revised in this resubmission. Besides, the details of u-channel and its grating cover are provided in this resubmission.
(c)	Sections A-A, B-B and C-C do not match with the existing site condition. Please review.	The section A-A, B-B and C-C is revised to match with the existing condition in this resubmission.

添比建設有限公司

Ratio Architecture & Construction Limited



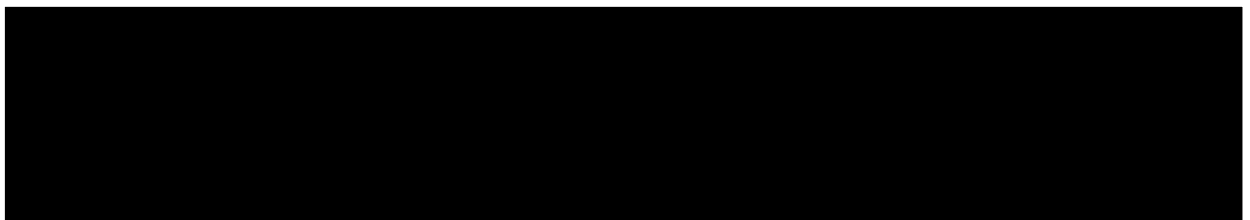
We are pleased to submit herewith revised stormwater drainage plan and revised catchment area plan of the captioned development for DSD's approval.

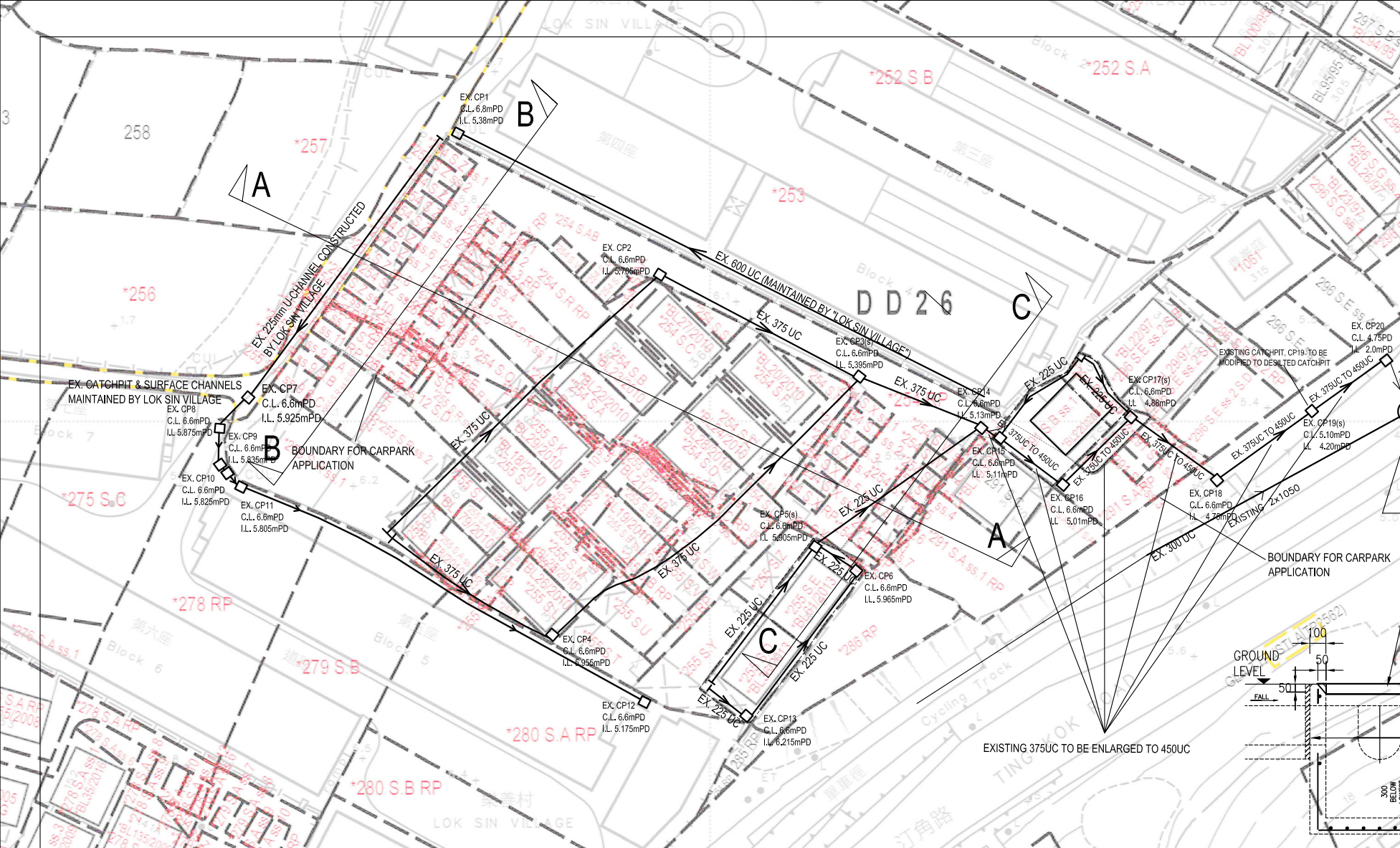
Yours faithfully,
On behalf of



Ratio Architecture & Construction Limited
Mr. Dennis TSE
Director

Encl. As stated





LEGEND:

- EX. 225UC
→ EXISTING 225mm U-CHANNEL
- EX. 375UC
→ EXISTING 375mm U-CHANNEL
- EX. CP
□ EXISTING CATCHPIT
- EX. CP3(s)
□ EXISTING DESILTED CATCHPIT

GENERAL NOTE

1. THE PROPOSED DRAINAGE WORK, WHETHER WITHIN OR OUTSIDE THE LOT BOUNDARY, SHOULD BE CONSTRUCTED AND MAINTAINED BY THE LOT OWNER AT HIS OWN EXPENSE. FOR WORKS TO BE UNDERTAKEN OUTSIDE THE LOT BOUNDARY, PRIOR CONSENT AND AGREEMENT FROM DLO AND/OR RELEVANT PRIVATE LOT OWNER SHOULD BE SOUGHT.

2. THE 100mm OPENING AT 1m C/C SHALL BE PROVIDED AT THE BOTTOM OF HOARDING OR WALLS IF ANY.

CONCRETE STRENGTH AND STEEL REINFORCEMENT SPECIFICATION FOR DRAINAGE DETAILS

1. CONCRETE GRADE FOR CATCHPITS AND U-CHANNEL SHALL BE 30D DESIGN IN COMPLIANCE WITH CS1 : 2010
FOR BLINDING LAYER SHALL BE 15D, DESIGN COMPLY WITH CS1-2010.
2. ALL MAIN BARS TO BE HOT ROLLED HIGH YIELD STEEL DEFORMED BAR COMPLM WITH CS2 : 2012
Y – HIGH YIELD BAR 500 MPa
M – MILD STEEL BAR 250 MPa
3. CONCRETE COVER TO MAIN REINFORCEMENT TO BE 50mm.
4. LAP LENGTH FOR ALL BARS TO BE 46x DIAMETER OF LARGER BAR TO BE LAPPED.
5. REACTIVE ALKALI CONTENT EXPRESSED IN SODIUM OXIDE PER CUBIC METER OF CONCRETE SHOULD NOT EXCEED 3KG AS PER PNAP APP-74.

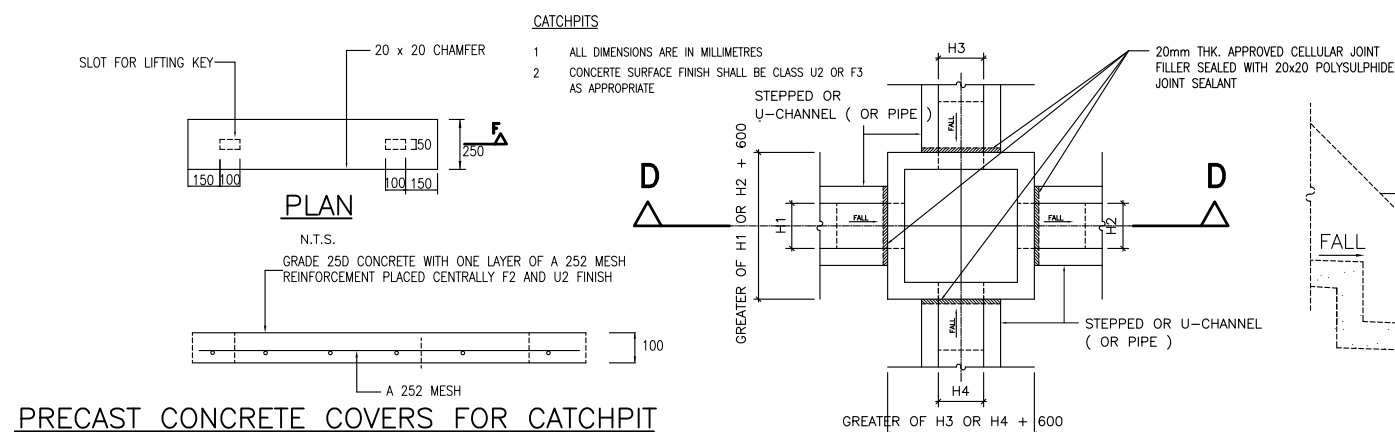
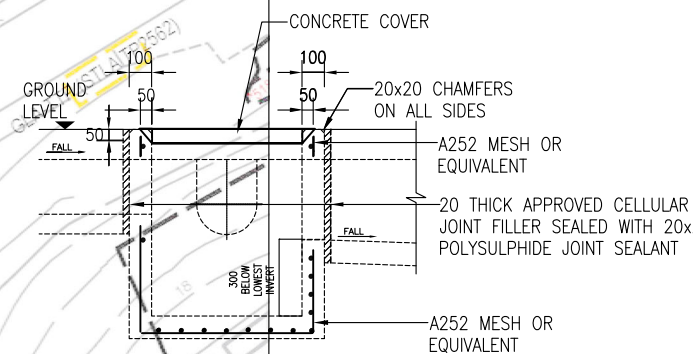
HALF ROUND, U, AND STEPPED – CHANNELS

- 1 ALL DIMENSIONS ARE IN MILLIMETERS
- 2 CONCRETE SURFACE FINISHING SHALL BE CLASS U2 OR F2 AS APPROPRIATE
- 3 FOR HALF ROUND AND U – CHANNEL, SPACING OF EXPANSION JOINT IN CHANNELS, BERMS AND APRON TO BE 10m MAXIMUN. FOR STEPPED CHANNELS, EXPANSION JOINTS TO BE PROVIDED AT A MAXIMUN SPACING OF 10m.
- 4 DIMENSIONS FOR HALF ROUND AND U-CHANNELS SEE TABLE 1.
- 5 THE COVER FOR U-CHANNELS AND CATCHPIT SHALL COMPLY WITH CEDD'S STANDARD DRAWINGS NO. C2405 TO C2407 AND C2412.
- 6 ALL PROPOSED U-CHANNELS SHALL BE COVERED WITH GRATING

TABLE 1 : DIMENSION OF U-CHANNEL AND HALF-ROUND CHANNEL

NORMAL SIZE H	T	B	REINFORCING
<300	100	100	NIL
375 – 675	150	150	NIL
750 – 900	175	175	A252 MESH PLACED CENTRALLY

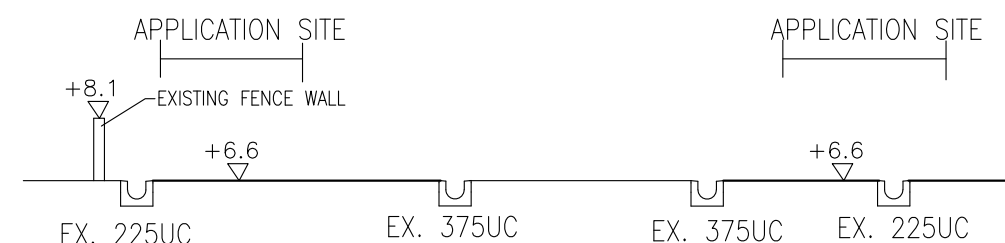
SECTION D – D WITH DESILTED TRAP COMPLY WITH CEDD'S DRAWING NO. DS C2405 AND C2406I



PRECAST CONCRETE COVERS FOR CATCHPIT

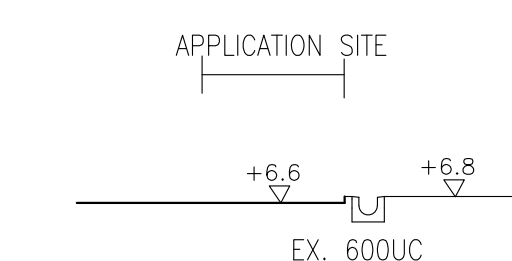
PLAN

TYPICAL DETAILS OF CATCHPIT



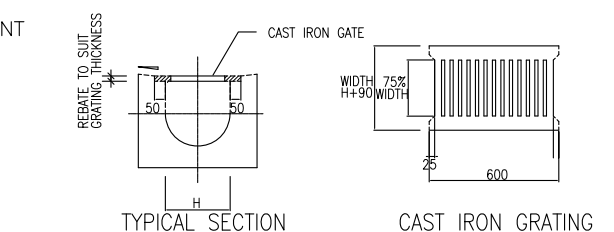
SECTION A-A

(NO FILLING AND EXCAVATION WORKS IS PROPOSED)

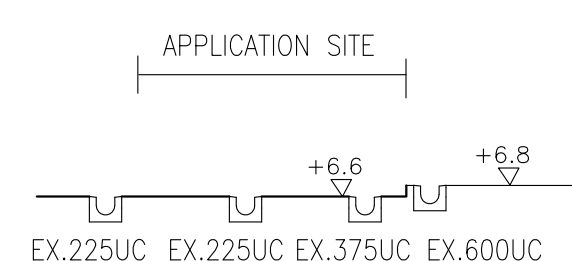


SECTION B-B

(NO FILLING AND EXCAVATION WORKS IS PROPOSED)

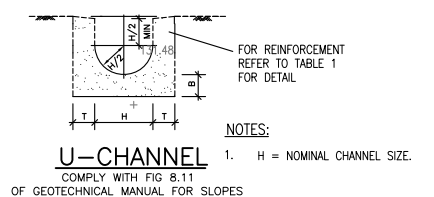


U-CHANNEL WITH CAST IRON GRATING



SECTION C-C

(NO FILLING AND EXCAVATION WORKS IS PROPOSED)



B	DSD's COMMENT	RC	AY	RY	FEB 25
A	DSD's COMMENT	RC	AY	RY	DEC 24
	DLO SUBMIT	RC	AY	RY	NOV 24
REV	DESCRIPTION	CHECKED	APPROVED	DWN	DATE

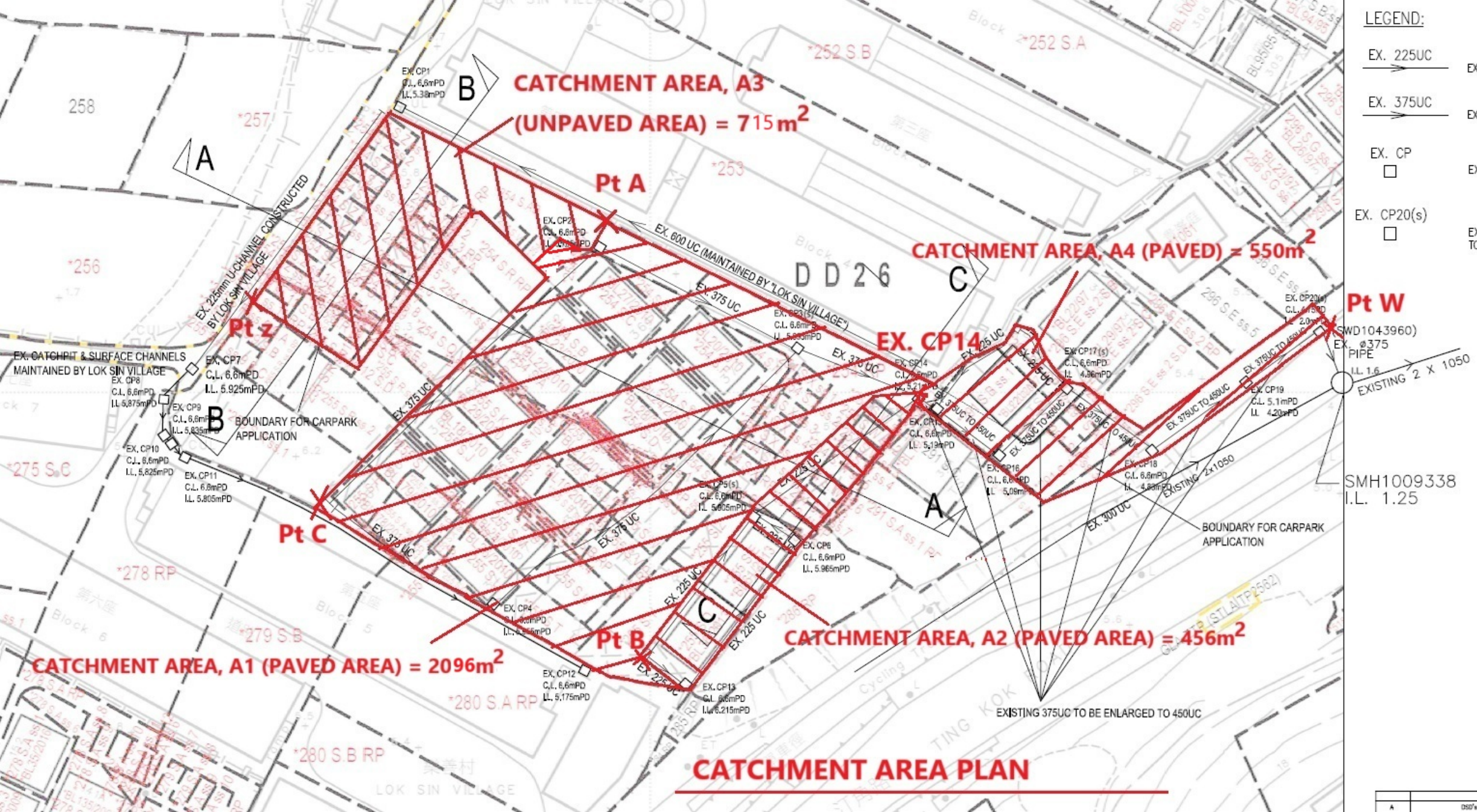
ENGINEERING CONSULTANT
RATIO ARCHITECTURE & CONSTRUCTION COMPANY
2/F, NO.73 KWONG FUK ROAD, TAI PO, N.T.

PROJECT TITLE:
STORMWATER DRAINAGE PROPOSAL FOR
TEMPORARY PRIVATE VEHICLE PARK (PRIVATE
CARS AND LIGHT GOODS VEHICLES ONLY) FOR
A PERIOD OF 3 YEARS AT VARIOUS LOT IN D.D. 26
AT SHUEN WAN CHIM UK, TAI PO

DRAWING TITLE:
DRAINAGE PROPOSAL PLAN
AND TYPICAL DETAILS

SCALE :	N.T.S.	CAD FILE:	CAD_REF
DRAWN	RY	DRAWING NO.	
S.D	RY		
DESIGNED	RC		
CHECKED	AY		
		B.D. REF. NO.:	SDP001B

EX. 225UC	→	EX
EX. 375UC	→	EX
EX. CP	<input type="checkbox"/>	EX
EX. CP20(s)	<input type="checkbox"/>	EX



Project No.: Drainage Design at Chim Uk, Tai Po Date: 7-Feb-25
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 375UC

Catchment area, A_1 = 2096 m^2 Assume $k = 0.95$ for paved surface

Total Catchment Area, $A = A_1 \times 0.95 = 1991.2 \text{ } m^2$

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m^2)

Longest distance from summit point to outlet, Ex. CP14

(L_d) = 102.00 m

Shortest distance from summit point to outlet, Ex. CP14

(L_s) = 84.00 m

Elevation of remote point (Pt C) = 6.60 mPD

Elevation of outlet point, Ex. CP14 = 4.80 mPD

Average fall, H = $(z_1 - z_2)/L_s \times 100$
 = 2.14 m per 100m

From TGN30

$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$
 = 5.93 min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$i = 310 \text{ mm/hr}$
 $Q = \frac{kiA}{60} \times 1.16$
 11934 lit/min

From TGN 43A1

For existing 375 UC with 1 in 100 gradient

Maximum capacity = 13500 lit/min > 11934 o.k.

The corresponding velocity = 2.00 m/s < 4 o.k.

Project No.: Drainage Design at Chim Uk, Tai Po Date: 14-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of existing 225UC

Catchment area,	A2	=	456	m ²	Assume k = 0.95 for paved surface
Total Catchment, A=	A2 x 0.95	=	433.2	m ³	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)
 k = Runoff coefficient
 i = Design mean intensity of rainfall (mm/hr)
 A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Y	(Ld) =	70.00	m
Shortest distance from summit point to outlet, Pt Y	(Ls) =	53.00	m

Elevation of remote point (Pt B)	=	6.60	mPD
Elevation of outlet point, Pt Y	=	4.35	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100
	=	4.25 m per 100m

From TGN30

T _c	=	0.14465 x L _d / (H ^{0.2} x A ^{0.1})	
	=	4.13	min

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

i	=	340	mm/hr
Q	=	kiA/60	x 1.16
		2848	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3510	lit/min	>	2848	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po
Prepared by: Ray Cheng

Date: 7-Feb-25

Check for the drainage capacity of existing 225UC

Catchment area,	A3	=	715	m ²	Assume k = 0.3 for unpaved surface
		=	715 x 0.3	m ²	
		=	214.5	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt Z	(Ld) =	69.00	m
Shortest distance from summit point to outlet, Pt Z	(Ls) =	55.00	m

Elevation of remote point (Pt A)	=	6.60	mPD
Elevation of outlet point, Pt Z	=	5.92	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100
	=	1.24 m per 100m

From TGN30

T _c	=	0.14465 x L _d / (H ^{0.2} x A ^{0.1})	
	=	5.59	min

Assume a 1 in 50 year design rainfall return period for rural area
From Geo-Manual (Fig 8.2)

i	=	315	mm/hr
Q	=	kiA/60	x 1.16
		1306	lit/min

From TGN 43A1

For existing 225 UC with 1 in 100 gradient

Maximum capacity	=	3510	lit/min	>	1306	o.k.
The corresponding velocity	=	1.40	m/s	<	4	o.k.

Project No.: Drainage Design at Chim Uk, Tai Po Date: 16-Dec-24
 Prepared by: Ray Cheng

Check for the drainage capacity of proposed 450UC

Catchment area,	A1	=	2085	m ²	Assume k = 0.95 for paved surface
	A2	=	456	m ²	
	A4	=	550	m ²	
Total Catchment Area, A = (A1+A2+A4) x 0.95 =			2936.45	m ²	

Use Rational Method from Geo-Manual

$$Q = kiA/3600$$

where,

Q = Maximum runoff (lit/sec)

k = Runoff coefficient

i = Design mean intensity of rainfall (mm/hr)

A = Total catchment area (m²)

Longest distance from summit point to outlet, Pt W	(Ld) =	161.00	m
Shortest distance from summit point to outlet, Pt W	(Ls) =	140.00	m

Elevation of remote point (Pt C)	=	6.60	mPD
Elevation of outlet point, Pt W	=	4.20	mPD

Average fall, H	=	(z ₁ -z ₂)/L _s x 100	
	=	1.71	m per 100m

From TGN30

$$T_c = 0.14465 \times L_d / (H^{0.2} \times A^{0.1})$$

$$= 9.41 \text{ min}$$

Assume a 1 in 50 year design rainfall return period for rural area
 From Geo-Manual (Fig 8.2)

$$i = 270 \text{ mm/hr}$$

$$Q = \frac{kiA}{60} \times 1.16$$

$$= 15038 \text{ lit/min}$$

From TGN 43A1

For proposed 450 UC with 1 in 100 gradient

Maximum capacity	=	22500	lit/min	>	15038	o.k.
The corresponding velocity	=	2.00	m/s	<	4	o.k.

The capacity of the existing 375mm dia. Underground pipe = 28,728 lit/min > 15038 lit/min O.K.

GEO Technical Guidance Note No. 30 (TGN 30)
Updated Intensity-Duration-Frequency Curves with Provision for
Climate Change for Slope Drainage Design

Issue No.: 2	Revision: -	Date: 23.10.2018	Page: 3 of 4
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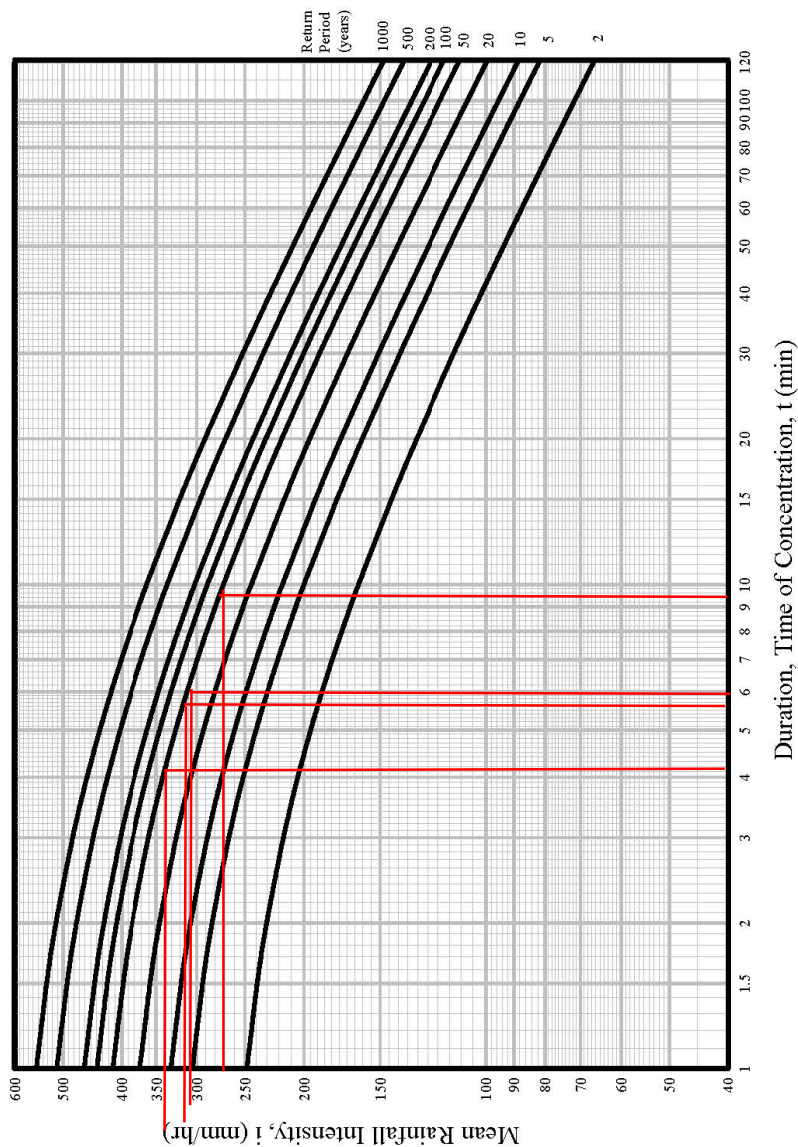


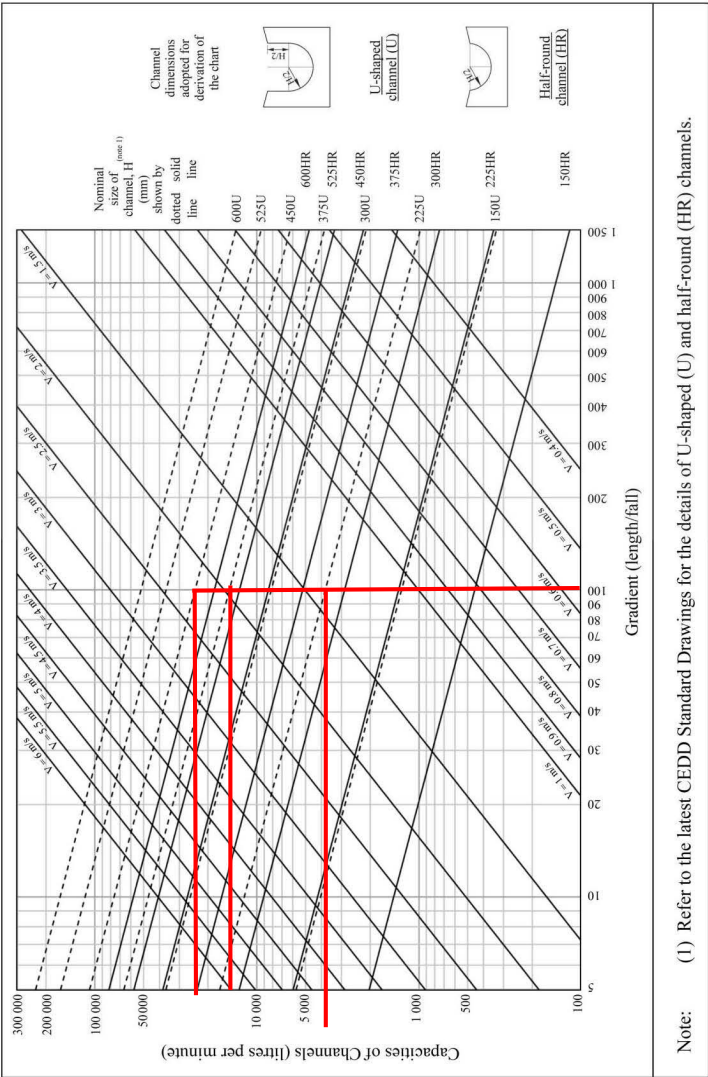
Figure 1 – Updated Intensity-Duration-Frequency Curves

- Notes:
1. These IDF curves are to supersede those given in Figure 8.2 of the Geotechnical Manual for Slopes (GCO, 1984).
 2. These IDF curves have not incorporated any projected climate change effects. Except for temporary slope drainage design, the mean rainfall intensity given by these IDF curves shall be increased by 13.8% for incorporating climate change effects.
 3. The mathematical formulae of these IDF curves are shown in Table 1 of Annex TGN 30 A1.

GEO Technical Guidance Note No. 43 (TGN 43)
Guidelines on Hydraulic Design of U-shaped and Half-round Channels on Slopes

Issue No.: 1 Revision: - Date: 05.06.2014 Page: 3 of 3

Figure 1 - Chart for the rapid design of U-shaped and half-round channels up to 600 mm



Since 10% reduction would be considered for deposition of sediment, the capacity of the proposed 225UC should be $3,900 \times 0.9 = 3,510 \text{ lit/min}$, 375UC should be $15,000 \times 0.9 = 13,500 \text{ lit/min}$, 450UC should be $25,000 \times 0.9 = 22,500 \text{ lit/min}$

Upstream level of SWD1043960 is +2.0mPD
 Downstream level of SWD1043960 is +1.6mPD
 Length of SWD1043960 is 5.4m
 Gradient of SWD1043960 = $5.4/(2-1.6) = 1:13.5$

ks = 0.600mm
 i = 0.004 to 0.1
 ie hydraulic gradient =
 1 in 250 to 1 in 10

Water (or sewage) at 15°C
 full bore conditions.
 velocities in m/s
 discharges in m³/s

18
 continued

The Capacity of SWD1043960 = $0.532 \times 1000 \times 60 \times 0.9 = 28,728 \text{ l/min} > 15,038 \text{ l/min}$

Gradient	Pipe diameters in mm:											
	350	375	400	450	500	525	600	675	700	750	800	825
0.02000 1/ 50	2.456 0.236	2.566 0.283	2.673 0.336	2.879 0.458	3.076 0.604	3.171 0.687	3.444 0.975	3.710 1.328	3.795 1.461	3.962 1.750	4.123 2.073	4.203 2.247
0.02200 1/ 45	2.577 0.248	2.692 0.297	2.804 0.352	3.020 0.480	3.227 0.634	3.327 0.720	3.617 1.023	3.892 1.393	3.981 1.532	4.156 1.836	4.325 2.174	4.409 2.357
0.02400 1/ 42	2.692 0.259	2.812 0.311	2.929 0.368	3.155 0.502	3.371 0.662	3.476 0.752	3.778 1.068	4.066 1.455	4.159 1.601	4.341 1.918	4.518 2.271	4.605 2.462
0.02600 1/ 38	2.803 0.270	2.928 0.323	3.050 0.383	3.284 0.522	3.509 0.689	3.618 0.783	3.933 1.112	4.233 1.515	4.329 1.666	4.519 1.996	4.703 2.364	4.794 2.563
0.02800 1/ 36	2.909 0.280	3.039 0.336	3.165 0.398	3.409 0.542	3.642 0.715	3.755 0.813	4.082 1.154	4.393 1.572	4.493 1.729	4.690 2.072	4.882 2.454	4.975 2.660
0.03000 1/ 33	3.012 0.290	3.146 0.347	3.277 0.412	3.529 0.561	3.770 0.740	3.888 0.842	4.225 1.195	4.548 1.627	4.652 1.790	4.855 2.145	5.053 2.540	5.151 2.753
0.03200 1/ 31	3.111 0.299	3.250 0.359	3.385 0.425	3.645 0.580	3.895 0.765	4.015 0.869	4.365 1.234	4.697 1.681	4.805 1.849	5.015 2.216	5.220 2.624	5.320 2.844
0.03400 1/ 29	3.207 0.309	3.350 0.370	3.489 0.438	3.758 0.598	4.015 0.788	4.140 0.896	4.500 1.272	4.842 1.733	4.953 1.906	5.170 2.284	5.381 2.705	5.484 2.932
0.03600 1/ 28	3.300 0.318	3.448 0.381	3.591 0.451	3.867 0.615	4.132 0.811	4.260 0.922	4.631 1.309	4.983 1.783	5.097 1.962	5.320 2.350	5.537 2.783	5.644 3.017
0.03800 1/ 26	3.391 0.326	3.542 0.391	3.690 0.464	3.974 0.632	4.245 0.834	4.377 0.948	4.758 1.345	5.120 1.832	5.237 2.015	5.466 2.415	5.689 2.860	5.799 3.100
0.04000 1/ 25	3.480 0.335	3.635 0.401	3.786 0.476	4.077 0.648	4.356 0.855	4.491 0.972	4.882 1.380	5.253 1.880	5.374 2.068	5.609 2.478	5.837 2.934	5.950 3.180
0.04200 1/ 24	3.566 0.343	3.725 0.411	3.880 0.488	4.178 0.665	4.464 0.877	4.602 0.996	5.005 1.415	5.384 1.926	5.507 2.119	5.747 2.539	5.982 3.007	6.097 3.259
0.04400 1/ 23	3.650 0.351	3.813 0.421	3.972 0.499	4.277 0.680	4.569 0.897	4.711 1.020	5.121 1.448	5.511 1.972	5.637 2.169	5.883 2.599	6.123 3.078	6.241 3.336
0.04600 1/ 22	3.733 0.359	3.899 0.431	4.061 0.510	4.374 0.696	4.672 0.917	4.817 1.043	5.236 1.481	5.635 2.016	5.764 2.218	6.016 2.658	6.261 3.147	6.381 3.411
0.04800 1/ 21	3.813 0.367	3.983 0.440	4.149 0.521	4.468 0.711	4.773 0.937	4.921 1.065	5.349 1.512	5.756 2.060	5.888 2.266	6.145 2.715	6.396 3.215	6.519 3.485
0.05000 1/ 20	3.892 0.374	4.066 0.449	4.235 0.532	4.560 0.725	4.872 0.957	5.023 1.087	5.460 1.544	5.875 2.102	6.010 2.313	6.272 2.771	6.528 3.281	6.654 3.557
0.05500 1/ 18	4.083 0.393	4.265 0.471	4.442 0.558	4.784 0.761	5.111 1.003	5.269 1.141	5.727 1.619	6.163 2.205	6.304 2.426	6.579 2.907	6.848 3.442	6.979 3.731
0.06000 1/ 17	4.265 0.410	4.455 0.492	4.640 0.583	4.997 0.795	5.338 1.048	5.504 1.191	5.982 1.692	6.437 2.304	6.585 2.534	6.872 3.036	7.153 3.595	7.290 3.897
0.06500 1/ 15	4.440 0.427	4.638 0.512	4.830 0.607	5.202 0.827	5.557 1.091	5.729 1.240	6.227 1.761	6.701 2.398	6.854 2.638	7.154 3.160	7.445 3.742	7.589 4.057
0.07000 1/ 14	4.608 0.443	4.813 0.532	5.013 0.630	5.399 0.859	5.767 1.132	5.946 1.287	6.463 1.827	6.954 2.489	7.113 2.738	7.424 3.280	7.727 3.884	7.876 4.210
0.07500 1/ 13	4.770 0.459	4.983 0.550	5.190 0.652	5.589 0.889	5.970 1.172	6.155 1.332	6.690 1.892	7.199 2.576	7.364 2.834	7.685 3.395	7.999 4.021	8.153 4.358
0.08000 1/ 13	4.927 0.474	5.147 0.568	5.360 0.674	5.772 0.918	6.167 1.211	6.358 1.376	6.910 1.954	7.436 2.661	7.606 2.927	7.938 3.507	8.262 4.153	8.420 4.501
0.08500 1/ 12	5.079 0.489	5.306 0.586	5.526 0.694	5.951 0.946	6.357 1.248	6.554 1.419	7.123 2.614	7.665 2.743	7.840 3.017	8.183 3.615	8.516 4.281	8.680 4.640
							7.320	7.888	8.068	8.420	8.764	8.932

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy ☐Confidential

Charlotte Tsz Wing WUN/PLAND

寄件者: cheung fennie [REDACTED]
寄件日期: 2025年02月12日星期三 12:00
收件者: Charlotte Tsz Wing WUN/PLAND
主旨: Re: 回覆: Planning Application No. A/NE-TK/800 - Comments from the Drainage Services Department

類別: Internet Email

Dear Miss Wun,

綠化地帶不填土位置現有石屎部份，如申請獲批後會清理，謝謝！

Best regards,
Fennie Cheung

**Relevant Extract of Town Planning Board Guidelines for
Application for Development within Green Belt Zone
under Section 16 of the Town Planning Ordinance
(TPB-PG No. 10)**

- (a) there is a general presumption against development (other than redevelopment) in a “GB” zone. In general the Board will only be prepared to approve applications for development in the context of requests to rezone to an appropriate use;
- (b) an application for new development in a “GB” zone will only be considered in exceptional circumstances and must be justified with very strong planning grounds. The scale and intensity of the proposed development including the plot ratio, site coverage and building height should be compatible with the character of surrounding areas;
- (c) the design and layout of any proposed development should be compatible with the surrounding area. The development should not involve extensive clearance of existing natural vegetation, affect the existing natural landscape, or cause any adverse visual impact on the surrounding environment;
- (d) the vehicular access road and parking provision proposed should be appropriate to the scale of the development and comply with relevant standards. Access and parking should not adversely affect existing trees or other natural landscape features. Tree preservation and landscaping proposals should be provided;
- (e) the proposed development should not overstrain the capacity of existing and planned infrastructure such as sewerage, roads and water supply. It should not adversely affect drainage or aggravate flooding in the area;
- (f) the proposed development should not be susceptible to adverse environmental effects from pollution sources nearby such as traffic noise, unless adequate mitigating measures are provided, and it should not itself be the source of pollution; and
- (g) any proposed development on a slope or hillside should not adversely affect slope stability.

Previous s.16 Application

Rejected Application

Application No.	Proposed Use/Development	Date of Consideration	Rejection Reason
A/NE-TK/314	Proposed House (New Territories Exempted House - Small House)	25.2.2011 (on review)	R1

Rejection Reason

- R1. The proposed development did not comply with the Interim Criteria for Consideration of Application for New Territories Exempted House/Small House in New Territories as the site encroached onto the possible future road widening area.

Similar s.16 Applications

Approved Applications

Application No.	Proposed Uses/Developments	Zoning(s)	Date of Consideration
A/NE-TK/714 ¹	Access Road for Connecting the Adjoining Temporary Private Car Parking Spaces for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/715 ²	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/716 ³	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/717 ⁴	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/718 ⁵	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/719 ⁶	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/720 ⁷	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/721 ⁸	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/722 ⁹	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/723 ¹⁰	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/724 ¹¹	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021

Application No.	Proposed Uses/Developments	Zoning(s)	Date of Consideration
A/NE-TK/725 ¹²	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/726 ¹³	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/727 ¹⁴	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/728 ¹⁵	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/729 ¹⁶	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/730 ¹⁷	Temporary Private Vehicle Park (Private Cars Only) for a Period of 3 Years	“GB”	15.10.2021
A/NE-TK/731 ¹⁸	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/733 ¹⁹	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	15.10.2021
A/NE-TK/802 ¹	Access Road for Connecting the Adjoining Temporary Private Car Parking Spaces for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/803 ²	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/804 ³	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/805 ⁴	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/806 ⁵	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024

Application No.	Proposed Uses/Developments	Zoning(s)	Date of Consideration
A/NE-TK/807 ⁶	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/808 ⁷	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/809 ⁸	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/810 ⁹	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/811 ¹⁰	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/812 ¹¹	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/813 ¹²	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/814 ¹³	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/815 ¹⁴	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/816 ¹⁵	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/817 ¹⁶	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/818 ¹⁷	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024
A/NE-TK/819 ¹⁸	Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of Three Years	“GB”	6.12.2024

Application No.	Proposed Uses/Developments	Zoning(s)	Date of Consideration
A/NE-TK/820 ¹⁹	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	6.12.2024

Remarks

- ¹: The site of previous application No. A/NE-TK/714 is the same site as current application No. A/NE-TK/802).
- ²: The site of previous application No. A/NE-TK/715 is the same site as current application No. A/NE-TK/803.
- ³: The site of previous application No. A/NE-TK/716 is the same site as current application No. A/NE-TK/804.
- ⁴: Part of the site under previous application No. A/NE-TK/717 is the same site as current application (No. A/NE-TK/805.
- ⁵: The site of previous application No. A/NE-TK/718 is the same site as current application No. A/NE-TK/806.
- ⁶: The site of previous application No. A/NE-TK/719 is the same site as current application No. A/NE-TK/807.
- ⁷: The site of previous application No. A/NE-TK/720 is the same site as current application No. A/NE-TK/808.
- ⁸: The site of previous application No. A/NE-TK/721 is the same site as current application No. A/NE-TK/809.
- ⁹: The site of previous application No. A/NE-TK/722 is the same site as current application No. A/NE-TK/810.
- ¹⁰: The site of previous application No. A/NE-TK/723 is the same site as current application No. A/NE-TK/811.
- ¹¹: The site of previous application No. A/NE-TK/724 is the same site as current application No. A/NE-TK/12.
- ¹²: The site of previous application No. A/NE-TK/725 is the same site as current application No. A/NE-TK/813.
- ¹³: The site of previous application No. A/NE-TK/726 is the same site as current application No. A/NE-TK/814.
- ¹⁴: The site of previous application No. A/NE-TK/727 is the same site as current application No. A/NE-TK/815.
- ¹⁵: The site of previous application No. A/NE-TK/728 is the same site as current application No. A/NE-TK/816.
- ¹⁶: The site of previous application No. A/NE-TK/729 is the same site as current application No. A/NE-TK/817.
- ¹⁷: The site of previous application No. A/NE-TK/730 is the same site as current application No. A/NE-TK/818.
- ¹⁸: The site of previous application No. A/NE-TK/731 is the same site as current application No. A/NE-TK/819.
- ¹⁹: The site of previous application No. A/NE-TK/733 is the same site as current application No. A/NE-TK/820.

Rejected Application

Application No.	Proposed Uses/Developments	Zoning(s)	Date of Consideration	Rejection Reasons
A/NE-TK/758	Temporary Private Vehicle Park (Private Cars Only) for a Period of Three Years	“GB”	14.10.2022	R1 and R2

Rejection Reasons

- R1. The applied use is not in line with the planning intention of the “Green Belt” zone, which is primarily for defining the limits of urban and sub-urban development areas by natural features and to contain urban sprawl as well as to provide passive recreational outlets. There is a general presumption against development within this zone. There is no strong planning justification in the submission for a departure from the planning intention, even on a temporary basis.
- R2. The applied use is not in line with the Town Planning Board Guidelines for ‘Application for Development within Green Belt Zone under Section 16 of the Town Planning Ordinance’ (TPB PG-No. 10) in that there are no strong planning grounds in the submission to merit an exceptional consideration and the applicant fails to demonstrate that the applied use does not involve extensive clearance of natural vegetation.

Government Departments' General Comments

1. Lands Administration

Comments of the District Lands Officer/Tai Po, Lands Department (DLO/TP, LandsD):

- no objection to the application;
- the Site comprises 53 Old Schedule Agricultural Lots all in D.D. 26, including Lot 291 s.B ss.1 with which a Building Licence No. 220/2007 was issued covering the said lot and Lot No. 291 s.B RP (not included in this application). Lot 291 s.B ss.1 in D.D. 26 is permitted under Building Licence No. 220/2007 to erect one building for non-industrial purposes. Except for the building site, the remainder of the said lots together with all Old Schedule Agricultural Lots under the Block Government Lease which contain the restriction that no structures are allowed to be erected without the prior approval of the Government;
- a recent inspection revealed that there were fences and gate found erected at the ingress and egress of the Site on government land. The concerned government land was enclosed and occupied as shown on **Attachment 1 of Appendix VI**. The applicant should clarify whether the concerned Government Land used as vehicular access will be included to the application;
- no permission is given for occupation of the government land adjoining the Site. Any occupation of Government land without Government's prior approval is not allowed;
- there is no guarantee to the grant of a right of way to the Site or approval of the emergency vehicular access thereto; and
- his advisory comments are at **Appendix VI**.

2. Nature Conversation

Comments of the Director of Agriculture, Fisheries and Conservation (DAFC):

- considering that the Site is formed and occupied by some existing structures, he has no comment on the application.

3. Landscape

Comments of the Chief Town Planner/Urban Design and Landscape, Planning Department (CTP/UD&L, PlanD):

- based on the aerial photo of 2023, the Site is situated in an area of rural coastal plains landscapes character comprising low rise residential development, village houses, car parks, vegetated area and clusters of trees groups. Significant impact on the landscape character arising from the applied use is not anticipated; and

- based on the recent site photos, the Site is hard paved and currently occupied by a carpark. No sensitive landscape resources is observed within the Site. Given that part of the Site has been cleared of vegetation and hard-paved, significant adverse landscape impact on the existing landscape resources arising from the applied use is not anticipated. She has no objection to the application from landscape planning perspective.

4. **Environment**

Comments of the Director of Environmental Protection (DEP):

- no objection to the application from the environmental perspective;
- no environmental complaint in relation to the Site was received in the past three years; and
- his advisory comments are at **Appendix VI**.

5. **Drainage**

Comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD):

- an approval condition on submission and implementation of drainage proposal for the Site is recommended to ensure that it will not cause adverse drainage impact to the adjacent areas;
- public sewerage are not available near the Site; and
- her advisory comments are at **Appendix VI**.

6. **Fire Safety**

Comments of the Director of Fire Services (D of FS):

- no in-principle objection to the application subject to fire service installations and water supplies for fire-fighting being provided to his satisfaction; and
- his advisory comments are at **Appendix VI**.

7. **Buildings Matters**

Comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):

- no objection to the application;
- there is no record of approval granted by the Building Authority (BA) for the existing structures at the Site;
- it is noted that movable canopies and land filling works is/are applied in the application. Before any new building works (including container / open sheds as temporary buildings, demolition and land filling, etc.) are to be carried out on the Site, prior approval and

consent of the BA should be obtained, otherwise they are unauthorized building works under the Buildings Ordinance (BO). An Authorized Person should be appointed as the coordinator for the proposed building works in accordance with the BO; and

- his advisory comments are at **Appendix VI**.

8. Other Departments

The following departments have no objection to/no comment on the application:

- Chief Highway Engineer/New Territories East, Highways Department (CHE/NTE, HyD);
- Chief Engineer/Construction, Water Supplies Department (CE/C, WSD);
- Head of Geotechnical Engineering Office, Civil Engineering and Development Department (H(GEO), CEDD);
- Project Manager/North, Civil Engineering and Development Department (PM/N, CEDD);
and
- District Officer (Tai Po), Home Affairs Department (DO(TP), HAD).

**Appendix VI of RNTPC
Paper No. A/NE-TK/800B**

Recommended Advisory Clauses

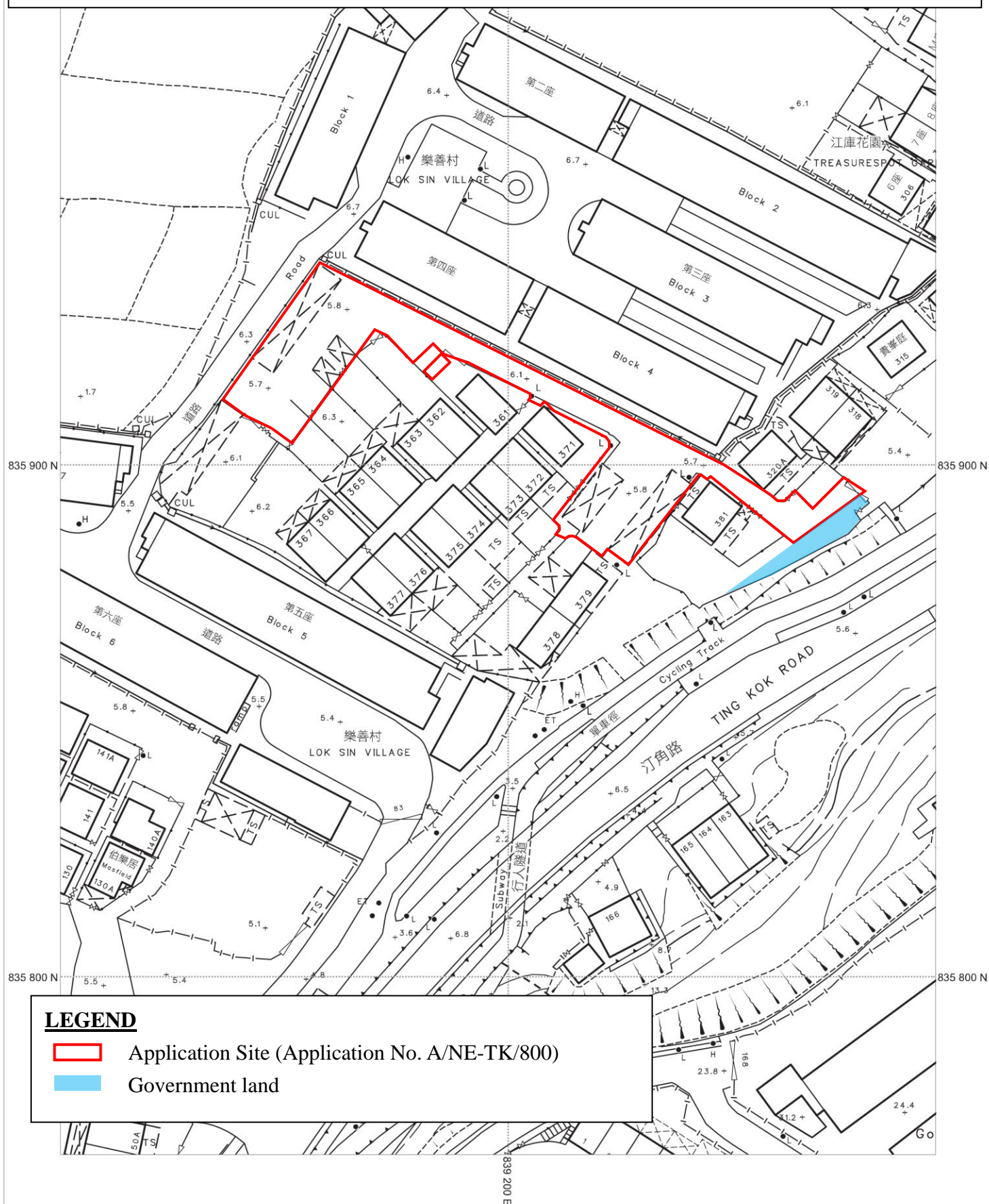
- (a) prior planning permission should have been obtained before commencing the applied use at the Site;
- (b) to resolve any land issue relating to the applied use with other concerned owner(s) of the Site;
- (c) failure to reinstate the “Green Belt” portion of the Site as required under the relevant approval condition upon expiry of the planning permission might constitute an unauthorized development under the Town Planning Ordinance and be subject to enforcement and prosecution actions;
- (d) to note the comments of the District Lands Officer/Tai Po, Lands Department (DLO/TP, LandsD) that:
 - (i) a recent inspection revealed that there were fences and gate found erected at the ingress and egress of the Site on government land. The concerned government land was enclosed and occupied as shown on **Attachment 1**. The applicant should clarify whether the concerned Government Land used as vehicular access will be included to the application;
 - (ii) no permission is given for occupation of the government land adjoining the Site. Any occupation of the government land without the Government’s prior approval is not allowed;
 - (iii) the lots owner(s) are required to submit applications for Short Term Waiver (“STW”) if they wish to erect structures on the Site. LandsD will consider the STW applications in accordance with the established procedures and guidelines. However, there is no guarantee at this stage that the STW applications would be approved. If the application is approved by LandsD acting in the capacity as landlord at its sole discretion, such approval will be subject to such terms and conditions as may be imposed by LandsD including the payment of, waiver fee and administrative fee as considered appropriate. Besides, given the applied use is temporary in nature, only erection of temporary structures will be considered;
 - (iv) the applicant will likely make use of the adjoining unleased/unallocated government land as vehicles access to and from Ting Kok Road. The maintenance and management responsibility of the said government land and any other government land leading to the Site should be sorted out with the relevant Government departments, prior to the use of access purpose. Moreover, access to the Site may also fall on adjoining private lots all in D.D. 26. The applicant should sort out the relevant issues with the lots owners concerned; and
 - (v) there is no guarantee to the grant of a right of way to the Site or approval of the emergency vehicular access thereto;
- (e) to note the comments of the Commissioner for Transport (C for T) that the concerned area and the village road connecting to the Site from Ting Kok Road is not managed by Transport

Department. Comments from the management and maintenance party of the concerned area and local road should be sought. In addition, it is noted that in order to access the parking spaces, vehicles may have to encroach onto the adjacent private lots. The applicant shall make their own arrangement with the concerned land owners for using the road, and its land status, management and maintenance responsibilities should be clarified with the relevant lands and maintenance authorities accordingly in order to avoid potential land disputes;

- (f) to note the comments of the Director of Environmental Protection (DEP) that the applicant is reminded to follow the relevant mitigation measures and requirements in Code of Practice on Handling the Environmental Aspects of Temporary Uses and Open Storage Sites and to meet the statutory requirements under relevant pollution control ordinances;
- (g) to note the comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD) that:
 - (i) while there are DSD's public stormwater drains in this area, the applicant should have its own stormwater collection and discharge system to cater for the runoff generated within the Site and overland flow from surrounding of the Site, e.g. surface channel of sufficient size along the perimeter of the Site; sufficient openings should be provided at the bottom of the boundary wall/fence to allow surface runoff to pass through the Site if any boundary wall/fence are to be erected. Any existing flow path affected should be re-provided. The applicant should neither obstruct overland flow nor adversely affect the existing natural streams, village drains, ditches and the adjacent areas. The applicant is required to maintain the drainage systems properly and rectify/modify the nearby existing/original drainage systems if they are found to be inadequate or ineffective to accommodate the additional runoff arisen from the development of the Site. The applicant shall also be liable for and shall indemnify claims and demands arising out of damage or nuisance caused by failure or ineffectiveness of the modified drainage systems caused by their works; and
 - (ii) the applicant shall resolve any conflict/disagreement with relevant lot owner(s) and seek LandsD's permission for laying new drains/channels and/or modifying/upgrading existing ones in other private lots or on government land (where required) outside the Site;
- (h) to note the comments of the Director of Fire Services (D of FS):
 - (i) the applicant should submit relevant layout plans incorporated with the proposed fire service installations (FSIs) for approval. The layout plans should be drawn to scale and depicted with dimensions and nature of occupancy and the location of the proposed FSIs to be installed should be clearly marked; and
 - (ii) the applicant is reminded that if the structure(s) is required to comply with the Buildings Ordinance (BO) (Cap. 123), detailed fire service requirements will be formulated upon receipt of formal submission of general building plans. If there is electric vehicle charging station involved, the requirement of Fireman's Emergency Switch at **Attachment 2** should be observed;
- (i) to note the comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):

- (iii) the Site shall be provided with means of obtaining access thereto from a street and emergency vehicular access in accordance with Regulations 5 and 41D of the Building (Planning) Regulations (B(P)R) respectively;
- (iv) if the existing structures (not being a New Territories Exempted House) is/are erected on lease land without the approval Building Authority, they are unauthorised building works (UBW) on the Site under the BO and should not be designated for any applied use under the application;
- (v) for the UBW erected on leased land, enforcement action may be taken by BD to effect their removal in accordance with the prevailing enforcement policy against UBW as and when necessary. The granting of any planning approval should not be constructed as an acceptance of any existing building works or UBW on the Site under the BO;
- (vi) any temporary shelters or converted container for office, storage, washroom or other uses are considered as temporary buildings are subject to control of Part VII of the B(P)R; and
- (vii) detailed checking under the BO will be carried out at the building plan submission stage.

**Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of 3 Years
and Associated Filling of Land
Various Lots in D.D. 26, Shuen Wan Chim Uk, Tai Po, N.T.
(Application No. A/NE-TK/800)**



Requirements for the Fireman's Emergency Switch

1. A fireman's emergency switch conforming EMSD's Code of Practice shall be provided to cut off the power supply of **all** EV charging facilities within the car parking facilities.
2. The switch shall be situated in a conspicuous position, yet out of reach of the public in general. Thus, switch(es) provided at vehicle entrance(s) shall be positioned no more than 3m but not less than 2.5 from ground level. Where more than one fireman's emergency switch is installed on any one building, such switches shall be clearly marked to distinguish one from another.
3. In case the switch is installed at a location other than the vehicle entrance, notice plate(s) shall be provided at conspicuous location(s) of vehicle entrance(s) acceptable to the Director of Fire Services to indicate the location of fireman's emergency switch.
4. The 'ON' and 'OFF' position of the fireman's emergency switch shall be conventional (i.e. push upward – 'OFF'; push downward – 'ON') and clearly indicated by lettering legible to a person standing on the ground at the intended site.
5. The switch is to be affixed on a board approximately 300 mm long by 250 mm wide, which is painted white and edged with a 50 mm red border. The inscription 'EV CHARGING FACILITIES - FIREMAN'S SWITCH' in English is to be painted on the top and '電動車充電設施 - 消防員開關掣' in Chinese at the bottom of the board in black. The switch is to be positioned in the middle of the board.

12

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

A/NE-TK/800

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

欲申請土地可作私人海車地盤，
也可以想加建蓬頂，方便雨天上落
車，因家有老人輪椅出入會方便些。

「提意見人」姓名/名稱 Name of person/company making this comment _____

簽署 Signature _____

日期 Date 20 Aug 2024



致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

To : Secretary, Town Planning Board

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

A/NE-TK/800 Received on 13/11/2024 , 12/11/2024



意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

① 因香港土地不足，這樣可以發揮土地的價值，作出最適
合用途，作為私家車同客貨車停泊，比起空置還有更大發揮價值

② 可以方便有關主戶出入、上班、小孩上學、購物同老人家睇
醫生，讓其他地方居民可以較大概會同方便使用公共交通，
系統獲得雙贏效果。

③ 因停車場比起同附近樂善村中轉屋所作地方不多簡直九牛一毛
對環境不會產生任何壞影響。

「提意見人」姓名/名稱 Name of person/company making this comment

Christine Chan

簽署 Signature

日期 Date

11 Dec 2024

至城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk

21

To: Secretary, Town Planning Board

By hand or post: 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax: 2877 0245 or 2522 8426

By e-mail: tpbpd@pland.gov.hk

有關的規劃申請編號：The application no. to which the comment relates
A/NE-TK/800 Received on 13/11/2024 及 12/11/2024



意見詳情 (如有需要，請另頁說明)：

Details of the Comment (use separate sheet if necessary)

「提意見人」姓名/名稱 Name of person/company making this comment

CHAN KWONG TAK

簽名 Signature

日期 Date



11 Dec 2024



There is constant debate between the need for development versus environment protection. After evaluating the pros and cons, I support the application of A/NE-TK/800.

1. Traffic is always a problem for most of the residents living along Ting Kok Road especially those in Shuen Wan Chim Uk. The situation deteriorates after the completion of Lok Sin Village which adds more than 2,000 residents to this district. Another transitional housing project near The Beverly Hills is to be completed too. Bus route 75K is the only bus serving between Tai Po Market Station and Tai Mei Tuk. However, the bus is not frequent as shown below from the Kowloon Motor Bus mobile app.

← 75K To TAI PO MARKET STATION BU... 大埔 寶珠站	
Arrival	Journey
Timetable	
Notice	
75K To TAI PO MARKET STATION BUS TERMINUS	
Mondays To Fridays	
	Minutes
05:25-05:45	20
05:45-09:10	10-15
09:10-19:55	15-20
19:55-23:40	25
Saturdays	
	Minutes
05:25-22:50	15-20
22:50-23:40	25
Sundays & Holidays	
	Minutes
05:25-06:15	25
06:15-21:10	15-20
21:10-23:40	25

There are also mini buses 20C and 20B available but usually they are already full at Shuen Wan Chim Uk when they depart from Tai Mei Tuk and Tung Tsz. Their schedule is infrequent too.

 											
New Territories GMB Route No. 20C Tai Po Market Station <-> Tai Mei Tuk											
Main roads en route											
From Tai Po Market Station via Nga Wan Road, Nam Wan Road, Tai Po Tai Wo Road, Yuen Shin Road and Ting Kok Road. From Tai Mei Tuk via Ting Kok Road, Yuen Shin Road, Tai Po Tai Wo Road, Nam Wan Road and Nga Wan Road.											
<div style="border: 1px solid black; border-radius: 50%; width: 150px; height: 150px; display: flex; align-items: center; justify-content: center; margin: 0 auto;"> <div style="text-align: center;"> <p>12 DEC 2024</p> <p>Town Planning Board</p> </div> </div>											
Tai Po Market Station <-> Tai Mei Tuk											
<table border="1"> <thead> <tr> <th>From Tai Po Market Station</th><th>From Tai Mei Tuk</th><th>Frequency (Minutes)</th></tr> </thead> <tbody> <tr> <td>Mondays to Fridays (Public Holidays Excepted) 07:00 am - 10:00 pm</td><td>07:00 am - 08:00 pm</td><td>12 - 15</td></tr> <tr> <td>Saturdays, Sundays and Public Holidays 09:00 am - 09:00 pm</td><td>09:00 am - 07:00 pm</td><td>12 - 15</td></tr> </tbody> </table>			From Tai Po Market Station	From Tai Mei Tuk	Frequency (Minutes)	Mondays to Fridays (Public Holidays Excepted) 07:00 am - 10:00 pm	07:00 am - 08:00 pm	12 - 15	Saturdays, Sundays and Public Holidays 09:00 am - 09:00 pm	09:00 am - 07:00 pm	12 - 15
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New Territories GMB Route No. 20B Tai Po Market Station <-> Tung Tsz											
Main roads en route											
From Tai Po Market Station via Nga Wan Road, Nam Wan Road, Wan Tau Street, Heung Sze Wui Street, Po Heung Street, Po Heung Bridge, On Cheung Road, On Chee Road, Ting Kok Road and Tung Tsz Road. From Tung Tsz via Tung Tsz Road, (Tung Tsz Shan Road, Tung Tsz Road), Ting Kok Road, On Chee Road, On Cheung Road, Po Heung Bridge, Po Heung Street, Kwong Fuk Road, Wan Tau Street, Nam Wan Road and Nga Wan Road. Remark: *Services are operated via Tung Tsz Shan Road after 10.00 am daily.											
Tai Po Market Station <-> Tung Tsz											
<table border="1"> <thead> <tr> <th>From Tai Po Market Station</th><th>From Tung Tsz</th><th>Frequency (Minutes)</th></tr> </thead> <tbody> <tr> <td>Mondays to Saturdays (Public Holidays Excepted) 05:25 am - 11:40 pm</td><td>05:50 am - 12:05 am</td><td>10 - 20</td></tr> <tr> <td>Sundays and Public Holidays 05:25 am - 11:40 pm</td><td>05:50 am - 12:05 am</td><td>12 - 20</td></tr> </tbody> </table>			From Tai Po Market Station	From Tung Tsz	Frequency (Minutes)	Mondays to Saturdays (Public Holidays Excepted) 05:25 am - 11:40 pm	05:50 am - 12:05 am	10 - 20	Sundays and Public Holidays 05:25 am - 11:40 pm	05:50 am - 12:05 am	12 - 20
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Private car provides an alternative to the residents but there is no public carpark nearby. If the application is successful, the residents can drive their private cars which not only make their life easier, but also help those who rely on public transport to have more chance to get on the bus or mini bus.

2. Turing the land into carpark should have minimal impact to the environment. The area is already pavement for the residents and for fire trucks and ambulance to enter in case of emergency. Compared to the construction of the nearby Lok Sin village which destructs hundreds of trees, the environment impact is insignificant.

Although the area applied is classified as green belt zone, it serves the best interest of the nearby residents to allow it as temporary parking lots. It helps realize the value of the land and provide benefits to the residents.

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

From:
Sent: 2024-08-28 星期三 15:45:53
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: Opposition to Planning Application A/NE-TK/800

To: Secretary, Town Planning Board

Subject: Opposition to Planning Application A/NE-TK/800

I am writing to formally oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. As a resident and owner of a property directly adjacent to the proposed development area, I have several concerns regarding this project.

1. Access Pathway Blockage to Private Property:

My rights to access my property at _____, is protected by Property Rights under Articles 6 and 105 of the Basic Law. The proposed development will block the only access pathway to my private properties adjacent to the proposed development area.

- a. The proposed development will block the entrance to my house which is a violation of my legal rights. I reserve the rights to take legal action to enforce these rights and remove the obstruction.
- b. The Building Ordinance regulates construction activities in Hong Kong. The current blockage (a gate has already been build) is unauthorized construction. Please investigate and enforce the law and protect property owner's right.
- c. Blocking access to my property is nuisance and trespass. I reserve the rights to seek legal remedies through the courts to remove the obstruction and claim any damages.
- d. This blockage will severely impact the residents' ability to enter and exit their property, posing significant inconvenience and safety risks, as well as, imposing significant impact and concern to the homeowners. Blockage to access for private property for the interest of the applicant is unlawful, inconsiderate and unreasonable.

2. Property Values:

There is a concern that the development could negatively impact property values in the surrounding area, which is a significant concern for homeowners.

3. Safety and Emergency Vehicle Access:

The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

4. Public Interests:

The development does not align with the broader public interests of our community. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents. Instead, it prioritizes private interests over the collective well-being of the community by occupying the already very limited leisure space for the community.

5. Environmental Impact:

The construction and associated filling of land will likely lead to the destruction of green spaces and wildlife habitats, which are crucial for maintaining the ecological balance in our area. Protecting our environment should be a priority in any development plan.

6. Traffic and Infrastructure:

The increase in traffic due to this development will exacerbate traffic congestion and put additional strain on our already overburdened infrastructure. This could lead to increased pollution and impact the harmony of the community.

7. Community Character:

The scale and design of the proposed development are not in keeping with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

8. Noise and Disturbance:

The parking area and operation directly adjacent to the residential area will cause disturbance to the community. The construction phase and subsequent increase in population will result in higher noise levels and disturbances, affecting the quality of life for existing residents.

I urge the planning committee to consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, the safety of our community, and the sustainability of our environment.

Thank you for your attention to this matter.

Best regards,

Ko Bo Bo

DISCLAIMER:

This e-mail and any attachments are confidential and may be privileged. If you are not the intended recipient, you must not further distribute this message, disclose its contents to any one, use it for any purpose, store, copy, or reproduce in whole or in part in any form, or take any action based on the contents hereof. You should inform the sender by return e-mail and delete this message immediately.

Hong Kong Aircraft Engineering Company Limited 香港飛機工程有限公司

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-134858-89260

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 13:48:58

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

小姐 Miss Ko Bo Bo

意見詳情

Details of the Comment :**Subject: Opposition to Planning Application A/NE-TK/800**

I am writing to formally oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. As a resident and owner of the property directly adjacent to the proposed development area, I have several serious concerns regarding this project.

1. Access Pathway Blockage to Private Property:

The proposed development will block the only access pathway to private properties adjacent to the proposed development area. This blockage will severely impact the residents' ability to enter and exit their property, posing significant inconvenience and safety risks, as well as, imposing significant impact and concern to the homeowners. Blockage to access for private property for the interest of the applicant is inconsiderate and unreasonable.

2. Property Values:

There is a concern that the development could negatively impact property values in the surrounding area, which is a significant concern for homeowners.

3. Safety and Emergency Vehicle Access:

The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

4. Public Interests:

The development does not align with the broader public interests of our community. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents. Instead, it prioritizes private interests over the collective well-being of the community by occupying the already very limited leisure space for the community.

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The construction and associated filling of land will likely lead to the destruction of green spaces and wildlife habitats, which are crucial for maintaining the ecological balance in our area. Protecting our environment should be a priority in any development plan.

6. Traffic and Infrastructure:

The increase in traffic due to this development will exacerbate traffic congestion and put additional strain on our already overburdened infrastructure. This could lead to increased pollution and impact the harmony of the community.

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The scale and design of the proposed development are not in keeping with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

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The parking area and operation directly adjacent to the residential area will cause disturbance to the community. The construction phase and subsequent increase in population will result in higher noise levels and disturbances, affecting the quality of life for existing residents.

I urge the planning committee to consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, the safety of our community, and the sustainability of our environment.

Thank you for your attention to this matter.

From:
Sent: 2024-08-28 星期三 15:01:56
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Cc: Peter Li <peter@cahk.org.hk>
Subject: Comments on the Section 16 Application No. A/NE-TK/800
Attachment: TPB20240830(TK800).pdf

Dear Sir/Madam,

Please refer to the attachment for the captioned.

Your faithfully,
Ng Hei Man (Mr.)
Campaign Manager
The Conservancy Association

Registered Name 註冊名稱 : The Conservancy Association 長春社
(Incorporated in Hong Kong with limited liability by guarantee 於香港註冊成立的擔保有限公司)

This email is for the sole use of the intended recipient(s) and may contain confidential information. Unauthorised use, disclosure or distribution of this email or its content is prohibited. If you have received this email in error, please delete it and notify the sender.



長春社

Since 1968

The Conservancy Association

30th August 2024

Town Planning Board
15/F North Point Government Offices
333 Java Road
North Point
Hong Kong

By e-mail: tpbpd@pland.gov.hk

Dear Sir/Madam,

Comments on the Section 16 Application No. A/NE-TK/800

The Conservancy Association (CA) OBJECTS to the captioned application.

1. Not in line with planning intention of Green Belt zone

According to the Approved Ting Kok Outline Zoning Plan (OZP) No. S/NE-TK/19, the planning intention of Green Belt zone *“is primarily for defining the limits of urban and sub-urban development areas by natural features and to contain urban sprawl as well as to provide passive recreational outlets. There is a general presumption against development within this zone”*. We opine that to facilitate daily commuting purpose, as stated in the planning application, can hardly be regarded as a strong justification for a departure from current planning intention of Green Belt, even on a temporary basis.

2. Undesirable precedent for similar applications

According to aerial photos of Google Earth Pro, this site has been subject to land formation and vegetation clearance (Figure 1-2) in November 2013. The site was then paved with concrete and car parking can also be spotted afterwards (Figure 3-4). Worse still, more structures have been erected on the site. We suspected that this is a case of “destroy first, build later”.

Planning Department and members of Town Planning Board (TPB) should alert that TPB has announced approaches to deter “destroy first, build later” activities in 2011. It stated that “the Board is determined to conserve the rural and natural environment and will not tolerate any deliberate action to destroy the rural and natural environment in



長春社

Since 1968

The Conservancy Association

the hope that the Board would give sympathetic consideration to subsequent development on the site concerned”¹. Therefore, this application should not be given any sympathetic consideration, or it will set an undesirable precedent for similar cases in future.

Yours faithfully,

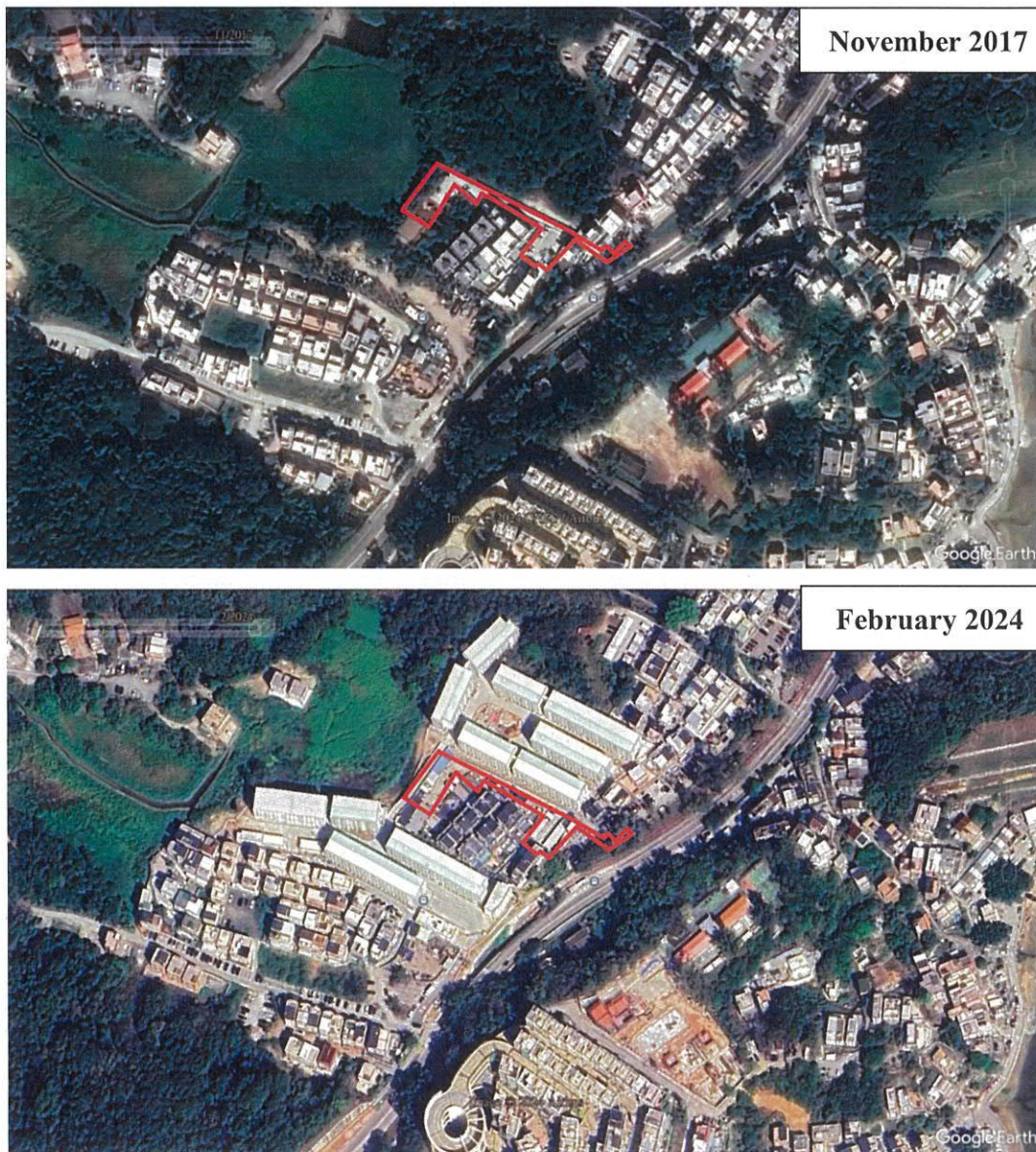
The Conservancy Association

¹ Town Planning Board adopts approaches to deter "destroy first, build later" activities (2011).
<http://www.info.gov.hk/gia/general/201107/04/P201107040255.htm>

Figure 1-2 According to aerial photos of Google Earth Pro, this site (marked in red) has been subject to land formation and vegetation clearance (Figure 1-2) in November 2013



Figure 3-4 The site (marked in red) was then paved with concrete and car parking can also be spotted afterwards



就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-152545-30275

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:25:45

有關的規劃申請編號

The application no. to which the comment relates: A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

女士 Ms. Anne Ko

意見詳情

Details of the Comment :

主題：反對規劃申請 A/NE-TK/800

我反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發。
原因：

1. 通道阻塞，影響居民進出其物業的能力，帶來極大的不便。
2. 安全風險，外面馬路路段車速快，住宅區頻密有車輛出入容易造成交通意外，加上馬路兩邊為單車徑，對使用單車人士亦構成危險。
3. 緊急車輛通道考慮，交通量的增加和開發的性質可能會妨礙緊急車輛的通行，可能會延誤關鍵的響應時間。

我敦促規劃委員會考慮這些觀點並拒絕該申請。優先考慮對鄰近交通安全和環境造成的影響。

感謝您對這一問題的關注。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-152606-38596

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:26:06

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

女士 Ms. bbk

意見詳情

Details of the Comment :

My rights to access my property at _____, is protected by Property Rights under the Basic Law: Articles 6 and 105 of the Basic Law. The proposed development right in front of my house is a violation of my legal rights. I reserve the rights to take legal action to enforce these rights and remove the obstruction. The Building Ordinance regulates construction activities in Hong Kong. The current blockage is unauthorized construction. Please investigate and enforce the law and protect property owner's right. Blocking access to my property is nuisance and trespass. I reserve the rights to seek legal remedies through the courts to remove the obstruction and claim any damages.

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-155301-30361

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:53:01

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. Wong Wai

意見詳情

Details of the Comment :

I have concerned that the development could negatively impact property values in the surrounding area. The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

The development does not align with the public interests of our community. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents.

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-183541-45871

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 18:35:41

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

女士 Ms. KO KEE KEE

意見詳情

Details of the Comment :

強烈反對規劃申請 A/NE-TK/800**強烈反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發的項目。原因：**

1. 項目會引致附近居民安全問題，包括通道阻塞，可能令居民繞道而造成交通意外。
 2. 相關停車場項目只會增加該區閑雜人，對區內治安構成嚴重威脅。
 3. 停車場製造大量車輛進出，產生更多汽車廢氣，對附近居民的健康造成嚴重影響。
 4. 有關地段施工工程產生環境污染，空氣、生態等，破壞環境之餘亦造成居民健康威脅。
 5. 對周邊地區的物業價值產生負面影響
 6. 開發項目可能會妨礙緊急車輛的通行，延誤關鍵服務(eg 消防、救援等)，令附近居民失去安全保障，失去使用有關救援服務的權利，對生命造成威脅。
- 為了社區和居民的福祉，請勿只考慮商業利益，強烈要求取消有關項目。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-222820-48427

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 22:28:20

有關的規劃申請編號

The application no. to which the comment relates: A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. ko man kit

意見詳情

Details of the Comment :

反對規劃申請 A/NE-TK/800

我反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發。

反對理由

安全問題

原道路使用者的權利會被剝削，間接變成私人土地，規劃地域接連旁邊的單車徑，令單車使用者、行人及車輛會有爭路的情況出現，容易發生意外。由於地域通道狹窄，只需一輛汽車等候，便能阻塞通道，這也大大增加緊急車輛進行救援時被車輛阻塞的機會。這也直接令這條行人路變成行車路，會影響路人安全，特別是不熟悉環境的訪客。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-230103-49816

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 23:01:03

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

女士 Ms. Joyce Wong

意見詳情

Details of the Comment :

Subject: Opposition to Planning Application A/NE-TK/800

I am cordially writing to strongly oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. Due to the following reasons, I have strong reservations against this proposed development as a resident and property owner directly next to it.

1. Access Pathway Blockage to Private Property:

The proposed development will block the only access pathway to private properties adjacent to the proposed development area. This blockage will severely impact the residents' ability to enter and exit their property, posing significant inconvenience and safety risks, as well as, imposing significant impact and concern to the homeowners. Such blockage to access for private property for the interest of the applicant is inconsiderate and unreasonable.

2. Property Valuation:

I believe that the development could negatively impact property valuation in the surrounding area, which is a significant concern for homeowners. This will pose severe economic challenges to all property owners in this region amidst the already challenging local property outlook.

3. Safety and Emergency Vehicle Access:

The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

4. Public Interests:

The development does not align with the broader public interests of our community at all. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents. Instead, it prioritizes private interests over the collective well-being of the community by occupying the already very limited leisure space for the community.

5. Environmental Impact:

The construction and associated filling of land will likely lead to the destruction of green spaces and wildlife habitats, which are crucial for maintaining the ecological balance in our area. Protecting our environment should be a priority in any development plan.

6. Traffic and Infrastructure:

The increase in traffic due to this development will exacerbate traffic congestion and put additional strain on our already overburdened infrastructure. This could lead to increased pollution and impact the harmony of the community.

7. Community Character:

The scale and design of the proposed development are not in keeping with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

8. Noise and Disturbance:

The parking area and operation directly adjacent to the residential area will cause disturbance to the community. The construction phase and subsequent increase in population will result in higher noise levels and disturbances, affecting the quality of life for existing residents.

Based on the above, I strongly urge the planning committee to carefully consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, the safety of our community, and the sustainability of our environment.

Thank you for your attention to this matter.

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-231203-49846

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 23:12:03

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

小姐 Miss Janice

意見詳情

Details of the Comment :

I am writing to formally express my opposition to planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. As a nearby resident, I have several concerns about this project.

Access Pathway Obstruction: The proposed development will obstruct the only access route to the adjacent private properties. This obstruction would greatly inconvenience residents, making it difficult for them to enter and exit their properties, and would raise significant safety concerns. Blocking access for the benefit of the applicant is both inconsiderate and unreasonable.

Impact on Property Values: There is a concern that the development may negatively affect property values in the surrounding area, which is a major issue for homeowners.

Safety and Emergency Access: This development poses serious safety risks. The increase in traffic and the nature of the project could hinder emergency vehicle access, potentially delaying critical response times. Ensuring unobstructed access for emergency services is crucial for the safety of all residents.

Public Interest: The proposed development does not align with the broader public interests of our community. The temporary parking area for private cars and light goods vehicles does not offer long-term benefits to residents, and instead, prioritizes private interests at the expense of our already limited community leisure space.

Environmental Concerns: The construction and associated landfilling are likely to destroy green spaces and wildlife habitats, which are essential for maintaining the ecological balance in our area. Environmental protection should be a priority in any development plan.

Traffic and Infrastructure: The expected increase in traffic due to this development will worsen traffic congestion and strain our already overburdened infrastructure, leading to increased pollution and disrupting the harmony of the community.

Preserving Community Character: The scale and design of the proposed development do not fit with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

Noise and Disturbance: The proximity of the parking area to residential areas will cause disturbance.

nces. The construction phase, along with the anticipated rise in population, will lead to higher noise levels and disruptions, affecting the quality of life for current residents.

I urge the planning committee to carefully consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, community safety, and environmental sustainability.

Thank you for your attention to this matter.

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240829-133329-46760

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

29/08/2024 13:33:29

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

女士 Ms. Rebecca Chan

意見詳情

Details of the Comment :

主題：反對規劃申請 A/NE-TK/800

本人反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發。

有關地區為私人物業及民居，已有足夠的泊車及停車空間，開發停車場實屬無必要。而此舉卻嚴重影響居民生活及帶來安全風險，為少數商家的利益而犧牲居民的福祉，實屬不合理。

擬議開發的規模和設計影響當區自然環境，並與大埔社區的特徵相違背，工程噪音和污染將對該區生態造成干擾。

有關地區鄰近單車徑，為本地居民和旅客假日休閒旅遊必到之處，有關工程將嚴重影響附近空氣質素及破壞環境面貌，影響整體經濟發展，而工程並非必要，實在不值得。

懇請有關當局重新審視有關申請，謝謝！

From:
Sent: 2024-08-29 星期四 03:46:40
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: A/NE-TK/800 DD 26 Shuen Wan Chim Uk

A/NE-TK/800

Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po

Site area: About 1,525sq.m

Zoning: "Green Belt", "VTD" and area shown as 'Road'

Applied use: 39 Vehicle Park / Filling of Land

Dear TPB Members,

It is not clear if the parking is for the private developments or to cater for the residents of the transitional housing?

Whatever, part of the site is a road and parking could pose safety, obstruction and noise pollution issues that members should inquire into.

Mary Mulvihill

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號
Reference Number: 240828-144830-27741

提交限期
Deadline for submission: 30/08/2024

提交日期及時間
Date and time of submission: 28/08/2024 14:48:30

有關的規劃申請編號
The application no. to which the comment relates: A/NE-TK/800

「提意見人」姓名/名稱
Name of person making this comment: 夫人 Mrs. 鄧太

**意見詳情
Details of the Comment :**

主題：反对规划申请 A/NE-TK/800

我写信正式反对规划申请 A/NE-TK/800，拟在新界大埔汀角船湾占屋第26约的多个地段进行开发。作为拟开发区域旁边物业的居民和业主，我对该项目有以下几个方面的担忧。

1. 私人物业通道阻塞：拟议的开发将阻塞通往拟开发区域旁边私人物业的唯一通道。这将严重影响居民进出其物业的能力，带来极大的不便和安全风险，并对业主造成重大影响和担忧。为了申请人的利益而阻塞私人物业的通道是不体贴和不合理的。
 2. 物业价值：担心该开发可能会对周边地区的物业价值产生负面影响，这对业主来说是一个重大关切。
 3. 安全和紧急车辆通道：拟议的开发引发了重大安全问题。交通量的增加和开发的性质可能会妨碍紧急车辆的通行，可能会延误关键的响应时间。确保紧急服务的通道畅通无阻对所有居民的安全至关重要。
 4. 公共利益：该开发不符合我们社区的更广泛公共利益。临时私人车辆停车场对居民没有长期利益。相反，它优先考虑私人利益而不是社区的整体福祉，占用了本已非常有限的社区休闲空间。
 5. 环境影响：施工和相关的填土可能会导致绿地和野生动物栖息地的破坏，这对于维持我们地区的生态平衡至关重要。保护环境应是任何开发计划的优先事项。
 6. 交通和基础设施：由于该开发导致的交通量增加将加剧交通拥堵，并对我们已经超负荷的基础设施造成额外压力。这可能会导致污染增加并影响社区的和谐。
 7. 社区特征：拟议开发的规模和设计与我们社区的特征不符。保持我们社区的独特身份和魅力非常重要，而该项目威胁到这一点。
 8. 噪音和干扰：紧邻住宅区的停车场和运营将对社区造成干扰。施工阶段和随之而来的人口增加将导致噪音水平和干扰增加，影响现有居民的生活质量。
- 我敦促规划委员会考虑这些观点并拒绝该申请。优先考虑邻近物业业主的权利、现有居民的福祉、我们社区的安全和环境的可持续性是非常重要的。
- 感谢您对这一问题的关注

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-150543-90237

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:05:43

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

小姐 Miss miss ko

意見詳情

Details of the Comment :

該申請範圍會完全阻塞通往381號地下嘅私人通道，引致381號嘅租客住戶業主商戶有辦法返回家中，堅決反對。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-150644-09744

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:06:44

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

小姐 Miss Li Ka Yan

意見詳情

Details of the Comment :

主题：反对规划申请 A/NE-TK/800

我寫信正式反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發。作為擬開發區域旁邊物業的居民和業主，我對該項目有以下幾個方面的擔憂。

私人物業通道阻塞：擬議的開發將阻塞通往擬開發區域旁邊私人物業的唯一通道。這將嚴重影響居民進出其物業的能力，帶來極大的不便和安全風險，並對業主造成重大影響和擔憂。為了申請人的利益而阻塞私人物業的通道是不體貼和不合理的。

物業價值：擔心該開發可能會對周邊地區的物業價值產生負面影響，這對業主來說是一個重大關切。

安全和緊急車輛通道：擬議的開發引發了重大安全問題。交通量的增加和開發的性質可能會妨礙緊急車輛的通行，可能會延誤關鍵的響應時間。確保緊急服務的通道暢通無阻對所有居民的安全至關重要。

公共利益：該開發不符合我們社區的更廣泛公共利益。臨時私人車輛停車場對居民沒有長期利益。相反，它優先考慮私人利益而不是社區的整體福祉，占用了本已非常有限的社區休閒空間。

環境影響：施工和相關的填土可能會導致綠地和野生動物棲息地的破壞，這對於維持我們地區的生態平衡至關重要。保護環境應是任何開發計劃的優先事項。

交通和基礎設施：由於該開發導致的交通量增加將加劇交通擁堵，並對我們已經超負荷的基礎設施造成額外壓力。這可能會導致污染增加並影響社區的和諧。

社區特徵：擬議開發的規模和設計與我們社區的特徵不符。保持我們社區的獨特身份和魅力非常重要，而該項目威脅到這一點。

噪音和干擾：緊鄰住宅區的停車場和運營將對社區造成干擾。施工階段和隨之而來的人口增加將導致噪音水平和干擾增加，影響現有居民的生活質量。

我敦促規劃委員會考慮這些觀點並拒絕該申請。優先考慮鄰近物業業主的權利、現有居民的福祉、我們社區的安全和環境的可持續性是至關重要的。

感謝您對這一問題的關注。

主题：反对规划申请 A/NE-TK/800

我写信正式反对规划申请 A/NE-TK/800，拟在新界大埔汀角船湾占屋第26约的多个地段进行开发。作为拟开发区域旁边物业的居民和业主，我对该项目有以下几个方面的担忧。

私人物业通道阻塞：拟议的开发将阻塞通往拟开发区域旁边私人物业的唯一通道。这将严重影响居民进出其物业的能力，带来极大的不便和安全风险，并对业主造成重大影响

和担忧。为了申请人的利益而阻塞私人物业的通道是不体贴和不合理的。

物业价值：担心该开发可能会对周边地区的物业价值产生负面影响，这对业主来说是一个重大关切。

安全和紧急车辆通道：拟议的开发引发了重大安全问题。交通量的增加和开发的性质可能会妨碍紧急车辆的通行，可能会延误关键的响应时间。确保紧急服务的通道畅通无阻对所有居民的安全至关重要。

公共利益：该开发不符合我们社区的更广泛公共利益。临时私人车辆停车场对居民没有长期利益。相反，它优先考虑私人利益而不是社区的整体福祉，占用了本已非常有限的社区休闲空间。

环境影响：施工和相关的填土可能会导致绿地和野生动物栖息地的破坏，这对于维持我们地区的生态平衡至关重要。保护环境应是任何开发计划的优先事项。

交通和基础设施：由于该开发导致的交通量增加将加剧交通拥堵，并对我们已经超负荷的基础设施造成额外压力。这可能会导致污染增加并影响社区的和谐。

社区特征：拟议开发的规模和设计与我们社区的特征不符。保持我们社区的独特身份和魅力非常重要，而该项目威胁到这一点。

噪音和干扰：紧邻住宅区的停车场和运营将对社区造成干扰。施工阶段和随之而来的人口增加将导致噪音水平和干扰增加，影响现有居民的生活质量。

我敦促规划委员会考虑这些观点并拒绝该申请。优先考虑邻近物业业主的权利、现有居民的福祉、我们社区的安全和环境的可持续性是最重要的。

感谢您对这一问题的关注

Subject: Opposition to Planning Application A/NE-TK/800

I am writing to formally oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. As a resident and owner of a property directly adjacent to the proposed development area, I have several concerns regarding this project.

Access Pathway Blockage to Private Property:

The proposed development will block the only access pathway to private properties adjacent to the proposed development area. This blockage will severely impact the residents' ability to enter and exit their property, posing significant inconvenience and safety risks, as well as, imposing significant impact and concern to the homeowners. Blockage to access for private property for the interest of the applicant is inconsiderate and unreasonable.

Property Values:

There is a concern that the development could negatively impact property values in the surrounding area, which is a significant concern for homeowners.

Safety and Emergency Vehicle Access:

The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

Public Interests:

The development does not align with the broader public interests of our community. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents. Instead, it prioritizes private interests over the collective well-being of the community by occupying the already very limited leisure space for the community.

Environmental Impact:

The construction and associated filling of land will likely lead to the destruction of green spaces and wildlife habitats, which are crucial for maintaining the ecological balance in our area. Protecting our environment should be a priority in any development plan.

Traffic and Infrastructure:

The increase in traffic due to this development will exacerbate traffic congestion and put additional strain on our already overburdened infrastructure. This could lead to increased pollution and impact the harmony of the community.

Community Character:

The scale and design of the proposed development are not in keeping with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

Noise and Disturbance:

The parking area and operation directly adjacent to the residential area will cause disturbance to the community. The construction phase and subsequent increase in population will result in higher noise levels and disturbances, affecting the quality of life for existing residents.

I urge the planning committee to consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, the safety of our community, and the sustainability of our environment.

Thank you for your attention to this matter.

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240828-151255-01264

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

28/08/2024 15:12:55

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. Chan chi wa

意見詳情

Details of the Comment :

主题：反对规划申请 A/NE-TK/800

我寫信正式反對規劃申請 A/NE-TK/800，擬在新界大埔汀角船灣占屋第26約的多個地段進行開發。作為擬開發區域旁邊物業的居民和業主，我對該項目有以下幾個方面的擔憂。

私人物業通道阻塞：擬議的開發將阻塞通往擬開發區域旁邊私人物業的唯一通道。這將嚴重影響居民進出其物業的能力，帶來極大的不便和安全風險，並對業主造成重大影響和擔憂。為了申請人的利益而阻塞私人物業的通道是不體貼和不合理的。

物業價值：擔心該開發可能會對周邊地區的物業價值產生負面影響，這對業主來說是一個重大關切。

安全和緊急車輛通道：擬議的開發引發了重大安全問題。交通量的增加和開發的性質可能會妨礙緊急車輛的通行，可能會延誤關鍵的響應時間。確保緊急服務的通道暢通無阻對所有居民的安全至關重要。

公共利益：該開發不符合我們社區的更廣泛公共利益。臨時私人車輛停車場對居民沒有長期利益。相反，它優先考慮私人利益而不是社區的整體福祉，占用了本已非常有限的社區休閒空間。

環境影響：施工和相關的填土可能會導致綠地和野生動物棲息地的破壞，這對於維持我們地區的生態平衡至關重要。保護環境應是任何開發計劃的優先事項。

交通和基礎設施：由於該開發導致的交通量增加將加劇交通擁堵，並對我們已經超負荷的基礎設施造成額外壓力。這可能會導致污染增加並影響社區的和諧。

社區特徵：擬議開發的規模和設計與我們社區的特徵不符。保持我們社區的獨特身份和魅力非常重要，而該項目威脅到這一點。

噪音和干擾：緊鄰住宅區的停車場和運營將對社區造成干擾。施工階段和隨之而來的人口增加將導致噪音水平和干擾增加，影響現有居民的生活質量。

我敦促規劃委員會考慮這些觀點並拒絕該申請。優先考慮鄰近物業業主的權利、現有居民的福祉、我們社區的安全和環境的可持續性是至關重要的。

感謝您對這一問題的關注。

主题：反对规划申请 A/NE-TK/800

我写信正式反对规划申请 A/NE-TK/800，拟在新界大埔汀角船湾占屋第26约的多个地段进行开发。作为拟开发区域旁边物业的居民和业主，我对该项目有以下几个方面的担忧。

私人物业通道阻塞：拟议的开发将阻塞通往拟开发区域旁边私人物业的唯一通道。这将严重影响居民进出其物业的能力，带来极大的不便和安全风险，并对业主造成重大影响。

和担忧。为了申请人的利益而阻塞私人物业的通道是不体贴和不合理的。

物业价值：担心该开发可能会对周边地区的物业价值产生负面影响，这对业主来说是一个重大关切。

安全和紧急车辆通道：拟议的开发引发了重大安全问题。交通量的增加和开发的性质可能会妨碍紧急车辆的通行，可能会延误关键的响应时间。确保紧急服务的通道畅通无阻对所有居民的安全至关重要。

公共利益：该开发不符合我们社区的更广泛公共利益。临时私人车辆停车场对居民没有长期利益。相反，它优先考虑私人利益而不是社区的整体福祉，占用了本已非常有限的社区休闲空间。

环境影响：施工和相关的填土可能会导致绿地和野生动物栖息地的破坏，这对于维持我们地区的生态平衡至关重要。保护环境应是任何开发计划的优先事项。

交通和基础设施：由于该开发导致的交通量增加将加剧交通拥堵，并对我们已经超负荷的基础设施造成额外压力。这可能会导致污染增加并影响社区的和谐。

社区特征：拟议开发的规模和设计与我们社区的特征不符。保持我们社区的独特身份和魅力非常重要，而该项目威胁到这一点。

噪音和干扰：紧邻住宅区的停车场和运营将对社区造成干扰。施工阶段和随之而来的人口增加将导致噪音水平和干扰增加，影响现有居民的生活质量。

我敦促规划委员会考虑这些观点并拒绝该申请。优先考虑邻近物业业主的权利、现有居民的福祉、我们社区的安全和环境的可持续性是最重要的。

感谢您对这一问题的关注

Subject: Opposition to Planning Application A/NE-TK/800

I am writing to formally oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories. As a resident and owner of a property directly adjacent to the proposed development area, I have several concerns regarding this project.

Access Pathway Blockage to Private Property:

The proposed development will block the only access pathway to private properties adjacent to the proposed development area. This blockage will severely impact the residents' ability to enter and exit their property, posing significant inconvenience and safety risks, as well as, imposing significant impact and concern to the homeowners. Blockage to access for private property for the interest of the applicant is inconsiderate and unreasonable.

Property Values:

There is a concern that the development could negatively impact property values in the surrounding area, which is a significant concern for homeowners.

Safety and Emergency Vehicle Access:

The proposed development raises significant safety concerns. The increase in traffic and the nature of the development could impede access for emergency vehicles, potentially delaying critical response times. Ensuring clear and unobstructed routes for emergency services is paramount for the safety of all residents.

Public Interests:

The development does not align with the broader public interests of our community. The temporary private vehicle park for private cars and light goods vehicles does not provide a long-term benefit to the residents. Instead, it prioritizes private interests over the collective well-being of the community by occupying the already very limited leisure space for the community.

Environmental Impact:

The construction and associated filling of land will likely lead to the destruction of green spaces and wildlife habitats, which are crucial for maintaining the ecological balance in our area. Protecting our environment should be a priority in any development plan.

Traffic and Infrastructure:

The increase in traffic due to this development will exacerbate traffic congestion and put additional strain on our already overburdened infrastructure. This could lead to increased pollution and impact the harmony of the community.

Community Character:

The scale and design of the proposed development are not in keeping with the character of our community. It is important to preserve the unique identity and charm of our neighborhood, which this project threatens to undermine.

Noise and Disturbance:

The parking area and operation directly adjacent to the residential area will cause disturbance to the community. The construction phase and subsequent increase in population will result in higher noise levels and disturbances, affecting the quality of life for existing residents.

I urge the planning committee to consider these points and reject the application. It is essential to prioritize the rights of adjacent property owners, the well-being of current residents, the safety of our community, and the sustainability of our environment.

Thank you for your attention to this matter.

17

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**參考編號****Reference Number:**

240830-130854-62425

提交限期**Deadline for submission:**

30/08/2024

提交日期及時間**Date and time of submission:**

30/08/2024 13:08:54

有關的規劃申請編號**The application no. to which the comment relates:**

A/NE-TK/800

「提意見人」姓名/名稱**Name of person making this comment:**

女士 Ms. Ms Kong

意見詳情**Details of the Comment :****Subject: Opposition to Planning Application A/NE-TK/800**

I am writing to formally oppose the planning application A/NE-TK/800 for the proposed development at various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po, New Territories.

1. The proposed area has been enclosed now and a security gate has been put up where password is required to both ENTER and EXIT the area.
2. There is no security guard, no contact number to call in case of enquiry, accident or emergency. It is imposing severe safety risk to the community. For old people and kids who need to be evacuated from the proposed area now, a "private" password has to be input to the gate. Without the password, there is no way to exit the area even during emergency.
3. The proposed area is too narrow for a gate / security booth to ensure 24-hours access. In case of fire or emergency, there is no way out.
4. The whole access is being controlled by one or two individual whose real identity unknown. There is no way for emergency vehicles such as ambulance or fire engines to reach the properties in the enclosed area. If the password holder does not "permit" or approve, then there is no way for any individual to exit or enter even their own properties / address in the area even if you are an owner or tenant.

18

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

240830-131414-40773

提交限期

Deadline for submission:

30/08/2024

提交日期及時間

Date and time of submission:

30/08/2024 13:14:14

有關的規劃申請編號

The application no. to which the comment relates:

A/NE-TK/800

「提意見人」姓名/名稱

Name of person making this comment:

夫人 Mrs. Teng

意見詳情

Details of the Comment :

Subject: 反對規劃申請 A/NE-TK/800

我寫信正式反對規劃申請 A/NE-TK/800，該申請涉及在新界大埔汀角船灣尖屋丈量約份第26約的多個地段進行開發。

該地區現已被封閉，並設置了一個需要密碼才能進入和退出的安全門。沒有保安人員，也沒有聯絡電話以便在有查詢、事故或緊急情況時聯絡。這對社區構成了嚴重的安全風險。對於需要從該地區撤離的老人和孩子來說，必須輸入“私人”密碼才能通過大門。在緊急情況下，如果沒有密碼，無法離開該地區。

該地區太狹窄，無法設置確保24小時通行的門或保安亭。在火災或緊急情況下，無法逃生。

整個通道由一兩個身份不明的個人控制。緊急車輛如救護車或消防車無法到達封閉區域內。

如果密碼持有者不“允許”或“批准”，則即使是業主或租戶，也無法進出該地區的房產或地址。

From:
Sent: 2024-12-04 星期三 03:10:32
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: Re: A/NE-TK/800 DD 26 Shuen Wan Chim Uk

Dear TPB Members,

Slight reduction is size to 1,512sq.m and number of vehicles 38.

Previous onjections relevant and upheld.

Mary Mulvihill

From:
To: tpbpd <tpbpd@pland.gov.hk>
Date: Thursday, 29 August 2024 3:46 AM HKT
Subject: A/NE-TK/800 DD 26 Shuen Wan Chim Uk

A/NE-TK/800

Various Lots in D.D. 26, Shuen Wan Chim Uk, Ting Kok, Tai Po

Site area: About 1,525sq.m

Zoning: "Green Belt", "VTD" and area shown as 'Road'

Applied use: 39 Vehicle Park / Filling of Land

Dear TPB Members,

It is not clear if the parking is for the private developments or to cater for the residents of the transitional housing?

Whatever, part of the site is a road and parking could pose safety, obstruction and noise pollution issues that members should inquire into.

Mary Mulvihill

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

From:
Sent: 2024-12-16 星期一 23:20:15
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: WWF submission on the proposed Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only) for a Period of 3 Years and Associated Filling of Land in Ting Kok, Tai Po (Application No.: A/NE-TK/800)
Attachment: s16A_NE_TK_800 20241216_WWF.pdf

Dear Sir/Madam,

Please find WWF-Hong Kong's submission on the captioned application.
See attached file:

s16A_NE_TK_800 20241216_WWF.pdf

Thank you for your attention.

Best regards,
Bonnie Leung (Ms.)

Conservation Officer, Conservation Policy | WWF 世界自然基金會香港分會

Tel:

Registered Name 註冊名稱: World Wide Fund For Nature Hong Kong 世界自然(香港)基金會
(Incorporated in Hong Kong with limited liability by guarantee 於香港註冊成立的擔保有限公司).
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世界自然基金會
香港分會

WWF-Hong Kong

16 December 2024

Chairman and members

Town Planning Board

15/F North Point Government Offices,
333 Java Road, North Point, Hong Kong

(E-mail: tpbpd@pland.gov.hk)

By E-mail ONLY

Dear Sir/ Madam,

Re: Temporary Private Vehicle Park (Private Cars and Light Goods Vehicles Only)
for a Period of 3 Years and Associated Filling of Land in “Green Belt”, “Village
Type Development” and area shown as ‘Road’ in Shuen Wan Chim Uk, Ting Kok,
Tai Po (A/NE-TK/800)

WWF would like to lodge an objection to the captioned proposal.

Undesirable precedent of “develop first, apply later”

According to the latest available satellite image, the site was used as a vehicle park until at least 11 February 2024 (Figure 1). Although the applicant claimed that vehicle park is needed to alleviate the added stress on transportation in the area due to the influx of residents in Lok Sin Village transitional housing, we found out that vegetation clearance dates back to 22 November 2013 (Figure 2) and the vehicle park has been in place since 3 March 2017 (Figure 3), well before the establishment of Lok Sin Village transitional housing. As such, we consider that the applicants' claim is unjustified.

Given our limited knowledge of the current environmental conditions at the site, we would be grateful if the Town Planning Board members could request relevant government departments to investigate whether the captioned proposal is linked to any unauthorised development. If that is the case, we urge the Town Planning Board to reject the application to prevent legitimizing actions to destroy the environment in pursuit of development approval.

together possible™

贊助人：中華人民共和國

香港特別行政區行政長官

李家超先生，大紫荊勳賢，SBS, PDMS

主席：白丹尼先生

行政總裁：黃碧茵女士

核數師：中審環環（香港）會計師事務所有限公司

公司秘書：嘉信秘書服務有限公司

義務司庫：匯豐銀行

註冊慈善機構

Patron: The Honourable John Lee Ka-chiu, GBM, SBS, PDMS

The Chief Executive, Hong Kong Special Administrative Region

People's Republic of China

Chairman: Mr Daniel R Bradshaw

CEO: Ms Nicole Wong

Auditors: Mazars CPA Limited

Company Secretary:

McCabe Secretarial Services Limited

Honorary Treasurer: HSBC

Registered Charity
(Incorporated With Limited Liability)

We would be grateful if our comments could be considered by the Town Planning Board.

Yours faithfully,
Ms. Bonnie LEUNG
Conservation Officer, Conservation Policy
WWF Hong Kong

Figure 1 Satellite image showing the site was used as a vehicle park until at least 11 February 2024

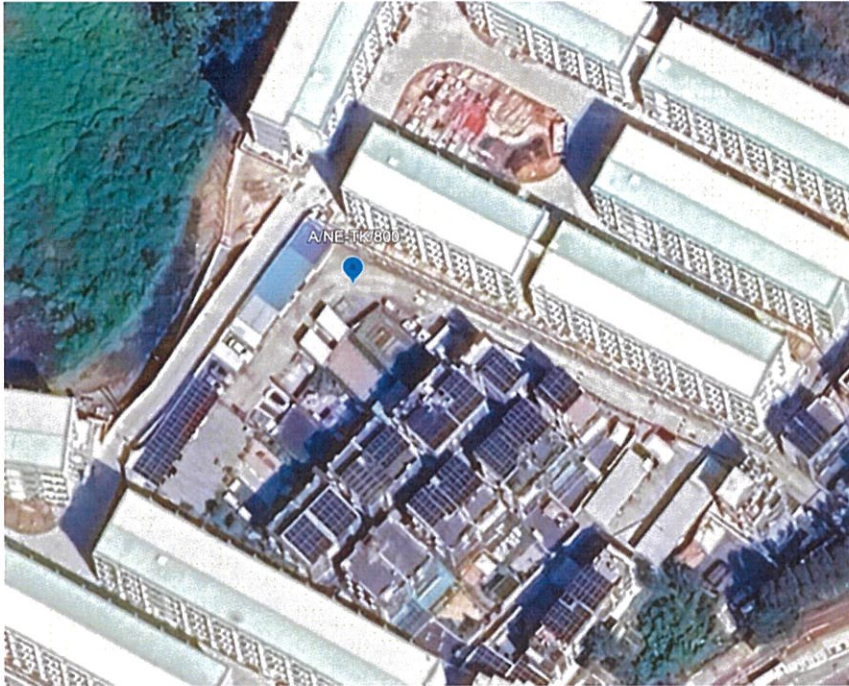


Figure 2 Satellite image showing vegetation clearance took place at the site at 22 November 2013



Figure 3 Satellite image showing the site has been used as a vehicle park since 3 March 2017

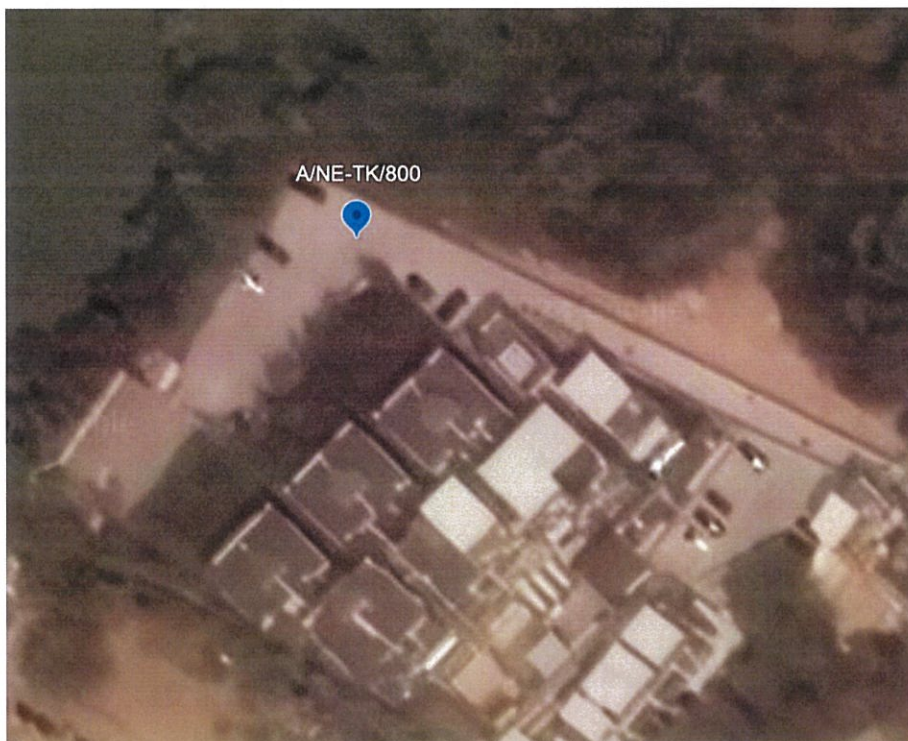


Image sources: Google Earth (Accessed on 9 December 2024)

☐Urgent ☐Return receipt ☐Expand Group ☐Restricted ☐Prevent Copy

From:
Sent: 2024-12-17 星期二 08:00:00
To: tpbpd/PLAND <tpbpd@pland.gov.hk>
Subject: Comments on the Section 16 Application No. A/NE-TK/800
Attachment: TPB20241217(TK800).pdf

Dear Sir/Madam,

Please refer to the attachment for the captioned.

Yours faithfully,
Ng Hei Man (Mr.)
Campaign Manager
The Conservancy Association

T:

D:

F:

Registered Name 註冊名稱 : The Conservancy Association 長春社
(Incorporated in Hong Kong with limited liability by guarantee 於香港註冊成立的擔保有限公司)

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長春社

Since 1968

The Conservancy Association

17th December 2024

Town Planning Board
15/F North Point Government Offices
333 Java Road
North Point
Hong Kong

By e-mail: tpbpd@pland.gov.hk

Dear Sir/Madam,

Comments on the Section 16 Application No. A/NE-TK/800

The Conservancy Association (CA) OBJECTS to the captioned application.

1. Not in line with planning intention of Green Belt zone

According to the Approved Ting Kok Outline Zoning Plan (OZP) No. S/NE-TK/19, the planning intention of Green Belt zone *“is primarily for defining the limits of urban and sub-urban development areas by natural features and to contain urban sprawl as well as to provide passive recreational outlets. There is a general presumption against development within this zone”*. We opine that to facilitate daily commuting purpose, as stated in the planning application, can hardly be regarded as a strong justification for a departure from current planning intention of Green Belt, even on a temporary basis.

2. Undesirable precedent for similar applications

According to aerial photos of Google Earth Pro, this site has been subject to land formation and vegetation clearance (Figure 1-2) in November 2013. The site was then paved with concrete and car parking can also be spotted afterwards (Figure 3-4). Worse still, more structures have been erected on the site. We suspected that this is a case of “destroy first, build later”.

Planning Department and members of Town Planning Board (TPB) should alert that TPB has announced approaches to deter “destroy first, build later” activities in 2011. It stated that “the Board is determined to conserve the rural and natural environment and



長春社

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The Conservancy Association

will not tolerate any deliberate action to destroy the rural and natural environment in the hope that the Board would give sympathetic consideration to subsequent development on the site concerned”¹. Therefore, this application should not be given any sympathetic consideration, or it will set an undesirable precedent for similar cases in future.

Yours faithfully,

The Conservancy Association

¹ Town Planning Board adopts approaches to deter "destroy first, build later" activities (2011).
<http://www.info.gov.hk/gia/general/201107/04/P201107040255.htm>

Figure 1-2 According to aerial photos of Google Earth Pro, this site (marked in red) has been subject to land formation and vegetation clearance (Figure 1-2) in November 2013

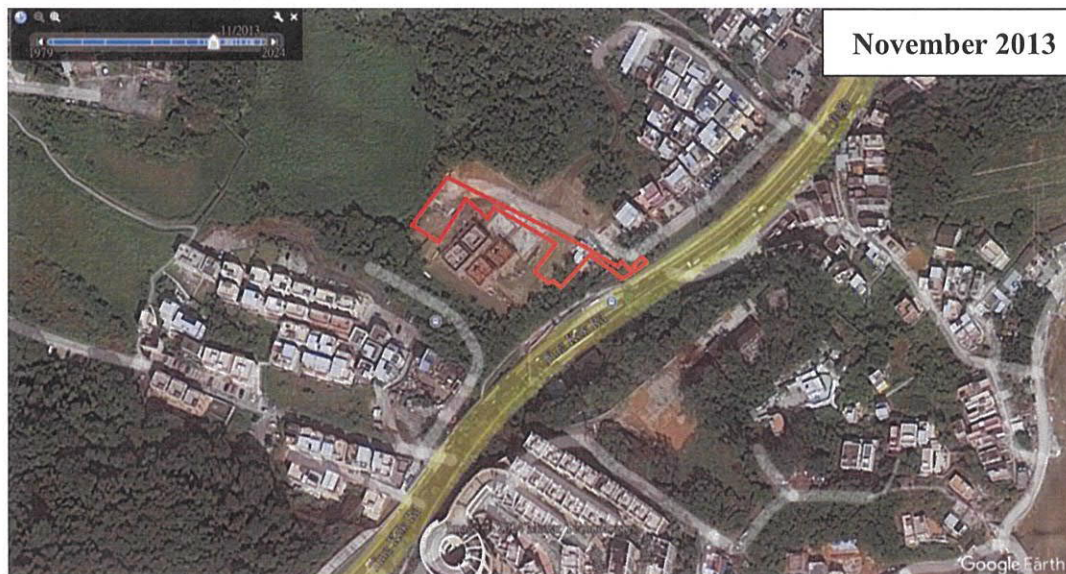


Figure 3-4 The site (marked in red) was then paved with concrete and car parking can also be spotted afterwards

