

表格第 S12A 號

Team 1  
S/N = 2202738  
By Hand = 26/10

# APPLICATION FOR AMENDMENT OF PLAN UNDER SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP.131)

根據《城市規劃條例》(第131章)  
第12A條遞交的修訂圖則申請

Applicant who would like to publish the notice of application in local newspapers to meet one of the Town Planning Board's requirements of taking reasonable steps to obtain consent of or give notification to the current land owner, please refer to the following link regarding publishing the notice in the designated newspapers:  
[https://www.info.gov.hk/tpb/en/plan\\_application/apply.html](https://www.info.gov.hk/tpb/en/plan_application/apply.html)

申請人如欲在本地報章刊登申請通知，以採取城市規劃委員會就取得現行土地擁有人的同意或通知現行土地擁有人所指定的其中一項合理步驟，請瀏覽以下網址有關在指定的報章刊登通知：  
[https://www.info.gov.hk/tpb/tc/plan\\_application/apply.html](https://www.info.gov.hk/tpb/tc/plan_application/apply.html)

2022年10月28日  
此文件在 2022年10月28日 收到。城市規劃委員會  
只會在收到所有必要的資料及文件後才正式確認收到  
申請的日期。

28 OCT 2022

This document is received on \_\_\_\_\_  
The Town Planning Board will formally acknowledge  
the date of receipt of the application only upon receipt  
of all the required information and documents.

## General Note and Annotation for the Form 填寫表格的一般指引及註解

# "Current land owner" means any person whose name is registered in the Land Registry as that of an owner of the land to which the application relates, as at 6 weeks before the application is made  
「現行土地擁有人」指在提出申請前六星期，其姓名或名稱已在土地註冊處註冊為該申請所關乎的土地的擁有人的人

& Please attach documentary proof 請夾附證明文件

^ Please insert number where appropriate 請在適當地方註明編號

Please fill "NA" for inapplicable item 請在不適用的項目填寫「不適用」

Please use separate sheets if the space provided is insufficient 如所提供的空間不足，請另頁說明

Please insert a 「✓」 at the appropriate box 請在適當的方格內上加上「✓」號

For Official Use Only 請勿填寫此欄	Application No. 申請編號	Y/TP/37
	Date Received 收到日期	28 OCT 2022

- The completed form and supporting documents (if any) should be sent to the Secretary, Town Planning Board (the Board), 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.  
申請人須把填妥的申請表格及其他支持申請的文件（倘有），送交香港北角渣華道 333 號北角政府合署 15 樓城市規劃委員會（下稱「委員會」）秘書收。
- Please read the "Guidance Notes" carefully before you fill in this form. The document can be downloaded from the Board's website at <http://www.info.gov.hk/tpb/>. It can also be obtained from the Secretariat of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong (Tel: 2231 4810 or 2231 4835), and the Planning Enquiry Counters of the Planning Department (Hotline: 2231 5000) (17/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong and 14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, New Territories).  
請先細閱《申請須知》的資料單張，然後填寫此表格。該份文件可從委員會的網頁下載（網址：<http://www.info.gov.hk/tpb/>），亦可向委員會秘書處（香港北角渣華道 333 號北角政府合署 15 樓 – 電話：2231 4810 或 2231 4835）及規劃署的規劃資料查詢處（熱線：2231 5000）（香港北角渣華道 333 號北角政府合署 17 樓及新界沙田上禾輦路 1 號沙田政府合署 14 樓）索取。
- This form can be downloaded from the Board's website, and obtained from the Secretariat of the Board and the Planning Enquiry Counters of the Planning Department. The form should be typed or completed in block letters. The processing of the application may be refused if the required information or the required copies are incomplete.  
此表格可從委員會的網頁下載，亦可向委員會秘書處及規劃署的規劃資料查詢處索取。申請人須以打印方式或以正楷填寫表格。如果申請人所提交的資料或文件副本不齊全，委員會可拒絕處理有關申請。

<b>1. Name of Applicant 申請人姓名/名稱</b>	
<input type="checkbox"/> Mr. 先生 / <input type="checkbox"/> Mrs. 夫人 / <input type="checkbox"/> Miss 小姐 / <input type="checkbox"/> Ms. 女士 / <input checked="" type="checkbox"/> Company 公司 / <input type="checkbox"/> Organisation 機構	
Fancy Lotus Limited	
<b>2. Name of Authorised Agent (if applicable) 獲授權代理人姓名/名稱（如適用）</b>	
<input type="checkbox"/> Mr. 先生 / <input type="checkbox"/> Mrs. 夫人 / <input type="checkbox"/> Miss 小姐 / <input type="checkbox"/> Ms. 女士 / <input checked="" type="checkbox"/> Company 公司 / <input type="checkbox"/> Organisation 機構	
Vision Planning Consultants Limited	
<b>3. Application Site 申請地點</b>	
(a) Whether the application directly relates to any specific site? 申請是否直接與某地點有關？	Yes 是 <input checked="" type="checkbox"/> No 否 <input type="checkbox"/> (Please proceed to Part 6 請繼續填寫第 6 部分)
(b) Full address/ location/ demarcation district and lot number (if applicable) 詳細地址／地點／丈量約份及地段號碼（如適用）	Lots Nos. 738 S.C. and 738 S.C. ss. 1 in D.D. 6 and adjoining Government Land, No. 74-75 Kam Shan Road, Tai Po, New Territories
(c) Site Area 申請地點面積	159.99 .....sq.m 平方米 <input checked="" type="checkbox"/> About 約



(d) Area of Government land included (if any) 所包括的政府土地面積 (倘有)	72.90 .....sq.m 平方米 <input checked="" type="checkbox"/> About 約
(e) Current use(s) 現時用途	<p>For Private Land Portion: Columbarium uses; For Government Land Portion: currently vacant</p> <p>(If there are any Government, institution or community facilities, please illustrate on plan and specify the use and gross floor area) (如有任何政府、機構或社區設施，請在圖則上顯示，並註明用途及總樓面面積)</p>

#### 4. "Current Land Owner" of Application Site 申請地點的「現行土地擁有人」

The applicant 申請人 -

- ☒ is the sole "current land owner"<sup>#&</sup> (please proceed to Part 6 and attach documentary proof of ownership).  
是唯一的「現行土地擁有人」<sup>#&</sup> (請繼續填寫第 6 部分，並夾附業權證明文件)。
- ☐ is one of the "current land owners"<sup>#&</sup> (please attach documentary proof of ownership).  
是其中一名「現行土地擁有人」<sup>#&</sup> (請夾附業權證明文件)。
- ☐ is not a "current land owner"<sup>#</sup>.  
並不是「現行土地擁有人」<sup>#</sup>。

- ☐ The application site is entirely on Government land (please proceed to Part 6).  
申請地點完全位於政府土地上 (請繼續填寫第 6 部分)。

#### 5. Statement on Owner's Consent/Notification

##### 就土地擁有人的同意/通知土地擁有人的陳述

- (a) According to the record(s) of the Land Registry as at .....17.10.2022..... (DD/MM/YYYY), this application involves a total of .....1..... "current land owner(s)"<sup>#</sup>.  
根據土地註冊處截至 .....2022..... 年 .....10..... 月 .....17..... 日的記錄，這宗申請共牽涉 .....1..... 名「現行土地擁有人」<sup>#</sup>。

(b) The applicant 申請人 -

- ☐ has obtained consent(s) of ..... "current land owner(s)"<sup>#</sup>.  
已取得 ..... 名「現行土地擁有人」<sup>#</sup>的同意。

Details of consent of "current land owner(s)" <sup>#</sup> obtained 取得「現行土地擁有人」 <sup>#</sup> 同意的詳情		
No. of 'Current Land Owner(s)' 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where consent(s) has/have been obtained 根據土地註冊處記錄已獲得同意的地段號碼/處所地址	Date of consent obtained (DD/MM/YYYY) 取得同意的日期 (日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has notified ..... “current land owner(s)”<sup>#</sup>  
已通知 ..... 名「現行土地擁有人」<sup>#</sup>。

Details of the “current land owner(s)” <sup>#</sup> notified 已獲通知「現行土地擁有人」 <sup>#</sup> 的詳細資料		
No. of ‘Current Land Owner(s)’ 「現行土地擁有人」數目	Lot number/address of premises as shown in the record of the Land Registry where notification(s) has/have been given 根據土地註冊處記錄已發出通知的地段號碼／處所地址	Date of notification given (DD/MM/YYYY) 通知日期(日/月/年)

(Please use separate sheets if the space of any box above is insufficient. 如上列任何方格的空間不足，請另頁說明)

- ☐ has taken reasonable steps to obtain consent of or give notification to owner(s):  
已採取合理步驟以取得土地擁有人的同意或向該人發給通知。詳情如下：

Reasonable Steps to Obtain Consent of Owner(s) 取得土地擁有人的同意所採取的合理步驟

- ☐ sent request for consent to the “current land owner(s)”<sup>#</sup>& on \_\_\_\_\_ (DD/MM/YYYY)  
於 \_\_\_\_\_ (日/月/年)向每一名「現行土地擁有人」<sup>#</sup>郵遞要求同意書<sup>&</sup>

Reasonable Steps to Give Notification to Owner(s) 向土地擁有人發出通知所採取的合理步驟

- ☐ published notices in local newspapers<sup>&</sup> on \_\_\_\_\_ (DD/MM/YYYY)  
於 \_\_\_\_\_ (日/月/年)在指定報章就申請刊登一次通知<sup>&</sup>
- ☐ posted notice in a prominent position on or near application site/premises<sup>&</sup> on \_\_\_\_\_ (DD/MM/YYYY)  
於 \_\_\_\_\_ (日/月/年)在申請地點／申請處所或附近的顯明位置貼出關於該申請的通知<sup>&</sup>
- ☐ sent notice to relevant owners’ corporation(s)/owners’ committee(s)/mutual aid committee(s)/management office(s) or rural committee<sup>&</sup> on \_\_\_\_\_ (DD/MM/YYYY)  
於 \_\_\_\_\_ (日/月/年)把通知寄往相關的業主立案法團/業主委員會/互助委員會或管理處，或有關的鄉事委員會<sup>&</sup>

Others 其他

- ☐ others (please specify)  
其他（請指明）

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Note: May insert more than one 「✓」.

Information should be provided on the basis of each and every lot (if applicable) and premises (if any) in respect of the application.

註：可在多於一個方格內加上「✓」號

申請人須就申請涉及的每一地段（倘適用）及處所（倘有）分別提供資料



**6. Plan Proposed to be Amended 擬議修訂的圖則**

(a) Name and number of the related statutory plan(s) 有關法定圖則的名稱及編號	Approved Tai Po Outline Zoning Plan No. S/TP/30
(b) Land use zone(s) involved (if applicable) 涉及的土地用途地帶(如適用)	"Village Type Development" ("V")

**7. Proposed Amendments 擬議修訂**

- (a) Propose to rezone the application site to the following zone(s)/use(s)  
(May insert more than one 「✓」) (Please illustrate the details on plan)  
建議將申請地點的用途地帶改劃作下列地帶 / 用途  
(可在多於一個方格內加上「✓」號)(請在圖則顯示詳情)

- |   |   |
|---|---|
| <input type="checkbox"/> Comprehensive Development Area [ ]<br>綜合發展區 [ ]  | <input type="checkbox"/> Commercial [ ] 商業 [ ]                                    |
| <input type="checkbox"/> Residential (Group <input type="checkbox"/> A/ <input type="checkbox"/> B/ <input type="checkbox"/> C/ <input type="checkbox"/> D/ <input type="checkbox"/> E) [ ]<br>住宅 ( <input type="checkbox"/> 甲類 / <input type="checkbox"/> 乙類 / <input type="checkbox"/> 丙類 / <input type="checkbox"/> 丁類 / <input type="checkbox"/> 戊類) [ ]  | <input type="checkbox"/> Village Type Development [ ]<br>鄉村式發展 [ ]                |
| <input type="checkbox"/> Agriculture [ ] 農業 [ ]   | <input type="checkbox"/> Industrial [ ] 工業 [ ]                                    |
| <input type="checkbox"/> Industrial (Group D) [ ] 工業 (丁類) [ ]   | <input type="checkbox"/> Open Storage [ ] 露天貯物 [ ]                                |
| <input checked="" type="checkbox"/> Government, Institution or Community [ 3 ]<br>政府、機構或社區 [ 3 ]  | <input type="checkbox"/> Open Space [ ] 休憩用地 [ ]                                  |
| <input type="checkbox"/> Recreation [ ] 康樂 [ ]  | <input type="checkbox"/> Green Belt [ ] 綠化地帶 [ ]                                  |
| <input type="checkbox"/> Country Park [ ] 郊野公園 [ ]  | <input type="checkbox"/> Coastal Protection Area [ ]<br>海岸保護區 [ ]                 |
| <input type="checkbox"/> Conservation Area [ ] 自然保育區 [ ]  | <input type="checkbox"/> Site of Special Scientific Interest [ ]<br>具特殊科學價值地點 [ ] |
| <input type="checkbox"/> Other Specified Uses ( <input type="checkbox"/> Business/ <input type="checkbox"/> Industrial Estate/ <input type="checkbox"/> Mixed Use/ <input type="checkbox"/> Rural Use/ <input type="checkbox"/> Petrol Filling Station/<br><input type="checkbox"/> Others (please specify _____)) [ ]<br>其他指定用途 ( <input type="checkbox"/> 商貿 / <input type="checkbox"/> 工業邨 / <input type="checkbox"/> 混合用途 / <input type="checkbox"/> 鄉郊用途 / <input type="checkbox"/> 加油站 /<br><input type="checkbox"/> 其他 (請註明: _____)) [ ] |   |
| <input type="checkbox"/> Road 道路  | <input type="checkbox"/> Others (please specify _____)<br>其他 (請註明: _____)         |

Please insert subzone in [ ] as appropriate.  
請於[ ]內註明支區，如適用。

## (b) Propose to amend the Notes of the Plan(s) 建議修訂圖則的《註釋》

☐ Covering Notes 《註釋》說明頁☒ Notes of the zone applicable to the Site 適用於申請地點土地用途地帶的《註釋》

Details of the proposed amendment(s) to the Notes of the Plan, where appropriate, are as follows:

(Please use separate sheets if the space below is insufficient)

建議修訂圖則的《註釋》的詳情，如適用：

(如下列空間不足，請另頁說明)

.....  
Please refer to the planning statement as attached.  
.....  
.....  
.....  
.....  
.....  
.....☒ Proposed Notes of Schedule of Uses of the zone attached  
夾附對《註釋》的擬議修訂**8. Details of Proposed Amendment (if any) 擬議修訂詳情 (倘有)**☒ Particulars of development are included in the Appendix.

附錄包括一個擬議發展的細節。

☐ No specific development proposal is included in this application.

這宗申請並不包括任何指定的擬議發展計劃。

**9. Justifications 理由**

The applicant is invited to provide justifications in support of the application. Use separate sheets if necessary.

現請申請人提供申請理由及支持其申請的資料。如有需要請另頁說明。

.....  
Please refer to the Planning Statement as attached.  
.....  
.....  
.....  
.....  
.....  
.....  
.....



This image shows a full page of primary-ruled paper. It features approximately 28 horizontal dotted lines spaced evenly down the page, providing a guide for handwriting practice. The paper is otherwise blank, with no margins or additional markings.

**10. Declaration 聲明**

I hereby declare that the particulars given in this application are correct and true to the best of my knowledge and belief.  
本人謹此聲明，本人就這宗申請提交的資料，據本人所知及所信，均屬真實無誤。

I hereby grant a permission to the Board to copy all the materials submitted in this application and/or to upload such materials to the Board's website for browsing and downloading by the public free-of-charge at the Board's discretion.

本人現准許委員會酌情將本人就此申請所提交的所有資料複製及/或上載至委員會網站，供公眾免費瀏覽或下載。

Signature  
簽署

☐ Applicant 申請人 / ☒ Authorised Agent 獲授權代理人

Kim On CHAN

Name in Block Letters

姓名（請以正楷填寫）

Managing Director

Position (if applicable)

職位（如適用）

Professional Qualification(s) ☐ Member 會員 / ☒ Fellow of 資深會員

專業資格

☒ HKIP 香港規劃師學會 / ☐ HKIA 香港建築師學會 /

☐ HKIS 香港測量師學會 / ☐ HKIE 香港工程師學會 /

☐ HKILA 香港園境師學會 / ☐ HKIUD 香港城市設計學會

☒ RPP 註冊專業規劃師

Others 其他 No. 29

on behalf of  
代表

Vision Planning Consultants Limited

☒ Company 公司 / ☐ Organisation Name and Chop (if applicable) 機構名稱及蓋章（如適用）

Date 日期

26.10.2022

(DD/MM/YYYY 日/月/年)

**Remark 備註**

The materials submitted in this application and the Board's decision on the application would be disclosed to the public. Such materials would also be uploaded to the Board's website for browsing and free downloading by the public where the Board considers appropriate.

委員會會向公眾披露申請人所遞交的申請資料和委員會對申請所作的決定。在委員會認為合適的情況下，有關申請資料亦會上載至委員會網頁供公眾免費瀏覽及下載。

**Warning 警告**

Any person who knowingly or wilfully makes any statement or furnish any information in connection with this application, which is false in any material particular, shall be liable to an offence under the Crimes Ordinance.

任何人在明知或故意的情況下，就這宗申請提出在任何要項上是虛假的陳述或資料，即屬違反《刑事罪行條例》。

**Statement on Personal Data 個人資料的聲明**

1. The personal data submitted to the Board in this application will be used by the Secretary of the Board and Government departments for the following purposes:

委員會就這宗申請所收到的個人資料會交給委員會秘書及政府部門，以根據《城市規劃條例》及相關的城市規劃委員會規劃指引的規定作以下用途：

- (a) the processing of this application which includes making available the name of the applicant for public inspection when making available this application for public inspection; and  
處理這宗申請，包括公布這宗申請供公眾查閱，同時公布申請人的姓名供公眾查閱；以及  
(b) facilitating communication between the applicant and the Secretary of the Board/Government departments.  
方便申請人與委員會秘書及政府部門之間進行聯絡。

2. The personal data provided by the applicant in this application may also be disclosed to other persons for the purposes mentioned in paragraph 1 above.

申請人就這宗申請提供的個人資料，或亦會向其他人士披露，以作上述第 1 段提及的用途。

3. An applicant has a right of access and correction with respect to his/her personal data as provided under the Personal Data (Privacy) Ordinance (Cap. 486). Request for personal data access and correction should be addressed to the Secretary of the Board at 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong.

根據《個人資料(私隱)條例》(第 486 章)的規定，申請人有權查閱及更正其個人資料。如欲查閱及更正個人資料，應向委員會秘書提出有關要求，其地址為香港北角渣華道 333 號北角政府合署 15 樓。



**APPLICATION FOR AMENDMENT OF PLAN UNDER  
SECTION 12A OF THE TOWN PLANNING ORDINANCE (CAP. 131)**

**根據城市規劃條例(第 131 章)第 12A 條遞交的修訂圖則申請**

Development Proposal (only for indicative purpose)

擬議發展的發展計劃 (只作指示用途)

**1. Development Proposal 擬議發展計劃**

<input checked="" type="checkbox"/> Proposed Gross floor area (GFA) 擬議總樓面面積	87.09 sq.m. 平方米	<input checked="" type="checkbox"/> About 約
<input checked="" type="checkbox"/> Proposed plot ratio 擬議地積比率	0.54	<input checked="" type="checkbox"/> About 約
<input checked="" type="checkbox"/> Proposed site coverage 擬議上蓋面積	54.43 %	<input checked="" type="checkbox"/> About 約
<input checked="" type="checkbox"/> Proposed number of blocks 擬議座數	1	
<input checked="" type="checkbox"/> Proposed number of storeys of each block 每座建築物的擬議層數	1 storeys 層	
	<input type="checkbox"/> include 包括.....storeys of basements 層地庫	
	<input type="checkbox"/> exclude 不包括.....storeys of basements 層地庫	
<input type="checkbox"/> Proposed building height of each block 每座建築物的擬議高度	..... m 米	<input type="checkbox"/> About 約
	..... mPD 米(主水平基準上)	<input type="checkbox"/> About 約
<input type="checkbox"/> Domestic part 住用部分		
GFA 總樓面面積	..... sq.m. 平方米	<input type="checkbox"/> About 約
number of units 單位數目	.....	
average unit size 單位平均面積	..... sq.m. 平方米	<input type="checkbox"/> About 約
estimated number of residents 估計住客數目	.....	
<input checked="" type="checkbox"/> Non-domestic part 非住用部分		
<input type="checkbox"/> hotel 酒店	87.09 sq.m.平方米	<input checked="" type="checkbox"/> About 約
	..... sq.m.平方米	<input type="checkbox"/> About 約
	(please specify the number of rooms 請註明房間數目: .....) )	
<input type="checkbox"/> office 辦公室	..... sq.m.平方米	<input type="checkbox"/> About 約
<input type="checkbox"/> shop and services/eating place 商店及服務行業/食肆	..... sq.m.平方米	<input type="checkbox"/> About 約
<input checked="" type="checkbox"/> Government, institution or community facilities 政府、機構或社區設施	(please specify the use(s) and concerned land area(s)/GFA(s)) (請註明用途及有關的地面面積/總樓面面積) Columbarium Use: 87.09 sq.m (existing building at the Subject Site), including 272 single-niches, 1, 062 twin- niches and 366 family-niches	
<input type="checkbox"/> other(s)其他	(please specify the use(s) and concerned land area(s)/GFA(s)) (請註明用途及有關的地面面積/總樓面面積) ..... ..... .....	
<input type="checkbox"/> Open space 休憩用地	(please specify land area(s)) (請註明面積)	
<input type="checkbox"/> private open space 私人休憩用地	..... sq.m.平方米	<input type="checkbox"/> Not less than 不少於
<input type="checkbox"/> public open space 公共休憩用地	..... sq.m.平方米	<input type="checkbox"/> Not less than 不少於

☐ Transport-related facilities 與運輸有關的設施

☐ parking spaces 停車位

(please specify type(s) and number(s))  
(請註明種類及數目)

Private Car Parking Spaces 私家車車位

Motorcycle Parking Spaces 電單車車位

Light Goods Vehicle Parking Spaces 輕型貨車泊車位

Medium Goods Vehicle Parking Spaces 中型貨車泊車位

Heavy Goods Vehicle Parking Spaces 重型貨車泊車位

Others (Please Specify) 其他 (請列明)

☐ loading/unloading spaces 上落客貨車位

(please specify type(s) and number(s))  
(請註明種類及數目)

Taxi Spaces 的士車位

Coach Spaces 旅遊巴車位

Light Goods Vehicle Spaces 輕型貨車車位

Medium Goods Vehicle Spaces 中型貨車車位

Heavy Goods Vehicle Spaces 重型貨車車位

Others (Please Specify) 其他 (請列明)

☐ other transport-related facilities

其他與運輸有關的設施

(please specify type(s) and number(s))  
(請註明種類及數目)

Use(s) of different floors (if applicable) 各樓層的用途(如適用)

[Block number] [座數]	[Floor(s)] [層數]	[Proposed use(s)] [擬議用途]
1	1	Columbarium use
.....	.....	.....
.....	.....	.....
.....	.....	.....
.....	.....	.....
.....	.....	.....

Proposed use(s) of uncovered area (if any) 露天地方(倘有)的擬議用途

.....  
Footpath, main entrance access point  
.....

Any vehicular access to the site? 是否有車路通往地盤?

Yes 是 ☒ There is an existing access. (please indicate the street name, where appropriate)

有一條現有車路。(請註明道路名稱(如適用))

Kam Shan Road

☐ There is a proposed access. (please illustrate on plan and specify the width)

有一條擬議車路。(請在圖則顯示，並註明車路的闊度)

No 否 ☐

**For Development involving columbarium use, please complete the table in the Annex to this Appendix.**

**如發展涉及靈灰安置用途，請填妥於此附件後附錄的表格。**



If necessary, please use separate sheets to indicate the proposed measures to minimise possible adverse impacts or give justifications/reasons for not providing such measures  
如需要的話，請另頁註明可盡量減少可能出現不良影響的措施，否則請提供理據/理由。

Appendix (Cont'd) 附錄 (續)

**For Developments involving Columbarium Use, please also complete the following:**  
**如發展涉及靈灰安置所用途，請另外填妥以下資料**

**Ash interment capacity 骨灰安放容量<sup>@</sup>**

Maximum number of sets of ashes that may be interred in the niches 在龕位內最多可安放骨灰的數量	3,860
Maximum number of sets of ashes that may be interred other than in niches 在非龕位的範圍內最多可安放骨灰的數量	N/a
<b>Total number of niches 龕位總數</b>	<b>1,700</b>
Total number of single niches 單人龕位總數	272
Number of single niches (sold and occupied) 單人龕位數目 (已售並佔用)	66
Number of single niches (sold but unoccupied) 單人龕位數目 (已售但未佔用)	96
Number of single niches (residual for sale) 單人龕位數目 (待售)	110
Total number of double niches 雙人龕位總數	1,062
Number of double niches (sold and fully occupied) 雙人龕位數目 (已售並全部佔用)	0
Number of double niches (sold and partially occupied) 雙人龕位數目 (已售並部分佔用)	0
Number of double niches (sold but unoccupied) 雙人龕位數目 (已售但未佔用)	0
Number of double niches (residual for sale) 雙人龕位數目 (待售)	1,062
Total no. of niches other than single or double niches (please specify type) 除單人及雙人龕位外的其他龕位總數 (請列明類別)	Family (4 members): 366
Number of niches (sold and fully occupied) 龕位數目 (已售並全部佔用)	0
Number of niches (sold and partially occupied) 龕位數目 (已售並部分佔用)	0
Number of niches (sold but unoccupied) 龕位數目 (已售但未佔用)	0
Number of niches (residual for sale) 龕位數目 (待售)	366

**Proposed operating hours 擬議營運時間** (a) & (b)

(a) 9am-5pm from Mondays to Saturdays;

(b) closure of the (proposed) existing private columbarium development during Ching Ming and Chung Yeung Festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day, as well as the 1st to 14th day of the Lunar Chinese New Year

<sup>@</sup> Ash interment capacity in relation to a columbarium means –  
就靈灰安置所而言，骨灰安放容量指：

- the maximum number of containers of ashes that may be interred in each niche in the columbarium;  
每個龕位內可安放的骨灰容器的最高數目；
- the maximum number of sets of ashes that may be interred other than in niches in any area in the columbarium; and  
在該靈灰安置所並非龕位的範圍內，總共最多可安放多少份骨灰；以及
- the total number of sets of ashes that may be interred in the columbarium.  
在該骨灰安置所內，總共最多可安放多少份骨灰。

<b>Gist of Application 申請摘要</b>			
(Please provide details in both English and Chinese as far as possible. This part will be circulated to relevant consultees, uploaded to the Town Planning Board's Website for browsing and free downloading by the public and available at the Planning Enquiry Counters of the Planning Department for general information.) (請盡量以英文及中文填寫。此部分將會發送予相關諮詢人士、上載至城市規劃委員會網頁供公眾免費瀏覽及下載及於規劃署規劃資料查詢處供一般參閱。)			
Application No. 申請編號	(For Official Use Only) (請勿填寫此欄)		
Location/address 位置／地址	Lots Nos. 738 S.C. and 738 S.C. ss. 1 in D.D. 6 and adjoining Government Land, No. 74 and 75 Kam Shan Road, Tai Po, New Territories		
Site area 地盤面積	159.99 sq. m 平方米 <input checked="" type="checkbox"/> About 約 (includes Government land of 包括政府土地 72.90 sq. m 平方米 <input checked="" type="checkbox"/> About 約)		
Plan 圖則	大埔分區計劃大綱核准圖編號 S/TP/30 Approved Tai Po Outline Zoning Plan No. S/TP/30		
Zoning 地帶	「鄉村式發展」 "Village Type Development"		
Proposed Amendment(s) 擬議修訂	<input type="checkbox"/> Amend the Covering Notes of the Plan 修訂圖則《註釋》的說明頁 <input checked="" type="checkbox"/> Amend the Notes of the zone applicable to the site 修訂適用於申請地點土地用途地帶的《註釋》 <input checked="" type="checkbox"/> Rezone the application site from "Village Type Development" to "Government, Institution or Community (3)" 把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」		
<b>Development Parameters (for indicative purpose only) 發展參數(只作指示用途)</b>			
(i) Gross floor area and/or plot ratio 總樓面面積及／或地積比率		sq.m 平方米	Plot Ratio 地積比率
	Domestic 住用	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	<input type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
	Non-domestic 非住用	87.09 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於	0.54 <input checked="" type="checkbox"/> About 約 <input type="checkbox"/> Not more than 不多於
(ii) No. of block 幢數	Domestic 住用		
	Non-domestic 非住用	1	
	Composite 綜合用途		



(iii) Building height/No. of storeys 建築物高度／層數	Domestic 住用	m 米 <input type="checkbox"/> (Not more than 不多於)
		mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於) ( <input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
	Non-domestic 非住用	m 米 <input type="checkbox"/> (Not more than 不多於)
		mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		1 Storeys(s) 層 <input checked="" type="checkbox"/> (Not more than 不多於) ( <input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
	Composite 綜合用途	m 米 <input type="checkbox"/> (Not more than 不多於)
		mPD 米(主水平基準上) <input type="checkbox"/> (Not more than 不多於)
		Storeys(s) 層 <input type="checkbox"/> (Not more than 不多於) ( <input type="checkbox"/> Include 包括 <input type="checkbox"/> Exclude 不包括 <input type="checkbox"/> Carport 停車間 <input type="checkbox"/> Basement 地庫 <input type="checkbox"/> Refuge Floor 防火層 <input type="checkbox"/> Podium 平台)
(iv) Site coverage 上蓋面積	54.43 % <input checked="" type="checkbox"/> About 約	
(v) No. of units 單位數目		
(vi) Open space 休憩用地	Private 私人	sq.m 平方米 <input type="checkbox"/> Not less than 不少於
	Public 公眾	sq.m 平方米 <input type="checkbox"/> Not less than 不少於

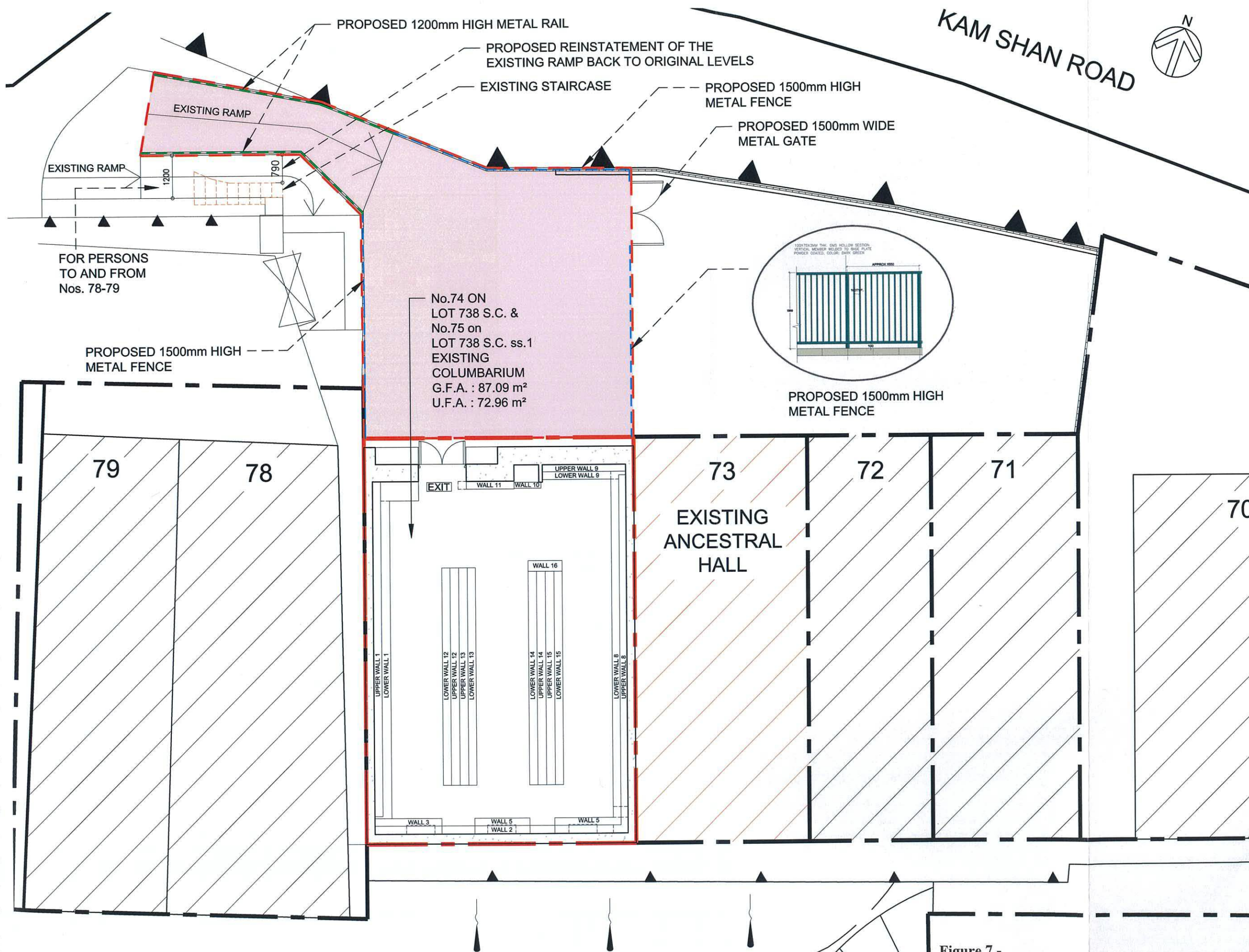
(vii) No. of parking spaces and loading / unloading spaces 停車位及上落客貨車位數目	Total no. of vehicle parking spaces 停車位總數  Private Car Parking Spaces 私家車車位 Motorcycle Parking Spaces 電單車車位 Light Goods Vehicle Parking Spaces 輕型貨車泊車位 Medium Goods Vehicle Parking Spaces 中型貨車泊車位 Heavy Goods Vehicle Parking Spaces 重型貨車泊車位 Others (Please Specify) 其他 (請列明) <hr/>	
	Total no. of vehicle loading/unloading bays/lay-bys 上落客貨車位／停車處總數  Taxi Spaces 的士車位 Coach Spaces 旅遊巴車位 Light Goods Vehicle Spaces 輕型貨車車位 Medium Goods Vehicle Spaces 中型貨車車位 Heavy Goods Vehicle Spaces 重型貨車車位 Others (Please Specify) 其他 (請列明) <hr/>	

Submitted Plans, Drawings and Documents 提交的圖則、繪圖及文件		
	Chinese 中文	English 英文
<b>Plans and Drawings 圖則及繪圖</b>		
Master layout plan(s)/Layout plan(s) 總綱發展藍圖／布局設計圖	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Block plan(s) 樓宇位置圖	<input type="checkbox"/>	<input type="checkbox"/>
Floor plan(s) 樓宇平面圖	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Sectional plan(s) 截視圖	<input type="checkbox"/>	<input type="checkbox"/>
Elevation(s) 立視圖	<input type="checkbox"/>	<input type="checkbox"/>
Photomontage(s) showing the proposed development 顯示擬議發展的合成照片	<input type="checkbox"/>	<input type="checkbox"/>
Master landscape plan(s)/Landscape plan(s) 園境設計總圖／園境設計圖	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他 (請註明)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Indicative columbarium niche wall diagrams		
<b>Reports 報告書</b>		
Planning Statement/Justifications 規劃綱領/理據	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Environmental assessment (noise, air and/or water pollutions) 環境評估 (噪音、空氣及／或水的污染)	<input type="checkbox"/>	<input type="checkbox"/>
Traffic impact assessment (on vehicles) 就車輛的交通影響評估	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Traffic impact assessment (on pedestrians) 就行人的交通影響評估	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Visual impact assessment 視覺影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Landscape impact assessment 景觀影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Tree Survey 樹木調查	<input type="checkbox"/>	<input type="checkbox"/>
Geotechnical impact assessment 土力影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Drainage impact assessment 排水影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Sewerage impact assessment 排污影響評估	<input type="checkbox"/>	<input type="checkbox"/>
Risk Assessment 風險評估	<input type="checkbox"/>	<input type="checkbox"/>
Others (please specify) 其他 (請註明)	<input type="checkbox"/>	<input type="checkbox"/>
<hr/> <hr/>		
Note: May insert more than one 「✓」. 註：可在多於一個方格內加上「✓」號		

Note: The information in the Gist of Application above is provided by the applicant for easy reference of the general public. Under no circumstances will the Town Planning Board accept any liabilities for the use of the information nor any inaccuracies or discrepancies of the information provided. In case of doubt, reference should always be made to the submission of the applicant.

註：上述申請摘要的資料是由申請人提供以方便市民大眾參考。對於所載資料在使用上的問題及文義上的歧異，城市規劃委員會概不負責。若有任何疑問，應查閱申請人提交的文件。





**NOTES :**  
DO NOT SCALE DRAWING.  
ALL DIMENSIONS MUST BE VERIFIED AT THE WORK BY THE CONTRACTOR.  
ALL PAPER, SPECIFICATIONS AND THEIR COPYRIGHT ARE THE PROPERTY OF THE ARCHITECTS AND SHALL BE RETURNED AT THE COMPLETION OF THE WORK.

**LEGEND :**

REZONING AREA (FOR "GIC(3)" ZONE) ---

RETAINING WALL ▲▲

Area to be submitted to LandsD for STT application upon approval (72.9m²)

Existing ancestral hall

Proposed 1500mm high metal fence ---

Proposed 1200mm high metal rail ---

REVISION		
NO.	DATE	DESCRIPTION

**ARCHITECT**  
**STUDIO | RCM**  
18/F | Xiu Hua Commercial Building  
211-213 Jaffe Road | Wan Chai | HK  
Tel: 3583-2000 | Fax: 8143-1130  
Email: mail@studiorca.com

**STUDIO | RAYMOND CHAU | ARCHITECTURE | LIMITED**

**PROJECT NAME**  
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" TO "GOVERNMENT, INSTITUTION OR COMMUNITY(3)" ZONE AT NOS. 71-75 KAM SHAN ROAD, TAI PO, NEW TERRITORIES  
CHEUNG HA CHING SHEA

**DRAWING TITLE**  
GROUND FLOOR PLAN

<b>DRAWN BY</b> JL	<b>CHECKED BY</b> RC
<b>SCALE</b> 1:100	<b>DATE</b> 07.10.2022
<b>JOB NO.</b> 1759	<b>DRAWING NO.</b> A-02

**Figure 7 -**  
**Indicative Layout of the (Proposed) Existing Columbarium**



**Rezoning Application from “Village  
Type Development” Zone to “Government,  
Institution or Community (3)” Zone at  
Nos. 74 and 75 Kam Shan Road  
and Adjoining Government Land,  
Tai Po, New Territories**

Applicants	: Fancy Lotus Limited
Planning Consultants	: Vision Planning Consultants Limited
Project Architect	: Studio Raymond Chau Architecture Limited
Traffic Consultants	: MVA Hong Kong Limited
Date of Submission	: 26th October, 2022

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## **EXECUTIVE SUMMARY**

This is a follow-up to previous rezoning proposals made under Application Nos. Y/TP/27 and Y/TP/32. It is a genuine attempt to work out a satisfactory solution in particular to meet Town Planning Board (“**TPB**”) Members’ lingering concerns on Application No. Y/TP/27 as expressed in the closed-door review meeting at the Deliberation Session on the 647<sup>th</sup> Meeting of RNTPC, and provide a standalone entrance access point with metal fence on the peripheral site boundary to strengthen the self-governing spatial arrangement in the area. The Applicant has retained all best practicable management methods that received positive responses from professional and technical and government departments in the present rezoning scheme.

The site under present proposed rezoning application is occupied partly by a row of two undivided (but sub-divided) single-storey old houses, namely Nos. 74 and 75 Kam Shan Road, respectively on Lots Nos. 738 S.C and 738 S.C ss.1 in D.D. 6 (a building lot with no user restrictions), and partly adjoining Government Land (“**GL**”) (hereinafter collectively called the “**Rezoning Area**”), Tai Po, New Territories. The present application aims to rezone the site from “Village Type Development” (“**V**”) to a site specific “Government, Institution or Community (3)” (“**G/IC(3)**”) sub-area to regularise an existing on-site columbarium to meet the licensing requirements under the Private Columbaria Ordinance (“**PCO**”).

Same as the previous two latest rezoning applications, this rezoning application comprises a total of 1,700 columbarium niches in three categories (i.e. 272 for single-niches; 1,062 for twin-niches and 366 for family-niches up to 4 members). They include 162 sold single-niches and 50 twin-niches (50 x 2=100 sets of ashes) now to be offered to indigenous villagers of Kam Shan Village at the average selling prices of public columbarium niches in that period of time.

The operation modes of this rezoning scheme include, but not limited to, the closure of operation during the Special Festival Periods, and the mandatory ‘visit-by-appointment’ system and house rules. These operation modes will be fully incorporated into the Management Plan to be submitted to the Private Columbaria Licensing Board (“**PCLB**”) under PCO soon after this rezoning application has been agreed by TPB. On 29.12.2021, the PCLB, after its careful and all-thought-out consideration, granted the Applicant an ‘Approval In-principle for Temporary Suspension of Liability (“**TSOL**”) with validity up to 29.12.2024 under PCO.

Achievable planning merits include:

- (i) responding to TPB Members’ concerns on the TP/27 Scheme;

- (ii) meeting the Licensing Scheme objectives under the PCO for existing private columbaria;
- (iii) saving at least 100 pieces of burial grounds (about 9,600ft<sup>2</sup> of land) entitled for granting to indigenous villagers of Kam Shan Village;
- (iv) encouraging local villagers to adopt cremation rather than ground burial;
- (v) rectifying the different uses in “V” zone with proper controls that have the supports from local villagers;
- (vi) providing a standalone entrance access point with metal fence around the site boundary to further minimise any possible annoyance to its surrounding development;
- (vii) setting a good practice and socially responsive example for the industry;
- (viii) enabling to provide a best physical distance from its nearby residential dwellings;  
and
- (ix) offering a meaningful alternative burial solution the community.

If this rezoning application is agreed by TPB, all the above planning merits will illustrate the exemplary spirit of adherence to the declared policy of the Administration.



## 行政摘要

是次改劃申請是針對先前改劃申請編號Y/TP/27及Y/TP/32方案之未能完善方面作出修正，認真地提出解決城規會委員在第 647 次的「鄉郊及新市鎮規劃小組委員會」的閉門會議中對申請編號 Y/TP/27 所提出的疑問，以及再提供一個獨立的出入口和加建圍欄及欄杆，進一步優化區內鄰里獨立活動空間的安排。是次擬議改劃方案完全保留了所有早前已獲得相關政府部門在專業及技術方面考慮後給予正面回應的創新管理方法。「申請地點」位於新界 大埔 錦山路 第 74 至 75 號（分別在丈量約份第 6 約地段第 738 S.C 及 738 S.C ss.1 號地段）一排不可分拆但被分割為兩間小屋的單層高古建築地塊（一片沒有用途限制的建築地塊）及毗連政府土地。申請人希望把申請地點由現時的「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶，藉此規範化現有的骨灰安置所用途，以符合根據《私營骨灰安置所條例》的發牌要求。

是次改劃申請合共有 1,700 個分配在三個不同類別的骨灰龕位：即單灰位有 272 個；雙灰位有 1,062 個；及四灰(家族)位有 366 個。所有骨灰龕位，分配在錦山路 74 及 75 號屋內；在擬議骨灰龕位總數量內，已包含 162 個已出售的龕位，以及擬議預留以當時公營龕位出售價之平均值的龕位出售給錦山村原住民的 50 個雙灰位(等同  $50 \times 2 = 100$  灰位)。

擬議改劃方案提出的營運或作業模式，包括在清明節及重陽節期間不營業、強制性「預約拜訪」機制、相關的交通安排，以及安放骨灰龕位的指定位置和配置分佈安排，都會包含於根據《私營骨灰安置所條例》下申請私營骨灰安置所牌照時必須提交的管理方案內，並由私營骨灰安置所發牌委員會直接監察和督導。在 2021 年 12 月 29 日該發委員會根據《私營骨灰安置所條例》經過審慎考慮後，批准申請人的暫免法律責任書的申請，有效期至 2024 年 12 月 29 日。

擬議改劃方案包括了以下規劃優點：

- (i) 回應「城規會」就先前改劃申請編號 Y/TP/27 提出疑問；
- (ii) 符合《私營骨灰安置所條例》對現有私營骨灰安置所的發牌政策目標；
- (iii) 節省最少 100 片（約 9,600 平方尺）給錦山村原住民的現擬議土葬地塊；
- (iv) 鼓勵原住民以火化形式取替傳統的土葬；
- (v) 修正「鄉村式發展」地帶內的不同用途，並採用更恰當的土地用途規劃來管治，同時也得到村民的支持；
- (vi) 提供一個獨立出入口，並以鐵欄圍封申請範圍，進一步減低對附近居民的滋擾；
- (vii) 成為業界及回應社區關注的好典範；
- (viii) 能夠提供與周邊住宅發展的最大隔離空間；及
- (ix) 就社區對這類設施提供一個有意義的選擇。

倘若「城規會」同意是次改劃申請，所有上列優點都能完全體現出落實政府施政合作夥伴的精神。

## 1. The Purpose

- 1.1 Vision Planning Consultants Limited has been commissioned by Fancy Lotus Limited (hereinafter called the “**Applicant**”) to prepare and to submit this rezoning application on its behalf. The site under the present rezoning application occupies a row of two undivided (but sub-divided) single-storey houses built in early 1910s, namely Nos. 74 and 75 Kam Shan Road (錦山路) and a small portion of adjoining vacant Government Land (“**GL**”) (hereinafter collectively called the “**Rezoning Area**”), Tai Po (大埔), New Territories. The Applicant is the owner of Lots Nos. 738 S.C (No. 74 Kam Shan Road) and 738 S.C ss.1 (No. 75 Kam Shan Road) in D.D. 6. **Figure 1** and **Figure 2** show the location of the Rezoning Area.
- 1.2 In the confirmed Minutes of the 647<sup>th</sup> Meeting of the RNTPC, held on 26.5.2020, which considered the previous rezoning application under Application No. Y/TP/27 (“**TP/27 Scheme**”) which was submitted to the Town Planning Board (“**TPB**”) on 4.6.2018 (**Appendix I**), some TPB Members made the following comments during the Deliberation Session:
- “...the Site was adjacent to an ancestral hall, thus a columbarium use was not entirely incompatible. Beside, as the subject columbarium was relatively small in scale....., its traffic impact should not be significant.....in view that some local residents had no objection to the subject columbarium, .....the application might warrant sympathetic consideration”;
  - “there was no planning gain to justify the current proposal”;
  - “the subject columbarium was located too close to residential dwellings and hence the rezoning application could not be supported”;
  - “would set undesirable precedent for other similar applications”; and
  - “the columbarium use at the Site was considered not compatible with the surroundings”.
- 1.3 The Applicant **felt aggrieved** as such questions were not raised in the presence of its representatives during the ‘Question & Answer’ (“**Q&A**”) Session. If so, they could have provided the needed information to facilitate TPB Members to consider the application in a more holistic manner.

- 1.4 In view of the above, on 21.10.2020, the Applicant submitted to TPB a fresh rezoning application (Application No. Y/TP/32) (“**TP/32 Scheme**”) with an attempt to split the same amount of columbarium niches proposed in TP/27 Scheme into Nos. 72 and 74 Kam Shan Road aiming to respond to TPB Members’ queries in a more holistic manner for more sympathetic consideration.
- 1.5 It should be noted that the present proposed rezoning scheme (“**PRS**”) is basically the same as those in latest previous rezoning schemes, namely TP/27 Scheme and TP/32 Scheme in terms of the nature and the scale of development, the number of columbarium niches and the best practical operation modes. The location of the proposed columbarium uses under application (i.e. at Nos. 74 and 75 Kam Shan Road) is exactly the same as that in TP/27 Scheme and that registered in the PCLB. In doing so, the subsequent application for a private columbarium licence can then be processed directly by the PCLB under PCO and as matter of fact that PCLB may not proceed further the case if the location of the existing columbarium niches is not the same as that registered under TSOL.
- 1.6 **Table 1** compares the key development parameters and operation modes between the TP/27 Scheme, the TP/32 Schemes and the PRS. Basically, these three schemes are the same in terms of the nature and scale of development, proposed number of niches and the best practicable operation modes.

**Table 1 Comparison of Key Development Parameters and Modes of Operation in TP/27 Scheme, TP/32 Scheme and the PRS**

	TP/27 Scheme# (a)	TP/32 Scheme@ (b)	PRS# (c)	Difference [(c)-(b)] (%)
<b>Key Development Parameters</b>				
Site Area (about)	87.09m <sup>2</sup>	230.64m <sup>2</sup>	159.99m <sup>2</sup>	-70.65m <sup>2</sup> (-30.63%)
- private lot	87.09m <sup>2</sup>	230.64m <sup>2</sup>	87.09m <sup>2</sup> (54.3%)	
- GL	0m <sup>2</sup>	0m <sup>2</sup>	72.90m <sup>2</sup> (45.7%)	
Total Floor Area (about)	87.09m <sup>2</sup>	230.64m <sup>2</sup>	87.09m <sup>2</sup>	-143.55 m <sup>2</sup> (-62.24%)
Plot Ratio	1.0	1.0	0.54	-0.46 (-46%)
Site Coverage	100%	100%	54.43%	-0.4557 (-45.57%)
No. of Block	1	1	1	<b>No Change</b>
No. of Storey	1 storey	1 storey	1 storey	<b>No Change</b>
Total GFA for Columbarium Use	87.09m <sup>2</sup>	About 80m <sup>2</sup>	87.09m <sup>2</sup>	+7.09 m <sup>2</sup> (+8.86%)
Total UFA for Columbarium Use	50.99m <sup>2</sup>	About 51.46m <sup>2</sup>	About 72.96m <sup>2</sup>	21.50 m <sup>2</sup> (+41.78%)
Number of Niches*	1,700 (162)*	1,700 (162)*	1,700 (162)*	<b>No Change</b>
• single-	• 272 (162)*	• 272 (162)*	• 272 (162)*	
• twin-	• 1,062 (0)	• 1,062 (0)	• 1,062 (0)	
• family-	• 366 (0)	• 366 (0)	• 366 (0)	
<b>Operation Modes</b>				
Special Arrangements			• provide a standalone pedestrian entrance with metal fence to be erected alongside the site boundary	Further minimize the potential nuisance to its neighbouring residential dwellings

	<ul style="list-style-type: none"> <li>closure of the (proposed) existing private columbarium development during Ching Ming and Chung Yeung Festival Days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day</li> </ul>	<ul style="list-style-type: none"> <li>closure of the (proposed) existing private columbarium development during Ching Ming and Chung Yeung Festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day, as well as the 1st to 14th day of the Lunar Chinese New Year</li> </ul>	Additional innovative mode of operation added
	-	<ul style="list-style-type: none"> <li>a total of 50 twin-niches are proposed to offer to the indigenous of Kam Shan Village at the latest average selling prices of public columbarium niches</li> </ul>	Remain unchanged
	<ul style="list-style-type: none"> <li>adoption of the “visit-by-appointment” system</li> </ul>		Remain unchanged

# Columbarium uses at No. 74 and 75 Kam Shan Road; @ Columbarium uses at Nos. 72 and 74 Kam Shan Road;

\* Number of niches already sold

- 1.7 Apart from the provision of a new standalone entrance access point with metal fence to be erected alongside the site boundary, the PRS has retained all best practicable management methods that received positive responses from concerned government departments from their professional and technical perspectives on the TP/27 Scheme (Table 1). Table 2 summarises all the relevant government Bureaux and departments that had no objection to/ no adverse comment on the TP/27 Scheme.

**Table 2 No Objection/ No Adverse Comment from Relevant Government Bureaux and Departments on TP/27 Scheme**

Department	No Objection/ No Adverse Comment
1. DLO/TP, LandsD	✓
2. C for T	✓
3. DEP	✓
4. UD&L, PlanD	✓
5. DSD	✓
6. D of FS	✓
7. BD	✓
8. S for Food Health	✓
9. DFEH	✓
10. WSD	✓
11. HyD	✓
12. PM, CEDD	✓
13. GEO, CEDD	✓
14. C of P	✓
15. HAD	✓

- 1.8 In view of the following considerations, all the above no objections/adverse comments from relevant government departments and bureaux should remain applicable to and valid for the PRS. As such, attention can be mainly focused on the remaining queries/concerns raised by Members of TPB on TP/27 Scheme.

- i. the overall nature and scale of development, number of proposed (existing) columbarium niches, general site location and the proposed operation modes of the PRS are basically the same as those in the TP/27 Scheme (**Table 1**);
  - ii. the TP/27 Application was considered by TPB on 26.5.2020;
  - iii. no material changes in the local land-use planning and road traffic circumstances has taken place since 26.5.2020; and
  - iv. due to (i), (ii) and (iii) above, for the same proposed development scheme at the same number of houses. The valuable time and extensive efforts made by the same government departments on the TP/27 Scheme should be respected and their positive responses retained and made applicable to this rezoning application.
- 1.9 To address TPB Members’ concerns in particular the existing columbarium is *“being located too close to residential dwellings”* and at the same time, as an attempt to meet the objective set out for the Licensing Scheme under PCO (i.e. to minimise nuisance to surrounding neighbourhood community), the Applicant has taken the liberty to provide a standalone entrance access point with metal fence to be erected around the Rezoning Area in the PRS.
- 1.10 In addition, the Applicant also consulted directly the existing resident of No.78 Kam Shan Road about the proposed (existing) columbarium uses at the Rezoning Area. Having considered the following factors, she has indicated that she does not object the proposed (existing) columbarium uses to be continued to operate next to them (**Appendix II**) and fully understood that such facility is acceptable and is a much-needed community facility nowadays like the ancestral hall in each village. Indeed, to them such facility is considered more friendly than convenience store and/or restaurant in terms of nuisances.
- i. the scale of development under application is small;
  - ii. the nature of the proposed (existing) columbarium is similar to that of the ancestral hall in each village;
  - iii. the physical appearance of the proposed (existing) columbarium is acceptable;
  - iv. even the proposed (existing) columbarium development is located right next to their house, they do not have any psychological problem at all. We are now



- living in 21 century and as matter of fact that such facility will consider more friendly than those annoying and inconsiderate neighbours;
- v. the future operation of the proposed (existing) columbarium will have to comply with the future Management Plan to be considered carefully and be registered in the PCLB under PCO, so it will be a controlled and enforceable operation;
  - vi. no nuisances in terms of noise, air-quality, hygiene or pedestrian flows due to the proposed (existing) columbarium use are anticipated as the whole operation will be strictly monitored and be enforced by the PCLB under PCO;
  - vii. there is no one prefect system to comfort all citizen, but a good administrative mechanism like PCLB under PCO can help improve the situations in a holistic manner towards building a harmony social inclusion neighbourhood community; and
  - viii. the approval of the proposed (existing) columbarium will also benefit to their families and/or nearby residents to place their deceased family members at this easily accessible location.
- 1.11 Indeed, since the first rezoning application to TPB on 16.11.2012 (i.e. Application No. Y/TP/18), the Applicant has been actively in cooperation to keep on providing all possible and practically feasible measures to improve this existing and much-needed local community facility from scale of development, to modes of operation, to the location of columbarium niches, and to the provision of a standalone main entrance access point.
- 1.12 Presently, the Rezoning Area falls within an area zoned “Village Type Development” (“V”) on the approved Tai Po Outline Zoning Plan (the “OZP”) No. S/TP/30 (**Figure 1**). In the Notes of the OZP, for “V” zone, ‘Columbarium’ use is neither a Column 1 nor a Column 2 use (**Appendix III**). Hence, a rezoning application under s.12A of the Town Planning Ordinance is the only planning mechanism to regularise the existing private columbarium development, which is fully legal under the lease, so as to allow it to continue to apply for a private columbarium licence under the PCO thereafter.
- 1.13 On 15.1.2019, the Applicant submitted to the PCLB an application for Licence, Exemption and TSOL under PCO, in order to regularise the current columbarium operation. On 29.12.2021, the PCLB granted the Applicant an “Approval-in-

principle for TSOL Application” with validity up to 29.12.2024 (**Appendix IV**). The Applicant is committed to expediting action to obtain approvals of the above applications under the PCO.

- 1.14 The purpose of this rezoning application aims to rezone the Rezoning Area from “V” to “Government, Institution or Community (3)” (“**G/IC(3)**”) to make way for regularising the existing 1,700 columbarium niches in three categories, namely single-niches, twin-niches and family-niches (up to 4 niches).
- 1.15 A proposed Schedule of Uses (“**SoU**”) for the proposed “G/IC(3)” sub-area under this rezoning application is attached in **Appendix V**. ‘Columbarium’ is under Column 1 use strictly adhered to site specific items, requirements and parameters stipulated in the Remarks attached to the draft Notes of this “G/IC(3)” sub-area. As such, TPB and relevant Government departments would have another opportunity to further improve the operation modes of the proposed (existing) columbarium uses, if and when required. Indeed, the scale of development under application is strictly limited at Nos. 74 and 75 Kam Shan Road with a view to standing for a unique commitment to TPB.
- 1.16 Upon receipt of the TPB’s agreement on the present rezoning application and the TPB’s approval on the subsequent planning application, all relevant planning and operation requirements set out in the proposed (existing) columbarium uses at the Rezoning Area will strictly be incorporated into the future Management Plan (“**MP**”) that will be reviewed and considered carefully by the PCLB under PCO, and upon its approval, this MP will be registered in the PCLB for on-going monitoring and reviewing from time to time to meet the changes in local circumstances, if and when necessary, under the PCO. A draft MP considered by the PCLB on application for TSOL is provided in **Appendix VI**.
- 1.17 The existing private columbarium has been in operation since late 2007 and contains a total of 3,860 single columbarium niches. However, in order to minimise the potential traffic impact on the local road network, to provide a greater degree of comfort inside the columbarium halls and to meet the soaring demand for twin-niches and family-niches (up to 4 members of family) in the community, the Applicant has decided to divide the existing 3,860 single-niches into three non-convertible columbarium niches [i.e. single-niches, twin-niches and family-niches

(up to 4 family members)] resulting in a total of 1,700 columbarium niches. The latter two types of niches are purpose-designed for deceased married couples and families. This also represents a social responsive proposal to meet the actual needs of the community.

- 1.18 Same as that in previous rezoning schemes, the PRS will also close the operation of the proposed private columbarium development during Ching Ming and Chung Yeung Festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each festival day (hereinafter collectively called the “**Special Festival Periods**”). This represents a dual-purpose proposal to tackle the local road traffic issues during such Special Festival Periods on one hand, and, on the other hand, to provide a meaningful alternative choice for the community.
- 1.19 Apart from this, the Applicant also proposes a closure during the 1st to 14th day of the Lunar Chinese New Year. If the present rezoning application is agreed by TPB, such operation modes will be stated clearly in the future MP and the Agreement of Sale to be monitored by the PCLB under PCO.
- 1.20 To facilitate Members of TPB to understand the background of the Rezoning Area and the PRS in details, this rezoning statement includes: the primary objectives of the Administration to establish a Licensing Scheme under PCO, history of Lot 738 and its surrounding conditions, the details of the proposed private columbarium development, the long lasting non-rectified uses within the subject “V” zone over the past 40 years, a brief outline of the indicative proposed rezoning scheme, as well as the planning merits that will be brought by the present rezoning application.

## **2. Objectives of Licensing Scheme**

- 2.1 Under PCO, a Licensing Scheme via PCLB to monitor and to oversee the existing and new private columbaria is in place with the following objectives<sup>1</sup>:
1. **ensuring that private columbaria are in compliance with statutory and government requirements;**
  2. **enhancing protection of consumer interests; and**

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<sup>1</sup> [www.fehd.gov.hk/rpc/english/index.htm](http://www.fehd.gov.hk/rpc/english/index.htm)

3. **ensuring that the industry adopts a sustainable mode of operation.**

2.2 In its Legislative Council Paper for Private Columbaria Bill (2016), the Foods and Health Bureau (“**FHB**”), represented the Administration to point out clearly that in achieving the above three specific objectives, there is a need to make efforts to strike a fine balance amongst the following competing interests<sup>2</sup>:

- (a) **the wider community interest, including meeting society’s needs for provision of columbaria;**
- (b) **the sentiment of the descendants, in particular their wish not to upset the resting place of the deceased as far as possible;**
- (c) **minimising the nuisance caused by such columbaria to their neighbourhood community** [for the present rezoning application, the psychological effect to the immediate neighbours should be minimised as far as practically feasible]; and
- (d) **the sustainable development of the trade in the long-run.**

2.3 Other supportive considerations include:

- (a) the fact that it is an existing use, permissible under the equity rights of the lease and done before the enactment of the PCO;
- (b) the over-general use zoning carried out 40 years ago by the OZP; and
- (c) the general “no objection” view of concerned professional/technical government departments to the innovative measures to meet their concerns in the TP/27 Scheme (**Table 2**).

2.4 The enactment of PCO and the subsequent licensing mechanism have represented a well-thought-out policy of the Administration “*to regulate and to rectify existing and potential private columbarium facilities*”, where conditions permit, through the PCLB under PCO.

2.5 Under reasonable circumstances, Government departments and Members of TPB are invited to act in unison with this well-thought-out policy of the

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<sup>2</sup> Para. 3 in LC Paper No. CB(2)587/15-16(05) on 12 January 2016

Administration to fully reflect the cooperation spirit between the various segments of government.

### **3. Rezoning Area and Its Surroundings**

- 3.1 The Rezoning Area under application comprises Nos. 74 and 75 Kam Shan Road and adjoining vacant GL, Tai Po, New Territories (**Figure 3**). It contains two undivided, but being sub-divided, single-storey old houses on Lot No. 738 in D.D. 6 (i.e. 738 S.C being No. 74 Kam Shan Road and 738 S.C ss.1 being No. 75 Kam Shan Road) and a small piece of vacant GL in front of these houses. It is located at the peripheral area of Kam Sheung Village and fronting directly to Kam Shan Road. Lot No. 738 is an isolated building lot and no direct physical connection with its adjoining buildings on two sides.
- 3.2 It covers a total land area of about 159.99m<sup>2</sup> [i.e. 87.09m<sup>2</sup> (or 54.43%) being private lots + 72.90m<sup>2</sup> (or 45.57%) being GL] (**Figure 2** and **Table 3**). The subject undivided building was built over 100 year ago.

**Table 3 Land Area Breakdown of the Rezoning Area**

<b>Street Number</b>	<b>Land Area (m<sup>2</sup>)</b>
74 Kam Shan Road	40.06
75 Kam Shan Road	47.03
<i>Sub-total</i>	<i>87.09 (54.43%)</i>
GL	72.90 (45.57%)
<b>Total</b>	<b>159.99 (100.0%)</b>

- 3.3 The **history of Lot 738** is summarised below:
- Lot 738 is governed by a New Grant by public auction for the term of 75 years from 1<sup>st</sup> day of July, 1898, with the right of renewal for a further term of 24 years less 3 days at a re-assessed Government Rent as Building Lots subject to the General Conditions of Sale published in Government Notification No. 365 of 1906 (Annex A in **Appendix VII**).
  - Lot 738 was a Building Lot sold by public auction in 1909 (Sales Document attached in Annex B of **Appendix VII**). The dimensions of Lot 738 were 70 feet for the north and south boundaries and 40 feet for the east and west boundaries.

- iii. In 1923, Lot 738 was divided into Lots 738 S.A (2 houses), 738 S.B (1 house), and 738 S.C (2 houses). The division of property was registered in Land Registry by Memorial No. TP57471 (attached in Annex C of **Appendix VII**).
  - iv. In 1951, Lot 738 S.C was further divided into Lot 738 S.C ss.1 and Lot 738 S.C. The Site Plan showing the division of the Lots was registered in the Land Registry by Memorial No. TP126990 (attached in Annex D of **Appendix VII**).
  - v. On the Site Plan, the user restriction of the five division lots of Lot 738 was indicated as ‘Temple’ (Annex B in **Appendix VII**).
  - vi. The five houses on ground do not cover the whole area of Lot 738.
  - vii. In its comments on the TP/27 Application, the District Lands Office/Tai Po states that “.....*subject to further legal advice, the applied use in a single-storey building may be permitted under lease and in such case, if the application is approved by the Board, the applicant is not required to seek a lease modification from LandsD to implement it.....*” [para 9.1.1 (b) in the RNTPC Paper for Y/TP/27C, 7.2.2020].
  - viii. Lot 738 is a **Building Lot with no user restrictions**.
- 3.4 At present, Nos. 71 and 72 Kam Shan Road are presently used as domestic store/vacant; No. 73 Kam Shan Road is the ancestral hall, known as the Yau’s Ancestral Hall - 邱氏祠堂; the two sub-divided lots of Lot 738 under application (i.e. Nos. 74 and 75 Kam Shan Road) are presently occupied by private columbarium uses.
- 3.5 The existing private columbarium development at Nos. 74 and 75 Kam Shan Road commenced operation since late 2007 and is known as Cheung Ha Ching Shea (祥霞精舍). The condition of the existing private columbarium is shown in **Figure 4**. The existing private columbarium development contains a total of 3,860 single-niches. Upon the TPB’s agreement on the present rezoning application, these columbarium niches will be divided into three types of columbarium niches (i.e.

single-, twin- and family-niches) with a maximum of 1,700 columbarium niches in total. It represents **a reduction of 56%**.

- 3.6 As confirmed by the Applicant, of the total proposed 1,700 columbarium niches, 162 columbarium niches were sold before the cut-off date of 8am 18<sup>th</sup> June 2014 under PCO (i.e. 66 of them being interred with ashes and 96 being not yet interred with ashes).
- 3.7 **Table 4** shows a breakdown of the future columbarium niches by types currently at Nos. 74 and 75 Kam Shan Road. The Applicant of the TP/27 Scheme had already reported to the Food and Environmental Hygiene Department (“**FEHD**”) the number of the actual sold columbarium niches in 2014, no further sell-and-purchase agreement has been undertaken with respect to the columbarium niches under application since then.

**Table 4 Breakdown of Columbarium Niches by Types (Same as TP/27 and TP/32 Schemes)**

	Single-Niche	Twin-Niche	Family-Niche (up to 4 Niches)	Total
Niches sold <b>and</b> interred with ashes before 30.6.2017	66	0	0	<b>66</b>
Niches sold but not yet interred with ashes before 30.6.2017	96	0	0	<b>96</b>
Vacant niches not yet sold	110	1,062	366	<b>1,538</b>
<b>Total</b>	<b>272</b>	<b>1,062</b>	<b>366</b>	<b>1,700</b>

- 3.8 The Rezoning Area is located south of Kam Shan Road (**Figure 2** and **Figure 3**). It is actually situated at the peripheral boundary of Kam Shan Village. At present, the Rezoning Area has been served by a pedestrian ramp which connects directly to Kam Shan Road for a long period of time. Photo 2 and Photo 4 in **Figure 3** show the condition of this existing pedestrian ramp.
- 3.9 **Figure 5** shows the existing non-Small House developments along the two sides of Kam Shan Road. They include shop and services, temples, columbaria, international school (about 3,500m<sup>2</sup> in extent), school, recreational areas, public convenience, refuse collection point, factory, tutorial school and eating place.

- 3.10 The Rezoning Area enjoys extremely high accessibility offered by various public transportation facilities. MTR Tai Wo Station is only about 200m to its northeast requiring only 5 to 10 minute walking time to reach the Rezoning Area via Tai Wo Plaza Phase II down to Kam Wo Bridge, and then walk further 150m alongside Lam Tsuen River to reach the Rezoning Area (**Figure 6**); a Public Transport Interchange (“**PTI**”) is located below MTR Tai Wo Station, this PTI provides various routes of franchised buses (Nos. 64K, 64P, 65K, 71K, 72, 73, 73A, 73B, 74A, 74C, 74D, 264R, 271P, 272E, 273C, 273P, 278P, 307P, 907B, 907C, N73 and T74) and Green Mini Bus (“**GMB**”) services (Nos. 21K, 25A, 25B and 25K). GMB No. 22K operates along Kam Shan Road, connecting Kam Shan to Tai Po Market MTR Station (**Figure 6**).

#### **4. Rezoning Proposals**

##### ***The Scheme (Figures 7 – 14)***

- 4.1 As mentioned in paragraphs 1.2 and 1.9 above, the present rezoning application attempts to further improve the development scheme in a well-thought-out implementable manner to address the lingering concerns raised by TPB Members at the Deliberation Session of 647<sup>th</sup> Meeting of RNTPC which considered the TP/27 Scheme and to take this opportunity to make further additional effort to refine the general setting of the proposed development by providing a standalone entrance access point with peripheral metal fence around the Rezoning Area to meet the Licensing Scheme’s objective.
- 4.2 To minimize the potential interface issues that may be caused to the adjacent village houses, the Applicant now proposes the installation of metal rails (1.2m tall) along the two sides of the existing ramp and metal fence (1.5m tall) around the peripheral boundary of the Rezoning Area (**Figure 7**). If TPB agrees to the present rezoning application, the proposed (existing) private columbarium halls will be completely fenced off from its nearest residential units at Nos.78 and 79 Kam Shan Road to its west, and the existing Ancestral Hall at No. 73 Kam Shan Road to its east. The operation of the existing Ancestral Hall will remain unaffected. A support letter issued by the representatives of Yau’s Ancestral Hall for the PRS is attached in **Appendix VIII**). A new gated access with a set of spare keys will be given to the representatives of the Yau’s Ancestral Hall for as well as to owners of Nos. 71 and



72 Kam Shan Road for private access. The Applicant will manage and maintain the peripheral railing, fence and the entrance gate at its own expenses.

- 4.3 The present proposed standalone entrance access point with metal fence around the edge of the subject GL under application aims to provide a self-governing pocket of land solely for the use of the G/IC(3) sub-area. As such, all on-site activities will be strictly confined within the new sub-area. It is noteworthy from the owners of Nos. 71 and 72 Kam Shan Road that the respective structures are mainly for ancillary storage use, in which limited visits by the owners are anticipated. A no objection letter issued by the owner of Nos. 71 and 72 Kam Shan Road with respect to the proposed (existing) columbarium use at the Rezoning Area is provided in **Appendix IX**.
- 4.4 The existing pedestrian access connecting Nos. 78 and 79 Kam Shan Road to Kam Shan Road remains unaffected (**Figure 7**). They can remain enjoy their existing staircase which was built in 1951 as shown in Photo 4 in **Figure 3**.
- 4.5 Upon TPB’s agreement on the present rezoning application, the Applicant will submit to the Lands Department an application for a Short-Term Tenancy (“**STT**”) to use, manage and maintain the subject GL under application (**Figure 7**). All the above-mentioned enhancement measures will also be incorporated in the future MP of the proposed (existing) columbarium development for obtaining the private columbarium licence application from the PCLB under PCO thereafter.

***The (Existing) Private Columbarium (Tables 3 - 4 and Figures 8 – 13)***

- 4.6 Same as that in TP/27 and TP/32 Schemes, the present rezoning application comprises of a total of 1,700 columbarium niches in three categories (**Table 4**); namely single-, twin- and family-niches (**Figures 8 - 13**). The total gross floor area (“**GFA**”) in the existing columbarium is about 87m (**Table 3**).

In this rezoning scheme, a total of 50 twin-niches out of the total 1,062 twin-niches are proposed to be offered to indigenous villagers of Kam Shan Village at the average selling prices of public columbarium niches in that year. The indigenous village representatives of Kam Shan Village have indicated their full support in their supporting letter with respect to such a special offer (**Appendix VIII**). It is the intention of the Applicant to contribute a meaningful supportive and positive encouragement to local indigenous villagers to consider to adopt cremation to treat

their deceased family member(s) instead of burial on ground taking account of the easily accessible location of the proposed columbarium for all ages.

***Proposed Operation***

- 4.7 Over the past decades, there is a general trend of members of the public tending to avoid to visit the columbarium at the Special Festival Periods. The main reasons behind this include: over-congestion, too rush, very low degree of comfort and/or time-consuming travelling problems. Nowadays, many prefer to visit the columbaria on the deceased’s birthday or his/her death anniversary, or non-Special Festival Periods as to them, these days are more sensible and meaningful to commemorate the departed without clashing in such rushing, noisy, overcrowding and severe traffic jam situations.
- 4.8 Same as those proposed in the TP/27 and TP/32 Schemes, the PRS is also proposed to close the operation of the proposed columbarium development during the **Special Festival Periods**. This represents a dual-purpose proposal to tackle the local road traffic issues during such Special Festival Periods on one hand, and, on the other hand, to provide a meaningful alternative choice for the community. The Applicant also proposes to close the operation during the 1st to 14th day of the Lunar Chinese New Year.
- 4.9 In the present rezoning application, the Applicant retains the original innovative management on the operation of the facility during the Special Festival Periods. This offers a unique choice for members of the public who wish to select such type of private columbaria in the community. This special operation mode will be briefly stated in the future MP of the private columbarium licence application to be submitted to and registered in the PCLB under PCO.
- 4.10 The above-mentioned special operation modes will also be clearly stipulated in the future Agreement for Sale. In fact, the Applicant has obtained the in-principle agreements from all previous buyers about such special operation modes since the TP/27 Scheme (**Appendix X**).
- 4.11 According to the Applicant, 2 full-time staff would be employed to operate, manage and maintain the entire facility. The proposed operation hours will be

between 9am and 5pm from Monday to Sunday. Plates and vases will be provided by the management for visitors’ use upon request.

- 4.12 A number of house rules are set for all visitors to follow. They are: no burning of offerings is permitted within the Rezoning Area, (if needed, burning of a small amount of smoke-free incense inside these two columbarium halls may be permitted); and no livestock or animal offerings, liquor and cooked meat are allowed to be brought or left at the proposed columbarium. A draft MP considered by the PCLB in the application of TSOL is attached in **Appendix VI**. No environmental problems/impact due to the proposed private columbarium development on the local area is anticipated.
- 4.13 Although a public toilet is provided about 30m north-west of the Rezoning Area across Kam Shan Road (**Figure 5**), it is the intention of the Applicant to provide a special friendly arrangement to share the existing (soak-away) toilet facilities (one for uni-sex toilet and one for disabled) in about 4m east of the Rezoning Area at Nos. 71 and 72 Kam Shan Road (**Figure 14**). A consent letter issued by the owner of Nos. 71 and 72 Kam Shan Road with respect to the sharing of such toilet facilities during normal operation hours of the proposed columbarium is in **Appendix XI**.
- 4.14 In order to ensure that all visitors will enjoy a high-level of comfort and relaxed environment, the Applicant has retained in the PRS the ‘visit-by-appointment’ system proposed in the TP/27 Scheme for the future operation of the proposed (existing) private columbarium development at the Rezoning Area. In doing so, no overflow of visitors due to the proposed (existing) columbarium development in the area is anticipated.
- 4.15 Same as the TP/32 Scheme, the number of visitors per each booking session will be limited to a maximum of 24 persons for the proposed (existing) columbarium hall. Compared to that in the TP/27 Scheme, the holding capacity in the present proposed private columbarium development will be decreased by 1 person at each booking session (i.e. from original 25 persons per session in the TP/27 Scheme to 24 persons per session in the the TP/32 Scheme and that in the present rezoning application). It represents a negligible change to the TP/27 Scheme.

- 4.16 With the implementation of the proposed ‘visit-by-appointment’ system, all visitors are required to make their booking at least 2 days in advance before any visit. Each confirmed booking will be given a confirmation number and each assigned visit will be limited to 30 minutes (i.e. 9:00-9:30, 9:30-10:00, and so on). The future management of the operation will be on a “no confirmed booking, no visit” basis.
- 4.17 In view of the above, it is logical to deduce that the results of the Traffic Impact Assessment (“TIA”) prepared for the TP/32 Scheme (**Appendix XII**) remains applicable to and valid for the present proposed rezoning scheme as the types and the number of the proposed columbarium niches, and the operation mode remain unchanged taking into account no material changes in local land-use planning circumstances and the local road system since 21.10.2020. .
- 4.18 All registered owners and potential buyers of the columbarium niches will be well informed about the above-mentioned ‘visit-by-appointment’ system beforehand via phone calls, emails, Whatsapp, or SMS. The traffic management plan under application is same as that in the TIA prepared for TP/32 Scheme in **Appendix XII**.
- 4.19 Upon receipt of the TPB’s agreement on the present rezoning application, and on the basis of the final agreed rezoning scheme, including the operational modes, special development measures and management proposals mentioned above, the Applicant will then prepare and submit to PCLB a revised MP accordingly for consideration.

## **5. Planning Justifications**

### ***Providing Minimal Nuisance to Nearby Residents***

- 5.1 In order to minimise the potential nuisance or interface issues between the proposed
- 5.2 (existing) columbarium use at the Rezoning Area, the Applicant proposes to provide a standalone pedestrian entrance point with metal rails of 1.2m tall and fence off the subject GL with 1.5m tall metal fence to confine all visitors’ activities within the Rezoning Area (**Figure 7**).
- 5.3 In addition, a no objection letter issued by the existing resident of No. 78 Kam Shan Road on the PRS is provided in **Appendix II**. This clearly demonstrates that the

previous TPB’s concern on “*too close to residential dwellings*” is not truly reflected the genuine situation in reality as outlined in paragraph 1.10 above.

- 5.4 This, together with the provision of a standalone entrance point and peripheral fence supported by its nearby users/owners/residents (**Appendix II, Appendix IX and Appendix XI**), has evidently demonstrated the proposed (existing) columbarium use at the Rezoning Area is acceptable and being supported at this locality. The previous TPB’s concern on “*too close to residential dwellings*” under this rezoning scheme is being addressed and clarified by evidences in this application.
- 5.5 Indeed, the enactment of PCO and the subsequent licensing mechanism are the collective efforts of the Administration in collaboration with the columbarium industry and **represents a well-thought-out policy of the Administration “to regulate and to rectify existing and potential private columbarium facilities”**, where conditions permit, through the PCLB under PCO. Under reasonable circumstances, Government departments and Members of TPB are invited to act in unison with this well-thought-out policy of the Administration to fully reflect the cooperation spirit between the various segments of government.
- 5.6 Town planning procedure is an essential tool to facilitate the implementation of Administration’s policies, including the execution of PCO and to provide proper mitigation measures, where appropriate, to guard and minimise the consequential potential effects that may be caused to the area. The extensive time and the collective efforts that the Administration had put into the PCO and its subsequent establishment of the Licensing Scheme indicates the seriousness of the policy as it really affects so many families in the community today and in future.
- 5.7 The “Approval-in-principle for TSOL Application” granted on the proposed (existing) private columbarium at the Rezoning Area by the PCLB with validity up to 29.12.2024 (**Appendix IV**) further consolidates the positive effects to facilitate the implementation of the Administration’s policy to cater for the needs of the community.

***Retaining a Compatible Community Facility in the Area***

- 5.8 The planning intention of “V” zone is not only solely for Small House development, but also for compatible community uses. Every residential zone should have its community-based facilities to meet the needs of the local community. The “V” zone in Kam Shan was drawn up since the first Tai Po OZP was gazetted in 1980, and over the past 4 decades, this “V” zone has been inter-mixed with various of community-based facilities including an international school (about 3,500m<sup>2</sup> in land area), a school (since 1962), shop and services, recreational areas, ancestral halls, religious institutions and private columbarium uses (**Figure 5**).
- 5.9 In particular, the area within the “V” zone has been infused with multiples of temples and religious institutions such as Yan Lo, Tai Kwong Yuen and Po Yin Yuen (**Figure 5**).
- 5.10 Clearly, this reflects a healthy diversity of uses in the “V” zoning area to cope with the much-needed facilities for the community as long as no significant adverse impact is or will be resulted in.
- 5.11 **Table 2** above has demonstrated that all technical departments have indicated that the proposed (existing) columbarium use at Nos. 74 and 74 Kam Shan Road is technically acceptable at this unique locality. This, together with a no objection letter provided by the resident of No. 78 Kam Shan Road (paragraph 1.10 and **Appendix II**), makes it logical to deduce that the nature, the scale and the location of the proposed (existing) private columbarium at this peripheral part of “V” zone area are generally acceptable. This means that **the existing private columbarium development under application is not incompatible with its surrounding developments as it has caused no unacceptable interface problem from land-use planning and other technical considerations.**
- 5.12 In fact, other worthwhile uses meeting the needs of the villagers have been added spontaneously over the years such as community facilities, ancestral hall, religious institution, commercial, recreational areas, school, eating place, etc., are compatible as indicated in the Notes of the OZP for “V” zone. Indeed, columbarium is one of the much-needed community facilities in our residential neighbourhood, it should be included in the Notes of the OZP for “V” zone.
- 5.13 On 10.2.2012, TPB partially agreed to a rezoning application under Application No. Y/ST/13 for rezoning a site (known as Chi Ha Yuen) in Sha Tin from “V” to

“G/IC(1)” (**Figure 15**); on 4.12.2020, TPB agreed on the similar rezoning case (Application No. Y/ST/42) in Sha Tin with the same land-use zoning circumstances (i.e. rezoned from “V” to “G/IC” zone) (**Figure 16**); and on 10.9.2021, TPB further agreed on the likewise rezoning case (Application No. Y/ST/47) in Fo Tan with similar land-use zoning circumstances (i.e. rezoned from “V” and “Green Belt” (“GB”) to “Other Specified Uses” annotated “Columbarium (1)” (**Figure 17**).

5.14 The proposed “G/IC(3)” sub-area aims to achieve the **following merits**:

- (i) to clearly define the existing uses (i.e. columbarium) and maintain the integrity of the equity rights of the lease;
- (ii) not to confuse the issue of land supply being reduced for Small House development;
- (iii) to provide a clear set of land-use development control for the proposed (existing) columbarium use at this locality;
- (iv) to ensure that the (proposed) existing private columbarium development will be implemented, operated and controlled in accordance with the draft SoU (**Appendix V**) under “G/IC(3)” zone to meet departmental requirements; and
- (v) to allow the facility to continue to meet the user sentiment in a sustainable way so as to partially fulfil the objective of the Licensing Scheme under PCO which is a well-thought-out policy of the Administration.

***Positive and Significant Achievable Planning Merit***

5.15 In the present rezoning application, the Applicant has committed to offer to the indigenous villagers of Kam Shan Village a total of 50 twin-niches at the average selling prices of public columbarium niches in that year. This means that it will encourage 100 local indigenous villagers to adopt cremation rather than ground burial upon death.

5.16 Based on an estimated plot of hillside burial ground for indigenous villager to be about 12 feet by 8 feet<sup>3</sup>, and if TPB agrees the present rezoning application, this

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<sup>3</sup> [http://ofomb.ombudsmann.hk/abc/files/D1248\\_full\\_TC-10\\_12\\_2015.pdf](http://ofomb.ombudsmann.hk/abc/files/D1248_full_TC-10_12_2015.pdf)



new initiative will save at least 100 burial ground (about 9,600 square feet i.e.  $12 \times 8 \times 100 = 9,600$ ). Under normal circumstances, an extra 20% of land area would be required for provision of ancillary footpaths and/or separation spaces between burial grounds. The environmental gain is considerable.

5.17 Indeed, the present rezoning application represents a **meaningful and a “win-win” project to benefit the community** as a whole:

- (a) **to those indigenous villagers:** they will attain a significant financial relief, enjoy closer and easily accessible place for memorial visits to deceased family member(s) and transform the local custom in a wider extent to adopt cremation instead of burial;
- (b) **to the community:** this will contribute quite significant effort to save our natural green slopes as fewer villagers will need to apply for burial grounds, the more the slope grounds can be saved, and the higher the chance to reduce the risk of hill fire during Ching Ming and Chung Yeung Festival periods, as well as to avoid creation of undesirable and eyesore visual intrusion in the area (**Figure 18** is one of the examples showing such undesirable and eyesore problems); and
- (c) **to the Government:** more administrative human resources could be used for other community services.

#### ***Meeting Government Policy Objectives***

5.18 Under the prevailing Government policy, it actively encourages cremation over burial. According to the statistics of the FEHD, in 2020<sup>4</sup>, approximately 93.5% (46,465) of the deceased were cremated and it was forecasted that the annual number of deaths will gradually increase from 50,663 in 2020 to 75,500 in 2040, with 1,200,000 cremations projected to be taken place from 2021 to 2040<sup>5</sup>. Therefore, it is anticipated that the demand pressure on columbarium niches for interment for the coming decades will be very high.

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<sup>4</sup> Food and Environmental Hygiene Department (2020). *Annual Report 2020*. Available at: <https://www.fehd.gov.hk/english/publications/annualrpt/2020/4.html>

<sup>5</sup> Food and Health Bureau (2019). *Public Works Programme Item No. 5841CL0 Site Formation and Associated Infrastructural Works for Development of Columbarium at Siu Ho Wan, Lantau Island*. Available at: <https://www.legco.gov.hk/yr20-21/english/panels/fseh/papers/fseh20210420cb2-972-5-e.pdf>

5.19 The Government has made the “all-out efforts<sup>6</sup>” to increase the supply of columbarium facilities to meet the soaring demand. Yet under the district-based columbarium development scheme, the Government has only obtained District Council (“DC”) support for 14 out of the 24 identified sites to supply a confirmed 599,755 new columbarium niches, of which only 212,755 niches have been completed.

5.20 **Table 5** summarises the breakdown of above short-term supply of columbarium niches by district.

**Table 5 Short-term Supply of New Columbarium Niches<sup>7,8,9,10,11</sup>**

Projects obtained support from DC	District Council	(Anticipated) Completion Year	Columbarium Niches (About)
<i>Completed Projects</i>			
<b>Diamond Hill Crematorium Extension</b>	Wong Tai Sin	2012	1,540
<b>Wong Nai Chung Road Columbarium</b>	Wan Chai	2019	855
<b>Tsang Tsui Columbarium</b>	Tuen Mun	2020	163,320
<b>Wo Hop Shek Cemetery (Phase 1)</b>	North	2020	44,000
<b>Cheung Chau Columbarium Extension</b>	Islands	2020	2,250
<b>Lai Chi Yuen Cemetery Extension</b>	Islands	2020	790
<b>Subtotal</b>			<b>212,755</b>
<i>Projects to be Completed</i>			
<b>Cape Collison Road</b>	Eastern	2022	25,000
<b>On Hing Lane, Shek Mun</b>	Sha Tin	2024	40,000
<b>Sandy Ridge Cemetery</b>	North	Phase 1: 2024 Phase 2: TBC	Phase 1: 54,000 Phase 2: 146,000
<b>Tsing Tsuen Road<sup>#</sup></b>	Kwai Tsing	2025	20,000
<b>Two sites at Ex-Kwai Chung Incineration Plant</b>	Kwai Tsing	2028	68,500
<b>Two sites at Siu Ho Wan</b>	Tsuen Wan	TBC	33,500
<b>Subtotal</b>			<b>387,000</b>
<b>Grand Total</b>			<b>599,755</b>

<sup>6</sup> Food and Health Bureau (2017). *Supply of Niches and Related Issues after the Enactment of the Private Columbaria Bill*. Available at: <http://www.legco.gov.hk/yr16-17/english/panels/fseh/papers/fsehcb2-1112-1-e.pdf>

<sup>7</sup> Food and Environmental Hygiene Department (2022). *Supply of Public Niches (from 2019 to 2024)*. Available at: [https://www.fehd.gov.hk/english/cc/notice\\_supply\\_of\\_public\\_niches.html](https://www.fehd.gov.hk/english/cc/notice_supply_of_public_niches.html)

<sup>8</sup> Food and Environmental Hygiene Department (2022). *Supply of Public Niches (from 2019 to 2024)*. Available at: [https://www.fehd.gov.hk/english/cc/notice\\_supply\\_of\\_public\\_niches.html](https://www.fehd.gov.hk/english/cc/notice_supply_of_public_niches.html)

<sup>9</sup> The Government of the Hong Kong Special Administrative Region (2022). *Press Release - LCQ18: Supply of niches*. Available at: <https://www.info.gov.hk/gia/general/202205/04/P2022050400446.htm>

<sup>10</sup> Kwai Ching District Council (2022). *Construction of Columbarium Facilities at the Former Site of the Kwai Chung Incineration Plant on Kwai Yue Street, Kwai Tsing [Paper no. 4/D/2022]*. Available at: [https://www.districtcouncils.gov.hk/kwt/doc/2020\\_2023/en/dc\\_meetings\\_doc/21701/4\\_D\\_2022\\_Revised.pdf](https://www.districtcouncils.gov.hk/kwt/doc/2020_2023/en/dc_meetings_doc/21701/4_D_2022_Revised.pdf)

<sup>11</sup> Food and Health Bureau (2019). *Legislative Council Panel on Food Safety and Environmental Hygiene - Provision of Columbarium and Garden of Remembrance at Sandy Ridge Cemetery (Phase 1 development) [LC Paper No. CB(2)1182/18-19(06)]*. Available at: <https://www.legco.gov.hk/yr18-19/english/panels/fseh/papers/fseh20190416cb2-1182-6-e.pdf>

- 5.21 Assuming all of above projects will be completed as planned, there will still be a shortage of about 600,245 (i.e. 1,200,000 – 599,755) columbarium niches to meet the community needs. In the light of this, it definitely requires further efforts to increase the provision of columbarium niches by either private or public columbaria, together with other possible alternative measures to relieve the demand pressure. This is why the Administration has always stressed the fact that there is a **wider community interest, including meeting society’s needs for provision of columbaria**, needed to be considered at all times.
- 5.22 Given the long-term shortage of land supply for the development of new columbaria, private initiatives in the provision of columbarium and the regularisation of existing private columbaria (same as the one under application) would be the most possible and **practical “quick-win” measures to resolve this severe social problem**.
- 5.23 If TPB agrees to the present rezoning application, a total of 1,538 new columbarium niches [i.e. 1,700 – 162 (sold) = 1,538] in three designated types of columbarium niches will be provided to **relieve the increasing public demand pressure for such type of private columbarium facilities, and more importantly they can offer an alternative choice to the wider community**.

#### ***Setting Good Example***

- 5.24 As indicated in the proposed SoU (**Appendix V**), the proposed (existing) columbarium scheme under application has committed to adhere to all planning requirements, proposed operation modes, scale of development, number of niches by types, mitigation measures and provision of special offers to the indigenous Kam Shan Villagers as outlined in the Remarks of the Notes of the “G/IC(3)” sub-area under the proposed SoU. The Applicant in its previous TP/27 Scheme has already demonstrated that the proposed (existing) columbarium use at this unique locality is technically acceptable (**Table 2**). The proposed (existing) columbarium use at the Rezoning Area is hence a good planning case taking into account the no objection letter issued by the resident of No. 78 Kam Shan Road.
- 5.25 The TPB’s agreement on the present rezoning application will no doubt encourage more existing columbaria to consider to put more positive efforts to improve their

existing development to achieve a **fine balancing amongst following competing interests** promulgated by the FHB for the Private Columbaria Bill (2016):

- (a) **the wider community interest**, including **meeting society’s needs for provision of columbaria**;
- (b) the sentiment of the descendants, in particular their wish **not to upset the resting place of the deceased as far as possible**;
- (c) **minimising the nuisance** caused by such columbaria to their neighbourhood community; and
- (d) the **sustainable development** of the trade in the long-run.

5.26 Indeed, there is no doubt that the **enactment of PCO** in June, 2017 enables the PCLB to impose conditions to legitimize such hitherto "unauthorized" establishments and regulate new facilities to meet the acute shortage of commemorative space. The primary intention of the Administration is to regularise those existing private columbaria with a good practice and best possible efforts to minimising nuisance to it neighbourhood community under a proper operation and management manner to be monitored by PCLB, particularly the potential impacts on local traffic, noise, air-quality and water quality.

5.27 The role of town planning as a tool to execute the Administration’s policy in harmony with the approach of proper mitigation measures to minimize nuisance to the neighbourhood with a sustainable mode of operation in the long-term is proper. Again, Government departments and Members of TPB are invited to act in unison with this well-thought-out policy of the Administration to fully reflect the cooperation spirit between the various segments of government.

#### ***Alternative Sustainable Operation Mode in Industry***

5.28 It is understood that there is an increasing number of community members who intended not to visit columbarium during Ching Ming and Chung Yeung Festival Days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day , as well as the 1st to 14th day of the Lunar Chinese New Year. Truly, most of them intend to avoid the over-

crowded situation within the columbarium and the severe road and public transport congestion situations on or during such Special Festival Periods.

- 5.29 The proposed closure of the operation during the Special Festival Periods aims not only to offer to members of general public an alternative choice of this type of much-needed community facility in town, but also to avoid the creation of additional traffic pressure on the local area during such Special Festival Periods.
- 5.30 The retention of ‘visit-by-appointment’ system proposed in the PRA will contribute a significant improvement in maintaining the maximum holding capacity of visitors by 24 persons per each 30-minutes visit session for the proposed private columbarium development. Afterall, it also enables to retain the existing quiet and relaxing rural setting in the area. **All these operation modes will be stated clearly in the Agreement of Sale to protect the consumer interests (an objective of the Licensing Scheme under PCO).**
- 5.31 It is anticipated that due to its easily accessible location, the present proposed private columbarium will become one of the localised community facilities in the Tai Po District in future. **It meets the objective of long-term sustainable mode of operation in the Licensing Scheme under PCO.**

#### ***Unique Building Lot History***

- 5.32 As mentioned in paragraph 3.3 above, the Rezoning Area is an integral part of Lot 738, sold in public auction in 1909, as a building lot with no user restrictions. This has been confirmed by DLO/TP that “...*if the application is approved by the Board, the Applicant is not required to seek a lease modification from LandsD.....*” [paragraph 3.3 (g) above]. In the Site Plan of Lot No.738 registered in the Land Registry under Memorial No. TP126990, the Rezoning Area was indicated as ‘Temple’ since 1951 (Annex B in **Appendix VII**). Clearly, the **Rezoning Area Site is unique** when compared to other private columbarium sites.
- 5.33 It is a fact that not every application has the equity right to legitimize an existing facility. This historical background makes the Rezoning Area unique.
- 5.34 Therefore, in a “V” zone, apart from village house development, it should also need to look after **the equity rights of the old schedule building lots, like Lot 738, which, by law, can be used for other purposes.**

***No Impact on Supply of Land for ‘Small House’ Applications***

- 5.35 The present (existing) private columbarium has been converted from an old single-storey building structure within Kam Shan Village in Lot 738 sold in public auction in 1909 (Annex A in **Appendix VII**). The present rezoning application aims to regularise the existing private columbarium facility which was in operation since late 2007 in Lot 738 and to continue to serve the community.
- 5.36 **The approval of this rezoning application will not cause any negative impact on the supply of land for new ‘Small House’ applications in this part of Kam Shan Village.** In return, it helps to define clearly the land use control on the existing (columbarium) uses in a set of proper land-use development control parameters.
- 5.37 It is as well noted that the proposed private columbarium development is one of the community-based facility. Having considered the fact that the size, the location, the site profile and the site figuration of subject GL as well as the need to provide a reasonable building set-back away from the subject building (Nos. 74 to 75 Kam Shan Road), it is impractical to use the subject GL under the present rezoning application for a new Small House development or for any other meaningful community facilities/ sitting-out area in this part of Kam Shan Village. Indeed, there are already ample provision of properly planned open spaces/ recreational facilities nearby the southern bank of Lam Tsuen River (**Figure 5**).

***Enforceable Management Plan under PCO***

- 5.38 The promulgation of PCO in June 2017 and the subsequent establishment of PCLB have clearly outlined all the required compliance with statutory procedures, including TPB’s planning permission, and supporting materials in particular the MP which covers all details about the operations of the existing columbarium mentioned above.
- 5.39 Upon receipt of TPB’s agreement on the present rezoning application and the approval of its associated General Building Plans to regularise the present existing private columbarium, the Applicant will submit to PCLB the required supporting documents for an application for the private columbarium licence.
- 5.40 Therefore, the whole approved rezoning scheme is totally enforceable and will also be monitored closely by PCLB under PCO in future.

***Nil Adverse Traffic Impact***

- 5.41 The proposed private columbarium facility only covers an area of about 87m<sup>2</sup> and will only provide a total of 1,700 niches. Its scale of development is minor in nature. These columbarium niches are primarily intended to serve the local community of the Tai Po District and those families with up to 4 deceased members. In addition, the proposed closure during the Special Festival Periods will totally avoid clashing with the severe traffic problems in that periods.
- 5.42 Although the TIA was conducted based on the TP/32 Scheme (i.e. relocating the existing niches from Nos. 74 and 75 Kam Shan Road into Nos. 72 and 74 Kam Shan Road), the number of niches compared to the current PRS remains unchanged. Under the current PRS, more stringent operation mode has been employed by further closing the existing private columbarium during 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar Chinese New Year. The results of TIA in **Appendix XII** have concluded that, as the existing columbarium is small in scale, the anticipated traffic impact on the key junctions and pedestrian links will be insignificant. The proposed close operation during the Special Festival Periods will definitely contribute positive improvement to the local traffic situation during these periods. In fact, the Rezoning Area is within 5 to 10 minutes’ walk to the MTR Tai Wo Station and ample car-parking spaces are available within 400m radius of the Rezoning Area (**Figure 6**), it is very clear that the anticipated nil traffic impact will be caused by the existing columbarium in this part of the Kam Shan area.

***No Adverse Environmental Impact***

- 5.43 As mentioned in paragraph 4.13 above, burning of offerings is prohibited the existing columbarium, (if needed, burning of a small amount of smoke-free incense inside these two columbarium halls may be permitted). It is a fact that the existing private columbarium development will not provide any burning facility within the Rezoning Area. Hence, no adverse environmental impact due to the proposed private columbarium development on the local community is anticipated.

**6. Conclusion**

- 6.1 The confirmed Minutes of the 647<sup>th</sup> Meeting of RNTPC stated that some Members considered the previous rezoning application under Application No. Y/TP/27 (“**TP/27 Scheme**”) would warrant sympathetic consideration. The Applicant of



TP/27 Scheme regret that such sentiment had not been adhere in the Q&A session, otherwise more lively and positive discussion could have ensured and the result could have been different. To further improve the physical setting in the area, it is now proposed to include a piece of vacant Government Land (“GL”) in front of the subject building to provide a standalone main entrance access point with peripheral metal fence to enclose the whole development with a view to further minimising the potential nuisance on its nearby residential dwellings.

- 6.2 This application aims to provide more detailed information and the most practical and implementable efforts to provide an independent pedestrian access point with peripheral fence to confine all on-site activities within the Rezoning Area, which covers Nos. 74 and 75 Kam Shan Road and adjoining GL, Tai Po. It is proposed to rezone the Rezoning Area from “V” zone to “G/IC(3)” sub-area.
- 6.3 This is only the proper planning process to regularise the existing private columbarium facility within the Rezoning Area, so that this much-needed community facility can continue to serve in this part of the Tai Po District. A proposed draft of Schedule of Uses specifically made for the present proposed “G/IC(3)” sub-area is attached for the consideration of TPB.
- 6.4 As the nature and scale of development, number of proposed columbarium niches, general site location and the proposed best practicable operation modes in the present proposed rezoning application are basically the same as that in the TP/27 Scheme (so as that in the TP/32 Scheme), so that positive responses from concerned Government departments from their professional and technical perspectives should remain valid for and applicable to the present proposed rezoning application.
- 6.5 The present proposed rezoning scheme (“**PRS**”) proposes to further improve and tackle the lingering concerns raised by Members of TPB on the TP/27 Scheme (i.e. *“being located too close to nearby residential dwellings”*) so as to meet the objective of the Administration to reduce potential nuisance to the neighbouring community.
- 6.6 Same as that in previous Schemes, the present proposed rezoning application comprises a total of 1,700 columbarium niches to be grouped in three types [i.e. single-, twin- and family-niches (up to 4 members)].

- 6.7 Of the total proposed columbarium niches, it contains 162 single-niches sold before the cut-off day in 2014 and 50 out of the total 1,062 twin-niches will be offered to indigenous villagers of Kam Shan Village at the average selling prices of the public columbarium niches in that year.
- 6.8 To further minimize the potential nuisances caused by the shared use of pedestrian access point with surrounding village houses, an independent pedestrian entrance access point is proposed for visitors in order to separate the pedestrian access to the proposed development and the village houses in Nos. 78 and 79 Kam Shan Road. Pedestrian control measures, such as metal rails, metal fence and main entrance gate, will be installed. A self-contained development setting will be provided for this new “G/IC(3)” sub-area. As such, no significant adverse interface problems between the proposed private columbarium development and its surrounding village developments is anticipated (**Figure 7**).
- 6.9 The operation of the proposed private columbarium development at the Rezoning Area will be closed during Ching Ming and Chung Yeung Festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each festival day, as well as the 1st to 14th day of the Lunar Chinese New Year; and a ‘visit-by-appointment’ system will also be retained. With all these special arrangements, the whole private columbarium will be operated in a controlled, peaceful, quiet, relaxing and conformable manner to tally with the local rural setting in the area. All these operation modes will be closely monitored and reviewed from time to time as required by the PCLB under PCO.
- 6.10 Although the whole columbarium proposal under application is considered small in development scale, it will bring a meaningful contribution to offer an alternative choice for the community.
- 6.11 **Planning Merits** include:
- (i) responding to TPB Members’ concerns on the TP/27 Scheme;
  - (ii) meeting the Licensing Scheme objectives under the PCO for existing private columbaria;
  - (iii) saving at least 100 pieces of burial grounds (about 9,600ft<sup>2</sup> of land) entitled for granting to indigenous villagers of Kam Shan Village;
  - (iv) encouraging local villagers to adopt cremation rather than ground burial;

- (v) rectifying the different uses in “V” zone with proper controls that have the supports from local villagers;
- (vi) providing a standalone entrance access point with metal fence around the site boundary to further minimise the possible annoyance to its surrounding development;
- (vii) setting a good practice and socially responsive example for the industry;
- (viii) enabling to provide a best physical distance from its nearby residential dwellings; and
- (ix) offering a meaningful alternative burial solution for the community.

6.12 It will be noticed that all the above merits will realise the spirit of cooperation with declared policy of the Administration.

6.13 We respectfully request TPB to agree to and to support the present rezoning application so as to allow the Applicant to continue to provide such a unique private columbarium facility to serve the local community.

**VISION PLANNING CONSULTANTS LTD.**

**26 October, 2022**

## **Figures**

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### Location of the Rezoning Area

(Extracted from the Approved Tai Po  
Outline Zoning Plan No. S/TP/30)

### Figure 1

1 : 5 000

**VISION**

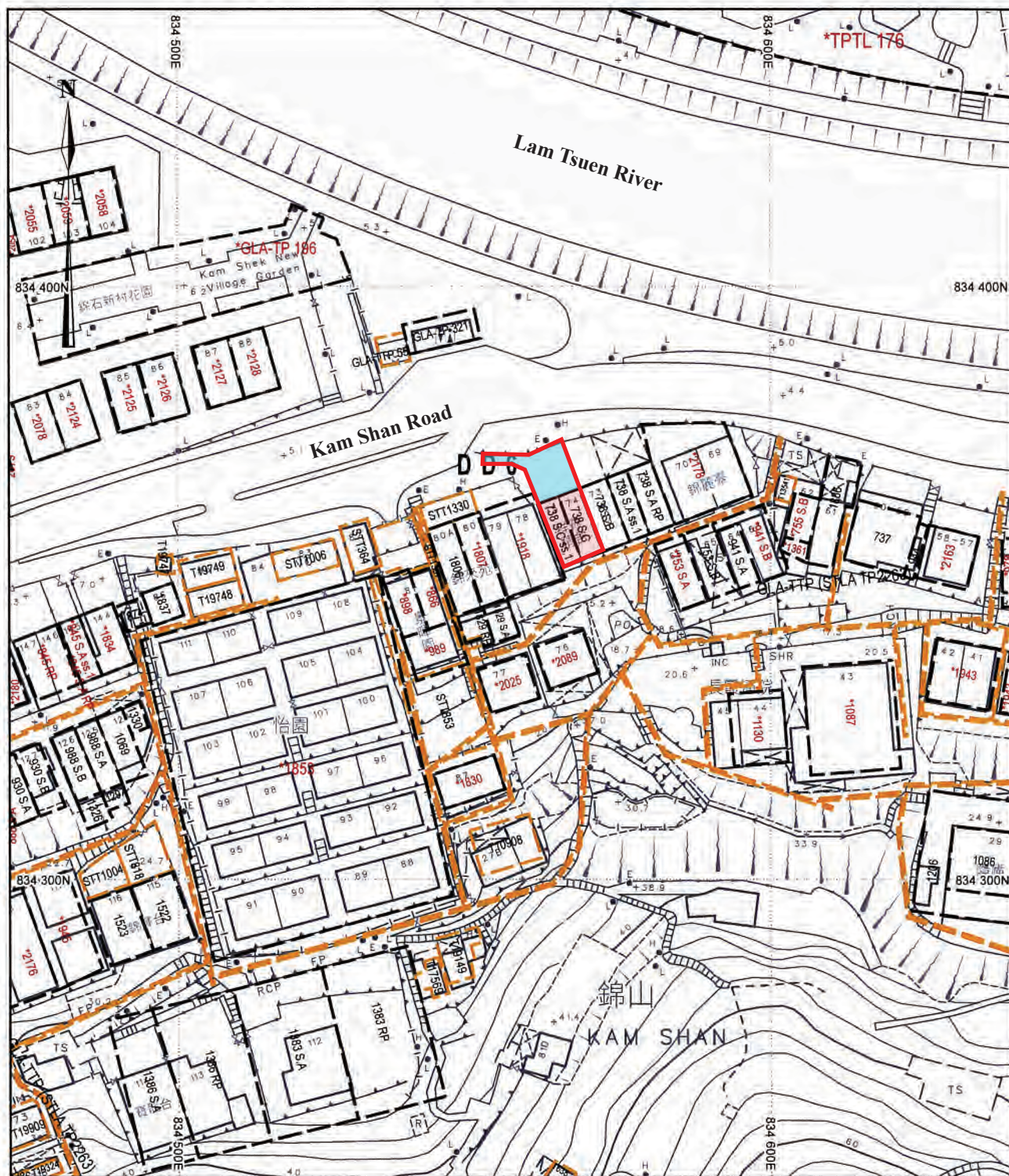
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VISION PLANNING CONSULTANTS LTD.

香港北角銀禧街 9-23 號秀明中心 20 樓 C 室  
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Tel : (852) 2566 9988 Fax: (852) 2566 9978 E-mail: vision@visionplanning.com.hk








# 地段索引圖 LOT INDEX PLAN



## Legend

-  Rezoning Area (159.99m<sup>2</sup>; 100%)
-  Private Lot (87.09m<sup>2</sup>; 54.63%)
-  Government Land (72.90m<sup>2</sup>; 45.37%)

## Lot Index Plan

(Base Plan Extracted from the Lot Index Plan  
No. ags\_S00000051543\_0002)

## Figure 2

1 : 1 000



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**Legend**

- Rezoning Area
- ▶ Viewing Angle

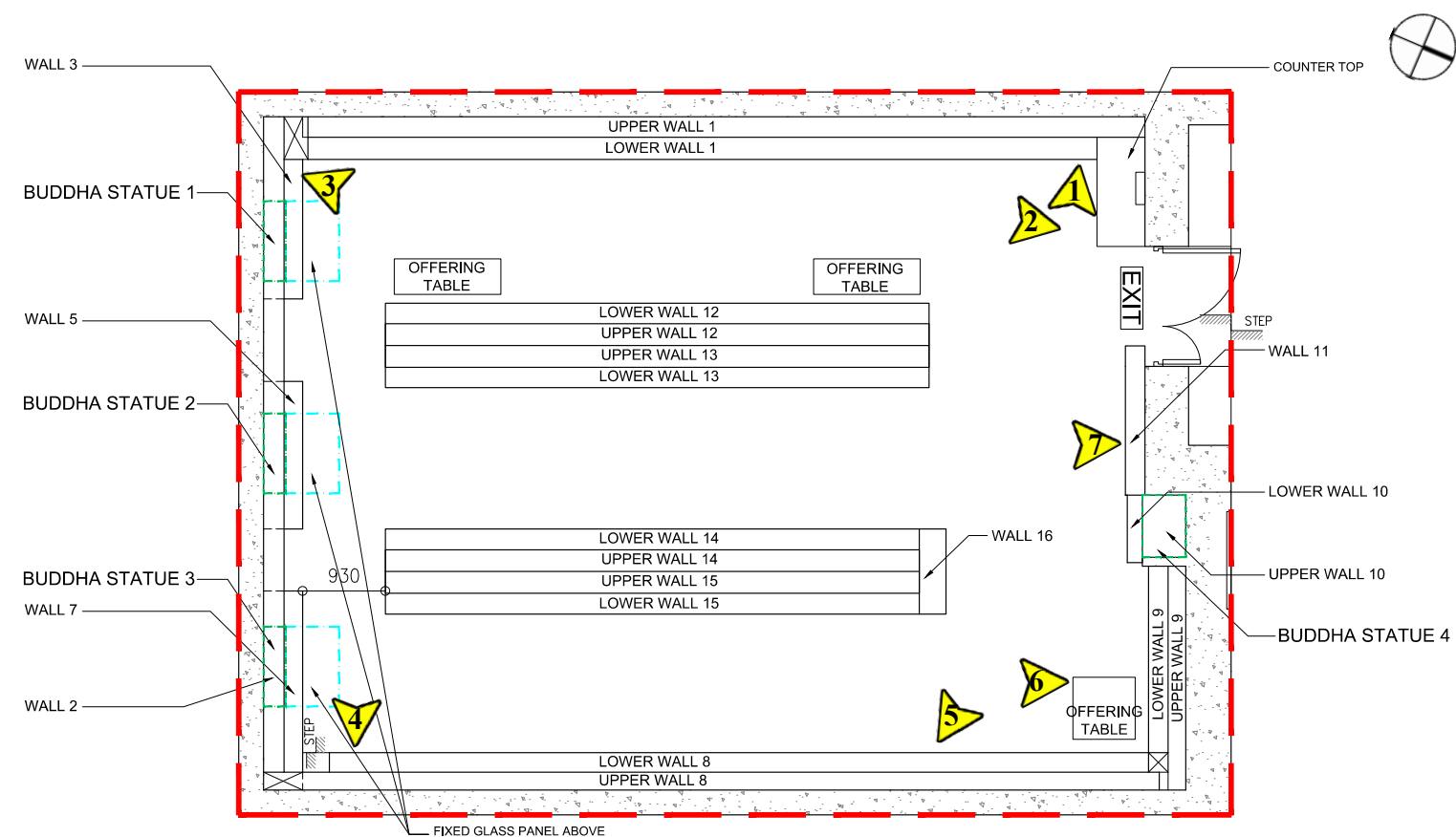
**Rezoning Area and Its Surroundings**  
 Extracted from the Government Survey Sheets No. 7-NW-8B & 7-NW-9A)

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**Figure 3**

**1: 1 500**





**Legend**

Viewing Angle

Lot Boundary of Nos. 74 & 75 Kam Shan Road

Existing Columbarium Uses at Nos. 74 & 75 Kam Shan Road

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Figure 4

N.T.S





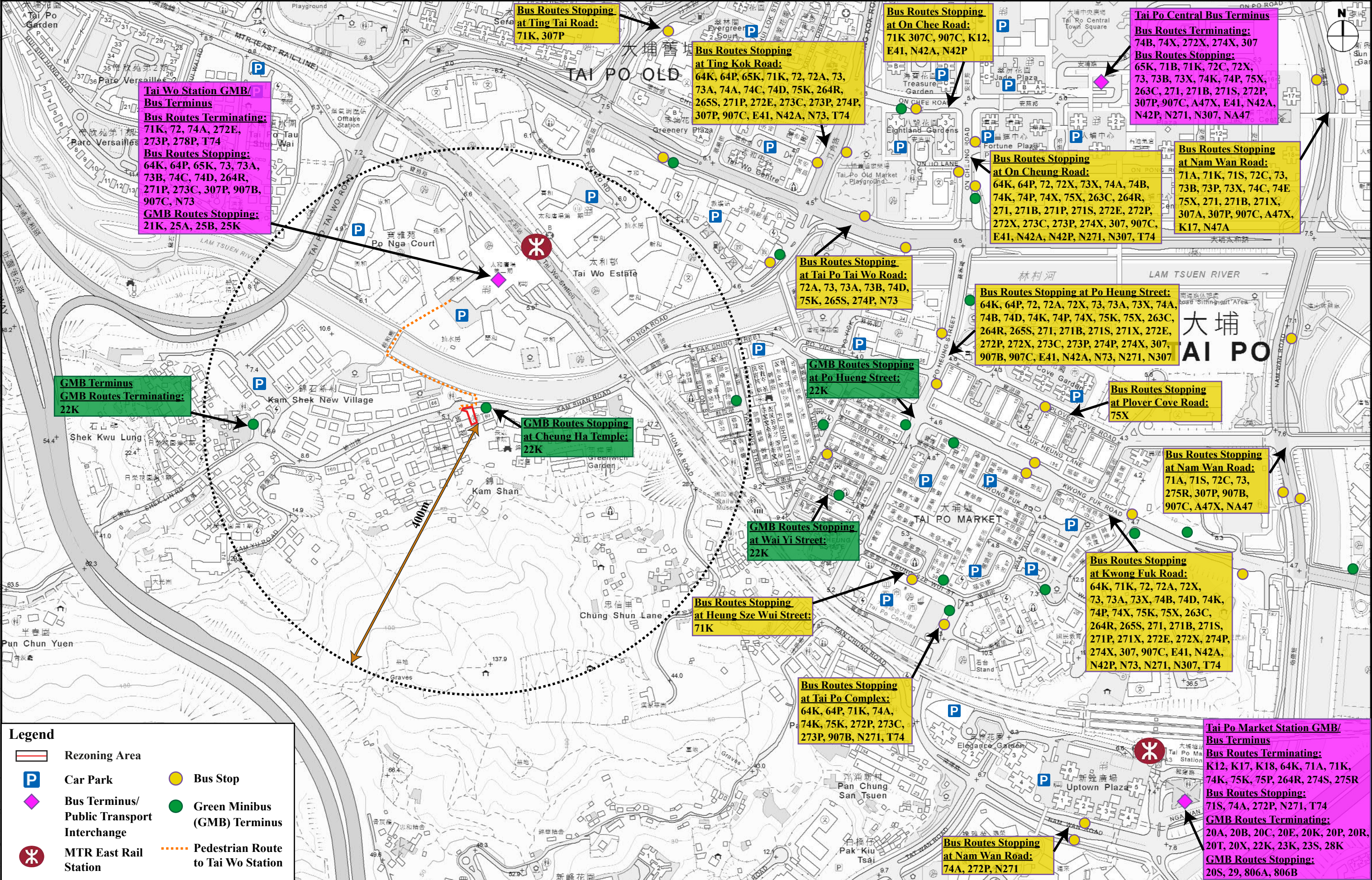
**Other Existing Uses in “V” Zone**  
 (Extracted from the Government Survey Sheets No. 7-NW-8B, 7-NW-8D, 7-NW-9A & 7-NW-9C)

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**Figure 5**

**1 : 2 500**



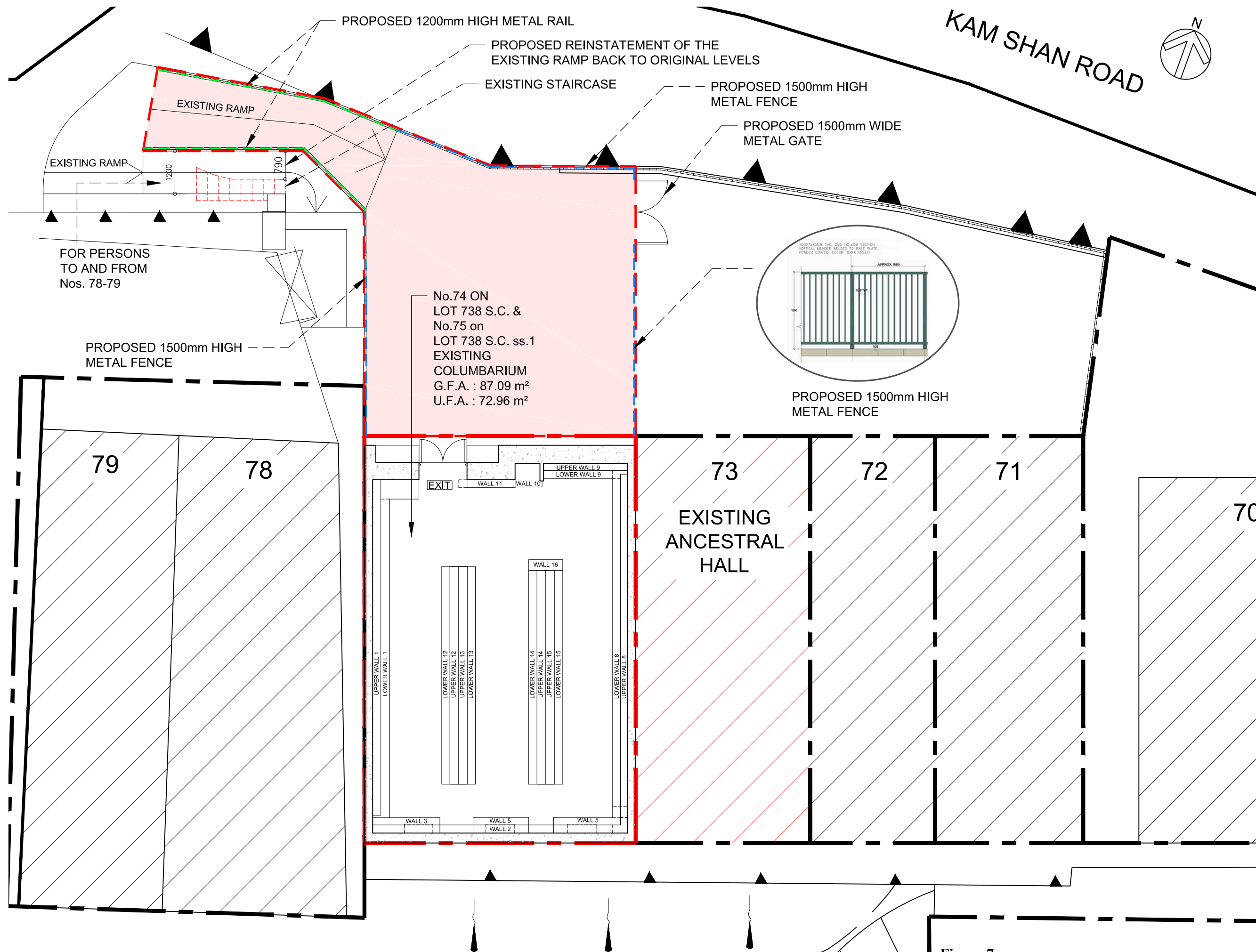


**Public Transport and Car-Parking Provisions Around Rezoning Area**  
(Extracted from the Government Survey Sheet No. 7-NW-B)

Figure 6

1 : 5 000





KAM SHAN ROAD



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- LEGEND :
- REZONING AREA (FOR "GIC(3)" ZONE)
  - RETAINING WALL
  - Area to be submitted to LandsD for STT application upon approval (72.9m²)
  - Existing ancestral hall
  - Proposed 1500mm high metal fence
  - Proposed 1200mm high metal rail

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NO.	DATE	DESCRIPTION

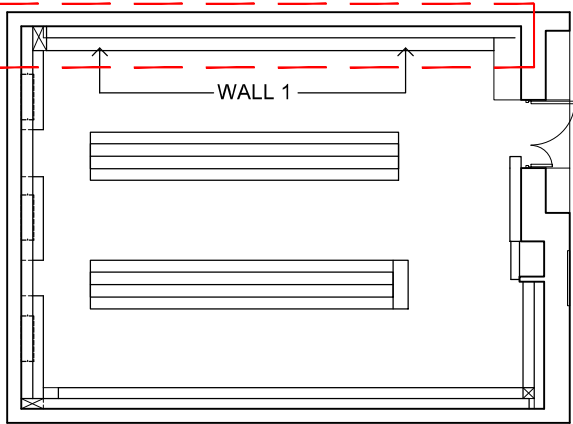
ARCHITECT  
**STUDIO | R&A**  
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Tel: 3583-2000 | Fax: 8143-1130  
Email: mail@studiorca.com  
**STUDIO | RAYMOND CHAU |**  
**ARCHITECTURE | LIMITED**

PROJECT NAME  
REZONING APPLICATION FROM  
"VILLAGE TYPE DEVELOPMENT" TO  
"GOVERNMENT, INSTITUTION OR  
COMMUNITY(3)" ZONE AT NOS. 71-75  
KAM SHAN ROAD, TAI PO,  
NEW TERRITORIES  
CHEUNG HA CHING SHEA

DRAWING TITLE  
  
GROUND FLOOR PLAN

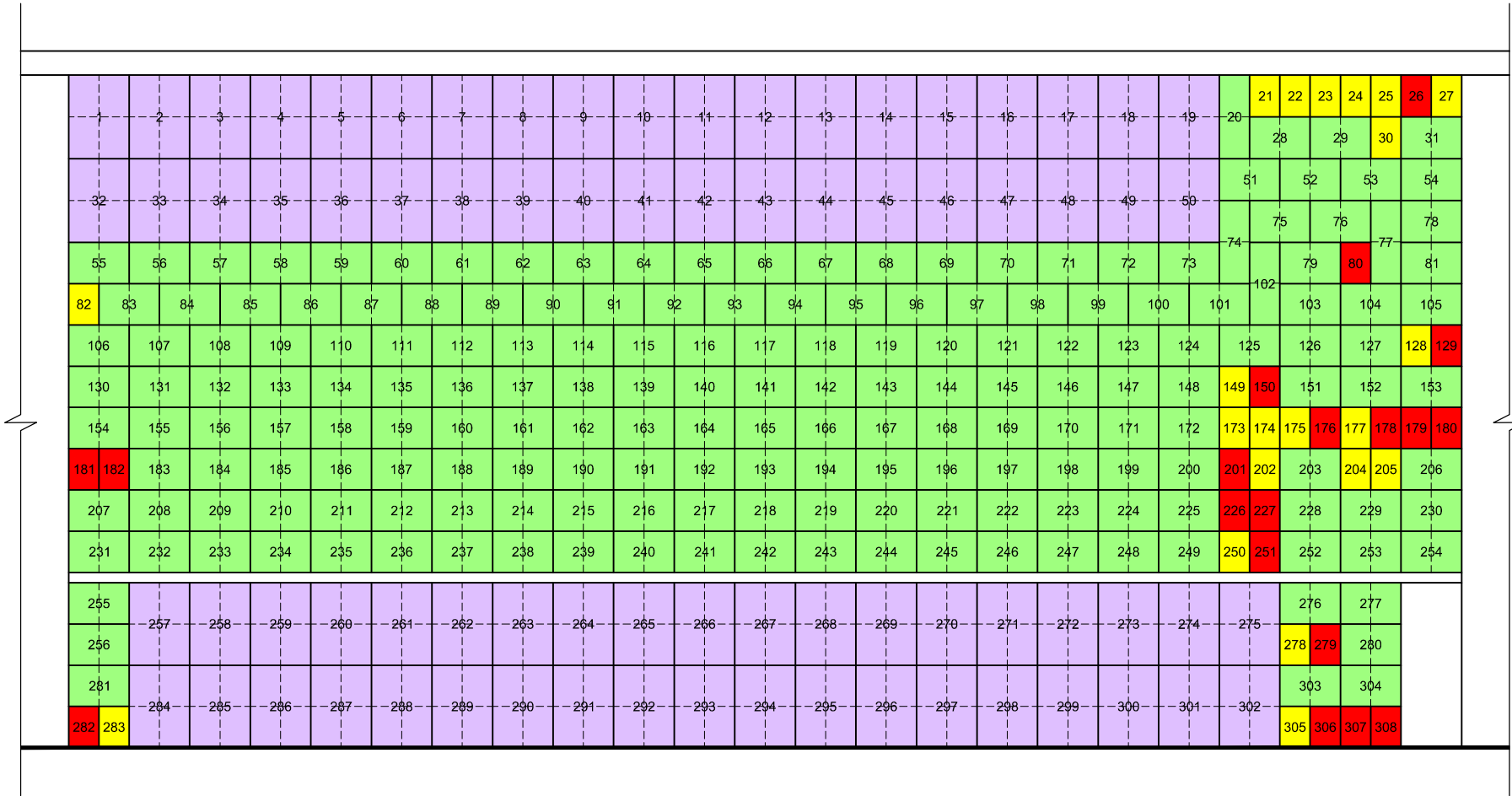
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JOB NO. 1759	DRAWING NO. A-02

Figure 7 -  
Indicative Layout of the (Proposed) Existing  
Columbarium



KEY PLAN

NUMBER OF NICHEs ON WALL 1 :	
- SINGLE NICHEs (REGULAR SIZE)	: 40
- SINGLE NICHEs (LARGE SIZE)	: 0
- TWIN NICHEs	: 192
- FAMILY NICHEs	: 76
TOTAL NUMBER OF NICHEs	
: 308	



DIAGRAMMATIC ELEVATION - WALL 1

NOTES :

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LEGEND :

Single Niches (Regular and Large Sizes) Sold and Occupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Sold and Unoccupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Large Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Double Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Family Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

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PROJECT NAME

REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" TO "GOVERNMENT, INSTITUTION OR COMMUNITY(3)" ZONE AT NOS. 71-75 KAM SHAN ROAD, TAI PO, NEW TERRITORIES

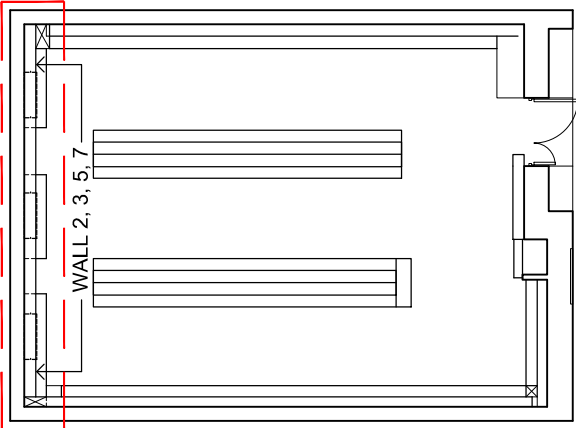
CHEUNG HA CHING SHEA

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DIAGRAMMATIC ELEVATION – WALL 1

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SCALE	DATE
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JOB NO.	DRAWING NO.
1759	A-3

Figure 8 - Indicative Columbarium Niche Wall Diagram - Wall 1

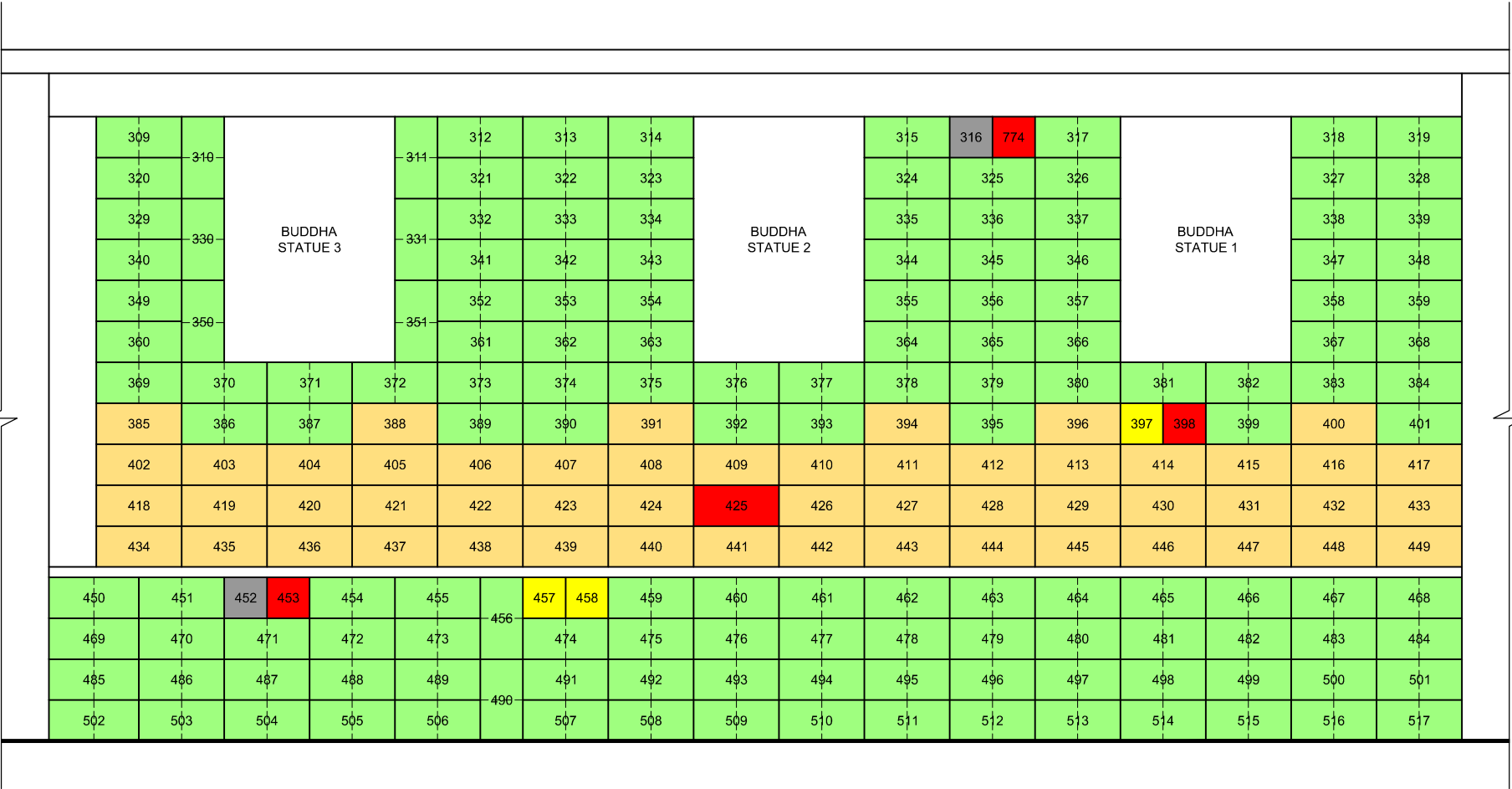


KEY PLAN

NUMBER OF NICHEs ON WALL 2, 3, 5, 7 :

- SINGLE NICHEs (REGULAR SIZE) : 8
- SINGLE NICHEs (LARGE SIZE) : 54
- TWIN NICHEs : 148
- FAMILY NICHEs : 0

TOTAL NUMBER OF NICHEs : 210



DIAGRAMMATIC ELEVATION - WALL 2, 3, 5, 7

Figure 9 -  
Indicative Columbarium Niche Wall Diagram - Wall 2, 3, 5, 7

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LEGEND :

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Double Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Family Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

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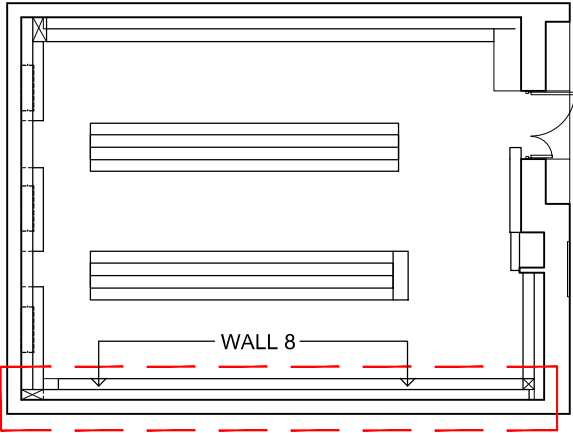
PROJECT NAME

REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" TO "GOVERNMENT, INSTITUTION OR COMMUNITY(3)" ZONE AT NOS. 71-75 KAM SHAN ROAD, TAI PO, NEW TERRITORIES  
CHEUNG HA CHING SHEA

DRAWING TITLE

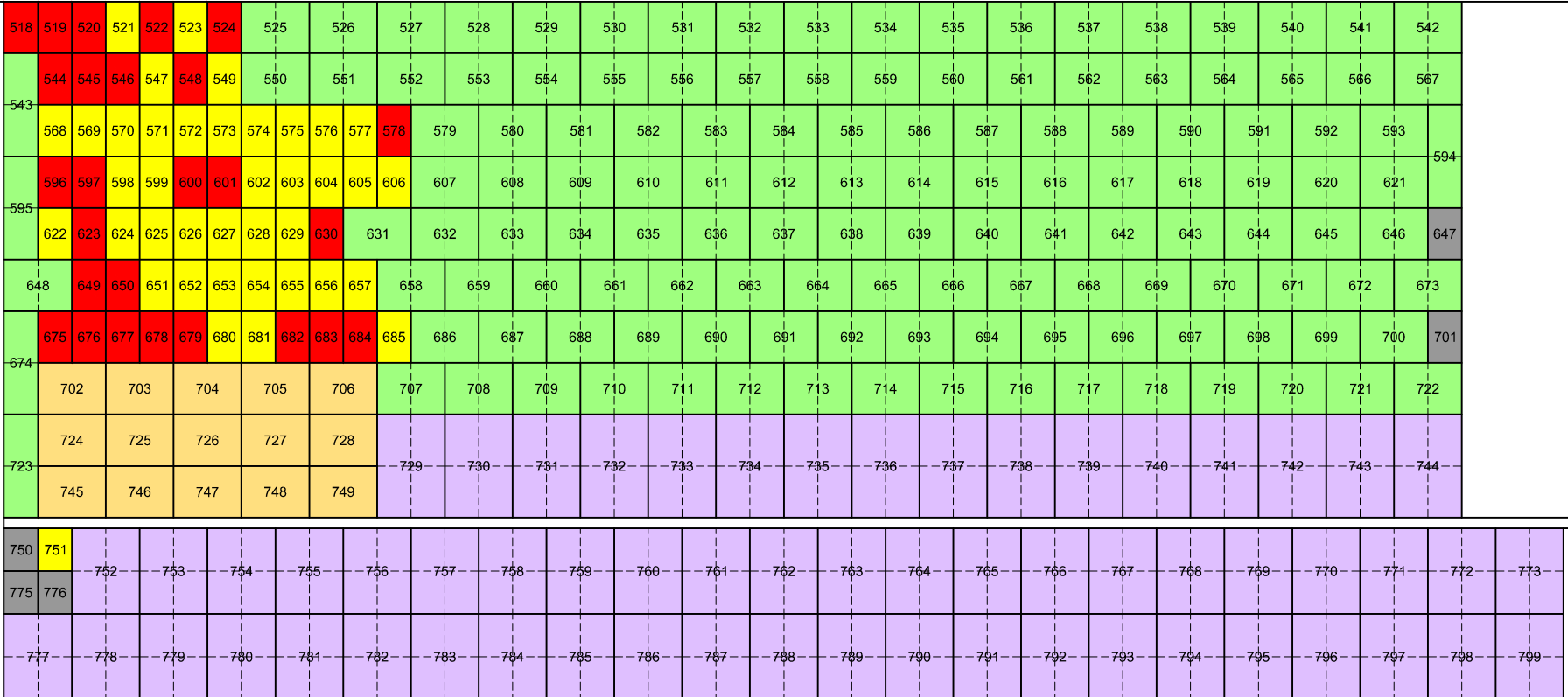
DIAGRAMMATIC ELEVATION – WALL 2, 3, 5, 7

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KEY PLAN

NUMBER OF NICHEs ON WALL 8 :	
- SINGLE NICHEs (REGULAR SIZE)	: 70
- SINGLE NICHEs (LARGE SIZE)	: 15
- TWIN NICHEs	: 135
- FAMILY NICHEs	: 61
TOTAL NUMBER OF NICHEs	
: 281	



DIAGRAMMATIC ELEVATION - WALL 8

Figure 10 -  
Indicative Columbarium Niche Wall Diagram - Wall 8

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DIAGRAMMATIC ELEVATION – WALL 8

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839	840	841	842		843		844	845	846	847	848	849
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861	862	863	864		865	866	867	868	869	870	871	872
873	874	875	876		877	878	879	880				
881	882	883	884		885	886	887	888				
889	890	891	892		893	894	895	896	897	898		
899	900	901	902	903	904	905	906	907	908	909		
910	911	912	913	914	915	916	917	918	919	920	921	

## DIAGRAMMATIC ELEVATION - WALL 9-11

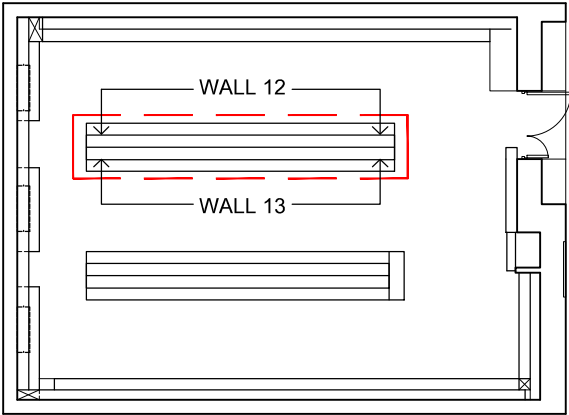
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	1698	1699	1700	

## DIAGRAMMATIC ELEVATION - WALL 16

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JOB NO. 1759	DRAWING NO. A-6

**Figure 11 -**  
**Indicative Columbarium Niche Wall Diagram - Wall 9 - 11, 16**

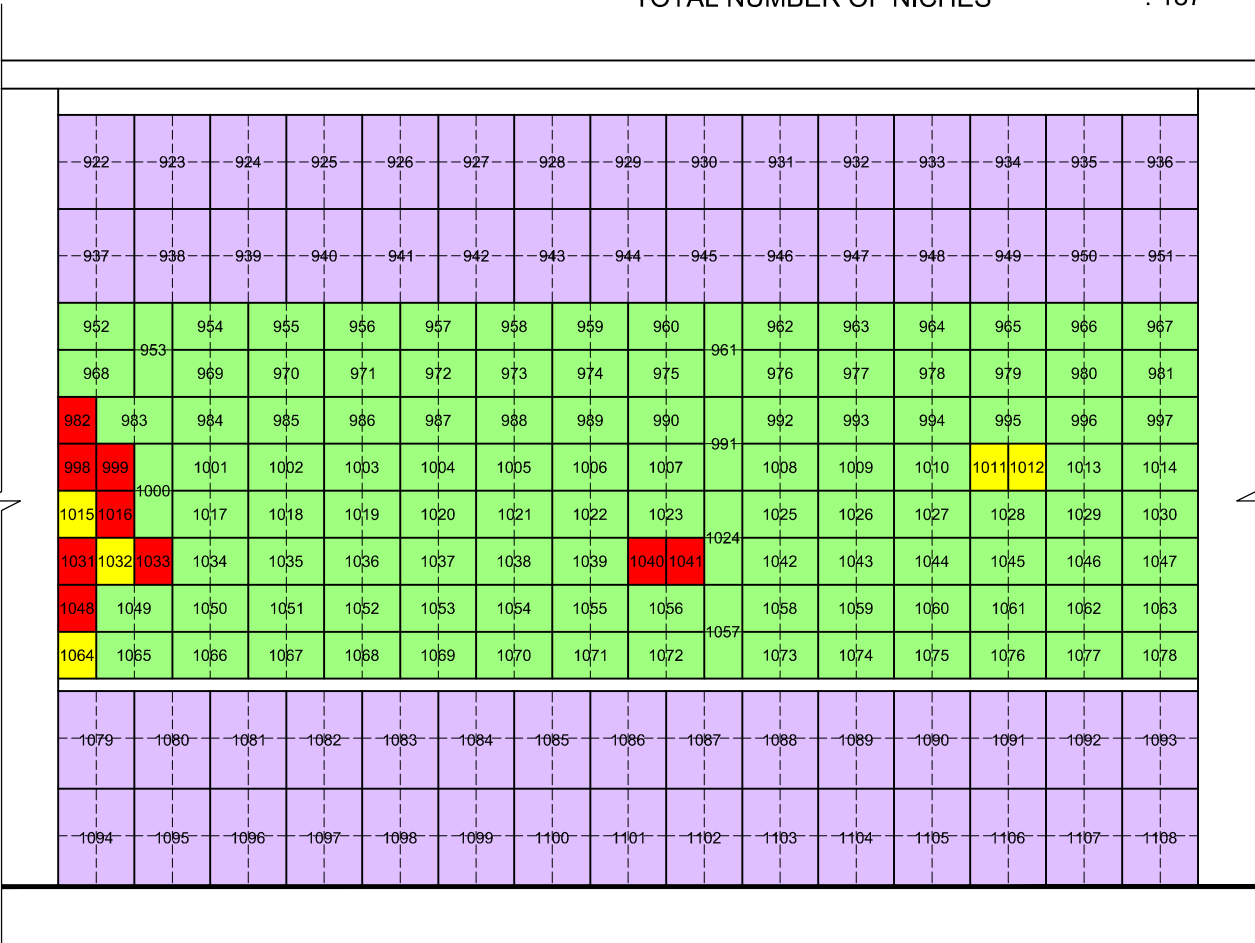




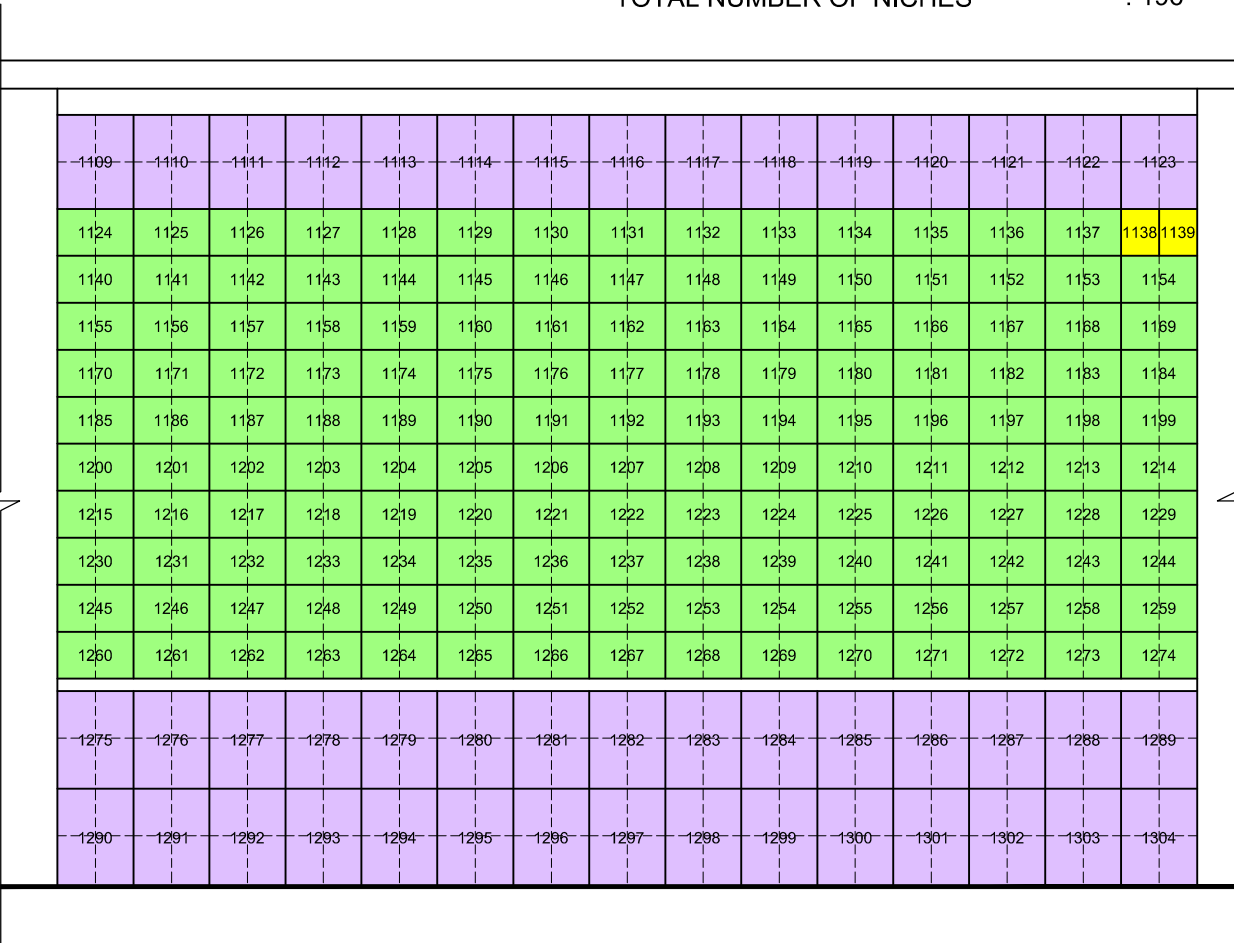
KEY PLAN

NUMBER OF NICHES ON WALL 12 :	
- SINGLE NICHES (REGULAR SIZE)	: 14
- SINGLE NICHES (LARGE SIZE)	: 0
- TWIN NICHES	: 113
- FAMILY NICHES	: 60
TOTAL NUMBER OF NICHES : 187	

NUMBER OF NICHES ON WALL 13 :	
- SINGLE NICHES (REGULAR SIZE)	: 2
- SINGLE NICHES (LARGE SIZE)	: 0
- TWIN NICHES	: 149
- FAMILY NICHES	: 45
TOTAL NUMBER OF NICHES : 196	



DIAGRAMMATIC ELEVATION - WALL 12



DIAGRAMMATIC ELEVATION - WALL 13

Figure 12 -  
Indicative Columbarium Niche Wall Diagram - Wall 12 - 13

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LEGEND :

Single Niches (Regular and Large Sizes) Sold and Occupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Sold and Unoccupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Large Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Double Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Family Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

REVISION		
NO.	DATE	DESCRIPTION

ARCHITECT

**STUDIO | R&A**

18/F | Xiu Hua Commercial Building  
211-213 Jaffe Road | Wan Chai | HK  
Tel: 3583-2000 | Fax: 8143-1130  
Email: mail@studioarca.com

STUDIO | RAYMOND CHAU |  
ARCHITECTURE | LIMITED

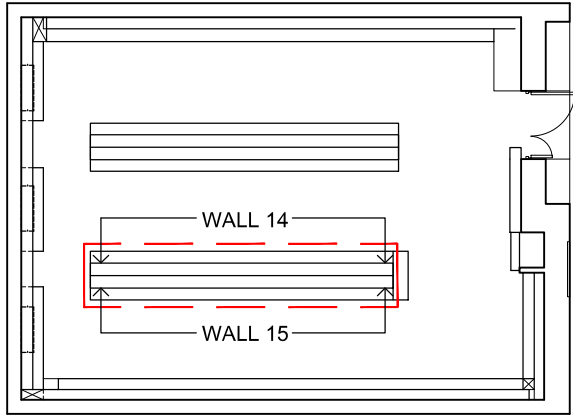
PROJECT NAME

REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" TO "GOVERNMENT, INSTITUTION OR COMMUNITY(3)" ZONE AT NOS. 71-75 KAM SHAN ROAD, TAI PO, NEW TERRITORIES  
CHEUNG HA CHING SHEA

DRAWING TITLE

DIAGRAMMATIC ELEVATION –  
WALL 12 & WALL 13

DRAWN BY	CHECKED BY
JL	RC
SCALE	DATE
N.T.S.	07.10.2022
JOB NO.	DRAWING NO.
1759	A-7



KEY PLAN

NUMBER OF NICHES ON WALL 14 :

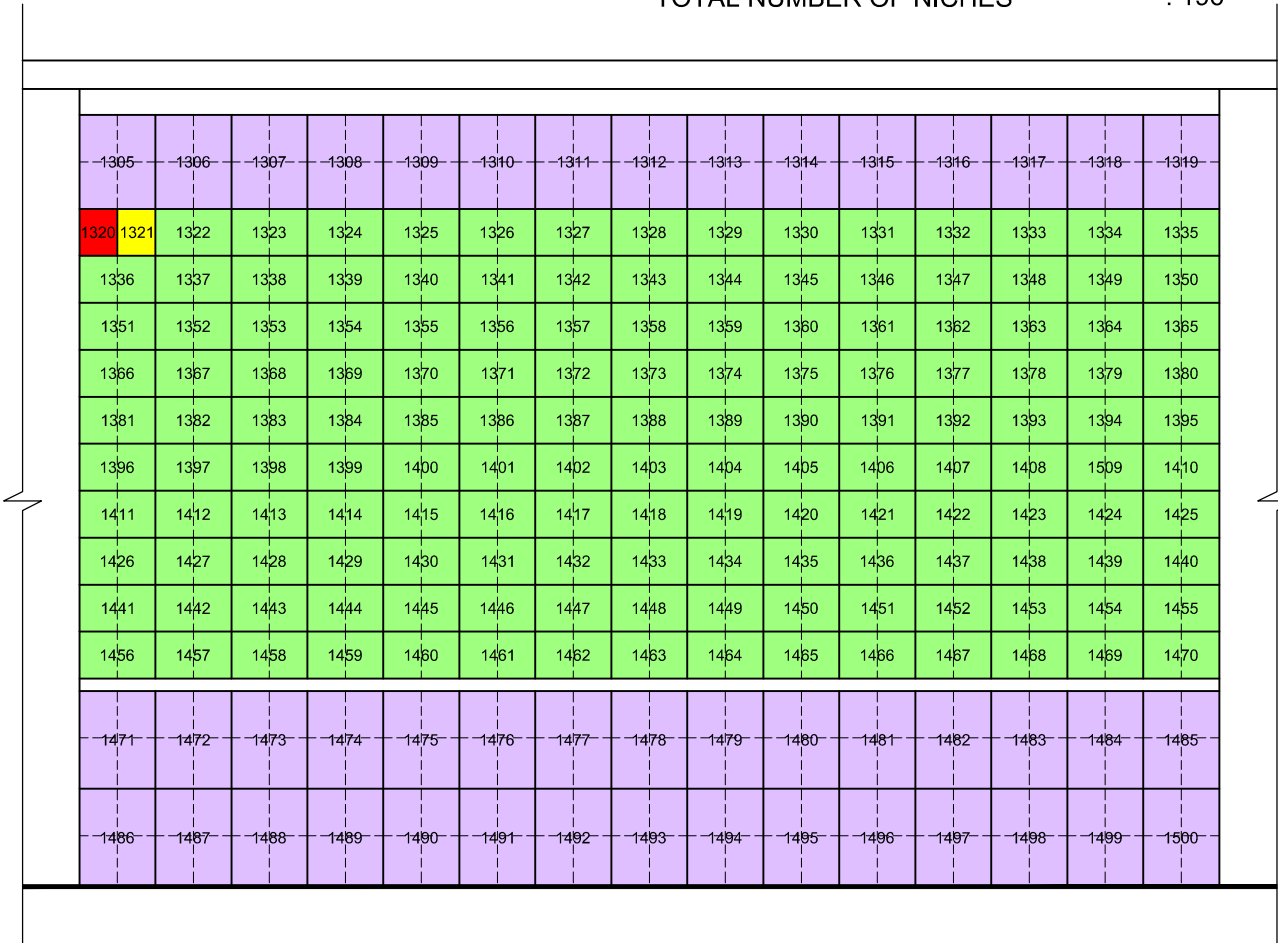
- SINGLE NICHES (REGULAR SIZE) : 2
- SINGLE NICHES (LARGE SIZE) : 0
- TWIN NICHES : 149
- FAMILY NICHES : 45

TOTAL NUMBER OF NICHES : 196

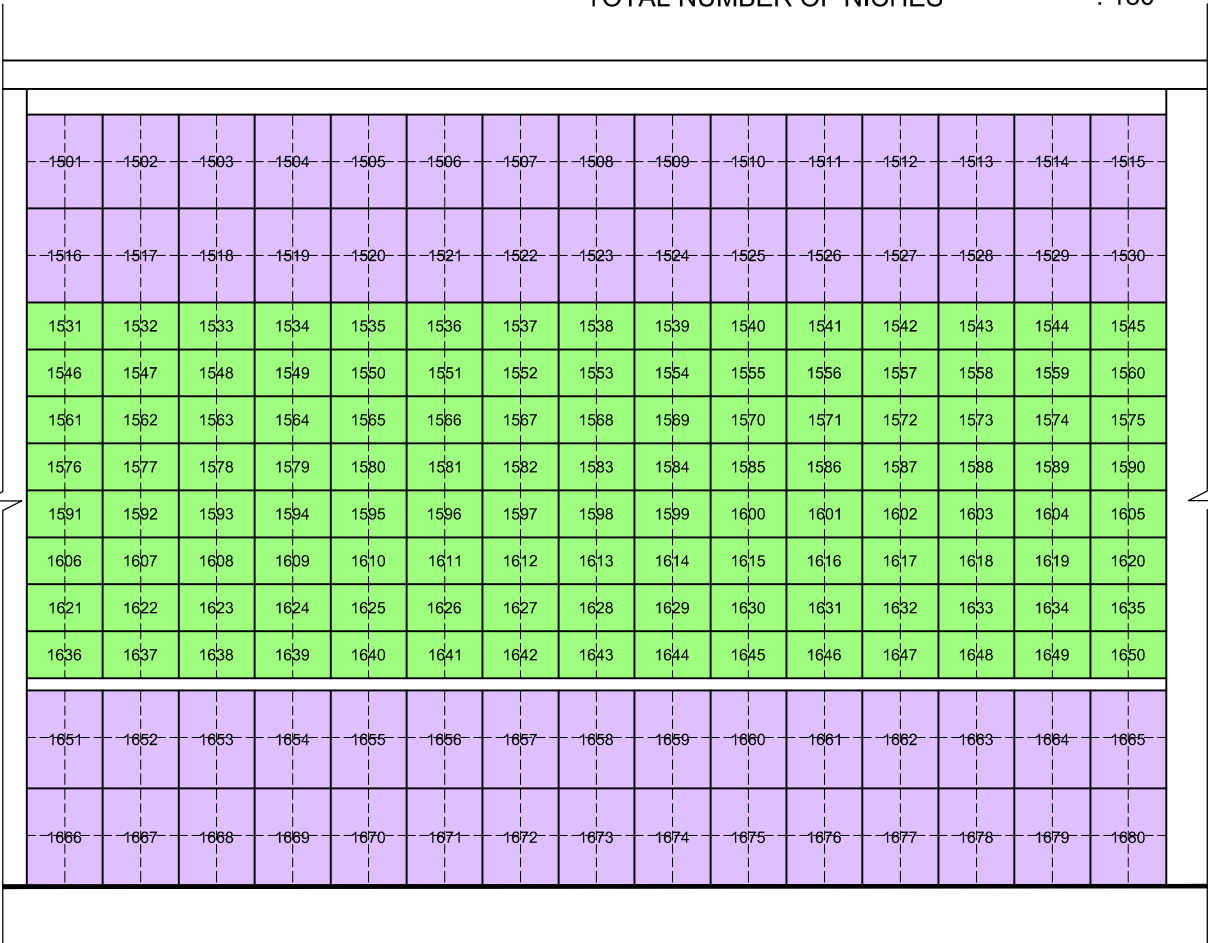
NUMBER OF NICHES ON WALL 15 :

- SINGLE NICHES (REGULAR SIZE) : 0
- SINGLE NICHES (LARGE SIZE) : 0
- TWIN NICHES : 120
- FAMILY NICHES : 60

TOTAL NUMBER OF NICHES : 180



DIAGRAMMATIC ELEVATION - WALL 14



DIAGRAMMATIC ELEVATION - WALL 15

NOTES :

DO NOT SCALE DRAWINGS.

ALL DIMENSIONS MUST BE VIEWED AT THE WORK BY THE CONTRACTOR.

ALL PRINTS, SPECIFICATIONS AND THEIR COPYRIGHT ARE THE PROPERTY OF THE ARCHITECTS AND SHALL BE RETURNED AT THE COMPLETION OF THE WORK.

LEGEND :

Single Niches (Regular and Large Sizes) Sold and Occupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Sold and Unoccupied Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Regular Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Single Niches (Large Size) Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Double Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

Family Niches Unsold Before cut-off-time (i.e 8a.m. 18 June 2014) Application for Licence and TSOL

REVISION		
NO.	DATE	DESCRIPTION

ARCHITECT

**STUDIO | R&A**

18/F | Xiu Hua Commercial Building  
211-213 Jaffe Road | Wan Chai | HK  
Tel: 3583-2000 | Fax: 8143-1130  
Email: mail@studiorca.com

STUDIO | RAYMOND CHAU |  
ARCHITECTURE | LIMITED

PROJECT NAME

REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" TO "GOVERNMENT, INSTITUTION OR COMMUNITY(3)" ZONE AT NOS. 71-75 KAM SHAN ROAD, TAI PO, NEW TERRITORIES  
CHEUNG HA CHING SHEA

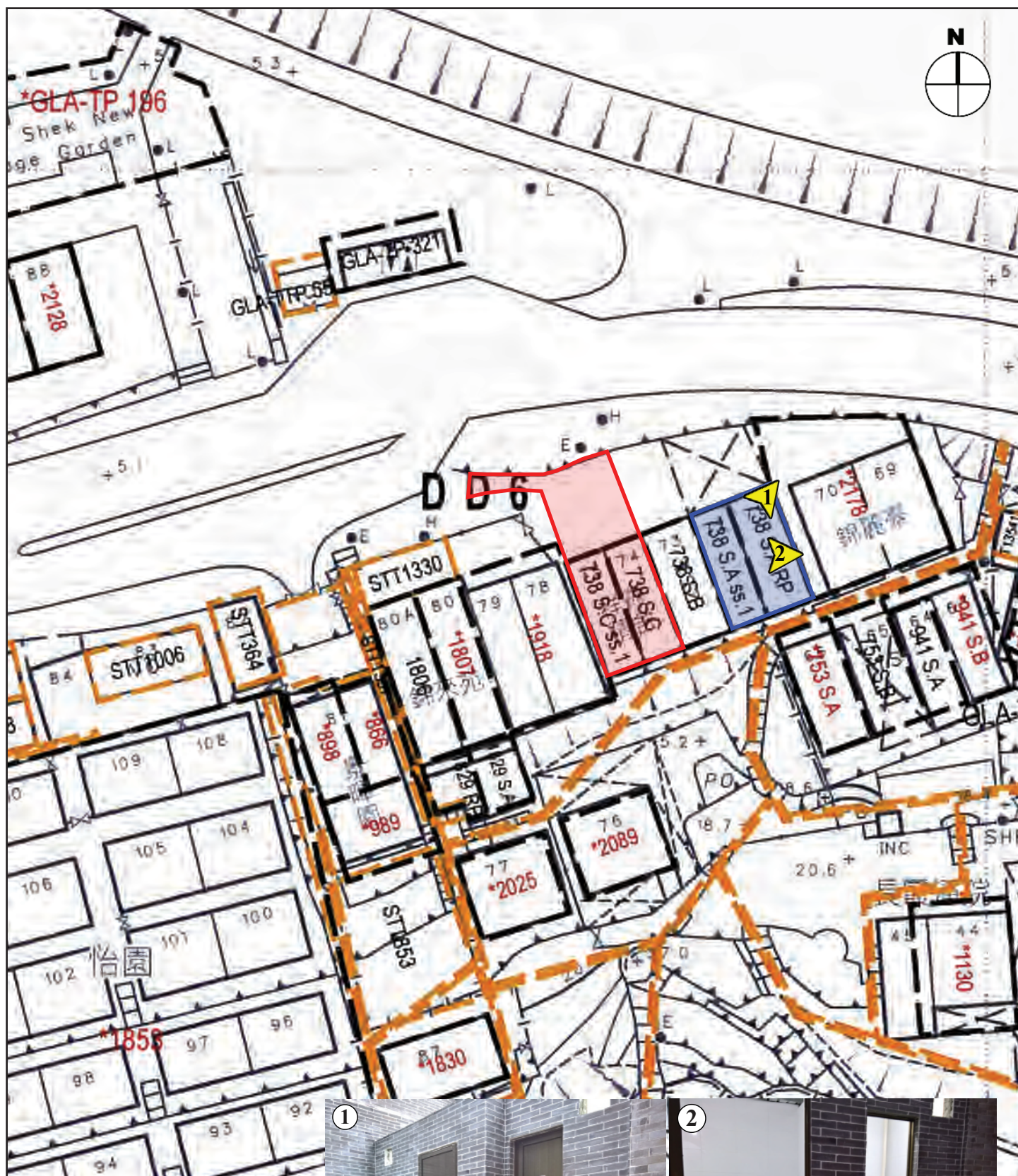
DRAWING TITLE

DIAGRAMMATIC ELEVATION –  
WALL 14 & WALL 15

DRAWN BY	CHECKED BY
JL	RC
SCALE	DATE
N.T.S.	07.10.2022
JOB NO.	DRAWING NO.
1759	A-8

Figure 13 -  
Indicative Columbarium Niche Wall Diagram - Wall 14 - 15





#### Legend

- Subject Site
- Location of Existing Toilet  
Washroom Facilities at Nos. 71 & 72  
Kam Shan Road
- View Points



#### Location of Shared Washroom Facilities

(Base Plan Extracted from the Lot Index Plan  
No. ags\_S00000051543\_0002)

Figure 14

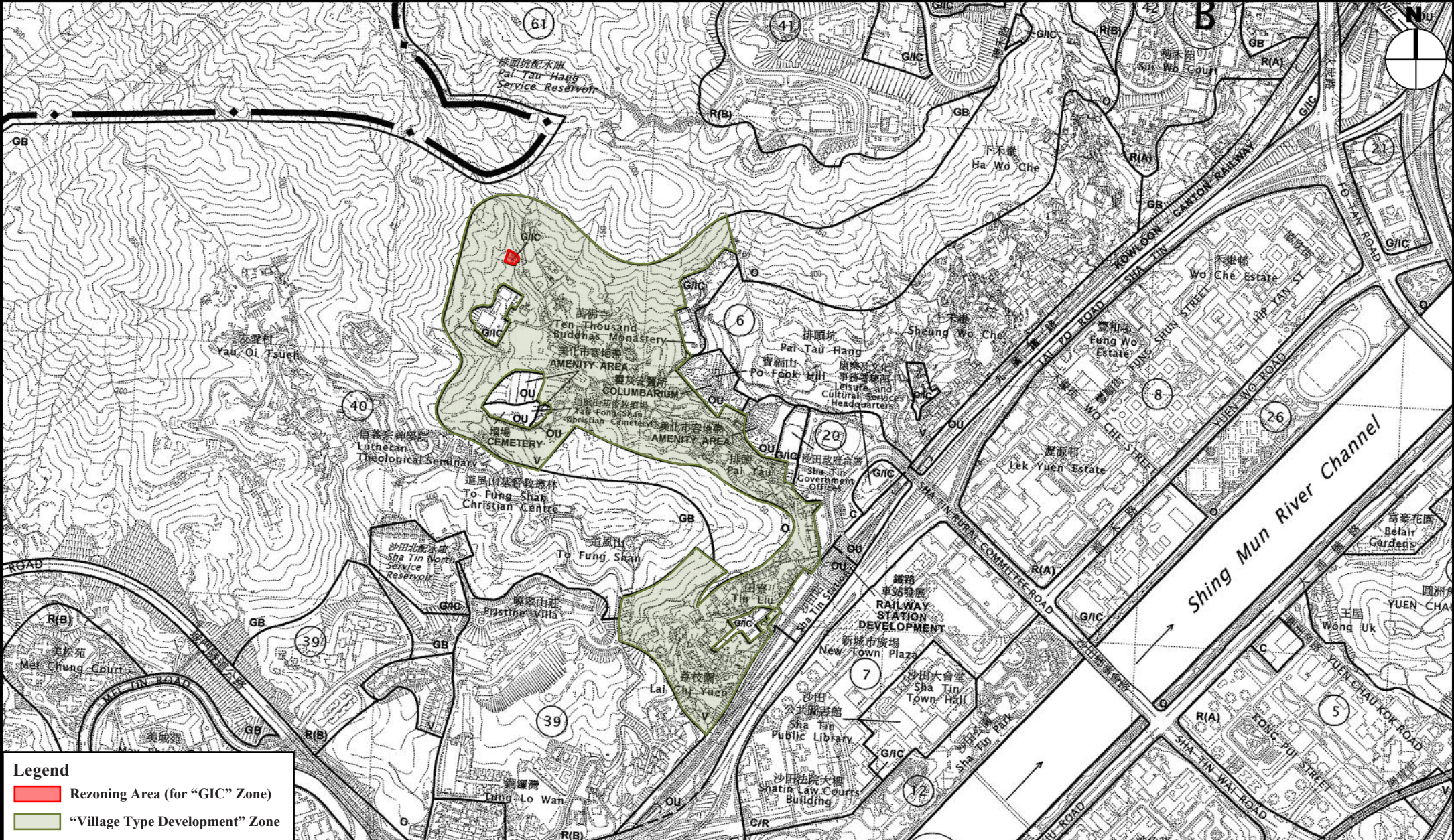
1: 500

**VISION**

弘域城市規劃顧問有限公司  
VISION PLANNING CONSULTANTS LTD.

香港北角燒灰街 9-23 號秀明中心 20 樓 C 室  
Unit C, 20/F., Seabright Plaza, 9-23 Shell Street, North Point, Hong Kong.  
Tel : (852) 2566 9988 Fax: (852) 2566 9978 E-mail: vision@visionplanning.com.hk





Similar Rezoning Application (No. Y/ST/13)  
 (Approved Sha Tin Outline Zoning Plan No. S/ST/35)

Figure 15

1 : 10 000



**VISION**

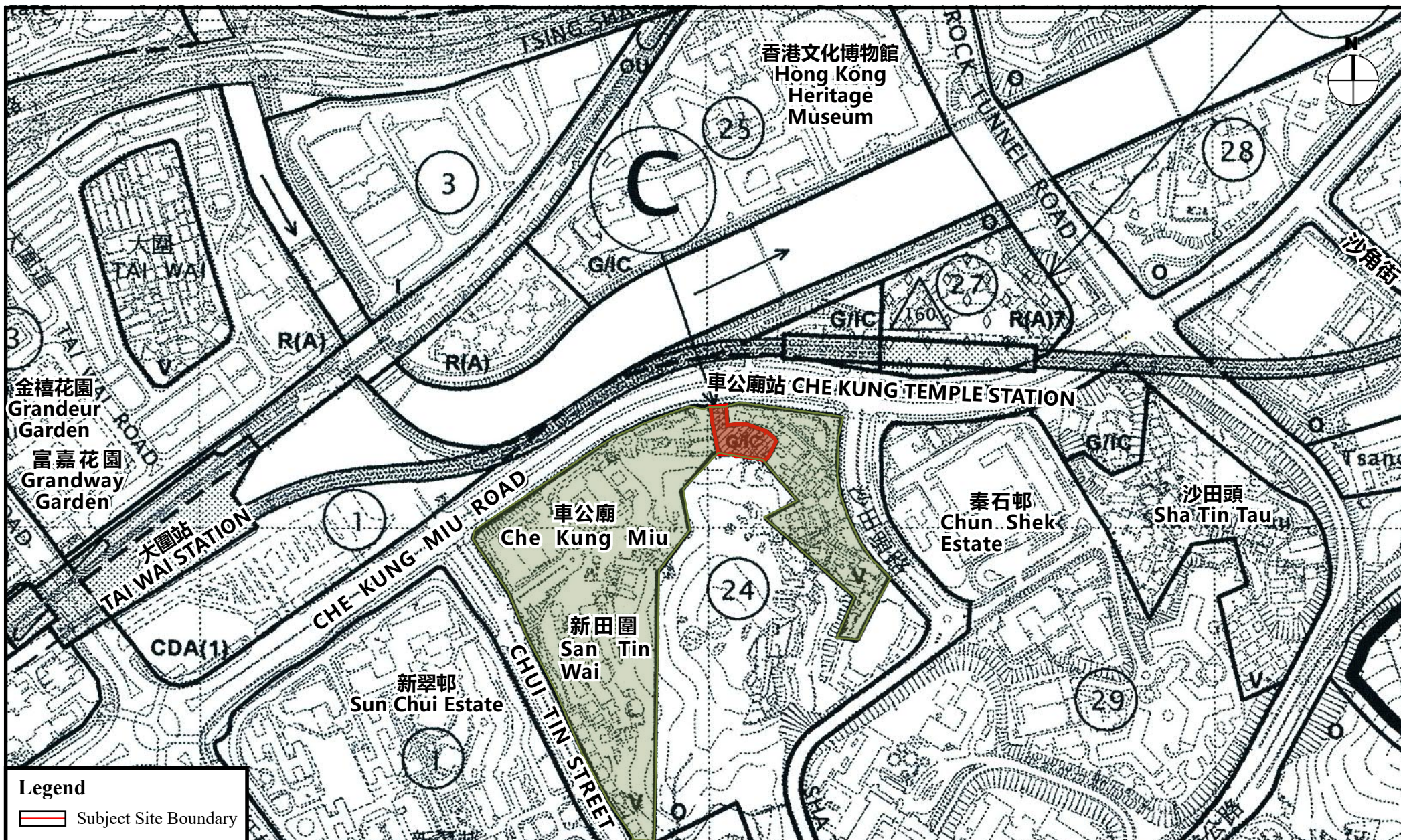
弘域城市規劃顧問有限公司  
 VISION PLANNING CONSULTANTS LTD.

香港北角銀禧街 9-23 號寶明中心 20 樓 C 室  
 Unit C, 20/F., Seabright Plaza, 9-23 Shell Street, North Point, Hong Kong.  
 Tel : (852) 2566 9988 Fax: (852) 2566 9978 E-mail: vision@visionplanning.com.hk

Figure 15

1 : 10 000









**Similar Rezoning Application (No. Y/ST/47)**  
(Base Plan extracted from the draft Sha Tin Outline Zoning Plan No. S/ST/35)

**Figure 17**

**1 : 5 000**





**Existing Conditions of the Permitted Burial Grounds in Yim Tin Tsai**  
(Extracted from Google Map Captured in August 2020)

**Figure 18**

**N.T.S.**



**弘域城市規劃顧問有限公司**  
**VISION PLANNING CONSULTANTS LTD.**

香港北角蠟殼街 9-23 號秀明中心 20 樓 C 室  
Unit C, 20/F., Seabright Plaza, 9-23 Shell Street, North Point, Hong Kong.  
Tel : (852) 2566 9988 Fax: (852) 2566 9978 E-mail: vision@visionplanning.com.hk

## **Appendix I**

---

*Extracted Minutes of 647th Meeting of RNTPC for Application No. Y/TP/27*



rejected by the Committee in 2015; and another one (No. Y/TP/29) was currently under processing;

- (b) Kam Shan village had a population of about 5,700 people; and
- (c) application No. Y/TP/29 was yet to be considered by the Committee.

25. As the applicant's representatives had no further points to raise and there were no further questions from Members, the Chairman informed the applicant's representatives that the hearing procedure for the application had been completed and the Committee would deliberate on the application in their absence and inform them of the Committee's decision in due course. The Chairman thanked the representatives of PlanD and the applicant's representatives for attending the meeting. They left the meeting at this point.

#### Deliberation Session

26. The Chairman recapitulated the major considerations of the subject application to facilitate Members' discussion. The Chairman also reminded Members that as it was a rezoning application, should the Committee decide to agree to the application, PlanD would recommend zoning amendments to the OZP for the consideration of the Committee before gazettal of the draft OZP for public inspection, and a subsequent s.16 application would still be required to regularise the columbarium use at the Site.

27. A Member did not support the subject rezoning application on the ground that the columbarium use at the Site was considered not compatible with the surroundings. The Vice-chairman echoed and said that the approval of the application would set an undesirable precedent for the application (No. Y/TP/29) to the immediate south of the Site and other similar applications. The Member who did not support the application further expressed that with the similar rezoning application (No. Y/TP/29) covering a much larger area in the vicinity, the wording of 'piecemeal' rezoning of the Site under rejection reason (a) might not be appropriate and should be suitably amended.

28. Another Member concurred that the approval of the subject rezoning application would set an undesirable precedent for other similar applications and also considered that

there was no planning gain to justify the current proposal.

29. Whilst agreeing that there was an increasing demand for columbarium use, a Member considered that the subject columbarium was located too close to residential dwellings and hence the rezoning application could not be supported.

30. A Member held a different view and said that the Site was adjacent to an ancestral hall, and thus a columbarium use was not entirely incompatible. Besides, as the subject columbarium was relatively small in scale especially when comparing with the one under application No. Y/TP/29 in the vicinity, its traffic impact should not be significant. In view that some local residents had no objection to the subject columbarium, the Member indicated that the application might warrant sympathetic consideration.

31. A Member noted that the subject application involved an existing instead of a new columbarium development, and asked whether it would still set a precedent for other cases. In response, the Chairman said that each application should be considered on its own circumstances. In any event, it might be quoted by other applicants in the future.

32. The Chairman concluded and Members agreed that a majority of the Members did not support the application. The meeting also agreed that the word “piecemeal” should be removed from rejection reason (a).

33. After further deliberation, the Committee decided not to agree to the application for the following reasons:

- “(a) the site falls within an area zoned “Village Type Development” (“V”) with the planning intention primarily for development of Small Houses by indigenous villagers. The proposed columbarium use is considered not compatible with the existing village setting of the area, particularly the residential dwellings located to its immediate west and south. There is no strong planning justification for rezoning of the site from “V” to “Government, Institution or Community (3)” zone to make provision for application for columbarium use. The current “V” zone for the site is considered appropriate; and

## **Appendix II**

---

*No Objection Letter from No. 78 Kam Shan Road Resident*

致各尊敬的

城市規劃委員會委員：

大埔錦山路 74-75 號丈量約份第 6 約地段  
第 738 號 C 分段及第 738 號 C 分段第 1 小分段  
由現有的「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶的改劃申請

不反對通知書

本人施美玉（身份證編號：[REDACTED]），是錦山村大埔錦山路78號的住戶，在8年多前入住時已知私營骨灰安置所用途。多年來的共處，從未出現任何不可接受的生活影響，也沒有任何心理壓力。本人不反對今次的規劃申請，因為這類的社區設施是必須的，而且位置也適合，有獨立出入口，加上有人流管制，是難求的人性化照顧周到設施。現今世代應該要以更正面的態度來接受這類需求壓力大的社區設施。

此上

錦山村錦山路 78 號住戶簽署：



施美玉

日期： 2022 年 10 月 10 日

## **Appendix III**

---

*Schedule of Uses for “V” Zone in the Approved Tai Po OZP Plan No. S/TP/30*

# VILLAGE TYPE DEVELOPMENT

Column 1 Uses always permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Agricultural Use Government Use (Police Reporting Centre, Post Office only) House (New Territories Exempted House only) On-Farm Domestic Structure Religious Institution (Ancestral Hall only) Rural Committee/Village Office	Eating Place Government Refuse Collection Point Government Use (not elsewhere specified) # House (not elsewhere specified) Institutional Use (not elsewhere specified) # Petrol Filling Station Place of Recreation, Sports or Culture Public Clinic Public Convenience Public Transport Terminus or Station Public Utility Installation # Public Vehicle Park (excluding container vehicle) Religious Institution (not elsewhere specified) # Residential Institution # School # Shop and Services Social Welfare Facility # Utility Installation for Private Project

In addition, the following uses are always permitted on the ground floor of a New Territories Exempted House:

Eating Place  
Library  
School  
Shop and Services

## Planning Intention

The planning intention of this zone is to reflect existing recognized and other villages, and to provide land considered suitable for village expansion and reprovisioning of village houses affected by Government projects. Land within this zone is primarily intended for development of Small Houses by indigenous villagers. It is also intended to concentrate village type development within this zone for a more orderly development pattern, efficient use of land and provision of infrastructures and services. Selected commercial and community uses serving the needs of the villagers and in support of the village development are always permitted on the ground floor of a New Territories Exempted House. Other commercial, community and recreational uses may be permitted on application to the Town Planning Board.

(Please see next page)

VILLAGE TYPE DEVELOPMENT (Cont'd)

Remarks

- (a) No new development, or addition, alteration and/or modification to or redevelopment of an existing building (except development or redevelopment to those annotated with #) shall result in a total development and/or redevelopment in excess of a maximum building height of 3 storeys (8.23m) or the height of the existing building, whichever is the greater.
- (b) Based on the individual merits of a development or redevelopment proposal, minor relaxation of the building height restriction stated in paragraph (a) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

## **Appendix IV**

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*Approval-in-principle for Temporary Suspension of Liability Application*



祥霞精舍  
暫免法律責任書申請

「原則上同意暫免法律責任書申請」

根據申請人目前遞交的文件，私營骨灰安置所發牌委員會(發牌委員會)認為除了**就龕位數目及骨灰安放容量的審核工作仍在進行中**外，上述暫免法律責任書申請基本上已符合暫免法律責任書的申請要求。發牌委員會已於 2021 年 12 月 30 日向申請人發出通知書，通知申請人發牌委員會就上述私營骨灰安置所給予「原則上同意暫免法律責任書申請」，有效期為 3 年，由 2021 年 12 月 30 日至 2024 年 12 月 29 日。在這有效期內，申請人須根據為了符合有關的暫免法律責任書申請要求而提交並獲發牌委員會接納的行動計劃連同時間表，合理地迅速採取所有所需步驟，以符合與該暫免法律責任書申請一併提交的牌照申請的要求。

請注意，給予「原則上同意暫免法律責任書申請」並不表示某所骨灰安置所最終一定可獲批牌照，如果申請人在「原則上同意暫免法律責任書申請」的有效期屆滿時仍未符合與該暫免法律責任書申請一併提交的牌照申請的所有規定和要求，亦未能證明致使發牌委員會信納申請人已合理地迅速採取所有上述所需步驟，發牌委員會將不會延續該「原則上同意暫免法律責任書申請」的期限及將可能拒絕整組申請，在這情況下，該骨灰安置所須結束營運，並按《私營骨灰安置所條例》規定處置骨灰。

若在「原則上同意暫免法律責任書申請」的有效期內，申請人已符合與該暫免法律責任書申請一併提交的牌照申請的所有規定和要求，發牌委員會會考慮舉行公開會議審議有關申請，以直接就該牌照申請作定奪。

## **Appendix V**

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*Drafted Schedule of Uses for the Proposed “G/IC(3)” Zone*

### **Government, Institution or Community (3)**

Column 1 Uses Always Permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Columbarium (as specified in Remarks a, b, c and d below only)	

#### **Planning Intention**

This zone is intended primarily for regularisation of the existing columbarium facility. Any new development or redevelopment within this zone shall be low-density and low-rise in nature and shall be compatible and blend in harmoniously with its surrounding environment.

#### **Remarks**

- (a) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of a maximum floor area of 87.09m<sup>2</sup>, a maximum site coverage of 54.43% and a maximum building height of 1 storey; and not more than a total floor area of 87.09m<sup>2</sup> for the purpose of columbarium use.
- (b) The total number of columbarium niches in this sub-area shall not exceed 1,700 (i.e. a maximum of 272 for single-niches; 1,062 for twin-niches and 366 for family-niches up to 4 members), which shall include not less than 50 twin-niches offered to indigenous villagers of Kam Shan Village at the average selling prices of public columbarium niches in that year. Columbarium development shall not be provided in areas other than Lot No. 738 S.C and Lot No. 738 S.C ss. 1 in D.D. 6 (i.e. at Nos. 74 and No. 75 Kam Shan Road respectively).
- (c) A self-contained entrance access point with boundary metal fence around the peripheral site boundary shall be provided and maintained at the expenses of the private columbarium operator at all times.
- (d) The proposed columbarium in this zone should be operated in accordance with the Management Plan, to be registered in the Private Columbarium Licensing Board (“**PCLB**”) which includes, but not limited to, the closure of the columbarium during the Special Festival Periods [i.e. Ching Ming and Chung Yeung Festival Days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day, as well as the 1st to 14th day of the Lunar Chinese New Year], the ‘visit-by-appointment’ system, operational hours, maximum number of visitors per each visit session, house rules, and other operational modes / requirements permitted / subsequently imposed by PCLB.
- (e) Based on the individual merits of a development or redevelopment proposal,

minor relaxation of the gross floor area/site coverage/building height restrictions stated in paragraph (a) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

## **Appendix VI**

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*Management Plan of the Subject Columbarium*

根據《私營骨灰安置所條例》（第 630 章）  
私營骨灰安置所牌照及暫免法律責任書申請

私營骨灰安置所位於  
新界 大埔 錦山 錦山路 74 及 75 號  
(丈量約份第 6 約地段第 738 號 C 分段及 738 號 C 分段第 1 小分段)

祥霞精舍私營骨灰安置所管理方案

申請人：喜蓮有限公司  
申請人代理：弘域城市規劃顧問有限公司

日期：2020 年 12 月 29 日

## 目錄

1. 基本資料
2. 場地資料及樓宇排列
3. 可容納的訪客量及入場管制
4. 交通及公共運輸安排
5. 人流管理
6. 保安全管理（以確保客戶及公眾安全）
7. 人手調配
8. 應對火警或其他緊急情況的應變方案
9. 確保遵從發牌委員會訂明的發牌條件或發出的指引及實務守則的措施
10. 投訴處理
11. 確保骨灰安置所持續營運的財務方案
12. 管理方案的執行人及批准人

## 附圖

- |    |                                |
|----|--------------------------------|
| 圖一 | <u>祥霞精舍</u> 的場地平面圖             |
| 圖二 | <u>祥霞精舍</u> 的建議佈局圖             |
| 圖三 | <u>祥霞精舍</u> 的建議樓面平面圖           |
| 圖四 | <u>祥霞精舍</u> 訪客等候區              |
| 圖五 | <u>祥霞精舍</u> 工作人員組織圖            |
| 圖六 | <u>祥霞精舍</u> 消防安全疏散指定集合地點圖      |
| 圖七 | <u>祥霞精舍</u> 火警／緊急事故逃生路線及滅火筒位置圖 |

## 1. 基本資料

- 1.1. 申請人喜蓮有限公司，根據《私營骨灰安置所條例》，委託弘域城市規劃顧問有限公司，就位於大埔 錦山 錦山路 74 及 75 號的私營骨灰安置所，向私營骨灰安置所發牌委員會(下稱「發牌委員會」)提交私營骨灰安置所牌照及暫免法律責任書的申請，現提供相關的管理方案及下列相關資料予發牌委員會審批：

### 私營骨灰安置所基本資料

- 1.2. 骨灰安置所名稱：祥霞精舍
- 1.3. 骨灰安置所詳細地址：新界 大埔 錦山 錦山路 74 及 75 號 (丈量約份第 6 約地段第 738 號 C 分段及 738 號 C 分段第 1 小分段) (圖一至圖二)
- 1.4. 開始營辦年份：2007 年
- 1.5. 營辦者名稱：喜蓮有限公司
- 1.6. 營辦者的身分：土地業權人（由 2007 年 10 月 12 日至 2047 年 6 月 30 日）
- 1.7. 骨灰安置所所屬宗教：無宗教
- 1.8. 骨灰安置所內的龕位放置數目：1,700 個龕位(當中包括 66 個在 2014 年 6 月 18 日上午 8 時(即「截算時間」)前已售並已上灰的單人龕位；及 96 個在「截算時間」前已售但未上灰的單人龕位; 110 未售單人龕位; 1,062 個未售雙人龕位; 及 366 個未售四灰(家族)位)

## 2. 場地資料及樓宇排列

- 2.1. 祥霞精舍私營骨灰安置所位於新界大埔 錦山 錦山路 74 及 75 號 (丈量約份第 6 約地段第 738 號 C 分段及 738 號 C 分段第 1 小分段) (圖一至圖二)，佔地面積約 87.09 平方米，私營骨灰安置所的總樓宇建築面積約 87.09 平方米；共有 1 幢 1 座單層骨灰



安置所大樓（圖三）。祥霞精舍沒有其他支援骨灰安置所運作的必要配套設施及建築物。

### 3. 可容納的訪客量及入場管制

- 3.1. 除了清明節及重陽節當天，以及該兩節日的前後兩星期內的星期六、日和公眾假期（以下統稱為「節日祭期」），每年農曆初一至初十四不開放外，餘下的開放時間為週一至週日，由上午九時至下午五時正為止。
- 3.2. 為了確保不會超出本骨灰安置所可容納訪客量的上限，以及給予一個有序和舒適的拜祭空間，祥霞精舍會採用「預約到訪」的人流管控安排機制。訪客能透過電話、書信、電話方式、Whatsapp（電話號碼：2685 1398）方式及網上途徑來進行預約，每節預約時段為三十分鐘，而每一時段的最高訪客量為 25 人。只有成功預約的訪客才可進入祥霞精舍拜祭先人。所有訪客在預約時須提供指明到訪的日期、時段及訪客人數，在本所確認後方為成功預約。
- 3.3. 本骨灰安置所會穩妥地保存所有透過電話、書信、電話方式、Whatsapp（電話號碼：2685 1398）方式及網上途徑來進行預約的記錄，以便政府部門監察其運作及有關記錄。
- 3.4. 成功預約的訪客將會收到確認訊息/信件連同確認編號，而訪客戶亦須根據確認編號在預約時段內到達祥霞精舍進行登記和拜祭先人。本骨灰安置所所有職員於大堂入口位置駐守，指示客戶前往等候區登記成功登記後，職員會按時間安排成功預約的訪客進入骨灰龕堂進行拜祭。而未有預約及逾時的訪客將不容許進入祥霞精舍。

### **入場管制安排**

- 3.5. 關於拜祭先人，祥霞精舍有以下的安排及規定，有關規定亦以指示形式張貼在祥霞精舍內當眼位置：
- a) 祥霞精舍只接待有成功預約的客戶，所有所內的設施不會對外開放；
  - b) 訪客不得作出任何騷擾行為，違者會被請離本所；
  - c) 節日祭期內不會開放，屆時大門將會用大鎖鎖上，同時掛上不開放的告示牌；
  - d) 骨灰龕堂內只提供小花瓶、供碟供拜祭先人之用。拜祭期間，必須保持環境清潔，不得亂拋或傾倒拜祭用品；
  - e) 未經許可不得進行任何形式之宗教法事；及
  - f) 骨灰龕堂內不得進行與祭祀無關的活動。

### **場內不設置寶爐**

- 3.6. 所有訪客均嚴禁在祥霞精舍內燃點香燭、大香、紙製冥鏹及祭品。
- 3.7. 祥霞精舍內外是沒有設任何化寶爐及化寶設備。

## **4. 交通及公共運輸安排**

- 4.1. 現時只有錦山路一條行車路線直達祥霞精舍。

### **場內沒有停車場**

- 4.2. 祥霞精舍不會提供任何停車位給訪客，並鼓勵所有訪客盡量乘坐公共交通來往祥霞精舍，加強友善鄰里環境和關係。

## **5. 人流管理**

- 5.1. 所有訪客必須經預約安排到訪時段。每一節時段為 30 分鐘，由上午 9:00 - 9:30 開始至下午 4:30 - 5:00 止。

5.2. 如訪客早到，將會由職員帶領到等候區排隊及登記(圖四)。訪客等候區的面積為 30 平方米，可容納約 25 人。到預約時段時，職員會請訪客到灰樓大堂。

5.3. 如沒有預約的訪客，會被拒絕進入灰樓大堂。本骨灰安置所有權追究任何因上述行為而導致的爭議，包括撤消該訪客的龕位。

5.4. 逾時的訪客，須重新預約。而當天的預約安排等同失效。

## 6. 保安全管理（以確保客戶及公眾安全）

6.1. 祥霞精舍除了在堂內周圍設置閉路電視監察情況外，平日亦不少於一位全職員工當值。

## 7. 人手調配

7.1. 祥霞精舍現有合共僱用 5 名全職員工，亦會按需要僱用兼職員工協助本骨灰安置所的運作，包括以下職位（圖五）：

- 總經理（1 人）
- 骨灰龕堂管理員（1 人）
- 副經理（1 人）
- 清潔工（1 人）
- 司機（1 人）

7.2. 當中會有四名員工以輪班制方式負責管理等候區的工作及骨灰龕堂內的人群拜祭秩序及清潔。

## 8. 應對火警或其他緊急情況的應變方案

### 應對火警的方案

8.1. 為避免火警發生危及人身安全，祥霞精舍作出以下多項措施以減少火警發生的機會；

- a) 祥霞精舍範圍內禁止燃燒蠟燭、紙製冥鏹、祭品和大香；

- b) 保持走火通道暢通；及
- c) 符合消防處要求的消防裝置及設備。

- 8.2. 祥霞精舍除了張貼消防安全指引在當眼處外，讓職員熟悉逃生路線，其中亦包括疏散和救援傷者；任何不足之處會在兩個星期內加以審查和糾正，並保存火警演習記錄。消防裝置需要放於指定位置。另外，祥霞精舍亦有定期向員工簡介消防安全指引及祥霞精舍面對火警時的應變方案（包括消防安全計劃、所有由祥霞精舍前往指定集合地點的逃生路線、消防裝置的位置及操作等）。祥霞精舍會向所有負責骨灰龕堂的員工，先行提供骨灰龕堂內應變火警練習和相關安全知識。
- 8.3. 當火警發生時，發現的職員須擊碎最近之消防警鐘玻璃並高聲呼喊「火警」以知會附近所有人。職員應立刻通報本骨灰安置所的總經理／副經理，並協助致電「999」以通知有關部門(如警方／消防局)並提供祥霞精舍的名稱及地址。在火勢不大及安全情況下，職員應使用滅火筒滅火；同時，祥霞精舍的總經理將會通知所有職員協助疏散人群，並有序地指示人群依照火警逃生路線，即時離開現場至集合地點。職員亦會向有行動困難者，例如殘障人士，老弱，小童及孕婦提供協助。當所有人員撤離後，職員會關上出現火警單位的大門及把所有客戶撤往指定集合地點。
- 8.4. **圖六**是祥霞精舍消防安全疏散指定集合地點位置圖。**圖七**是祥霞精舍火警／緊急事故逃生路線及滅火筒位置圖。

#### 其他緊急情況

- 8.5. 祥霞精舍已在辦公室當眼位置張貼緊急電話表，亦備有急救設備和急救箱，定期補充和更新急救物資。
- 8.6. 當有任何其他緊急情況發生時，職員應立即通報本骨灰安置所的總經理或當值副經理。若事件導致任何生命受威脅，例如：

有人昏迷不醒、心跳停頓或呼吸停頓、大量出血、或身處極危險境地時，本骨灰安置所的總經理／副經理會盡快致電「999」緊急熱線報警。若有人受傷而情況並未涉及即時人命安全時，有「急救證書」的職員會評估情況並進行適當的急救和護理，同時，於有需要時電召救護車送院治理。

## **9. 確保遵從發牌委員會訂明的發牌條件或發出的指引及實務守則的措施**

- 9.1. 祥霞精舍會遵從發牌委員會訂明的發牌條件或相關指引及實務守則的措施，本骨灰安置所會在獲得發牌條件時，立刻作出檢討並在限期內糾正任何未達指引要求的問題。管理層亦會定期（尤其在更新相關指引時）向各職員講解有關的發牌指引，並派發筆記，確保管理人員及前線職員熟悉該指引。祥霞精舍亦會向新入職人員在入職首天講解有關的發牌指引及發放筆記。
- 9.2. 另外，祥霞精舍亦會密切監察員工有否遵從牌照指引。若有員工被發現未符合發牌委員會訂明的發牌條件或相關的指引時，祥霞精舍會向涉事員工提供指示。管理委員會會每年檢討祥霞精舍有否確切遵從發牌條件、指引及守則，並編寫報告，提出適當可行的改善方案。

## **10. 投訴處理**

- 10.1. 若客戶對祥霞精舍有任何投訴，可以透過電郵 (fortuneglow7475@gmail.com)、書信、電話或 Whatsapp (電話號碼：2685 1398)方式，向祥霞精舍職員備案。所有查詢、意見或投訴，不論以口頭或書面提出，必須從速處理，及早回覆。



祥霞精舍私營骨灰安置所管理方案

私營骨灰安置所位於新界 大埔 錦山 錦山路 74 及 75 號

丈量約份第 6 約地段第 738 號 C 分段及 738 號 C 分段第 1 小分段 (即祥霞精舍範圍)

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**負責批准管理方案的人員的資料**

姓名：駱德祥

職位：董事

聯絡電話：

電郵地址：

**代表祥霞精舍提交本管理方案的人員資料**

姓名：黃鳳珍

職位：經理

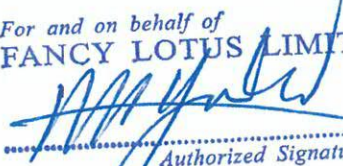
聯絡電話：

電郵地址：

獲授權人士：袁善本

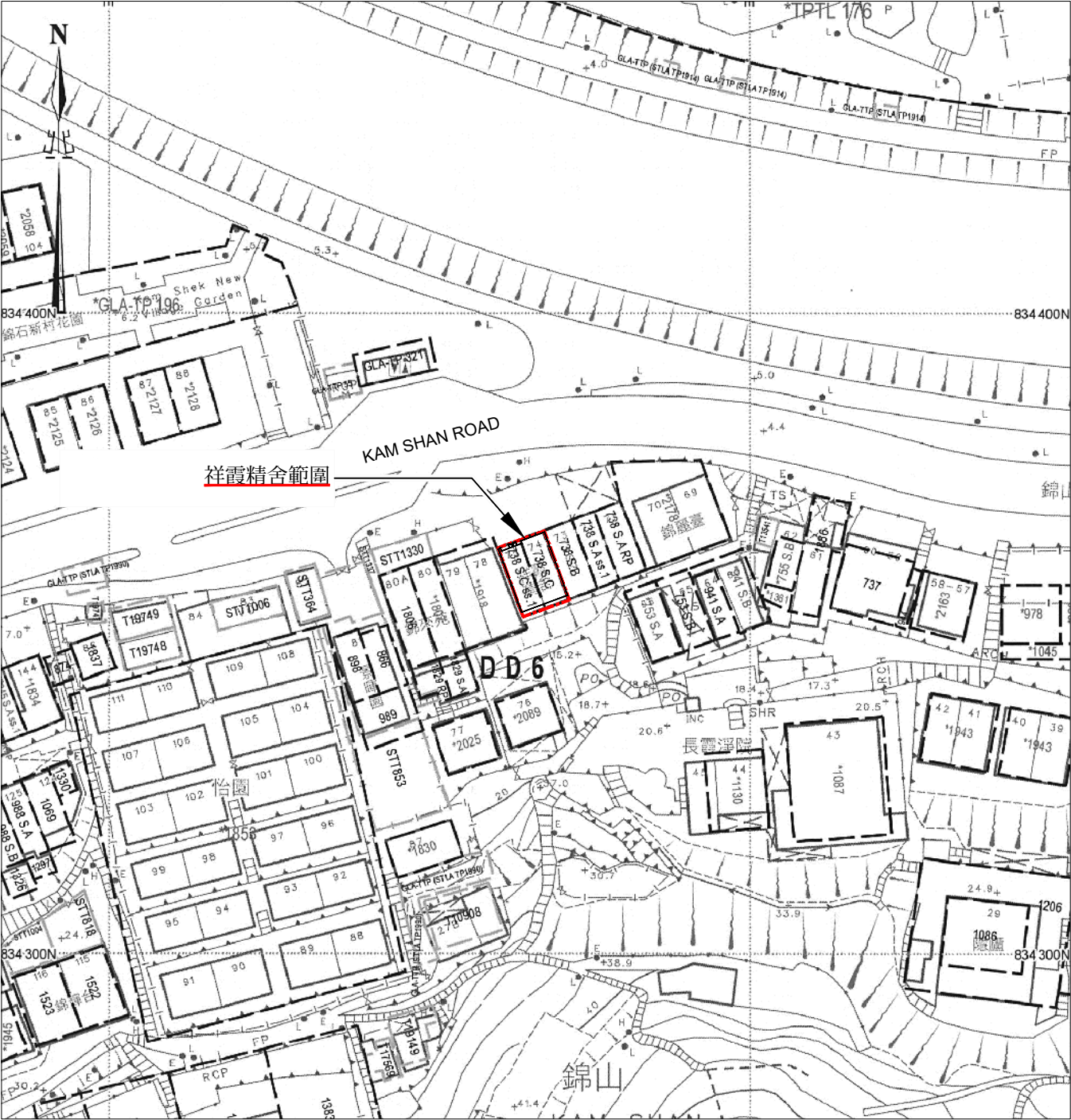
簽署：

日期：

For and on behalf of  
FANCY LOTUS LIMITED  
  
Authorized Signature(s)

29.12.2020





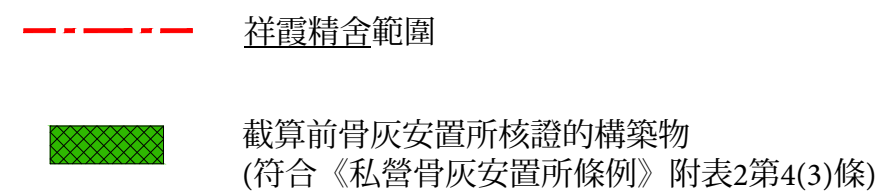
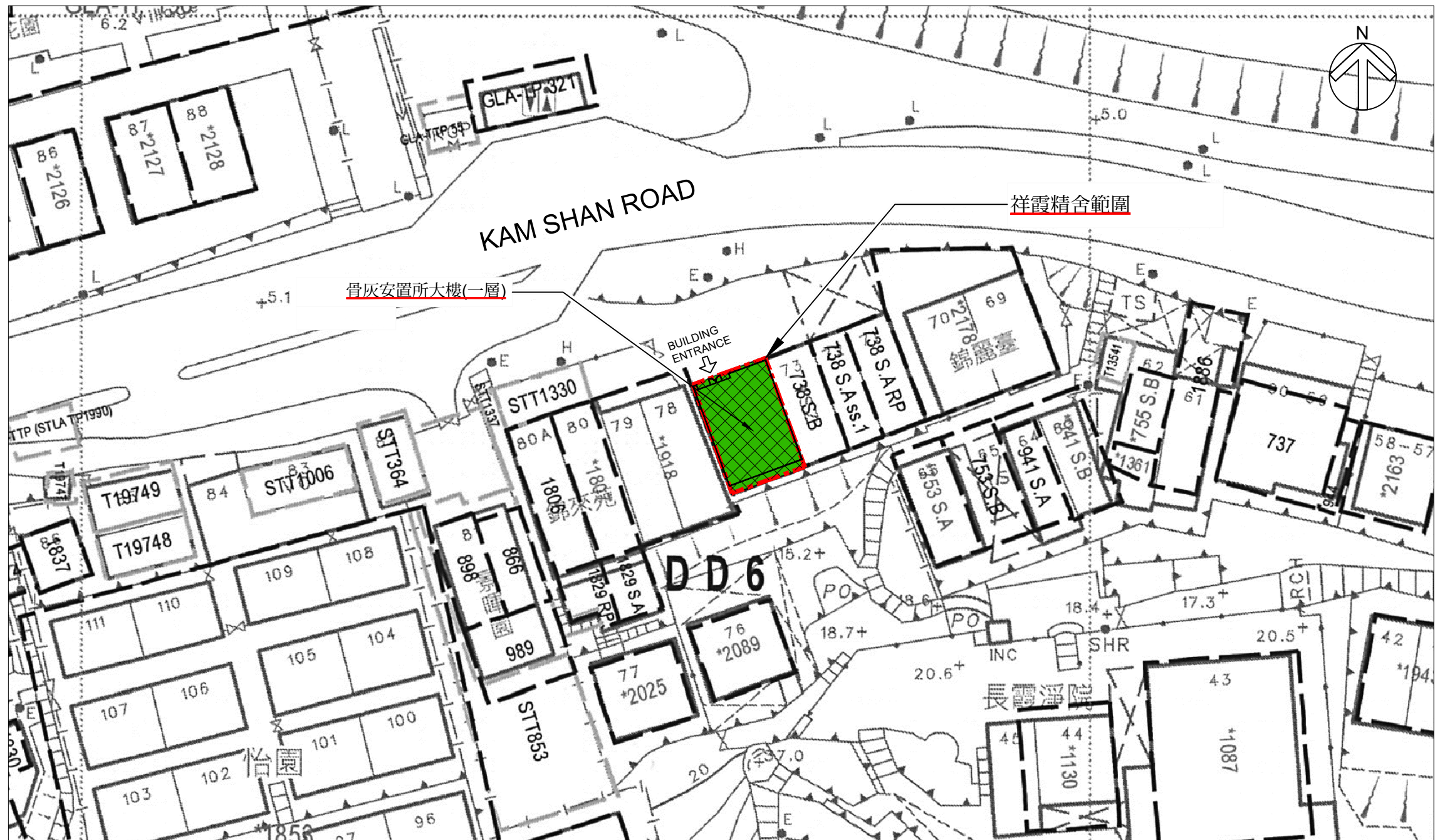
祥霞精舍範圍

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祥霞精舍範圍

圖例: 1:1 000

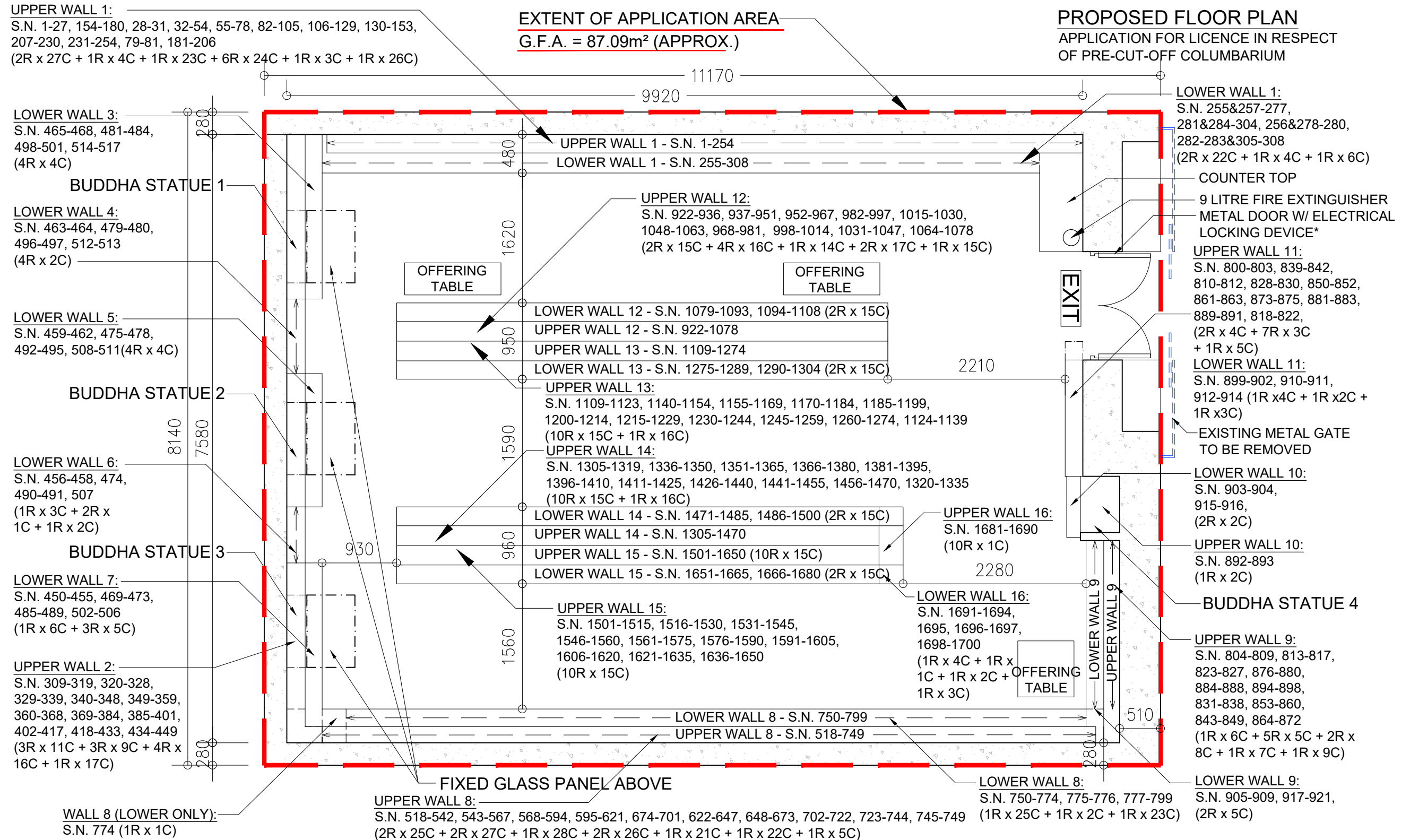
祥霞精舍的場地平面圖



圖例: 1: 500

祥霞精舍的建議佈局圖

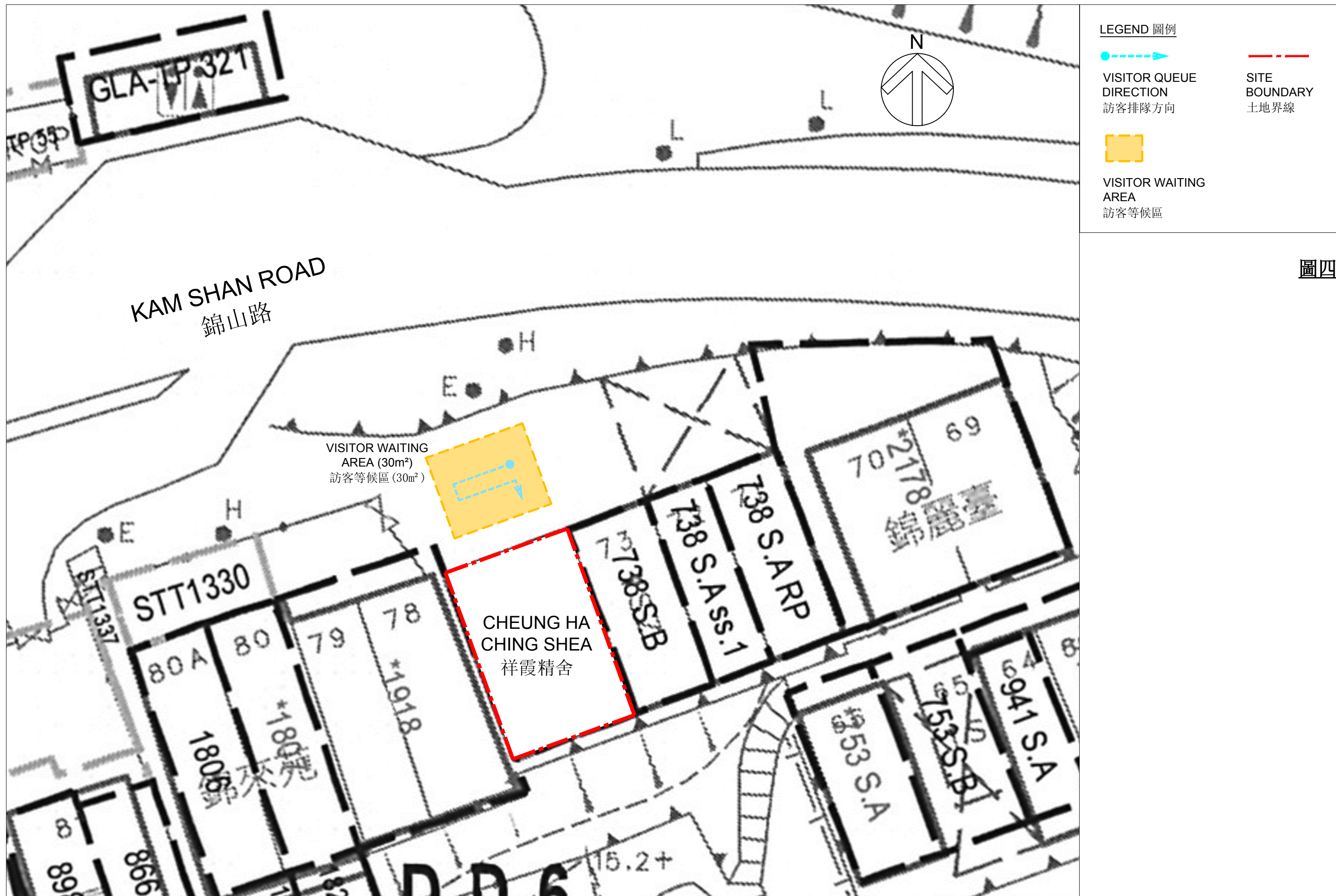




--- 祥霞精舍範圍

圖例: 1: 50

祥霞精舍的建議樓面平面圖



LEGEND 圖例



VISITOR QUEUE  
DIRECTION  
訪客排隊方向



SITE  
BOUNDARY  
土地界線

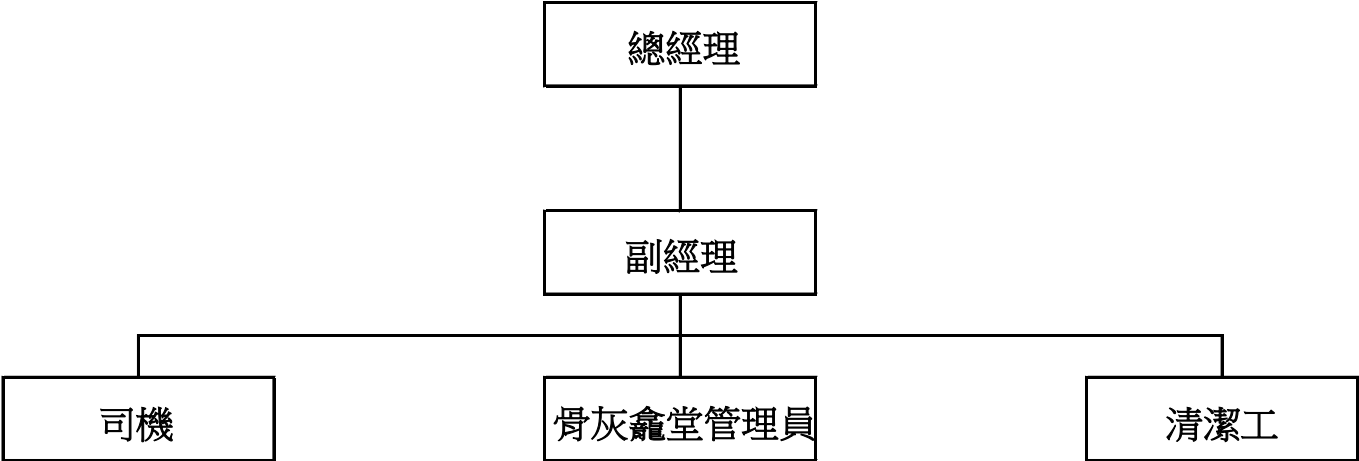


VISITOR WAITING  
AREA  
訪客等候區

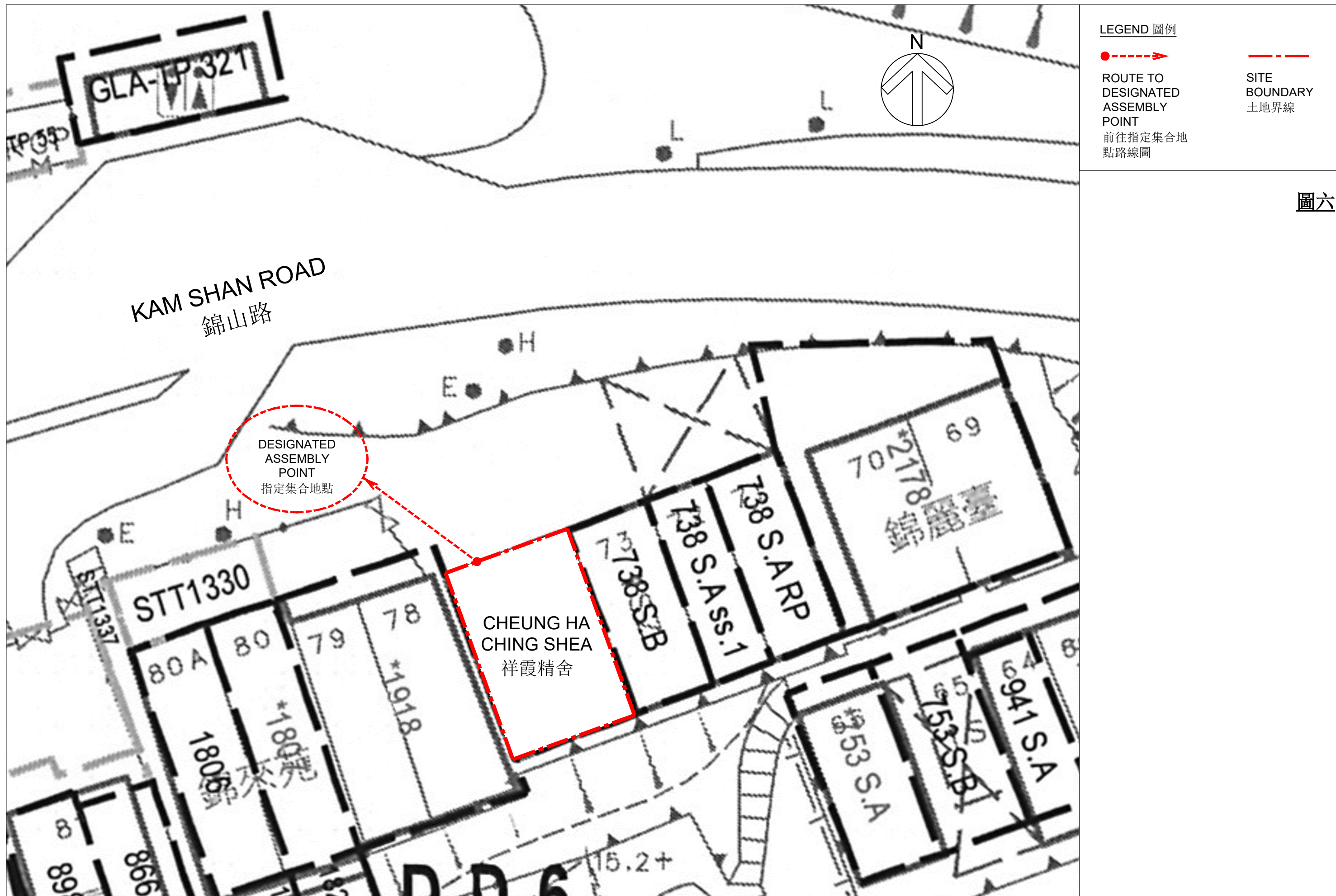
圖四

CHEUNG HA CHING SHEA VISITOR WAITING AREA  
祥霞精舍訪客等候區

祥霞精舍工作人員組織圖

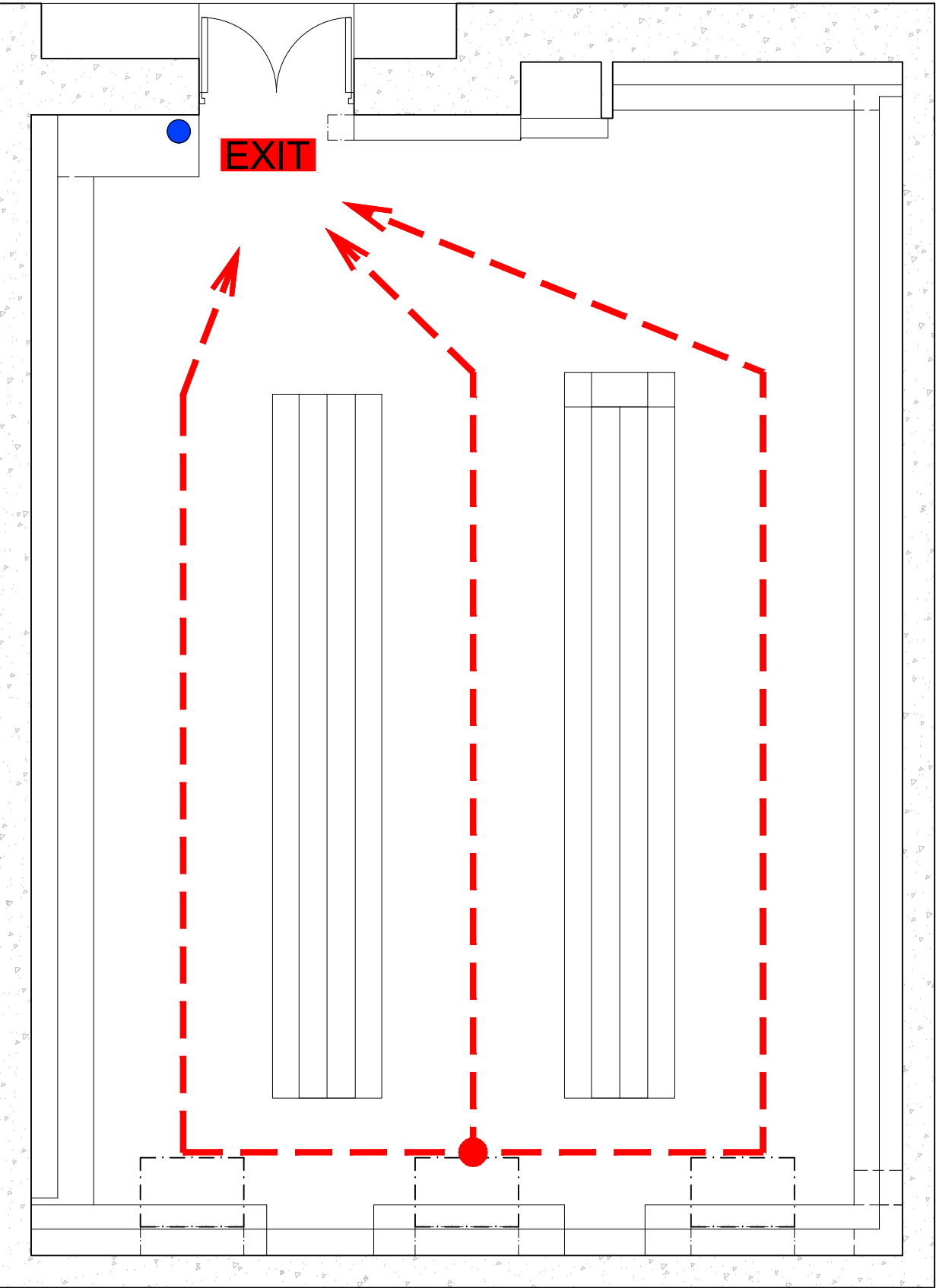






圖六

CHEUNG HA CHING SHEA FIRE SAFETY EVACUATION 'DESIGNATED ASSEMBLY POINT' LOCATION  
祥霞精舍消防安全疏散指定集合地點圖



LEGEND 圖例

- EXIT 出口
- FIRE EXTINGUISHER 滅火筒

圖七



## **Appendix VII**

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*Land Record of Lot 738*

No. 258.—It is hereby notified that the following Sale of Crown Land by Public Auction will be held at the District Land Office, Tai Po, at 10 a.m., on Saturday, the 1st day of May, 1909.

The Lots are sold for the term of Seventy-five years from the 1st day of July, 1898, with the right of renewal for a further term of 24 years less 3 days at a re-assessed Crown Rent as Building Lots subject to the General Conditions of Sale published in Government Notification No. 365 of 1906.

The amount to be spent in rateable improvements on each Lot under the General Condition No. 5 is \$100.

## PARTICULARS OF THE LOTS.

Registry No.	Locality.	Boundary Measurements.				Contents in Sq. ft.	Upset Price.	Annual Crown Rent.
		N.	S.	E.	W.			
		feet.	feet.	feet.	feet.		\$	¢
Survey District 6. Lot No. 739. Lot No. 739.	Kam Shan. Do.	70. 56	70 56	40 40	40 40	2,306 2,240	25 25	6.50 5.50

No. 259.—It is hereby notified that the following Sale of Crown Land by Public Auction will be held at the District Land Office, Tai Po, at 10 a.m., on Saturday, the 1st day of May, 1909.

The Lot is sold for the term of Seventy-five years from the 1st day of July, 1898, with the right of renewal for a further term of 24 years less 3 days at a re-assessed Crown Rent as a Building Lot subject to the General Conditions of Sale published in Government Notification No. 365 of 1906.

The amount to be spent in rateable improvements under the General Condition No. 5 is \$100.

## PARTICULARS OF THE LOT.

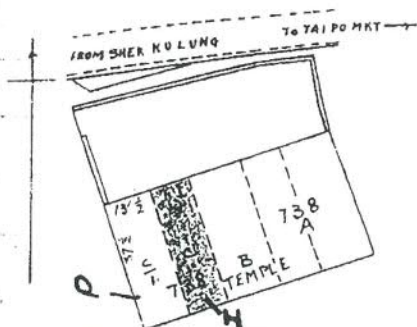
Registry No.	Locality.	Boundary Measurements.				Contents in Sq. feet.	Upset Price.	Annual Crown Rent.
		N.	S.	E.	W.			
		feet.	feet.	feet.	feet.		\$	\$
Survey District 113. Lot No. 1080.	Sai Pin Wai.	34	34	10	10	240	4	1



P-1

D.D. 6, Kham Shan Site Plan.

for Land Registrar  
7.2.2003



066971-N

Lot No. 738C1. -  $13\frac{1}{2} \times 37\frac{1}{2} = 506\frac{1}{4}$  s.f. or .02 ac.

Lot No. 738C. -  $11\frac{1}{2} \times 37\frac{1}{2} = 431\frac{1}{4}$  s.f. or .01 ac.

*98*  
20/1/51

Scale 50 : To One Inch.

Copy.No. 2486 1909

L. O. No: 20  
1909.

From — LAND OFFICER  
Place — HONGKONG  
Date — 13th. April, 1909.  
Received — 14th. — " —

RETURN  
TO  
PUBLIC RECORDS OFFICE  
OF HONG KONG

## SUBJECT.

Land at Kam Shan, New Territories.  
Survey District 6 Lot Nos. 738-739.

Application from Yau Chak  
Shing of Shek Kiu Lung for  
..... for building purposes.

Last previous paper.

Next subsequent paper.

Correspondence attached.



To The Land Officer.

DISTRICT LAND OFFICE

7th April 1903

Application has been made for land at *Kamshan* of which

I give particulars and recommendations below.

*W. S. B. Cross*  
Assistant Land Officer.

Name of applicant.	S. D. No	Locality.	Area in acres or sq. feet.	For what purpose required.	Period of years recommended.	Premium recommended.	Annual Crown Rent recommended.	Special Conditions
<i>San Chak Shing of Shek Kee Long</i>	<i>S.D. 6</i>	<i>Kamshan</i>	<i>N.S. 70' E.W. 40' = 2800 sq. ft.</i>	<i>Building</i>	<i>75 years</i>	<i>\$2800</i>	<i>\$6 50</i>	
<i>ditto</i>	<i>S.D. 6</i>	<i>ditto</i>	<i>N.S. 56' E.W. 40' = 2240</i>	<i>ditto</i>	<i>ditto</i>	<i>\$2300</i>	<i>\$5 50</i>	<i>27. 6. 1904</i>



TO THE LAND OFFICER.

HONG KONG

In No. *2486/99* C. S. O.

1909.

Return of Sale of Crown ~~land~~ <sup>2 1/2</sup> by Public Auction held by me at <sup>2 1/2</sup> on *Saturday*, the *12* day of *May* 1909

Registry No.	Name & Address of Purchaser	Amount of Premium.	Remarks.
<i>6</i>	<i>Van Chak Shing Co</i>	<i>\$ 28.00</i>	
<i>6</i>	<i>Quinty Sau Sun Kwong of Shek Kee Lung</i>	<i>\$ 23.00</i>	
	<i>ditto</i>		

Hon. Col. Treasurer.

Hon. C. S.

*adj. S. B. O. R.*

For information & to pass to Local Auditor & C.S.O.

Noted & passed to Local Auditor

Asst. Land Officer.

*adj. G. H. Hekman*

Hon. Col. Secy.  
Noted.

Land Officer.

Col. Treasurer

*2.5.1909*

Local Auditor.  
1909.

Memorial No.  
TP57471

No. 57471.

接獲日期  
(日/月/年)  
Received On  
(DD/MM/YYYY)  
27/11/1923

Memorial required to be registered in the District Office,  
according to the provisions of Ordinance No. 1 of 1844  
and Ordinance No. 34 of 1910.

and object of Instrument.	Division of Property
Date of Instrument.	27 <sup>th</sup> Nov. 1923.
Name and additions of Parties.	Yain Kwan Kwong & Shek Ben Hong Yain (Wan Joo) See (Joan Kwong CT)
Land or Premises affected and where	Yain Yung Shing
Consideration to whom and how paid	
Encumbrances to which the premises are subject and other special Covenants on particulars of the Instrument.	See attached list
and additions of Witnesses.	張祿文
of Parties signing Memorial	丘寶鑑 丘全光 丘銀光

Received at the District Office at Tai Po and certified as a correct Memorial and  
Registered as Memorial No. 57471 on 27<sup>th</sup> day the 27<sup>th</sup> day of Nov. 1923 at 10.

o'clock in the *for* noon according to section 31 of Ordinance No. 34 of 1910.

LR:YL



Name	Village	D.D.	Lot No.	Area	Class	C. Rent	
						\$	¢
Yan Nwa Kung Shek Kung		6	738A	2 ha		2	50
			739A	2 ha		3	00
			752	25 m <sup>2</sup>			
Yan Wan Kung Tso	Shek Kung		738B	1 ha		1	50
Yan Wan Kung							
Yan Kung Shek Kung			738C	2 ha		2	50
			739B	2 ha		3	00

50¢ increase on 739 this is caused by  
each share being over 100

George J. D. Smith  
3-10/23

## **Appendix VIII**

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*Support Gained from the Representatives of Yau's Ancestral Hall*

致各尊敬的

城市規劃委員會委員：

大埔錦山路 74-75 號丈量約份第 6 約地段  
第 738 號 C 分段及第 738 號 C 分段第 1 小分段  
由現有的「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶的改劃申請不反對通知書

本人丘錦明 YAU Kam Ming (身份證編號：[REDACTED])，是錦山村 (原住民代表) 村長，得悉申請人在上述地點向「城規會」提出擬議改劃土地用途申請，由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶，以重置現有私營骨灰安置所用途。在清楚了解下列主要發展參數內容後，本人代表錦山村原住民及邱氏祖祠\*支持上述申請。

根據申請方案，申請人願意以公營龕位出售價之平均值售予錦山村原住民合共 50 個雙灰位。這不但可以為錦山村原住民帶來多了一個選擇，在有助減少原住民傳統土葬的同時，也對進一步減少破壞現有山坡地帶的優處。況且，該申請地點位於本村的入口，絕對是方便本村村民的設施之一，尤其是對行動不便的村民，對老齡化的今天更為人性化，切合社會需求和需要。大部份其他村民也有同感。

該骨灰龕場在此營運多年來與左鄰右里和睦；再者，今次的申請方案又清楚劃分自家的出入口及活動範圍欄，是負責任的做法，更值得讚賞。此外，有不少海外的原住民亦親自反映有強烈意願，並交帶在離世後，將其骨灰安置於本村內，而祥霞精舍正正滿足本村原住民的需求。

其實，申請地點的單層建築房是在 1909 年由政府拍賣而建的，該地不可能，也不應該視作屬於鄉村發展的備用土地。今次的申請改劃從位置，用途，發展規模，以及惠及本村需要的 50 個雙灰位，是完全符合本村今天的發展性質和切合本村的實際需要，本人及眾村民一致支持。況且，該申請也沒有影響邱氏祖祠的日常運作，本人為邱氏祖祠之持份者之一，也沒有意見。

此上

錦山村村民代表簽署

丘錦明

丘錦明

錦山村原住民邱(丘)氏祖祠後人：

丘錦明 丘浩源 丘惠玲 丘美容

丘裕珍 丘華光 邱丁友 邱天里

邱二牛 邱炳 邱富喜 邱志誠

邱華生 丘國強 邱勝

\*邱姓字與丘姓字是共同歸屬於此祖祠

日期：2022 年 10 月 12 日



## **Appendix IX**

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*No Objection Letter from Owner of Nos. 71-72 Kam Shan Road*

致各尊敬的

城市規劃委員會委員：

大埔錦山路 74-75 號丈量約份第 6 約地段  
第 738 號 C 分段及第 738 號 C 分段第 1 小分段  
由現有的「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶的改劃申請  
不反對通知書

Winner Faith Limited 不反對喜蓮有限公司在錦山路 73 號與 74 號前方加建圍欄及閘門，如果喜蓮有限公司能夠承擔圍欄及閘門日後維修及保養責任，以及安排妥善配置予本公司一套該閘門的鎖匙。

此上

錦山村錦山路 71 及 72 號業主簽署



駱德祥先生

Winner Faith Limited 董事

日期： 2022 年 10 月 8 日

## **Appendix X**

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*Consent Obtained from Previous Niche Buyers*

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擠  
情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期  
期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期  
間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20008	鍾合安	

福位持有人 鍾合安

日期：2018 年 9 月 11 日

①

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擠  
情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期  
期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期  
間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20009	鄭康錦	

福位持有人 鄭康錦

日期：2018 年 9 月 11 日

②

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擠  
情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期  
期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期  
間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20011	蘇妹	

福位持有人 蘇妹

日期：2018 年 9 月 11 日

③

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擠  
情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期  
期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期  
間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20012	郭志生	

福位持有人 郭志生

日期：2018 年 9 月 11 日

④



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20018	方秀娟	

福位持有人 方秀娟

日期：2018年9月11日

⑤

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20019 20020	吳榮發	

福位持有人 吳榮發

日期：2018年9月11日

⑥

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20021	唐珠英	

福位持有人 唐珠英

日期：2018年9月11日

⑦

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20025	鄧國華	

福位持有人 鄧國華

日期：2018年9月11日

⑧

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20029	杜艷薇	

福位持有人 杜艷薇

日期：2018 年 9 月 11 日

(9)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20031	陳偉雄	

福位持有人 陳偉雄

日期：2018 年 9 月 11 日

(10)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20032 20034	葉若梅	

福位持有人 葉若梅

日期：2018 年 9 月 11 日

(11)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20035	梁偉明	

福位持有人 梁偉明

日期：2018 年 9 月 11 日

(12)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20037	李遠明	

福位持有人 李遠明

日期：2018 年 9 月 11 日

(13)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20039	張桂蓮	

福位持有人 張桂蓮

日期：2018 年 9 月 11 日

(14)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20038	張桂蓮	

福位持有人 張桂蓮

日期：2018 年 9 月 11 日

(15)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20040	李月貞	

福位持有人 李月貞

日期：2018 年 9 月 11 日

(16)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20024 20023	吳玉霞	

福位持有人 吳玉霞

日期：2018 年 9 月 11 日

17

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20036	丘紹孝	

福位持有人 丘紹孝

日期：2018 年 9 月 11 日

18

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20060	張智堅	

福位持有人 張智堅

日期：2018 年 9 月 11 日

19

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20065	馬合嬌	

福位持有人 馬合嬌

日期：2018 年 9 月 11 日

20



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20052	馬健焜	

福位持有人 馬健焜

日期：2018 年 9 月 11 日

21

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20066	陳屏山	

福位持有人 陳屏山

日期：2018 年 9 月 11 日

22

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20067	陳淑芬	

福位持有人 陳淑芬

日期：2018 年 9 月 11 日

23

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20058 20059	韓翠雲	

福位持有人 韓翠雲

日期：2018 年 9 月 11 日

24

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20070 20071	高健成	

福位持有人 高健成

日期：2018年9月11日

25

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20061	陳玉笑	

福位持有人 陳玉笑

日期：2018年9月11日

26

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20072 20073 20074	鄭卓元	

福位持有人 鄭卓元

日期：2018年9月11日

27

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20075	張貴平	

福位持有人

日期：2018年9月11日

28

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20076 20077	蘇憶慈	

福位持有人

日期：2018年9月11日

29

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20078 20081 20084 20079 20082 20085 20080 20083 20086	邱觀娣	

福位持有人 邱觀娣

日期：2018年9月11日

30

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20087	畢美茹	

福位持有人 畢美茹

日期：2018年9月11日

31

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20088 20089	蘇八妹	

福位持有人 蘇八妹

日期：2018年9月11日

32

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20090 20091	莫柏堅	

福位持有人 莫柏堅

日期：2018 年 9 月 11 日

33

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20093	江壽松	

福位持有人 江壽松

日期：2018 年 9 月 11 日

34

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20094 20095	林欣	

福位持有人 林欣

日期：2018 年 9 月 11 日

35

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20096	徐玉蘭	

福位持有人 徐玉蘭

日期：2018 年 9 月 11 日

36



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪老人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20097	李秀珍	

福位持有人 李秀珍

日期：2018 年 9 月 11 日

37

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪老人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20098	陳漢輝	

福位持有人 陳漢輝

日期：2018 年 9 月 11 日

38

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪老人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20099	黃俊祺	

福位持有人 黃俊祺

日期：2018 年 9 月 11 日

39

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪老人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20010	李素珍	

福位持有人 李素珍

日期：2018 年 9 月 11 日

40

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20101 20102	李淑珠	

福位持有人 李淑珠

日期：2018 年 9 月 11 日

(41)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20107	蘇仁妹	

福位持有人 蘇仁妹

日期：2018 年 9 月 11 日

(42)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20108 20109	曾潔雯	

福位持有人 曾潔雯

日期：2018 年 9 月 11 日

(43)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20110 20111 20112	吳雙	

福位持有人 吳雙

日期：2018 年 9 月 11 日

(44)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20113 20115 20114 20116	黃俊華	

福位持有人 黃俊華

日期：2018 年 9 月 11 日

45

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20117 20118	馬青蔚	

福位持有人 馬青蔚

日期：2018 年 9 月 11 日

46

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20121 20122	李香蓮	

福位持有人 李香蓮

日期：2018 年 9 月 11 日

47

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20119 20120	蘇桂強	

福位持有人

日期：2018 年 9 月 11 日

48

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20123	陳玉英	

福位持有人 陳玉英

日期：2018 年 9 月 11 日

(49)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20124	李永洪	

福位持有人 李永洪

日期：2018 年 9 月 12 日

(50)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20125	胡好	

福位持有人

日期：2018 年 9 月 12 日

(51)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20134, 20135	張細嬌	

福位持有人 張細嬌

日期：2018 年 9 月 12 日

(52)



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20126 20128 20127 20129	張錦媚	

福位持有人 張錦媚

日期：2018年9月12日

(53)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20130 20132 20131 20133	張天喜	

福位持有人 張天喜

日期：2018年9月12日

(54)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20138	劉美蓮	

福位持有人 劉美蓮

日期：2018年9月12日

(55)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20136 20137	沈璇娣	

福位持有人 沈璇娣

日期：2018年9月12日

(56)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20139 20140 20141	蘇仁妹	

福位持有人 蘇仁妹

日期：2018 年 9 月 12 日

(57)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20142 20143	石有信	

福位持有人 石有信

日期：2018 年 9 月 12 日

(58)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20144 20145	馮愛嫻	

福位持有人 馮愛嫻

日期：2018 年 9 月 12 日

(59)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20146	徐成	

福位持有人 徐成

日期：2018 年 9 月 12 日

(60)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20147 20148	蘇嘉慧	

福位持有人 蘇嘉慧

日期：2018 年 9 月 12 日

(61)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20149	朱偉東	

福位持有人 朱偉東

日期：2018 年 9 月 12 日

(62)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20150 20151	蘇敬希	

福位持有人 蘇敬希

日期：2018 年 9 月 12 日

(63)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20152	楊建華	

福位持有人 楊建華

日期：2018 年 9 月 12 日

(64)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20156 20157 20158	畢惠潔	

福位持有人 畢惠潔

日期：2018年9月12日

65

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20153 20154 20155	李金先	

福位持有人 李金先

日期：2018年9月12日

66

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20159 20160	蘇月洋	

福位持有人 蘇月洋

日期：2018年9月12日

67

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20165 20166	李穎娜	

福位持有人 李穎娜

日期：2018年9月12日

68



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20167 20169	梁文佳	

福位持有人 梁文佳

日期：2018 年 9 月 12 日

(69)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20168	鄺碧琪	

福位持有人 鄺碧琪

日期：2018 年 9 月 12 日

(70)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20164	黃碧華	

福位持有人 黃碧華

日期：2018 年 9 月 12 日

(71)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20170 20172 20171 20173	李保英	

福位持有人 李保英

日期：2018 年 9 月 12 日

(72)

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20174	林大華	

福位持有人 林大華

日期：2018 年 9 月 12 日

73

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20175	黎明禮	

福位持有人 黎明禮

日期：2018 年 9 月 12 日

74

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20176	陳寶明	

福位持有人 陳寶明

日期：2018 年 9 月 12 日

75

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20177	陳平山	

福位持有人 陳平山

日期：2018 年 9 月 12 日

76

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20178	左愛娟	

福位持有人 左愛娟

日期：2018年9月12日

77

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20179 20180	曾智亨	

福位持有人

日期：2018年9月12日

78

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20181 20182	曾煥駿	

福位持有人

日期：2018年9月12日

79

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20183 20184	魏子強	

福位持有人

日期：2018年9月12日

80

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話：
20185 20186	曾潮福	

福位持有人

日期：2018年9月12日

81

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話：
20187 20189 20188 20190	梁錦群	

福位持有人

日期：2018年9月12日

82

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話：
20191 20193 20192 20194	駱廷恩	

福位持有人

日期：2018年9月12日

83

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話：
20195 20197 20196 20198	駱廷賢	

福位持有人

日期：2018年9月12日

84



致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20199 20200	梁嘉欣	

福位持有人

日期：2018 年 9 月 12 日

85

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20201 20202	梁嘉珊	

福位持有人

日期：2018 年 9 月 12 日

86

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20203 20204	周芳梅	

福位持有人

日期：2018 年 9 月 12 日

87

致：Fancy Lotus Limited – 祥霞精舍

為免於清明節及重陽節期間對大埔錦山路及附近路段造成交通擁擠

情況，本人：

1. 同意並接受祥霞精舍於清明節/重陽節當日、兩個節日前後的星期日及所有在該兩星期內的公眾假期不對外開放的安排；及
2. 遵守祥霞精舍於拜祭期間的所有有關預約拜祭要求和在探訪期間對到訪者人要求的所有操守規則。

福位編號	福位持有人	聯絡電話
20205 20207 20206 20208	梁漢凌	

福位持有人

日期：2018 年 9 月 12 日

88

## **Appendix XI**

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*Consent Letter Obtained from Owners of Nos. 71-72 Kam Shan Road for  
Share Toilet Facilities*

## WINNER FAITH LIMITED

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祥霞精舍



喜蓮有限公司  
(袁善本先生)

日期: 2021 年 10 月 25 日

袁先生：

在 新界 大埔 錦山路 第 71 及 72 號 (即丈量約份第 6 約地段第 738 號 A 分段餘段及第 738 號 A 分段第 1 小分段) 的共用洗手間設施同意書

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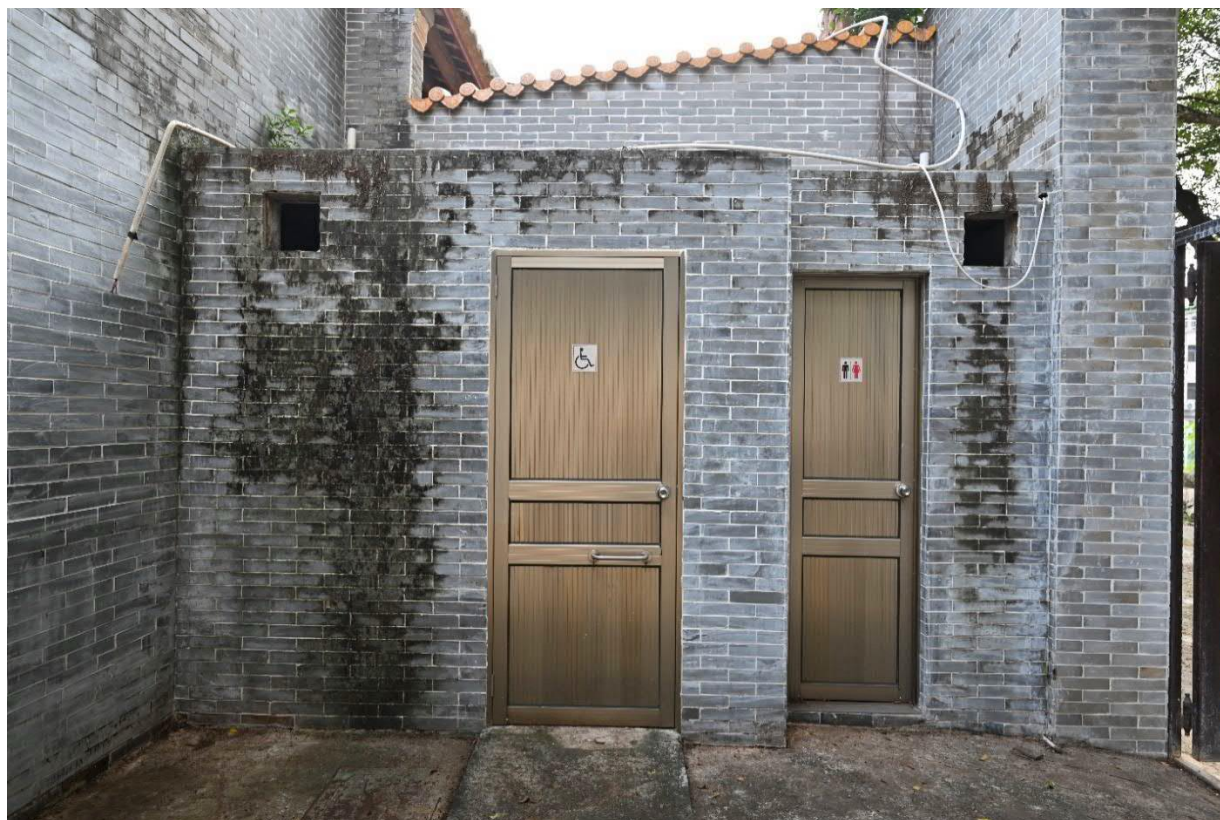
Winner Faith Limited 特此同意喜蓮有限公司及其位於新界 大埔 錦山 錦山路 74 及 75 號 (丈量約份第 6 約地段第 738 號 C 分段及第 738 號 C 分段第 1 小分段) 的骨灰安置所 (祥霞精舍) 的訪客及拜祭人士可使用本公司位於錦山路 71 及 72 號 的一所男女均可使用的洗手間 (一個座廁及一個洗手盆) 及一所傷殘人士洗手間 (一個座廁及一個洗手盆) (附圖 1 及附圖 2) 等設施。另外，本公司會負責洗手間的日常清潔，管理及維修保養該兩所洗手間的設施。

簽署

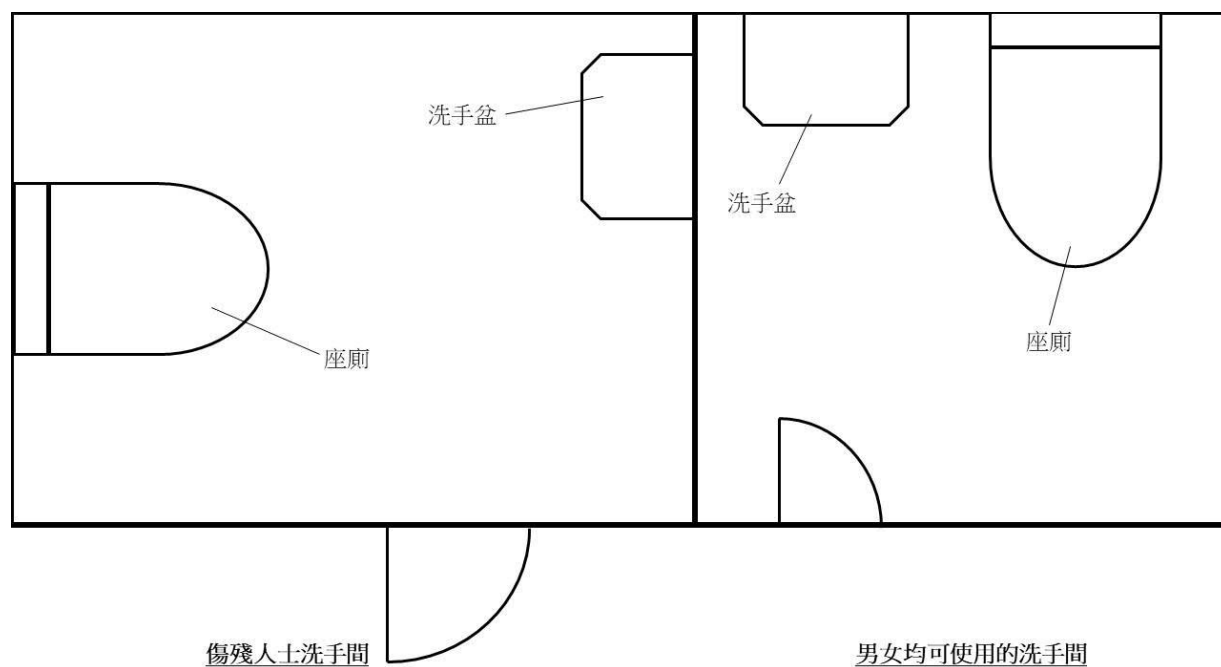
  
駱德祥先生  
Winner Faith Limited 董事



附圖 1



附圖 2



共用洗手間示意圖

(N.T.S.)

## **Appendix XII**

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### *Traffic Impact Assessment*



**Disclaimer:**

**This Traffic Impact Assessment (“TIA”) was an updated version of TP/32 scheme dated April 2021 at Nos. 72 and 74 Kam Shan Road. As the same amount of columbarium niches and same modes of operation are proposed at Nos. 74 and 75 Kam Shan Road, the results of this TIA are considered applicable to and valid for the present rezoning application in terms of the Traffic Management Plan and LOS in the area.**

Reference number CHK50549110

# **REZONING APPLICATION FROM “VILLAGE TYPE DEVELOPMENT” ZONE TO “GOVERNMENT, INSTITUTION OR COMMUNITY (3)” ZONE AT NOS. 71-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES**

## **TRAFFIC IMPACT ASSESSMENT**



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# 1 INTRODUCTION

## 1.1 Background

1.1.1 The Subject Site is located at Nos. 71-75 Kam Shan Road and adjoining Government Land, Tai Po, as indicated in **Figure 1.1**, which is currently zoned as “Village Type Development” (“V”) according to the approved Tai Po Outline Zoning Plan No. S/TP/28 (the “OZP”).

1.1.2 It is intended to regularise the existing 1,700 columbarium niche units at the Subject Site, via a rezoning application from “V” zone to “Government, Institution or Community (3)” (“G/IC (3)”) zone under Section 12A of the Town Planning Ordinance. A Traffic Impact Assessment (TIA) study is required in support of the proposed columbarium development at the Subject Site.

1.1.3 In order to minimise the unnecessary potential nuisances to its nearby dwellings, the Applicants have decided to fence off the existing pedestrian ramp serving the Subject Site in the form of a self-contained entrance alley to serve the whole proposed rezoning development:

- Columbarium niches at Nos. 72 and 74 Kam Shan Road;
- Ancillary office and store facilities at Nos. 71 and 75 Kam Shan Road; and
- An independent operation of an existing ancestral hall at No. 73 Kam Shan Road.

## 1.2 Study Objectives

1.2.1 The primary objective of this TIA study is to address possible traffic and transport concerns in association with the proposed columbarium use at the Subject Site.

- Evaluate the current traffic conditions for both vehicular and pedestrians in the vicinity of the Subject Site;
- Estimate traffic demand forecast for an appropriate design year;
- Estimate the traffic generations and attractions of the proposed and other adjacent columbarium developments during the following periods:
  - Festival Days (i.e. Ching Ming Festival and Chung Yeung Festival);
  - Shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days; and
  - Typical weekends (excluding the period from the 1<sup>st</sup> to 14<sup>th</sup> day of Lunar Chinese New Year during which the proposed (existing) columbarium will be closed).
- Investigate the traffic impact to the identified key road links and junctions by the proposed columbarium development;



- Review the anticipated pedestrian access routes and assess the operation performance at critical sections;
- Determine the demand for the internal transport facilities (e.g. parking, pick-up / drop-off, loading / unloading, etc.) regarding the proposed columbarium development and justify the proposed provision with regard to the site conditions and constraints; and,
- Suggest any appropriate traffic improvement and management measures in order to alleviate the foreseeable problems.

## 1.3 Structure of the Report

1.3.1 After this introductory chapter, there are five further chapters, they are:

- Chapter 2 – Existing Traffic Context, reviews the current traffic condition and public transport facility in the vicinity;
- Chapter 3 – The Proposed Development, describes the key parameters of the proposed columbarium development and presents the traffic management measures during Festival Days, shadow weekends and typical weekends and the closure of operation will also be extended to the Lunar Chinese New Year period (i.e. day one to day fourteenth)
- Chapter 4 – Traffic Impact Assessment, presents the traffic assessments of operation performance in terms of vehicular and pedestrian traffic as well as parking facilities; and
- Chapter 5 – Conclusion, summarises the study findings and presents the final conclusions accordingly.

## 2 EXISTING TRAFFIC CONTEXT

### 2.1 Existing Road Network

- 2.1.1 The Subject Site can be only accessed by Kam Shan Road, which is a single two-lane carriageway south of Lam Tsuen River, connecting to the major road network via Pak Shing Street and Kwong Fuk Road.
- 2.1.2 Pak Shing Street is a single two-lane carriageway south of Lam Tsuen River, linking Kam Shan Road in the west and Kwong Fuk Road in the east. There is a signalised pedestrian crossing at Pak Shing Street near Tai Wo Bridge across the Lam Tsuen River.
- 2.1.3 Kwong Fuk Road is a single 4-lane carriageway with local road linkage to Tai Po Central and Tai Po Market, as well as linkages to/from Tolo Highway for Shatin and Kowloon.
- 2.1.4 The major vehicular ingress and egress routes to the Subject Site are shown in **Figure 2.1**.

### 2.2 Public Transport Service

- 2.2.1 In terms of public transport, Kam Shan Road is served by GMB route 22K which only provides AM Peak services.
- 2.2.2 However, MTR Tai Wo Station is only 5-10 minute walk from the Subject Site via Kam Wo Bridge. A wide range of public transport modes are available in the vicinity of the Subject Site. In addition to the MTR East Rail Line, a vast number of road-based public transport services like franchised bus and Green Mini Bus (GMB) routes are also found as listed in **Table 2.1** and illustrated in **Figure 2.2**.

**Table 2.1 Public Transport Provision (Sunday / Public Holiday Services)**

Route No.	Description	Frequency (min.)
<b>Public Transport at Kam Shan Road</b>		
<b>GMB</b>		
22K	Kam Shan Road – Tai Po Market (Nam Shing Street)	AM Peak Only
<b>Public Transport at Tai Wo Station</b>		
<b>Franchised Bus</b>		
64K	Yuen Long West – Tai Po Market Station	7 – 10
71K	Tai Wo – Tai Po Market Station	15
72	Tai Wo – Cheung Sha Wan	20
73	Tai Po Industrial Estate – Wah Ming	30
73A	Wah Ming – Yu Chui Court	30
73B	Tai Po Nethersole Hospital – Sheung Shui	30
74A	Tai Wo – Kai Yip	60
74D	Kau Lung Hang – Kwun Tong Ferry	60
264R	Yuen Long (West) – Tai Po Market Station	20
<b>GMB</b>		
21K	Wai Tau Tsuen – Tai Po Market (Nam Shing Street)	15 – 30
25A	Nam Wa Po – Tai Po Market (Nam Shing Street)	6 – 11
25B	Kau Lung Heng/Yuen Leng – Tai Po Market (Nam Shing Street)	5 – 9
25K	Ng Tung Chai – Tai Po Market (Nam Shing Street)	5 – 12

2.2.3 Other than MTR Tai Wo Station, Tai Po Market and Kwong Fuk Road are in fact within 15-20 minutes of walking distance from the Subject Site.

2.2.4 In view of the availability of various transport modes within walking distance, the Subject Site enjoys an excellent accessibility in terms of public transport.

## 2.3 Traffic Count Survey

2.3.1 In view of the outbreak of COVID-19 and the regulations of group gathering (Cap. 599G), on-site traffic surveys are suspended since March 2020. Past traffic count surveys conducted at Kam Shan Road are thus adopted as described in **Table 2.2** to establish the background traffic conditions in the area. A supplementary traffic survey will be conducted when the situation is resumed.

**Table 2.2 Survey Periods**

No.	Description	Date	Time
1	Chung Yeung Festival Day	28 October 2017	09:00 – 17:00
2	Shadow Weekend of Chung Yeung Festival	29 October 2017	09:00 – 17:00
3	Typical Weekend	12 November 2017	09:00 – 17:00
4	Ching Ming Festival Day	5 April 2018	09:00 – 17:00

2.3.2 With the public transport provision being identified in **Figure 2.2**, the major access route for pedestrian to/from the Subject Site are anticipated as shown in **Figure 2.3**.

2.3.3 Pedestrian head count surveys have also been conducted at the identified key footpaths and crossings during the 4 survey periods as described in **Table 2.2** to establish the current pedestrian demand.

2.3.4 Based on the Level-of-Service (“LOS”) stipulated in the Transport Planning & Design Manual (“TPDM”) as enclosed in **Appendix A**, operational assessments for the observed peak hour flow have been carried out at the identified key pedestrian links.

**Table 2.3 Existing Performance of Pedestrian Links**

Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)			
				Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Year 2017 / 2018 Existing Scenario							
P1	Kam Wo Bridge	Footbridge	5.2	1103 3.5 (A)	770 2.5 (A)	555 1.8 (A)	1532 4.9 (A)
P2	Kam Shek New Village	Footpath	2.2	721 5.5 (A)	474 3.6 (A)	311 2.4 (A)	990 7.5 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	523 8.7 (A)	159 2.7 (A)	157 2.6 (A)	895 14.9 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	572 3.2 (A)	305 1.7 (A)	331 1.8 (A)	979 5.4 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	334 22.3 (B)	126 8.4 (A)	93 6.2 (A)	896 59.7(E)

2.3.5 As shown in **Table 2.3** and **Figure 2.4**, all the identified key pedestrian links are providing a comfortable walking environment with LOS A, except during peak seasons in Ching Ming Festival and Chung Yeung Festival respectively.

2.3.6 In addition to footpaths, the existing pedestrian crossing at Kam Shan Road, as shown in **Figure 2.4** has been assessed with the “Suggested Warrant for Formal Pedestrian Crossing Facilities” enclosed in **Appendix B**.

**Table 2.4 Assessment for Pedestrian Crossing at Kam Shan Road**

Parameters	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Year 2017 / 2018 Existing Scenario</b>				
Pedestrian crossing P (ped/hr)	430	202	110	767
2-way Vehicular Flow V (veh/hr)	435	261	193	542
PV <sup>2</sup> (10 <sup>8</sup> )	0.81	0.14	0.04	2.25
Recommendation of Pedestrian Crossing Facility	No Formal Crossing	No Formal Crossing	No Formal Crossing	Formal Crossing Required

2.3.7 As shown in **Table 2.4**, the existing cautionary crossing is considered sufficient for the observed level of pedestrian and vehicular flows at Kam Shan Road, except during Ching Ming Festival, whilst formal crossing (i.e. zebra or signal) or traffic management measures are required due to the surge volume of vehicular and pedestrian traffic at Kam Shan Road.

2.3.8 In addition, the existing junction performance in the vicinity of the Subject Site is also summarised in **Table 2.5**, which is operating with ample capacity for all surveyed periods. The traffic flows and calculation details are shown in **Figure 2.5** and **Appendix C** respectively.

**Table 2.5 Existing Junction Performance in the Vicinity of the Subject Site**

Table 2.5 Existing Junction Performance in the Vicinity of the Subject Site						
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity			
			Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Year 2017 / 2018 Existing Scenario						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.14	0.08	0.06	0.16
J3	Kam Shan Road/ Shek Lin Road	Priority	0.09	0.05	0.04	0.13

## 2.4 Existing Parking Provision

2.4.1 The public car parks available in vicinity of the Subject Site are illustrated in **Figure 2.6**.

2.4.2 The lay-by at Kam Shan Road (CP1) near the Subject Site provides a total of 7 parking spaces for Kam Shan Village, whilst most of the parking spaces are often occupied by local residents.

2.4.3 Instead, there is an outdoor meter parking of some 77 spaces available at Kam Shek New Village (CP2), which is some 300m further west along Kam Shan Road.

2.4.4 Besides, two indoor hourly public parking each with around 200 spaces, can be found in Tai Wo Estate across Lam Tsuen River off Po Nga Road (CP3) and Kai Wo Road (CP4) respectively.

2.4.5 To appraise the existing utilization of the above-mentioned car parks, occupancy surveys have been focused on CP2, CP3 and CP4 (excluding CP1 due to limited no. of parking spaces) during the three survey periods.

**Table 2.6 Observed Occupancy of Car Parks in the Vicinity of the Subject Site**

Survey Date and Location	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Year 2017 / 2018 Existing Scenario</b>				
<b>CP2 - Kam Shek New Village (outdoor meter parking)</b>				
Capacity	77	77	77	77
Peak Occupancy	77 (100%)	70 (91%)	60 (78%)	73 (95%)
Peak Availability	0 (0%)	7 (9%)	17 (22%)	4 (5%)
<b>CP3 - Tai Wo Estate near Po Nga Road (indoor hourly public parking)</b>				
Capacity	199	199	199	199
Peak Occupancy	199 (100%)	190 (95%)	164 (82%)	192 (96%)
Peak Availability	0 (0%)	9 (5%)	35 (18%)	7 (4%)
<b>CP4 - Tai Wo Estate near Kai Wo Road (indoor hourly public parking)</b>				
Capacity	208	208	208	208
Peak Occupancy	208 (100%)	208 (100%)	180 (87%)	197 (95%)
Peak Availability	0 (0%)	0 (0%)	28 (13%)	11 (5%)

2.4.6 As shown in **Table 2.6**, the public parking spaces are almost fully occupied during the festival days and the shadow weekend. However, spare parking spaces are available on the typical weekend for all 3 carparks.

2.4.7 Nevertheless, the kerbside utilisation along Kam Shan Road westbound has been observed on festival days, as well as the shadow weekend and the typical weekend in November 2017 respectively as shown in **Table 2.7** and **Figure 2.7**.

**Table 2.7 Observed Kerbside Utilisation along Kam Shan Road Westbound**

Survey Date and Location	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Kam Shan Road Westbound</b>				
Available Kerbside Length (m)	250	250	250	250
Equivalent Spaces Available	41	41	41	41
Peak Occupancy	40 (98%)	32 (78%)	24 (59%)	31 (76%)

2.4.8 Therefore, the shortage of parking provision and issues of illegal parking were mainly confined during the Festival Days, whilst the situation would be substantially improved on the Typical Weekend.

## 2.5 Observed Trip Generation for Private Columbarium Developments

2.5.1 In addition to the Subject Site, there are other private columbaria identified nearby Kam Shan Road. Trip generation surveys were carried out at these private columbarium developments including the Subject Site. The corresponding trip rates as observed on Festival Days, as well as the shadow weekend and typical weekend are shown in **Table 2.8**.



**Table 2.8 Observed Trip Rates for Columbarium Developments in the Vicinity**

Private Columbarium Developments in the vicinity	Approx. No. of Niches Sold & Occupied	No. of Trips (pph) in Peak Hour Peak Hour Trip Rate (pph/niche)				
		Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Shadow Weekend 02/04/2018	Ching Ming Festival 05/04/2018
Cheung Ha Ching Shea (The Subject Site)	66	20 0.303	17 0.258	14 0.212	-	300 4.545
Poh Yea Ching Shea	1,035	51 0.049	29 0.028	20 0.019	-	503 0.486
Buddhist Cheung Ha Temple	3,816	891 0.233	393 0.103	158 0.041	-	1,706 0.447
Ling Hin Fat Yuen(1)	174	-	-	-	25 0.143	93 0.534

Note:

- (1) The survey data for Ling Hin Fat Yuen were adopted from the "Counting Survey of Vehicular and Pedestrian Traffic conducted in 2018 Ching Ming Festival in respect of private columbaria that claimed to have pre-cut-off status" (PED 42.2)

[https://www.fhb.gov.hk/download/press\\_and\\_publications/otherinfo/160700\\_columbarium/full\\_set\\_of\\_survey\\_data.zip](https://www.fhb.gov.hk/download/press_and_publications/otherinfo/160700_columbarium/full_set_of_survey_data.zip)

2.5.2 It can be shown that the number of visitors and trip rates for columbarium developments are significantly higher during Ching Ming Festival than other survey periods.

2.5.3 In the absence of traffic management measures, the arrival pattern of visitors to the Subject Site is highly confined. The number of visitors during the peak hour is contributing about 30% of total visitors during the Festival day, especially the scale of the Subject Site is substantially smaller with fewer number of niches.

2.5.4 In terms of modal splits, the survey results for the private columbarium visitors along Kam Shan Road during the Festival Days, as well as the shadow weekend (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, and typical weekend in November 2017 are summarised in **Table 2.9** below:

**Table 2.9 Observed Modal Split for the Columbarium Visitors in the Vicinity**

Transport Mode	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Private Car	7%	7%	5%	26%
Taxi	2%	2%	1%	8%
Public Transport / Walking	91%	91%	94%	66%
Total	100%	100%	100%	100%

2.5.5 According to the observation and interview surveys, public transport was the dominant mode for at least 66% of columbarium visitors at Kam Shan Road, given the excellent accessibility to MTR, Bus and GMB services. It is also reflected by the significant increase in pedestrian flows at Kam Wo Bridge during the Festival Days as shown in **Table 2.3**.

2.5.6 Although on-site parking provision in the private columbarium developments nearby Kam Shan Road is nil, there are considerable number of visitors travelling by private cars and taxis, given that the weather was unstable on the Ching Ming Festival Day.

## 2.6 Supplementary Traffic Surveys

2.6.1 In order to reveal the traffic conditions on “non-peak days”, supplementary traffic surveys have been conducted on a typical Saturday and Sunday in September 2019 as summarised in **Table 2.10** below.

**Table 2.10 Supplementary Traffic Surveys – Survey Periods**

No.	Description	Date	Time
5	Typical Weekend – Saturday	21 September 2019	09:00 – 17:00
6	Typical Weekend - Sunday	22 September 2019	09:00 – 17:00

2.6.2 The performance of pedestrian links and crossing during the typical Saturday and Sunday are shown in **Tables 2.11 and 2.12** respectively. The corresponding location of pedestrian links and crossing are illustrated in **Figure 2.8**.

**Table 2.11 Supplementary Traffic Surveys – Performance of Pedestrian Links**

Table 2.11: Supplementary Traffic Surveys – Performance of Pedestrian Links					
Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)	
				Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
Year 2019 Supplementary Traffic Survey					
P1	Kam Wo Bridge	Footbridge	5.2	575 1.8 (A)	590 1.9 (A)
P2	Kam Shek New Village	Footpath	2.2	325 2.5 (A)	330 2.5 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	140 2.3 (A)	150 2.5 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	345 1.9 (A)	350 1.9 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	85 5.7 (A)	95 6.3 (A)

Note: Level-of-Service (LOS) refers to in the Transport Planning & Design Manual (TPDM) in **Appendix A**

**Table 2.12 Supplementary Traffic Surveys – Performance of Pedestrian Crossing**

Parameters	Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
<b>Year 2019 Supplementary Traffic Survey</b>		
Pedestrian crossing P (ped/hr)	100	110
2-way Vehicular Flow V (veh/hr)	190	205
PV <sup>2</sup> (10 <sup>8</sup> )	0.04	0.05
Recommendation of Pedestrian Crossing Facility	No Formal Crossing	No Formal Crossing

Notes: Refers to “Suggested Warrant for Formal Pedestrian Crossing Facilities” in **Appendix B**.

2.6.3 The junction performance in the vicinity of the Subject Site is reviewed in **Table 2.13**. The traffic flows and calculation details are shown in **Figure 2.9** and **Appendix C** respectively.

**Table 2.13 Supplementary Traffic Surveys – Performance of Junctions**

Table 2.13 Supplementary Traffic Survey – Performance of Junctions				
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity	
			Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
Year 2019 Supplementary Traffic Survey				
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.06	0.06
J3	Kam Shan Road/ Shek Lin Road	Priority	0.04	0.04

Notes: Calculation details can be referred to **Appendix C**.

2.6.4 Nevertheless, the parking occupancy and kerbside utilisation have also been reviewed in **Tables 2.14 and 2.15**, and **Figure 2.10**.

**Table 2.14 Supplementary Traffic Surveys – Parking Occupancy**

Survey Date and Location	Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
<b>Year 2019 Supplementary Traffic Survey</b>		
<b>CP2 - Kam Shek New Village (outdoor meter parking)</b>		
Capacity	77	77
Peak Occupancy	58 (75%)	63 (82%)
Peak Availability	19 (25%)	14 (18%)
<b>CP3 - Tai Wo Estate near Po Nga Road (indoor hourly public parking)</b>		
Capacity	199	199
Peak Occupancy	168 (84%)	172 (86%)
Peak Availability	31 (16%)	27 (14%)
<b>CP4 - Tai Wo Estate near Kai Wo Road (indoor hourly public parking)</b>		
Capacity	208	208
Peak Occupancy	175 (84%)	178 (86%)
Peak Availability	33 (16%)	30 (14%)

**Table 2.15 Supplementary Traffic Surveys – Kerbside Utilisation**

Survey Date and Location	Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
<b>Year 2019 Supplementary Traffic Survey – Kam Shan Road Westbound</b>		
Available Kerbside Length (m)	250	250
Equivalent Spaces Available	41	41
Peak Occupancy	22 (54%)	25 (61%)

2.6.5 In view of the above, it can be shown that the traffic conditions along Kam Shan Road are comparable between Saturday and Sunday during the typical weekend.

2.6.6 Nevertheless, the visitors numbers and associated traffic impact for columbarium uses are considered insignificant during weekdays (Monday – Friday). Therefore, the traffic conditions during “non-peak days” for columbarium uses can be justified by the surveys and assessments on typical weekends (including Saturday and Sunday during non-peak days).

### 3 THE PROPOSED DEVELOPMENT

#### 3.1 The Proposed Columbarium

- 3.1.1 The Subject Site is proposed for private columbarium uses with a total number of 1,700 niche units, subject to an agreement of the Town Planning Board and the subsequent approval by the Private Columbaria Licensing Board (“PCLB”).
- 3.1.2 The number of sold niche units before cut-off dated 18 June 2014 is 162, which is regarded as the baseline condition, whilst the potential traffic impact brought the “proposed” number of 1,538 niche units will be addressed by this TIA report.

#### 3.2 Traffic Management Plan

##### Introduction of ‘Visit-by-appointment’ System for Visitors

- 3.2.1 With consideration to the relatively small scale of the proposed columbarium development (a total number of 1,700 niches units), the holding capacity of the proposed columbarium development is intended to be 24 visitors.
- 3.2.2 In terms of crowd management, a ‘visit-by-appointment’ system will be adopted with 2 sessions each hour for all visitors of the proposed columbarium development at the Subject Site.
- 3.2.3 Therefore, the actual vehicular traffic generation and parking demand can be well controlled at 48 visitors per hour.

##### Promotion of Access by Public Transport

- 3.2.4 Due to its small development scale and the limitation of the site, the anticipated demand of parking for the proposed columbarium development, if any, are expected to be accommodated by the existing facilities in the vicinity.
- 3.2.5 By the time of appointment, all visitors will be notified well in advance that parking facilities will not be provided on-site. Indeed, they are expected to be travelling by public transportation facilities to and from the proposed columbarium. The alternative parking facilities in the vicinity, such as Kam Shek New Village and Tai Wo Estate, will be indicated in the Proposed Travel Plan for the visitors.
- 3.2.6 To encourage the use of the public transport, the location of public transport facilities, e.g. MTR Station, bus/GMB stops, etc. and the corresponding access routes as indicated in **Figure 2.2**. The potential impact to public transport services are estimated to be very minimal.
- 3.2.7 In view of the availability of public transport services within walking distance, shuttle services are considered not necessary for the proposed columbarium development.

#### Proposed Closure of Operation during Peak Seasons

- 3.2.8 For columbarium developments, the Ching Ming Festival and Chung Yeung Festival (i.e. the Festival Days), as well as their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, are regarded as the peak season(s) throughout a typical year.
- 3.2.9 Due to the absence of management plans for the other existing columbarium developments in the vicinity, the traffic conditions along Kam Shan Road during the peak seasons are considered critical and are assumed to continue for the worst-case scenario.
- 3.2.10 Therefore, the closure of the whole operation during the traffic peak seasons is proposed. Apart from the above mentioned peak season(s), the proposed columbarium will also be closed from the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year.
- 3.2.11 During the closure in the peak seasons, the entrance door of the subject columbarium will be completely locked up by chains with notice display.
- 3.2.12 The above closure arrangement is proposed to be included in the future purchasing agreements between the Applicants and future niche buyers; and will form part of the licensing conditions for the consideration of the PCLB.

#### Proposed Fence-off the Existing Ramp Connecting Kam Shan Road and the Subject Site

- 3.2.13 To separate the pedestrian access of columbarium visitors and the nearby residents, it is also proposed to fence-off the existing pedestrian ramp connecting Kam Shan Road to the Subject Site and to shown in **Figure 3.1**.
- 3.2.14 Given that the holding capacity of the proposed columbarium development is 24 visitors, the level-of-service of the subject pedestrian ramp can be well managed at LOS A (16 ped/m/min) via the future management plan to be approved by the PCLB.

### **3.3 Provision of Internal Transport Facilities**

- 3.3.1 With regard to the proposed columbarium use, the Hong Kong Planning Standards and Guidelines (HKPSG) have no recommendation on the provision of internal transport facilities.
- 3.3.2 Due to the limited footprint and inherent site constraints of the Subject Site, it is not feasible to provide any internal transport facilities within the proposed columbarium development.
- 3.3.3 The anticipated demand of parking and servicing, including pick-up/drop-off, loading/unloading, etc. are expected to be accommodated by the existing facilities in the vicinity.



## 4 TRAFFIC IMPACT ASSESSMENT

### 4.1 Design Year

- 4.1.1 Based on the operational experience from the Applicants, the number of columbarium visitors is generally the highest at the first intake and will gradually decline in the long term.
- 4.1.2 Year 2025 is assumed to be the first year of operation for the full approval by the Town Planning Board (TPB) and the PCLB. For the purpose of traffic impact assessment, the design year is assumed to be 2028 (3 years after the operation of the proposed columbarium)
- 4.1.3 Regarding the traffic forecast in terms of vehicles and pedestrians, the expected growth is derived with reference to the historical growth trend and future planning data.

#### Historical Growth Trend

- 4.1.4 The traffic counts reported in the Annual Traffic Census ("ATC") over a period of eight years inclusively between 2009 and 2016 are shown in **Table 4.1**.

**Table 4.1 ATC Counting Station Records in the Local Area**

Stn No.	Road Name	From	To	Annual Average Daily Traffic (AADT) <sup>(1)</sup>								Growth Rate <sup>(2)</sup>
				2009	2010	2011	2012	2013	2014	2015	2016	2009-2016
5009	Kwong Fuk Rd	Nam Wan Rd	Wan Tau St	20,930	20,730	21,010	20,660	20,510	20,400	20,150	20,570	-0.4%
5646	Kwong Fuk Rd	Wan Tau St	Po Heung St	15,330	15,050	15,270	15,180	15,300	15,140	14,830	14,920	-0.3%
6040	Po Heung St	Kwong Fuk Rd	Tai Po Tai Wo Rd	28,700	28,970	29,700	29,380	32,170	31,830	31,570	32,880	+2.0%
Total				64,960	64,750	65,980	65,220	67,980	67,370	66,550	68,370	+0.7%

Note:

(1) \* denotes AADT estimated by Growth Factor;

(2) The best-fitted growth is adopted by an exponential trend line (i.e.  $y = b * m^x$ ) by regression analysis; whereas the bracket value (b) represents the estimated AADT at Year 0 (i.e. 2008).

- 4.1.5 As indicated in **Table 4.1**, the annual average daily traffic ("A.A.D.T.") between 2009 and 2016 marginally declines at Kwong Fuk Road and gradually increases at Po Heung Street. In overall terms, the traffic growth pattern in terms of A.A.D.T. can be represented by the best-fitted trend with a positive growth rate of +0.7% per annum.

#### Population Projection in the Vicinity Area

- 4.1.6 With reference to Table 15 of the Projections of Population Distribution 2021-2029 issued by the Planning Department in March 2021, of which the latest Census and Statistics Department's projections of territorial population published in September 2020 were adopted as the control totals. The population projection for 2019-2025 in the vicinity of the Subject Site are reviewed in terms of Tertiary Planning Units (TPU).

- 4.1.7 The population projection in the corresponding TPU zones between 2019 and 2025, are incorporated and summarised in **Table 4.2**.

**Table 4.2 Population Distribution Projection in terms of Tertiary Planning Unit (TPU)**

Tertiary Planning Unit (TPU)		Year 2019	Year 2020	Year 2021	Year 2022	Year 2023	Year 2024	Year 2025
7.2.3	Tai Po	76,200	74,900	74,800	74,100	73,100	72,500	71,600
Average Growth 2016-2022		-1.0% per annum						

#### Adopted Growth Rate

- 4.1.8 Taking account of the historical traffic pattern and the future population projection, a nominal growth rate of +1.0% per annum is adopted in conservative terms for the purpose of this traffic study between years 2018 and 2028, given that there will not be any substantial intakes of population and further employment in major developments along Kam Shan Road.

## 4.2 Trip Generation for Private Columbarium Developments

- 4.2.1 According to the management measure in **Section 3.2**, a ‘visit-by-appointment’ system will be adopted for all visitors of the proposed columbarium development. The maximum holding capacity of the proposed columbarium development is intended to be 24 visitors for two sessions each hour. Therefore, the number of trip generation and attraction can be well controlled at 48 visitors per hour.
- 4.2.2 Considering the scale of the proposed columbarium and the latest registration information from the Applicants, most of the current niche owners are local residents in Tai Po District. For the worst-case scenario, the modal split of Ching Ming Festival Day is adopted for potential visitors and summarised in **Table 4.3** below:

**Table 4.3 Adopted Modal Split for the Potential Columbarium Visitors**

Proposed Development		Parameters	Peak Visitor Number (pph)		
			IN	OUT	2-WAY
Cheung Ha Ching Shea (The Subject Site)		1,700 no. of niche units	48	48	96
Private Car		26%	13	13	26
Taxi		8%	4	4	8
Bus / GMB		10%	5	5	10
MTR		28%	13	13	26
Walk		28%	13	13	26
Kam Shan Road: Route P3	Private Car / Taxi:	34%	17	17	34
Kam Wo Bridge: Route P3-C1-P2-P1	MTR / Bus / GMB / Walk	42%	19	19	38
Kwong Fuk Road Route P3-C1-P4	Bus / GMB / Walk	12%	6	6	12
Kwong Fuk Road Route P3-P5	Bus / GMB / Walk	12%	6	6	12

4.2.3 Based on the traffic surveys during Chung Yeung Festival, the average occupancy rate was 3 visitors per vehicle for private cars and taxis respectively.

4.2.4 Hence, it is estimated that 5 private cars ( $13 / 3 = 4.33$ ) and 2 taxis ( $4 / 3 = 1.3$ ) will be attracted to and generated from the Subject Site respectively.

### 4.3 Operational Performance for Pedestrian Links and Road Junctions

4.3.1 The operational performance of the identified key pedestrian links was assessed with the anticipated 2028 pedestrian flows as shown in **Figures 4.1** and **4.2** respectively. The results of reference case (without Proposed Development) and design case (with Proposed Development) are summarised in **Table 4.4**.

**Table 4.4 Year 2028 Performance of Pedestrian Links**

Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)			
				Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
Year 2028 Reference Scenario (Without Development)							
P1	Kam Wo Bridge	Footbridge	5.2	1,231 3.9 (A)	859 2.8 (A)	619 2.0 (A)	1,692 5.4 (A)
P2	Kam Shek New Village	Footpath	2.2	804 6.1 (A)	529 4 (A)	347 2.6 (A)	1,094 8.3 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	583 9.7 (A)	177 3.0 (A)	175 2.9 (A)	989 16.5 (B)
P4	Kam Shan Road (E) – North side	Footpath	3.0	638 3.5 (A)	340 1.9 (A)	369 2.1 (A)	1,081 6.0 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	372 24.8 (C)	141 9.4 (A)	104 6.9 (A)	990 66.0 (E)
Year 2028 Design Scenario (With Development)							
P1	Kam Wo Bridge	Footbridge	5.2	1,269 4.1 (A)	897 2.9 (A)	657 2.1 (A)	1,730 5.5 (A)
P2	Kam Shek New Village	Footpath	2.2	842 6.4 (A)	567 4.3 (A)	385 3.0 (A)	1,132 8.6 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	679 11.3 (A)	273 4.6 (A)	271 4.5 (A)	1,085 18.1 (B)
P4	Kam Shan Road (E) – North side	Footpath	3.0	650 3.6 (A)	352 2.0 (A)	381 2.1 (A)	1,093 6.1 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	384 25.6 (C)	153 10.2 (A)	116 7.7 (A)	1002 66.8 (E)

4.3.2 With the proposed management plan, a maximum number of 24 persons will be present within the Subject Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors per hour. All of the pedestrian links in the vicinity will be operated with an acceptable performance of LOS A during shadow weekends and typical weekends, except during peak seasons in Ching Ming Festival and Chung Yeung Festival respectively.

4.3.3 The junction performance in the vicinity of the Subject Site is summarised in **Table 4.5**, which will be operating with ample capacity in Year 2028 with or without the proposed development. The traffic flows and calculation details are shown in **Figure 4.3**, **Figure 4.4** and **Appendix C** respectively.

**Table 4.5 Year 2028 Junction Performance in the Vicinity of the Subject Site**

Table 4.5 Year 2028 Junction Performance in the Vicinity of the Subject Site						
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity			
			Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
Year 2028 Reference Scenario (Without Development)						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.15	0.09	0.06	0.17
J3	Kam Shan Road/ Shek Lin Road	Priority	0.10	0.06	0.05	0.14
Year 2028 Design Scenario (With Development)						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.16	0.09	0.07	0.18
J3	Kam Shan Road/ Shek Lin Road	Priority	0.10	0.07	0.05	0.14

4.3.4 In terms the cautionary crossing at Kam Shan Road, the performance for the reference and design scenario in year 2028 are reviewed in **Table 4.6**.

**Table 4.6 Year 2028 Performance of Pedestrian Crossing at Kam Shan Road**

Parameters	Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
<b>Year 2028 Reference Scenario (Without Development)</b>				
Pedestrian crossing P (ped/hr)	480	225	123	847
2-way Vehicular Flow V (veh/h)	485	291	215	599
PV <sup>2</sup> (10 <sup>8</sup> )	1.13	0.19	0.06	3.04
Recommendation of Pedestrian Crossing Facility	Formal Crossing Required	No formal Crossing	No formal Crossing	Formal Crossing Required
<b>Year 2028 Design Scenario (With Development)</b>				
Pedestrian crossing P (ped/hr)	530	275	173	897
2-way Vehicular Flow V (veh/h)	499	305	229	613
PV <sup>2</sup> (10 <sup>8</sup> )	1.32	0.26	0.09	3.37
Recommendation of Pedestrian Crossing Facility	Formal Crossing Required	No formal Crossing	No formal Crossing	Formal Crossing Required

4.3.5 It can be shown that the existing cautionary crossing will be sufficient for the proposed columbarium development at the Subject Site during the shadow weekend and typical weekend respectively.

4.3.6 In the absence of management plans for the other existing columbarium developments in the vicinity of the Subject Site, formal crossing (i.e. zebra or signal) or crowd control measures will be required during the Festival Days due to the surge volume of vehicular and pedestrian traffic at Kam Shan Road.

- 4.3.7 To avoid the critical traffic conditions during peak seasons, it is proposed to **close the operation during Ching Ming and Chung Yeung Festivals**, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days. The proposed columbarium will also be closed from **the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year**.

#### 4.4 Availability of Parking Facilities in the Vicinity

- 4.4.1 Given the excellent provision of public transport services at Kam Shan Road, the modal split of vehicular traffic for the proposed columbarium development is anticipated to be fairly low.
- 4.4.2 Also, with consideration to the relatively small scale of the proposed columbarium development (a total number of 1,700 niches units), the actual vehicular traffic generation and attraction are estimated to be very minimal at only 5 private cars and 2 taxis per hour in one direction.
- 4.4.3 Recalling the recent observation and traffic surveys as shown in **Tables 2.5** and **2.6**, it is revealed that the shortage of parking provision and issues of illegal parking were mainly confined during the Festival Days, due to the absence of management plans for the columbarium developments in the vicinity.
- 4.4.4 During typical weekends, however, there are plenty of vacant parking spaces available at Kam Shek New Village and Tai Wo Estate respectively.
- 4.4.5 With the proposed traffic management plan in **Section 3.2** (including closure during peak seasons, promotion of public transport and admission control to 48 visitors per hour), the provision of parking facilities in the vicinity are sufficient for the minimal parking demand by the Subject Site.

#### 4.5 Other Private Columbarium Developments in the Vicinity

- 4.5.1 In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road. The details and location of these private columbaria are summarised in **Table 4.7** and **Figure 4.5**.

**Table 4.7 Private Columbarium Developments in the Vicinity**

Private Columbarium Developments	Latest Planning Application	Application Status	No. of Units under Application	
			Columbarium Niches	Memorial Tablets
Cheung Ha Ching Shea (The Subject Site)	Y/TP/32	Withdrawn on 12/03/2021	1,700	-
Poh Yea Ching Shea	A/TP/657	Approved on 29/11/2019	5,302	-
Buddhist Cheung Ha Temple	Y/TP/34	Submitted on 01/02/2021	13,426	3,049
Ling Hin Fat Yuen	A/TP/652	Approved with conditions on 19/07/2019	757	1,782
Total			21,185	5,050

Source: Statutory Planning Portal 2, Town Planning Board (<https://www2.ozp.tpb.gov.hk/>)



- 4.5.2 In terms of cumulative traffic impact for the columbarium developments in the vicinity, a nominal rate of +1.0% per annum between years 2018 and 2028 has been assumed as described in **Section 4** to represent the gradual intakes of “sold and unoccupied niches”, given that there will not be any substantial intakes of population and employment by major developments along Kam Shan Road.
- 4.5.3 To account for the full-intakes of the “sold niches before cut-off” as the worst-case scenario, a sensitivity test is included in **Appendix D** to examine the corresponding pedestrian links and road junctions.
- 4.5.4 With the proposed closure of operation during Ching Ming and Chung Yeung Festivals, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, the critical traffic conditions during peak seasons at Kam Shan Road will no longer be correlated to the Subject Site. The proposed columbarium will also be closed from the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year.
- 4.5.5 Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons per each 30-minute session will be allowed within the Subject Site. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors per hour. The level-of-service of the self-contained ramp can also be well managed at LOS A (16 ped/m/min).
- 4.5.6 Therefore, the traffic conditions during typical weekends (including Saturday and Sunday during non-peak days) will be operated at acceptable level with the proposed columbarium development at the subject site and the proposed management measures.

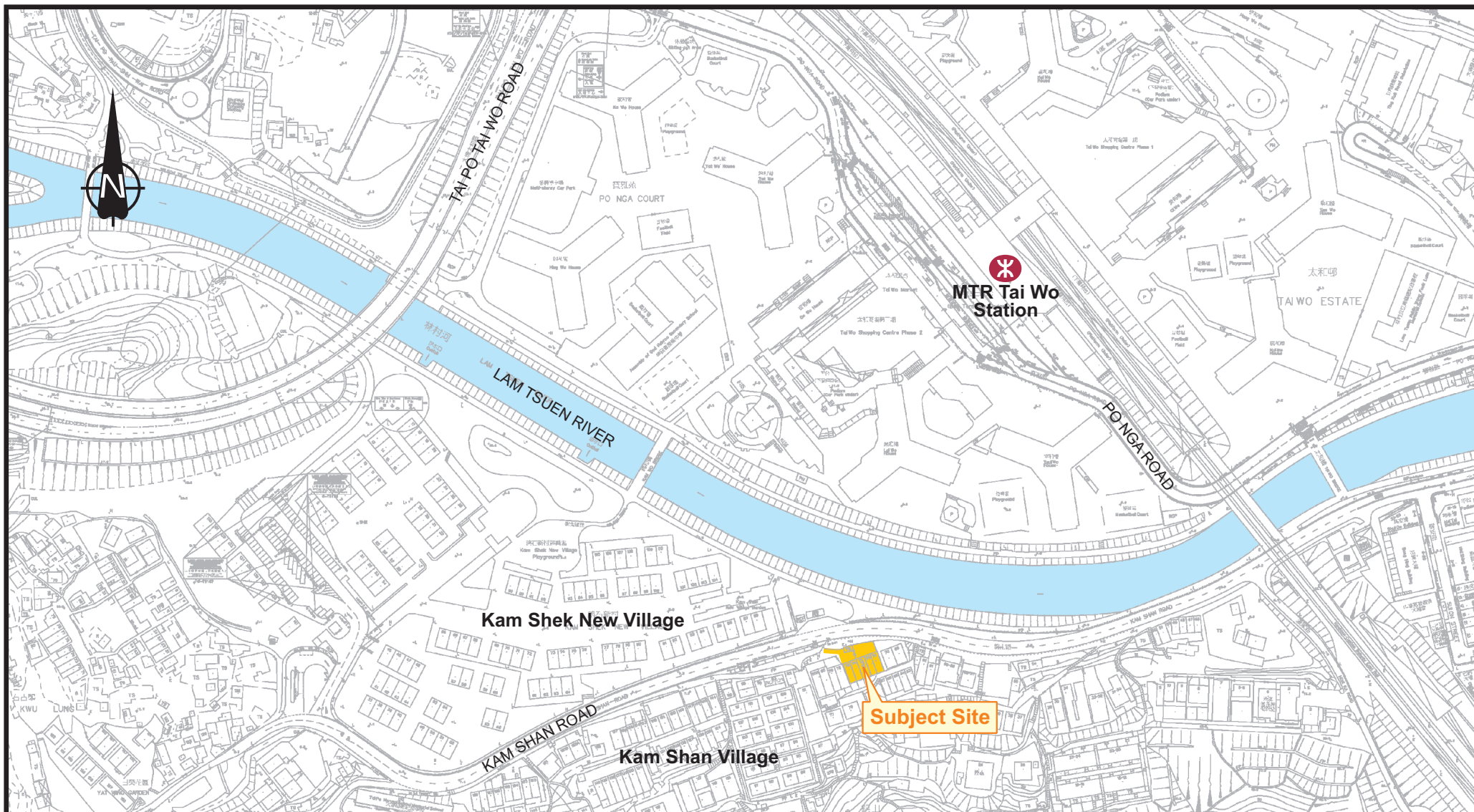
## 5 SUMMARY AND CONCLUSION


### 5.1 Summary

- 5.1.1 This report is prepared to present the TIA findings and the proposed traffic management for an existing private columbarium development proposed at the Subject Site (covering Nos. 71-75 Kam Shan Road and adjoining Government Land), Tai Po. A total number of 1,700 niche units (i.e. 162 sold and 1,538 new niches) are proposed.
- 5.1.2 The number of sold niche units before cut-off dated 18 June 2014 is 162, which is regarded as the baseline condition, whilst the potential traffic impact brought by the “proposed” number of 1,538 niche units will be addressed by this TIA report.
- 5.1.3 Vehicular access of the proposed columbarium development is provided by Kam Shan Road, which is a single two-lane carriageway south of Lam Tsuen River.
- 5.1.4 In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road, such as Buddhist Cheung Ha Temple, Poh Yea Ching Shea and Ling Hin Fat Yuen.
- 5.1.5 For columbarium developments, it is revealed from traffic surveys that the Ching Ming Festival and Chung Yeung Festival (i.e. the Festival Days), as well as their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, are regarded as the peak season(s) throughout a typical year.
- 5.1.6 Due to the absence of management plans for the other existing columbarium developments in the vicinity, the traffic conditions along Kam Shan Road are considered critical during the peak seasons.
- 5.1.7 On the other hand, the traffic conditions along Kam Shan Road are acceptable during typical weekends (including Saturday and Sunday).
- 5.1.8 To avoid the critical traffic conditions during peak seasons, it is proposed to **close the operation during Ching Ming and Chung Yeung Festivals**, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days. The proposed columbarium will also be closed from **the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year**.
- 5.1.9 During the closure in the peak seasons, the entrance door of the subject columbarium will be completely locked up by chains with notice display.
- 5.1.10 Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons per each 30-minute visit session will be permitted in the Subject Site. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors per each hour during operation hours.
- 5.1.11 In order to minimise the potential nuisances to its nearby dwellings, the Applicants have decided to fence off the existing pedestrian ramp serving the Subject Site in the form of a self-contained entrance alley to serve the proposed rezoning development.

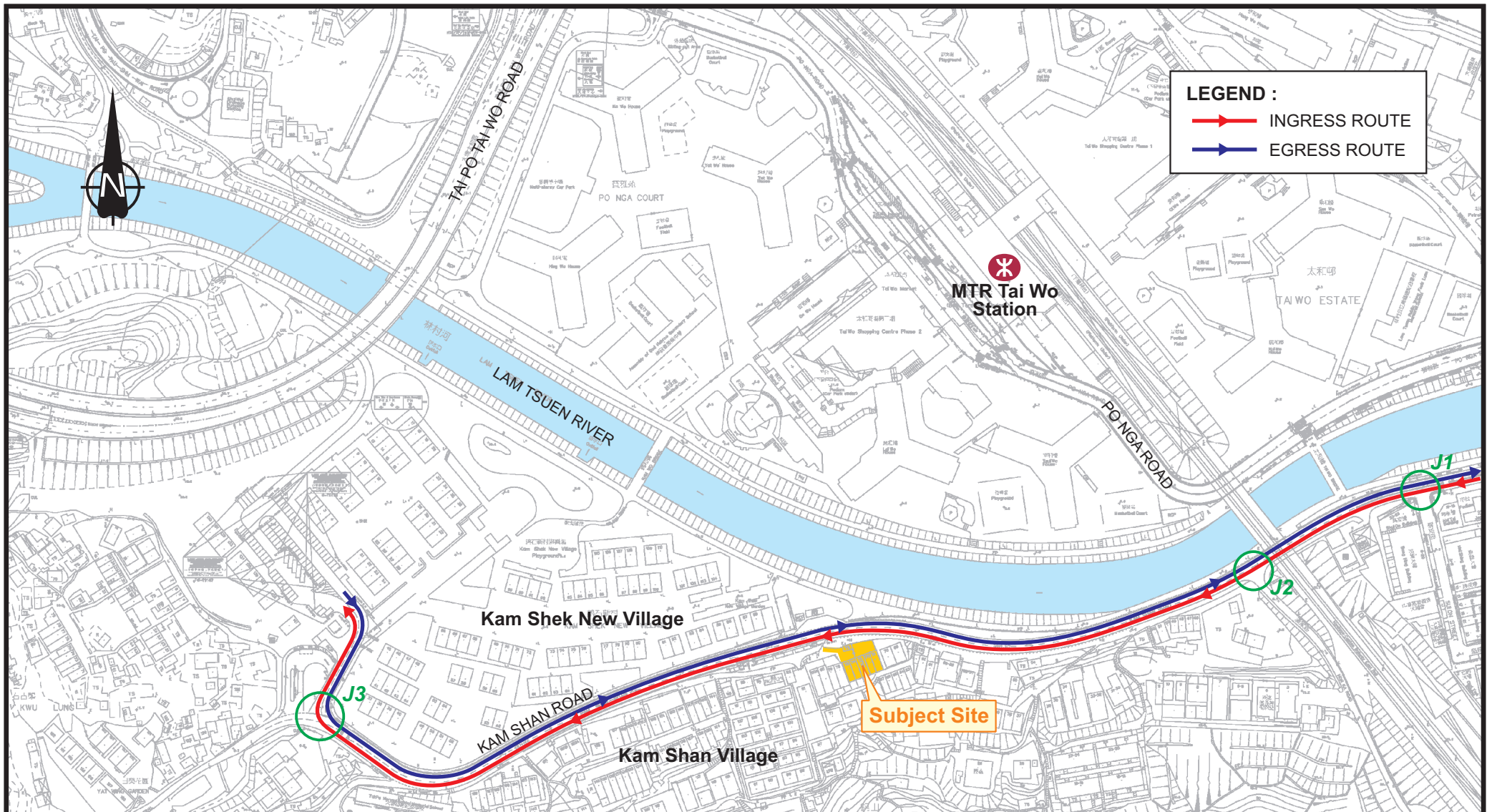
- 5.1.12 The level-of-service of the subject pedestrian ramp would be well managed at LOS A (16 ped/m/min) via the future management plan to be approved by the PCLB.
- 5.1.13 Parking will not be provided on-site. Access by public transport is highly encouraged by the proposed management plan, where the available public transport facilities, e.g. MTR Station, bus/GMB stops, etc. and the corresponding access routes will be clearly informed to all visitors.
- 5.1.14 Nevertheless, the cumulative traffic impact for the columbarium developments in the vicinity have been incorporated by a nominal growth rate accounting for the gradual intakes of “sold and unoccupied niches”. The full intakes of “sold and unoccupied niches” have also been accounted by a sensitivity test as the worst-case scenario.
- 5.1.15 In view of the assessment results with the implementation of the proposed management measures, the local traffic conditions during typical weekends (including Saturday and Sunday) will be operated at acceptable level.
- 5.1.16 Therefore, it can be concluded that the proposed columbarium development with a total of 1,700 niche units in this part of the Kam Shan area is considered acceptable in traffic engineering and planning terms.

## FIGURES

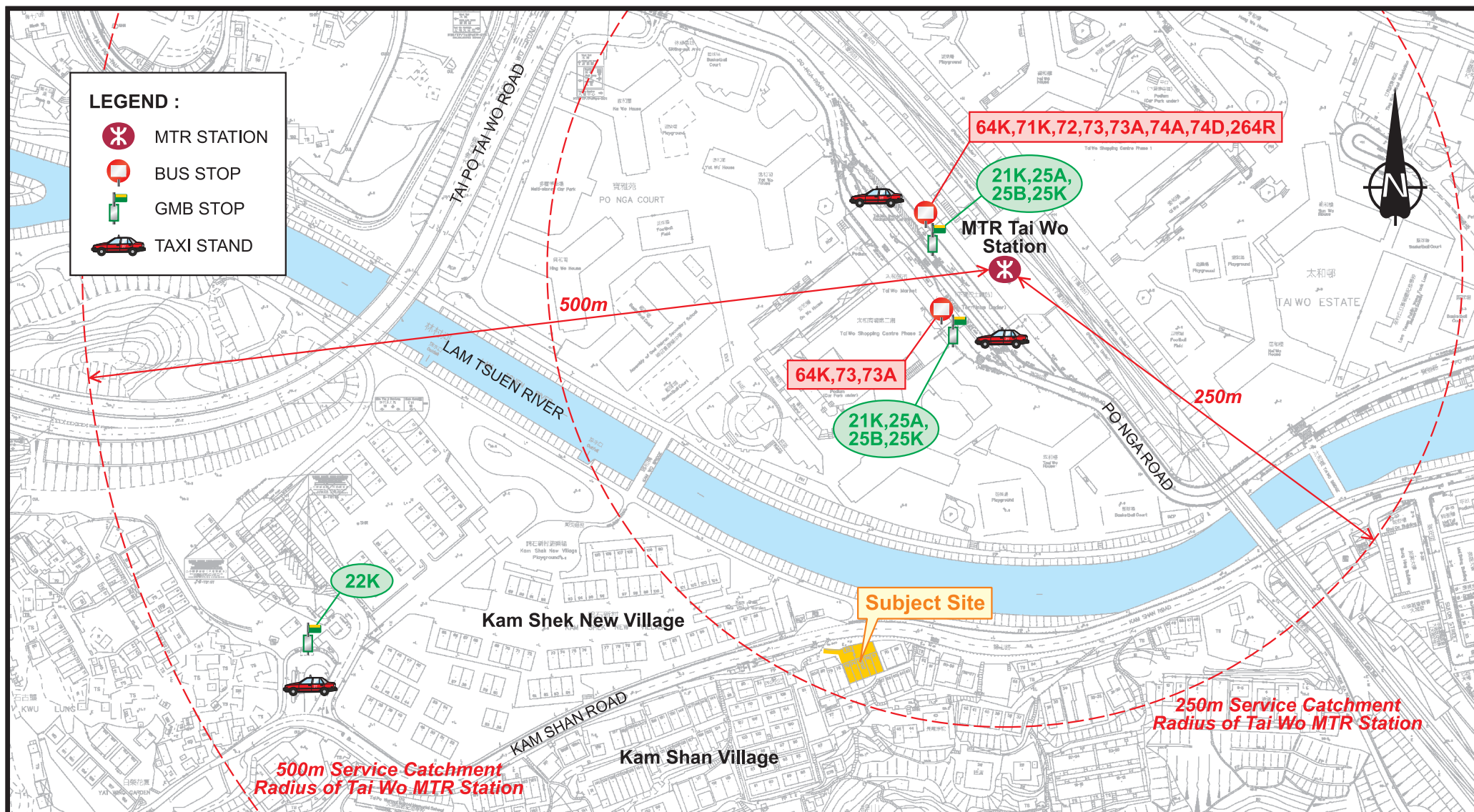


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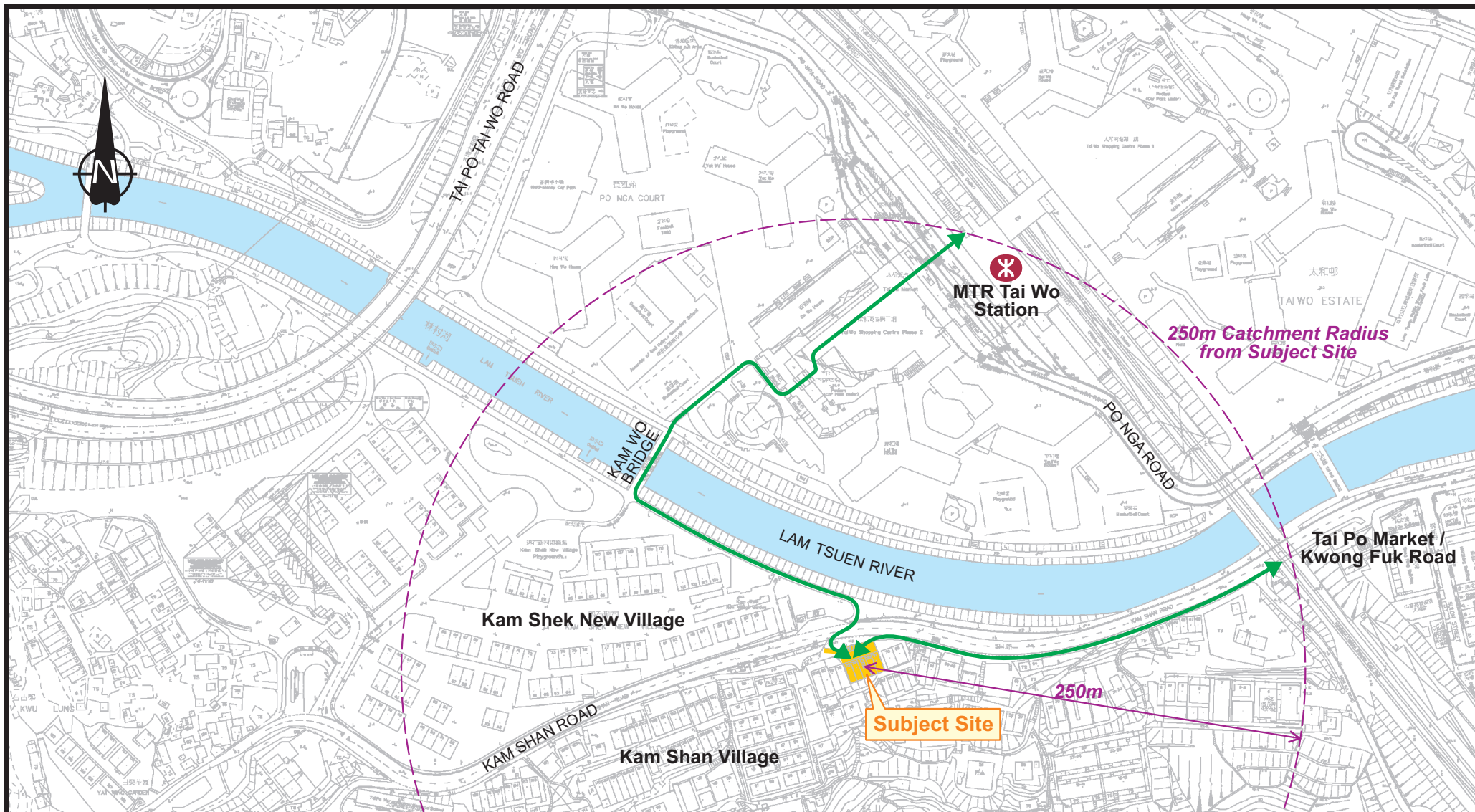



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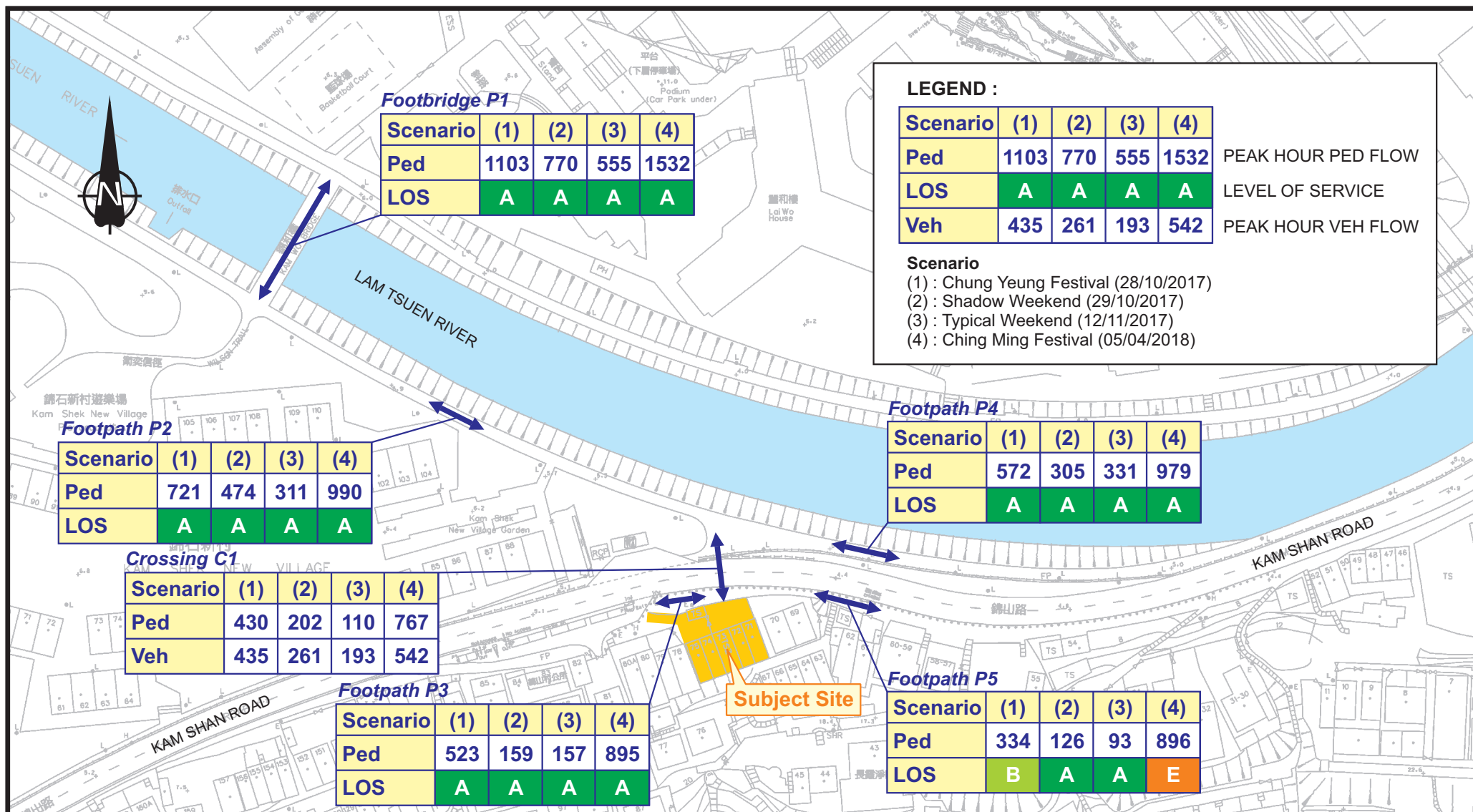


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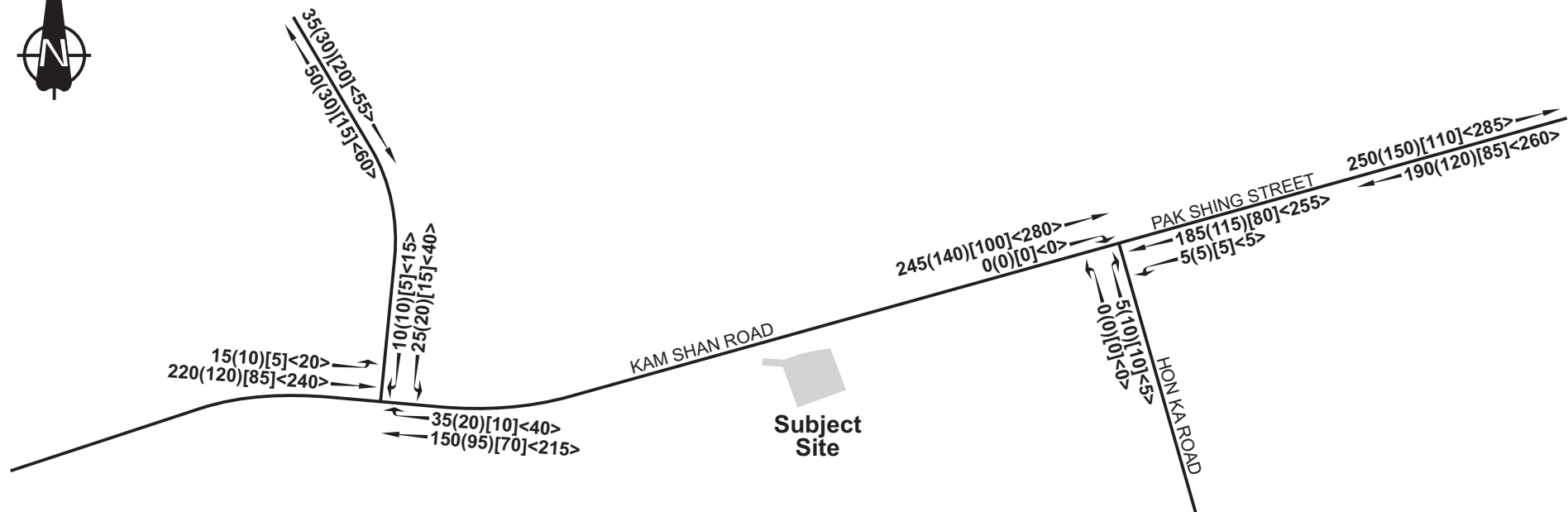




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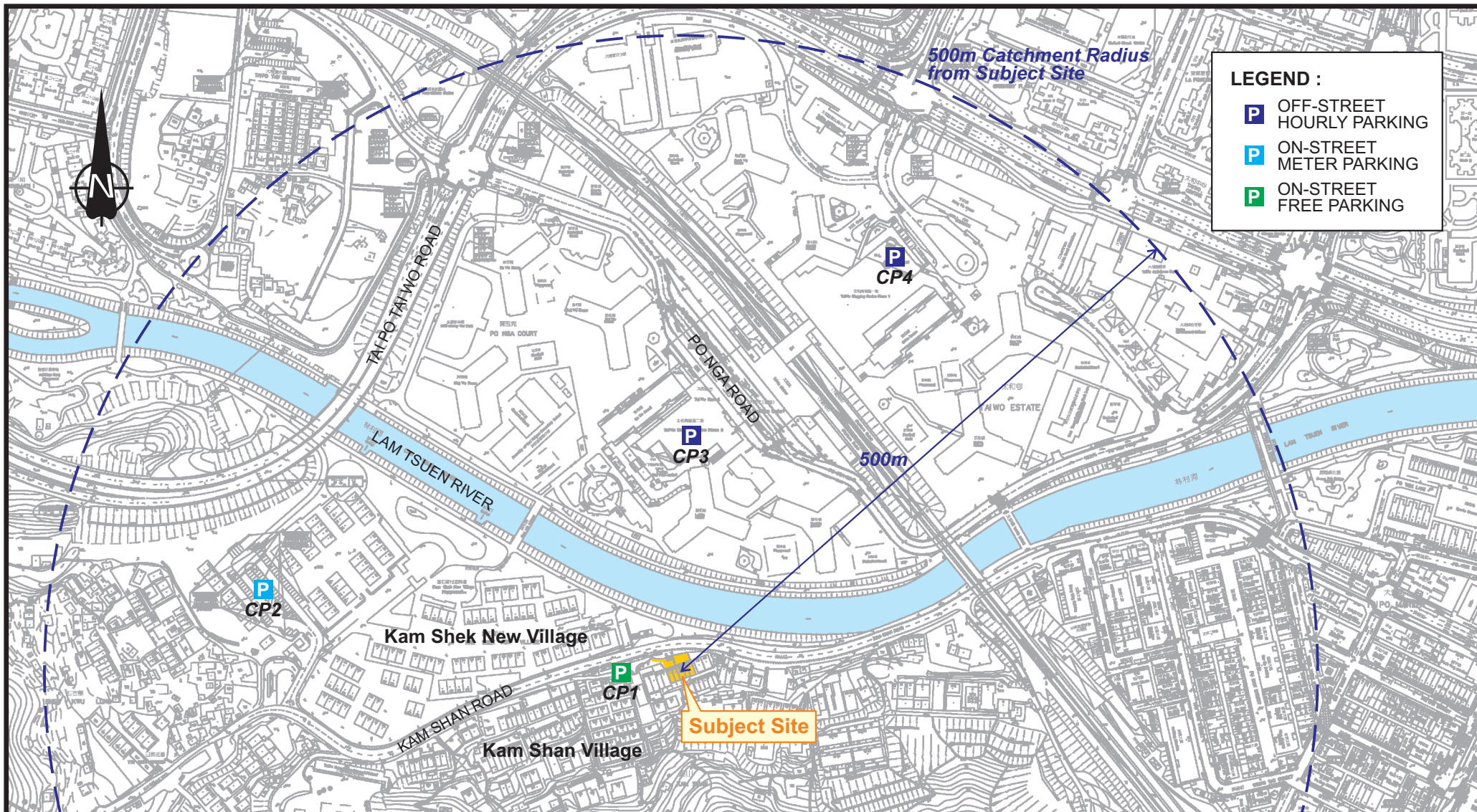


#### LEGEND :

- 185 CHUNG YEUNG PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (115) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [80] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <255> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

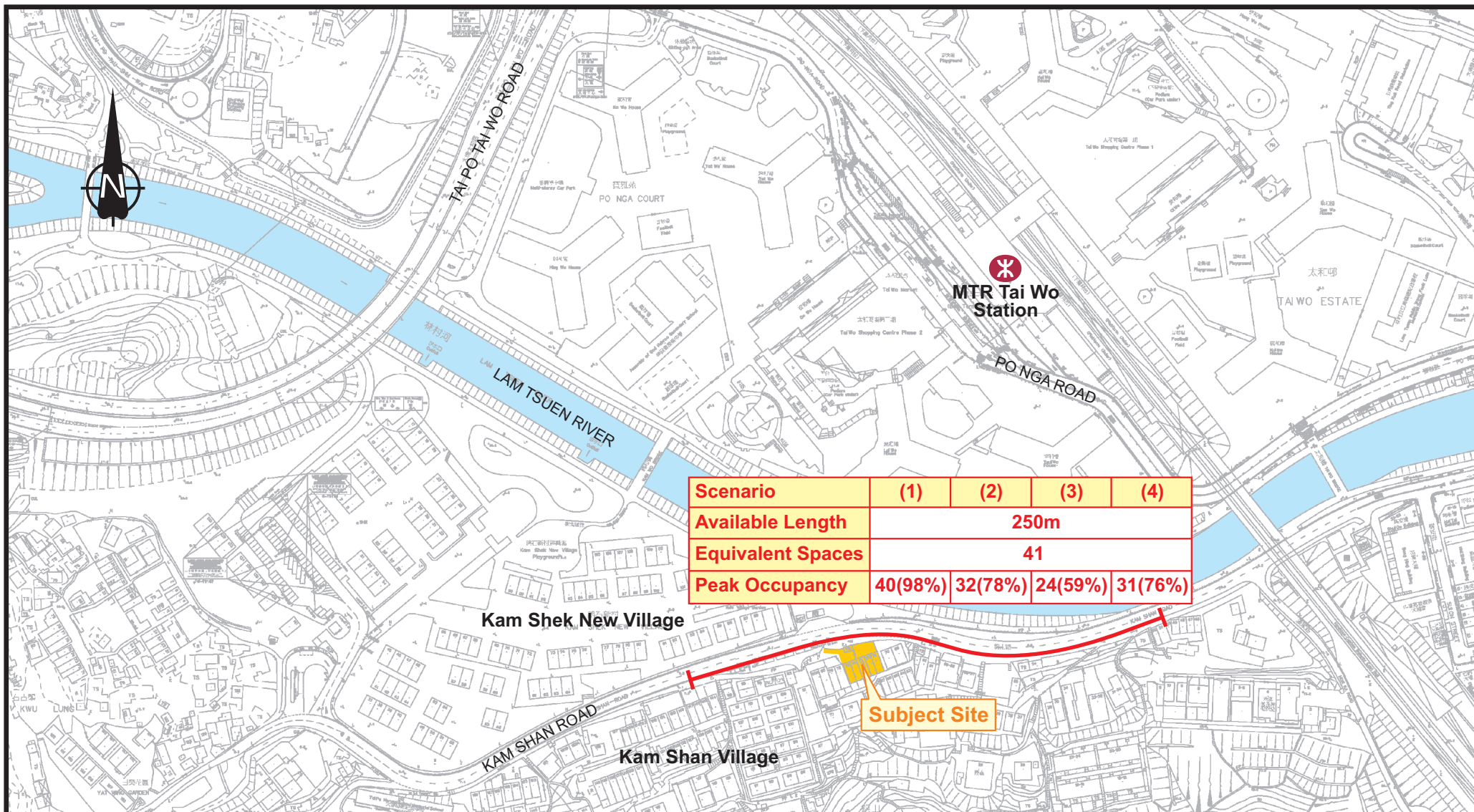
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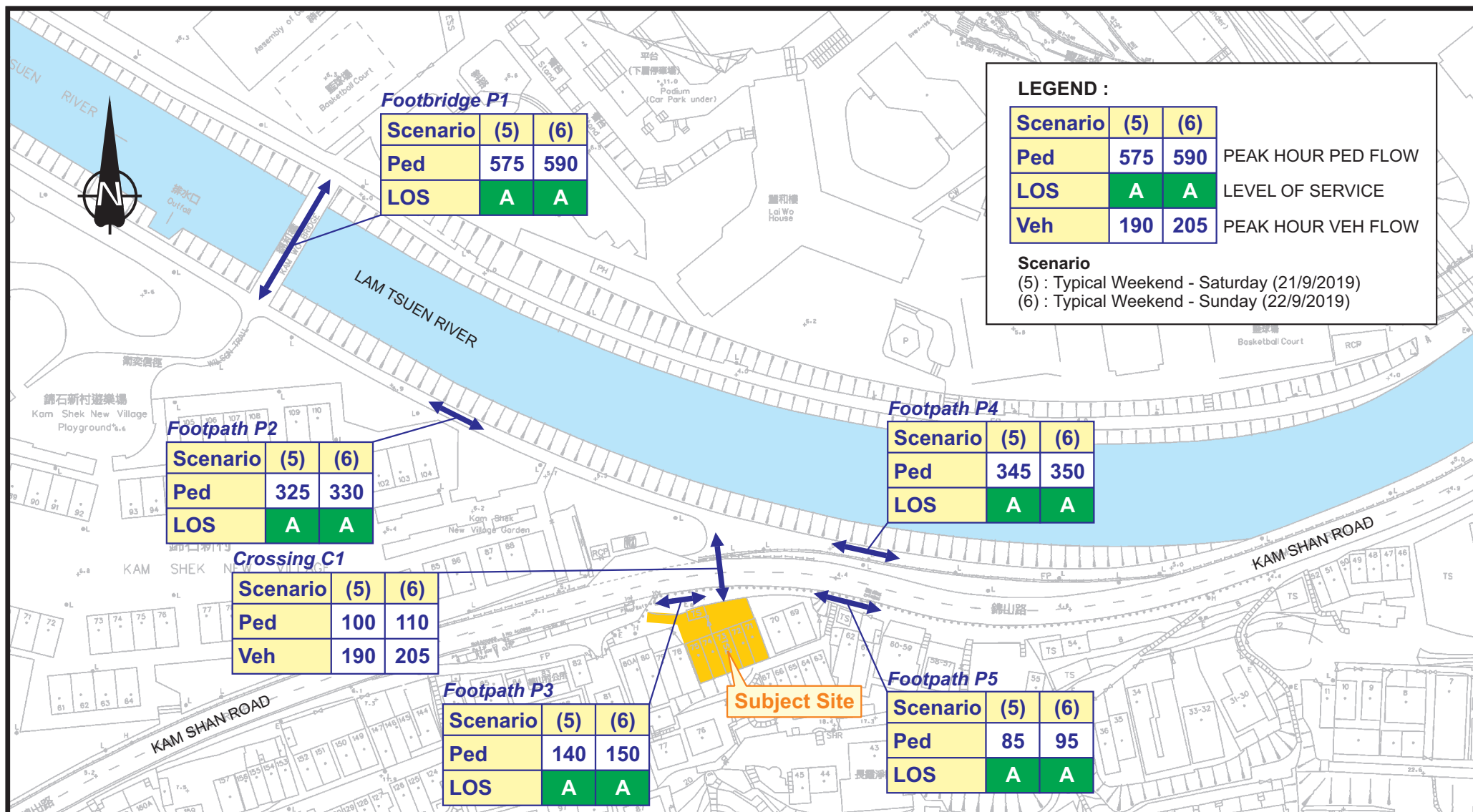




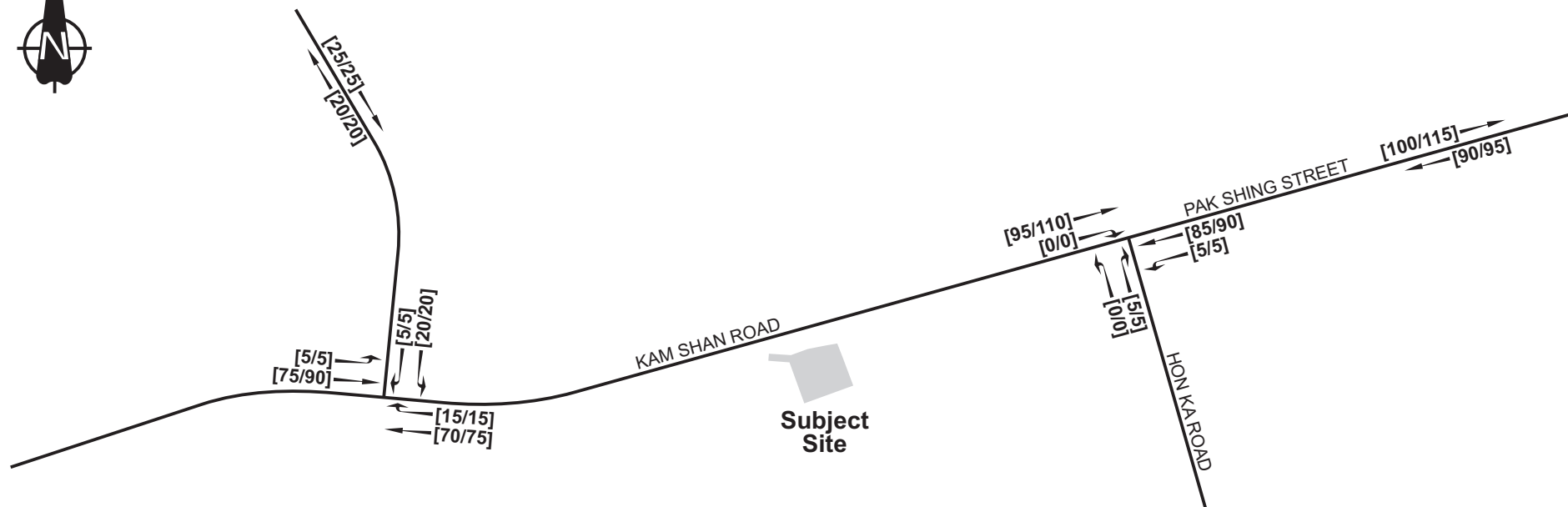
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Available Length	250m			
Equivalent Spaces	41			
Peak Occupancy	40(98%)	32(78%)	24(59%)	31(76%)

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Description	-	-	-	Description	-	-	-	-	Description	-	-
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				Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021
								Date	APR 2021	Drawing No.	2.7
								Rev.	-		

**SYSTRA**  
MVA




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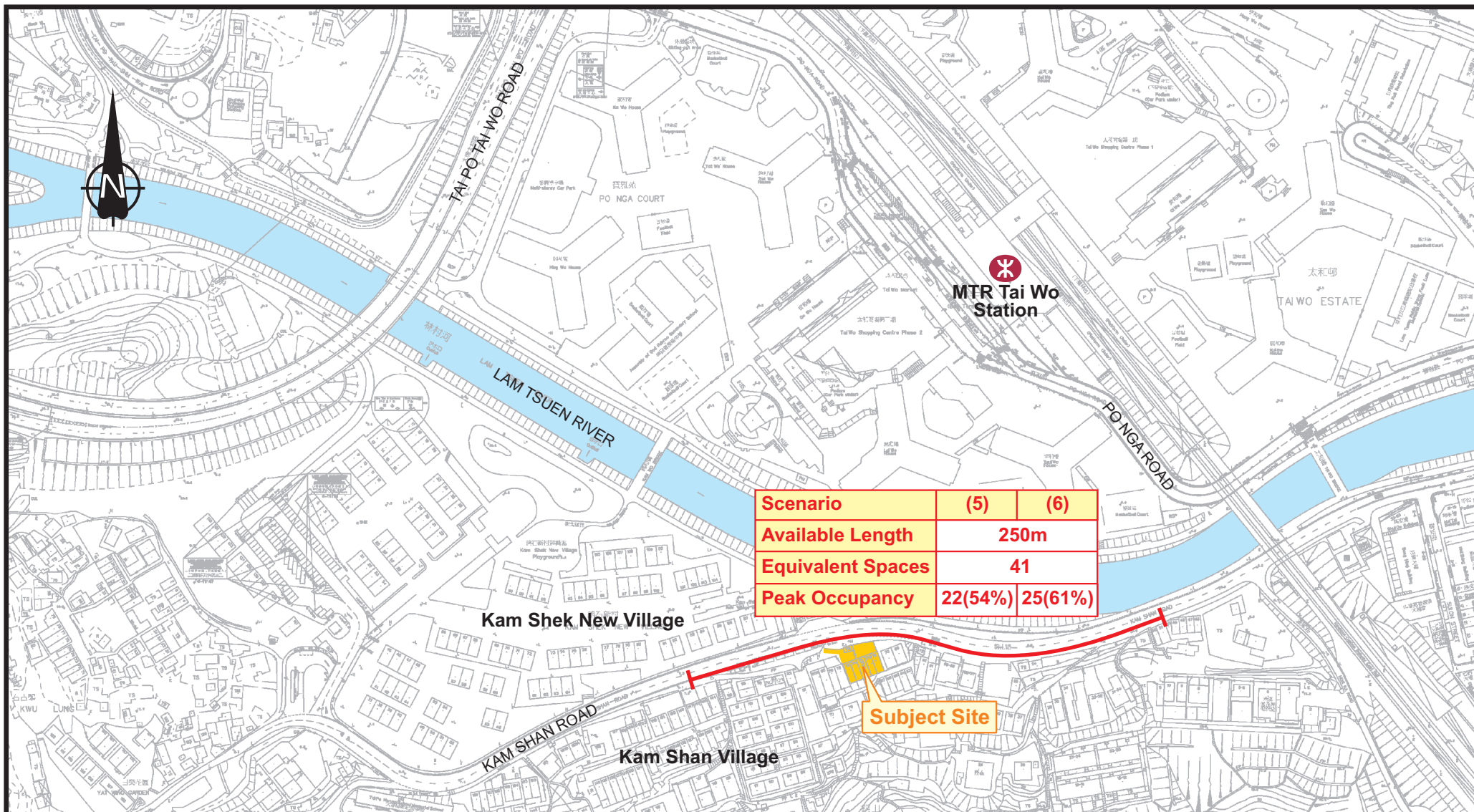
**Subject Site**

**LEGEND :**


[85/90] TYPICAL WEEKEND [SAT/SUN]  
PEAK HOUR TRAFFIC FLOW (PCU/HR)

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Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	
Project Title				Drawing Title								
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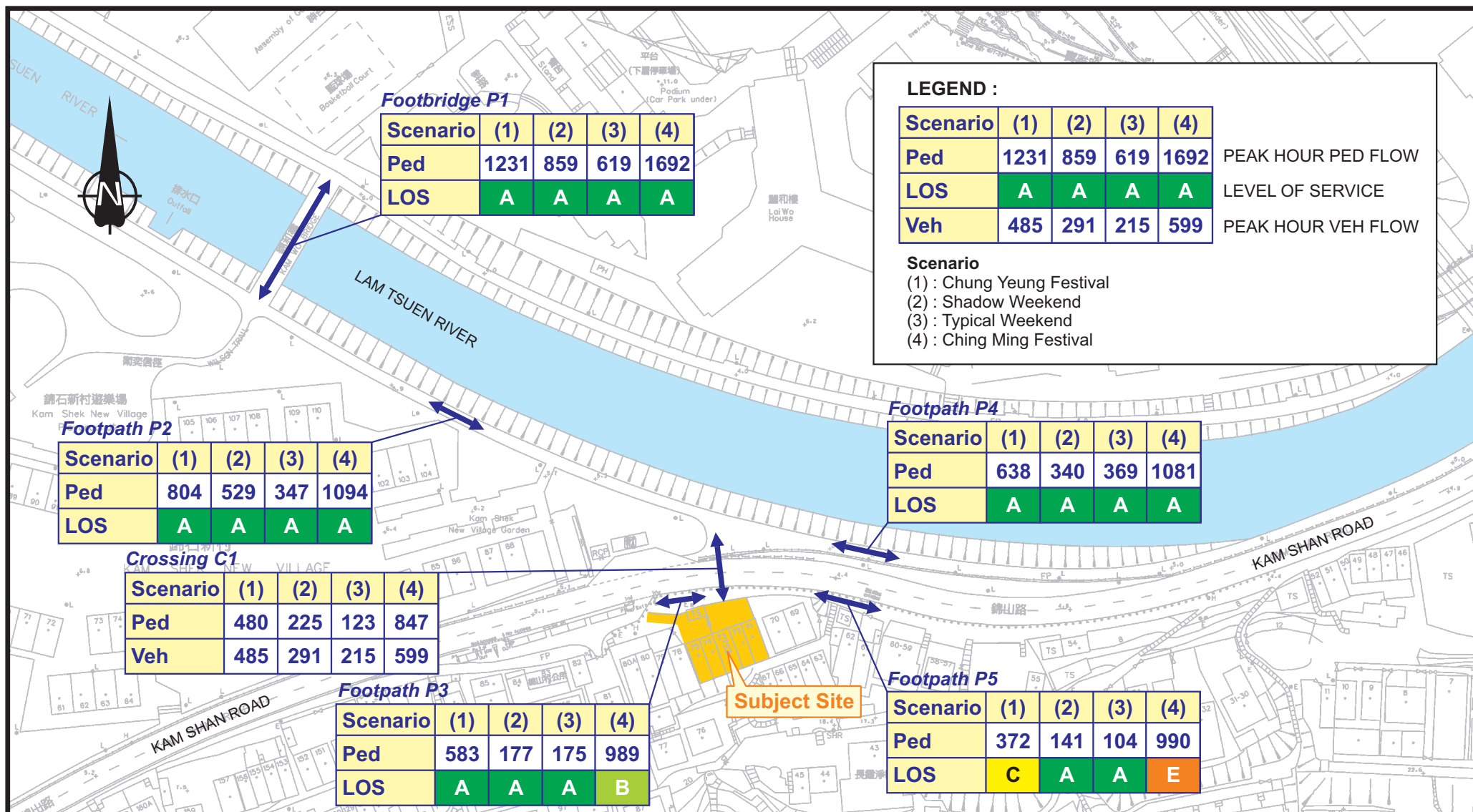
Scenario	(5)	(6)
Available Length	250m	
Equivalent Spaces	41	
Peak Occupancy	22(54%)	25(61%)

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Project Title				Drawing Title							
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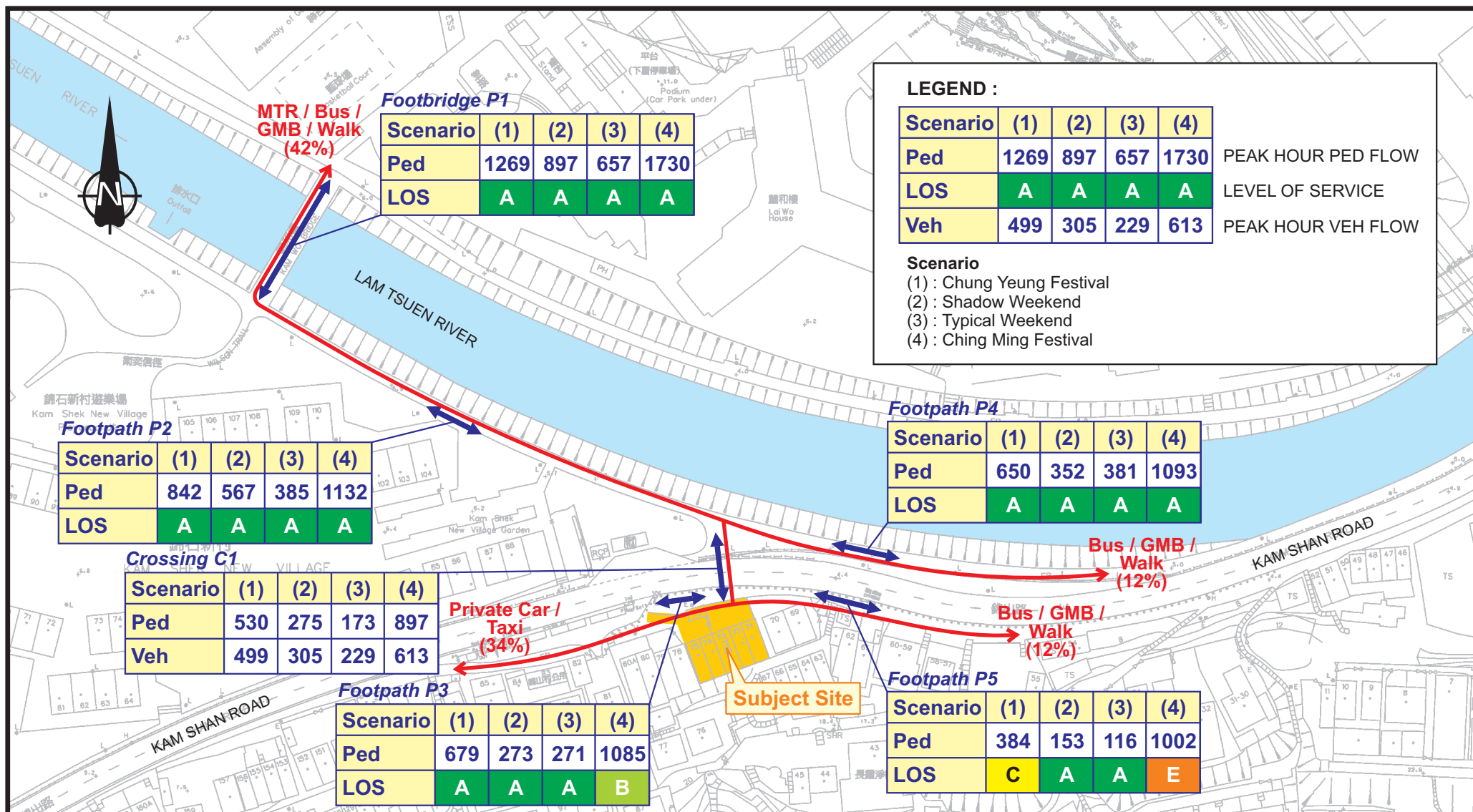




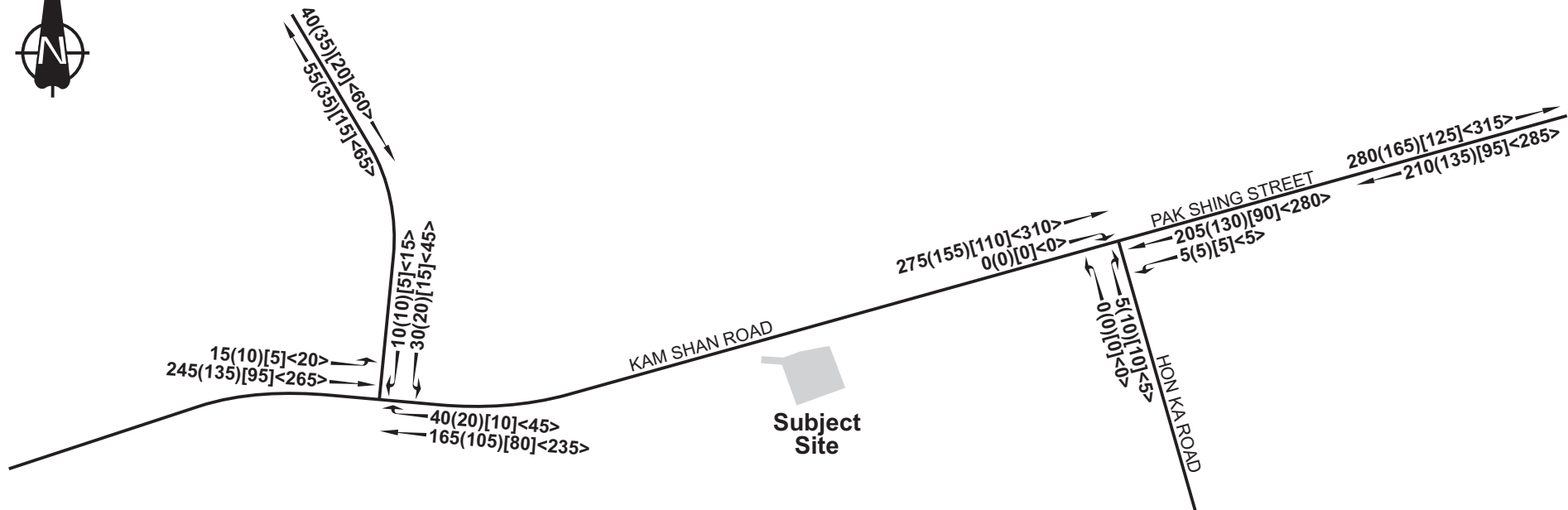




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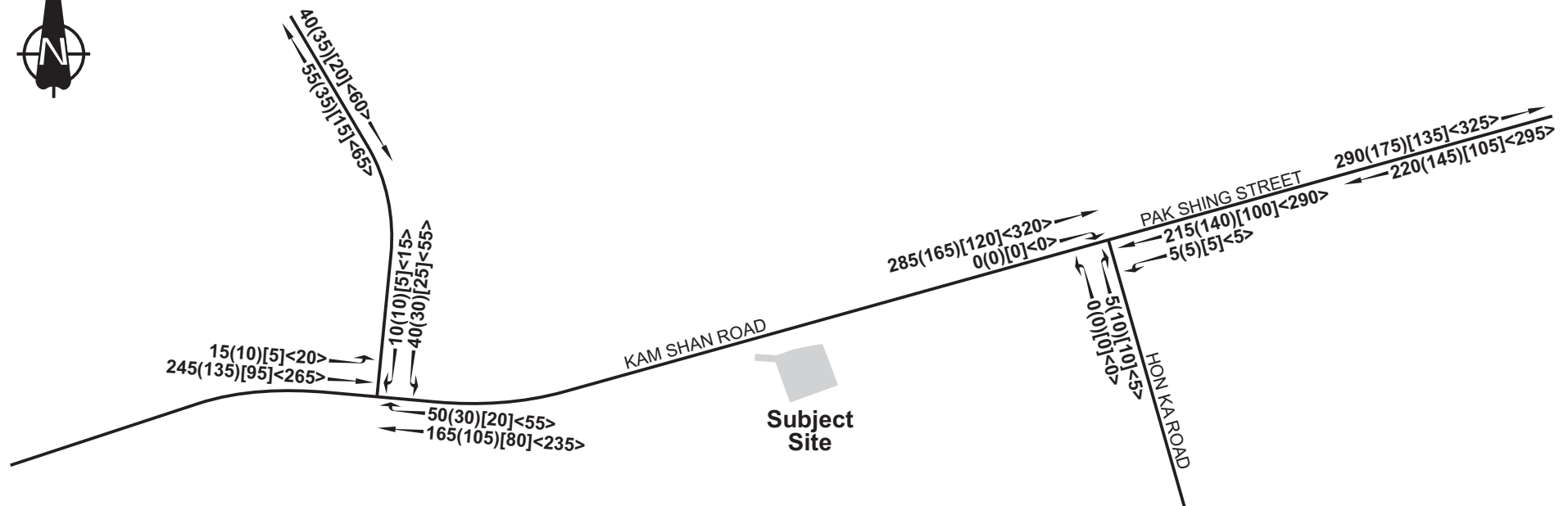
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#### LEGEND :


- 205 CHUNG YEUNG PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (130) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [90] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <280> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

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Project Title				Drawing Title							
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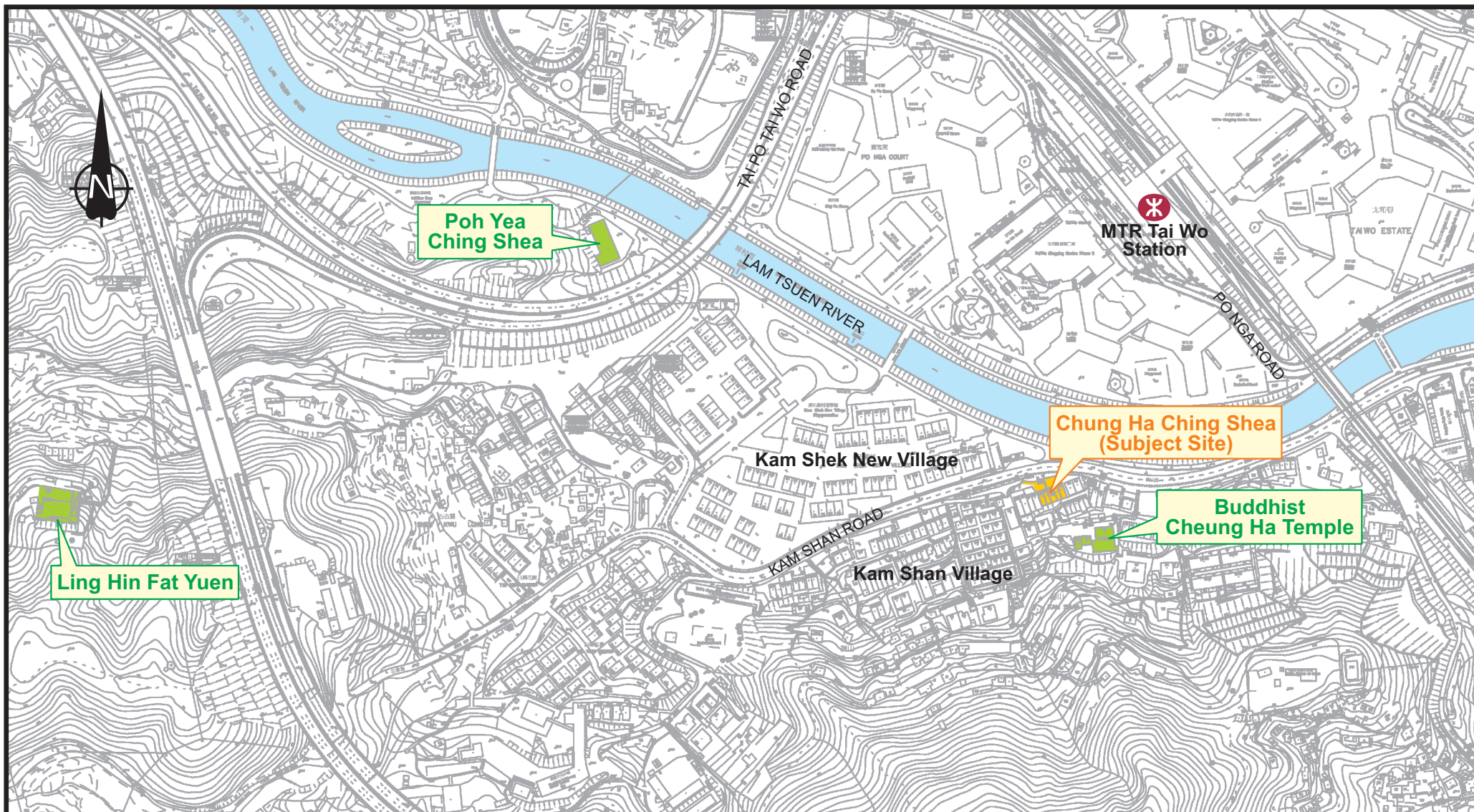



#### LEGEND :

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- (140) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [100] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <290> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

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Designed		TLY		Checked		LHW		Scale		NTS		Date		APR 2021		Drawing No.		4.4		Rev.		-	











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Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021	Drawing No.	4.5	Rev.	-

## APPENDIX A

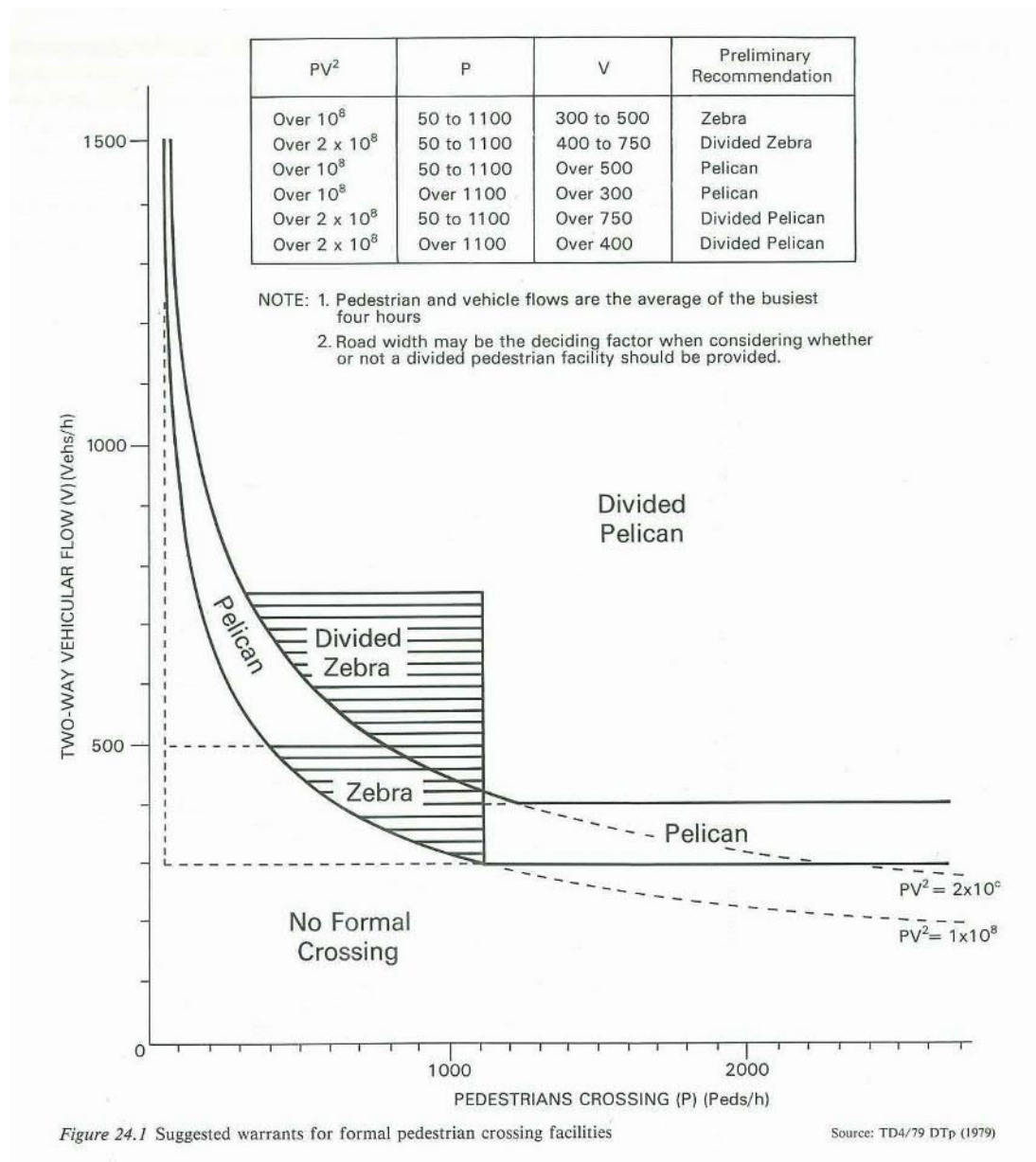
## APPENDIX A Definition of Level-of-Service (LOS) in Pedestrian Walkway Systems

LOS	Flow Rate (ped/min/m)	Description	Illustration
<b>A</b>	$\leq 16$	Pedestrians move in desired paths without altering their movements in response to other pedestrians. Walking speeds are freely selected, and conflicts between pedestrians are unlikely.	
<b>B</b>	16 – 23	Sufficient space is provided for pedestrians to freely select their walking speeds, to bypass other pedestrians and to avoid crossing conflicts with others. At this level, pedestrians begin to be aware of other pedestrians and to respond to their presence in the selection of walking paths.	
<b>C</b>	23 – 33	Sufficient space is available to select normal walking speeds and to bypass other pedestrians primarily in unidirectional stream. Where reverse direction or crossing movement exist, minor conflicts will occur, and speed and volume will be somewhat lower.	
<b>D</b>	33 – 49	Freedom to select individual walking speeds and bypass other pedestrians is restricted. Where crossing or reverse-flow movements exist, the probability of conflicts is high and its avoidance requires changes of speeds and position. The LOS provides reasonable fluid flow; however considerable friction and interactions between pedestrians are likely to occur.	
<b>E</b>	49 – 75	Virtually, all pedestrians would have their normal walking speeds restricted. At the lower range of this LOS, forward movement is possible only by shuffling. Space is insufficient to pass over slower pedestrians. Cross-and reverse-movement are possible only with extreme difficulties. Design volumes approach the limit of walking capacity with resulting stoppages and interruptions to flow.	
<b>F</b>	$> 75$	Walking speeds are severely restricted. Forward progress is made only by shuffling. There are frequent and unavoidable conflicts with other pedestrians. Cross-and reverse-movements are virtually impossible. Flow is sporadic and unstable. Space is more characteristics of queued pedestrians than of moving pedestrian streams.	

## APPENDIX B



## APPENDIX B Suggested Warrants for Formal Pedestrian Crossing Facilities



(Reference: Roads and Traffic in Urban Areas, Chapter 24 Facilities for Pedestrians, Institution of Highways and Transportation, Department of Transport)



## APPENDIX C

# TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2019

Description: 2019 Supplementary - Typical Weekend (Saturday)

Designed By: CHA

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SAT							
					Left	Right		SAT		SAT		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St WB	↑	A	1	4.000						2015	2015	100	0.050	0.050	0	0.000	0.000		
Pak Shing St EB	↑	A	1	4.000						2015	2015	90	0.045		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*		
Notes:				Flow: (pcu/hr) <div><div>↑ N</div><div>→ 90</div><div>← 100</div></div>								Group		A,Bp	Group				
												y		0.050	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		807%	R.C. (%)				
Stage / Phase Diagrams																			
1. <div>A → ← A</div>				2. <div>Bp ↕</div>				3.				4.				5.			
I/G= 3 I/G= #N/A #N/A				I/G= 5 I/G= #N/A #N/A				15 I/G= #N/A				I/G= I/G= I/G=				I/G= I/G=			

Date: SEP, 2019 Junction: Pak Shing Street near Sui On Street (J1)

# TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2019

Description: 2019 Supplementary - Typical Weekend (Sunday)

Designed By: CHA

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SUN							
					Left	Right		SUN		SUN		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St WB	↑	A	1	4.000						2015	2015	115	0.057	0.057	0	0.000	0.000		
Pak Shing St EB	↑	A	1	4.000						2015	2015	95	0.047		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5		+	10	=	15			*			*		
Notes:				Flow: (pcu/hr) <div><div><div>→ 95</div><div>← 115</div></div><div>↑ N</div></div>								Group		A,Bp	Group				
												y		0.057	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		688%	R.C. (%)				
Stage / Phase Diagrams																			
1. <div><div>→</div><div>← A</div></div>				2. <div><div>↑</div><div>Bp</div><div>↓</div></div>				3.				4.				5.			
I/G= 3				I/G= 5		15		I/G=				I/G=			I/G=				
I/G= #N/A	#N/A			I/G= #N/A		#N/A		I/G=				I/G=			I/G=				

Date: SEP, 2019 Junction: Pak Shing Street near Sui On Street (J1)

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2017Description: 2017 Observed - Chung Yeung FestivalDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																								
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																			
Pak Shing St WB	↑	A	1	4.000						2015	2015	250	0.124	0.124	0	0.000	0.000																																			
Pak Shing St EB	↑	A	1	4.000						2015	2015	190	0.094		0	0.000																																				
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																			
Notes:				Flow: (pcu/hr)								<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A, Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.124</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>263%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>					Group		A, Bp	Group			y		0.124	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		263%	R.C. (%)		
Group		A, Bp	Group																																																	
y		0.124	y																																																	
L (sec)		22	L (sec)																																																	
C (sec)		44	C (sec)																																																	
y pract.		0.450	y pract.																																																	
R.C. (%)		263%	R.C. (%)																																																	
Stage / Phase Diagrams																																																				
1.				2.				3.				4.				5.																																				
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																				
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																				
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																								

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2017Description: 2017 Observed - Shadow WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																								
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																			
Pak Shing St WB	↑	A	1	4.000						2015	2015	150	0.074	0.074	0	0.000	0.000																																			
Pak Shing St EB	↑	A	1	4.000						2015	2015	120	0.060		0	0.000																																				
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																			
Notes:				Flow: (pcu/hr)								<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A, Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.074</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>505%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>					Group		A, Bp	Group			y		0.074	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		505%	R.C. (%)		
Group		A, Bp	Group																																																	
y		0.074	y																																																	
L (sec)		22	L (sec)																																																	
C (sec)		44	C (sec)																																																	
y pract.		0.450	y pract.																																																	
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Stage / Phase Diagrams																																																				
1.				2.				3.				4.				5.																																				
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																				
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																				
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																								



## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2017Description: 2017 Observed - Typical WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																									
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																				
Pak Shing St WB	↑	A	1	4.000						2015	2015	110	0.055	0.055	0	0.000	0.000																																				
Pak Shing St EB	↑	A	1	4.000						2015	2015	85	0.042		0	0.000																																					
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15				*		*																																				
Notes:				Flow: (pcu/hr)										<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A, Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.055</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>724%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>				Group		A, Bp	Group			y		0.055	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		724%	R.C. (%)		
Group		A, Bp	Group																																																		
y		0.055	y																																																		
L (sec)		22	L (sec)																																																		
C (sec)		44	C (sec)																																																		
y pract.		0.450	y pract.																																																		
R.C. (%)		724%	R.C. (%)																																																		
Stage / Phase Diagrams																																																					
1.				2.				3.				4.				5.																																					
I/G= 3		I/G= 5		I/G=		I/G=		I/G=		I/G=		I/G=		I/G=		I/G=																																					
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=		I/G=		I/G=		I/G=		I/G=																																					
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																									

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2018Description: 2018 Observed - Ching Ming FestivalDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																									
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																				
Pak Shing St WB	↑	A	1	4.000						2015	2015	285	0.141	0.141	0	0.000	0.000																																				
Pak Shing St EB	↑	A	1	4.000						2015	2015	260	0.129		0	0.000																																					
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																				
Notes:				Flow: (pcu/hr)										<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A, Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.141</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>218%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>				Group		A, Bp	Group			y		0.141	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		218%	R.C. (%)		
Group		A, Bp	Group																																																		
y		0.141	y																																																		
L (sec)		22	L (sec)																																																		
C (sec)		44	C (sec)																																																		
y pract.		0.450	y pract.																																																		
R.C. (%)		218%	R.C. (%)																																																		
Stage / Phase Diagrams																																																					
1.				2.				3.				4.				5.																																					
I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=		I/G=		I/G=		I/G=		I/G=																																					
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=		I/G=		I/G=		I/G=		I/G=																																					
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																									

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED




Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Chung Yeung Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak							
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St WB	↑	A	1	4.000						2015	2015	280	0.139	0.139	0	0.000	0.000		
Pak Shing St EB	↑	A	1	4.000						2015	2015	210	0.104		0	0.000			
Pedestrian Crossing	Bp	2	MIN GREEN + FLASH =	5	+	10	=	15						*			*		
Notes:				Flow: (pcu/hr)								Group			A,Bp	Group			
												y			0.139	y			
												L (sec)			22	L (sec)			
												C (sec)			44	C (sec)			
												y pract.			0.450	y pract.			
												R.C. (%)			224%	R.C. (%)			
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
																			
I/G= 3				I/G= 5				I/G=				I/G=				I/G=			
I/G= #N/A				I/G= #N/A				I/G=				I/G=				I/G=			
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street							

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Reference - Shadow WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																							
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																		
Pak Shing St WB	↑	A	1	4.000						2015	2015	165	0.082	0.082	0	0.000	0.000																																		
Pak Shing St EB	↑	A	1	4.000						2015	2015	135	0.067		0	0.000																																			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =	5	+	10	=	15					*			*																																		
Notes:		Flow: (pcu/hr)												<table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A,Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.082</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>450%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table>		Group		A,Bp	Group			y		0.082	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		450%	R.C. (%)		
Group		A,Bp	Group																																																
y		0.082	y																																																
L (sec)		22	L (sec)																																																
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1.				2.				3.				4.				5.																																			
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																			
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																			
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																							

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Reference - Typical WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																							
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																		
Pak Shing St WB	↑	A	1	4.000						2015	2015	125	0.062	0.062	0	0.000	0.000																																		
Pak Shing St EB	↑	A	1	4.000						2015	2015	95	0.047		0	0.000																																			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																		
Notes:		Flow: (pcu/hr)												<table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A,Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.062</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>625%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table>		Group		A,Bp	Group			y		0.062	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		625%	R.C. (%)		
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y		0.062	y																																																
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1.				2.				3.				4.				5.																																			
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																			
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																			
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																							



## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Reference - Ching Ming FestivalDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																									
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																				
Pak Shing St WB	↑	A	1	4.000						2015	2015	315	0.156	0.156	0	0.000	0.000																																				
Pak Shing St EB	↑	A	1	4.000						2015	2015	285	0.141		0	0.000																																					
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																				
Notes:		Flow: (pcu/hr)										<div style="text-align: center;"> </div>			<table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A,Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.156</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>188%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table>			Group		A,Bp	Group			y		0.156	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		188%	R.C. (%)		
Group		A,Bp	Group																																																		
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1.				2.				3.				4.				5.																																					
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																					
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																					
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																									

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Design - Chung Yeung FestivalDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																									
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																				
Pak Shing St WB	↑	A	1	4.000						2015	2015	290	0.144	0.144	0	0.000	0.000																																				
Pak Shing St EB	↑	A	1	4.000						2015	2015	220	0.109		0	0.000																																					
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15				*		*																																				
Notes:		Flow: (pcu/hr)										<div style="text-align: center;"> </div>			<table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A,Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.144</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>213%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table>			Group		A,Bp	Group			y		0.144	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		213%	R.C. (%)		
Group		A,Bp	Group																																																		
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Stage / Phase Diagrams																																																					
1.				2.				3.				4.				5.																																					
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																					
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Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																									

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Design - Shadow WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																								
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																			
Pak Shing St WB	↑	A	1	4.000						2015	2015	175	0.087	0.087	0	0.000	0.000																																			
Pak Shing St EB	↑	A	1	4.000						2015	2015	145	0.072		0	0.000																																				
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																			
Notes:				Flow: (pcu/hr)								<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A, Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.087</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>418%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>					Group		A, Bp	Group			y		0.087	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		418%	R.C. (%)		
Group		A, Bp	Group																																																	
y		0.087	y																																																	
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Stage / Phase Diagrams																																																				
1.				2.				3.				4.				5.																																				
I/G= 3				I/G= 5		15		I/G=				I/G=				I/G=																																				
I/G= #N/A		#N/A		I/G= #N/A		#N/A		I/G=				I/G=				I/G=																																				
Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																								

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Design - Typical WeekendDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																									
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																				
Pak Shing St WB	↑	A	1	4.000						2015	2015	135	0.067	0.067	0	0.000	0.000																																				
Pak Shing St EB	↑	A	1	4.000						2015	2015	105	0.052		0	0.000																																					
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																				
Notes:				Flow: (pcu/hr)										<div style="display: flex; align-items: center;"> <div style="margin-right: 20px;"> </div> <table border="1"> <thead> <tr> <th>Group</th> <th></th> <th>A,Bp</th> <th>Group</th> <th></th> <th></th> </tr> </thead> <tbody> <tr> <td>y</td> <td></td> <td>0.067</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>572%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </tbody> </table> </div>				Group		A,Bp	Group			y		0.067	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		572%	R.C. (%)		
Group		A,Bp	Group																																																		
y		0.067	y																																																		
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Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																									

## TRAFFIC SIGNALS CALCULATION

Job No.: CHK50299010

MVA HONG KONG LIMITED

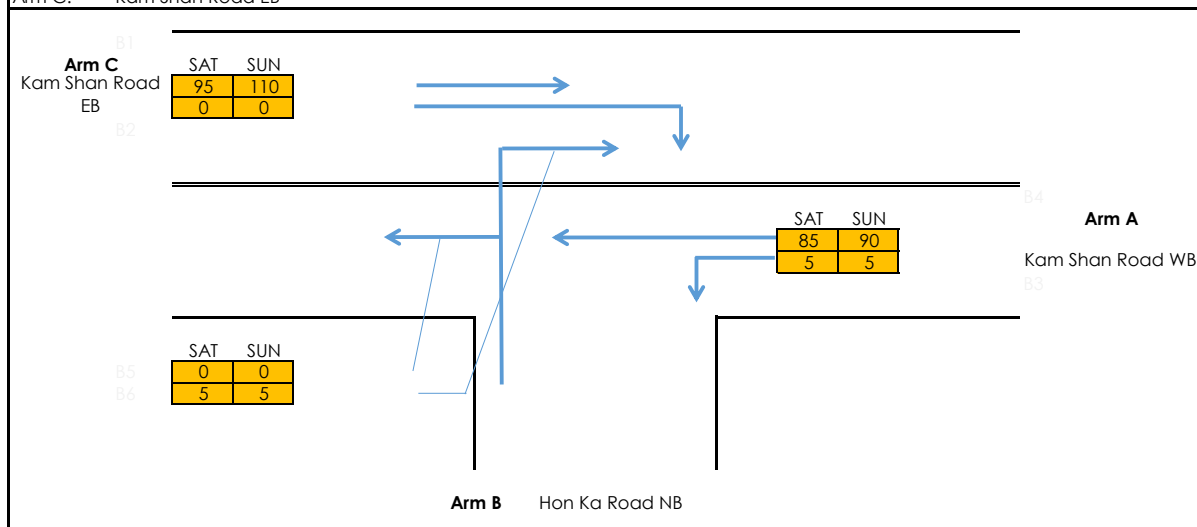
Junction: Pak Shing Street near Sui On StreetDesign Year: 2028Description: 2028 Design - Ching Ming FestivalDesigned By: SCWChecked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak																																							
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y																																		
Pak Shing St WB	↑	A	1	4.000						2015	2015	325	0.161	0.161	0	0.000	0.000																																		
Pak Shing St EB	↑	A	1	4.000						2015	2015	295	0.146		0	0.000																																			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*																																		
Notes:		Flow: (pcu/hr)												<table border="1"> <tr> <td>Group</td> <td></td> <td>A,Bp</td> <td>Group</td> <td></td> <td></td> </tr> <tr> <td>y</td> <td></td> <td>0.161</td> <td>y</td> <td></td> <td></td> </tr> <tr> <td>L (sec)</td> <td></td> <td>22</td> <td>L (sec)</td> <td></td> <td></td> </tr> <tr> <td>C (sec)</td> <td></td> <td>44</td> <td>C (sec)</td> <td></td> <td></td> </tr> <tr> <td>y pract.</td> <td></td> <td>0.450</td> <td>y pract.</td> <td></td> <td></td> </tr> <tr> <td>R.C. (%)</td> <td></td> <td>179%</td> <td>R.C. (%)</td> <td></td> <td></td> </tr> </table>		Group		A,Bp	Group			y		0.161	y			L (sec)		22	L (sec)			C (sec)		44	C (sec)			y pract.		0.450	y pract.			R.C. (%)		179%	R.C. (%)		
Group		A,Bp	Group																																																
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Date: FEB, 2019												Junction: Pak Shing Street near Sui On Street																																							



## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J2) Kam Shan Road/ Hon Ka Road	Checked by:	
Scheme:	Supplementary Traffic Surveys	Date:	Sep-19
Design Year: 2019 (SAT, SUN)	Job No.: 50299010		
Arm A:	Kam Shan Road WB		
Arm B:	Hon Ka Road NB		
Arm C:	Kam Shan Road EB		



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843
	VI(b-a)	50		E	0.897
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.748

### ANALYSIS

		SAT	SUN
TRAFFIC FLOWS (pcu/hr)	q(c-a)	95	110
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	85	90
	q(b-a)	5	5
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	495.1	491.8
	Q(c-a)	1800	1800
	Q(c-b)	675.1	673.8
RFC's	c-a	0.05	0.06
	c-b	0.00	0.00
	b-ac	0.01	0.01
<b>RFC</b>		<b>0.05</b>	<b>0.06</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1+0.094(w(b-a)-3.65))(1+0.0009(Vr(b-a)-120))(1+0.0006(VI(b-a)-150))$$

$$E = (1+0.094(w(b-c)-3.65))(1+0.0009(Vr(b-c)-120))$$

$$F = (1+0.094(w(c-b)-3.65))(1+0.0009(Vr(c-b)-120))$$

$$Y = 1-0.0345W$$

f = proportion of minor traffic turning left

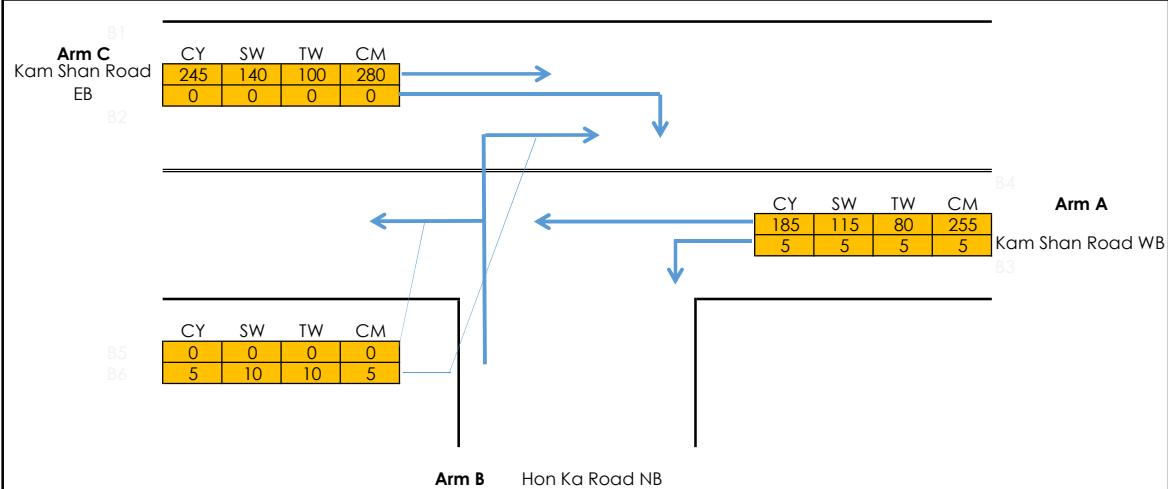
$$Q(b-ac) = Q(b-c)*Q(b-a)/(1-f)*Q(b-c)+f*Q(b-a)$$

Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road	Designed by:
Junction: (J2) Kam Shan Road/ Hon Ka Road	Checked by:
Scheme: Existing	Date: Feb-19
Design Year: 2017 (CY, SW, CM); 2018 (CM)	Job No.: 50299010
Arm A: Kam Shan Road WB	
Arm B: Hon Ka Road NB	
Arm C: Kam Shan Road EB	



## GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843
	VI(b-a)	50		E	0.897
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.748

## ANALYSIS

		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	245	140	100	280
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	185	115	80	255
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	450.4	481.7	495.5	429.3
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	649.6	667.4	676.4	631.7
RFC's	c-a	0.14	0.08	0.06	0.16
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.14</b>	<b>0.08</b>	<b>0.06</b>	<b>0.16</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

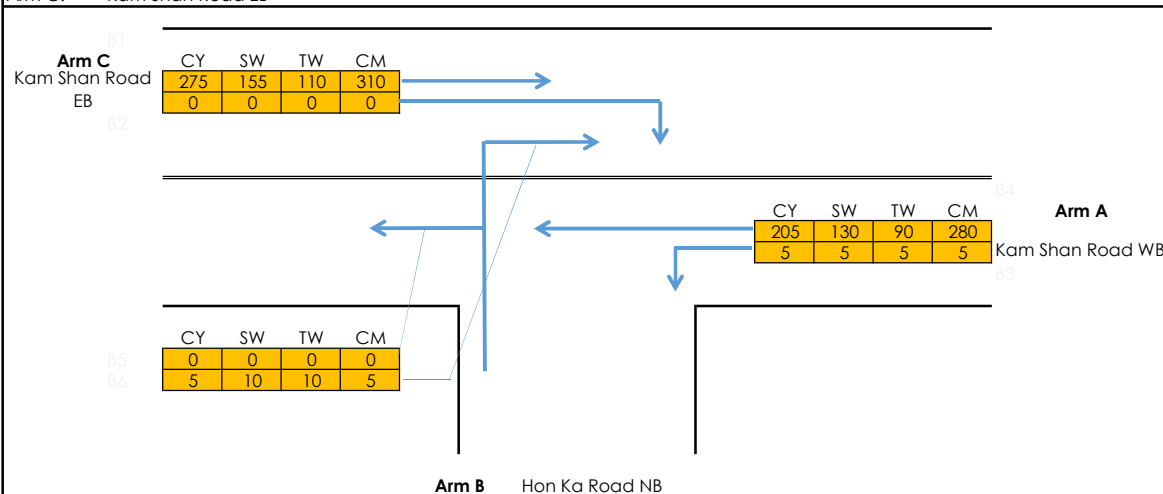
f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a \text{ Capacity of combined streams})$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road	Designed by:
Junction: (J2) Kam Shan Road/ Hon Ka Road	Checked by:
Scheme: Reference	Date: Feb-19
Design Year: 2028	Job No.: 50299010
Arm A: Kam Shan Road WB	
Arm B: Hon Ka Road NB	
Arm C: Kam Shan Road EB	



## GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843
	VI(b-a)	50		E	0.897
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.748

## ANALYSIS

		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	275	155	110	310
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	205	130	90	280
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	441.5	476.1	491.8	419.2
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	644.5	663.6	673.8	625.3
RFC's	c-a	0.15	0.09	0.06	0.17
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.15</b>	<b>0.09</b>	<b>0.06</b>	<b>0.17</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

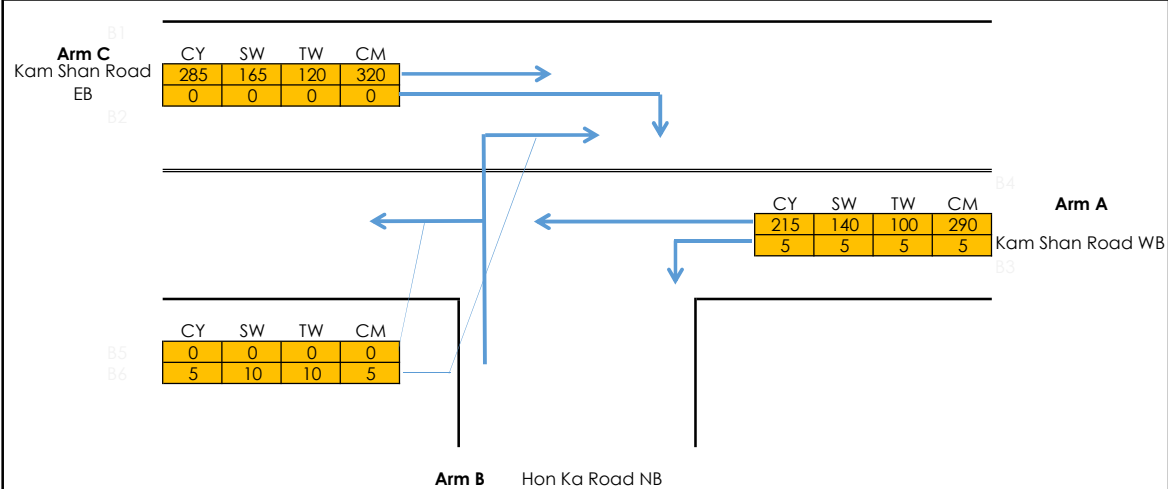
f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1-f) * Q(b-c) + f * Q(b-a \text{ Capacity of combined streams})$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road	Designed by:
Junction: (J2) Kam Shan Road/ Hon Ka Road	Checked by:
Scheme: Design	Date: Feb-19
Design Year: 2028	Job No.: 50299010
Arm A: Kam Shan Road WB	
Arm B: Hon Ka Road NB	
Arm C: Kam Shan Road EB	



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843
	VI(b-a)	50		E	0.897
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.748

### ANALYSIS

		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	285	165	120	320
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	215	140	100	290
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	437.8	472.3	488	415.5
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	641.9	661.1	671.3	622.8
RFC's	c-a	0.16	0.09	0.07	0.18
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.16</b>	<b>0.09</b>	<b>0.07</b>	<b>0.18</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

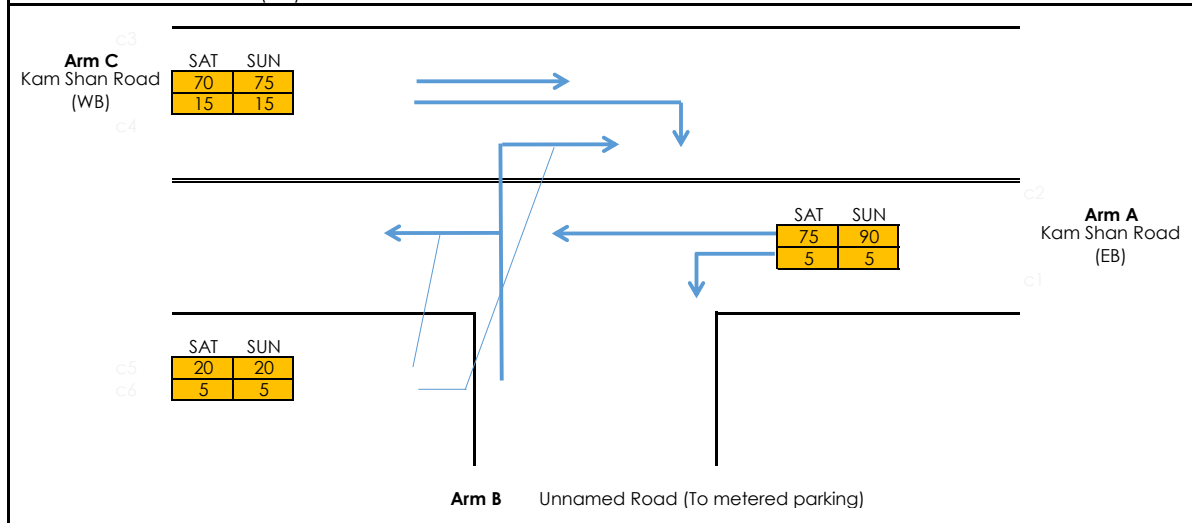
f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1-f) * Q(b-c) + f * Q(b-a \text{ Capacity of combined streams})$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Supplementary Traffic Surveys	Date:	Sep-19
Design Year: 2019 (SAT, SUN)	Job No.: 50299010		
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



GEOMETRY					
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865
	VI(b-a)	30		E	0.932
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.759
ANALYSIS					
			SAT	SUN	
TRAFFIC FLOWS (pcu/hr)	q(c-a)		70	75	
	q(c-b)		15	15	
	q(a-b)		5	5	
	q(a-c)		75	90	
	q(b-a)		5	5	
	q(b-c)		20	20	
	f		0.80	0.80	
CAPACITIES (pcu/hr)	Q(b-ac)		633.4	629.3	
	Q(c-a)		1761	1761	
	Q(c-b)		693.3	689.3	
RFC's	c-a		0.04	0.04	
	c-b		0.02	0.02	
	b-ac		0.04	0.04	
RFC			0.04	0.04	

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1+0.094(w(b-a)-3.65))(1+0.0009(Vr(b-a)-120))(1+0.0006(VI(b-a)-150))$$

$$E = (1+0.094(w(b-c)-3.65))(1+0.0009(Vr(b-c)-120))$$

$$F = (1+0.094(w(c-b)-3.65))(1+0.0009(Vr(c-b)-120))$$

$$Y = 1-0.0345W$$

f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c)*Q(b-a)/(1-f)*Q(b-c)+f*Q(b-a)$$

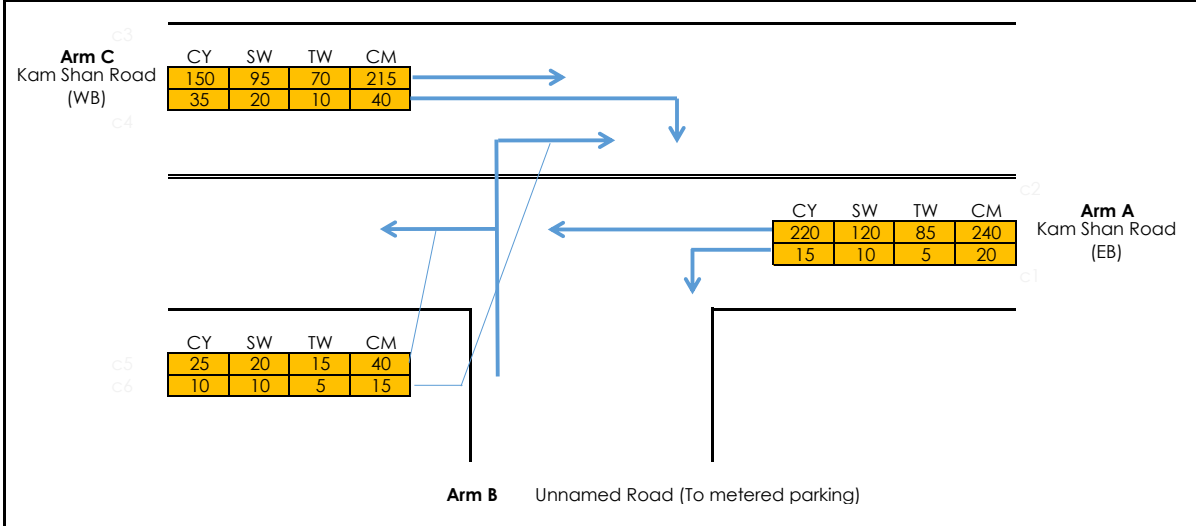
Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1



# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Existing	Date:	May-19
Design Year:	2017 (CY, SW, CM); 2018 (CM)	Job No.:	50299010
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



## GEOMETRY

Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865
	VI(b-a)	30		E	0.932
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.759

## ANALYSIS

		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	150	95	70	215
	q(c-b)	35	20	10	40
	q(a-b)	15	10	5	20
	q(a-c)	220	120	85	240
	q(b-a)	10	10	5	15
	q(b-c)	25	20	15	40
	f	0.71	0.67	0.75	0.73
CAPACITIES (pcu/hr)	Q(b-ac)	570.9	593.9	621.9	562.9
	Q(c-a)	1703	1747	1774	1688
	Q(c-b)	652.2	680	690.6	645.6
RFC's	c-a	0.09	0.05	0.04	0.13
	c-b	0.05	0.03	0.01	0.06
	b-ac	0.06	0.05	0.03	0.10
RFC		0.09	0.05	0.04	0.13

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

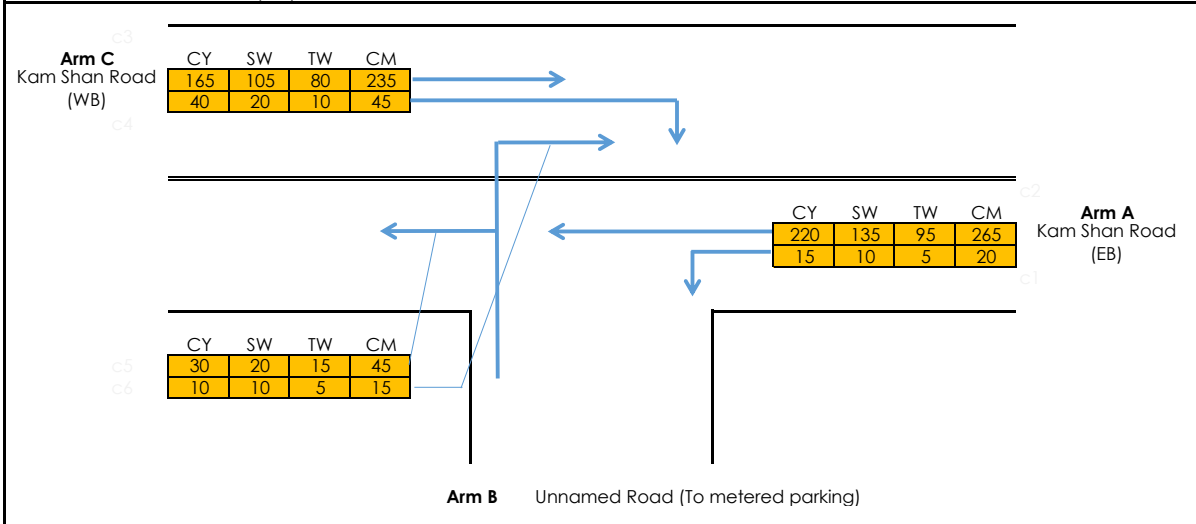
f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1-f) * Q(b-c) + f * Q(b-a) \text{ Capacity of combined streams}$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Reference	Date:	May-19
Design Year:	2028	Job No.:	50299010
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



GEOMETRY							
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70		
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70		
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90		
Combined stream on minor arm	Y/N?	Y					
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865		
	VI(b-a)	30		E	0.932		
	Vr(b-c)	40		F	0.959		
	Vr(c-b)	50		Y	0.759		
ANALYSIS				CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	165	105	80	235		
	q(c-b)	40	20	10	45		
	q(a-b)	15	10	5	20		
	q(a-c)	220	135	95	265		
	q(b-a)	10	10	5	15		
	q(b-c)	30	20	15	45		
	f	0.75	0.67	0.75	0.75		
CAPACITIES (pcu/hr)	Q(b-ac)	576.7	589.4	618.8	559.5		
	Q(c-a)	1690	1747	1774	1673		
	Q(c-b)	652.2	676.1	688	639		
RFC's	c-a	0.10	0.06	0.05	0.14		
	c-b	0.06	0.03	0.01	0.07		
	b-ac	0.07	0.05	0.03	0.11		
RFC		0.10	0.06	0.05	0.14		

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

f = proportion of minor traffic turning left

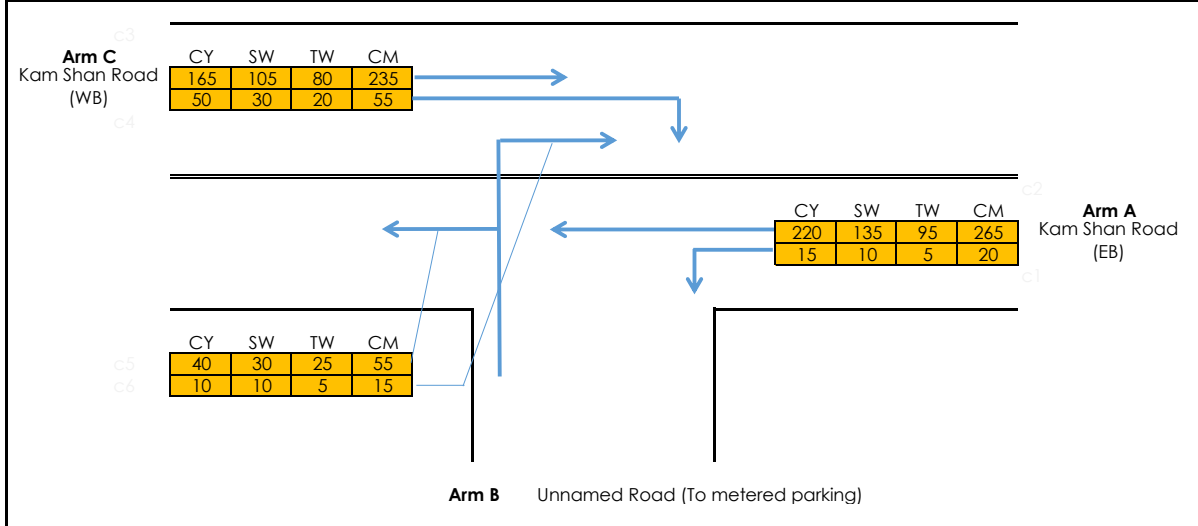
$$Q(b-ac) = Q(b-c) * Q(b-a) / (1-f) * Q(b-c) + f * Q(b-a)$$

Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Design	Date:	May-19
Design Year:	2028	Job No.:	50299010
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



GEOMETRY							
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70		
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70		
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90		
Combined stream on minor arm	Y/N?	Y					
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865		
	VI(b-a)	30		E	0.932		
	Vr(b-c)	40		F	0.959		
	Vr(c-b)	50		Y	0.759		
ANALYSIS				CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	165	105	80	235		
	q(c-b)	50	30	20	55		
	q(a-b)	15	10	5	20		
	q(a-c)	220	135	95	265		
	q(b-a)	10	10	5	15		
	q(b-c)	40	30	25	55		
	f	0.80	0.75	0.83	0.79		
CAPACITIES (pcu/hr)	Q(b-ac)	586.6	604	633.9	566.6		
	Q(c-a)	1662	1720	1748	1645		
	Q(c-b)	652.2	676.1	688	639		
RFC's	c-a	0.10	0.06	0.05	0.14		
	c-b	0.08	0.04	0.03	0.09		
	b-ac	0.09	0.07	0.05	0.12		
RFC		0.10	0.07	0.05	0.14		

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1-f) * Q(b-c) + f * Q(b-a)$$

Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## APPENDIX D

## Appendix D Sensitivity Test – Full Intakes of Unoccupied Niches in the Vicinity

### Other Private Columbarium Developments in the Vicinity

1. In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road as shown in **Table D1** below:

**Table D1 Private Columbarium Developments in Vicinity**

Private Columbarium Developments	Latest Planning Application No.	Sold before Cut-off and Approved to be Interred		Sold and Occupied		Occupancy Rate	
		Niches	Tablets	Niches	Tablets	Niches	Tablets
Cheung Ha Ching Shea (The Subject Site)	Y/TP/32	162		66		41%	
Poh Yea Ching Shea	A/TP/657	5,302		1,035		20%	
Buddhist Cheung Ha Temple	Y/TP/34	5,508	1,877	3,816	1,877	69%	100%
Ling Hin Fat Yuen	A/TP/652	757	1,782	174	877	23%	49%
Sub-total:		11,729	3,659	5,091	2,754	44%	75%
Total:		15,388		7,845		51%	

Source: Statutory Planning Portal 2, Town Planning Board (<https://www2.ozp.tpb.gov.hk/>)

2. To account for the full-intakes of the “sold niches before cut-off” and the “approved niches to be interred” as the worst-case scenario, a sensitivity test is included to examine the corresponding pedestrian links and road junctions.

### Operational Performance for Pedestrian Links and Road Junctions

3. With the proposed closure of operation during Ching Ming and Chung Yeung Festivals, and their shadow weekends (i.e. any Saturdays, Sundays and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days), the critical traffic conditions during peak seasons at Kam Shan Road will be unlikely correlated to the Subject Site. The proposed columbarium will also be closed from the 1st to 14th day of the Lunar New Year.
4. Hence, the operational performance of the pedestrian links and road junctions are examined for typical weekends only (including Saturday and Sunday during non-peak days). Given that the occupancy rate is about 51% among the sold/approved niches and tablets, a multiplying factor of 2.0 is assumed for the full intake of sold/approved niches. It is also assumed that the visitors are NOT controlled by any management measures as the worst-case scenario.
5. Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons will be present on the Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled at 48 visitors per hour.
6. As shown in **Tables D2 and D3**, all of the pedestrian links and road junctions in the vicinity will be operated with an acceptable level despite the full intakes of sold/approved niches and the proposed columbarium development at subject site during typical weekends. The existing cautionary crossing will be sufficient and no formal crossing is required as shown in **Table D4**.
7. In view of the assessment results of the sensitivity tests, the traffic conditions during typical weekends (including Saturday and Sunday) will be operated at acceptable level, despite the full-intakes of sold/approved niches and the completion of the proposed columbarium development at the Subject Site. Therefore, the cumulative traffic impact can be justified through the above sensitivity test.
8. Nevertheless, under the statutory applications through Town Planning Board (“TPB”) and the Private Columbaria Licensing Board (“PCLB”), appropriate traffic management plans and mitigation measures would be available for the existing and future private columbarium developments. The overall traffic conditions in the area are expected to be clearly monitored with proper mitigation measures when undesirable traffic conditions have been observed.



**Table D2 Year 2028 Performance of Pedestrian Links (with Full Intakes of Sold/Approved Niches)**

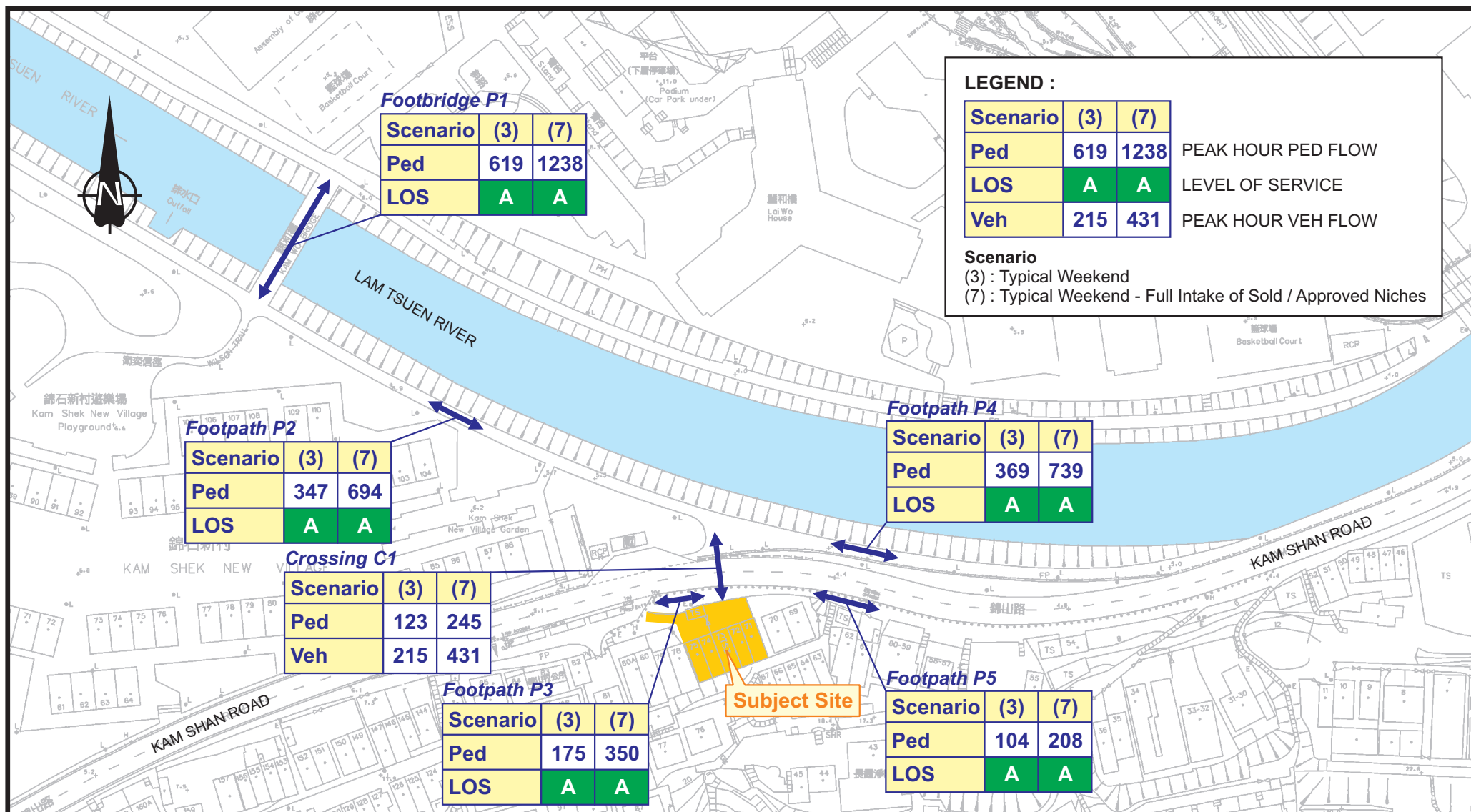
Table B2 - Year 2028 Performance of Pedestrian Links (with Full Intakes of Solid/Approved Niches)					
Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)	
				Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake by a factor of 2.0)
Year 2028 Reference Scenario (Without Development)					
P1	Kam Wo Bridge	Footbridge	5.2	619 2.0 (A)	1,238 4.0 (A)
P2	Kam Shek New Village	Footpath	2.2	347 2.6 (A)	694 5.3 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	175 2.9 (A)	350 5.8 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	369 2.1 (A)	739 4.1 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	104 6.9 (A)	208 13.8 (A)
Year 2028 Design Scenario (With Development)					
P1	Kam Wo Bridge	Footbridge	5.2	657 2.1 (A)	1,276 4.1 (A)
P2	Kam Shek New Village	Footpath	2.2	385 3.0 (A)	732 5.6 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	271 4.7 (A)	446 7.6 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	381 2.1 (A)	751 4.2 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	116 7.7 (A)	220 14.7 (A)

**Table D3 Year 2028 Performance of Road Junctions (with Full Intakes of Sold/Approved Niches)**

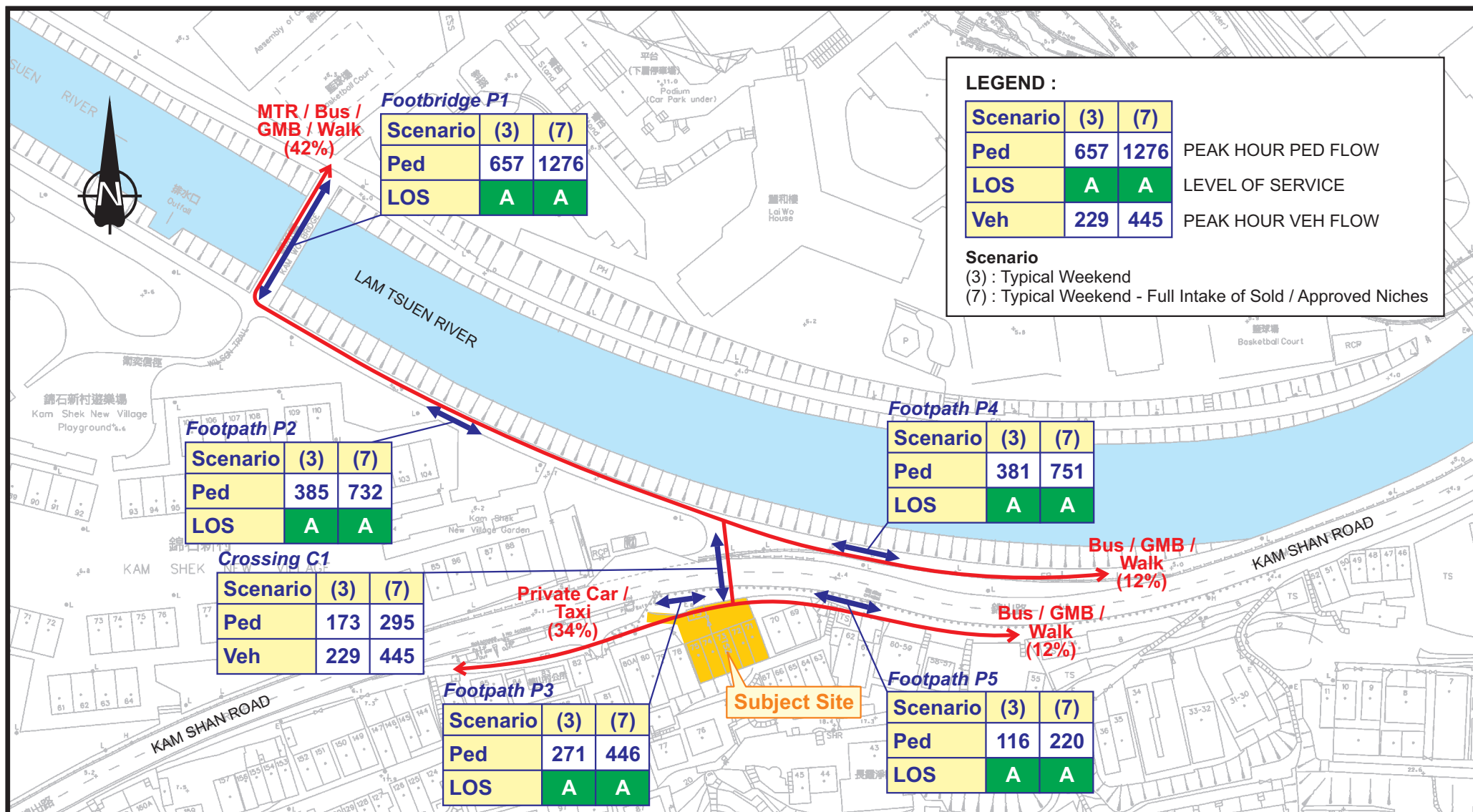
Table D5: Year 2028 Performance of Road Junctions (with Full Intakes of SDA/Approved Inlets)				
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity	
			Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake by a factor of 2.0)
Year 2028 Reference Scenario (Without Development)				
J1	Pak Shing Street near Tai Wo Bridge	Signalised Crossing	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.06	0.11
J3	Kam Shan Road/ Shek Lin Road	Priority	0.05	0.08
Year 2028 Design Scenario (With Development)				
J1	Pak Shing Street near Tai Wo Bridge	Signalised Crossing	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.07	0.12
J3	Kam Shan Road/ Shek Lin Road	Priority	0.05	0.08

**Table D4 Year 2028 Performance of Pedestrian Crossing (with Full Intakes of Sold/Approved Niches)**

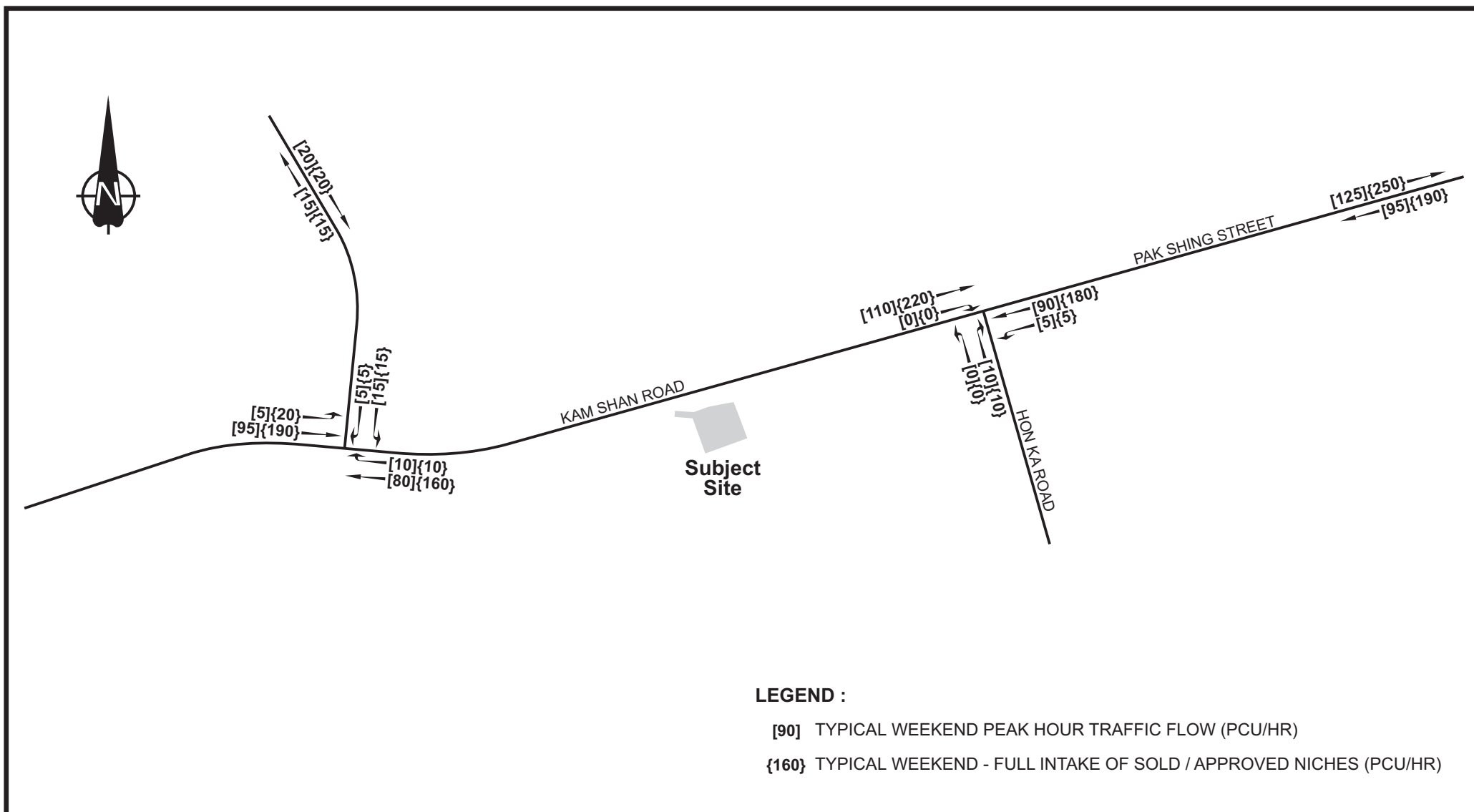
Parameters	Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake by a factor of 2.0)
<b>Year 2028 Reference Scenario (Without Development)</b>		
Pedestrian crossing P (ped/hr)	123	245
2-way Vehicular Flow V (veh/h)	215	432
PV <sup>2</sup> (10 <sup>8</sup> )	0.06	0.46
Recommendation of Pedestrian Crossing Facility	No formal Crossing	No formal Crossing
<b>Year 2028 Design Scenario (With Development)</b>		
Pedestrian crossing P (ped/hr)	173	295
2-way Vehicular Flow V (veh/h)	229	445
PV <sup>2</sup> (10 <sup>8</sup> )	0.09	0.58
Recommendation of Pedestrian Crossing Facility	No formal Crossing	No formal Crossing



Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 71-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 REFERENCE PEDESTRIAN FLOW AND LOS - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021	Drawing No.	D1	Rev.	-




Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 71-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 DESIGN PEDESTRIAN FLOW AND LOS - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021	Drawing No.	D2	Rev.	-

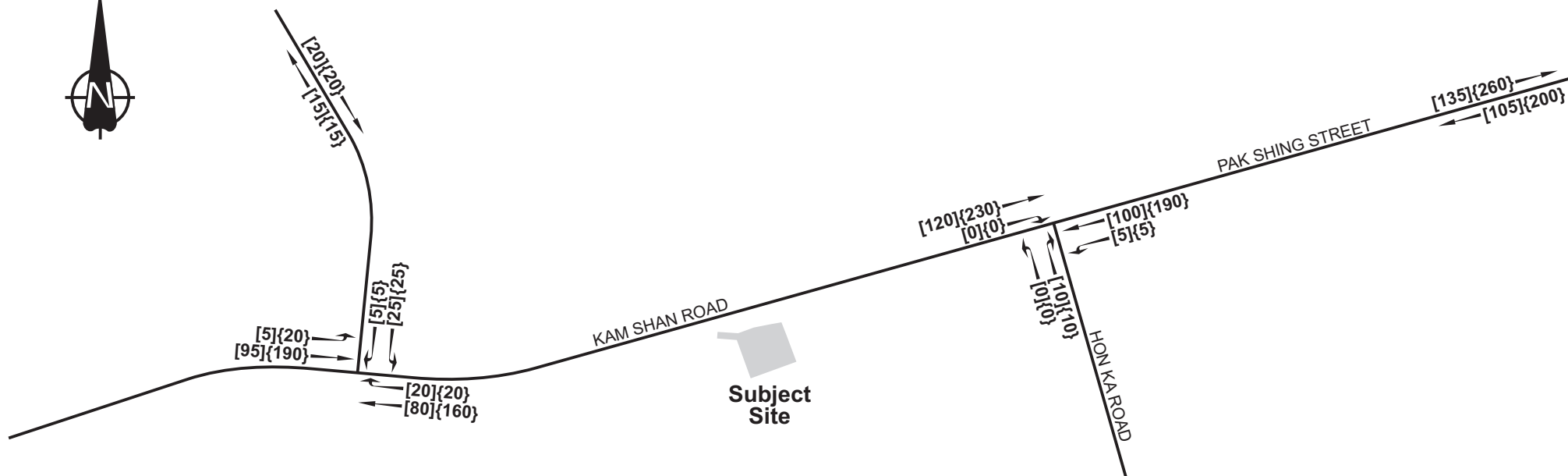


#### LEGEND :

[90] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)

{160} TYPICAL WEEKEND - FULL INTAKE OF SOLD / APPROVED NICHES (PCU/HR)

-	-	-	-	-	-	-	-	-	-	-	
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM “VILLAGE TYPE DEVELOPMENT” ZONE TO “GOVERNMENT, INSTITUTION OR COMMUNITY (3)” ZONE AT NOS. 71-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 REFERENCE TRAFFIC FORECAST - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021	Drawing No.	D3		




**Subject Site**

**LEGEND :**

[100] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)

{190} TYPICAL WEEKEND - FULL INTAKE OF SOLD / APPROVED NICHES (PCU/HR)

-	-	-	-	-	-	-	-	-	-	-	
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM “VILLAGE TYPE DEVELOPMENT” ZONE TO “GOVERNMENT, INSTITUTION OR COMMUNITY (3)” ZONE AT NOS. 71-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 DESIGN TRAFFIC FORECAST - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	APR 2021	Drawing No.	D4	Rev.	



Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Typical Weekend (Full Intake)

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Typical Weekend			Typical Weekend - Full Intakes		
					Left	Right		Typical Weekend	Typical Weekend - Full Intakes	Typical Weekend	Typical Weekend - Full Intakes	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Pak Shing St WB	↑	A	1	4.000						2015	2015	125	0.062	0.062	250	0.124	0.124
Pak Shing St EB	↑	A	1	4.000						2015	2015	95	0.047		190	0.094	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15			*			*

Notes:

Flow: (pcu/hr)

↑ N

→ 95

← 125

Group		A,Bp	Group		A,Bp
y		0.062	y		0.124
L (sec)		22	L (sec)		22
C (sec)		44	C (sec)		44
y pract.		0.450	y pract.		0.450
R.C. (%)		625%	R.C. (%)		263%

Stage / Phase Diagrams

1.

A → ← A

2.

Bp ↑ ↓

3.

4.

5.

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
Date: DEC, 2019								Junction: Pak Shing Street near Sui On Street (J1)	

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Typical Weekend (Full Intake)

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Typical Weekend			Typical Weekend - Full Intakes		
					Left	Right		Typical Weekend	Typical Weekend - Full Intakes	Typical Weekend	Typical Weekend - Full Intakes	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Pak Shing St WB	↑	A	1	4.000						2015	2015	135	0.067	0.067	260	0.129	0.129
Pak Shing St EB	↑	A	1	4.000						2015	2015	105	0.052		200	0.099	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*

Notes:

Flow: (pcu/hr)

↑ N

→ 105

← 135

Group		A,Bp	Group		A,Bp
y		0.067	y		0.129
L (sec)		22	L (sec)		22
C (sec)		44	C (sec)		44
y pract.		0.450	y pract.		0.450
R.C. (%)		572%	R.C. (%)		249%

Stage / Phase Diagrams

1.

A → ← A

2.

Bp ↑ ↓

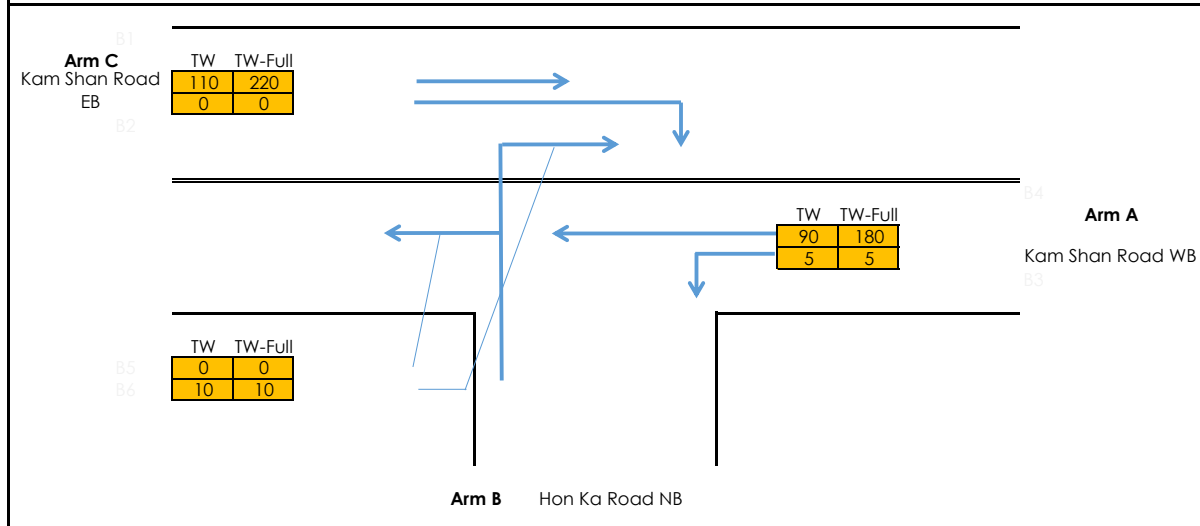
3.

4.

5.

# Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road			
Junction: (J2) Kam Shan Road/ Hon Ka Road			Designed by:
Scheme: Reference - Full Intake		Checked by:	
Design Year: 2028	Job No.: 50299010	Date:	Dec-19
Arm A:	Kam Shan Road WB		
Arm B:	Hon Ka Road NB		
Arm C:	Kam Shan Road EB		



## GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843
	VI(b-a)	50		E	0.897
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.748

## ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	110	220
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	90	180
	q(b-a)	10	10
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	491.8	455.2
	Q(c-a)	1800	1800
	Q(c-b)	673.8	650.9
RFC's	c-a	0.06	0.12
	c-b	0.00	0.00
	b-ac	0.02	0.02
<b>RFC</b>		<b>0.06</b>	<b>0.12</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

f = proportion of minor traffic turning left

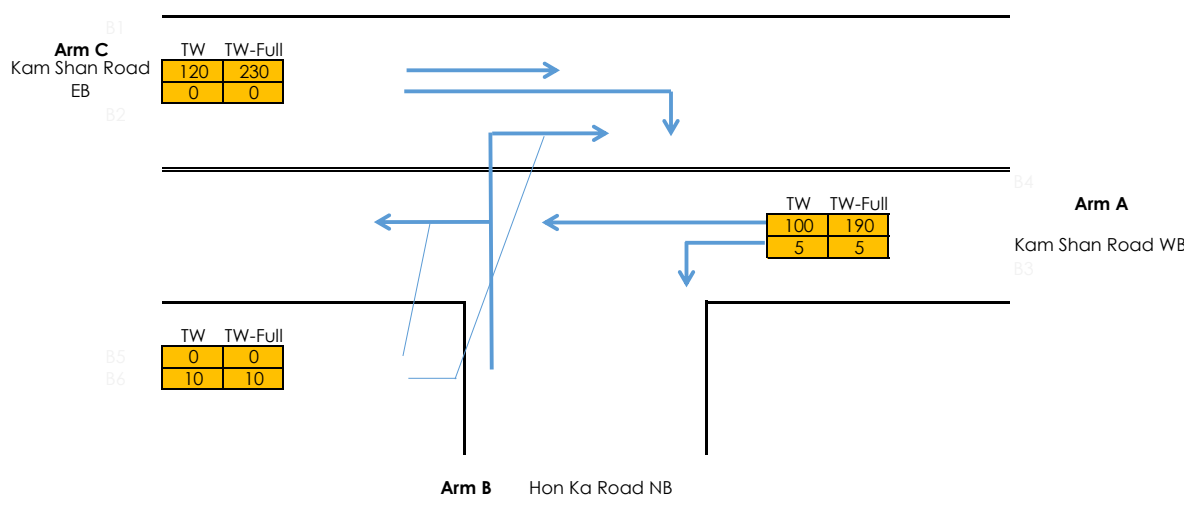
$$Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$$

Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

# Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by:	
Junction: (J2) Kam Shan Road/ Hon Ka Road		Checked by:	
Scheme: Design - Full Intake		Date: Dec-19	
Design Year: 2028		Job No.: 50299010	
Arm A: Kam Shan Road WB			
Arm B: Hon Ka Road NB			
Arm C: Kam Shan Road EB			

**Arm C** Kam Shan Road EB

**Arm A** Kam Shan Road WB

**Arm B** Hon Ka Road NB

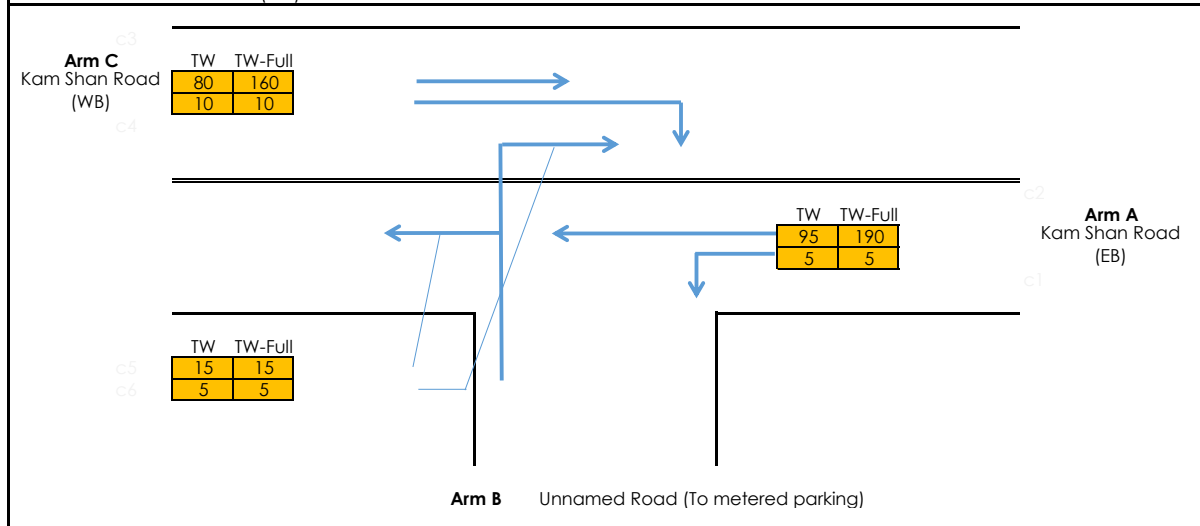
GEOMETRY																													
Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50																								
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50																								
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65																								
Combined stream on minor arm	Y/N?	Y																											
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.843																								
	VI(b-a)	50		E	0.897																								
	Vr(b-c)	20		F	0.937																								
	Vr(c-b)	50		Y	0.748																								
ANALYSIS																													
TRAFFIC FLOWS (pcu/hr)		<table border="1"> <thead> <tr> <th></th> <th>TW</th> <th>TW-Full</th> </tr> </thead> <tbody> <tr> <td>q(c-a)</td> <td>120</td> <td>230</td> </tr> <tr> <td>q(c-b)</td> <td>0</td> <td>0</td> </tr> <tr> <td>q(a-b)</td> <td>5</td> <td>5</td> </tr> <tr> <td>q(a-c)</td> <td>100</td> <td>190</td> </tr> <tr> <td>q(b-a)</td> <td>10</td> <td>10</td> </tr> <tr> <td>q(b-c)</td> <td>0</td> <td>0</td> </tr> <tr> <td>f</td> <td>0.00</td> <td>0.00</td> </tr> </tbody> </table>					TW	TW-Full	q(c-a)	120	230	q(c-b)	0	0	q(a-b)	5	5	q(a-c)	100	190	q(b-a)	10	10	q(b-c)	0	0	f	0.00	0.00
	TW	TW-Full																											
q(c-a)	120	230																											
q(c-b)	0	0																											
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q(b-c)	0	0																											
f	0.00	0.00																											
CAPACITIES (pcu/hr)		<table border="1"> <thead> <tr> <th></th> <th>TW</th> <th>TW-Full</th> </tr> </thead> <tbody> <tr> <td>Q(b-ac)</td> <td>488</td> <td>451.5</td> </tr> <tr> <td>Q(c-a)</td> <td>1800</td> <td>1800</td> </tr> <tr> <td>Q(c-b)</td> <td>671.3</td> <td>648.3</td> </tr> </tbody> </table>					TW	TW-Full	Q(b-ac)	488	451.5	Q(c-a)	1800	1800	Q(c-b)	671.3	648.3												
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Q(b-ac)	488	451.5																											
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RFC's		<table border="1"> <thead> <tr> <th></th> <th>TW</th> <th>TW-Full</th> </tr> </thead> <tbody> <tr> <td>c-a</td> <td>0.07</td> <td>0.13</td> </tr> <tr> <td>c-b</td> <td>0.00</td> <td>0.00</td> </tr> <tr> <td>b-ac</td> <td>0.02</td> <td>0.02</td> </tr> </tbody> </table>					TW	TW-Full	c-a	0.07	0.13	c-b	0.00	0.00	b-ac	0.02	0.02												
	TW	TW-Full																											
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<b>RFC</b>		<b>0.07 0.13</b>																											

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1+0.094(w(b-a)-3.65))(1+0.0009(Vr(b-a)-120))(1+0.0006(VI(b-a)-150))$   
 $E = (1+0.094(w(b-c)-3.65))(1+0.0009(Vr(b-c)-120))$   
 $F = (1+0.094(w(c-b)-3.65))(1+0.0009(Vr(c-b)-120))$   
 $Y = 1-0.0345W$   
 f = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c)*Q(b-a)/(1-f)*Q(b-c)+f*Q(b-a)$  Capacity of combined streams

**All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1**

# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Reference - Full Intake	Date:	Dec-19
Design Year:	2028	Job No.:	50299010
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



## GEOMETRY

Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865
	VI(b-a)	30		E	0.932
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.759

## ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	80	160
	q(c-b)	10	10
	q(a-b)	5	5
	q(a-c)	95	190
	q(b-a)	5	5
	q(b-c)	15	15
	f	0.75	0.75
CAPACITIES (pcu/hr)	Q(b-ac)	618.8	589.9
	Q(c-a)	1774	1773
	Q(c-b)	688	662.8
RFC's	c-a	0.05	0.09
	c-b	0.01	0.02
	b-ac	0.03	0.03
RFC		0.05	0.09

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1+0.094(w(b-a)-3.65))(1+0.0009(Vr(b-a)-120))(1+0.0006(VI(b-a)-150))$$

$$E = (1+0.094(w(b-c)-3.65))(1+0.0009(Vr(b-c)-120))$$

$$F = (1+0.094(w(c-b)-3.65))(1+0.0009(Vr(c-b)-120))$$

$$Y = 1-0.0345W$$

f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c)*Q(b-a)/(1-f)*Q(b-c)+f*Q(b-a)$$

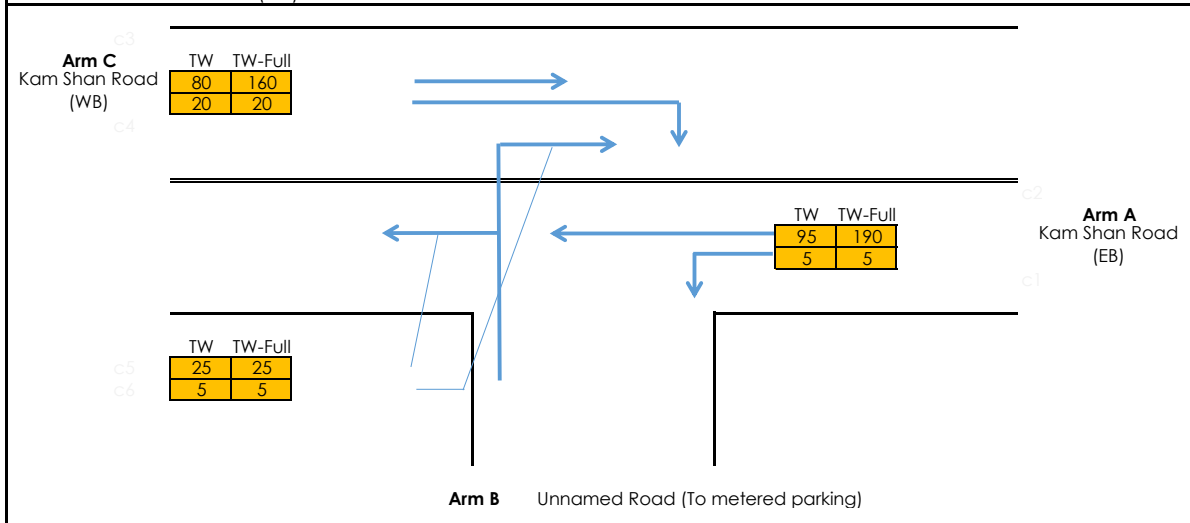
Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1



# Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	
Scheme:	Design - Full Intake	Date:	Dec-19
Design Year:	2028	Job No.:	50299010
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



GEOMETRY					
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.865
	VI(b-a)	30		E	0.932
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.759
ANALYSIS			TW TW-Full		
TRAFFIC FLOWS (pcu/hr)	q(c-a)		80	160	
	q(c-b)		20	20	
	q(a-b)		5	5	
	q(a-c)		95	190	
	q(b-a)		5	5	
	q(b-c)		25	25	
	f		0.83	0.83	
CAPACITIES (pcu/hr)	Q(b-ac)		633.9	606.3	
	Q(c-a)		1748	1746	
	Q(c-b)		688	662.8	
RFC's	c-a		0.05	0.09	
	c-b		0.03	0.03	
	b-ac		0.05	0.05	
RFC			0.05	0.09	

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1+0.094(w(b-a)-3.65))(1+0.0009(Vr(b-a)-120))(1+0.0006(VI(b-a)-150))$$

$$E = (1+0.094(w(b-c)-3.65))(1+0.0009(Vr(b-c)-120))$$

$$F = (1+0.094(w(c-b)-3.65))(1+0.0009(Vr(c-b)-120))$$

$$Y = 1-0.0345W$$

f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c)*Q(b-a)/(1-f)*Q(b-c)+f*Q(b-a)$$

Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

VISION PLANNING CONSULTANTS LTD.  
弘域城市規劃顧問有限公司

Our Ref: TP-KSR2/RA/CHCS/20-07  
Your Ref: TPB/Y/TP/37  
Date: 30 December, 2022

**By Hand & Email**  
(tpbpd@pland.gov.hk)

The Secretary,  
Town Planning Board,  
c/o Town Planning Board Section,  
Planning Department,  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong.

Dear Sirs,

**Rezoning Application from “Village Type Development” Zone to “Government, Institution or Community (3)” Zone at Nos. 74-75 Kam Shan Road and Adjoining Government Land, Tai Po, New Territories (*Application No. Y/TP/37*) – Further Information**

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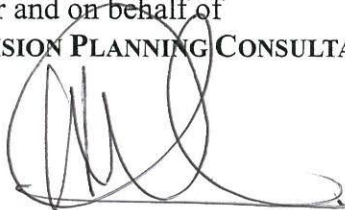
The Applicant would like to submit herewith, a total of 70 hard copies of the following materials to respond to comments raised by the Transport Department on the captioned application:

- i. Responses-to-Comments (“R-to-C”) Table dated 30.12.2022 (**Table A**);
- ii. An updated report of the Traffic Impact Assessment (**Annex A**); and
- iii. Revised Figure 5 to replace the original one in the submitted Planning Statement (**Annex B**).

Should you have any queries with regard to the above, please do not hesitate to contact our Miss Charissa LEUNG or the undersigned at 2566 9060.

Thank you very much for your kind attention.

Yours faithfully,  
for and on behalf of  
**VISION PLANNING CONSULTANTS LTD.**



Kim On CHAN  
Managing Director  
[KC/CL]  
[TP-KSR2/RA/CHCS/22-07]

c.c. STNDPO (Attn.: Mr. YU Wai Kin, Nicol)  
Client

By email  
By email

Our Ref: TP-KSR2/RA/CHCS/20-12  
Your Ref: TPB/Y/TP/37  
Date: 17 May, 2023

By Hand & Email  
(tpbpd@pland.gov.hk)

The Secretary,  
Town Planning Board,  
c/o Town Planning Board Section,  
Planning Department,  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong.

Dear Sirs,

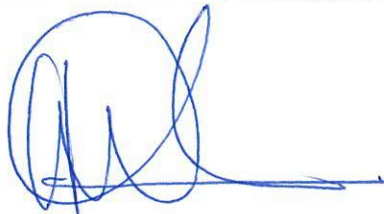
**Rezoning Application from “Village Type Development” Zone to “Government, Institution or Community (3)” Zone at Nos. 74-75 Kam Shan Road and Adjoining Government Land, Tai Po, New Territories (Application No. Y/TP/37) – Further Information**

On behalf of the Applicant, we would to withdraw our deferment letter (Ref: TP-KSR2/RA/CHCS/20-11), dated 16 May 2023. Please find attached ,herewith, a Responses-to-Comments (“R-to-C”) Table (**Annex A**) to respond to comments raised by the Food and Environmental Hygiene Department, Urban Design and Landscape Section of the Planning Department, Lands Department, Drainage Service Department, Traffic Department and public comments on the captioned application.

Should you have any queries with regard to the above, please do not hesitate to contact our Miss Charissa LEUNG or the undersigned at 2566 9060.

Thank you very much for your kind attention.

Yours faithfully,  
for and on behalf of  
**VISION PLANNING CONSULTANTS LTD.**



Kim On CHAN  
Managing Director  
[KC/CL]

[TP-KSR2/RA/CHCS/22-12]

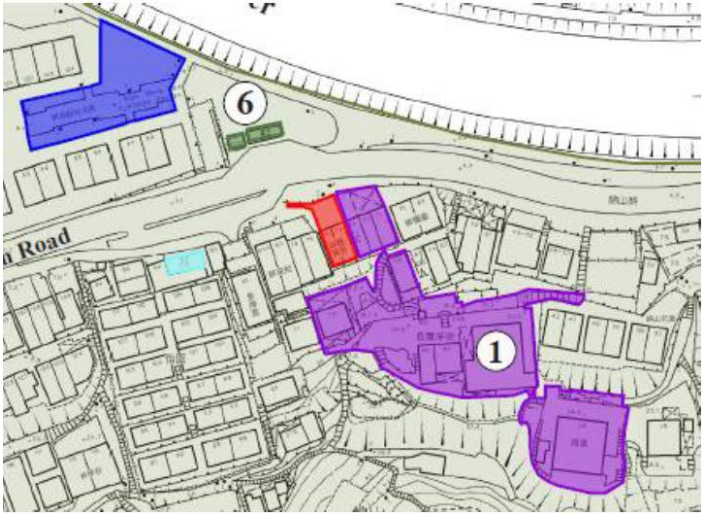
c.c. STNDPO (Attn.: Mr. YU Wai Kin, Nicol)  
Client

By email  
By email

Departmental Comments	Response
<b>A. Food and Environmental Hygiene Department</b>	
<p>1. Under the Private Columbaria Ordinance (the PCO), only private columbaria that have obtained a licence may sell or newly let out niches. Any private columbarium which applies for a licence must comply with the Town Planning Ordinance (Cap. 131) and other requirements prescribed in the PCO or specified by the Private Columbaria Licensing Board (the PCLB), including land-related, building-related, right to use the columbarium premises, deed of mutual covenant (if applicable), fire safety, etc. Section 18(2) of the PCO stipulates that the PCLB may grant a licence only if a management plan submitted by the applicant has been approved by the PCLB. The management plan submitted to the PCLB should cover matters including traffic and public transport arrangement or management, crowd management, manpower deployment on peak grave sweeping days or periods and other days or periods, etc. For cases in which planning permission has been given by the Town Planning Board (TPB), the management plan should include the crowd and traffic management measures accepted (and the conditions imposed) by the relevant departments and the TPB when the TPB approved the planning application. In considering the management plan submitted by a licence applicant, the PCLB will take into account comments provided by concerned departments including Transport Department,</p>	Noted.

	the Police, Fire Services Department and the Planning Department. If a licence application is approved by the PCLB, the Private Columbaria Affairs Office will oversee the monitoring of the licensee's implementation of the approved management plan in coordination with other concerned departments.	
2.	Regarding the private columbarium namely "祥霞精舍" at Nos. 74 & 75 Kam Shan Road, Kam Shan, Tai Po, N.T. (Lots No. 738 S.C. & 738 S.C. ss.1 in D.D.6), a set of the specified instrument (SI) application (viz. a licence and TSOL) in respect of a pre-cut-off columbarium was received by PCLB. An Approval-in-Principle of TSOL was granted on 29.12.2021 and valid until 19.12.2024 whereas the application for licence is being processed by the Private Columbaria Affairs Office (PCAO). Based on the niche information submitted by the applicant on 25.10.2021 and 8.11.2021 in support of her SI applications, it was preliminary noted that the total number of niches, types of niches, its breakdown and generally location apparently tally with that as shown in the planning statement submitted in support of this planning application. Having said that, we are still in the course of detailed checking of niche information to verify its accuracy and subject to on-site verification by PCAO.	Noted.
3.	Apart from above, we had spotted that the boundary of shaded rezoning	Noted. The site boundary in Figure 5 has been



	area in figure 2 and figure 5 on the planning statement is inconsistent.	revised accordingly and has been attached in the Further Information (FI) submission dated 30.12.2022 ( <i>See Annex B</i> ).
		
4.	As regards the applicant's proposed measures of "not to open the proposed columbarium development during Ching Ming and Chung Yeung Festival days and their shadow periods", the PCAO will have no comments provided that:	
	(i) the applicant undertakes to lock up the columbarium during the proposed closure periods;	Yes, the Applicant will undertake this commitment at all times.

	<p>(ii) the proposed period of closure of the columbarium is clearly stated in the proposal (e.g. during Ching Ming Festival and Chung Yeung Festival Days and the two weekends before and after the two festival days, including any public holiday between festival days and the two weekends mentioned);</p>	<p>Yes, the Applicant will undertake this commitment at all times. In addition, the Applicant would like to supplement that apart from closure during Ching Ming and Chung Yeung festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each festival day, the proposed (existing) columbarium will also be closed from the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar Chinese New Year.</p>
	<p>(i) as some niches in the columbarium have already been sold, the applicant undertakes to:</p> <p>1) inform the affected parties (including the purchasers of the sold niches) in writing that these measures are proposed by the applicant and will be implemented if its planning application and licence application are approved; and</p> <p>2) remind the concerned parties by letter, email, fax, SMS and any possible notification means of the total closure of the columbarium during the specified periods at least one month in advance of each closure period; and</p>	<p>Noted.</p>
	<p>(ii) for future buyers of the interment rights of niches in the concerned columbarium, the applicant undertakes to state clearly in the sales agreements that the columbarium will be completely closed during the specified periods and a ‘visit-by-</p>	<p>Noted.</p>

	appointment' arrangement will be adopted for the specified periods and explain these arrangements to the potential buyers before entering into the sales agreements.	
	As regards the applicant's proposed measures of the 'visit-by-appointment' system in the Management Plan, PCAO will have no comments provided that:	
	(i) the applicant undertakes to (a) inform the affected parties (including the purchasers of the sold niches) in writing that the House Rules, special traffic arrangements and a Visit-By-Appointment System arrangement are proposed by the applicant and will be implemented; and (b) remind the concerned parties by letter, email, fax, SMS and any possible notification means of the above-mentioned arrangements during grave sweeping festival periods at least one month in advance of each period;	Noted.
	(ii) for future buyers of the interment rights of niches, the applicant undertakes to state clearly in the sales agreements that above-mentioned arrangements will be adopted and explain these arrangements to the potential buyers before entering into the sales agreements;	Noted.

	(iii) the applicant should keep a register for the "visit-by-appointment system" for all the visitors, to record the information include (but not limited to): the date and time of visit session, niche no. to be visited, the name of visitors, the no. of visitors, the time of leaving the columbarium, the date of booking and the means of booking & etc. so that monitoring on compliance of such mandatory "visit-by-appointment system" arrangement could be conducted by FEHD staff on the spot.	Noted.
e.	(a) In respect of the Management Plan submitted by the applicant, please refer to <b><u>Appendix I</u></b> .	Noted.
<b>B.</b>	<b>Urban Design and Landscape Section, Planning Department</b>	
	According to the Planning Statement (PS), the proposal mainly involves an existing columbarium development in a single-storey building (with the development parameters including gross floor area of about 87.09m <sup>2</sup> , plot ratio of 0.54, and site coverage of 54.43%) and provision of a new standalone entrance access point with a 1.5m-high metal fence along the boundary of the Site with an area of about 159.99m <sup>2</sup> . In view of the small scale of the proposal, there are no particular comments from the urban design and visual impact perspectives.	Noted.

C.	<b>Lands Department</b>	
1.	<p>The application site comprises Lot Nos. 738 S.C ss. 1 in D.D.6 and Government land. The original lease of the mother lot, Lot No. 738 is untraceable. However, as per our file records, it seems the mother lot is a building lot subject to General Conditions of the Sale published in Government Notification No. 365 of 1906 (the said GN"). The salient conditions are summarized below:</p> <p>a. No verandah shall be constructed so as to project over Crown land; and</p> <p>b. No house shall be more than two stores in height.</p>	Noted.
2.	<p>Under the said GN, there is no prohibition against the lot being used as a columbarium. If the proposal is approved by the TPB, the applicant is not required to seek a lease modification from LandsD to implement it. Therefore, any planning conditions, if imposed by the TPB, cannot be written into the lease through lease modification.</p>	Noted.
3.	<p>No permission is given for occupation of Government land (GL) included in the application site. Any occupation of GL without the Government's prior approval is not allowed. The applicant is required to apply for short term tenancy (STT) in respect of the GL within the application site if the proposed amendment to the OZP is approved by the TPB.</p>	Noted. Upon Town Planning Board (TPB)'s agreement on the present rezoning application, the applicant will submit to the Lands Department an application for a Short-Term Tenancy to use, manage and maintain the subject Government Land.
4.	<p>Regarding the proposed STT, it is noted that the said GL would also lead to the neighboring private lots. Hence, comments from the relevant</p>	Please note that the existing development at Nos. 71-75 and Nos. 78-79 Kam Shan Road have been served



	<p>departments including TD, DO(TP) and PCAO on the need and extent of GL for accessibility and operation of columbarium development are required. The relevant departments may wish to advised this office of any additional land-related condition proposed to be included in the STT upon receipt of the STT application.</p>	<p>by their own existing pedestrian walkway respectively since their establishments many decades ago. The proposed fence off the subject STT will avoid unnecessary disturbance to the neighboring private lots.</p> <p>It should be noted that spare keys will be provided to the representatives of Yau's ancestral Hall and owners of Nos. 71 and 72 Kam Shan Road for the gated access. As told by the owners of Nos. 71-72 Kam Shan Road that these two structures are mainly for ancillary storage uses.</p>
<b>D.</b>	<b>Drainage Services Departments</b>	
1.	<p>The applicant should have its own stormwater collection and discharge system to cater for the runoff generated within the Site and overland flow from surrounding of the Site, e.g. surface channel of sufficient size along the perimeter of the Site; sufficient opening should be provided at the bottom of the boundary wall/fence to allow surface runoff to pass through the Site if any boundary wall/fence are to be erected. Any existing flow path affected should be re-provided. The applicant should neither obstruct overland flow nor adversely affect the existing natural streams, village drains, ditches and the adjacent areas. The</p>	<p>The building under application was built in early 1910's and the open area in front of the building was paved with concrete for decades ago. No additional surface runoff will be generated by the proposed rezoning scheme (to regularize the existing columbarium) into the local drainage system. The subject open area will be fenced by metal railings with openings at the bottom of this fence.</p>

	<p>applicant is required to maintain the drainage systems properly and rectify/modify the nearby existing/original drainage systems if they are found to be inadequate or ineffective to accommodate the additional runoff arisen from the development of the Site. The applicant shall also be liable for and shall indemnify claims and demands arising out of damage or nuisance caused by failure or ineffectiveness of the modified drainage systems caused by their works. Please be reminded that the runoff within the subject premise including the runoff from the rooftop shall be served by a designated stormwater collection and discharge system and shall not be drained to the public sewerage network.</p>	
2.	<p>DSD's maintained public sewers exist in the vicinity but the feasibility of sewerage connection is subject to the invert level of discharge connection pipe leading from the proposed site. No stud pipe is reserved for sewage connection. The applicant shall demonstrate the technical feasibility of sewerage connection. Should the applicant choose to dispose of the sewage of the proposed development through other means, views and comments from EPD should be sought. A copy of our drainage record is attached for your reference.</p>	<p>While the rezoning site does not provide any toilet facilities, the proposed scheme will undertake to provide a special arrangement to share the existing (soak-away) toilet facilities (one for uni-sex toilet and one for disabled) at Nos. 71 and 72 Kam Shan Road. Upon TPB's agreement on present rezoning application, the Applicant shall apply for an operation licence under the Private Columbaria Ordinance. The Applicant shall follow strictly to the Application Guide for Private Columbarium License and Other Specified Instruments and comply with the environmental related requirement in providing detailed specifications of the soak-away system, including particulars of the</p>

		company to provide regular services for such treatment facilities as specified in Chapter 15 and Annex 10.
3.	Provision of written consent(s) from relevant lot owner(s) and/or LandsD's permission for laying new drains/channels and/or modifying/upgrading existing ones outside the application lot(s) or on government land (where required).	Noted. Upon Town Planning Board (TPB)'s agreement on the present rezoning application, the applicant will submit to the Lands Department an application for a Short-Term Tenancy to use, manage and maintain the subject Government Land.
4.	The cost and work of drainage and sewerage connection as well as future maintenance responsibility shall be borne by the applicant(s).	If and when necessary, the Applicant has no objection to undertake this commitment at all times.
<b>E. Transport Department</b>		
1.	R-to-C 3, para. 3.2.13 and drawing no.3.1: It is noted that the proposed railing on the existing ramp are not necessary from the traffic engineering point of view. Nevertheless, as the ramp is on unallocated government land, our previous comments from the management/maintenance parties of the access shall be consulted.	Noted. Upon Town Planning Board's agreement on the present rezoning application, the applicant will submit to the Lands Department an application for a Short-Term Tenancy to use, manage and maintain the subject Government Land, including the existing ramp under application.
2.	R-to-C 5, para. and drawing no. 3.1: Our Comments remain valid. The proposed holding area is on unallocated government land, comments from the concerned management/maintenance parties shall be sought.	Noted. Upon Town Planning Board's agreement on the present rezoning application, the applicant will submit to the Lands Department an application for a

	As the holding area is also an important measure in the traffic management plan, PCAO shall advise whether the proposed measure is enforceable by them.	Short-Term Tenancy to use, manage and maintain the subject Government Land, including the existing ramp under application.
3.	Para. 4.5.4 and Appendix D: The Applicant shall advise how the traffic impact of other columbarium in the neighboring area are being considered in the cumulative traffic impact. In particular, the applicant shall advise the assumptions and details on how the visitors to other columbarium are assigned in the critical footpaths, junctions and road links.	Noted. The revised report of the TIA will be submitted shortly.
F.	Public Comments	
1.	The columbarium development is not in line with the planning intention of “Village Type Development” zone and incompatible with the surrounding village development.	The purpose of this rezoning aims to address the subject planning intention matter. It should be noted that private columbarium is one of the community-needed facilities nowadays like schools, churches, RCHE, shop and services, eating places, and other social welfare facilities, many which are commonly found within “V” zones. “V” zone alone shall not and cannot be exclusively used for village housing development. As long as it is being accepted by the local residents, in particular these neighboring residents, such community facility should be treated and justifiable and compatible use in the area. Local supports and no objection given by neighboring

		<p>residents have already demonstrated the nature and the scale of development under application are acceptable. The proposed peripheral metal railing fence will avoid unnecessary disturbance during operation.</p>
2.	<p>The columbarium development is in proximity with the surrounding residential development and causes disturbance to the residents nearby.</p>	<p>The purpose of this rezoning aims to address the subject planning intention matter. It should be noted that private columbarium is one of the community-needed facilities nowadays like schools, churches, RCHE, shop and services, eating places, and other social welfare facilities, many which are commonly found within “V” zones. “V” zone alone shall not and cannot be exclusively used for village housing development. As long as it is being accepted by the local residents, in particular these neighboring residents, such community facility should be treated and justifiable and compatible use in the area. Local supports and no objection given by neighboring residents have already demonstrated the nature and the scale of development under application are acceptable. The proposed peripheral metal railing fence will avoid unnecessary disturbance during operation.</p>



3.	The columbarium development will reduce land reserved for Small House development.	It should be noted that the existing properties are under a Land Sale Tender Agreement and will have no implication Small House site application.
4.	The columbarium will cause traffic congestion and illegal parking problems and adversely affect the pedestrian safety, especially during the Ching Ming and Chung Yeung Festival days.	<p>The proposed development will be (i) closed its operation during Ching Ming and Chung Yeung festival days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks each festival day, as well as the 1st to 14th day of the Lunar Chinese New Year; and (ii) the adoption of a mandatory ‘visit-by-appointment’ system will strictly limit the number of visitors.</p> <p>The results of the TIA (FI) have demonstrated that, with the proposed management measures (i) and (ii), the potential traffic impact on the key road junctions and pedestrian links due to the rezoning scheme will be insignificant.</p>
5.	「先經營，後申請」會成該區的不良先例	祥霞精舍在 2007 年開始營運，而在《私營骨灰安置所條例》（第 630 章）於 2017 年 6 月 30 日生效後，申請人已於 2019 年向私營骨灰安置所發牌委員會遞交了相關的牌照及暫免法律責任書申請，亦停止了繼續銷售新的骨灰龕位。該《條例》的目的，

		<p>是透過發牌制度一併過規範化早前存在不理想的骨灰龕場營運狀況。該《條例》是政府經過深思熟慮和以儘量不影響已上灰先人及減低滋擾其家庭成員的理念而訂定的。是次申請主要是進一步優化現有設施，配合立法原意，提升共融空間，多元化服務社會，做法也得到鄉眾支持和讚賞；同時，申請方案也沒有構成不良影響，所以不存在「先經營，後申請」的問題。</p>
6.	現時公眾龕位有足夠供應，基本上無需輪候，此改建計劃多此一舉。	<p>食物環境衛生署 2019 年的數據指出，從 2021 年到 2040 年將會進行約 1,200,000 次火葬。因此，公眾對未來的安葬需求將非常龐大。如用政府已公開有關龕位供應的統計數字粗略計算（詳盡計算請看規劃綱領），現時尚欠約 600,245（即 1,200,000－599,755）。有鑑於此，將私營骨灰安置正規化及所增加私營供應是最實際的“速贏”措施。同時，也給社會多一個選擇。</p>

Our Ref: TP-KSR2/RA/CHCS/20-13  
Your Ref: TPB/Y/TP/37  
Date: 28 July, 2023

By Hand & Email  
(tpbpd@pland.gov.hk)

The Secretary,  
Town Planning Board,  
c/o Town Planning Board Section,  
Planning Department,  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong.

Dear Sirs,

**Rezoning Application from “Village Type Development” Zone to “Government, Institution or Community (3)” Zone at Nos. 74-75 Kam Shan Road and Adjoining Government Land, Tai Po, New Territories (Application No. Y/TP/37) – Further Information III**

The Applicant would like to submit herewith, a total of 70 hard copies of the following materials to respond to comments raised by the Transport Department and a bundle of supporting/no objection letters collected from the nearby residents and Kam Shan Village Residents’ meeting with respect to the captioned rezoning application:

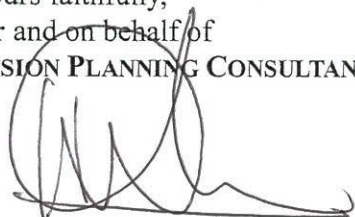
- i. An updated report of the Traffic Impact Assessment (**Annex A**);
- ii. Results of Kam Shan Village Residents’ Meeting dated 17.7.2023 (**Annex B**); and
- iii. A bundle of supporting/no objection letters issued by nearby residents/owners (**Annex C**);
- iv. **Plan A** shows the location of these local supporters.

The Applicant would like to point out the fact that the local people of Kam Shan Village, including those living immediately next to the rezoning area, are supporting the proposed columbarium facility under application.

Should you have any queries with regard to the above, please do not hesitate to contact our Miss Charissa LEUNG or the undersigned at 2566 9060.

Thank you very much for your kind attention.

Yours faithfully,  
for and on behalf of  
**VISION PLANNING CONSULTANTS LTD.**



Kim On CHAN  
Managing Director

[KC/CL] [TP-KSR2/RA/CHCS/22-13]p

c.c. STNDPO (Attn.: Mr. YU Wai Kin, Nicol)

By email

By email

## **Annex A**

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*Updated Report of the Traffic Impact Assessment*

Reference number CHK50650510

**REZONING APPLICATION FROM “VILLAGE TYPE DEVELOPMENT”  
ZONE TO “GOVERNMENT, INSTITUTION OR COMMUNITY (3)” ZONE  
AT NOS. 74 AND 75 KAM SHAN ROAD AND ADJOINING  
GOVERNMENT LAND, TAI PO, NEW TERRITORIES**

**TRAFFIC IMPACT ASSESSMENT**





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# 1 INTRODUCTION

## 1.1 Background

- 1.1.1 The Subject Site is located at Nos. 74-75 Kam Shan Road and adjoining Government Land, Tai Po, as indicated in **Figure 1.1**, which is currently zoned as “Village Type Development” (“V”) according to the approved Tai Po Outline Zoning Plan No. S/TP/30 (the “OZP”).
- 1.1.2 It is intended to regularise the existing 1,700 columbarium niche units at the Subject Site, via a rezoning application from “V” zone to “Government, Institution or Community (3)” (“G/IC (3)”) zone under Section 12A of the Town Planning Ordinance. A Traffic Impact Assessment (TIA) study is required in support of the proposed columbarium development at the Subject Site.
- 1.1.3 The development scale is identical to the previous rezoning proposals made under Application No. Y/TP/27 and No. Y/TP/32.
- 1.1.4 In order to minimise the potential nuisances to its nearby dwellings, the Applicants have decided to provide a standalone entrance access point with metal fence on the peripheral site boundary to strengthen the self-governing spatial arrangement in the area.
- 1.1.5 The Applicant has retained all best practicable management methods that received positive responses from professional and technical and government departments in the present rezoning scheme.

## 1.2 Study Objectives

- 1.2.1 The primary objective of this TIA study is to address possible traffic and transport concerns in association with the proposed columbarium use at the Subject Site.
- Evaluate the current traffic conditions for both vehicular and pedestrians in the vicinity of the Subject Site;
  - Estimate traffic demand forecast for an appropriate design year;
  - Estimate the traffic generations and attractions of the proposed and other adjacent columbarium developments during the following periods:
    - Festival Days (i.e. Ching Ming Festival and Chung Yeung Festival);
    - Shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days; and
    - Typical weekends (excluding the period from the 1<sup>st</sup> to 14<sup>th</sup> day of Lunar Chinese New Year during which the proposed (existing) columbarium will be closed).
  - Investigate the traffic impact to the identified key road links and junctions by the proposed columbarium development;

- Review the anticipated pedestrian access routes and assess the operation performance at critical sections;
- Determine the demand for the internal transport facilities (e.g. parking, pick-up / drop-off, loading / unloading, etc.) regarding the proposed columbarium development and justify the proposed provision with regard to the site conditions and constraints; and,
- Suggest any appropriate traffic improvement and management measures in order to alleviate the foreseeable problems.

### 1.3 Structure of the Report

1.3.1 After this introductory chapter, there are five further chapters, they are:

- Chapter 2 – Existing Traffic Context, reviews the current traffic condition and public transport facility in the vicinity;
- Chapter 3 – The Proposed Development, describes the key parameters of the proposed columbarium development and presents the traffic management measures during Festival Days, shadow weekends and typical weekends;
- Chapter 4 – Traffic Impact Assessment, presents the traffic assessments of operation performance in terms of vehicular and pedestrian traffic as well as parking facilities; and
- Chapter 5 – Conclusion, summarises the study findings and presents the final conclusions accordingly.

## 2 EXISTING TRAFFIC CONTEXT

### 2.1 Existing Road Network

- 2.1.1 The Subject Site can be only accessed by Kam Shan Road, which is a single two-lane carriageway south of Lam Tsuen River, connecting to the major road network via Pak Shing Street and Kwong Fuk Road.
- 2.1.2 Pak Shing Street is a single two-lane carriageway south of Lam Tsuen River, linking Kam Shan Road in the west and Kwong Fuk Road in the east. There is a signalised pedestrian crossing at Pak Shing Street near Tai Wo Bridge across the Lam Tsuen River.
- 2.1.3 Kwong Fuk Road is a single 4-lane carriageway with local road linkage to Tai Po Central and Tai Po Market, as well as linkages to/from Tolo Highway for Shatin and Kowloon.
- 2.1.4 The major vehicular ingress and egress routes to the Subject Site are shown in **Figure 2.1**.

### 2.2 Public Transport Service

- 2.2.1 In terms of public transport, Kam Shan Road is served by GMB route 22K which only provides AM Peak services.
- 2.2.2 However, MTR Tai Wo Station is only 5-10 minute walk from the Subject Site via Kam Wo Bridge. A wide range of public transport modes are available in the vicinity of the Subject Site. In addition to the MTR East Rail Line, a vast number of road-based public transport services like franchised bus and Green Mini Bus (GMB) routes are also found as listed in **Table 2.1** and illustrated in **Figure 2.2**.

**Table 2.1 Public Transport Provision (Sunday / Public Holiday Services)**

Route No.	Description	Frequency (min.)
<b>Public Transport at Kam Shan Road</b>		
<b>GMB</b>		
22K	Kam Shan Road – Tai Po Market (Nam Shing Street)	AM Peak Only
<b>Public Transport at Tai Wo Station</b>		
<b>Franchised Bus</b>		
64K	Yuen Long West – Tai Po Market Station	7 – 10
71K	Tai Wo – Tai Po Market Station	15
72	Tai Wo – Cheung Sha Wan	20
73	Tai Po Industrial Estate – Wah Ming	30
73A	Wah Ming – Yu Chui Court	30
73B	Tai Po Nethersole Hospital – Sheung Shui	30
74A	Tai Wo – Kai Yip	60
74D	Kau Lung Hang – Kwun Tong Ferry	60
264R	Yuen Long (West) – Tai Po Market Station	20
<b>GMB</b>		
21K	Wai Tau Tsuen – Tai Po Market (Nam Shing Street)	15 – 30
25A	Nam Wa Po – Tai Po Market (Nam Shing Street)	6 – 11
25B	Kau Lung Heng/Yuen Leng – Tai Po Market (Nam Shing Street)	5 – 9
25K	Ng Tung Chai – Tai Po Market (Nam Shing Street)	5 – 12



2.2.3 Other than MTR Tai Wo Station, Tai Po Market and Kwong Fuk Road are in fact within 15-20 minutes of walking distance from the Subject Site.

2.2.4 In view of the availability of various transport modes within walking distance, the Subject Site enjoys an excellent accessibility in terms of public transport.

## 2.3 Traffic Count Survey

2.3.1 In view of the pandemic of COVID-19 and the regulations of group gathering (Cap. 599G), the past traffic count surveys conducted at Kam Shan Road are adopted as described in **Table 2.2** to establish the background traffic conditions in the area.

**Table 2.2 Survey Periods**

No.	Description	Date	Time
1	Chung Yeung Festival Day	28 October 2017	09:00 – 17:00
2	Shadow Weekend of Chung Yeung Festival	29 October 2017	09:00 – 17:00
3	Typical Weekend	12 November 2017	09:00 – 17:00
4	Ching Ming Festival Day	5 April 2018	09:00 – 17:00

2.3.2 With the public transport provision being identified in **Figure 2.2**, the major access route for pedestrian to/from the Subject Site are anticipated as shown in **Figure 2.3**.

2.3.3 Pedestrian head count surveys have also been conducted at the identified key footpaths and crossings during the 4 survey periods as described in **Table 2.2** to establish the current pedestrian demand.

2.3.4 Based on the Level-of-Service (“LOS”) stipulated in the Transport Planning & Design Manual (“TPDM”) as enclosed in **Appendix A**, operational assessments for the observed peak hour flow have been carried out at the identified key pedestrian links.

**Table 2.3 Existing Performance of Pedestrian Links**

Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)			
				Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Year 2017 / 2018 Existing Scenario							
P1	Kam Wo Bridge	Footbridge	5.2	1103 3.5 (A)	770 2.5 (A)	555 1.8 (A)	1532 4.9 (A)
P2	Kam Shek New Village	Footpath	2.2	721 5.5 (A)	474 3.6 (A)	311 2.4 (A)	990 7.5 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	523 8.7 (A)	159 2.7 (A)	157 2.6 (A)	895 14.9 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	572 3.2 (A)	305 1.7 (A)	331 1.8 (A)	979 5.4 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	334 22.3 (B)	126 8.4 (A)	93 6.2 (A)	896 59.7(E)

2.3.5 As shown in **Table 2.3** and **Figure 2.4**, all the identified key pedestrian links are providing a comfortable walking environment with LOS A, except during peak seasons in Ching Ming Festival and Chung Yeung Festival respectively.

- 2.3.6 In addition to footpaths, the existing pedestrian crossing at Kam Shan Road, as shown in **Figure 2.4** has been assessed with the “Suggested Warrant for Formal Pedestrian Crossing Facilities” enclosed in **Appendix B**.

**Table 2.4 Assessment for Pedestrian Crossing at Kam Shan Road**

Parameters	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Year 2017 / 2018 Existing Scenario</b>				
Pedestrian crossing P (ped/hr)	430	202	110	767
2-way Vehicular Flow V (veh/hr)	435	261	193	542
PV <sup>2</sup> (10 <sup>8</sup> )	0.81	0.14	0.04	2.25
Recommendation of Pedestrian Crossing Facility	No Formal Crossing	No Formal Crossing	No Formal Crossing	Formal Crossing Suggested

- 2.3.7 As shown in **Table 2.4**, the existing cautionary crossing is considered sufficient for the observed level of pedestrian and vehicular flows at Kam Shan Road, except during Ching Ming Festival, whilst formal crossing (i.e. zebra or signal) or traffic management measures are required due to the surge volume of vehicular and pedestrian traffic at Kam Shan Road.
- 2.3.8 In addition, the existing junction performance in the vicinity of the Subject Site is also summarised in **Table 2.5**, which is operating with ample capacity for all surveyed periods. The traffic flows and calculation details are shown in **Figure 2.5** and **Appendix C** respectively.

**Table 2.5 Existing Junction Performance in the Vicinity of the Subject Site**

Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity			
			Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Year 2017 / 2018 Existing Scenario						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.14	0.08	0.06	0.16
J3	Kam Shan Road/ Shek Lin Road	Priority	0.09	0.05	0.04	0.13

## 2.4 Existing Parking Provision

- 2.4.1 The public car parks available in vicinity of the Subject Site are illustrated in **Figure 2.6**.
- 2.4.2 The lay-by at Kam Shan Road (CP1) near the Subject Site provides a total of 7 parking spaces for Kam Shan Village, whilst most of the parking spaces are often occupied by local residents.
- 2.4.3 Instead, there is an outdoor meter parking of some 77 spaces available at Kam Shek New Village (CP2), which is some 300m further west along Kam Shan Road.
- 2.4.4 Besides, two indoor hourly public parking each with around 200 spaces, can be found in Tai Wo Estate across Lam Tsuen River off Po Nga Road (CP3) and Kai Wo Road (CP4) respectively.
- 2.4.5 To appraise the existing utilization of the above-mentioned car parks, occupancy surveys have been focused on CP2, CP3 and CP4 (excluding CP1 due to limited no. of parking spaces) during the three survey periods.

**Table 2.6 Observed Occupancy of Car Parks in the Vicinity of the Subject Site**

Survey Date and Location	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Year 2017 / 2018 Existing Scenario</b>				
<b>CP2 - Kam Shek New Village (outdoor meter parking)</b>				
Capacity	77	77	77	77
Peak Occupancy	77 (100%)	70 (91%)	60 (78%)	73 (95%)
Peak Availability	0 (0%)	7 (9%)	17 (22%)	4 (5%)
<b>CP3 - Tai Wo Estate near Po Nga Road (indoor hourly public parking)</b>				
Capacity	199	199	199	199
Peak Occupancy	199 (100%)	190 (95%)	164 (82%)	192 (96%)
Peak Availability	0 (0%)	9 (5%)	35 (18%)	7 (4%)
<b>CP4 - Tai Wo Estate near Kai Wo Road (indoor hourly public parking)</b>				
Capacity	208	208	208	208
Peak Occupancy	208 (100%)	208 (100%)	180 (87%)	197 (95%)
Peak Availability	0 (0%)	0 (0%)	28 (13%)	11 (5%)

- 2.4.6 As shown in **Table 2.6**, the public parking spaces are almost fully occupied during the festival days and the shadow weekend. However, spare parking spaces are available on the typical weekend for all 3 carparks.
- 2.4.7 Nevertheless, the kerbside utilisation along Kam Shan Road westbound has been observed on festival days, as well as the shadow weekend and the typical weekend in November 2017 respectively as shown in **Table 2.7** and **Figure 2.7**.

**Table 2.7 Observed Kerbside Utilisation along Kam Shan Road Westbound**

Survey Date and Location	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
<b>Kam Shan Road Westbound</b>				
Available Kerbside Length (m)	250	250	250	250
Equivalent Spaces Available	41	41	41	41
Peak Occupancy	40 (98%)	32 (78%)	24 (59%)	31 (76%)

- 2.4.8 Therefore, the shortage of parking provision and issues of illegal parking were mainly confined during the Festival Days, whilst the situation would be substantially improved on the Typical Weekend.

## 2.5 Observed Trip Generation for Private Columbarium Developments

2.5.1 In addition to the Subject Site, there are other private columbaria identified in the Kam Shan Road catchment. Trip generation surveys were carried out at these private columbarium developments including the Subject Site. The corresponding trip rates as observed on Festival Days, as well as the shadow weekend and typical weekend are shown in **Table 2.8**.

**Table 2.8 Observed Trip Rates for Columbarium Developments in the Vicinity**

Private Columbarium Developments in the vicinity	Approx. No. of Niches Sold & Occupied	No. of Trips (pph) in Peak Hour Peak Hour Trip Rate (pph/niche)				
		Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Shadow Weekend 02/04/2018	Ching Ming Festival 05/04/2018
Cheung Ha Ching Shea (The Subject Site)	66	20 0.303	17 0.258	14 0.212	-	300 4.545
Poh Yea Ching Shea (No. A/TP/652)	1,035	51 0.049	29 0.028	20 0.019	-	503 0.486
Buddhist Cheung Ha Temple (No. Y/TP/35)	3,816	891 0.233	393 0.103	158 0.041	-	1,706 0.447
Ling Hin Fat Yuen <sup>(1)</sup> (No. A/TP/657)	174	-	-	-	25 0.143	93 0.534
Pun Chun Yuen <sup>(2)</sup> (No. A/TP/681)	2,466	-	-	-	112 0.045	154 0.062
Everest Rest Temple <sup>(3)</sup> (No. Y/TP/36)	215	-	-	-	63 0.293	73 0.340

Note: The survey data were adopted from the "Counting Survey of Vehicular and Pedestrian Traffic conducted in 2018 Ching Ming Festival in respect of private columbaria that claimed to have pre-cut-off status" [https://www.eeb.gov.hk/food/en/press\\_and\\_publications/otherinfo/160700\\_columbarium/index.html](https://www.eeb.gov.hk/food/en/press_and_publications/otherinfo/160700_columbarium/index.html)

(1) Ling Hin Fat Yuen: (PED 42.2)

(2) Pun Chun Yuen: (PED 42.1)

(3) Everest Rest Temple: (PED 43.4)

2.5.2 It can be shown that the number of visitors and trip rates for columbarium developments are significantly higher during Ching Ming Festival than other survey periods.

2.5.3 In the absence of traffic management measures, the arrival pattern of visitors to the Subject Site is highly confined. The number of visitors during the peak hour is contributing about 30% of total visitors during the Festival day, especially the scale of the Subject Site is substantially smaller with fewer number of niches.

2.5.4 In terms of modal splits, the survey results for the private columbarium visitors along Kam Shan Road during the Festival Days, as well as the shadow weekend (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, and typical weekend in November 2017 are summarised in **Table 2.9** below:

**Table 2.9 Observed Modal Split for the Columbarium Visitors in the Vicinity**

Transport Mode	Chung Yeung Festival 28/10/2017	Shadow Weekend 29/10/2017	Typical Weekend 12/11/2017	Ching Ming Festival 05/04/2018
Private Car	7%	7%	5%	26%
Taxi	2%	2%	1%	8%
Public Transport / Walking	91%	91%	94%	66%
Total	100%	100%	100%	100%

- 2.5.5 According to the observation and interview surveys, public transport was the dominant mode for at least 66% of columbarium visitors at Kam Shan Road, given the excellent accessibility to MTR, Bus and GMB services. It is also reflected by the significant increase in pedestrian flows at Kam Wo Bridge during the Festival Days as shown in **Table 2.3**.
- 2.5.6 Although on-site parking provision in the private columbarium developments nearby Kam Shan Road is nil, there are considerable number of visitors travelling by private cars and taxis, given that the weather was unstable on the Ching Ming Festival Day.

## 2.6 Supplementary Traffic Surveys

- 2.6.1 In order to reveal the traffic conditions on “non-peak days”, supplementary traffic surveys have been conducted in September 2019 and December 2022 as summarised in **Table 2.10** below.

**Table 2.10 Supplementary Traffic Surveys – Survey Periods**

No.	Description	Date	Time
5	Typical Weekend – Saturday	21 September 2019	09:00 – 17:00
6	Typical Weekend - Sunday	22 September 2019	09:00 – 17:00
7	Typical Weekday – Thursday	15 December 2022	09:00 – 17:00
8	Typical Weekend - Sunday	18 September 2022	09:00 – 17:00

- 2.6.2 The performance of pedestrian links and crossing during the typical Saturday and Sunday are shown in **Tables 2.11 and 2.12** respectively. The corresponding location of pedestrian links and crossing are illustrated in **Figure 2.8**.

**Table 2.11 Supplementary Traffic Surveys – Performance of Pedestrian Links**

Supplementary Traffic Survey							
P1	Kam Wo Bridge	Footbridge	5.2	575 1.8 (A)	590 1.9 (A)	490 1.6 (A)	530 1.7 (A)
P2	Kam Shek New Village	Footpath	2.2	325 2.5 (A)	330 2.5 (A)	295 2.2 (A)	315 2.4 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	140 2.3 (A)	150 2.5 (A)	55 0.9 (A)	75 1.3 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	345 1.9 (A)	350 1.9 (A)	275 1.5 (A)	300 1.7 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	85 5.7 (A)	95 6.3 (A)	35 2.3 (A)	45 3.0 (A)

Note: Level-of-Service (LOS) refers to in the Transport Planning & Design Manual (TPDM) in **Appendix A**



**Table 2.12 Supplementary Traffic Surveys – Performance of Pedestrian Crossing**

Parameters	Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019	Typical Weekday - Thursday 15/12/2022	Typical Weekend - Sunday 18/12/2022
<b>Year 2019 Supplementary Traffic Survey</b>				
Pedestrian crossing P (ped/hr)	100	110	30	35
2-way Vehicular Flow V (veh/hr)	190	205	165	190
PV <sup>2</sup> (10 <sup>8</sup> )	0.04	0.05	0.01	0.01
Recommendation of Pedestrian Crossing Facility	No Formal Crossing	No Formal Crossing	No Formal Crossing	No Formal Crossing

Notes: Refers to “Suggested Warrant for Formal Pedestrian Crossing Facilities” in **Appendix B**.

2.6.3 The junction performance in the vicinity of the Subject Site is reviewed in **Table 2.13**. The traffic flows and calculation details are shown in **Figure 2.9** and **Appendix C** respectively.

**Table 2.13 Supplementary Traffic Surveys – Performance of Junctions**

Table 2.13 Supplementary Traffic Surveys – Performance of Junctions						
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity			
			Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019	Typical Weekday - Thursday 15/12/2022	Typical Weekend - Sunday 18/12/2022
Year 2019 Supplementary Traffic Survey						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.05	0.06	0.05	0.06
J3	Kam Shan Road/ Shek Lin Road	Priority	0.04	0.04	0.04	0.04

Notes: Calculation details can be referred to **Appendix C**.

2.6.4 According to the operator of the existing columbarium, most of the visitors are observed during Saturday and Sunday. The performance of road junctions, pedestrian links and pedestrian crossing in December 2022 are observed to be less critical during weekdays than during weekends. Therefore, the associated traffic impact for columbarium uses are considered insignificant during weekdays (Monday – Friday).

2.6.6 Nevertheless, the parking occupancy and kerbside utilisation have also been reviewed in **Tables 2.14 and 2.15.** and **Figure 2.10.**

**Table 2.14 Supplementary Traffic Surveys – Parking Occupancy**

Survey Date and Location	Typical Weekend - Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
<b>Year 2019 Supplementary Traffic Survey</b>		
<b>CP2 - Kam Shek New Village (outdoor meter parking)</b>		
Capacity	77	77
Peak Occupancy	58 (75%)	63 (82%)
Peak Availability	19 (25%)	14 (18%)
<b>CP3 - Tai Wo Estate near Po Nga Road (indoor hourly public parking)</b>		
Capacity	199	199
Peak Occupancy	168 (84%)	172 (86%)
Peak Availability	31 (16%)	27 (14%)
<b>CP4 - Tai Wo Estate near Kai Wo Road (indoor hourly public parking)</b>		
Capacity	208	208
Peak Occupancy	175 (84%)	178 (86%)
Peak Availability	33 (16%)	30 (14%)

**Table 2.15 Supplementary Traffic Surveys – Kerbside Utilisation**

Survey Date and Location	Typical Weekend – Saturday 21/09/2019	Typical Weekend - Sunday 22/09/2019
<b>Year 2019 Supplementary Traffic Survey – Kam Shan Road Westbound</b>		
Available Kerbside Length (m)	250	250
Equivalent Spaces Available	41	41
Peak Occupancy	22 (54%)	25 (61%)

2.6.7 In view of the above, it can be shown that the traffic conditions along Kam Shan Road are comparable between Saturday and Sunday during the typical weekend.

2.6.8 Therefore, the traffic conditions during “non-peak days” for columbarium uses can be justified by the surveys and assessments on typical weekends (including Saturday and Sunday during non-peak days).

### 3 THE PROPOSED DEVELOPMENT

#### 3.1 The Proposed Columbarium

- 3.1.1 The Subject Site is proposed for private columbarium uses with a total number of 1,700 niche units, subject to an agreement of the Town Planning Board and the subsequent approval by the Private Columbaria Licensing Board (“PCLB”).
- 3.1.2 The number of sold niche units before cut-off dated 18 June 2014 is 162, which is regarded as the baseline condition, whilst the potential traffic impact brought the “proposed” number of 1,538 niche units will be addressed by this TIA report.

#### 3.2 Traffic Management Plan

##### Introduction of ‘Visit-by-appointment’ System for Visitors

- 3.2.1 With consideration to the relatively small scale of the proposed columbarium development (a total number of 1,700 niches units), the holding capacity of the proposed columbarium development is intended to be 24 visitors.
- 3.2.2 In terms of crowd management, a ‘visit-by-appointment’ system will be adopted with 2 sessions each hour for all visitors of the proposed columbarium development at the Subject Site.
- 3.2.3 Therefore, the actual vehicular traffic generation and parking demand can be well controlled at 48 visitors per hour.

##### Promotion of Access by Public Transport

- 3.2.4 Due to its small development scale and the limitation of the site, the anticipated demand of parking for the proposed columbarium development, if any, are expected to be accommodated by the existing facilities in the vicinity.
- 3.2.5 By the time of appointment, all visitors will be notified well in advance that parking facilities will not be provided on-site. Indeed, they are expected to be travelling by public transportation facilities to and from the proposed columbarium. The alternative parking facilities in the vicinity, such as Kam Shek New Village and Tai Wo Estate, will be indicated in the Proposed Travel Plan for the visitors.
- 3.2.6 To encourage the use of the public transport, the location of public transport facilities, e.g. MTR Station, bus/GMB stops, etc. and the corresponding access routes as indicated in **Figure 2.2**. The potential impact to public transport services are estimated to be very minimal.
- 3.2.7 In view of the availability of public transport services within walking distance, shuttle services are considered not necessary for the proposed columbarium development.

#### Proposed Closure of Operation during Peak Seasons

- 3.2.8 For columbarium developments, the Ching Ming Festival and Chung Yeung Festival (i.e. the Festival Days), as well as their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, are regarded as the peak season(s) throughout a typical year.
- 3.2.9 Due to the absence of management plans for the other existing columbarium developments in the vicinity, the traffic conditions along Kam Shan Road during the peak seasons are considered critical and are assumed to continue for the worst-case scenario.
- 3.2.10 Therefore, the closure of the whole operation during the traffic peak seasons is proposed. Apart from the above mentioned peak season(s), the proposed columbarium will also be closed from the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year.
- 3.2.11 During the closure in the peak seasons, the entrance door of the subject columbarium will be completely locked up by chains with notice display.
- 3.2.12 The above closure arrangement is proposed to be included in the future purchasing agreements between the Applicants and future niche buyers; and will form part of the licensing conditions for the consideration of the PCLB.

#### Proposed Fence-off the Existing Ramp Connecting Kam Shan Road and the Subject Site

- 3.2.13 To separate the pedestrian access of columbarium visitors and the nearby residents, it is also proposed to fence-off the existing pedestrian ramp connecting Kam Shan Road to the Subject Site as shown in **Figure 3.1**.
- 3.2.14 Given that the holding capacity of the proposed columbarium development is 24 visitors for each 30-minute session, the flow rate of the subject pedestrian ramp with a minimum width of 1.5m is estimated to below 16 ped/m/session, which can be well managed at LOS A (16 ped/m/min) via the future management plan to be approved by the PCLB.

#### Proposed Holding Area for Visitors Arriving Early to the Subject Site

- 3.2.15 As part of the Traffic Management Plan, it is proposed a holding area of approximately 30m<sup>2</sup> outside the columbarium with a capacity of 25 visitors. Visitors who arrive early can complete the registration and wait in the holding area for the next session.
- 3.2.16 The average waiting space is equivalent to 1.2m<sup>2</sup> per visitor, of which the level-of service for pedestrian queuing area is LOS A with reference to Highway Capacity Manual (HCM).

### **3.3 Provision of Internal Transport Facilities**

- 3.3.1 With regard to the proposed columbarium use, the Hong Kong Planning Standards and Guidelines (HKPSG) have no recommendation on the provision of internal transport facilities.
- 3.3.2 Due to the limited footprint and inherent site constraints of the Subject Site, it is not feasible to provide any internal transport facilities within the proposed columbarium development.
- 3.3.3 The anticipated demand of parking and servicing, including pick-up/drop-off, loading/unloading, etc. are expected to be accommodated by the existing facilities in the vicinity.

## 4 TRAFFIC IMPACT ASSESSMENT

### 4.1 Design Year

- 4.1.1 Based on the operational experience from the Applicants, the number of columbarium visitors is generally the highest at the first intake and will gradually decline in the long term.
- 4.1.2 Year 2025 is assumed to be the first year of operation for the full approval by the Town Planning Board (TPB) and the PCLB. For the purpose of traffic impact assessment, the design year is assumed to be 2028 (3 years after the operation of the proposed columbarium)
- 4.1.3 Regarding the traffic forecast in terms of vehicles and pedestrians, the expected growth is derived with reference to the historical growth trend and future planning data.

#### Historical Growth Trend

- 4.1.4 The traffic counts reported in the Annual Traffic Census ("ATC") over a period of eight years inclusively between 2014 and 2021 are shown in **Table 4.1**.

**Table 4.1 ATC Counting Station Records in the Local Area**

Stn No.	Road Name	From	To	Annual Average Daily Traffic (AADT) <sup>(1)</sup>								Growth Rate <sup>(2)</sup>
				2014	2015	2016	2017	2018	2019	2020	2021	2014-2021
5009	Kwong Fuk Rd	Nam Wan Rd	Wan Tau St	20,400	20,150	20,570	20,100	20,210	19,720	18,230	19,010	-1.3%
5646	Kwong Fuk Rd	Wan Tau St	Po Heung St	15,140	14,830	14,920	14,690	15,050	14,940	13,110	14,140	-1.3%
6040	Po Heung St	Kwong Fuk Rd	Tai Po Tai Wo Rd	31,830	31,570	32,880	32,850	29,520	29,310	28,100	29,220	-1.9%
Total				67,370	66,550	68,370	67,640	64,780	63,970	59,440	62,370	-1.6%

Note:

(1) \* denotes AADT estimated by Growth Factor;

(2) The best-fitted growth is adopted by an exponential trend line (i.e.  $y = b * m^x$ ) by regression analysis; whereas the bracket value (b) represents the estimated AADT at Year 0 (i.e. 2014).

- 4.1.5 As indicated in **Table 4.1**, the annual average daily traffic ("A.A.D.T.") between 2014 and 2021 generally declines at both Kwong Fuk Road and Po Heung Street. In overall terms, the traffic growth pattern in terms of A.A.D.T. can be represented by the best-fitted trend with a negative growth rate of -1.6% per annum.

#### Population Projection in the Vicinity Area

- 4.1.6 With reference to Table 15 of the Projections of Population Distribution 2021-2029 issued by Planning Department in March 2021, of which the latest Census and Statistics Department's projections of territorial population published in September 2020 were adopted as the control totals. The population projection for 2019-2025 in the vicinity of the Subject Site are reviewed in terms of Tertiary Planning Units (TPU).



- 4.1.7 The population projection in the corresponding TPU zones between 2019 and 2025, are incorporated and summarised in **Table 4.2**.

**Table 4.2 Population Distribution Projection in terms of Tertiary Planning Unit (TPU)**

Tertiary Planning Unit (TPU)		Year 2019	Year 2020	Year 2021	Year 2022	Year 2023	Year 2024	Year 2025
7.2.3	Tai Po	76,200	74,900	74,800	74,100	73,100	72,500	71,600
Average Growth 2019-2025		-1.0% per annum						

#### Adopted Growth Rate

- 4.1.8 Taking account of the historical traffic pattern and the future population projection, a nominal growth rate of +1.0% per annum is adopted in conservative terms for the purpose of this traffic study between years 2018 and 2028, given that there will not be any substantial intakes of population and further employment in major developments along Kam Shan Road.

## 4.2 Trip Generation for Private Columbarium Developments

- 4.2.1 According to the management measure in **Section 3.2**, a ‘visit-by-appointment’ system will be adopted for all visitors of the proposed columbarium development. The maximum holding capacity of the proposed columbarium development is intended to be 24 visitors for two sessions each hour. Therefore, the number of trip generation and attraction can be well controlled at 48 visitors per hour.
- 4.2.2 Considering the scale of the proposed columbarium and the latest registration information from the Applicants, most of the current niche owners are local residents in Tai Po District. For the worst-case scenario, the modal split of Ching Ming Festival Day is adopted for potential visitors and summarised in **Table 4.3** below:

**Table 4.3 Adopted Modal Split for the Potential Columbarium Visitors**

Proposed Development		Parameters	Peak Visitor Number (pph)		
			IN	OUT	2-WAY
Cheung Ha Ching Shea (The Subject Site)		1,700 no. of niche units	48	48	96
Private Car		26%	13	13	26
Taxi		8%	4	4	8
Bus / GMB		10%	5	5	10
MTR		28%	13	13	26
Walk		28%	13	13	26
Kam Shan Road: Route P3	Private Car / Taxi:	34%	17	17	34
Kam Wo Bridge: Route P3-C1-P2-P1	MTR / Bus / GMB / Walk	42%	19	19	38
Kwong Fuk Road Route P3-C1-P4	Bus / GMB / Walk	12%	6	6	12
Kwong Fuk Road Route P3-P5	Bus / GMB / Walk	12%	6	6	12

4.2.3 Based on the traffic surveys during Chung Yeung Festival, the average occupancy rate was 3 visitors per vehicle for private cars and taxis respectively.

4.2.4 Hence, it is estimated that 5 private cars ( $13 / 3 = 4.33$ ) and 2 taxis ( $4 / 3 = 1.3$ ) will be attracted to and generated from the Subject Site respectively.

### 4.3 Operational Performance for Pedestrian Links and Road Junctions

4.3.1 The operational performance of the identified key pedestrian links was assessed with the anticipated 2028 pedestrian flows as shown in **Figures 4.1** and **4.2** respectively. The results of reference case (without Proposed Development) and design case (with Proposed Development) are summarised in **Table 4.4**.

**Table 4.4 Year 2028 Performance of Pedestrian Links**

Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)			
				Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
Year 2028 Reference Scenario (Without Development)							
P1	Kam Wo Bridge	Footbridge	5.2	1,231 3.9 (A)	859 2.8 (A)	619 2.0 (A)	1,692 5.4 (A)
P2	Kam Shek New Village	Footpath	2.2	804 6.1 (A)	529 4 (A)	347 2.6 (A)	1,094 8.3 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	583 9.7 (A)	177 3.0 (A)	175 2.9 (A)	989 16.5 (B)
P4	Kam Shan Road (E) – North side	Footpath	3.0	638 3.5 (A)	340 1.9 (A)	369 2.1 (A)	1,081 6.0 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	372 24.8 (C)	141 9.4 (A)	104 6.9 (A)	990 66.0 (E)
Year 2028 Design Scenario (With Development)							
P1	Kam Wo Bridge	Footbridge	5.2	1,269 4.1 (A)	897 2.9 (A)	657 2.1 (A)	1,730 5.5 (A)
P2	Kam Shek New Village	Footpath	2.2	842 6.4 (A)	567 4.3 (A)	385 3.0 (A)	1,132 8.6 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	679 11.3 (A)	273 4.6 (A)	271 4.5 (A)	1,085 18.1 (B)
P4	Kam Shan Road (E) – North side	Footpath	3.0	650 3.6 (A)	352 2.0 (A)	381 2.1 (A)	1,093 6.1 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	384 25.6 (C)	153 10.2 (A)	116 7.7 (A)	1002 66.8 (E)

4.3.2 With the proposed management plan, a maximum number of 24 persons will be present within the Subject Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors per hour. All of the pedestrian links in the vicinity will be operated with an acceptable performance of LOS A during shadow weekends and typical weekends, except during peak seasons in Ching Ming Festival and Chung Yeung Festival respectively.

4.3.3 The junction performance in the vicinity of the Subject Site is summarised in **Table 4.5**, which will be operating with ample capacity in Year 2028 with or without the proposed development. The traffic flows and calculation details are shown in **Figure 4.3**, **Figure 4.4** and **Appendix C** respectively.

**Table 4.5 Year 2028 Junction Performance in the Vicinity of the Subject Site**

Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity			
			Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
Year 2028 Reference Scenario (Without Development)						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.15	0.09	0.06	0.17
J3	Kam Shan Road/ Shek Lin Road	Priority	0.10	0.06	0.05	0.14
Year 2028 Design Scenario (With Development)						
J1	Pak Shing Street near Tai Wo Bridge	Signalised Pedestrian Crossing	>100%	>100%	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.16	0.09	0.07	0.18
J3	Kam Shan Road/ Shek Lin Road	Priority	0.10	0.07	0.05	0.14

- 4.3.4 In terms the cautionary crossing at Kam Shan Road, the performance for the reference and design scenario in year 2028 are reviewed in **Table 4.6**.

**Table 4.6 Year 2028 Performance of Pedestrian Crossing at Kam Shan Road**

Parameters	Chung Yeung Festival	Shadow Weekend	Typical Weekend	Ching Ming Festival
<b>Year 2028 Reference Scenario (Without Development)</b>				
Pedestrian crossing P (ped/hr)	480	225	123	847
2-way Vehicular Flow V (veh/h)	485	291	215	599
PV <sup>2</sup> (10 <sup>8</sup> )	1.13	0.19	0.06	3.04
Recommendation of Pedestrian Crossing Facility	Formal Crossing Suggested	No formal Crossing	No formal Crossing	Formal Crossing Suggested
<b>Year 2028 Design Scenario (With Development)</b>				
Pedestrian crossing P (ped/hr)	530	275	173	897
2-way Vehicular Flow V (veh/h)	499	305	229	613
PV <sup>2</sup> (10 <sup>8</sup> )	1.32	0.26	0.09	3.37
Recommendation of Pedestrian Crossing Facility	Formal Crossing Suggested	No formal Crossing	No formal Crossing	Formal Crossing Suggested

- 4.3.5 It can be shown that the existing cautionary crossing will be sufficient for the proposed columbarium development at the Subject Site during the shadow weekend and typical weekend respectively.
- 4.3.6 In the absence of management plans for the other existing columbarium developments in the vicinity of the Subject Site, formal crossing (i.e. zebra or signal) or crowd control measures will be required during the Festival Days due to the surge volume of vehicular and pedestrian traffic at Kam Shan Road.
- 4.3.7 To avoid the critical traffic conditions during peak seasons, it is proposed to **close the operation during Ching Ming and Chung Yeung Festivals**, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days. The proposed columbarium will also be closed from **the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year**.

#### 4.4 Availability of Parking Facilities in the Vicinity

- 4.4.1 Given the excellent provision of public transport services at Kam Shan Road, the modal split of vehicular traffic for the proposed columbarium development is anticipated to be fairly low.
- 4.4.2 Also, with consideration to the relatively small scale of the proposed columbarium development (a total number of 1,700 niches units), the actual vehicular traffic generation and attraction are estimated to be very minimal at only 5 private cars and 2 taxis per hour in one direction.
- 4.4.3 Recalling the recent observation and traffic surveys as shown in **Tables 2.5** and **2.6**, it is revealed that the shortage of parking provision and issues of illegal parking were mainly confined during the Festival Days, due to the absence of management plans for the columbarium developments in the vicinity.
- 4.4.4 During typical weekends, however, there are plenty of vacant parking spaces available at Kam Shek New Village and Tai Wo Estate respectively.
- 4.4.5 With the proposed traffic management plan in **Section 3.2** (including closure during peak seasons, promotion of public transport and admission control to 48 visitors per hour), the provision of parking facilities in the vicinity are sufficient for the minimal parking demand by the Subject Site.

#### 4.5 Other Private Columbarium Developments in the Vicinity

- 4.5.1 In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road. The details and location of these private columbaria are summarised in **Table 4.7** and **Figure 4.5**.

**Table 4.7 Private Columbarium Developments in the Vicinity**

Private Columbarium Developments	Latest Planning Application	Application Status	No. of Units under Application	
			Columbarium Niches	Memorial Tablets
Cheung Ha Ching Shea (The Subject Site)	Y/TP/37	Ongoing	1,700	-
Poh Yea Ching Shea	A/TP/657	Approved on 29/11/2019	5,302	174
Buddhist Cheung Ha Temple	Y/TP/35	Ongoing	11,726	3,049
Ling Hin Fat Yuen	A/TP/652	Approved with conditions on 19/07/2019	757	1,782
Pun Chun Yuen	A/TP/681	Approved with conditions on 26/08/2022	3,834	-
Everest Rest Temple	Y/TP/36	Ongoing	3,507	-
Total			26,826	5,005

Source: Statutory Planning Portal 2, Town Planning Board (<https://www2.ozp.tpb.gov.hk/>)

- 4.5.2 In terms of cumulative traffic impact for the columbarium developments in the vicinity, a nominal rate of +1.0% per annum between years 2018 and 2028 has been assumed as described in **Section 4** to represent the gradual intakes of “sold and unoccupied niches” and the planned private columbarium developments in the vicinity, given that there will not be any substantial intakes of population and employment by major developments along Kam Shan Road.
- 4.5.3 To account for the full-intakes of the “sold niches before cut-off” and the planned private columbarium developments in the vicinity as the worst-case scenario, a sensitivity test is included in **Appendix D** to examine the corresponding pedestrian links and road junctions.
- 4.5.4 With the proposed closure of operation during Ching Ming and Chung Yeung Festivals, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, the critical traffic conditions during peak seasons at Kam Shan Road will no longer be correlated to the Subject Site. The proposed columbarium will also be closed from the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year.
- 4.5.5 Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons per each 30-minute session will be allowed within the Subject Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors per hour. The level-of-service of the self-contained pedestrian ramp can also be well managed at LOS A (16 ped/m/min).
- 4.5.6 Therefore, the traffic conditions during typical weekends (including Saturday and Sunday during non-peak days) will be operated at acceptable level with the proposed columbarium development at the subject site and the proposed management measures.



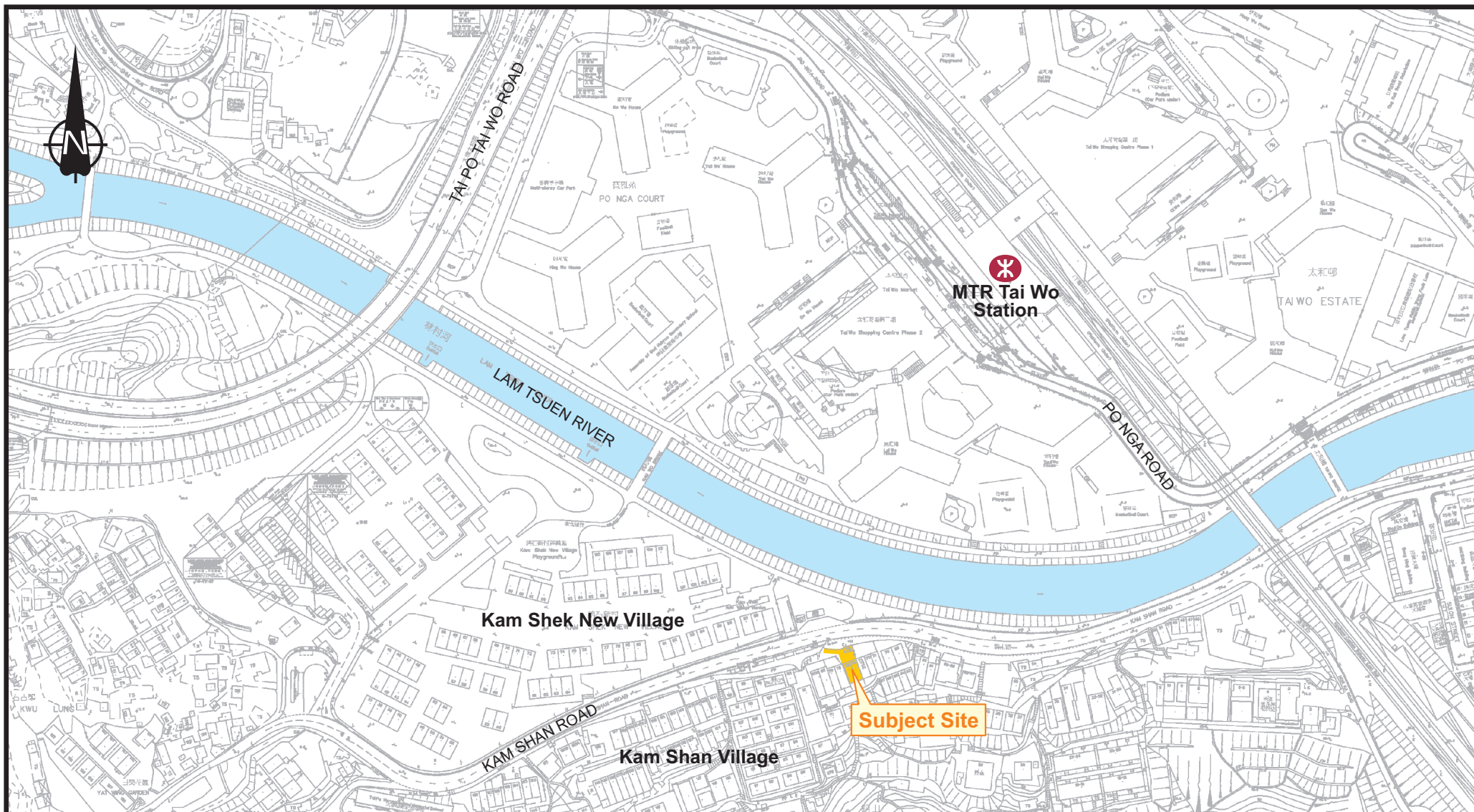
## 5 SUMMARY AND CONCLUSION


### 5.1 Summary

- 5.1.1 This report is prepared to present the TIA findings and the proposed traffic management for an existing private columbarium development proposed at the Subject Site (covering Nos. 74-75 Kam Shan Road and adjoining Government Land, Tai Po). A total number of 1,700 niche units (i.e. 162 sold and 1,538 new niches) are proposed.
- 5.1.2 The number of sold niche units before cut-off dated 18 June 2014 is 162, which is regarded as the baseline condition, whilst the potential traffic impact brought by the “proposed” number of 1,538 niche units will be addressed by this TIA report.
- 5.1.3 Vehicular access of the proposed columbarium development is provided by Kam Shan Road, which is a single two-lane carriageway south of Lam Tsuen River.
- 5.1.4 In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road, such as Buddhist Cheung Ha Temple, Poh Yea Ching Shea, Ling Hin Fat Yuen, Pun Chun Yuen and Everest Rest Temple.
- 5.1.5 For columbarium developments, it is revealed from traffic surveys that the Ching Ming Festival and Chung Yeung Festival (i.e. the Festival Days), as well as their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days, are regarded as the peak season(s) throughout a typical year.
- 5.1.6 Due to the absence of management plans for the other existing columbarium developments in the vicinity, the traffic conditions along Kam Shan Road are considered critical during the peak seasons.
- 5.1.7 On the other hand, the traffic conditions along Kam Shan Road are acceptable during typical weekends (including Saturday and Sunday).
- 5.1.8 To avoid the critical traffic conditions during peak seasons, it is proposed to **close the operation during Ching Ming and Chung Yeung Festivals**, and their shadow weekends (i.e. any Saturday and Sunday) and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days. The proposed columbarium will also be closed from **the 1<sup>st</sup> to 14<sup>th</sup> day of the Lunar New Year**.
- 5.1.9 During the closure in the peak seasons, the entrance door of the subject columbarium will be completely locked up by chains with notice display.
- 5.1.10 Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons per 30-minute visit session will be permitted within the Subject Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled in 48 visitors each hour during the operation hours.
- 5.1.11 In order to minimise the potential nuisances to its nearby dwellings, the Applicants have decided to fence off the existing pedestrian ramp serving the Subject Site in the form of a self-contained entrance alley to serve the proposed rezoning development.

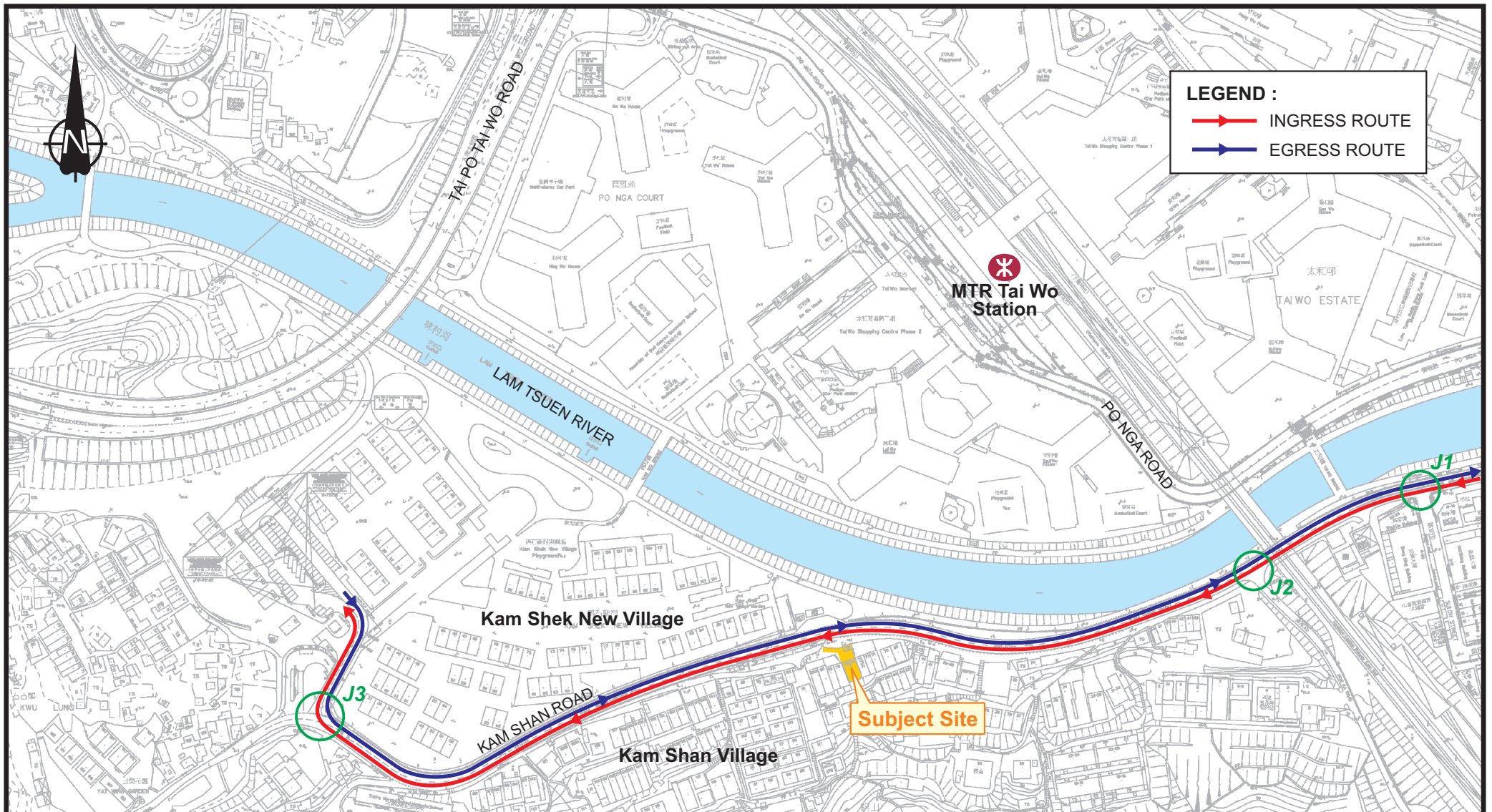
- 5.1.12 The level-of-service of the subject pedestrian ramp with a minimum width of 1.5m would be well managed at LOS A (16 ped/m/min) via the future management plan to be approved by the PCLB.
- 5.1.13 As part of the Traffic Management Plan, it is proposed a holding area of approximately 30m<sup>2</sup> for visitors waiting outside the columbarium. The average waiting space is equivalent to 1.2m<sup>2</sup> per visitor, of which the level-of service for pedestrian queuing area is LOS A with reference to Highway Capacity Manual (HCM).
- 5.1.14 Parking will not be provided on-site. Access by public transport is highly encouraged by the proposed management plan, where the available public transport facilities, e.g. MTR Station, bus/GMB stops, etc. and the corresponding access routes will be clearly informed to all visitors.
- 5.1.15 Nevertheless, the cumulative traffic impact for the columbarium developments in the vicinity have been incorporated by a nominal growth rate accounting for the gradual intakes of “sold and unoccupied niches”. The full intakes of planned niches in the vicinity have also been accounted by a sensitivity test as the worst-case scenario.
- 5.1.16 In view of the assessment results with the implementation of the proposed management measures, the local traffic conditions during typical weekends (including Saturday and Sunday) will be operated at acceptable level.
- 5.1.17 Therefore, it can be concluded that the proposed columbarium development with a total of 1,700 niche units in this part of the Kam Shan area is considered acceptable in traffic engineering and planning terms.

## FIGURES

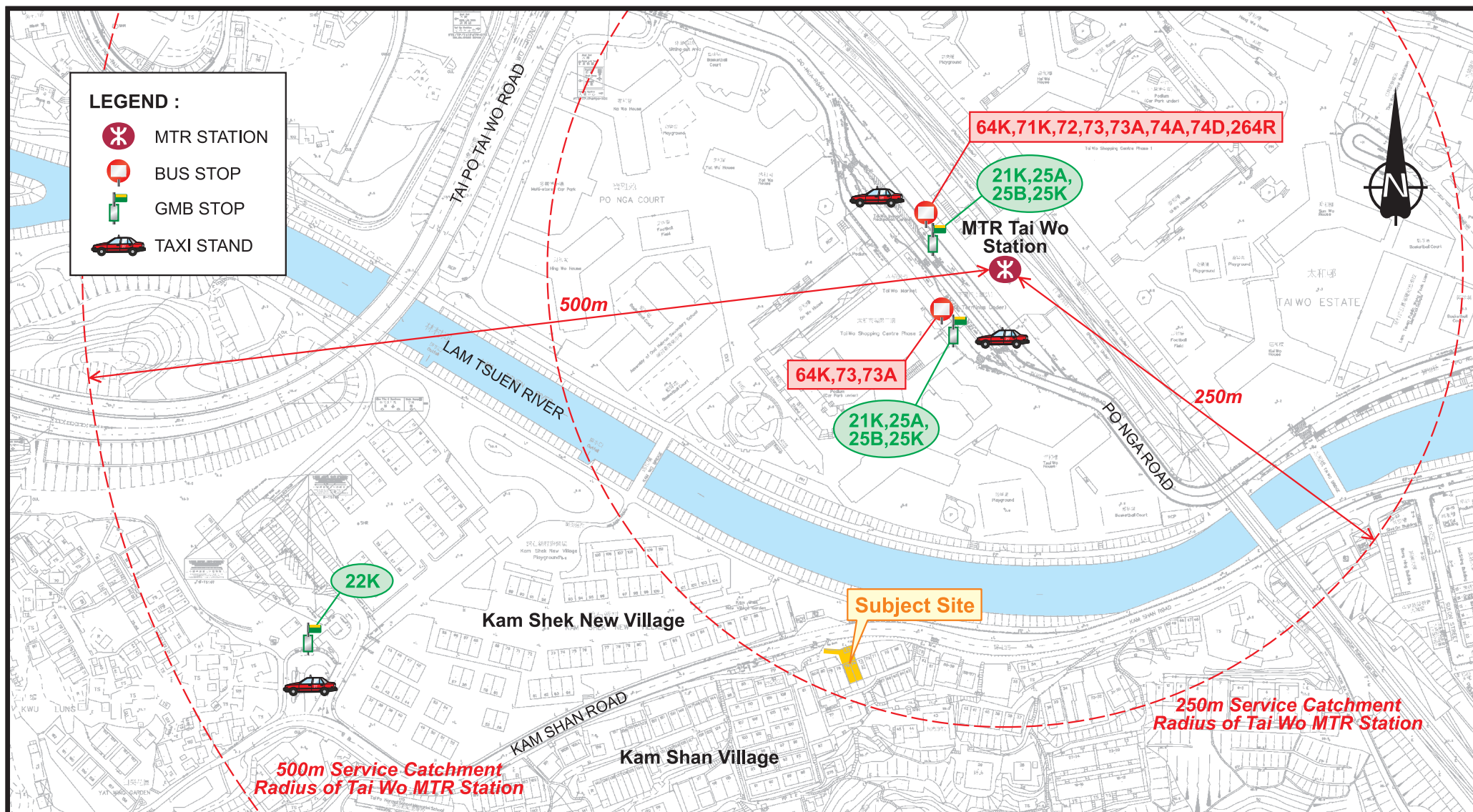


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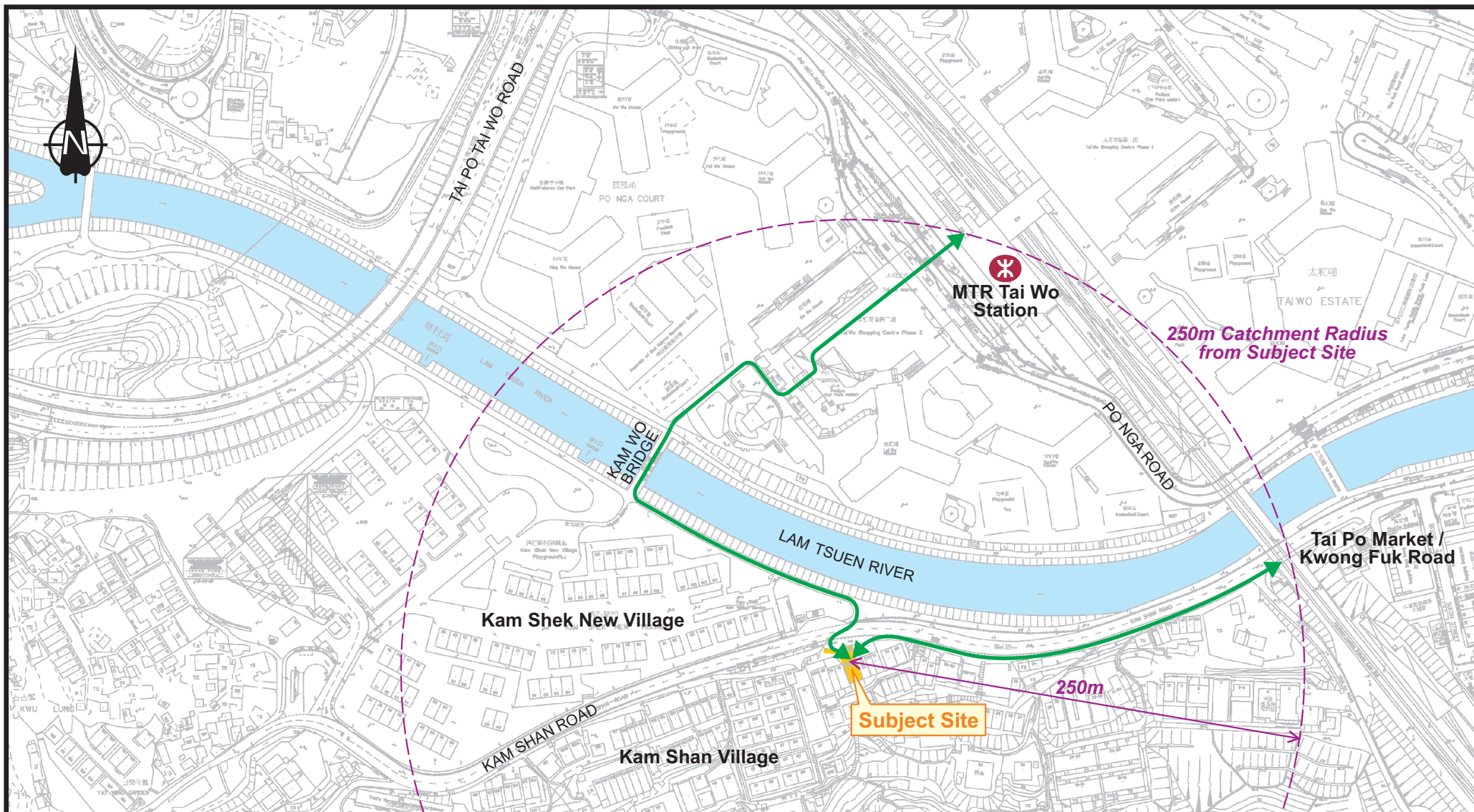


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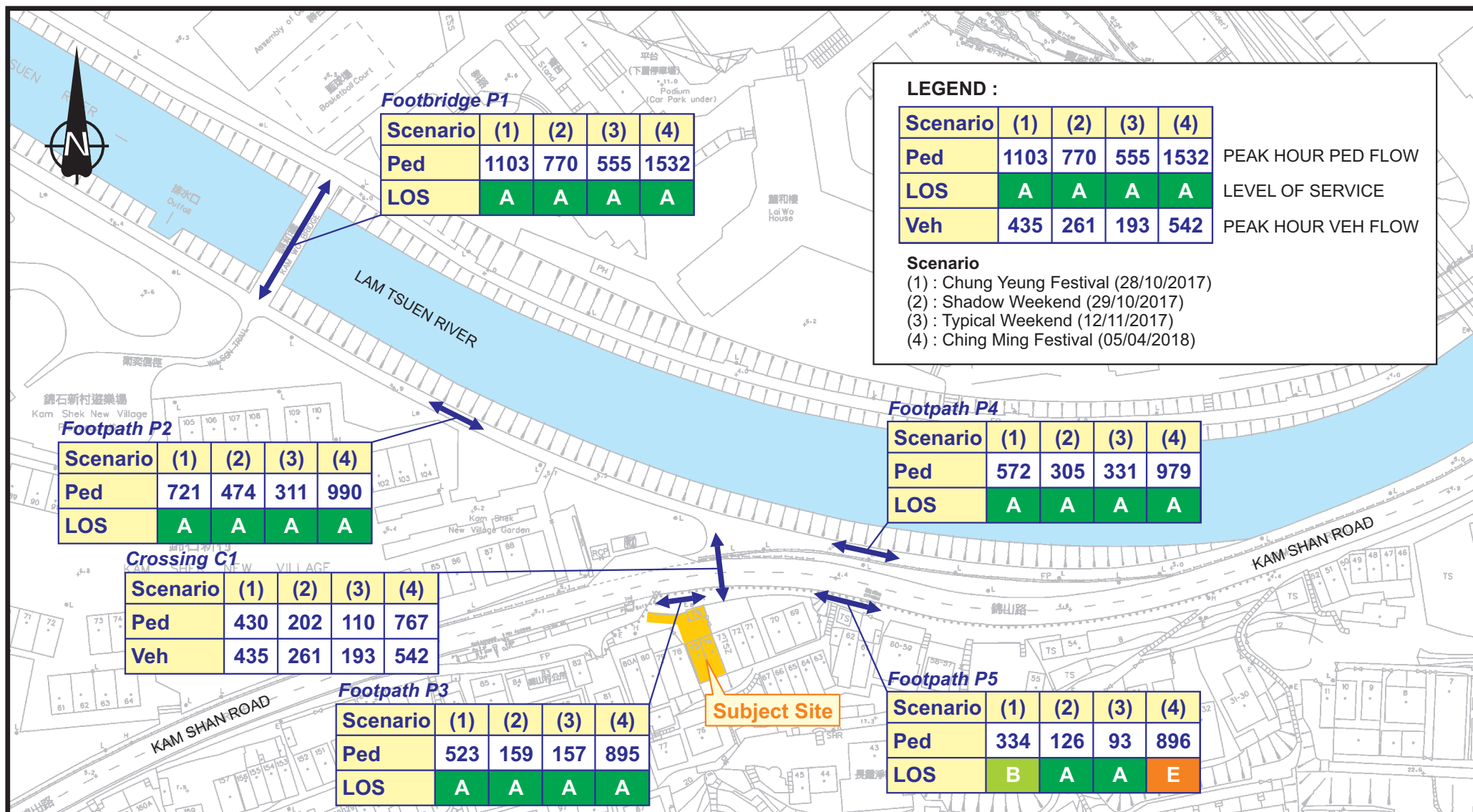
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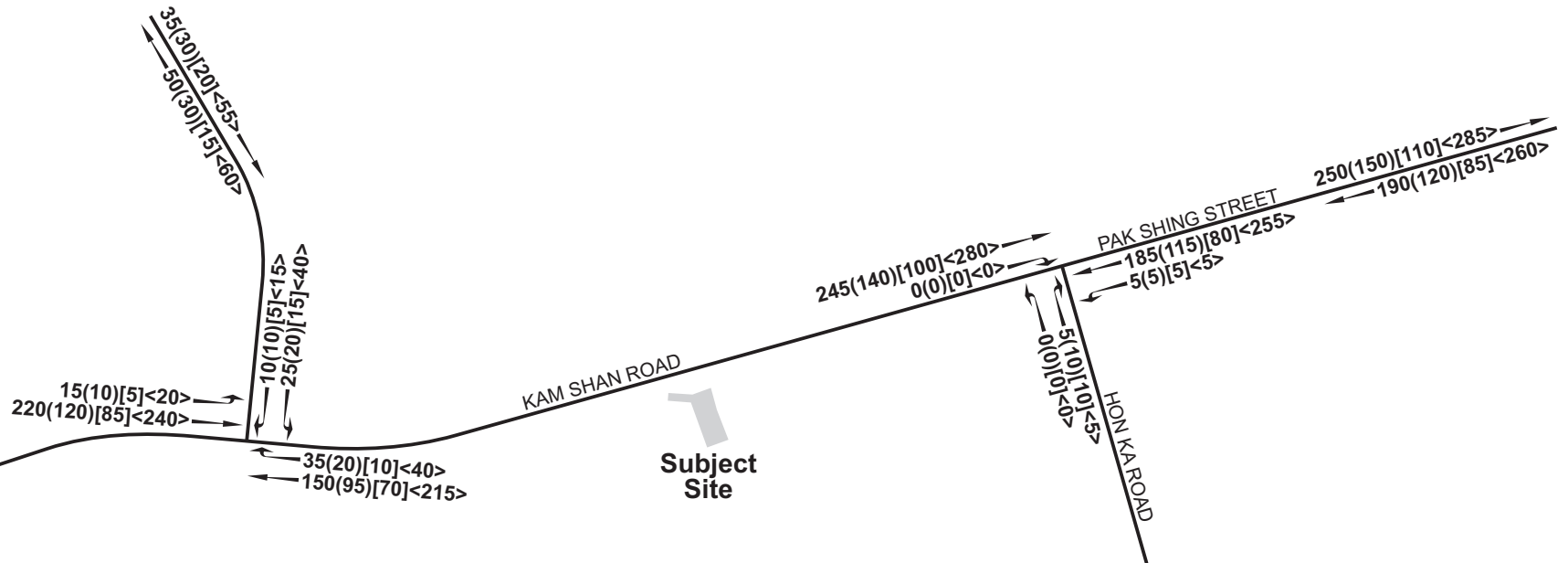


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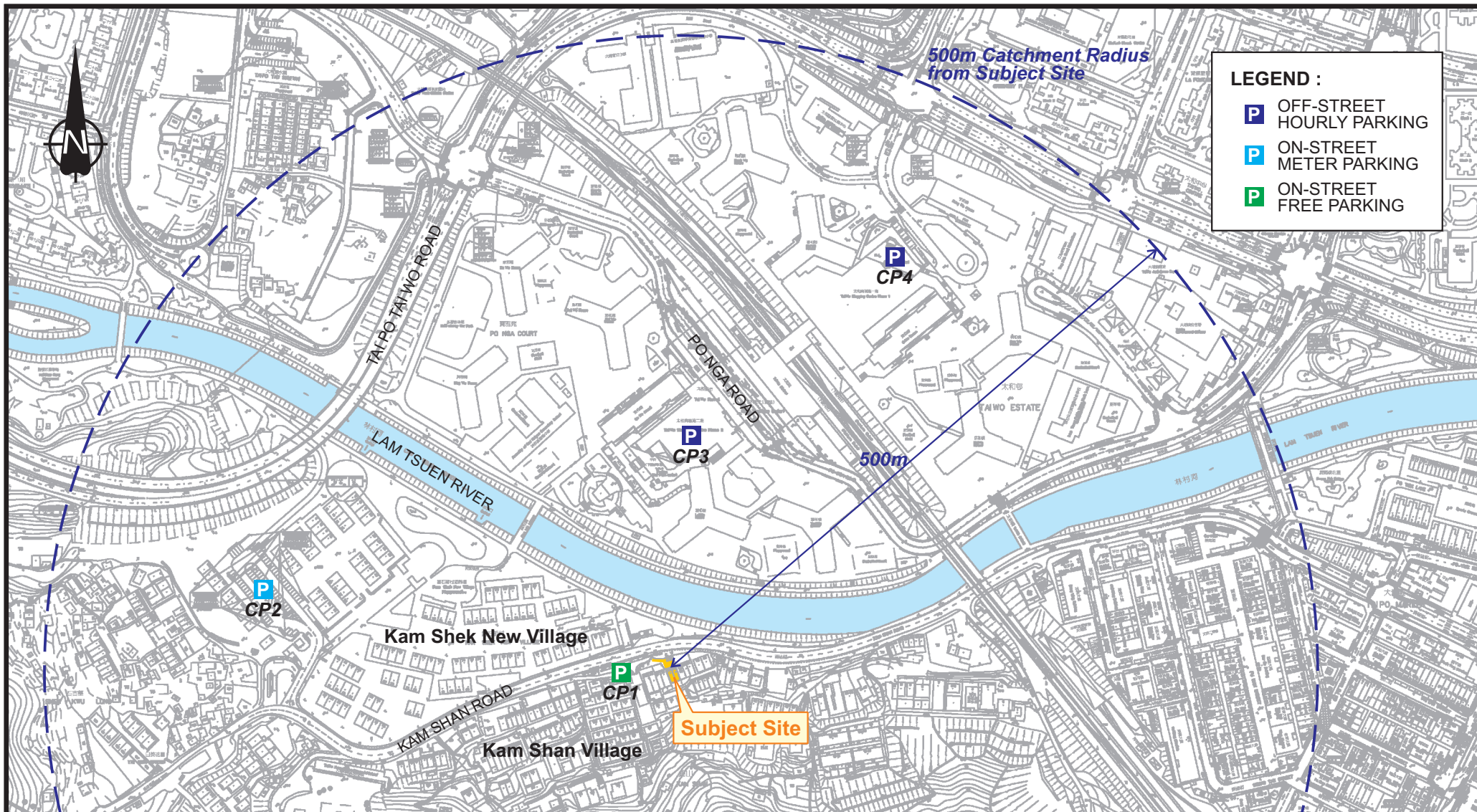


#### LEGEND :

- 185 CHUNG YEUNG PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (115) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [80] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <255> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

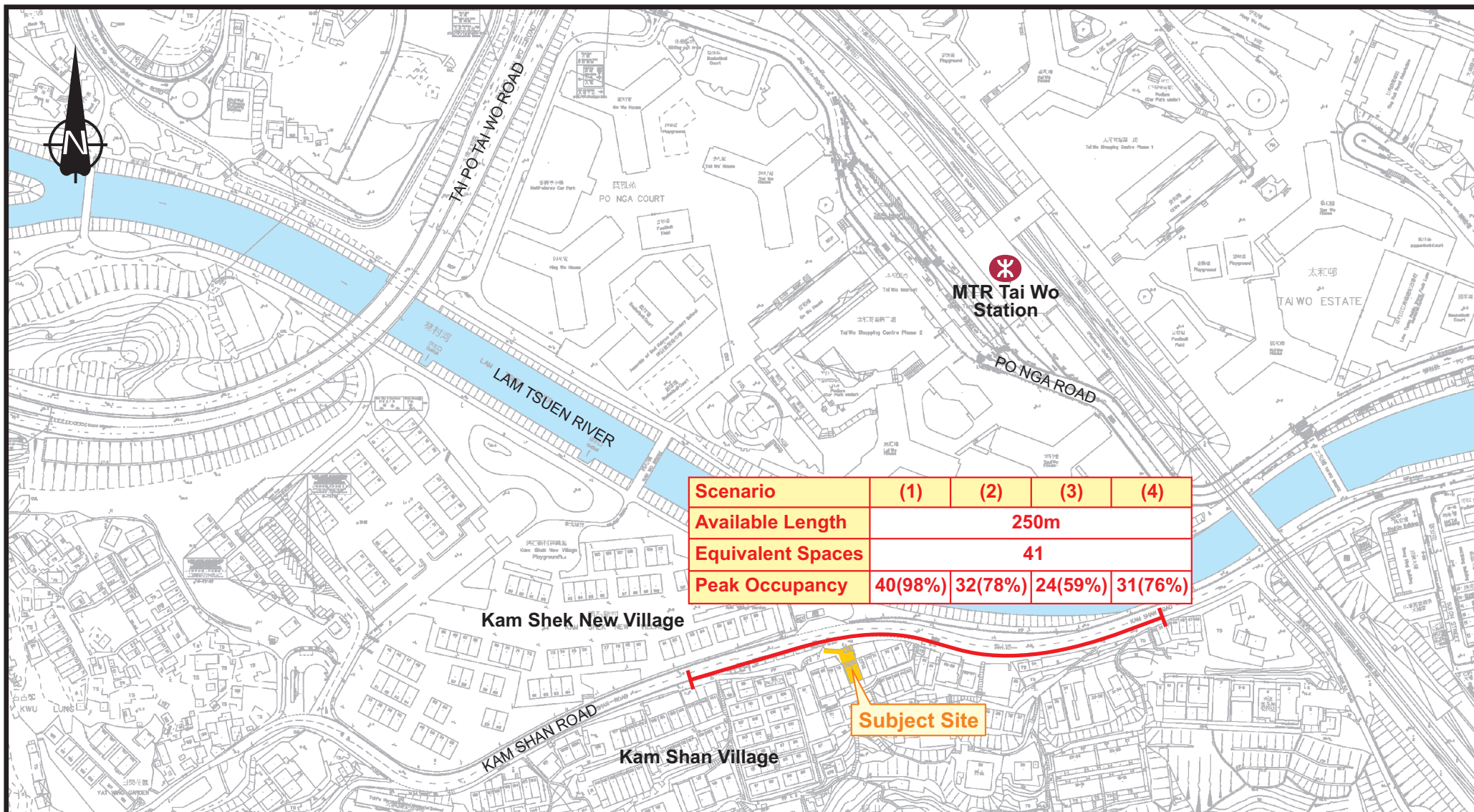
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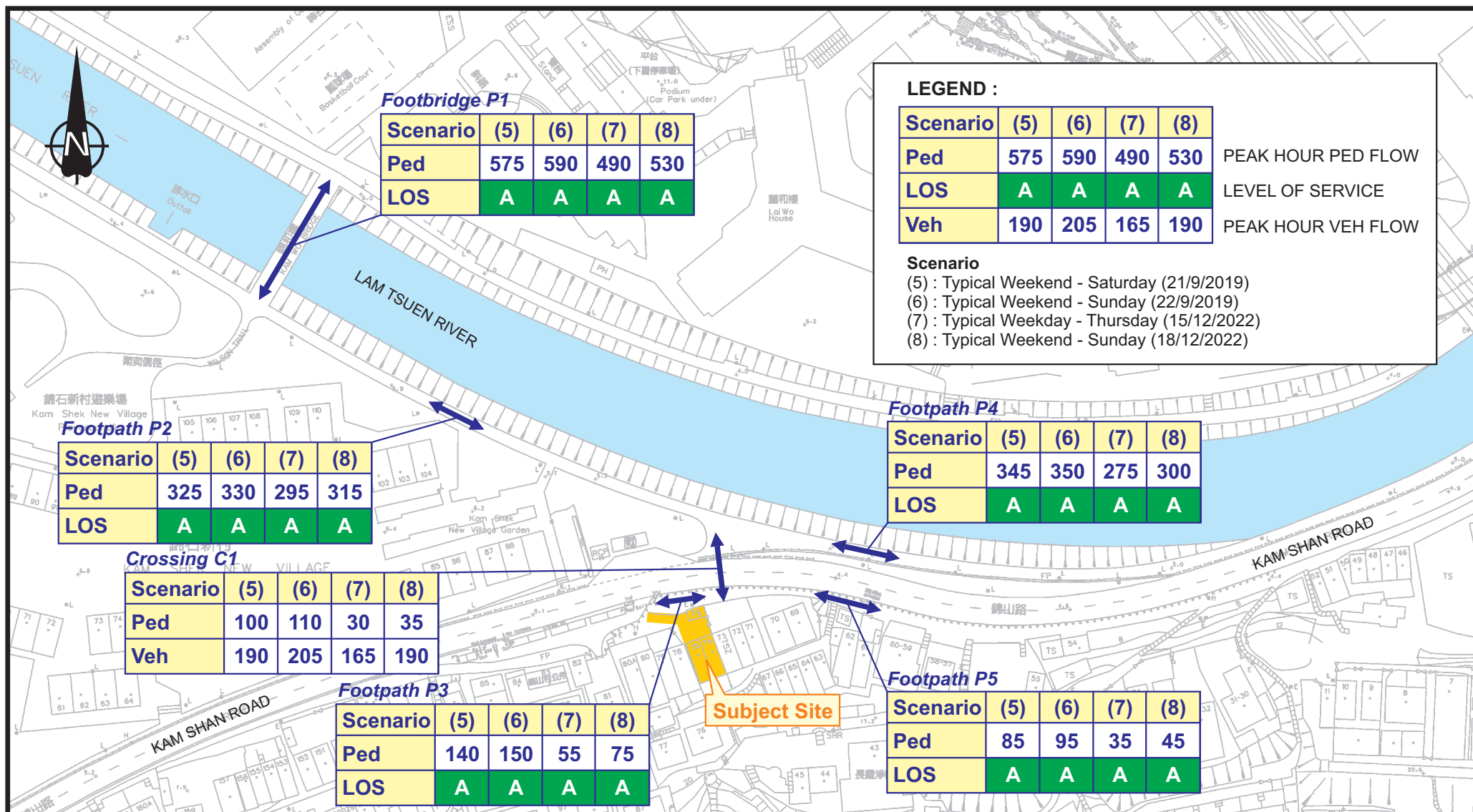


Scenario	(1)	(2)	(3)	(4)
Available Length	250m			
Equivalent Spaces	41			
Peak Occupancy	40(98%)	32(78%)	24(59%)	31(76%)

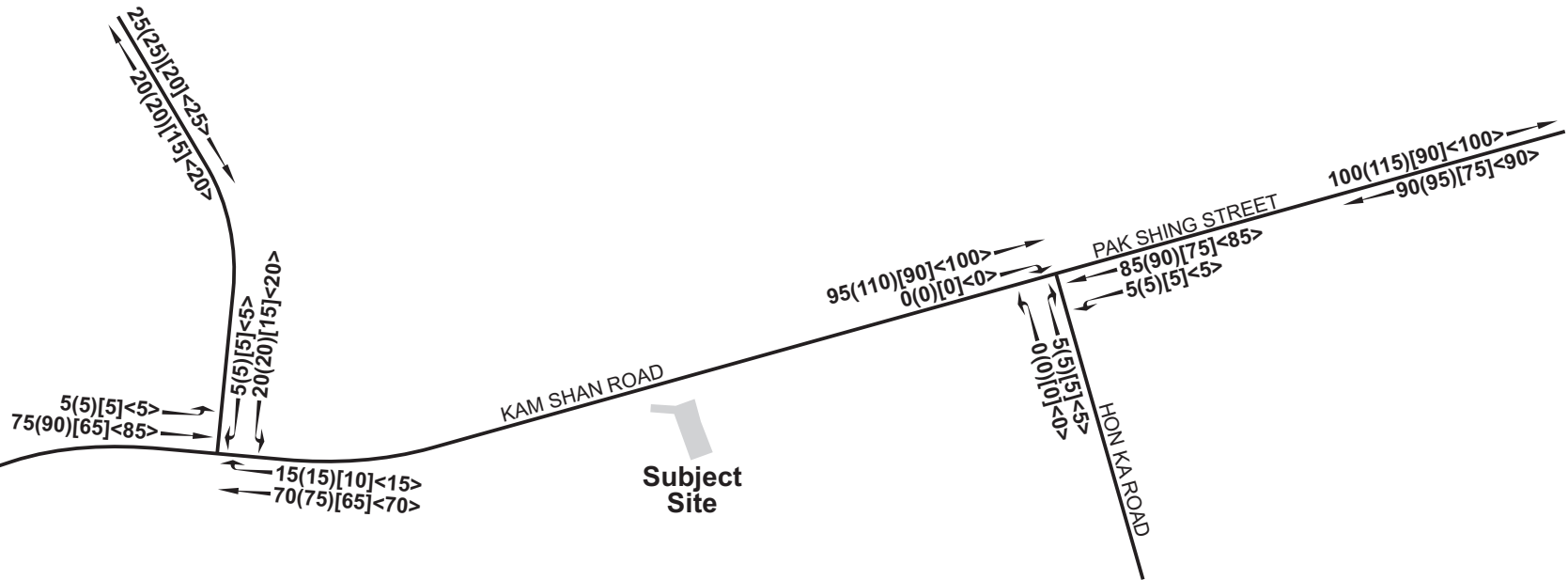
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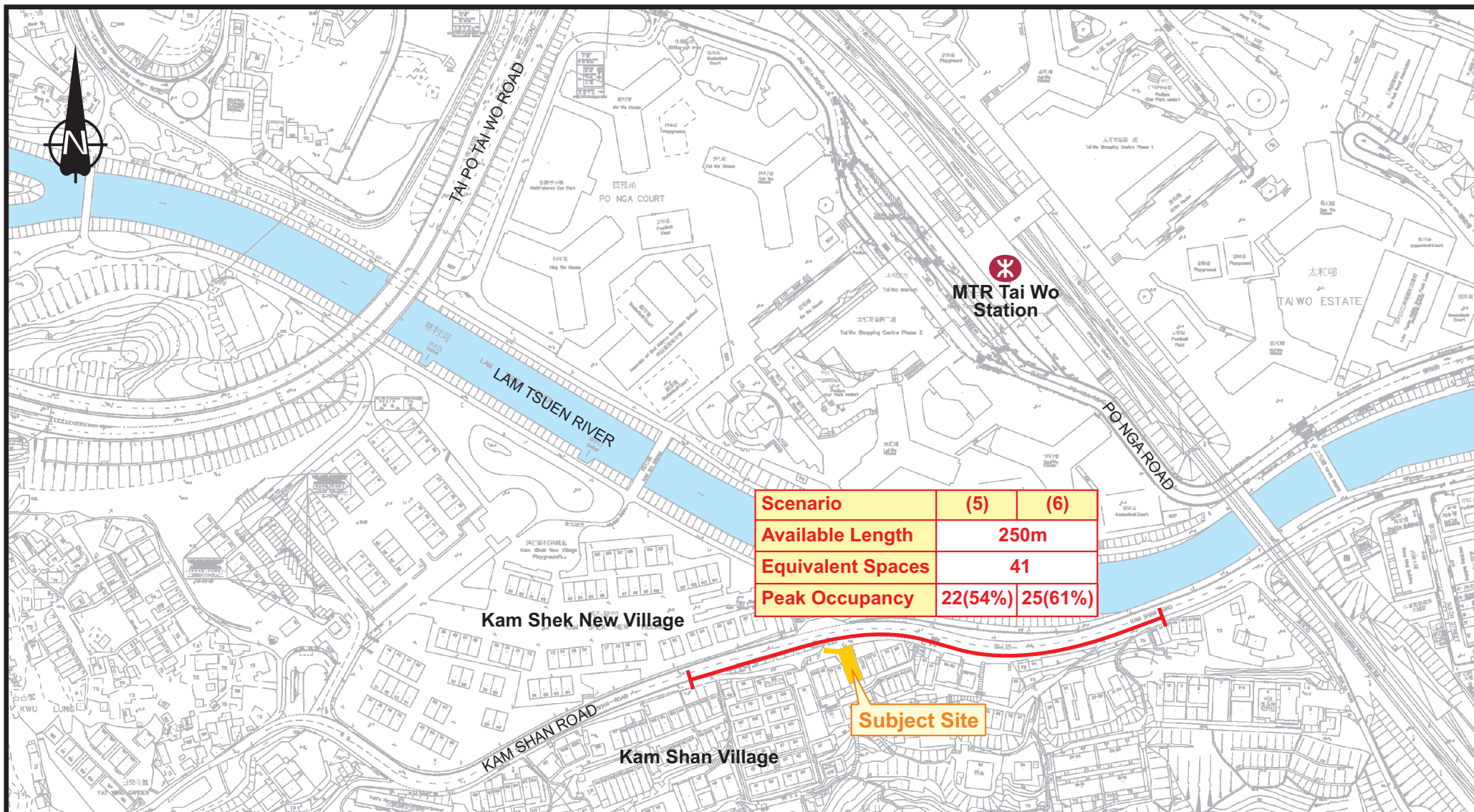
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Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022	Drawing No.	2.8	Rev.	-




#### LEGEND :

- 85 TYPICAL WEEKEND - SATURDAY (21/9/2019) PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (90) TYPICAL WEEKEND - SUNDAY (22/9/2019) PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [75] TYPICAL WEEKDAY - THURSDAY (15/12/2022) PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <85> TYPICAL WEEKEND - SUNDAY (18/12/2022) PEAK HOUR TRAFFIC FLOW (PCU/HR)

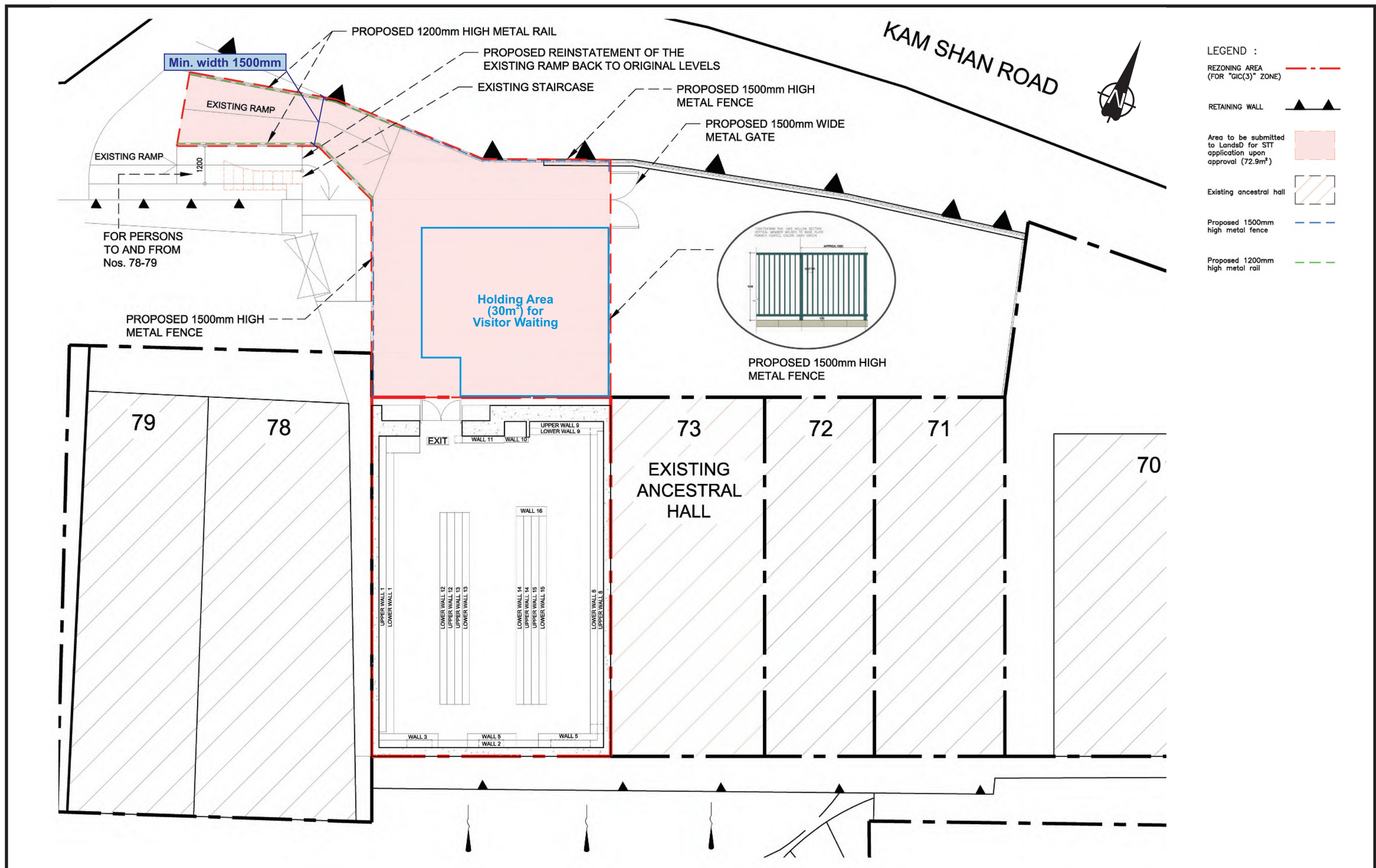
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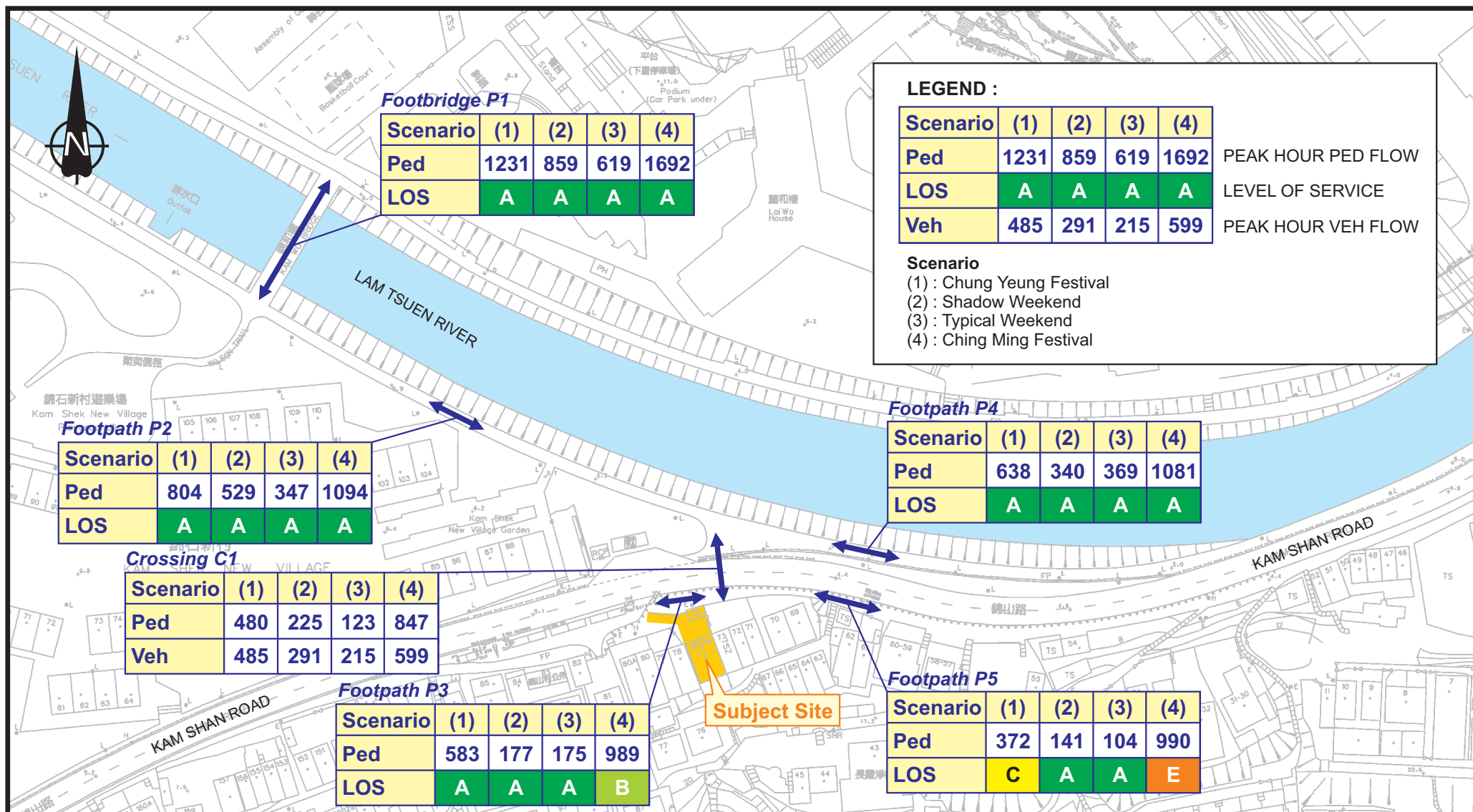
Scenario	(5)	(6)
Available Length	250m	
Equivalent Spaces	41	
Peak Occupancy	22(54%)	25(61%)

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Project Title				Drawing Title							
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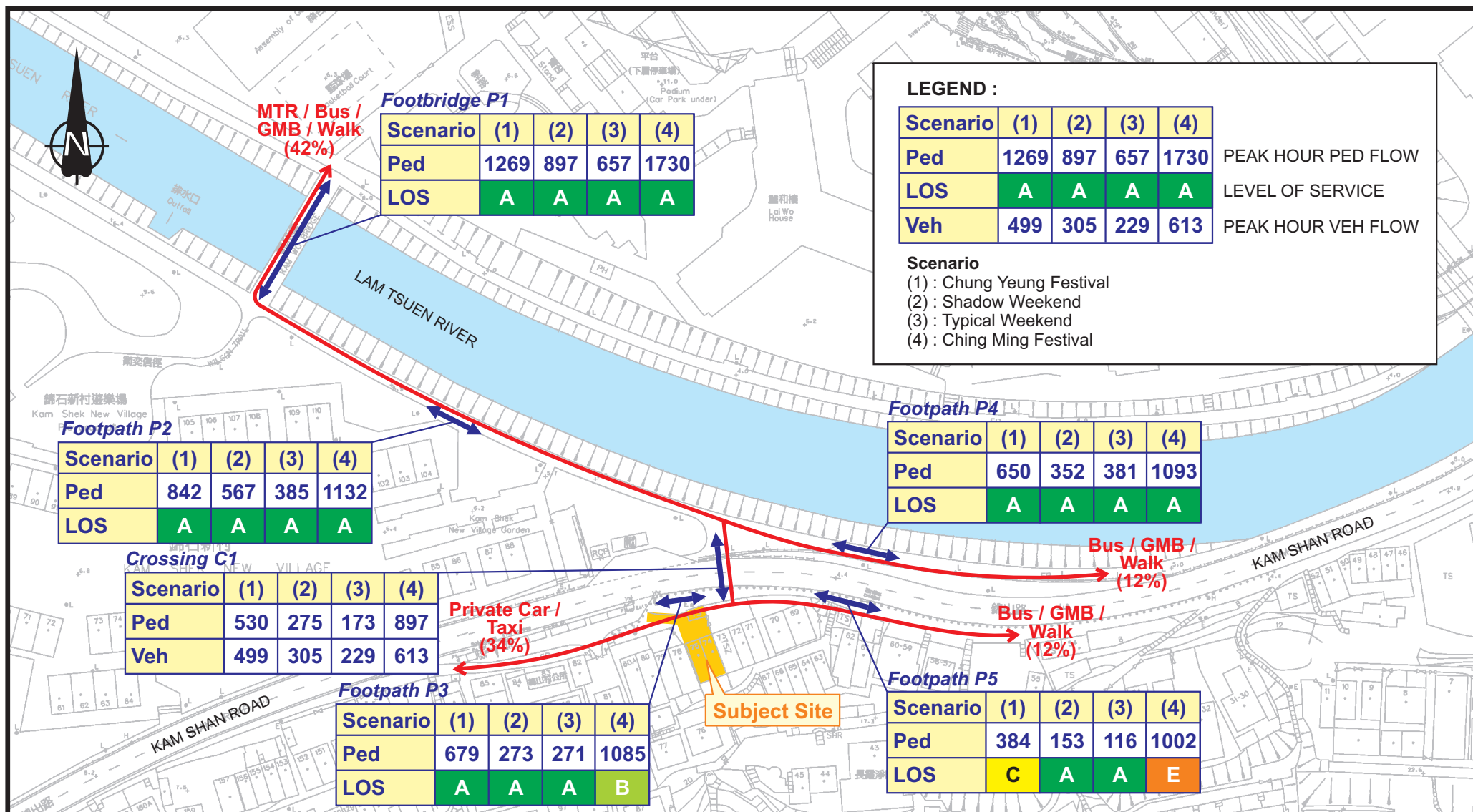


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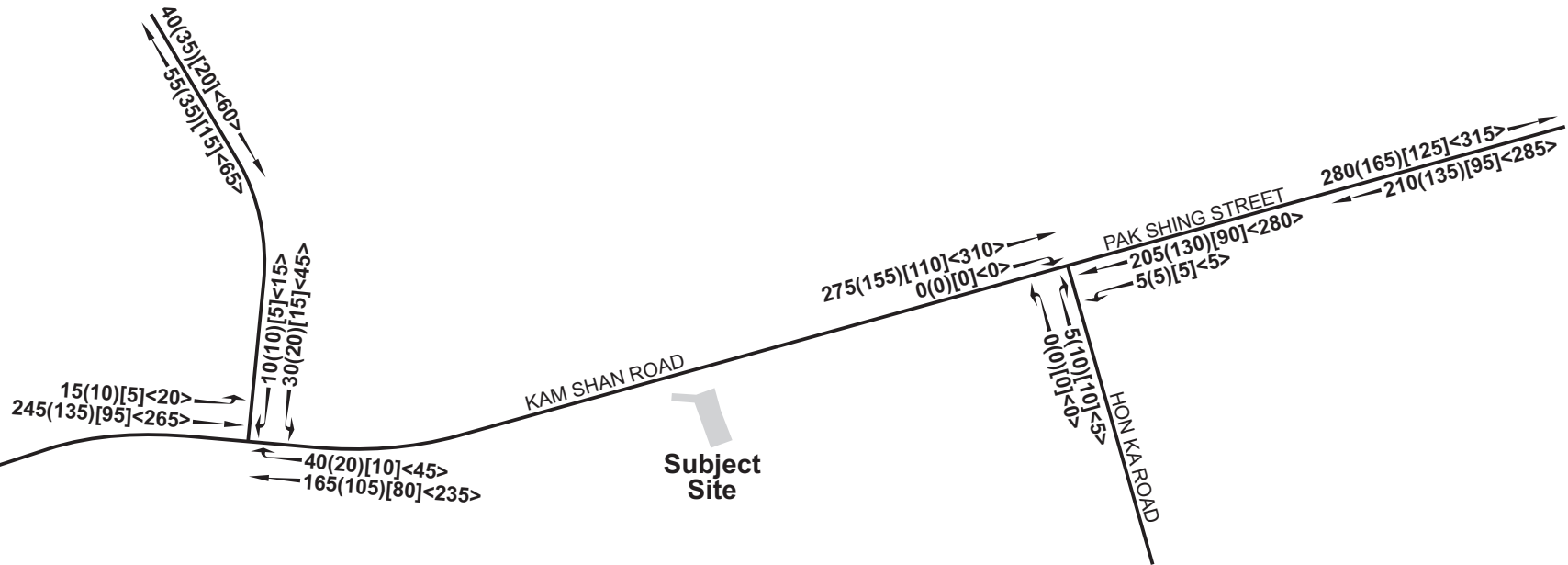


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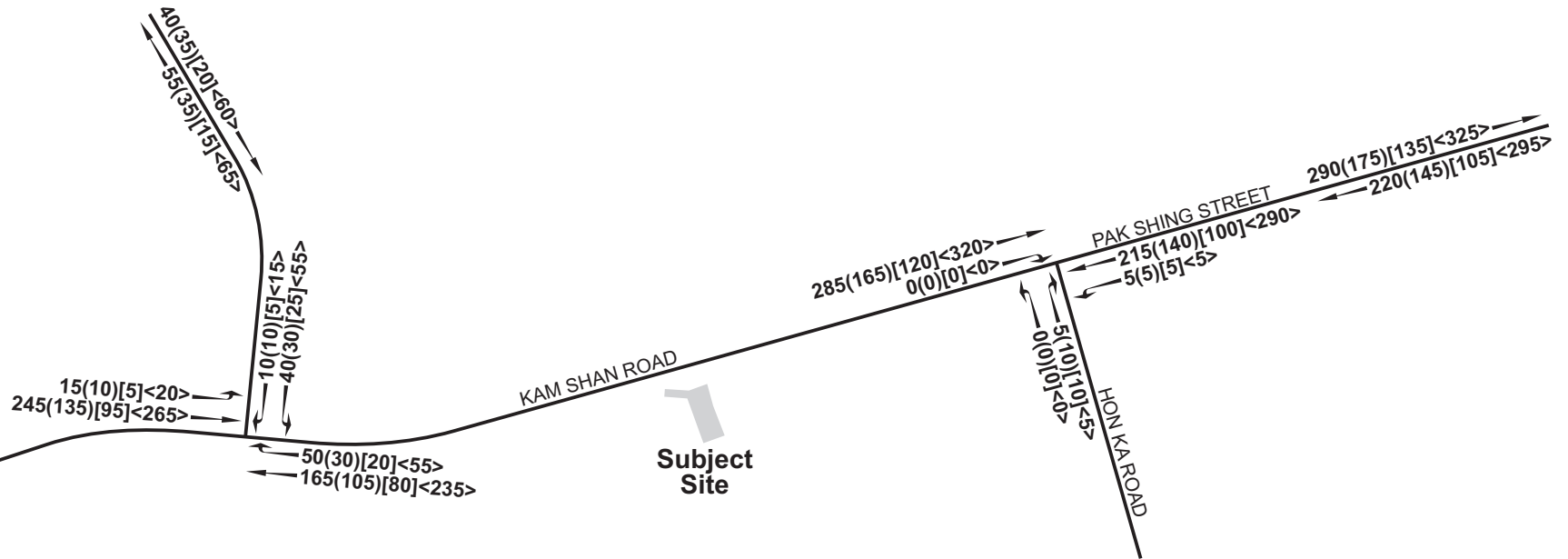
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Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022	Drawing No.	4.2	Rev.	-



**LEGEND :**


- 205 CHUNG YEUNG PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (130) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [90] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <280> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

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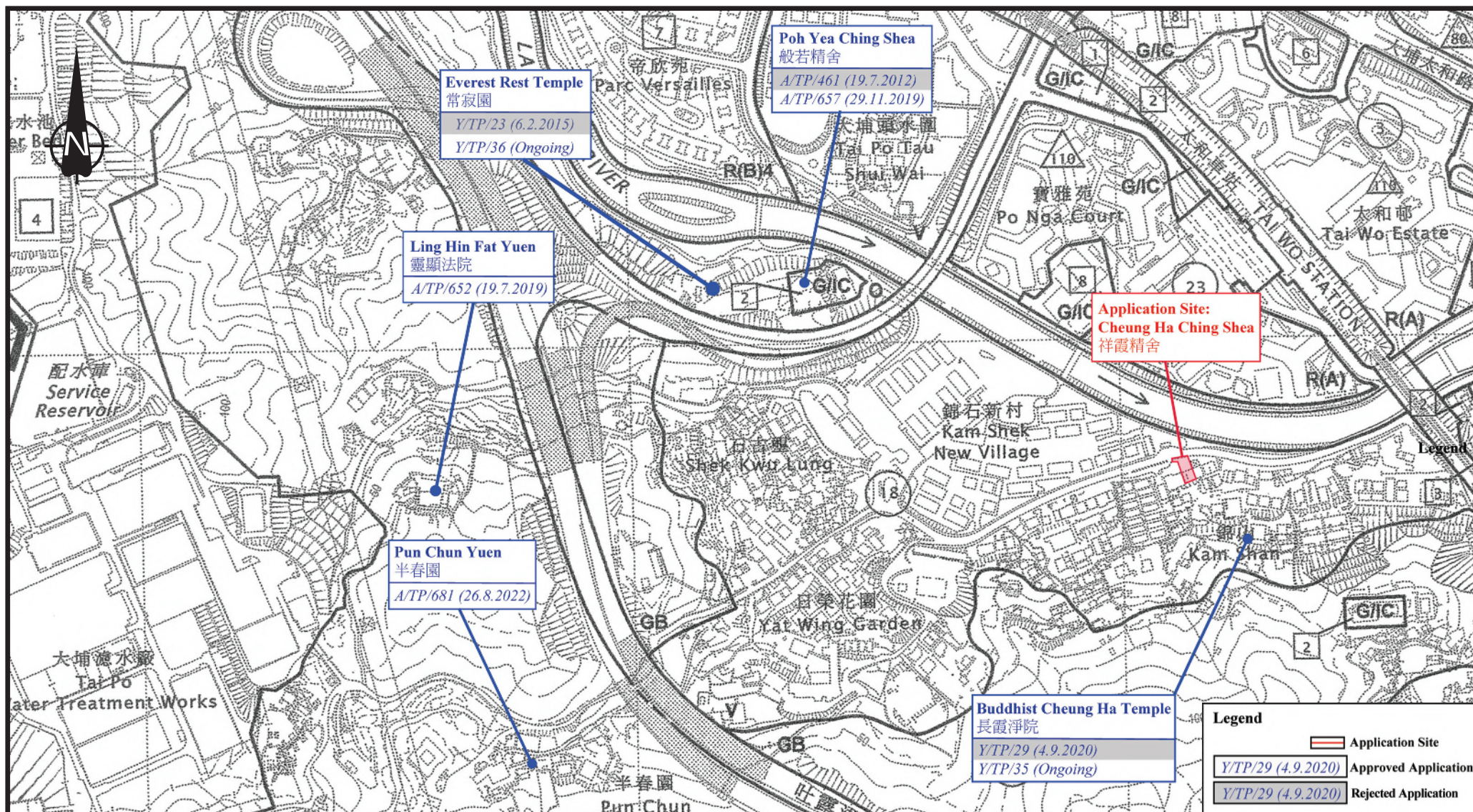


#### LEGEND :

- 215 CHUNG YEUNG PEAK HOUR TRAFFIC FLOW (PCU/HR)
- (140) SHADOW WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- [100] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)
- <290> CHING MING PEAK HOUR TRAFFIC FLOW (PCU/HR)

-	-	-	-	-	-	-	-	-	-	-	
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 DESIGN TRAFFIC FORECAST							
				Designed	TLY	Checked	LHW	Scale	NTS		











Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				PRIVATE COLUMBARIUM DEVELOPMENTS IN THE VICINITY							
Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022	Drawing No.	4.5	Rev.	-



## APPENDIX A

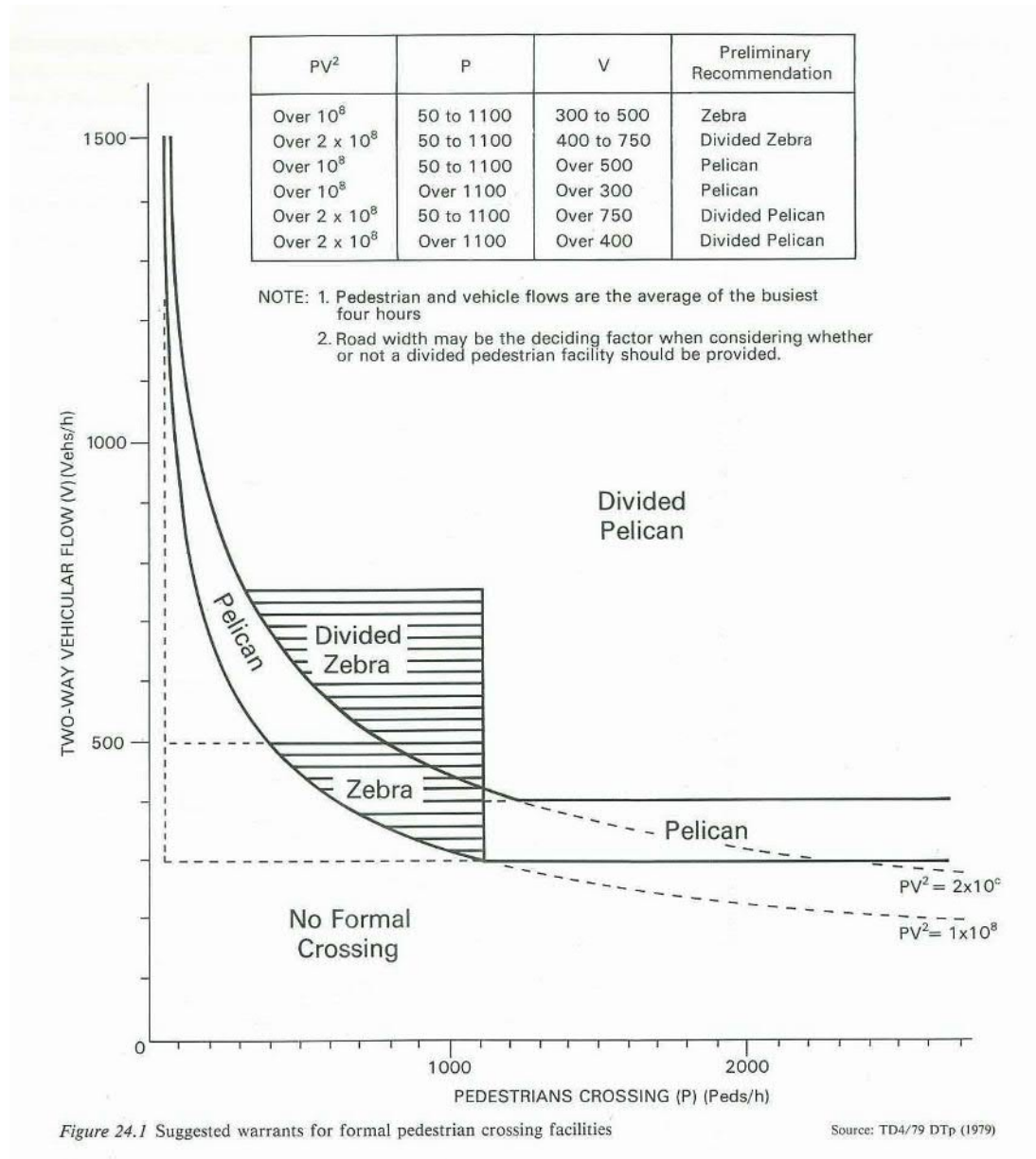


## APPENDIX A Definition of Level-of-Service (LOS) in Pedestrian Walkway Systems

LOS	Flow Rate (ped/min/m)	Description	Illustration
A	$\leq 16$	Pedestrians move in desired paths without altering their movements in response to other pedestrians. Walking speeds are freely selected, and conflicts between pedestrians are unlikely.	
B	16 – 23	Sufficient space is provided for pedestrians to freely select their walking speeds, to bypass other pedestrians and to avoid crossing conflicts with others. At this level, pedestrians begin to be aware of other pedestrians and to respond to their presence in the selection of walking paths.	
C	23 – 33	Sufficient space is available to select normal walking speeds and to bypass other pedestrians primarily in unidirectional stream. Where reverse direction or crossing movement exist, minor conflicts will occur, and speed and volume will be somewhat lower.	
D	33 – 49	Freedom to select individual walking speeds and bypass other pedestrians is restricted. Where crossing or reverse-flow movements exist, the probability of conflicts is high and its avoidance requires changes of speeds and position. The LOS provides reasonable fluid flow; however considerable friction and interactions between pedestrians are likely to occur.	
E	49 – 75	Virtually, all pedestrians would have their normal walking speeds restricted. At the lower range of this LOS, forward movement is possible only by shuffling. Space is insufficient to pass over slower pedestrians. Cross-and reverse-movement are possible only with extreme difficulties. Design volumes approach the limit of walking capacity with resulting stoppages and interruptions to flow.	
F	$> 75$	Walking speeds are severely restricted. Forward progress is made only by shuffling. There are frequent and unavoidable conflicts with other pedestrians. Cross-and reverse-movements are virtually impossible. Flow is sporadic and unstable. Space is more characteristics of queued pedestrians than of moving pedestrian streams.	

## APPENDIX B

## APPENDIX B Suggested Warrants for Formal Pedestrian Crossing Facilities



(Reference: Roads and Traffic in Urban Areas, Chapter 24 Facilities for Pedestrians, Institution of Highways and Transportation, Department of Transport)

## APPENDIX C

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2017

Description: 2017 Observed - Chung Yeung Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	250	0.124	0.124	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	190	0.094		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 250</div><div>← 190</div></div> <div>↑ N</div>	Group		A,Bp	Group		
		y		0.124	y		
		L (sec)		22	L (sec)		
		C (sec)		44	C (sec)		
		y pract.		0.450	y pract.		
		R.C. (%)		263%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	



TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2017

Description: 2017 Observed - Shadow Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	150	0.074	0.074	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	120	0.060		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 150</div><div>← 120</div></div> <div>↑ N</div>										Group		A,Bp	Group		
											y		0.074	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		505%	R.C. (%)		

Stage / Phase Diagrams				
1.	2.	3.	4.	5.
<div>A →</div> <div>← A</div>	<div>↑</div> <div>Bp</div> <div>↓</div>			

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2017

Description: 2017 Observed - Typical Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	110	0.055	0.055	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	85	0.042		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>↑ N</div><div>→ 110</div><div>85 ←</div></div>										Group		A,Bp	Group		
											y		0.055	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		724%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2018

Description: 2018 Observed - Ching Ming Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak			Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
					Left	Right		Peak		Peak										
Pak Shing St EB	↑	A	1	4.000						2015	2015			285	0.141	0.141		0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015			260	0.129			0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =			5	+	10	=	15					*				*
Notes:				<div>Flow: (pcu/hr)</div> <div><div><div>→ 285</div><div>← 260</div></div><div>↑ N</div></div>								Group		A,Bp	Group					
												y		0.141	y					
												L (sec)		22	L (sec)					
												C (sec)		44	C (sec)					
												y pract.		0.450	y pract.					
												R.C. (%)		218%	R.C. (%)					
Stage / Phase Diagrams																				
1.				2.				3.				4.				5.				
<div>A → ← A</div>				<div>↑ Bp ↓</div>																
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=				
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=				

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2019

Description: 2019 Supplementary - Typical Weekend (Saturday)

Designed By: TLY

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SAT							
					Left	Right		SAT		SAT		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St EB	↑	A	1	4.000						2015	2015	100	0.050	0.050	0	0.000	0.000		
Pak Shing St WB	↑	A	1	4.000						2015	2015	90	0.045		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*		
Notes:				Flow: (pcu/hr)								<div><div><div>↑ N</div><div>→ 100</div><div>90 ←</div></div></div>							
												Group		A,Bp	Group				
												y		0.050	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		807%	R.C. (%)				
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
<div>A → ← A</div>				<div>↑ Bp ↓</div>															
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=			
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=			

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2019

Description: 2019 Supplementary - Typical Weekend (Sunday)

Designed By: TLY

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SUN					
					Left	Right		SUN		SUN		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	115	0.057	0.057	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	95	0.047		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div></div><div>→ 115</div><div>95 ←</div></div> <div><div>↑ N</div></div>		Group		A,Bp	Group		
			y		0.057	y		
			L (sec)		22	L (sec)		
			C (sec)		44	C (sec)		
			y pract.		0.450	y pract.		
			R.C. (%)		688%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A →</div> <div>← A</div>				<div>↑</div> <div>Bp</div> <div>↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	



TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2022

Description: 2022 Supplementary - Typical Weekend (Thursday)

Designed By: TLY

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SAT							
					Left	Right		SAT		SAT		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St EB	↑	A	1	4.000						2015	2015	90	0.045	0.045	0	0.000	0.000		
Pak Shing St WB	↑	A	1	4.000						2015	2015	75	0.037		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*		
Notes:				Flow: (pcu/hr)								<div><div><div>↑ N</div><div>→ 90</div><div>75 ←</div></div></div>							
												Group		A,Bp	Group				
												y		0.045	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		908%	R.C. (%)				
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
<div>A → ← A</div>				<div>↑ Bp ↓</div>															
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=			
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=			

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2022

Description: 2022 Supplementary - Typical Weekend (Sunday)

Designed By: TLY

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		SUN							
					Left	Right		SUN		SUN		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St EB	↑	A	1	4.000						2015	2015	100	0.050	0.050	0	0.000	0.000		
Pak Shing St WB	↑	A	1	4.000						2015	2015	90	0.045		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5		+	10	=	15			*			*		
Notes:				<div>Flow: (pcu/hr)</div> <div><div>→ 100</div><div>90 ←</div></div> <div>↑ N</div>								Group		A,Bp	Group				
												y		0.050	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		807%	R.C. (%)				
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
<div>A → ← A</div>				<div>↑ Bp ↓</div>															
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=			
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=			

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Chung Yeung Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	280	0.139	0.139	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	210	0.104		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 280</div><div>← 210</div></div> <div>↑ N</div>										Group		A,Bp	Group		
											y		0.139	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		224%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Shadow Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	165	0.082	0.082	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	135	0.067		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div><div></div><div>165</div></div><div><div>135</div><div></div></div></div> <div><div>↑</div><div>N</div></div>	Group		A,Bp	Group		
		y		0.082	y		
		L (sec)		22	L (sec)		
		C (sec)		44	C (sec)		
		y pract.		0.450	y pract.		
		R.C. (%)		450%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Typical Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak							
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y		
Pak Shing St EB	↑	A	1	4.000						2015	2015	125	0.062	0.062	0	0.000	0.000		
Pak Shing St WB	↑	A	1	4.000						2015	2015	95	0.047		0	0.000			
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*		
Notes:				Flow: (pcu/hr)								<div><div><div>↑ N</div><div>→ 125</div><div>95 ←</div></div></div>							
												Group		A,Bp	Group				
												y		0.062	y				
												L (sec)		22	L (sec)				
												C (sec)		44	C (sec)				
												y pract.		0.450	y pract.				
												R.C. (%)		625%	R.C. (%)				
Stage / Phase Diagrams																			
1.				2.				3.				4.				5.			
<div>A → ← A</div>				<div>↑ Bp ↓</div>															
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=			
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=			

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)



TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Ching Ming Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	315	0.156	0.156	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	285	0.141		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 315</div><div>← 285</div></div> <div>↑ N</div>										Group		A,Bp	Group		
											y		0.156	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		188%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Chung Yeung Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	290	0.144	0.144	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	220	0.109		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 290</div><div>← 220</div></div> <div>↑ N</div>										Group		A,Bp	Group		
											y		0.144	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		213%	R.C. (%)		

Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Shadow Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	175	0.087	0.087	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	145	0.072		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>↑ N</div><div>→ 175</div><div>← 145</div></div>										Group		A,Bp	Group		
											y		0.087	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		418%	R.C. (%)		

Stage / Phase Diagrams				
1.	2.	3.	4.	5.
<div>A →</div> <div>← A</div>	<div>↑</div> <div>Bp</div> <div>↓</div>			

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Typical Weekend

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak			Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
					Left	Right		Peak		Peak										
Pak Shing St EB	↑	A	1	4.000						2015	2015			135	0.067	0.067		0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015			105	0.052			0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5		+	10	=	15					*				*
Notes:				Flow: (pcu/hr)								<div>↑ N</div>		Group		A,Bp	Group			
				<div>→ 135</div>										y		0.067	y			
				<div>← 105</div>										L (sec)		22	L (sec)			
														C (sec)		44	C (sec)			
														y pract.		0.450	y pract.			
														R.C. (%)		572%	R.C. (%)			
Stage / Phase Diagrams																				
1.				2.				3.				4.				5.				
<div>A → ← A</div>				<div>↑ Bp ↓</div>																
I/G= 3				I/G= 5	15			I/G=				I/G=				I/G=				
I/G= #N/A	#N/A			I/G= #N/A	#N/A			I/G=				I/G=				I/G=				

Date: FEB, 2023

Junction: Pak Shing Street near Sui On Street (J1)

TRAFFIC SIGNALS CALCULATION

Job No.: CHK50650510

MVA HONG KONG LIMITED

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Ching Ming Festival

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Peak					
					Left	Right		Peak		Peak		Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y

Pak Shing St EB	↑	A	1	4.000						2015	2015	325	0.161	0.161	0	0.000	0.000
Pak Shing St WB	↑	A	1	4.000						2015	2015	295	0.146		0	0.000	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:	<div>Flow: (pcu/hr)</div> <div><div>→ 325</div><div>← 295</div></div> <div>↑ N</div>										Group		A,Bp	Group		
											y		0.161	y		
											L (sec)		22	L (sec)		
											C (sec)		44	C (sec)		
											y pract.		0.450	y pract.		
											R.C. (%)		179%	R.C. (%)		

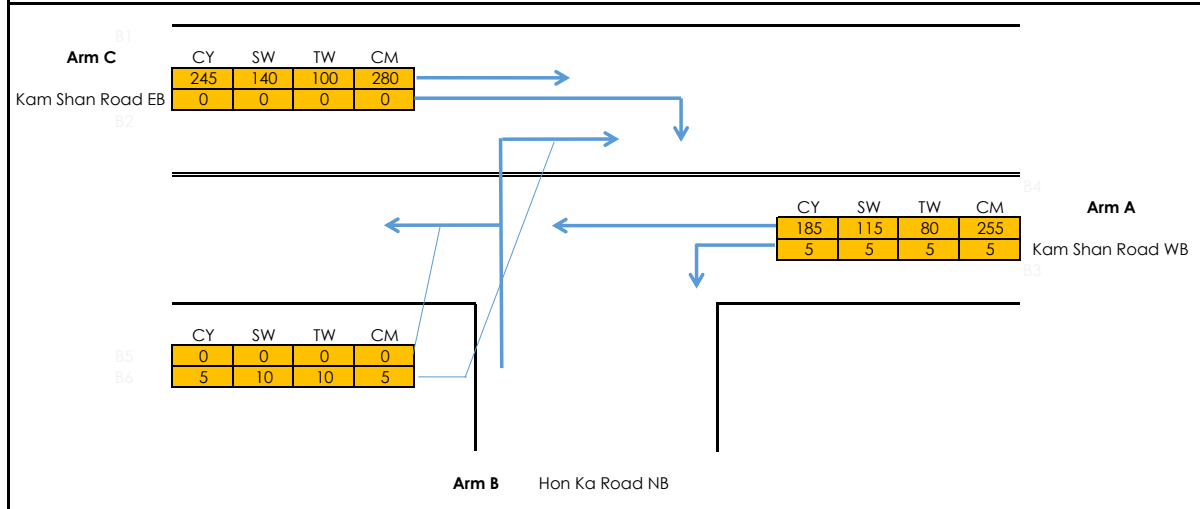
Stage / Phase Diagrams																	
1.				2.				3.				4.				5.	
<div>A → ← A</div>				<div>↑ Bp ↓</div>													

I/G= 3		I/G= 5	15	I/G=		I/G=		I/G=	
I/G= #N/A	#N/A	I/G= #N/A	#N/A	I/G=		I/G=		I/G=	
Date: FEB, 2023								Junction: Pak Shing Street near Sui On Street (J1)	



## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	TLY
Junction:	(J2) Kam Shan Road/ Hon Ka Road	Checked by:	LHW
Scheme:	Existing	Date:	Dec-22
Design Year:	2017 (CY, SW, CM); 2018 (CM)	Job No.:	CHK50650510
Arm A:	Kam Shan Road WB		
Arm B:	Hon Ka Road NB		
Arm C:	Kam Shan Road EB		



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.8433
	VI(b-a)	50		E	0.8972
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.7482

### ANALYSIS

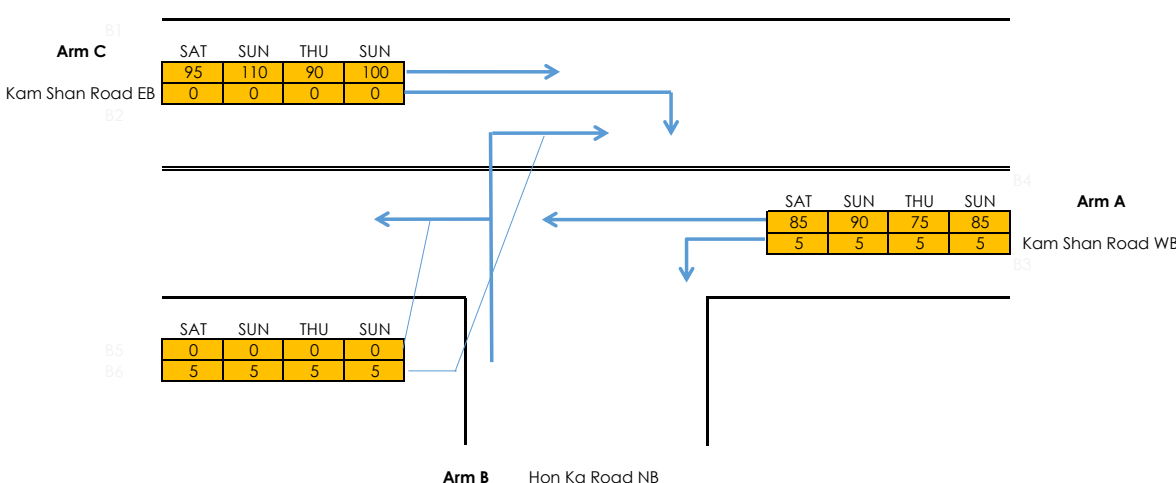
		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	245	140	100	280
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	185	115	80	255
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	450.43	481.68	495.5	429.3
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	649.58	667.44	676.38	631.72
RFC's	c-a	0.14	0.08	0.06	0.16
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.14</b>	<b>0.08</b>	<b>0.06</b>	<b>0.16</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f$  = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J2) Kam Shan Road/ Hon Ka Road		Checked by: LHW	
Scheme: Supplementary Traffic Surveys		Date: Dec-22	
Design Year: 2019 (SAT, SUN) / 2022 (THU, SUN)		Job No.: CHK50650510	
Arm A: Kam Shan Road WB			
Arm B: Hon Ka Road NB			
Arm C: Kam Shan Road EB			

**Arm C** Kam Shan Road EB

	SAT	SUN	THU	SUN
95	110	90	100	
0	0	0	0	

**Arm A** Kam Shan Road WB

	SAT	SUN	THU	SUN
85	90	75	85	
5	5	5	5	

**Arm B** Hon Ka Road NB

	SAT	SUN	THU	SUN
0	0	0	0	
5	5	5	5	

GEOMETRY			
Major Road Width (m)	W	7.30	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	20	
	VI(b-a)	50	
	Vr(b-c)	20	
	Vr(c-b)	50	
Calculated Parameters	D	0.8433	
	E	0.8972	
	F	0.937	
	Y	0.7482	
Lane widths (m)			
	w(b-a)	3.50	
	w(b-c)	3.50	
	w(c-b)	3.65	

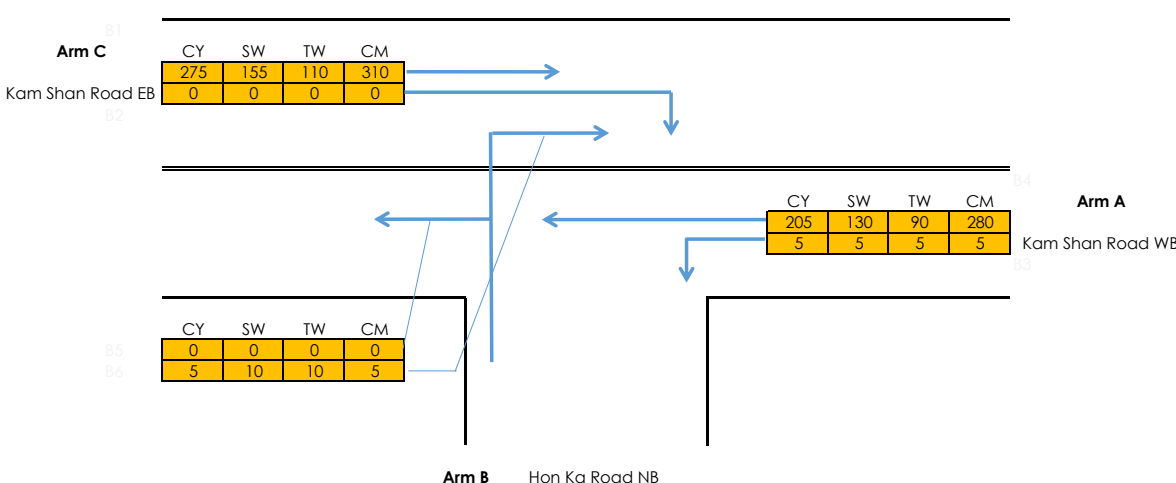
ANALYSIS		SAT	SUN	THU	SUN
TRAFFIC FLOWS (pcu/hr)	q(c-a)	95	110	90	100
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	85	90	75	85
	q(b-a)	5	5	5	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	495.07	491.76	498.09	494.35
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	675.1	673.82	677.65	675.1
RFC's	c-a	0.05	0.06	0.05	0.06
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.01	0.01	0.01
<b>RFC</b>		<b>0.05</b>	<b>0.06</b>	<b>0.05</b>	<b>0.06</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65)) / (1 + 0.0009(Vr(b-a) - 120)) / (1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65)) / (1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65)) / (1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

**All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1**

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J2) Kam Shan Road/ Hon Ka Road		Checked by: LHW	
Scheme: Reference		Date: Dec-22	
Design Year: 2028		Job No.: CHK50650510	
Arm A: Kam Shan Road WB			
Arm B: Hon Ka Road NB			
Arm C: Kam Shan Road EB			

**Arm C** Kam Shan Road EB

CY	SW	TW	CM
275	155	110	310
0	0	0	0

**Arm A** Kam Shan Road WB

CY	SW	TW	CM
205	130	90	280
5	5	5	5

**Arm B** Hon Ka Road NB

CY	SW	TW	CM
0	0	0	0
5	10	10	5

GEOMETRY			
Major Road Width (m)	W	7.30	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	20	
	VI(b-a)	50	
	Vr(b-c)	20	
	Vr(c-b)	50	
	Lane widths (m)	w(b-a)	3.50
		w(b-c)	3.50
		w(c-b)	3.65
	Calculated Parameters	D	0.8433
		E	0.8972
		F	0.937
		Y	0.7482

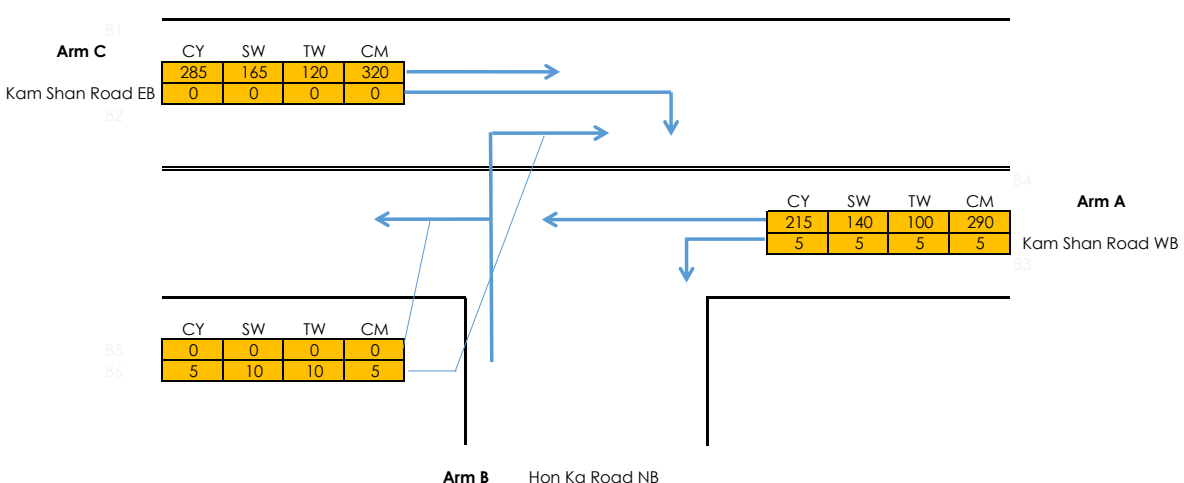
ANALYSIS		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	275	155	110	310
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	205	130	90	280
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	441.5	476.07	491.76	419.22
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	644.48	663.62	673.82	625.34
RFC's	c-a	0.15	0.09	0.06	0.17
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.15</b>	<b>0.09</b>	<b>0.06</b>	<b>0.17</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f$  = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

**All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1**

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J2) Kam Shan Road/ Hon Ka Road		Checked by: LHW	
Scheme: Design		Date: Dec-22	
Design Year: 2028		Job No.: CHK50650510	
Arm A: Kam Shan Road WB			
Arm B: Hon Ka Road NB			
Arm C: Kam Shan Road EB			

**Arm C** Kam Shan Road EB

CY	SW	TW	CM
285	165	120	320
0	0	0	0

**Arm A** Kam Shan Road WB

CY	SW	TW	CM
215	140	100	290
5	5	5	5

**Arm B** Hon Ka Road NB

CY	SW	TW	CM
0	0	0	0
5	10	10	5

GEOMETRY			
Major Road Width (m)	W	7.30	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	20	
	VI(b-a)	50	
	Vr(b-c)	20	
	Vr(c-b)	50	
	Lane widths (m)	w(b-a)	3.50
		w(b-c)	3.50
		w(c-b)	3.65
	Calculated Parameters	D	0.8433
		E	0.8972
		F	0.937
		Y	0.7482

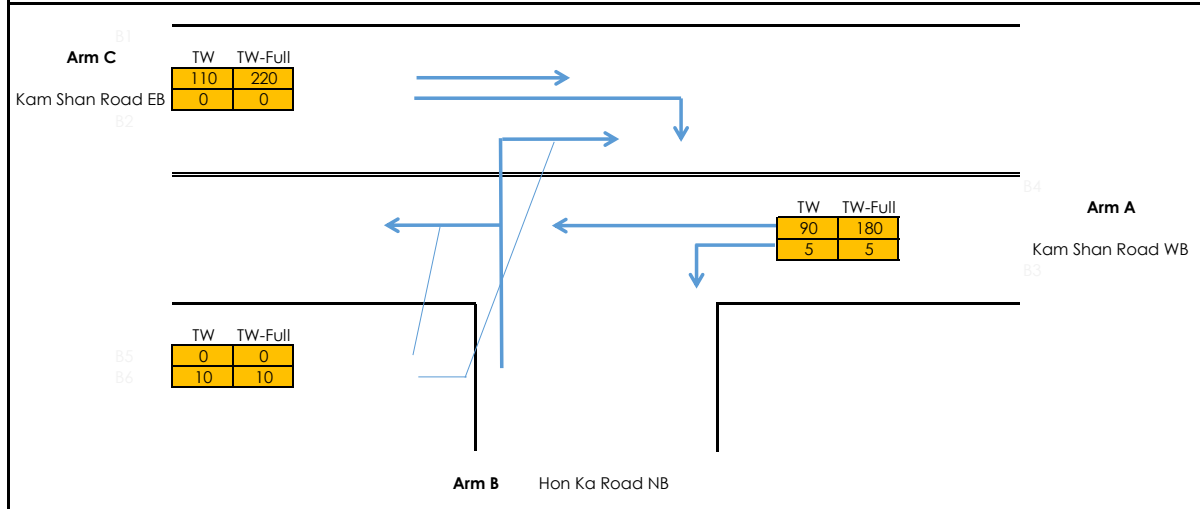
ANALYSIS		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	285	165	120	320
	q(c-b)	0	0	0	0
	q(a-b)	5	5	5	5
	q(a-c)	215	140	100	290
	q(b-a)	5	10	10	5
	q(b-c)	0	0	0	0
	f	0.00	0.00	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	437.76	472.33	488.01	415.48
	Q(c-a)	1800	1800	1800	1800
	Q(c-b)	641.93	661.07	671.27	622.79
RFC's	c-a	0.16	0.09	0.07	0.18
	c-b	0.00	0.00	0.00	0.00
	b-ac	0.01	0.02	0.02	0.01
<b>RFC</b>		<b>0.16</b>	<b>0.09</b>	<b>0.07</b>	<b>0.18</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road		
Junction:	(J2) Kam Shan Road/ Hon Ka Road	Designed by:	TLY
Scheme:	Reference - Full Intake	Checked by:	LHW
Design Year: 2028	Job No.: CHK50650510	Date:	Dec-22
Arm A:	Kam Shan Road WB		
Arm B:	Hon Ka Road NB		
Arm C:	Kam Shan Road EB		



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.8433
	VI(b-a)	50		E	0.8972
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.7482

### ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	110	220
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	90	180
	q(b-a)	10	10
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	491.76	455.19
	Q(c-a)	1800	1800
	Q(c-b)	673.82	650.86
RFC's	c-a	0.06	0.12
	c-b	0.00	0.00
	b-ac	0.02	0.02
<b>RFC</b>		<b>0.06</b>	<b>0.12</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

f = proportion of minor traffic turning left

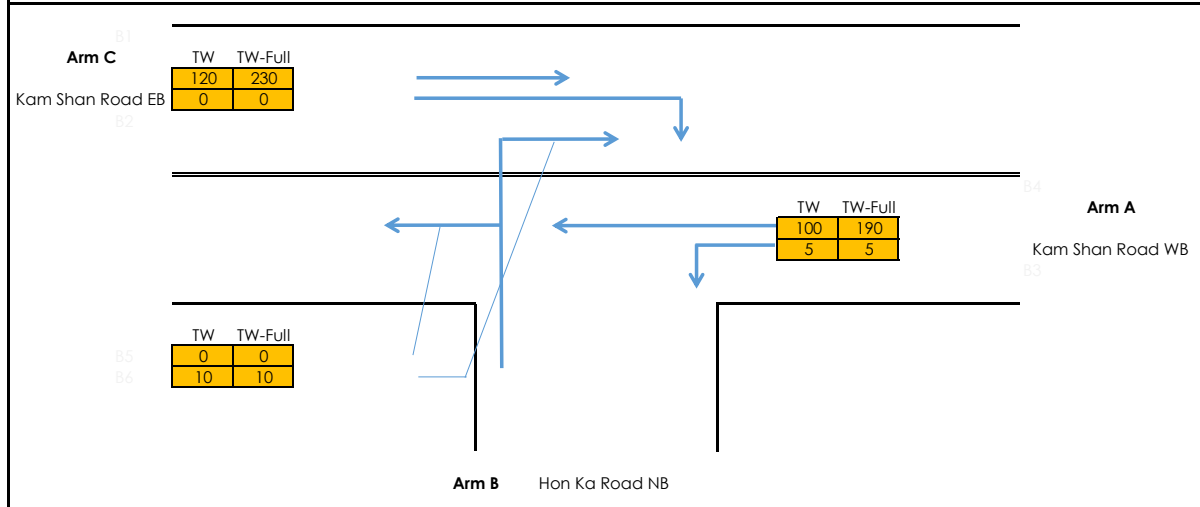
$$Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a) \quad \text{Capacity of combined streams}$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1



## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	
Junction:	(J2) Kam Shan Road/ Hon Ka Road	Designed by: TLY
Scheme:	Design - Full Intake	Checked by: LHW
Design Year: 2028	Job No.: CHK50650510	Date: Dec-22
Arm A:	Kam Shan Road WB	
Arm B:	Hon Ka Road NB	
Arm C:	Kam Shan Road EB	



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.8433
	VI(b-a)	50		E	0.8972
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.7482

### ANALYSIS

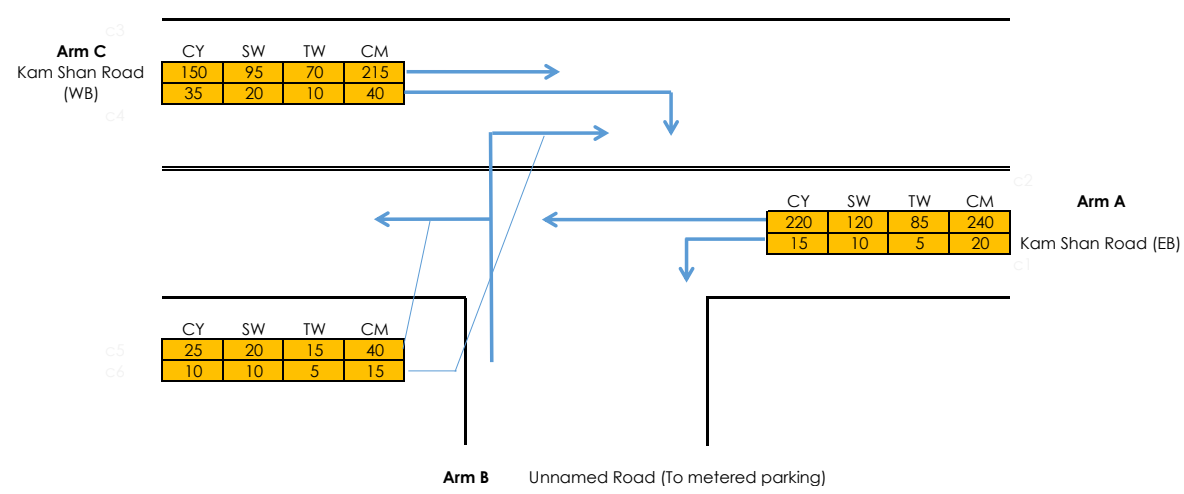
		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	120	230
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	100	190
	q(b-a)	10	10
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	488.01	451.45
	Q(c-a)	1800	1800
	Q(c-b)	671.27	648.31
RFC's	c-a	0.07	0.13
	c-b	0.00	0.00
	b-ac	0.02	0.02
<b>RFC</b>		<b>0.07</b>	<b>0.13</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 f = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c) * Q(b-a) / ((1-f) * Q(b-c) + f * Q(b-a))$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Existing		Date: Dec-22	
Design Year: 2017 (CY, SW, CM); 2018 (CM)		Job No.: CHK50650510	
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C** Kam Shan Road (WB)

	CY	SW	TW	CM
c3	150	95	70	215
c4	35	20	10	40

**Arm A** Kam Shan Road (EB)

	CY	SW	TW	CM
c2	220	120	85	240
c1	15	10	5	20

**Arm B** Unnamed Road (To metered parking)

	CY	SW	TW	CM
c5	25	20	15	40
c6	10	10	5	15

GEOMETRY			
Major Road Width (m)	W	7.00	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	40	
	VI(b-a)	30	
	Vr(b-c)	40	
	Vr(c-b)	50	
Calculated Parameters	D	0.8652	
	E	0.9324	
	F	0.959	
	Y	0.7585	
Lane widths (m)	w(b-a)	3.70	
	w(b-c)	3.70	
	w(c-b)	3.90	

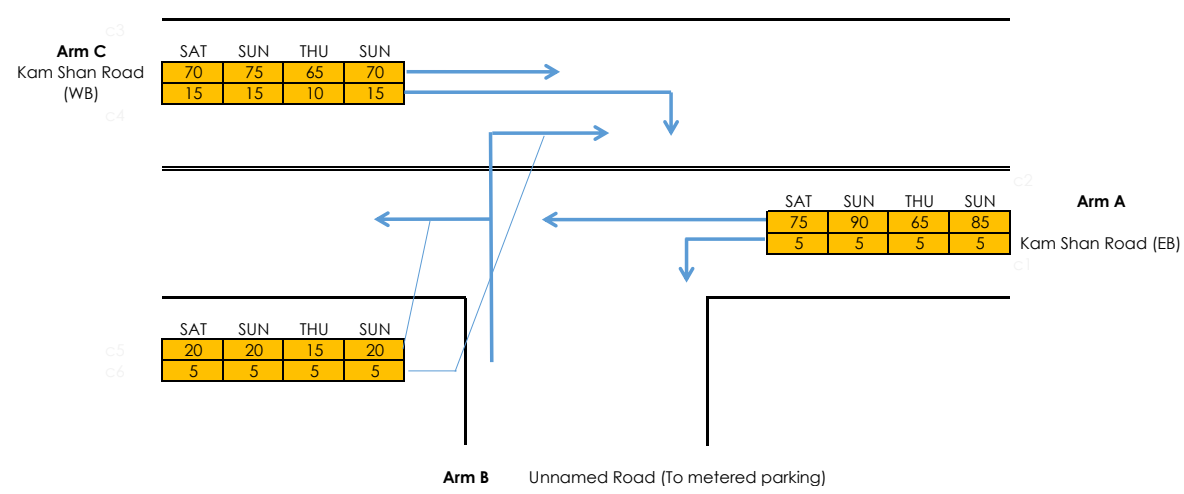
ANALYSIS		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	150	95	70	215
	q(c-b)	35	20	10	40
	q(a-b)	15	10	5	20
	q(a-c)	220	120	85	240
	q(b-a)	10	10	5	15
	q(b-c)	25	20	15	40
	f	0.71	0.67	0.75	0.73
CAPACITIES (pcu/hr)	Q(b-ac)	570.92	593.9	621.88	562.88
	Q(c-a)	1703.4	1747.1	1773.9	1688.5
	Q(c-b)	652.25	680.05	690.64	645.63
RFC's	c-a	0.09	0.05	0.04	0.13
	c-b	0.05	0.03	0.01	0.06
	b-ac	0.06	0.05	0.03	0.10
<b>RFC</b>		<b>0.09</b>	<b>0.05</b>	<b>0.04</b>	<b>0.13</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Supplementary Traffic Surveys		Date: Dec-22	
Design Year: 2019 (SAT, SUN) / 2022 (THU, SUN)		Job No.: CHK50650510	
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C** Kam Shan Road (WB)

	SAT	SUN	THU	SUN
70	75	65	70	
15	15	10	15	

**Arm A** Kam Shan Road (EB)

	SAT	SUN	THU	SUN
75	90	65	85	
5	5	5	5	

**Arm B** Unnamed Road (To metered parking)

	SAT	SUN	THU	SUN
20	20	15	20	
5	5	5	5	

GEOMETRY			
Major Road Width (m)	W	7.00	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	40	
	VI(b-a)	30	
	Vr(b-c)	40	
	Vr(c-b)	50	
	Lane widths (m)	w(b-a)	3.70
		w(b-c)	3.70
		w(c-b)	3.90
	Calculated Parameters	D	0.8652
		E	0.9324
		F	0.959
		Y	0.7585

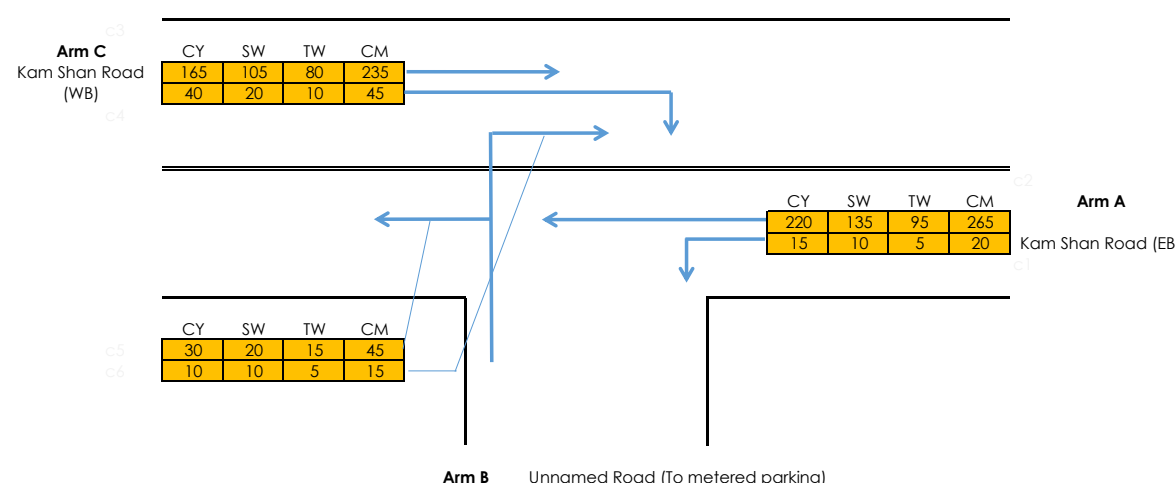
ANALYSIS		SAT	SUN	THU	SUN
TRAFFIC FLOWS (pcu/hr)	q(c-a)	70	75	65	70
	q(c-b)	15	15	10	15
	q(a-b)	5	5	5	5
	q(a-c)	75	90	65	85
	q(b-a)	5	5	5	5
	q(b-c)	20	20	15	20
	f	0.80	0.80	0.75	0.80
CAPACITIES (pcu/hr)	Q(b-ac)	633.36	629.29	627.25	630.8
	Q(c-a)	1761.1	1760.8	1774.1	1760.9
	Q(c-b)	693.29	689.32	695.93	690.64
RFC's	c-a	0.04	0.04	0.04	0.04
	c-b	0.02	0.02	0.01	0.02
	b-ac	0.04	0.04	0.03	0.04
<b>RFC</b>		<b>0.04</b>	<b>0.04</b>	<b>0.04</b>	<b>0.04</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65)) / (1 + 0.0009(Vr(b-a) - 120)) / (1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65)) / (1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65)) / (1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Reference		Date: Dec-22	
Design Year: 2028		Job No.: CHK50650510	
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C** Kam Shan Road (WB)

CY	SW	TW	CM
165	105	80	235
40	20	10	45

**Arm A** Kam Shan Road (EB)

CY	SW	TW	CM
220	135	95	265
15	10	5	20

**Arm B** Unnamed Road (To metered parking)

CY	SW	TW	CM
30	20	15	45
10	10	5	15

GEOMETRY			
Major Road Width (m)	W	7.00	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	40	
	VI(b-a)	30	
	Vr(b-c)	40	
	Vr(c-b)	50	
	Lane widths (m)	w(b-a)	3.70
		w(b-c)	3.70
		w(c-b)	3.90
	Calculated Parameters	D	0.8652
		E	0.9324
		F	0.959
		Y	0.7585

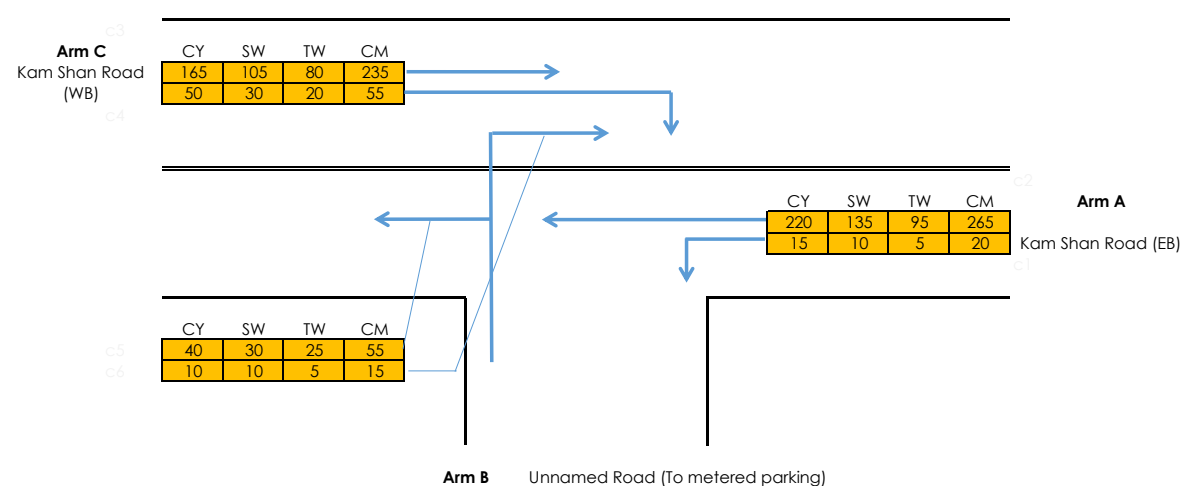
ANALYSIS		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	165	105	80	235
	q(c-b)	40	20	10	45
	q(a-b)	15	10	5	20
	q(a-c)	220	135	95	265
	q(b-a)	10	10	5	15
	q(b-c)	30	20	15	45
	f	0.75	0.67	0.75	0.75
CAPACITIES (pcu/hr)	Q(b-ac)	576.74	589.36	618.76	559.53
	Q(c-a)	1689.6	1746.8	1773.8	1673.2
	Q(c-b)	652.25	676.08	687.99	639.01
RFC's	c-a	0.10	0.06	0.05	0.14
	c-b	0.06	0.03	0.01	0.07
	b-ac	0.07	0.05	0.03	0.11
<b>RFC</b>		<b>0.10</b>	<b>0.06</b>	<b>0.05</b>	<b>0.14</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Design		Date: Dec-22	
Design Year: 2028		Job No.: CHK50650510	
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C**  
Kam Shan Road (WB)

CY	SW	TW	CM
165	105	80	235
50	30	20	55

**Arm A**  
Kam Shan Road (EB)

CY	SW	TW	CM
220	135	95	265
15	10	5	20

**Arm B**  
Unnamed Road (To metered parking)

CY	SW	TW	CM
40	30	25	55
10	10	5	15

GEOMETRY			
Major Road Width (m)	W	7.00	
Central Reserve Width (m)	Wcr	0.00	
Blockage of major road right turn	Y/N?	Y	
Combined stream on minor arm	Y/N?	Y	
Visibility Distances (m)	Vr(b-a)	40	
	VI(b-a)	30	
	Vr(b-c)	40	
	Vr(c-b)	50	
	Lane widths (m)	w(b-a)	3.70
		w(b-c)	3.70
		w(c-b)	3.90
	Calculated Parameters	D	0.8652
		E	0.9324
		F	0.959
		Y	0.7585

ANALYSIS		CY	SW	TW	CM
TRAFFIC FLOWS (pcu/hr)	q(c-a)	165	105	80	235
	q(c-b)	50	30	20	55
	q(a-b)	15	10	5	20
	q(a-c)	220	135	95	265
	q(b-a)	10	10	5	15
	q(b-c)	40	30	25	55
	f	0.80	0.75	0.83	0.79
CAPACITIES (pcu/hr)	Q(b-ac)	586.6	603.99	633.93	566.65
	Q(c-a)	1662	1720.1	1747.7	1645.1
	Q(c-b)	652.25	676.08	687.99	639.01
RFC's	c-a	0.10	0.06	0.05	0.14
	c-b	0.08	0.04	0.03	0.09
	b-ac	0.09	0.07	0.05	0.12
<b>RFC</b>		<b>0.10</b>	<b>0.07</b>	<b>0.05</b>	<b>0.14</b>

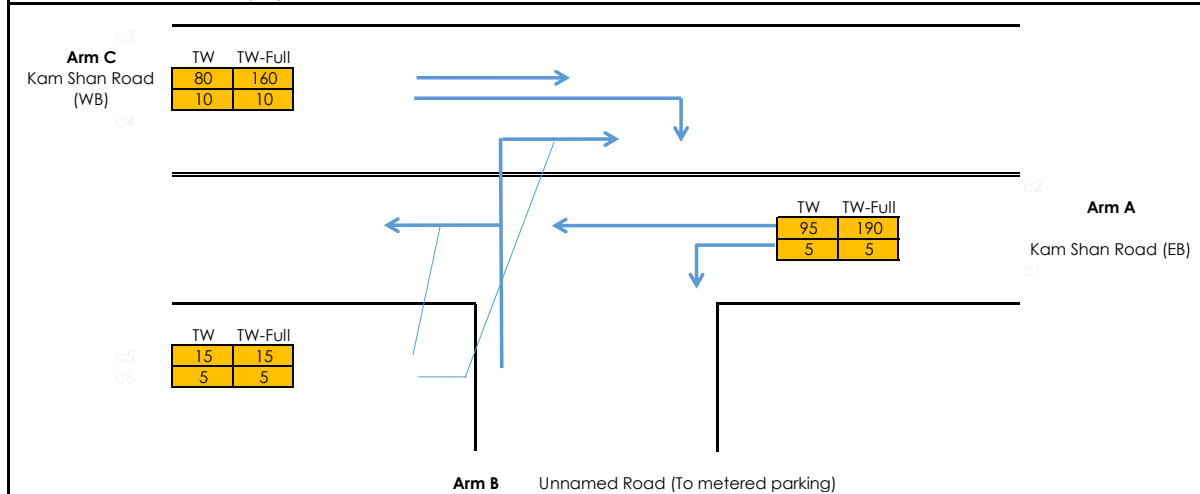
Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1



## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	TLY
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	LHW
Scheme:	Reference - Full Intake	Date:	Dec-22
Design Year:	2028	Job No.:	CHK50650510
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



### GEOMETRY

Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.8652
	VI(b-a)	30		E	0.9324
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.7585

### ANALYSIS

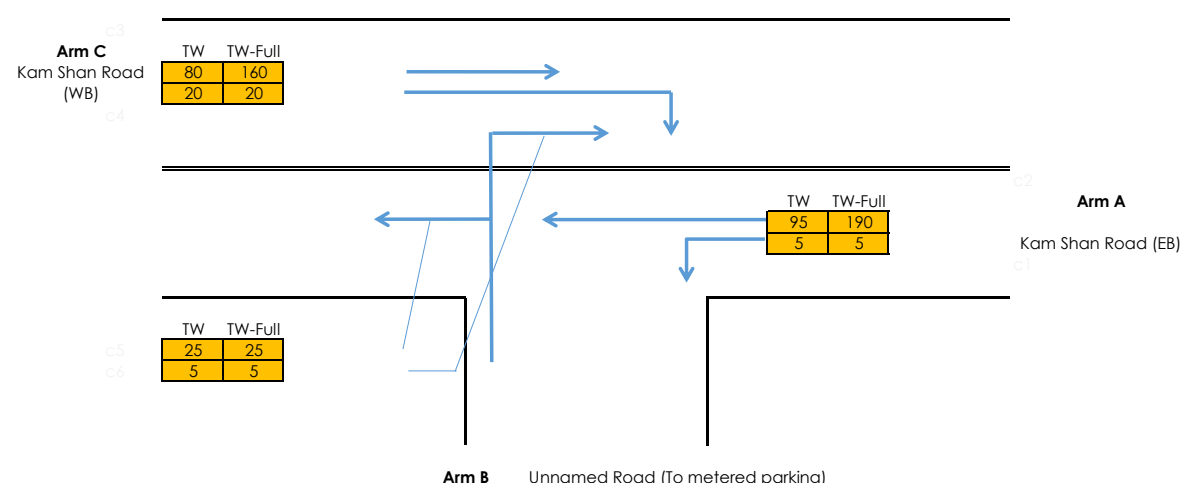
		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	80	160
	q(c-b)	10	10
	q(a-b)	5	5
	q(a-c)	95	190
	q(b-a)	5	5
	q(b-c)	15	15
	f	0.75	0.75
CAPACITIES (pcu/hr)	Q(b-ac)	618.76	589.87
	Q(c-a)	1773.8	1772.8
	Q(c-b)	687.99	662.84
RFC's	c-a	0.05	0.09
	c-b	0.01	0.02
	b-ac	0.03	0.03
<b>RFC</b>		<b>0.05</b>	<b>0.09</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 f = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c) * Q(b-a) / ((1-f) * Q(b-c) + f * Q(b-a))$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Design - Full Intake		Date: Dec-22	
Design Year: 2028	Job No.: CHK50650510		
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C**  
Kam Shan Road (WB)

TW	TW-Full
80	160
20	20

**Arm A**  
Kam Shan Road (EB)

TW	TW-Full
95	190
5	5

**Arm B**  
Unnamed Road (To metered parking)

TW	TW-Full
25	25
5	5

GEOMETRY					
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.8652
	VI(b-a)	30		E	0.9324
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.7585
ANALYSIS					
TRAFFIC FLOWS (pcu/hr)	q(c-a)	80	160		
	q(c-b)	20	20		
	q(a-b)	5	5		
	q(a-c)	95	190		
	q(b-a)	5	5		
	q(b-c)	25	25		
	f	0.83	0.83		
CAPACITIES (pcu/hr)	Q(b-ac)	633.93	606.27		
	Q(c-a)	1747.7	1745.7		
	Q(c-b)	687.99	662.84		
RFC's	c-a	0.05	0.09		
	c-b	0.03	0.03		
	b-ac	0.05	0.05		
<b>RFC</b>		<b>0.05</b>	<b>0.09</b>		

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

**All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1**

## APPENDIX D

## Appendix D Sensitivity Test – Full Intakes of Planned Niches in the Vicinity

### Other Private Columbarium Developments in the Vicinity

1. In addition to the Subject Site, there are other private columbaria identified along Kam Shan Road as shown in **Table D1** below:

**Table D1 Private Columbarium Developments in Vicinity**

Private Columbarium Developments	Latest Planning Application No.	No. of Units under Application		Visitors per hour (Trip Rate per niche) under Traffic Management Plan	Modal Split	
		Niches	Tablets		PC/Taxi	PT/Walk
Cheung Ha Ching Shea (The Subject Site)	Y/TP/37	1,700	-	48 (0.028)	34%	66%
Poh Yea Ching Shea	A/TP/657 <sup>(1)</sup>	5,302	174	170 <sup>(4)</sup> (0.031)	34%	66%
Buddhist Cheung Ha Temple	Y/TP/35	11,726	3,049	424 <sup>(4)</sup> (0.029)	34%	66%
Ling Hin Fat Yuen	A/TP/652 <sup>(2)</sup>	757	1,782	120 <sup>(4)</sup> (0.047)	50%	50%
Pun Chun Yuen	A/TP/681 <sup>(3)</sup>	3,834	-	200 <sup>(4)</sup> (0.052)	50%	50%
Everest Rest Temple	Y/TP/36	3,507	-	200 <sup>(4)</sup> (0.057)	34%	66%
Total:		26,826	5,055	1,162	-	-

Source: Statutory Planning Portal 2, Town Planning Board (<https://www2.ozp.tpb.gov.hk/>)

(1) A/TP/657: Approved on 29/11/2019

(2) A/TP/652: Approved on 19/07/2019

(3) A/TP/681: Approved on 26/08/2022

(4) Number of visitors are estimated by the Traffic Management Plans in the corresponding TIA

2. To account for the full-intakes of the planned niches as the worst-case scenario, a sensitivity test is included to examine the corresponding pedestrian links and road junctions.

### Operational Performance for Pedestrian Links and Road Junctions

3. With the proposed closure of operation during Ching Ming and Chung Yeung Festivals, and their shadow weekends (i.e. any Saturdays, Sundays and public holiday(s) within 2 weeks before and 2 weeks after the Festival Days), the critical traffic conditions during peak seasons at Kam Shan Road will be unlikely correlated to the Subject Site. The proposed columbarium will also be closed from the 1st to 14th day of the Lunar New Year.
4. Hence, the operational performance of the pedestrian links and road junctions are examined for typical weekends only (including Saturday and Sunday during non-peak days).
5. Besides, taking account of the admission control in the proposed management plan, a maximum number of 24 persons will be present on the Subject Site at any time. The trip generation by the proposed columbarium development are expected to be well controlled at 48 visitors per hour.
6. For the other private columbarium developments in the vicinity, the number of visitors are estimated by Traffic Management Plans in the corresponding TIA. The modal split is assumed with respect to the proximity to public transport services as per the Subject Site.
7. As shown in **Tables D2 and D3**, all of the pedestrian links and road junctions in the vicinity will be operated with an acceptable level despite the full intakes of unoccupied and planned niches in the vicinity during typical weekends. The existing cautionary crossing will be sufficient and no formal crossing is required as shown in **Table D4**.
8. In view of the assessment results of the sensitivity tests, the traffic conditions during typical weekends (including Saturday and Sunday) will be operated at acceptable level, despite the full-intakes of the proposed columbarium developments at the Subject Site and the planned niches in the vicinity. Therefore, the cumulative traffic impact can be justified through the above sensitivity test.
9. Nevertheless, under the statutory applications through Town Planning Board ("TPB") and the Private Columbaria Licensing Board ("PCLB"), appropriate traffic management plans and mitigation measures would be available for the existing and future private columbarium developments. The overall traffic conditions in the area are expected to be clearly monitored with proper mitigation measures when undesirable traffic conditions have been observed.

**Table D2 Year 2028 Performance of Pedestrian Links (with Full Intakes of Planned Niches)**

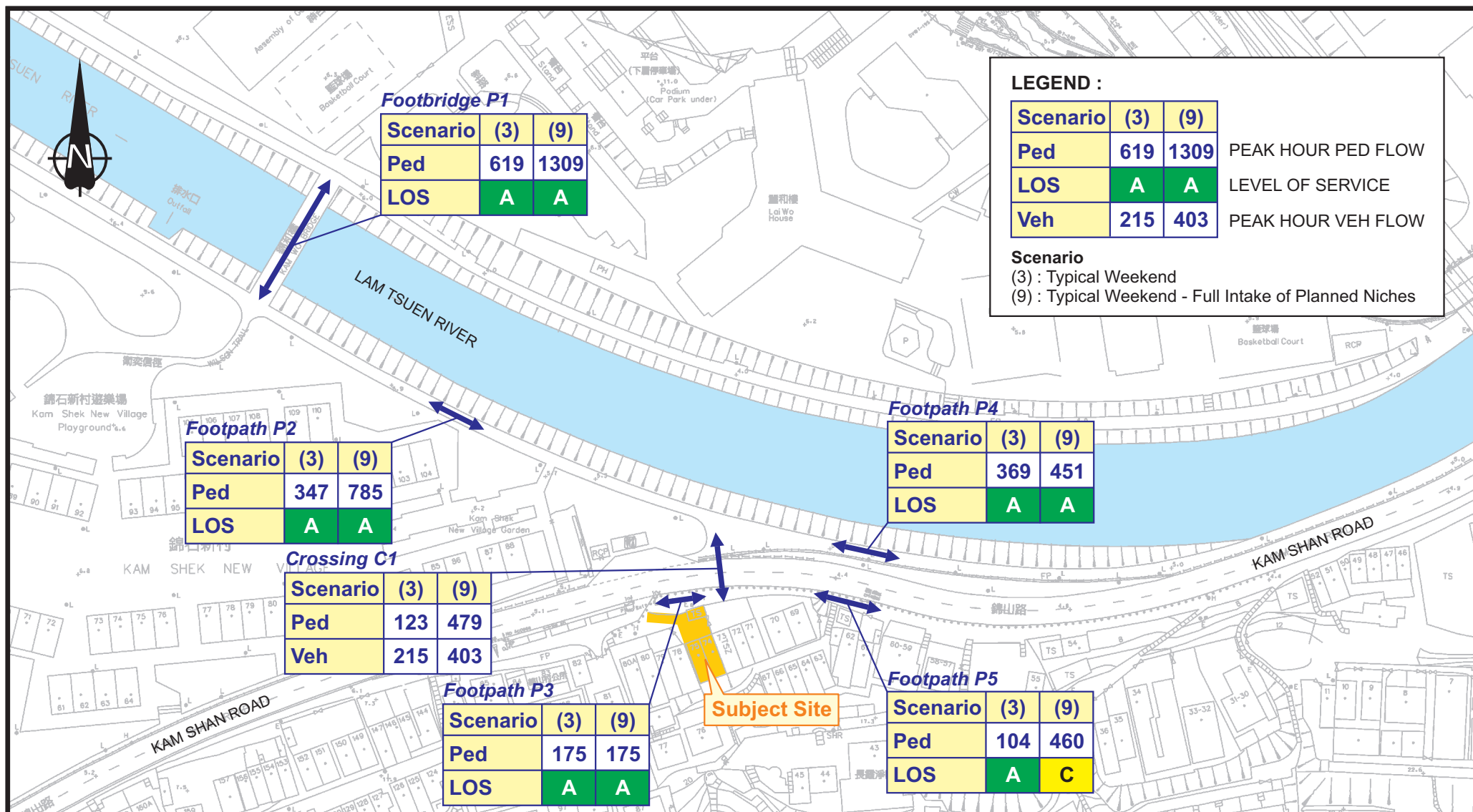
Table D2: Year 2028 Performance of Pedestrian Links (with Full Intakes of Planned Niches)					
Index	Location	Type	Effective Width (m)	Ped Flows (2-way/hour) Ped/min/m (LOS)	
				Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake of Planned Niches)
Year 2028 Reference Scenario (Without Development)					
P1	Kam Wo Bridge	Footbridge	5.2	619 2.0 (A)	1309 4.2 (A)
P2	Kam Shek New Village	Footpath	2.2	347 2.6 (A)	785 5.9 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	175 2.9 (A)	175 2.9 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	369 2.1 (A)	451 2.5 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	104 6.9 (A)	460 30.7 (C)
Year 2028 Design Scenario (With Development)					
P1	Kam Wo Bridge	Footbridge	5.2	657 2.1 (A)	1347 4.3 (A)
P2	Kam Shek New Village	Footpath	2.2	385 3.0 (A)	823 6.2 (A)
P3	Kam Shan Road (W) – South side	Footpath	1.0	271 4.7 (A)	271 4.5 (A)
P4	Kam Shan Road (E) – North side	Footpath	3.0	381 2.1 (A)	463 2.6 (A)
P5	Kam Shan Road (E) – South side	Footpath	0.25	116 7.7 (A)	472 31.5 (C)

**Table D3 Year 2028 Performance of Road Junctions (with Full Intakes of Planned Niches)**

Table D5: Year 2028 Performance of Road Junctions (With Full Intakes of Planned Niches)				
Index	Location	Type	Reserve Capacity (%) / Ratio of Flow to Capacity	
			Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake of Planned Niches)
Year 2028 Reference Scenario (Without Development)				
J1	Pak Shing Street near Tai Wo Bridge	Signalised Crossing	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.06	0.11
J3	Kam Shan Road/ Shek Lin Road	Priority	0.05	0.14
Year 2028 Design Scenario (With Development)				
J1	Pak Shing Street near Tai Wo Bridge	Signalised Crossing	>100%	>100%
J2	Kam Shan Road/ Hon Ka Road	Priority	0.07	0.12
J3	Kam Shan Road/ Shek Lin Road	Priority	0.05	0.15

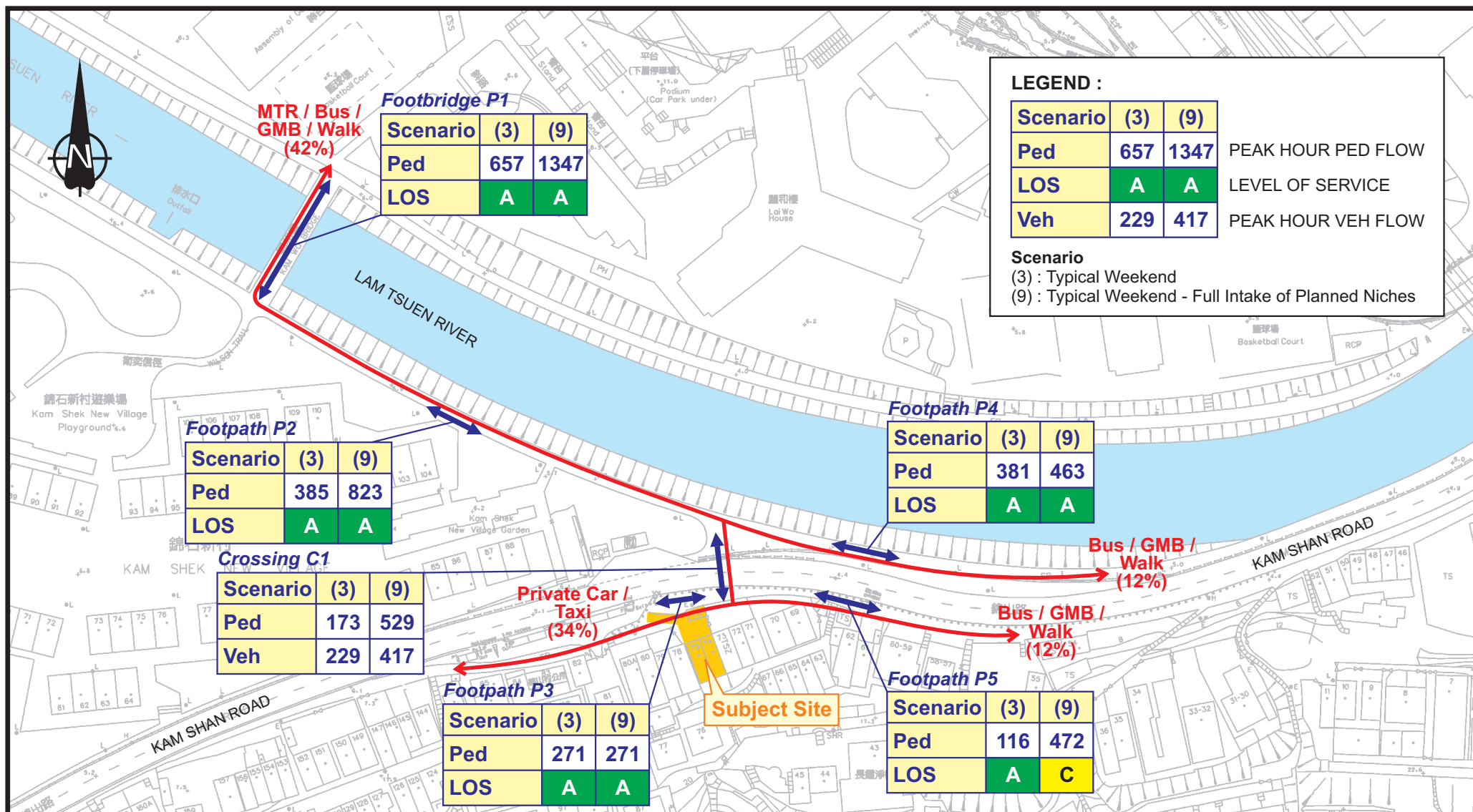
**Table D4 Year 2028 Performance of Pedestrian Crossing (with Full Intakes of Planned Niches)**

Parameters	Typical Weekend (Gradual Intake at 1.0% p.a.)	Typical Weekend (Full Intake of Planned Niches)
<b>Year 2028 Reference Scenario (Without Development)</b>		
Pedestrian crossing P (ped/hr)	123	479
2-way Vehicular Flow V (veh/h)	215	403
PV <sup>2</sup> (10 <sup>8</sup> )	0.06	0.78
Recommendation of Pedestrian Crossing Facility	No formal Crossing	No formal Crossing
<b>Year 2028 Design Scenario (With Development)</b>		
Pedestrian crossing P (ped/hr)	173	529
2-way Vehicular Flow V (veh/h)	229	417
PV <sup>2</sup> (10 <sup>8</sup> )	0.09	0.92
Recommendation of Pedestrian Crossing Facility	No formal Crossing	No formal Crossing

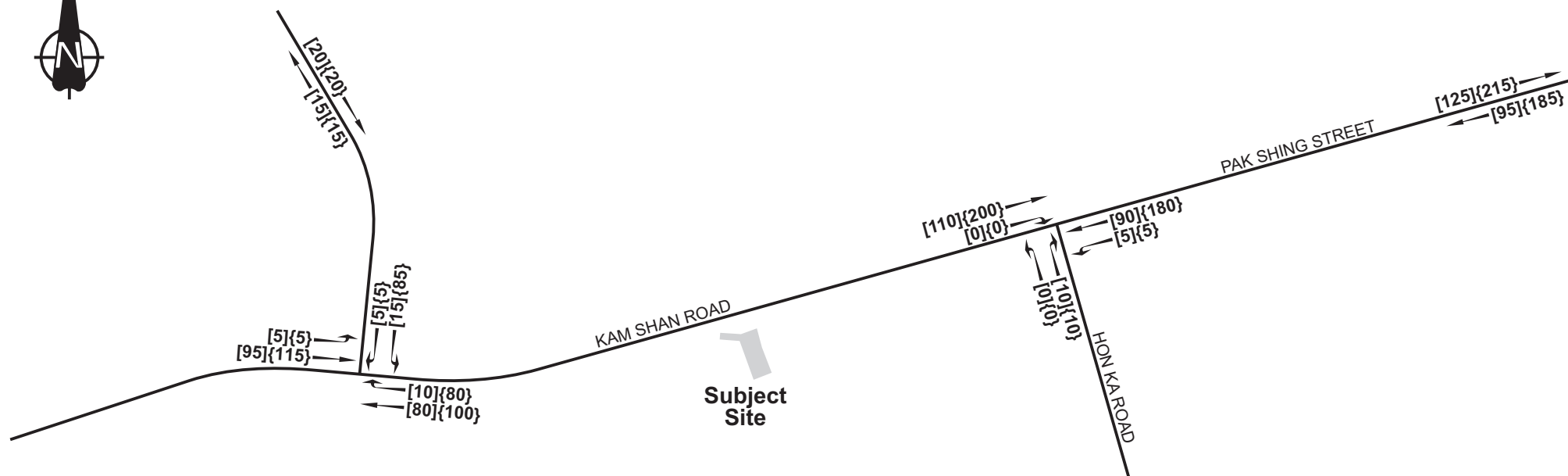


A RESPONSE TO TD'S COMMENT				B UPDATED TRIP RATES							
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 REFERENCE PEDESTRIAN FLOW AND LOS - SENSITIVITY TEST							
				Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022
								Drawing No.	D1		Rev. B






A RESPONSE TO TD'S COMMENT				B UPDATED TRIP RATES							
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 DESIGN PEDESTRIAN FLOW AND LOS - SENSITIVITY TEST							
Designed		Checked		Scale		Date		Drawing No.		Rev.	
TLY		LHW		NTS		DEC 2022		D2		B	

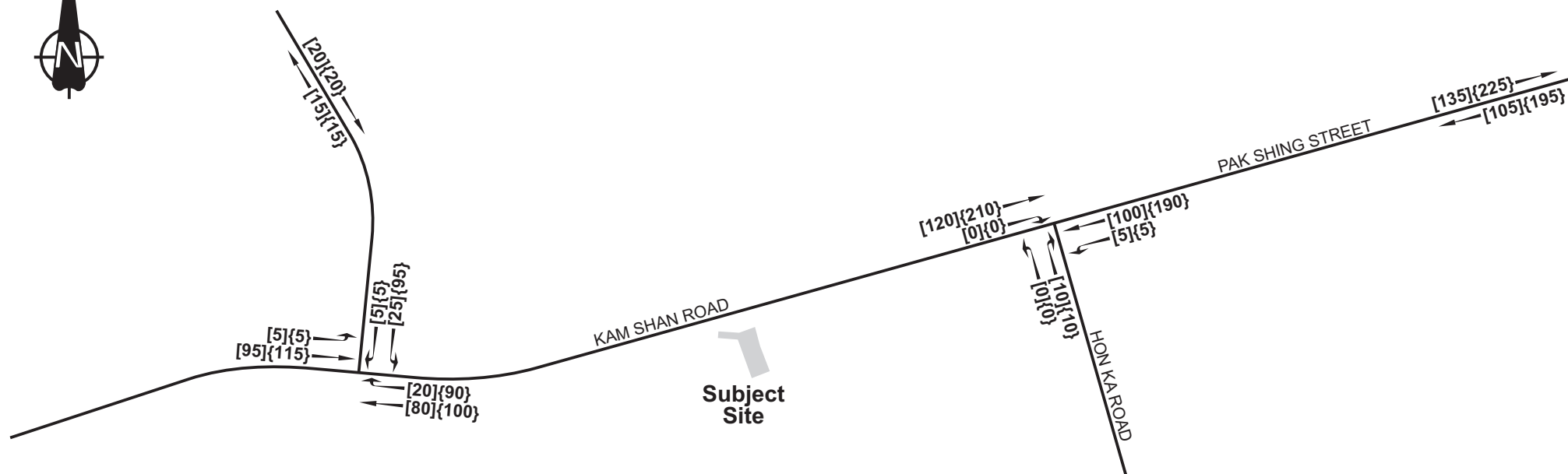


# LEGEND :

[90] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)

{180} TYPICAL WEEKEND - FULL INTAKE OF PLANNED NICHES (PCU/HR)


A	RESPONSE TO TD'S COMMENT	LHW	17FEB23	-	-	-	-	-	-	-	-
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 REFERENCE TRAFFIC FORECAST - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022	Drawing No.	D3	Rev.	



# LEGEND :

[100] TYPICAL WEEKEND PEAK HOUR TRAFFIC FLOW (PCU/HR)

{190} TYPICAL WEEKEND - FULL INTAKE OF PLANNED NICHES (PCU/HR)

A	RESPONSE TO TD'S COMMENT	LHW	17FEB23	-	-	-	-	-	-	-	-
Rev.	Description	Checked	Date	Rev.	Description	Checked	Date	Rev.	Description	Checked	Date
Project Title				Drawing Title							
REZONING APPLICATION FROM "VILLAGE TYPE DEVELOPMENT" ZONE TO "GOVERNMENT, INSTITUTION OR COMMUNITY (3)" ZONE AT NOS. 74-75 KAM SHAN ROAD AND ADJOINING GOVERNMENT LAND, TAI PO, NEW TERRITORIES				YEAR 2028 DESIGN TRAFFIC FORECAST - SENSITIVITY TEST							
Designed	TLY	Checked	LHW	Scale	NTS	Date	DEC 2022	Drawing No.	D4	Rev.	

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Reference - Typical Weekend (Full Intake)

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Typical Weekend			Typical Weekend - Full Intakes		
					Left	Right		Typical Weekend	Typical Weekend - Full Intakes	Typical Weekend	Typical Weekend - Full Intakes	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Pak Shing St EB	↑	A	1	4.000						2015	2015	125	0.062	0.062	215	0.107	0.107
Pak Shing St WB	↑	A	1	4.000						2015	2015	95	0.047		185	0.092	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =		5	+	10	=	15				*			*

Notes:

Flow: (pcu/hr)

↑ N

→ 125 (215)

← 95 (185)

Group		A,Bp	Group		A,Bp
y		0.062	y		0.107
L (sec)		22	L (sec)		22
C (sec)		44	C (sec)		44
y pract.		0.450	y pract.		0.450
R.C. (%)		625%	R.C. (%)		322%

Stage / Phase Diagrams

1.

A → ← A

2.

Bp ↑ ↓

3.

4.

5.

Junction: Pak Shing Street near Sui On Street

Design Year: 2028

Description: 2028 Design - Typical Weekend (Full Intake)

Designed By: SCW

Checked By: LHW

Approach	Movements	Phase	Stage	Width (m)	Radius (m)		Gradient (%)	Pro. Turning (%)		Revised Saturation Flow (pcu/hr)		Typical Weekend			Typical Weekend - Full Intakes		
					Left	Right		Typical Weekend	Typical Weekend - Full Intakes	Typical Weekend	Typical Weekend - Full Intakes	Flow (pcu/hr)	y Value	Critical y	Flow (pcu/hr)	y Value	Critical y
Pak Shing St EB	↑	A	1	4.000						2015	2015	135	0.067	0.067	225	0.112	0.112
Pak Shing St WB	↑	A	1	4.000						2015	2015	105	0.052		195	0.097	
Pedestrian Crossing		Bp	2	MIN GREEN + FLASH =				5	+	10	=	15		*			*

Notes:

Flow: (pcu/hr)

↑ N

→ 135 (225)

← 105 (195)

Group		A,Bp	Group		A,Bp
y		0.067	y		0.112
L (sec)		22	L (sec)		22
C (sec)		44	C (sec)		44
y pract.		0.450	y pract.		0.450
R.C. (%)		572%	R.C. (%)		303%

Stage / Phase Diagrams

1.

A → ← A

2.

Bp ↑ ↓

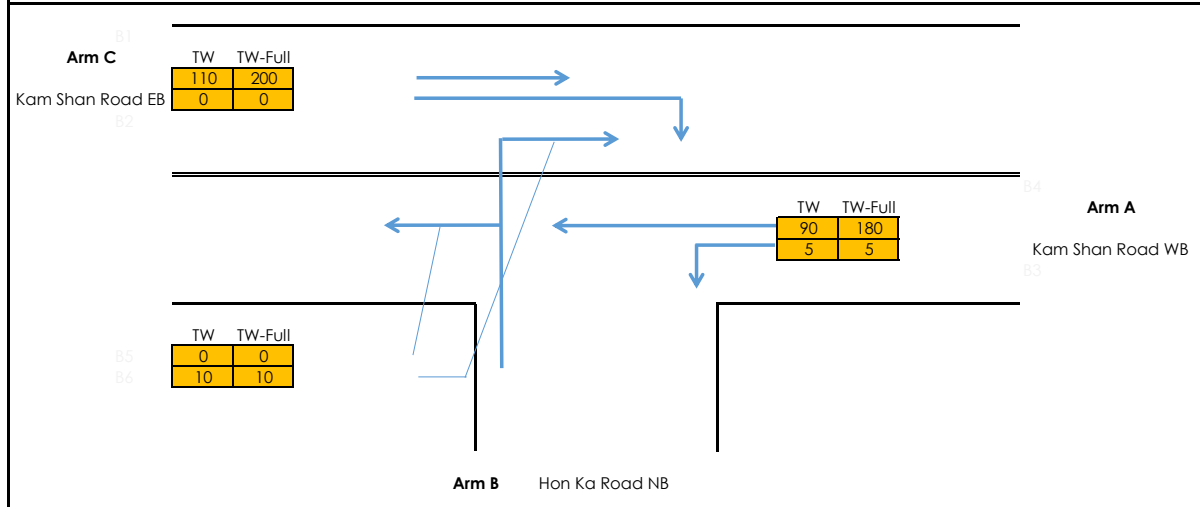
3.

4.

5.

## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road		Designed by:	TLY
Junction:	(J2) Kam Shan Road/ Hon Ka Road		Checked by:	LHW
Scheme:	Reference - Full Intake		Date:	Feb-23
Design Year: 2028	Job No.: CHK50650510			
Arm A:	Kam Shan Road WB			
Arm B:	Hon Ka Road NB			
Arm C:	Kam Shan Road EB			



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.8433
	VI(b-a)	50		E	0.8972
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.7482

### ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	110	200
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	90	180
	q(b-a)	10	10
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	491.76	458.08
	Q(c-a)	1800	1800
	Q(c-b)	673.82	650.86
RFC's	c-a	0.06	0.11
	c-b	0.00	0.00
	b-ac	0.02	0.02
<b>RFC</b>		<b>0.06</b>	<b>0.11</b>

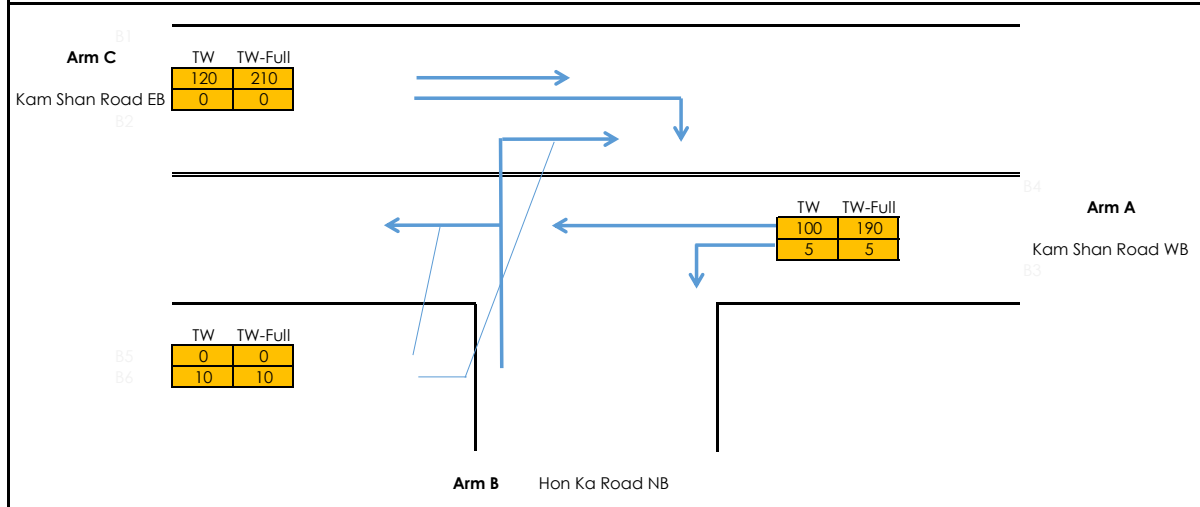
Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 f = proportion of minor traffic turning left  
 $Q(b-ac) = Q(b-c) * Q(b-a) / ((1-f) * Q(b-c) + f * Q(b-a))$  Capacity of combined streams

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1



## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	
Junction:	(J2) Kam Shan Road/ Hon Ka Road	Designed by: TLY
Scheme:	Design - Full Intake	Checked by: LHW
Design Year: 2028	Job No.: CHK50650510	Date: Feb-23
Arm A:	Kam Shan Road WB	
Arm B:	Hon Ka Road NB	
Arm C:	Kam Shan Road EB	



### GEOMETRY

Major Road Width (m)	W	7.30	Lane widths (m)	w(b-a)	3.50
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.50
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.65
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	20	Calculated Parameters	D	0.8433
	VI(b-a)	50		E	0.8972
	Vr(b-c)	20		F	0.937
	Vr(c-b)	50		Y	0.7482

### ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	120	210
	q(c-b)	0	0
	q(a-b)	5	5
	q(a-c)	100	190
	q(b-a)	10	10
	q(b-c)	0	0
	f	0.00	0.00
CAPACITIES (pcu/hr)	Q(b-ac)	488.01	454.34
	Q(c-a)	1800	1800
	Q(c-b)	671.27	648.31
RFC's	c-a	0.07	0.12
	c-b	0.00	0.00
	b-ac	0.02	0.02
<b>RFC</b>		<b>0.07</b>	<b>0.12</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

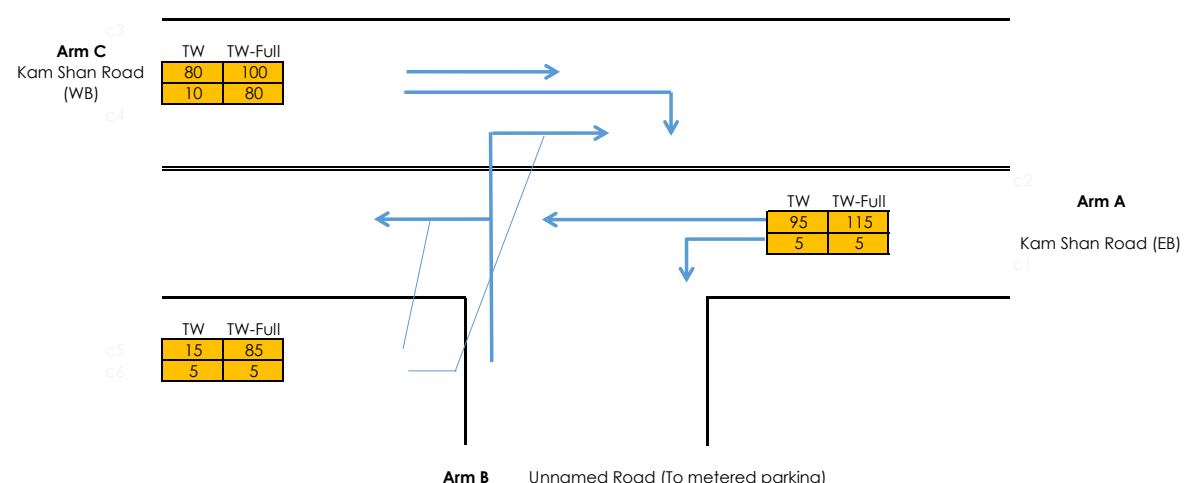
f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a) \quad \text{Capacity of combined streams}$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## Simplified Priority Junction Capacity Calculation

Job Title: Proposed Columbarium at Kam Shan Road		Designed by: TLY	
Junction: (J3) Kam Shan Road/ Shek Lin Road		Checked by: LHW	
Scheme: Reference - Full Intake		Date: Feb-23	
Design Year: 2028	Job No.: CHK50650510		
Arm A: Kam Shan Road (EB)			
Arm B: Unnamed Road (To metered parking)			
Arm C: Kam Shan Road (WB)			

**Arm C**  
Kam Shan Road (WB)

TW	TW-Full
80	100
10	80

**Arm A**  
Kam Shan Road (EB)

TW	TW-Full
95	115
5	5

**Arm B**  
Unnamed Road (To metered parking)

TW	TW-Full
15	85
5	5

GEOMETRY					
Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.8652
	VI(b-a)	30		E	0.9324
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.7585

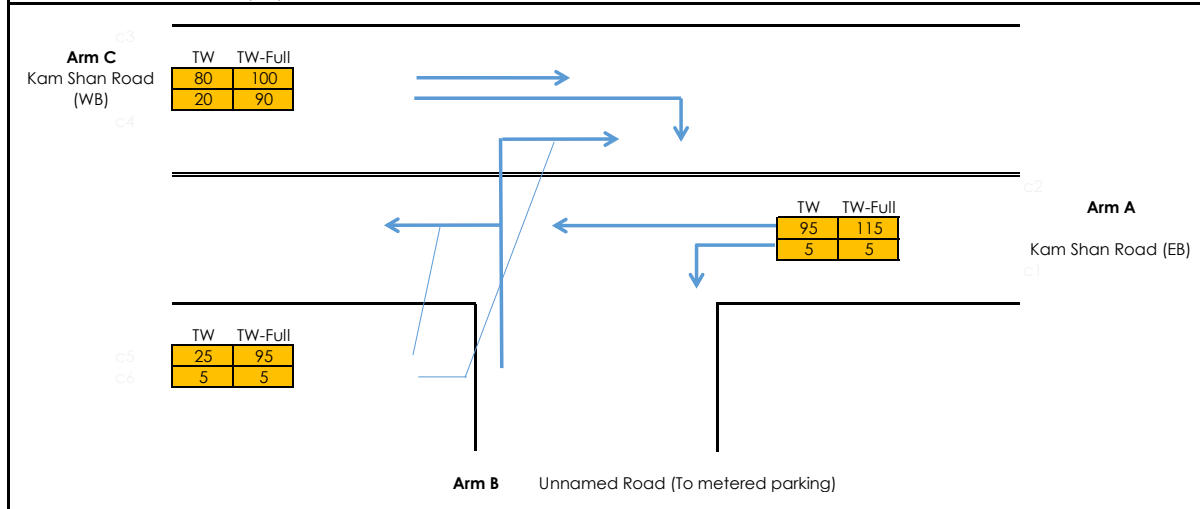
ANALYSIS		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	80	100
	q(c-b)	10	80
	q(a-b)	5	5
	q(a-c)	95	115
	q(b-a)	5	5
	q(b-c)	15	85
	f	0.75	0.94
CAPACITIES (pcu/hr)	Q(b-ac)	618.76	649.8
	Q(c-a)	1773.8	1589.1
	Q(c-b)	687.99	682.7
RFC's	c-a	0.05	0.06
	c-b	0.01	0.12
	b-ac	0.03	0.14
<b>RFC</b>		<b>0.05</b>	<b>0.14</b>

Where VI and Vr are visibility distances to the left or right of the respective streams  
 $D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$   
 $E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$   
 $F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$   
 $Y = 1 - 0.0345W$   
 $f = \text{proportion of minor traffic turning left}$   
 $Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a)$  Capacity of combined streams

**All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1**

## Simplified Priority Junction Capacity Calculation

Job Title:	Proposed Columbarium at Kam Shan Road	Designed by:	TLY
Junction:	(J3) Kam Shan Road/ Shek Lin Road	Checked by:	LHW
Scheme:	Design - Full Intake	Date:	Feb-23
Design Year:	2028	Job No.:	CHK50650510
Arm A:	Kam Shan Road (EB)		
Arm B:	Unnamed Road (To metered parking)		
Arm C:	Kam Shan Road (WB)		



### GEOMETRY

Major Road Width (m)	W	7.00	Lane widths (m)	w(b-a)	3.70
Central Reserve Width (m)	Wcr	0.00		w(b-c)	3.70
Blockage of major road right turn	Y/N?	Y		w(c-b)	3.90
Combined stream on minor arm	Y/N?	Y			
Visibility Distances (m)	Vr(b-a)	40	Calculated Parameters	D	0.8652
	VI(b-a)	30		E	0.9324
	Vr(b-c)	40		F	0.959
	Vr(c-b)	50		Y	0.7585

### ANALYSIS

		TW	TW-Full
TRAFFIC FLOWS (pcu/hr)	q(c-a)	80	100
	q(c-b)	20	90
	q(a-b)	5	5
	q(a-c)	95	115
	q(b-a)	5	5
	q(b-c)	25	95
	f	0.83	0.95
CAPACITIES (pcu/hr)	Q(b-ac)	633.93	650.91
	Q(c-a)	1747.7	1562.7
	Q(c-b)	687.99	682.7
RFC's	c-a	0.05	0.06
	c-b	0.03	0.13
	b-ac	0.05	0.15
<b>RFC</b>		<b>0.05</b>	<b>0.15</b>

Where VI and Vr are visibility distances to the left or right of the respective streams

$$D = (1 + 0.094(w(b-a) - 3.65))(1 + 0.0009(Vr(b-a) - 120))(1 + 0.0006(VI(b-a) - 150))$$

$$E = (1 + 0.094(w(b-c) - 3.65))(1 + 0.0009(Vr(b-c) - 120))$$

$$F = (1 + 0.094(w(c-b) - 3.65))(1 + 0.0009(Vr(c-b) - 120))$$

$$Y = 1 - 0.0345W$$

f = proportion of minor traffic turning left

$$Q(b-ac) = Q(b-c) * Q(b-a) / (1 - f) * Q(b-c) + f * Q(b-a) \quad \text{Capacity of combined streams}$$

All the above formulas are in accordance to T.P.D.M. Volume 2 Chapter 4 Appendix 1

## **Annex B**

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*Results of Kam Shan Village Residents' Meeting dated 17.7.2023*

至各尊敬的

城市規劃委員會：

**有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
由現有的「鄉村式發展」地帶改劃為「政府，機構或社區 (3)」地帶的改劃申請  
(編號：Y/TP/37)**

本村於 2023 年 7 月 17 日舉行了村委會，審議有關就「祥霞精舍」在錦山路 74-75 號向「城規會」申請私營骨灰安置所的規劃申請，在清楚了解該申請的發展內容、規模和性質，以及該發展日後都會得到私營骨灰安置所發牌委員會的法定監管，經過詳細討論後，在場議會人員及村代表，一致表示不反對，並認為該項目亦是應對今天村民對該設施的需求。同時，又有獨立出入口和人流管控，位置適合；多年來一直都是鄰里和睦，本村村民也沒有投訴，批准該申請，是今天多元社區配合設施的方向。

考慮上述，本村村委會接受該申請。

此上

出席者：

**錦山村村民代表**

邱炳

邱本立

錦山村村民代表簽署

邱錦明

邱錦明

邱志明

邱來福

邱美音

邱丁友

邱妙霞

邱二牛

邱天生

邱平

邱格珍

邱大牛

邱永發

邱勝

邱立仁

邱長勝

邱才

邱大志

邱志強

## **Annex C**

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*Supporting/No Objection Letters issued by Nearby Residents/Owners*



至各尊敬的

城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
由現有的「鄉村式發展」地帶改劃為「政府，機構或社區 (3)」地帶的改劃申請  
(編號：Y/TP/37)

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專此

李有仔

簽署：\_\_\_\_\_

No: \_\_\_\_\_ 錦山路

至各尊敬的

城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
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簽署：

李友

No: [REDACTED] 錦山路

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No:



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No:  錦山路



至各尊敬的

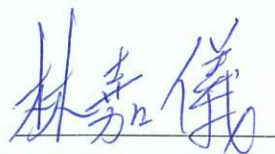
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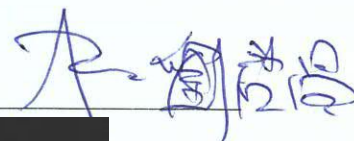
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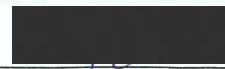
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簽署：

施華玉

No:



錦山路

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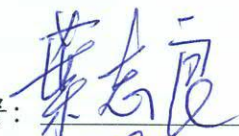
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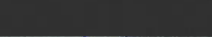
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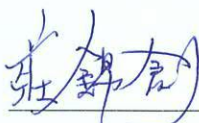
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專此

簽署：

洪志偉

No:

[Redacted]

錦山路

錦石新村

至各尊敬的

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簽署： 鄧明志

No:                      錦山路

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簽署： 何志明

No:                      錦山路



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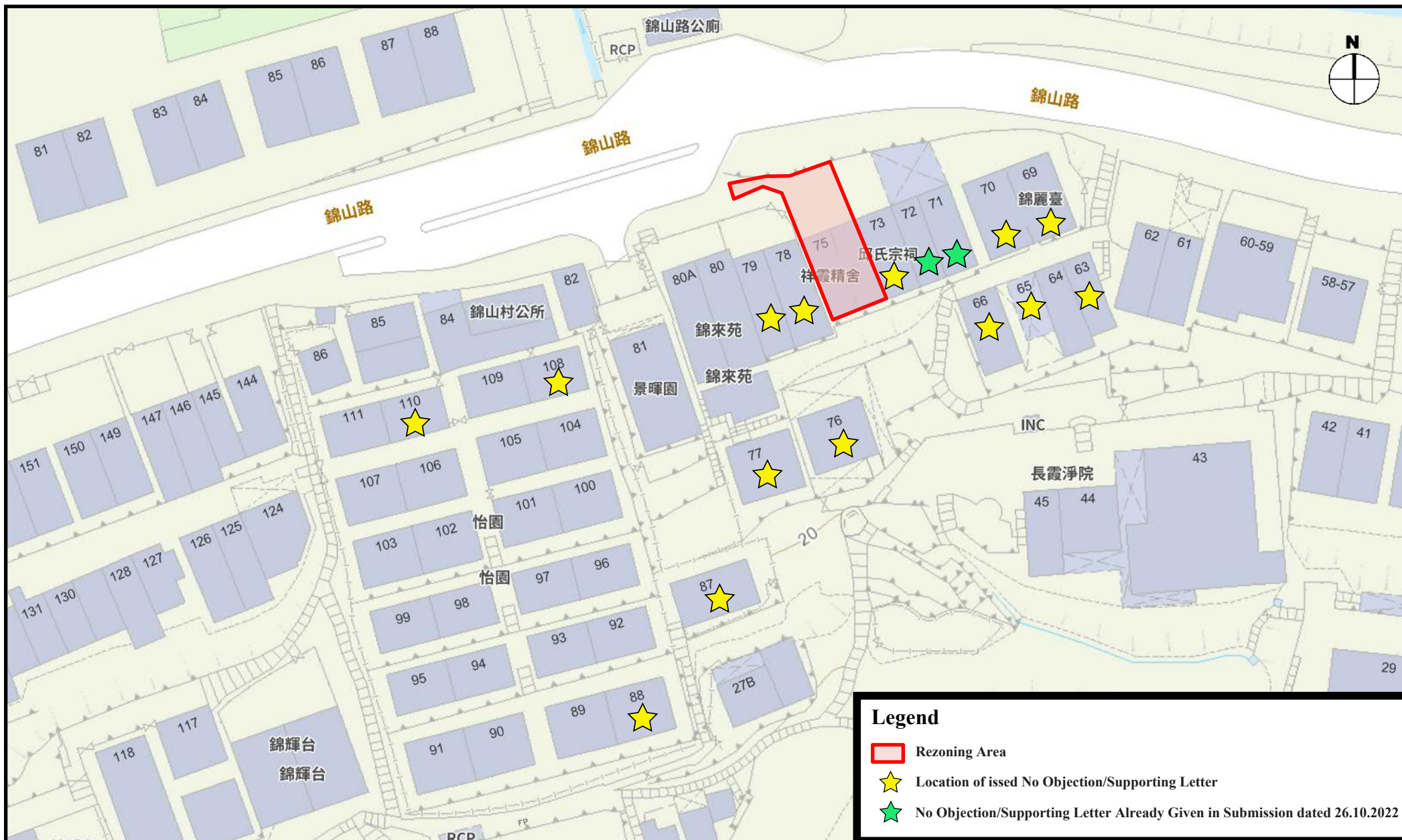
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專此

簽署：

No:  錦山路



### Legend

- Rezoning Area
- ★ Location of issued No Objection/Supporting Letter
- ★ No Objection/Supporting Letter Already Given in Submission dated 26.10.2022

Supporting/No Objection Letters issued by Nearby Residents/Owners

Plan A

Not to Scale

**VISION**

弘域城市規劃顧問有限公司  
VISION PLANNING CONSULTANTS LTD.

香港北角蜆殼街 9-23 號秀明中心 20 樓 C 室  
Unit C, 20/F., Seabright Plaza, 9-23 Shell Street, North Point, Hong Kong.  
Tel : (852) 2566 9988 Fax: (852) 2566 9978 E-mail: vision@visionplanning.com.hk



VISION PLANNING CONSULTANTS  
弘域城市規劃顧問有限

Appendix Ie of RNTPC  
**Paper No. Y/TP/37A**

Our Ref: TP-KSR2/RA/CHCS/20-15  
Your Ref: TPB/Y/TP/37  
Date: 25 September, 2023

**By Hand & Email**  
(tpbpd@pland.gov.hk)

The Secretary,  
Town Planning Board,  
c/o Town Planning Board Section,  
Planning Department,  
15/F, North Point Government Offices,  
333 Java Road,  
North Point,  
Hong Kong.

Dear Sirs,

**Rezoning Application from “Village Type Development” Zone to “Government, Institution or Community (3)” Zone at Nos. 74-75 Kam Shan Road and Adjoining Government Land, Tai Po, New Territories (Application No. Y/TP/37) – R-to-C Table**

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On behalf of the Applicant, we would like to submit herewith, a Responses-to-Comments (“R-to-C”) Table (**Annex A**) to respond to comments raised by the Transport Department on the captioned application.

Should you have any queries with regard to the above, please do not hesitate to contact our Miss Charissa LEUNG or the undersigned at 2566 9060.

Thank you very much for your kind attention.

Yours faithfully,  
for and on behalf of  
**VISION PLANNING CONSULTANTS LTD.**

Kim On CHAN  
Managing Director  
[KC/CL]

[TP-KSR2/RA/CHCS/22-15]

c.c. STNDPO (Attn.: Mr. YU Wai Kin, Nicol)  
Client

By email  
By email

## Annex A

Departmental Comments		Response
	Transport Department	
1.	<p><b><u>Proposed Holding Area</u></b></p> <p>The proposed holding area is an important crowd management measure proposed by the applicant to contain visitors arriving earlier than the pre-booked time slot and to avoid visitors spilling out to the public road managed by TD.</p> <p>It is noted that the proposed holding area falls outside of the site boundary and PCAO “will oversee the monitoring of the licensee's implementation of the approved management plan within the site boundary covered by the licence” only. Therefore, PCAO will not enforce the monitoring of implementation of holding area as proposed the applicant. Without the monitoring works and enforcement, site users may spill out to the public road (i.e. Kam Shan Road) managed by TD which is unacceptable.</p> <p>Under such circumstance, the applicant shall provide further assessment to substantiate that the</p>	<p>The proposed holding area will form part and parcel of the whole private columbarium development. It has been clearly indicated in the proposed crowd control plan highlighted in page 3-4 and Figure 4 in the Management Plan (“MP”) previously vetted and being accepted in-principle by the PCAO. TD is one of the consulting departments to oversee the relevant traffic and transport matters in association with the proposed private columbarium development at the subject site.</p> <p>The full copy of the MP has already incorporated in the planning statement (i.e. in <b>Appendix VI</b>). Upon receipt of the Town Planning Board’s agreement on the present rezoning proposal, the Applicant will also submit to the Lands Department an application for a Short-Term Tenancy to use, manage and maintain the subject Government Land, both the holding area and the existing ramp under application.</p> <p>With the implementation of the ‘visit by-appointment’ system (allows only a maximum of 25 persons per each 30-minute booking session) and the provision of a sizeable holding area which will be enclosed by fence, no on-site users will be spilled out onto Kam Shan Road at any circumstances is anticipated.</p>



## Annex A

	proposed columbarium use would not bring adverse traffic impact without the proposed holding area.	Please note that the MP under PCO will be revised from time to time to mitigate or to improve the operational/crowd control plan if and when necessary to the satisfaction for PCAO.
2.	<b><u>Proposed Access Ramp</u></b> 3. R-to-C, para. 3.2.13 and drawing no. 3.1: It is noted that the proposed railings on the existing ramp are not necessary from the traffic engineering point of view. Nevertheless, as the ramp is on unallocated government land, our previous comment remains valid, that is, comments from the management / maintenance parties of the access shall be consulted.	Noted. Upon Town Planning Board's agreement on the present rezoning application, the Applicant will submit to the Lands Department an application for a Short-Term Tenancy to use, manage and maintain the subject Government Land, both the holding area and the existing ramp under application. The Applicant has no objection to delete the proposed railings on the existing ramp as advised by TD.

### **Government, Institution or Community (3)**

Column 1 Uses Always Permitted	Column 2 Uses that may be permitted with or without conditions on application to the Town Planning Board
Columbarium (as specified in Remarks a, b, c and d below only)	

#### **Planning Intention**

This zone is intended primarily for regularisation of the existing columbarium facility. Any new development or redevelopment within this zone shall be low-density and low-rise in nature and shall be compatible and blend in harmoniously with its surrounding environment.

#### **Remarks**

- (a) No new development, or addition, alteration and/or modification to or redevelopment of an existing building shall result in a total development and/or redevelopment in excess of a maximum floor area of 87.09m<sup>2</sup>, a maximum site coverage of 54.43% and a maximum building height of 1 storey; and not more than a total floor area of 87.09m<sup>2</sup> for the purpose of columbarium use.
- (b) The total number of columbarium niches in this sub-area shall not exceed 1,700 (i.e. a maximum of 272 for single-niches; 1,062 for twin-niches and 366 for family-niches up to 4 members), which shall include not less than 50 twin-niches offered to indigenous villagers of Kam Shan Village at the average selling prices of public columbarium niches in that year. Columbarium development shall not be provided in areas other than Lot No. 738 S.C and Lot No. 738 S.C ss. 1 in D.D. 6 (i.e. at Nos. 74 and No. 75 Kam Shan Road respectively).
- (c) A self-contained entrance access point with boundary metal fence around the peripheral site boundary shall be provided and maintained at the expenses of the private columbarium operator at all times.
- (d) The proposed columbarium in this zone should be operated in accordance with the Management Plan, to be registered in the Private Columbarium Licensing Board (“**PCLB**”) which includes, but not limited to, the closure of the columbarium during the Special Festival Periods [i.e. Ching Ming and Chung Yeung Festival Days and their shadow weekends (Saturday and Sunday) and public holiday(s) 2 weeks before and 2 weeks after each Festival Day, as well as the 1st to 14th day of the Lunar Chinese New Year], the ‘visit-by-appointment’ system, operational hours, maximum number of visitors per each visit session, house rules, and other operational modes / requirements permitted / subsequently imposed by PCLB.
- (e) Based on the individual merits of a development or redevelopment proposal,



minor relaxation of the gross floor area/site coverage/building height restrictions stated in paragraph (a) above may be considered by the Town Planning Board on application under section 16 of the Town Planning Ordinance.

**Detailed Comments from Relevant Government Departments**

**Licensing Aspect**

1. Comments of the Director of Food and Environmental Hygiene (DFEH):
  - (a) as regards the applicant's proposed measures of 'not to open the proposed columbarium development during Ching Ming and Chung Yeung Festival days and their shadow periods' and 'visit-by-appointment' system, PCAO will have no comments provided that:
    - (i) the applicant undertakes to lock up the columbarium during the proposed closure periods;
    - (ii) the proposed period of closure of the columbarium is clearly stated in the proposal (e.g. during Ching Ming Festival and Chung Yeung Festival Days and the two weekends before and two weeks after the two festival days, including any public holiday between festival days and the two weekends mentioned);
    - (iii) as some niches in the columbarium have already been sold, the applicant undertakes to :
      - inform the affected parties (including the purchasers of the sold niches) in writing that these measures are proposed by the applicant and will be implemented if its planning application and licence application are approved; and
      - remind the concerned parties by letter, email, fax, SMS and any possible notification means of the total closure of the columbarium during the specified periods at least one month in advance of each closure period; and
    - (iv) for future buyers of the interment rights of niches in the concerned columbarium, the applicant undertakes to state clearly in the sales agreements that the columbarium will be completely closed during the specified periods and a 'visit-by-appointment' arrangement will be adopted for the specified periods and explain these arrangements to the potential buyers before entering into the sales agreements.
  - (b) as regards the applicant's proposed measures of the 'visit-by-appointment' system in the Management Plan, PCAO will have no comments provided that;

- (i) the applicant undertakes to (a) inform the affected parties (including the purchasers of the sold niches) in writing that the House Rules, special traffic arrangements and a 'visit-by-appointment' system arrangement are proposed by the applicant and will be implemented; and (b) remind the concerned parties by letter, email, fax, SMS and any possible notification means of the above-mentioned arrangements during grave sweeping festival periods at least one month in advance of each period;
- (ii) for future buyers of the interment rights of niches, the applicant undertakes to state clearly in the sales agreements that above-mentioned arrangements will be adopted and explain these arrangements to the potential buyers before entering into the sales agreements; and
- (iii) the applicant should keep a register for the 'visit-by-appointment' system for all the visitors, to record the information include (but not limited to): the date and time of visit session, niche no. to be visited, the name of visitors, the no. of visitors, the time of leaving the columbarium, the date of booking, the means of booking, etc so that monitoring on compliance of such mandatory 'visit-by-appointment' system arrangement could be conducted by FEHD staff on the spot.

#### Drainage

- 2. Comments of the Chief Engineer/Mainland North, Drainage Services Department (CE/MN, DSD):
  - (a) the applicant should have its own stormwater collection and discharge system to cater for the runoff generated within the Site and overland flow from surrounding of the Site, e.g. surface channel of sufficient size along the perimeter of the Site; sufficient openings should be provided at the bottom of the boundary wall/fence to allow surface runoff to pass through the Site if any boundary wall/fence are to be erected. Any existing flow path affected should be re-provided. The applicant should neither obstruct overland flow nor adversely affect the existing natural streams, village drains, ditches and the adjacent areas. The applicant is required to maintain the drainage systems properly and rectify/modify the nearby existing/original drainage systems if they are found to be inadequate or ineffective to accommodate the additional runoff arisen from the development of the Site. The applicant shall also be liable for and shall indemnify claims and demands arising out of damage or nuisance caused by failure or ineffectiveness of the modified drainage systems caused by their works. Please be reminded that the runoff within the subject premise including the runoff from the rooftop shall be served by a designated stormwater collection and discharge system and shall not be drained to the public sewerage network;

- (b) DSD's maintained public sewers exist in the vicinity but the feasibility of sewerage connection is subject to the invert level of discharge connection pipe leading from the Site. No stud pipe is reserved for sewerage connection. The applicant shall demonstrate the technical feasibility of sewerage connection. Should the applicant choose to dispose of the sewage of the proposed development through other means, views and comments from EPD should be sought;
- (c) provision of written consent(s) from relevant lot owner(s) and/or LandsD's permission for laying new drains/channels and/or modifying/upgrading existing ones outside the Sites or on government land (where required); and
- (d) the cost and work of drainage and sewerage connection shall be borne by the applicant.

### Building Matters

- 3. Comments of the Chief Building Surveyor/New Territories West, Buildings Department (CBS/NTW, BD):
  - (a) the applicant's attention is drawn to the following points:
    - (i) if the existing structures are New Territories Exempted House (NTEH) under the Building Ordinance (Application to the New Territories) (Cap 121 or the previous Cap 322), DLO/TP should be in a better position to comment on the application;
    - (ii) if there are existing structures which had been erected on leased land without approval of the BD (not being an NTEH), they are unauthorized under the BO and should not be designated for any approved use under the subject application unless such are permissible under the PCO (Cap 630);
    - (iii) for unauthorized building works (UBW) erected on leased land, enforcement action may be taken by BD to effect their removal in accordance with BD's enforcement policy against UBW as and when necessary. The granting of any planning approval should not be construed as an acceptance of any existing building works or UBW on the site under the BO;
    - (iv) if the applied use is subject to the issue of a licence, the applicant should be reminded that any existing structures on the Site intended to be used for such purposes are required to comply with the building safety and other relevant requirements as may be imposed by the licensing authority;
    - (v) the Site shall be provided with means of obtaining access thereto from a street and emergency vehicular access (EVA) in accordance with Regulations 5 and 41D of the Building (Planning) Regulations (B(P)R) respectively;

- (vi) if the Site does not abut on a specified street of not less than 4.5m wide, its permitted development intensity shall be determined under Regulation 19(3) of the B(P)R at the building plan submission stage;
- (vii) noting that the applied use is a columbarium, the premises should comply with specified design and construction requirements for columbarium facilities as outlined in PNAP APP-154; and
- (viii) detailed comments under the BO will be provided at the building plan submission stage.

From: [REDACTED]  
To: letplm4@landsd.gov.hk  
Date: 2022/11/10 上午 07:48  
Subject: 有吳Y/TP/37申請

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羅靜文女仕，地政主任

我是錦山[REDACTED]業主，強烈反對Y/TP/37申請改變土地用途。是次已經超過兩三次申請，之前已經被否決多次，我不明白地政署又可以接受申請。

因為現時祥霞精社屋頂已非法加建了兩大天窗，我住[REDACTED]經常見到有煙霧出，並聞到燒香味，令我家人及附近住居覺到不安。

希望羅主任派員入祥霞精社內查證有否偷步非常改建及放置骨灰龕場。

我代表多位居民反對意見轉達，期望早日徹底解決。

錦山[REDACTED]林生林太地下吳生

錦石[REDACTED]李小姐[REDACTED]李生

錦石[REDACTED]全体住户

[REDACTED]業主

葉繼鴻上

聯絡電話 [REDACTED]



# 規劃申請 PLANNING APPLICATION



申請編號 Application No.	Y/TP/37
地點 Location (見下圖 See Plan Below)	大埔錦山路74-75號丈量約份第6約地段第738號C分段及第738號C分段第1小分段毗連政府土地 Lots 738 S.C and 738 S.C ss.1 in D.D. 6 and Adjoining Government Land, 74-75 Kam Shan Road, Tai Po
圖則 Plan	大埔分區計劃大綱核准圖編號S/TP/30 Approved Tai Po Outline Zoning Plan No. S/TP/30
建議 Proposal	把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶 To rezone the application site from "Village Type Development" to "Government, Institution or Community (3)"

任何人士均可就這宗申請提出意見。有關意見必須於2022年11月25日或之前，以專人送遞或郵遞(香港北角渣華道333號北角政府合署15樓)、傳真(2877 0245或2522 8426)或電郵(tpbpd@pland.gov.hk)方式，向城市規劃委員會提出。

Any person may make comment on this application. The comment must be made to the Town Planning Board by hand or post (15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong), fax (2877 0245 or 2522 8426) or e-mail (tpbpd@pland.gov.hk) on or before 25 Nov 2022.

## 詳情 Particulars

- 這是根據《城市規劃條例》(下稱「條例」)第12A條提出的申請。  
This is an application made under section 12A of the Town Planning Ordinance (the Ordinance).
- 公眾可在城市規劃委員會(下稱「委員會」)或該宗申請作出考慮前，登入委員會的網頁([https://www.info.gov.hk/tb/en/plan\\_application/Y\\_TP\\_37.html](https://www.info.gov.hk/tb/en/plan_application/Y_TP_37.html))或掃描本通告的二維碼，及到下列地點查詢這宗申請。  
Until the application has been considered by the Town Planning Board (the Board), the application is available for public inspection on the Board's website ([https://www.info.gov.hk/tb/en/plan\\_application/Y\\_TP\\_37.html](https://www.info.gov.hk/tb/en/plan_application/Y_TP_37.html)) or scanning the QR code in this Notice and at the following locations.

規劃署規劃資料查詢處 Planning Enquiry Counters, Planning Department  
(熱線 Hotline: 2231 5000)  
香港北角渣華道333號北角政府合署17樓  
17/F, North Point Government Offices, 333 Java Road, North Point, H.K.  
新界沙田上禾輋路1號沙田政府合署14樓  
14/F, Sha Tin Government Offices, 1 Sheung Wo Che Road, Sha Tin, N.T.

- 根據條例，所有向委員會提出的意見，均會供公眾查閱。  
All comments made to the Board will be available for public inspection under the Ordinance.

### 個人資料聲明 Statement on Personal Data

委員會就任何意見所收到的個人資料會交給委員會秘書及政府部門，以根據條例及相關的城規會委員會規例指引的規定作以下用途：

- (a) 處理這宗申請，包括公佈有關意見供公眾查閱，同時公佈提出意見人士(下稱「意見人」)的姓名及供公眾查閱；以及
- (b) 方便從意見人與委員會秘書及政府部門之間進行聯絡。

The personal data submitted to the Board in any comment will be used by the Secretary of the Board and Government departments for the following purposes:

- (a) the processing of this application which includes making available the name of the person making the comment (hereafter known as "commenter") for public inspection when making available the comment for public inspection; and
- (b) facilitating communication between the "commenter" and the Secretary of the Board/Government departments.

In accordance with the provisions of the Ordinance and the relevant Town Planning Board Guidelines.

### 重要提示 Important Notes

- 委員會考慮申請的暫定會議日期已上載於委員會的網頁([www.info.gov.hk/tb/](http://www.info.gov.hk/tb/))。考慮規劃申請與舉行的會議(進行商議的部分除外)，會於公眾開放，歡迎公眾出席。請於擬定會議日期前，向秘書處索取表格，以便向委員會秘書處索取表格。表格會按先到先得的原則分配。  
The tentative date of the Board to consider the application has been uploaded to the Board's website ([www.info.gov.hk/tb/](http://www.info.gov.hk/tb/)). The meeting for considering planning applications, except the deliberation parts, will be open to the public. For observation of the meeting, reservation of seats can be made with the Secretariat of the Board by telephone (2231 5001), fax (2877 0245 or 2522 8426) or e-mail (tpbpd@pland.gov.hk) at least one day before the meeting. Seats will be allocated on a first-come-first-served basis.
- 秘書處將在考慮申請的工作，會在該日期前向委員會秘書處索取表格。表格會按先到先得的原則分配。  
The paper for consideration of the Board in relation to the application will be available for public inspection after issue to the Board Members at the Planning Enquiry Counters of the meeting. Seats will be allocated on a first-come-first-served basis.

## 位置圖 Location Plan

(只作識別用 for identification purpose only)



城市規劃委員會  
2022年11月4日  
Town Planning Board  
4 Nov 2022

(任何人在未經委員會許可而塗污、修改、毀壞或移走本通告，均可被檢控或受罰。)  
(Any person who obliterates, defaces, destroys or removes this notice without the authorization of the Board may commit a criminal offence.)



就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

221112-105921-59353

提交限期

Deadline for submission:

25/11/2022

提交日期及時間

Date and time of submission:

12/11/2022 10:59:21

有關的規劃申請編號

The application no. to which the comment relates:

Y/TP/37

「提意見人」姓名/名稱

Name of person making this comment:

CHAN

意見詳情

Details of the Comment :

本人反對是次申請，有關公司已經多次不同時間及相關位置，把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶，村民已多次反對，但有關公司不斷申請，只是想村民睇唔到的一次，博成功，或是政府漠視有關反對意見，多次都不成功，何以覺得會也得到村民的支持???

提供一個獨立出入口，並以鐵欄圍封申請範圍，進一步減低對附近居民的滋擾<~~~~但本身存在已是一個滋擾，對附近的樓價做成不可逆轉的創傷，申請者未有任何成功申請，但現已於附近地方進行改建，包括對本區及附近的土地利用、交通、環境、排水、排污、園景、視覺景觀及岩土造成非常大的不良影響，明顯已做成對附近居民的更加深及進一步的滋擾。

成為業界及回應社區關注的好典範<~~~~大埔區議員胡耀昌曾表示，有關位置太接近民居，就長霞兩次向城規會申請更改土地用途，其議辦便分別收到約500份及100份反對意見書，居民主要憂慮長霞若成為正規的私營骨灰龕場，「佢將來會唔會繼續擴張經營，吸引更多人去拜祭」，骨灰龕位「有價有市」，恐會加劇對居民的影響!!!在未批准先改建下，只會立下不良先例，造成的連鎖效應會令鄉郊環境惡化，或吸引更多不合法的例子，骨灰龕位明顯是有價有市。

能夠提供與周邊住宅發展的最大隔離空間及就社區對這類設施提供一個有意義的選擇<~

有關公司已經多次不同時間及相關位置，把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶，村民已多次反對，但有關公司不斷申請。村民已經作出拒絕的明顯選擇。

鄉村式發展根本不應該改政府、機構或社區(3)」地帶，這樣才是最大隔離空間給予鄉村式發展地帶。

有關申請機構已將附近屋村改建為宗教特色建築物，嚴重違申請地點原來的鄉村式發展，如果批准是次申請只能立下不良先例，縱容及包庇不妥行為。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

221116-142315-23029

提交限期

Deadline for submission:

25/11/2022

提交日期及時間

Date and time of submission:

16/11/2022 14:23:15

有關的規劃申請編號

The application no. to which the comment relates:

Y/TP/37

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. Yeung

意見詳情

Details of the Comment :

本人反對於大埔錦山路 74-75 號丈量約份第 6 約地段第 738 號 C 分段及第 738 號 C 分段第 1 小分段和毗連政府土地，把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶。

由於該位置鄰近村屋，若改變該地段用途將會增加該處人流，變相造成治安問題。其次該位置路段狹小，泊車位置少，假日交通阻塞將會十分嚴重。另外改變用地後所衍生的其他問題如空氣污染、噪音問題等均沒有提出任何方案來改善問題。

所以本人極度反對將該段改劃其他用途。

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review****參考編號**

221117-220917-47452

**Reference Number:****提交限期**

25/11/2022

**Deadline for submission:****提交日期及時間**

17/11/2022 22:09:17

**Date and time of submission:****有關的規劃申請編號**

Y/TP/37

**The application no. to which the comment relates:****「提意見人」姓名/名稱**

先生 Mr. Yau Chi Leung

**Name of person making this comment:****意見詳情****Details of the Comment :**

作為錦山村的原居民村長本人代表本村村民強力反對是次改劃的申請，該「鄉村式發展」地帶內的土地，主要預算供原居民興建小型屋宇之用。申請闢設的靈灰安置所提供1 700個龕位。申請會導致靈灰安置所用途與住宅用途為鄰，位置過於接近，造成掃墓者與村民交雜，與錦山村現時的環境不相協調。倘批准這宗申請，會為同一「鄉村式發展」地帶內其他同類申請立下不良先例。累積影響所及，會形成零散的靈灰安置所發展，導致該區的鄉村環境變差，整體環境質素亦會下降。申請興建的龕場位於錦山村中央，毗鄰民居、祠堂及村公所，距離附近鄉村亦只是一步之遙，對附近的村落造成極大困擾，而距離太近更對附近的傳統風水造成一個負面影響。批准是次申請等同縱容違規龕場先營運後申請的模式，亦對其他同類申請立下不良的先例。倘批准該等同類申請，累積影響所及，會導致骨灰安置所用途在該「鄉村式發展」地帶內進一步擴散，繼而令鄉村環境中土地用途不協調的問題惡化。」

根據食物環境衛生署網頁所述，公眾骨灰龕位供應 (2019年至2024年) - 政府以地區為本推行骨灰安置所發展計劃，已在各區物色到可供發展公眾骨灰安置所的用地，其中14個項目合共提供約60萬個新骨灰龕位。除了每年1次綜合編配公眾骨灰龕位之外，食物環境衛生署自2020年12月起每月都推出約1,700個位於曾咀靈灰安置所的新建龕位供市民申請。因此，現時公眾龕位有足夠供應，基本上無需輪候。故懇請各委員不批准是次申請

# 就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號  
**Reference Number:** 221125-181210-97636

提交限期  
**Deadline for submission:** 25/11/2022

提交日期及時間  
**Date and time of submission:** 25/11/2022 18:12:10

有關的規劃申請編號  
**The application no. to which the comment relates:** Y/TP/37

「提意見人」姓名/名稱  
**Name of person making this comment:** 女士 Ms. E Tam

意見詳情  
**Details of the Comment :**

大埔錦山村已經有多間骨灰龕場，並不適宜再有所增加，春秋二祭時交通十分影響附近居民。  
焚燒香燭和製品亦對附近居民做成很大的滋擾。

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**Application No.: Y/TP/37**

19/11/2022 22:35

From:

To:

File Ref:

tpbpd@pland.gov.hk

致城規會：

本人是錦山村村民，得悉祥霞精舍向貴會申請把「鄉村式發展」地帶內的土地，改劃為靈灰安置所，以提供1700個骨灰龕位，就這宗申請，本人提出下列反對意見！

1. 申請不符合「鄉村式發展」土地規劃意向；
2. 拜祭時所燃燒的冥鏹灰燼，會隨風擴散滋擾周邊民居；
3. 錦山路交通負荷日益嚴重，難抵額外的人流車流；
4. 為附近一帶的未獲發牌的骨灰龕場立下極壞先例；
5. 助長私營龕場先經營，後申請的經營方式

就此本人特些來信，望貴會審核（祥霞精舍）申請時，將本人所關注和意見納入規劃考慮中。謝謝！

姓名：Suk Yi YAU（錦山村原居民）

19.11.2022



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**Y/TP/37 DD 6 1-75 Kam Shan Road Niches**

25/11/2022 03:08

From: [REDACTED]  
To: tpbpd <tpbpd@pland.gov.hk>  
File Ref:

Y/TP/37

Lots 738 S.C and 738 S.C ss.1 in D.D. 6 and adjoining Government Land, 74-75 Kam Shan Road, Tai Po

Site area : About 159.99sq.m Includes Government Land of about 72.9sq.m

Zoning: "VTD"

Proposed Amendment: Rezone " to "GIC (3)" / 1,700 Niches / ??? Parking

Dear TPB Members,

So site size reduced but number of niches the same. Rent seeking from the dead is infinitely more lucrative than renting subdivided units to the living.

The provision of so many niches in a building surrounded by residences and with no parking facilities is untenable.

Previous objections upheld.

Mary Mulvihill

**From:** [REDACTED]  
**To:** tpbpd <tpbpd@pland.gov.hk>  
**Date:** Thursday, 26 November 2020 3:41 AM CST  
**Subject:** Y/TP/32 DD 6 1-75 Kam Shan Road Niches

Y/TP/32

Lots 738 S.A ss. 1, 738 S.A RP, 738 S.B, 738 S.C and 738 S.C ss.1 in D.D. 6, 71-75 Kam Shan Road, Tai Po

Site area : About 230.64sq.m

Zoning : "VTD"

Proposed Amendment : Rezone to "GIC (3)" / **1,700 Niches**

Dear TPB Members,

The discussion on May 27 did not appear to consider the commercial element of the facility.

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review****參考編號**

221118-004845-88477

**Reference Number:****提交限期**

25/11/2022

**Deadline for submission:****提交日期及時間**

18/11/2022 00:48:45

**Date and time of submission:****有關的規劃申請編號**

Y/TP/37

**The application no. to which the comment relates:****「提意見人」姓名/名稱**

先生 Mr. Kenny Yau

**Name of person making this comment:****意見詳情****Details of the Comment :**

建議範圍太接近民居，改為骨灰龕安置所勢必影響附近居民生活。春秋二祭期間亦造成嚴重交通問題。市場及政府已有足夠骨灰龕供應，此改建計劃多此一舉。

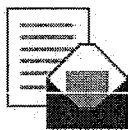
20. In response, Messrs Johnny Wong and Yuen Seen Pun, the applicant's representatives, made the following main points: (a) the subject columbarium had a total of 1,700 columbarium niches, of which **162 were sold and 66 were occupied;**

Kam Shan village had a population of about **5,700** people;

While the Applicant talks about providing for local demand, clearly it would take many generations of villagers to take up the niches, so the intention is to sell them to outsiders. In other words a business so neither a VTD or GIC use.

Mary Mulvihill

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**Fw: For the District Officer Tai Po - Town Planning Board section 12A application Y/TP/37**

17/11/22 09:30

From: Janice Hui Ling TANG/PLAND/HKSARG  
 To: Yuki Man Yin YIU/PLAND/HKSARG@PLAND  
 Cc: Carman Chui Ying CHEUNG/PLAND/HKSARG@PLAND, Feddy Sui Hei LEUNG/PLAND/HKSARG@PLAND  
 File Ref:

Dear Yuki,

I refer to the preceding email forwarded by DO(TP),HAD, I should be grateful if you could count the comment from Mr. Chung below as one of the public comments for the application. Thank you.

Regards,  
 Janice TANG  
 TPG/TP1, STN DPO  
 Tel: 2158 6240

----- Forwarded by Enquiry TPDO/HAD/HKSARG on 10/11/2022 15:53 -----



**Danny Chung**

08/11/2022 17:01

To dotp@had.gov.hk  
 cc

Subject For the District Officer Tai Po - Town Planning Board section 12A application Y/TP/37

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For the District Officer Tai Po

Dear Sir/Madam

I refer to the latest application for columbarium use Y/TP/37 submitted to the Town Planning Board.

Apparently in the application, the applicant claims widespread support from indigenous villagers and the Yau clan Tso/Tong for this application.

I strongly dispute this claim.

The applicant's claim cannot possibly be believed.

According to previous media reports, the Yau clan Tso/Tong, when taken as a whole, is strongly opposed to the columbarium.

To my knowledge, the Yau clan Tso/Tong, when taken as a whole, remain opposed to the columbarium.

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## Submission of comment in respect of Section 12A application Y/TP/37

25/11/2022 13:28

From: [REDACTED]  
To: Town Planning Board <tpbpd@pland.gov.hk>  
File Ref:

22 attachments

 Apple Daily report 2 Feb 2009.pdf  
 Comment by DO for YTP27.jpg  
 Extract of Ming Pao report 1 Mar 2009.jpg  
 Photo on 9 October 2022.jpeg  
 1669352321149.jpeg  
 1669352511529.jpeg  
 R1025817.jpeg  
 Enlargement of R1025817.jpeg  
 unknown.jpg  
 unknown.jpg  
 Extract of CACV 231-2011 .jpg  
 Extract of TPB review of April 2014.jpg  
 OZP at Pai Tau Village.jpg  
 Map of Y-ST-13 Pai Tau Village .jpg  
 Building height restriction press release.jpg  
 R1201643.JPG  
 R1201801.JPG  
 R0100629.JPG  
 IMG\_4028.jpeg  
 unknown.jpg  
 R1221724.jpeg  
 unknown.jpg

Dear Sir/Madam

I refer to the section 12A application Y/TP/37 to rezone V zoning to GIC zoning to allow regularisation of columbarium use.

This the FIFTH attempt by the applicant before the Town Planning Board.

How does it feel to be begging again, on your knees, for the FIFTH time?

I was very happy when the first application Y/TP/14 was withdrawn in 2012

I was very happy when the second application Y/TP/18 was rejected in 2013.

I was very happy when the third application Y/TP/27 was rejected in May 2020.

I was very happy when the fourth application Y/TP/32 was withdrawn in March 2021

It is the destiny for this fifth application to fail. I will make sure of it.

The applicant has been pissing about with this business project since 2007 when it bought the village house to tart up to serve as the columbarium.

Now 15 years later, the project still has not been the money maker the project proponent thought it would be.

This columbarium was supposed to be a get-rich-quick scheme.

Set up a columbarium, sell a few niches at high prices, the high prices sold would serve to allow a high valuation of the business.

This business is then sold to a clueless investor and the original owners no longer have any responsibility for the niches and have a good return on investment.

The owners are now desperate to sell.

I can feel it.

The only way they can sell is if the columbarium can secure planning permission which then allows for securing a licence to operate a private columbarium.

No planning permission, no licence, no sale of the business, no return on investment.

This business will hang like a millstone around the necks of the investors until they too meet their makers.

By then, the owners will be past caring about what happens to the columbarium and the niche owners and the visitors.

The owners will have totally given up.

And that is how it should be.

They should have never gone into the columbarium business.

This columbarium will fail, it is its destiny to fail.

For this latest application, it will be my pleasure to subject this application to ridicule and contempt that it clearly deserves.

### **Para 1.3 - feeling aggrieved**

What was there to be aggrieved about?

Was the applicant such a pussy that he is easily upset by being shut out of a TPB meeting?

Didn't the applicant know that deliberation sessions are done behind closed doors after all the supporting materials and statements have been submitted?

What other information could the applicant could have possibly offered that was so powerful, so convincing, that the members could decide in a more holistic manner?

The comments by members made for Y/TP/27 were essentially the same as those made by members for the previous application Y/TP/18.

Whatever information that needed to be more powerful, more convincing should have been included for Y/TP/27.

Does the applicant possibly believe that giving the information verbally to the members makes it more convincing to the members?

In the last application Y/TP/32, the applicant also expressed he was aggrieved with the members during Y/TP/27 but for some reason withdrew the application in March 2021.

### **Para 1.9 - location to close to residential buildings**

So the applicant now finally understands that the columbarium was, perhaps, slightly too close to residential buildings?

After all this time.

So what happened to the last application Y/TP/32 in November 2020 when the applicant proposed moving the niches from no 74 to the other side of the ancestral hall at no 72?

The fantastic plan was now to have a gap of one house lot between no 74 and the village house at no 78.

Try and put some distance in from the residential village house at no 78.

I suspect the Planning Dept thought the new proposal for Y/TP/32 was a non-starter.

Now the applicant has decided to copy the Buddhist Cheung Ha approach, provide a sealed off access area to the columbarium, in the hope that this will minimise nuisance and somehow create separation.

### **Para 1.10 - consultation with resident at no 78**

The applicant claims the resident at no 78 was consulted.

What is the exact status of this resident, a tenant or the owner-occupier?

Resident on the G/F, 1/F or 2/F?



All the floors were sold off separately after the house was completed.  
The applicant could only get one resident at 78 (on which floor?) to give support?  
How about the residents on G/F, 1/F and 2/F of no 79?

So what happened? Couldn't get the others to give support?

As a tenant, the resident could fuck off any time to another place, free from columbarium niches.

An owner-occupier has no such option unless he wants to sell, and trying to sell a property next to a columbarium is difficult.

**Para 1.10 iv - claim of no psychological problem**

So one resident at no 78 has no problem living next to a columbarium and the applicant assumes then that everybody would have no problem.

Is the applicant that deluded?

In 2010 (reported in the news) when some fucking idiot tried to set up a columbarium at the village house at 114 Kam Shan, there was universal opposition from residents.

That fucking idiot eventually gave up and was forced to dismantle the niches and sell the property.

The house at no 114 reverted to the previous residential use.

And what is the comment about the 21st century and being more friendly?

Fuck that.

The act of opening a columbarium next to an ancestral hall and next to a residential building was an open act of hostility, an act of aggression.

A friendly neighbourhood columbarium?

What a laugh.

If you ask a lot of people, they would prefer a convenience store or restaurant rather than a columbarium next to them.

What a fucking stupid claim to make.

**Para 1.11 - claim of first application Y/TP/18**

This is entirely misleading.

The first application was Y/TP/14 in December 2010.

I still have the documents on file.

It was first submitted by the solicitors acting for the applicant.

These solicitors were in way over their head, completely clueless about town planning procedure.

The applicant then appointed Vision Planning Consultants to take over from the solicitors.

Seems like Vision completely forgot about this first application which was withdrawn on by mid 2012.

**Para 1.12 - "fully legal under the lease"**

The New Grant for the lot was an early period New Grant, the details of which cannot be located by Lands Dept or the Land Registry.

Early period New Grants did not have robust user restrictions unlike those granted in the 1930s onwards.

At the time, the District Officer who supervised land sales would not have envisioned the risk of future columbarium use.

Most of the uses acceptable at the time were village housing or ancestral hall, and certainly not columbarium.

There was no reason to think that such uses would continue into the future at the time in

1909.

Unfortunately for Kam Shan, these early period New Grants did not have a specific use restriction.

As such to say that columbarium use is “fully legal under the lease” is quite misleading. There is no use restriction, a historical mistake that was corrected later in other New Grants by the government.

The lease does not specifically endorse columbarium use.

That endorsement can only come from the Town Planning Board and it is not forthcoming now, or ever.

**Para 1.15 - propose Column 1 use for Schedule of Uses**

I see the applicant has again attempted to circumvent section 16 applications by demanding that columbarium use is put in column 1 in the proposed schedule of uses.

Doing so will prevent the Town Planning Board from exercising any more control over columbarium use.

I strongly object to this proposal.

**Para 1.18 - providing meaningful alternative choice**

What is this with “meaningful”?

Is there a deep philosophical meaning in how to handle cremated ashes?

There is nothing meaningful about this columbarium.

To the ancestral hall, the columbarium is “meaningful” in the way it outrageously insults the Yau clan by setting up next to the hall.

To me, it will be all the more “meaningful” when this columbarium fails to succeed and is forced to close.

**Para 4.2 - “support letter” issued by representatives of Yau’s Ancestral Hall**

This “support letter” is extremely misleading.

The Yau clan Tso/Tong consists of three fong 房, with two fong in Lam Tsuen Tai Po and one fong in Kam Shan.

To my knowledge, the Yau clan Tso/Tong have yet to agree upon an authorised manager as per section 15 of the New Territories Ordinance.

I have been informed that for a long time, the two fong in Lam Tsuen are in dispute with the fong in Kam Shan over who should be manager.

The two fong in Lam Tsuen are extremely displeased with the Kam Shan fong due to the presence of the columbarium.

As such it is far from settled that this so-called “support letter” actually and accurately represents the unanimous view of the entire Yau clan Tso/Tong.

Remember it is the Yau clan Tso/Tong that owns the the ancestral hall, not just the “representatives” of the Kam Shan fong of the Yau clan.

May I remind Members of the Board that in the briefing paper for Y/TP/27, the District Officer wrote the following:

May I remind Members of the Board of the news media report on 2 Feb 2009, copy below:

May I also remind Members of the Board that the columbarium took legal action against the Yau clan Tso/Tong under High Court case: HCA576/09 as reported in this report in Ming Pao newspaper 1 Mar 2009:

It appears to me that the chief signatory for this “support letter”, the village representative Mr

Yau, has apparently unilaterally appointed himself manager of the Tso/Tong.  
However Mr Yau it would appear he has not gone through the process prescribed under section 15 of the New Territories Ordinance.

As such this "support letter" does not possess any legal authority or weight.

Mr Yau here does not have any authority to represent the Yau clan Tso/Tong.

I do not believe for one minute that the Yau clan Tso/Tong have ever agreed to this columbarium and its subsequent planning applications.

This opposition is still being maintained.

#### **Para 4.3 - use of no 71 and 72 Kam Shan as ancillary storage**

From the planning statement for Y/TP/32:

Total area of 71 Kam Shan is 47.55 m<sup>2</sup>

Total area of 72 Kam Shan is 40.00 m<sup>2</sup>

What kind of owner of land with building entitlement can possibly allow valuable building land to be used merely as "ancillary storage use"?

Is the owner really that generous or completely idiotic?

Such a use represents a supreme example of wasteful use of building land.

Land that could be used for building three storey NTEHs is now reduced to carrion duty of supplying storage use and toilet use.

#### **Para 4.5 - intention to apply for Short term Tenancy for Government land**

When the columbarium first commenced operations in 2008-9, it attempted to apply for a short term tenancy for the government land.

Members of the Yau clan Tso/Tong, upon regular inspection visits of the ancestral hall, discovered this application to apply for a STT.

On the last day to submit letters of objection, members of the Tso/Tong managed to submit a letter of objection and Lands Department rejected the application.

I do not believe the Yau clan Tso/Tong would ever agree to have an area fenced off as STT for exclusive use by the columbarium.

#### **Para 4.6 - Offer to ell 50 twin niches to indigenous villagers of Kam Shan**

What makes the applicant think that all indigenous villagers are buried in the traditional manner, in a coffin lowered into the ground?

My grandfather, an indigenous villager, was cremated.

I witnessed his coffin go into the furnace, for fuck's sake.

No massive grave site for him in some remote area in the NT.

The majority of people who die in Hong Kong are cremated as a matter of course, even if they are indigenous villagers.

As such the claim that this will save land in the countryside is misleading.

This offer of 50 twin niches reserved for indigenous villagers is nothing but a little disguised inducement for indigenous villagers to agree to this columbarium.

Members of the Board should reject this offer out of hand.

#### **Para 4.8 - Proposal to close operation during Special Festival Periods**

If the applicant thinks his proposal to use prior appointments for visits will work, he is likely to be disappointed.

Some people will still prefer to just turn up on the fly without a moment's thought, without

making prior appointments.

Here is a photo that I took on 9 October 2022, Sunday after the Chung Yeung Festival on 4 October:

As Members of the Board can see, there are people hanging about outside, unable to enter the columbarium and generally making themselves look like intruding outsiders.

The person in charge of the columbarium appeared to be nowhere in sight.

People are just going to turn up anyway.

If they can't get inside the proposed fenced off area, they will simply do the tombsweeping activities right on the pedestrian pavement and on the lay-by by Kam Shan Road.

The photo taken on 15 November 2022 below shows the area leading up the slope to the application site:

Below is another view of the area, taken on 15 November 2022:

Visitors are simply going to spill out onto the pedestrian pavement and lay-by if they cannot get into the fenced off area.

You can see how close the houses at no 78 and 79 are to the application site.

#### **Para 4.12 - Claim of no burning offerings**

There may be no burning of offerings in the proposed Rezoning Area, but how about outside the Rezoning Area?

There is nothing to stop people burning offerings outside the Rezoning Area, right on the pedestrian pavement along Kam Shan Road.

Below is an extract of my comment in respect of the submission of comment for the previous application Y/TP/32:

#### **Para 4.12 - no burning of offerings in the halls or at no 71 and no 75**

The applicant claims there will be no burning of offerings at the niche halls at no 72 and 74 and also at no 71 and 75.

However the applicant does not mention anything about banning the burning of offerings OUTSIDE of the application site, on the open space on government land in front of no 71-75 Kam Shan.

So what is this photo, taken on 25 October 2020 on Chung Yeung Festival day:

See the flames and the smoke and the location?

Think the photo is too small to see? Here's an enlargement below:

This act is particularly despicable, placing the furnace nearer the Yau clan ancestral hall, as if pretending that it is the members of the clan paying respects at the ancestral hall.

But notice that the gates to the ancestral hall are not open and thus these worshippers cannot possibly be members of the Yau clan.

In fact, these worshippers were visiting and went inside the columbarium.

To think that the applicant claims its house rules will prevent burning of offerings.

Don't make me laugh.

End of extract

Let us face reality, people are going to burn offerings all the same.

This proposed ban isn't going to work.

**Para 4.13 - Toilets at no 71 and 72 Kam Shan**

Again what kind of idiotic property owner is content to have his two houses at no 71 and 72 Kam Shan be used as storage and toilets?

The property owner, in this case Winner Faith, is losing money hand over fist in such an arrangement.

If the columbarium use was removed, the two lots could be redeveloped as 3 storey NTEH and would be able to be rented out as village housing.

Instead, these two lots are being absolutely wasted on being storage and toilets.

There is a severe housing shortage in Hong Kong and these two lots are being wasted on storage and toilets.

The priorities of the applicant are plain to see, pure self-enrichment.

**Para 4.16 - use of appointment system**

The columbarium could be using this appointment system now.

With reference to my comments in respect of Para 4,8 above, I fail to see how people will keep to this arrangement.

Based on what I have seen, some people will just prefer to turn up unexpectedly and do the tomb sweeping rites outside the columbarium.

Don't believe me, what's this then? A photo I took on 5 April 2019, right on Ching Ming Festival, when the columbarium boasted that it would be closed on festival days.

Want to see more? Here's another photo on the same day:

People will just continue to fucking turn up unannounced, do the tombsweeping bollocks and then fuck off afterwards.

**Para 5.1 - Proposal to establish fenced off area on government land**

I seem to recall the entrance to the columbarium was once had some metal railing in front of it, see the photo of 5 April 2019 above.

Then it was removed, presumably because the metal railing stood on government land and Lands Dept complained.

I believe it is safe to say that the Yau clan Tso/Tong, the vast majority of its members, and not just the members living in Kam Shan, will not look kindly on this ridiculous proposal to seal off part of the government land.

This will prevent easy access to the Tsz Tong, even if the applicant gives keys to the Yau clan.

What happens if the person in charge of the keys dies and did not tell anyone where he put them?

What happens if the person in charge of the keys has mislaid the keys and cannot find them?

What happens if the lock has mechanical failure?

The Yau clan Tso/Tong is accustomed to the present practice of simply walking over government land to the Tsz Tong.

They are not going to be especially pleased at having to go through a gate and then another gate just to get to the Tsz Tong.

**Para 5.3 - so called no objection letter from resident at no 78 Kam Shan**

The person who signed this letter, I consider to be having mental problems.

Only an idiot or a person of reduced mental capacity would agree to sign a letter like that. I strongly suspect the letter was drafted by the applicant and simply given to the person to sign.

If this person agreed, why didn't the other residents agree as well?

There are six flats at 78-79 Kam Shan.

So the applicant could not get all the residents in the six flats to agree to sign?

Does the applicant honestly believe that just because one person signed a no objection letter, that this is enough to overcome legitimate concerns regarding "too close to residential buildings"?

That this no objection letter is sufficient to allay concerns about "too close to residential buildings"?

In the last application Y/TP/32, the fact that the applicant proposed to empty house at no 75 Kam Shan of niches and move it over to no 73 Kam Shan is a direct acknowledgement of the fact that the columbarium is "too close to residential buildings"

Only in Kam Shan do we have an idiot operator who thought he could get away with having a columbarium right next to a house.

#### **Para 5.6 - town planning an essential tool**

When the applicant first bought the property in 2007, he certainly did not intend to adhere to town planning with regard to the siting of a columbarium.

He would have found out there and then in 2007 that columbarium would be considered absolutely inappropriate.

Only now does the applicant realise that getting town planning approval is the prerequisite for any planned operation of a columbarium.

However it is too late to use town planning to save the applicant's columbarium project.

#### **Para 5.8 - Claims that columbarium use is a compatible community use in a Village zoning**

I certainly do not agree with this claim by the applicant.

I believe the logic of the applicant went like this in his brain as follows this:

- columbarium is used by the community, so
- columbarium is a community use, and
- community use is compatible with village zoning, therefore
- columbarium use is compatible with village zoning

The applicant is aiming to muddy the waters over the extent of community uses in the Village zoning at Kam Shan.

The OZP was first imposed in 1980 but there were other uses that were existing prior to 1980 and they did not involve columbarium use for commercial profit.

My father and his brothers and sisters attended the former Buddhist primary school here back in the 1940s to 1970s.

"Private columbarium uses", in the sense for commercial profit, did not exist in Kam Shan prior to 1980.

If an operator wanted to use a property for columbarium use after the coming into effect of the OZP in 1980, he would have had to first apply for planning permission beforehand.

Any "private columbarium uses" after 1980 were clearly in breach of town planning regulations.



Is columbarium use really a vital part of community use in a V zoning?

Not really.

Columbariums can be located far away from the V zoning, away from causing a nuisance to adjacent residential properties.

Columbarium might be regarded by some people as a community use but that community use does not have to be in a V zoning.

Even this community use performed by columbarium is slowly but surely being usurped by the growing use of Gardens of Remembrance and sea burial.

Time to bring in more legal firepower, from the judges of the Court of Appeal when it looked at The Shrine court case (CACV 231/2011).

The Court of Appeal made it absolutely clear.

On the town planning level, "Village Development" V zonings are almost always restricted to providing village housing, and did not permit columbarium use, see extract below:

This Court of Appeal has interpreted the town planning regulations as this and it is now established law.

Allowing columbarium use by changing the zoning of village zoning will establish a precedent for other would-be columbarium businessmen hoping to establish a columbarium in a village at a convenient distance to the New Towns in the NT.

And as I mentioned earlier above, the Court of Final Appeal has established that Ding house rights are protected under the Basic Law.

Village zonings are for village housing.

Village zonings are not for columbarium use.

#### **Para 5.9 - existence of religious institutions in V zonings**

The reason why these religious institutions existed in V zoning was because of the prevailing religious practices at the time in early 20th century.

When there was no town planning at all to regulate use of land in villages so people did what they liked.

Columbarium use was not widespread at the time because the custom then was burial in cemeteries or burial plots for villagers far away from the village.

If this applicant went back in time to the 1920s and tried to set up a columbarium next to the ancestral hall, he would have been met with fierce resistance from villagers.

#### **Para 5.11 - applicant concluding that existing columbarium use is compatible with V zonings**

So just because the proposal ticks all the boxes with regard to land administration, environmental protection, traffic, drainage, building design, water supply, highways, etc, and coupled with that ridiculous no objection letter from the resident at no 78 Kam Shan, and that variety of use is desirable in V zoning, and that columbarium use is a community use, that this proposal should therefore be considered acceptable with regard to town planning matters.

Does the applicant honestly believe this is so?

The reason that the previous application Y/TP/27 was shot down by the Town Planning Board was because it was too close to neighbouring residential buildings.

The applicant has only himself to blame because he put the columbarium in the most

inappropriate place.

**Para 5.13 - quoting other planning applications to support this application**

The Town Planning Board has the absolute right to seek its own counsel, not the counsel of the applicant's planning consultant.

Section 12A application Y/ST/13 in Pai Tau village in Shatin district applied for rezoning to GIC zoning and was partially accepted.

Columbarium use was put into column 2 and so required a section 16 application.

What the applicant's planning consultant has failed to mention also is the fate of the subsequent section 16 application A/ST/816.

So what happened with A/ST/816?

In November 2013 the Rural and New Territories Planning Committee rejected the section 16 application.

In subsequent review of A/ST/816 in April 2014, the Town Planning Board on review upheld the previous decision of the RNTPC.

So what reasons did the Town Planning Board give?

Let's look at the meeting minutes, shall we?

Such applications for columbarium use "would set an undesirable precedent for similar applications".

Let the words sink in, undesirable precedent.

At the last minute, the Town Planning Board prevented a huge mistake being made.

There has been no subsequent section 16 application since then.

Ironic, isn't it?

You get section 12A approval but you can't use it.

The applicant's planning consultant has also conveniently neglected to advise the Board on the distribution of the privately held lots near the site of Y/ST/13.

This a map of the site of Y/ST/13 in terms of land use zonings:

The following is a map of the privately held lots near the site of Y/ST/13:

As one can see, there is much less concentration of privately held lots whether building lots or agricultural lots on the other side of the stream opposite to the site.

The rest is government land, some of which are either squatter huts or have a licence to occupy.

Kam Shan on the other hand has much higher concentration of privately held lots near the applicant's columbarium.

One also has to consider that the site of Y/ST/13 was originally government land and was illegally occupied without government permission.

So the columbarium had no business being there to begin with.

As for Y/ST/42, I may not have agreed with the decision but the Planning Dept noted in its briefing paper that the application site enjoyed slightly better objective conditions than the ones faced by the present applicant.

As for Y/ST/47, I may not have agreed with the decision by the Board but objectively, the application site was a fair distance away from the neighbouring village houses.

What kind of fucking idiot locates a columbarium right next to a residential village house?

#### **Para 5.15 - Offer to sell niches at cheaper prices of public niches**

This offer to commit to sell 50 niches at cheaper prices to indigenous villagers is nothing but a barely disguised inducement to get indigenous villagers to agree to the application.

The vast majority of bodies are cremated and not buried in the ground.

The niches are then interred in the family funeral urn "the Kam Taap" of the indigenous villager's family which has already been set up.

As such, there is next to no additional taking of a land just to provide the last resting place of the deceased.

Unless, of course they deceased in buried in a horseshoe shaped which takes up more land and this more expensive.

Few people, unless with great social standing, nowadays are prepared to go for a ostentatious horseshoe grave.

In any case, even if the person is buried, Chinese custom dictates that the person's remains are exhumed after about 5 years.

The bones are then placed in an urn and the urn placed in a Kam Taap.

The burial spot that was exhumed is then prepared for the next occupant who dies in the future.

Where do I get all this information?

Does the applicant honestly think that we residents at Kam Shan are complete ignorant idiots who are easily fooled?

Here, this is where I got the information:

<https://www.jstor.org/stable/23881299>

It is from the following scholarly publication:

Journal of the Hong Kong Branch of the Royal Asiatic Society Vol.1 (1960-61), pp.115-123,  
Published by: Royal Asiatic Society Hong Kong Branch

Another version of this paper appears here:

<https://www.britishempire.co.uk/article/hongkongthenappendixb.htm>

So the claim that offering niches will save 9600 square feet of land (see para 5.16) is extremely misleading.

#### **Para 5.19 - Government difficulty in finding suitable locations for columbarium**

The applicant mentions that only 14 out of 24 potential sites for public columbarium have secured District Council support.

What reason would the applicant like to advance that would explain this?

Let me answer.

Because residents in the some Districts are simply uncomfortable with having columbarium facilities built in their district, even if they are public operated.

It was only when the Government put its foot down in 2010-11 that some District Councils grudgingly agreed to allow public columbarium to be developed.

Does the applicant honestly believe that just because his columbarium is privately operated, it will receive less opposition from local residents?

#### **5.23 - Claim of 1538 new niches will relieve demand pressure**

This claim of a massive quantity of 1538 new niches making a tremendous contribution to relieving pressure on demand for niches is nothing but a claim.

Given the demand for niches, as the applicant claims, all these 1538 niches would be sold inside a year.

Once all sold, the columbarium is full up, and not more niches can be added.

The columbarium will serve no future purpose on meeting future demand for niches.

These 1538 niches are drop in the bucket.

#### **5.24 - setting a good example for other unauthorised columbarium**

Setting a good example, that's a laugh.

Do you call coming in in 2007, buying a village property, and despite objections, setting up a columbarium next to an ancestral hall a good example?

Good practice?

During that particular period, the New Territories experienced such behaviour from unscrupulous columbarium operators looking to make a quick buck.

Only now, after the imposition of the Ordinance, does the applicant acknowledge that perhaps it would have been wiser to get planning approval first.

#### **5.34 - equity rights of lots**

Has the applicant's planning consultant got his facts mixed up?

The original lot, lot 738 was sold by public auction in 1909 as a New Grant – refer to para 5.32. of the planning statement.

In Lands Department terminology, old schedule lots refer to the lots included in the Block Crown Leases, now Block Government Leases.

Lot 738 is not an old schedule lot.

It is a New Grant lot, albeit one that was issued very early on during British tenure of the New Territories.

It is statute law, such as the Town Planning Ordinance that ultimately controls land use and the intensity of that use.

Just look at development in Kowloon City area when Kai Tak airport was still in operation.

Height controls were imposed by legislation to control building heights even if the government lease allowed for more height.

Here's a government press release on this in 1998:

Hence even if lot 738 was a freehold lot, it would still come under statute law in terms of land use.

You can't do what you like with your land just because by virtue of ownership, whether under leasehold or freehold, and there is no restriction in terms of use or size.



Does the applicant believe he enjoys land use rights by virtue of the legal position established under the Melhado court decision of 1983?

Evidently the applicant is aggrieved that town planning legislation prevents him from enjoying "equity rights" for his lot, meaning he wants to be able to do what he likes because the lot has no use restrictions.

Well, tough. Life isn't fair.

Statute overrides whatever contractual arrangement.

### **5.35 - no impact on small house development**

Here the applicant has been strenuously arguing that there is no impact on Small House development.

Well unfortunately in November 2021 the Court of Final Appeal has finally settled the question of Ding rights for indigenous villagers. It is a right protected by the Basic Law.

Would the applicant and his consultant care to read the news report on the decision?

<https://hk.news.yahoo.com/丁權案終院駁回上訴-裁丁屋政策合憲-屬-基本法-保障權益-060000163.html>

I can't seem to find any clause in the Basic Law that protects columbarium use. I wonder why that is...?

Using no 74 and no 75 as columbarium has meant that 2 house lots have been removed from the land supply for Small House development.

Using no 71 and no 72 as ancillary storage and toilet has meant that another 2 house lots have been removed from the land supply for Small House development.

Old village houses next to ancestral halls can and have been redeveloped into Small Houses to supply more village housing.

This has occurred in several villages in the NT.

I know because I used to inspect newly built NTEH for rating valuation purposes on behalf of the Rating and Valuation Department.

When you visit dozens of villages in the New Territories, you start to notice a few things.

So using four house lots will not reduce land supply for Small Houses?

What do the following photos show then?

At Cheung Lek village:

At Kai Kuk Shue Ha village:

At Pan Long Wan village:

At Sheung Yeung Village:

At Fo Tan Village:

See the house lots next to the Tsz Tongs?

They got redeveloped into residential houses.

Is this too difficult for the applicant to understand?

Is this too difficult for the applicant's consultant to understand?

What about the impact on EXISTING Small Houses and other village houses?

Referring to a previously rejected application Y/TP/29 for Buddhist Cheung Ha columbarium, it was revealed in the application that one of the village houses, a Small House

at no 76 Kam Shan, in the application site was used as an office.

In fact, that Small House referred to was previously used as a vegetarian restaurant after it was bought by persons associated with the nearby Buddhist Cheung Ha.

The fact that the Small House, which is on DD6 lot 2089, has been used first as a vegetarian restaurant and then as an office means that the Small House has lost 12 years of residential use.

It will continue to lose that function for the remainder of the government lease.

This Small House, all three floors of it, could have been used by indigenous villagers or other villagers for residential purpose.

But no, the applicant of Y/TP/29 saw fit to waste this valuable resource on a vegetarian restaurant that to all intents and purposes turned out to be an unprofitable business venture and was then turned into an office.

Does an office really require all three floors of a Small House?

So the current planning statement before us claims no impact on Small House applications?

An undesirable precedent will be set if this application is approved.

The fourth application Y/TP/32 which was withdrawn has already revealed what potential future plans the applicant has for the village house lots at no 71 and 72 Kam Shan.

Approving this application will eventually mean that in future four house lots will be completely wasted on columbarium use.

Approving this application will mean other idiotic columbarium operators will try the same trick of putting columbarium in the middle of other villages.

#### **The story of section 16 application A/TP/429 (2009)**

Ever heard of section 16 application A/TP/429 for redevelopment to 2 storey village house at piece of land in front of 39-40 Kam Shan and adjacent to 55 Kam Shan?

This application was granted permission in July 2009 but redevelopment never took place.

The permission expired in July 2013 and the lots in question have been sitting empty, devoid of any redevelopment activity.

Why do you think that is? Have a look at this picture I took in April 2014:

Below is another photo I took in March 2021:

Just look at the site now, overgrown with vegetation.

It was about 2007 onwards that Buddhist Cheung Ha was starting to transform itself into a columbarium with theme park pretensions.

As a result, the owner of the site never went ahead with development even after getting planning permission.

Now in 2022, the site still lies empty devoid of any redevelopment work after over 15 years.

The two proposed houses, which were to be built to much higher Building Ordinance regulations as stipulated in the planning permission, were NEVER built.

The two proposed houses that could have been used for residential purpose after sale or renting never came into existence.

In fact according to the Land Registry searches, the lots in question, DD6 lots 978, 983 and 1045 which were bought in 1996 for HK\$5.5 million were sold to another company in September 2010 for a lower consideration of HK\$4.8 million.

The new company presumably seeing Buddhist Cheung Ha nearby decided to not proceed with redevelopment even after paying \$4.8 million for the site.



Any fool can see why.

Just look at the location.

Next to it are the flight of steps leading up to the columbarium, the main route of entry for visitors.

Any owner or occupier will not be very happy at hundreds and thousands of people walking up the stairs, peering at their house with curiosity, during the festival periods.

I myself made a personal visit on 2 March 2021 to the Building Information Centre at the Government Offices at West Kowloon to check whether there were any approved building plans.

I was advised by staff there that they did not have any building plans registered for the site.

These proposed houses were not ordinary NTEHs which are exempt from most of the BO regulations.

Submission of building plans for approval by BD was a condition of the planning permission by the Town Planning Board.

The validity of the planning permission expired on 24 July 2013 and the new owner did not make any attempt to extend the validity period of planning permission.

The presence of a columbarium thus acts more as a deterrent to redevelopment of NTEH or Small Houses and thus reduces the potential to increase residential housing.

### **The parasitic and unsustainable nature of columbarium use**

Columbarium use is ultimately a **PARASITIC** use of land.

Columbarium niches are an extremely wasteful use in terms of sustainable land use.

You put the cremated ashes in the niche and forget about them until Ching Ming and Chung Yeung festivals.

And the ashes are supposed to stay in the niche forever because according to traditional custom, you're not supposed to move the dead about because it hurts the feelings of the deceased.

That means niches are a one use only product.

Unlike other forms of property such as residential, commercial, industrial, etc which can be reused again and again as owners and tenants change.

In fact, properties can be redeveloped into other uses as the old use become economically unviable.

This is not possible with columbariums.

Niches cannot be reused by other users and the columbarium cannot be redeveloped into another use due to cultural restrictions.

With spare land in Hong Kong in short supply columbarium use is an extremely wasteful use of land.

Although public niches now have limited terms of period of use, thereby allowing for reuse, this can in no way keep up with annual demand for niches as it is already difficult to find suitable locations that won't have nearby residents complaining.

And private columbariums can only differentiate themselves from public columbariums by offering permanent niches to customers otherwise why would people choose a private niche when they could go to a public niche, even allowing for the longer wait for a public niche?

When a columbarium has exhausted its stock of niches, it will no longer have any future incoming revenues from the sale of niches.

The only possible revenue stream left would be management fees and donations.

As such it would be difficult to dispose of a completely sold out columbarium as a viable business because future revenue streams would be very low.

Therefore it would be extremely difficult to attract any potential buyer.

No investor would buy a columbarium that has no future potential to generate revenue and profits.

In fact, no investor would buy into any business if it has no future potential to generate revenue.

And why would a potential investor bind themselves to a long term service contract that was signed years ago with the previous operator?

It would not make business sense.

The current owners of the Cheung Ha will one day have to face the fact that they would have retire at some point in the future.

As it would be difficult to attract a potential buyer, the current owners will be unable to offload the columbarium and would be stuck with it.

Till they too die.

Their descendants would have to assume the burden of either continuing the operation with no prospect of making any money or take the easy way out and simply wind up the columbarium.

Columbariums are an unsustainable business operation.

### **So-called letters of support**

I would hardly attach any weight to standard letters of support from owners of the niches.

Of course, niche owners would support the application.

But it is a clear case of conflict of interest.

### **Concluding remarks**

This application is nothing but last ditch attempt to save the columbarium project for the project proponent.

I have nothing but ridicule and absolute contempt for this application.

I spit on this application.

I want to see the applicant despair over the future of his columbarium.

I call upon members of the Board to reject this application with extreme prejudice.

This application has no right to succeed.

The columbarium has no right to even exist.

Yours faithfully

Danny Chung



# 村民抗議骨灰龕破壞風水

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邱氏村民指祠堂旁邊的精舍設骨灰龕破壞風水，昨拉橫額抗議。



【本報訊】大埔錦山村邱氏族人，不滿有宗教團體在祠堂旁的精舍設骨灰龕，認為破壞風水，並指村內近日出現不吉利事情與此有關，昨日有20多名村民，在精舍外拉橫額及標語抗議。

## 祠堂與精舍毗鄰

據邱氏村民指出，其祖先於100多年前從大埔梧桐寨搬來錦山村，現時大部份村民已搬出居住，至農曆年三十回來祭祖，但當日知悉祠堂旁的一間精舍，竟設有骨灰龕，龕位共有4000個之多。村民指祠堂做紅事，骨灰龕則做白事，兩者有相沖並破壞風水，令家宅不安。

村民又指，村長邱錦明任航空公司地勤的女兒，去年5月遭劫殺，另有村民近日喪妻，以及出現疾病等，不排除與風水被破壞有關；同時，村民又發現祠堂有死魚、死貓及死老鼠，懷疑有人故意棄於祠堂內。

他們曾向精舍交涉，並向地政署及區議員求助，但沒有結果，昨晨9時許，約20多名村民手持橫額及標語，寫有「祠堂旁做庵堂，祖先難得安寢」、「村內擺放骨灰龕，招惹遊魂野鬼，村民被迫與鬼為鄰」字句。村民聲言會抗爭到底，堅決反對骨灰龕設在祠堂旁邊。



9.1.10 Comments of the District Officer/Tai Po, Home Affairs Department (DO(TP), HAD):

- according to his record, there was strong opposition from residents of Kam Shan Village and members of Yau Ancestral

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Hall (邱氏祠堂) on the columbarium activities. Complaints have been lodged to LandsD and reported on media. While the opposing sentiments quieted down recently, it is still expected that there will be grave local concerns and objections from residents, especially those living near the Site.

# 對 骨灰龕堂告誹謗

【明報專訊】在大埔錦山村祠堂旁經營骨灰龕堂的「祥霞精舍」，日前入稟高院控告兩名錦山村及梧桐寨村民誹謗，指他們今年2月誘導其他村民分別在祠堂內及精舍門外，掛上內容含誹謗成分的匾牌，令聲稱業務值1.2億元的精舍生意額大跌，要求禁止兩村民再作上述行為。

原訴公司是經營精舍的喜蓮有限公司，兩名被告分別是村內其中一名祖堂經理邱新光（譯音）以及該祖堂的村民邱觀連。精舍負責人鄭文昨向記者表

示，該地原有5幅地皮，其中4幅地皮已由原訴公司買入，其餘一幅由精舍持有。

精舍於去年1月斥資100萬元為物業裝修，至同年10月完工，兩個月後精舍正式開業。鄭文指精舍共有3900個骨灰位，價錢由1.7萬元至37萬元不等。

根據入稟狀，疑村民反對精舍開業，今年2月9日及15日，兩被告誘導其他村民分別在祠堂內及精舍門外，掛上誹謗精舍的匾牌，令其商譽受損，與訟要求法庭頒下禁制令，禁止兩被告再作誹謗行為。

## 指滋擾購買骨灰位客人

鄭文表示，自今年農曆新年過後，村民不時於周六及周日前來滋擾，每次一行4至5人，年齡40至50歲，他們會舉橫額指精舍影響村內風水等，有時客人到來購買骨灰位亦會受滋擾。他續稱，事實上錦山村起碼有5至10間精舍，不明白為何只有「祥霞精舍」影響風水。鄭表示，曾經主動與有關村民接洽，但遭對方拒絕，由於一直不知村民有何要求，故諮詢律師意見後決定以法律途徑解決。記者昨日至截稿前，仍未能與兩名被告聯絡上。

【入稟編號：HCA576/09】

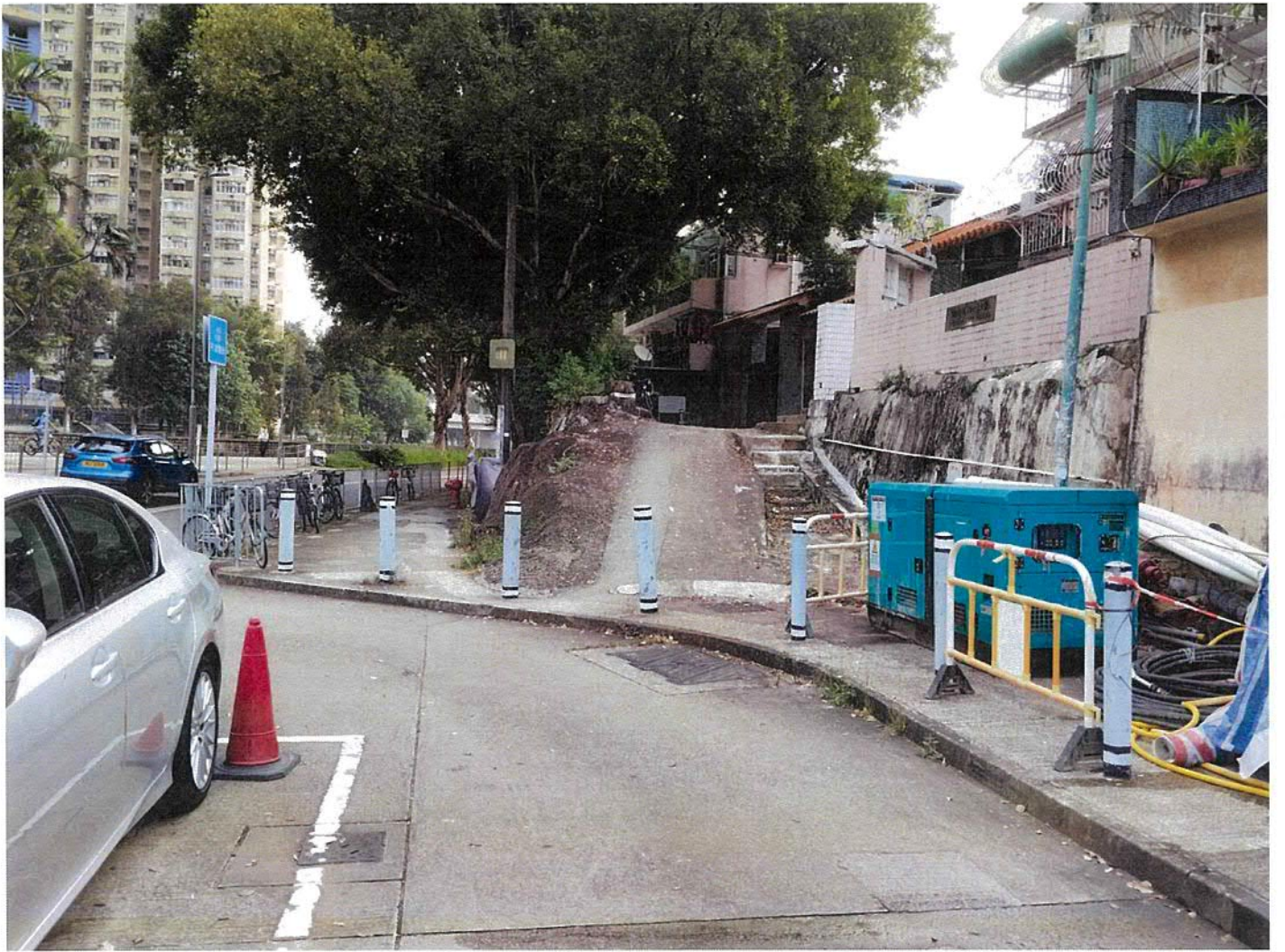


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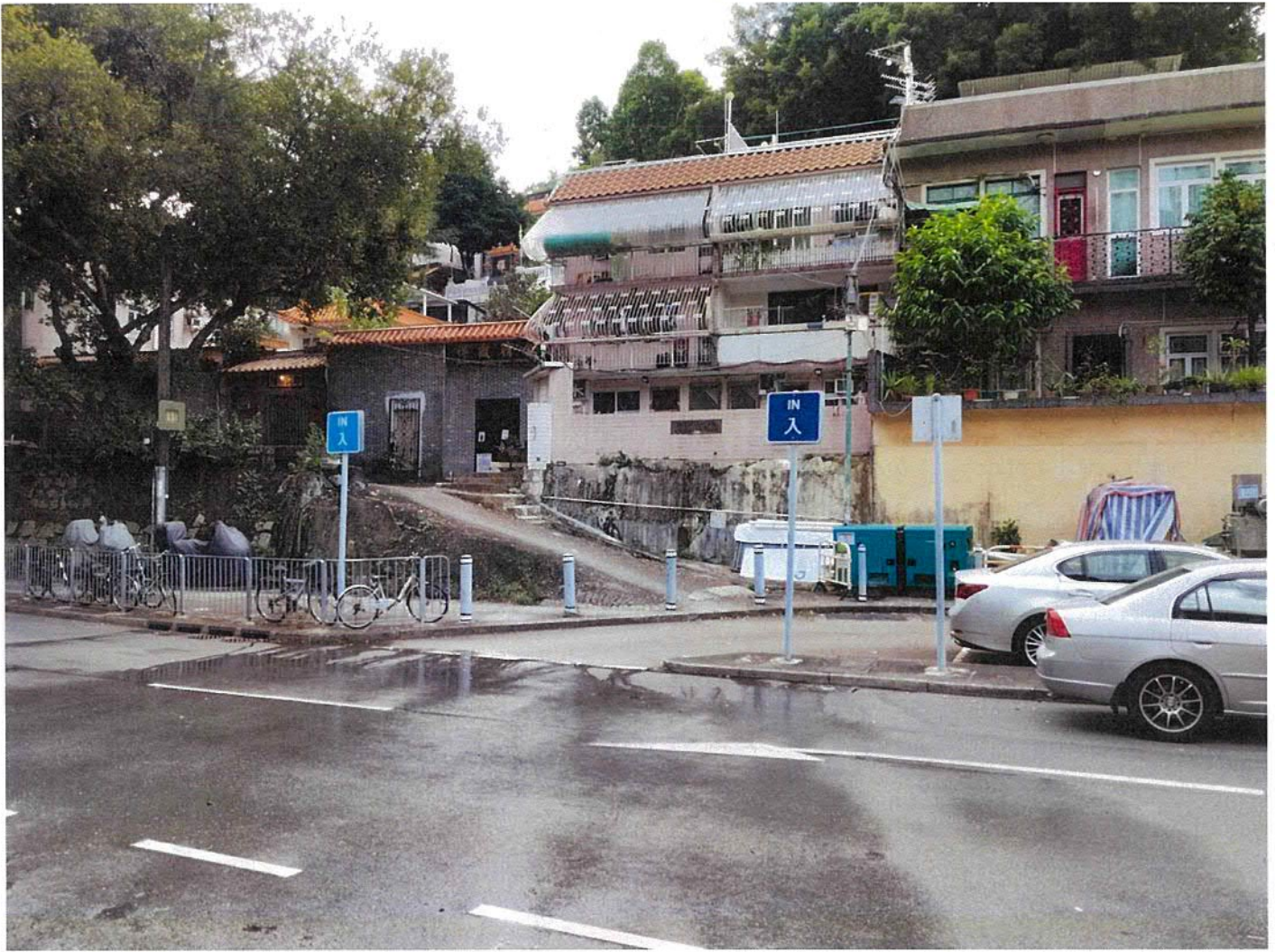










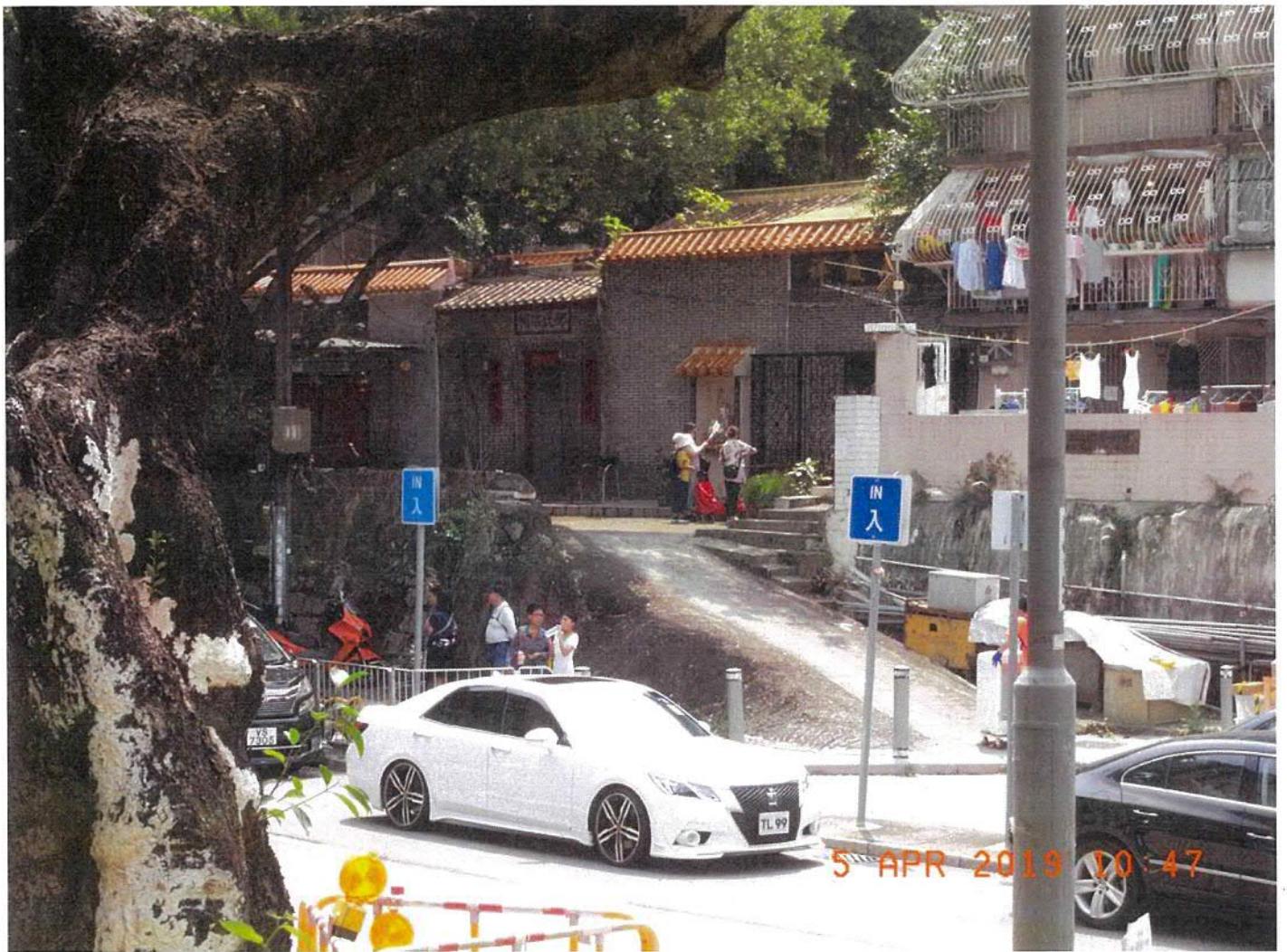


















16. But importantly, the OZP goes on to set out the “planning intention” of the V zoning :

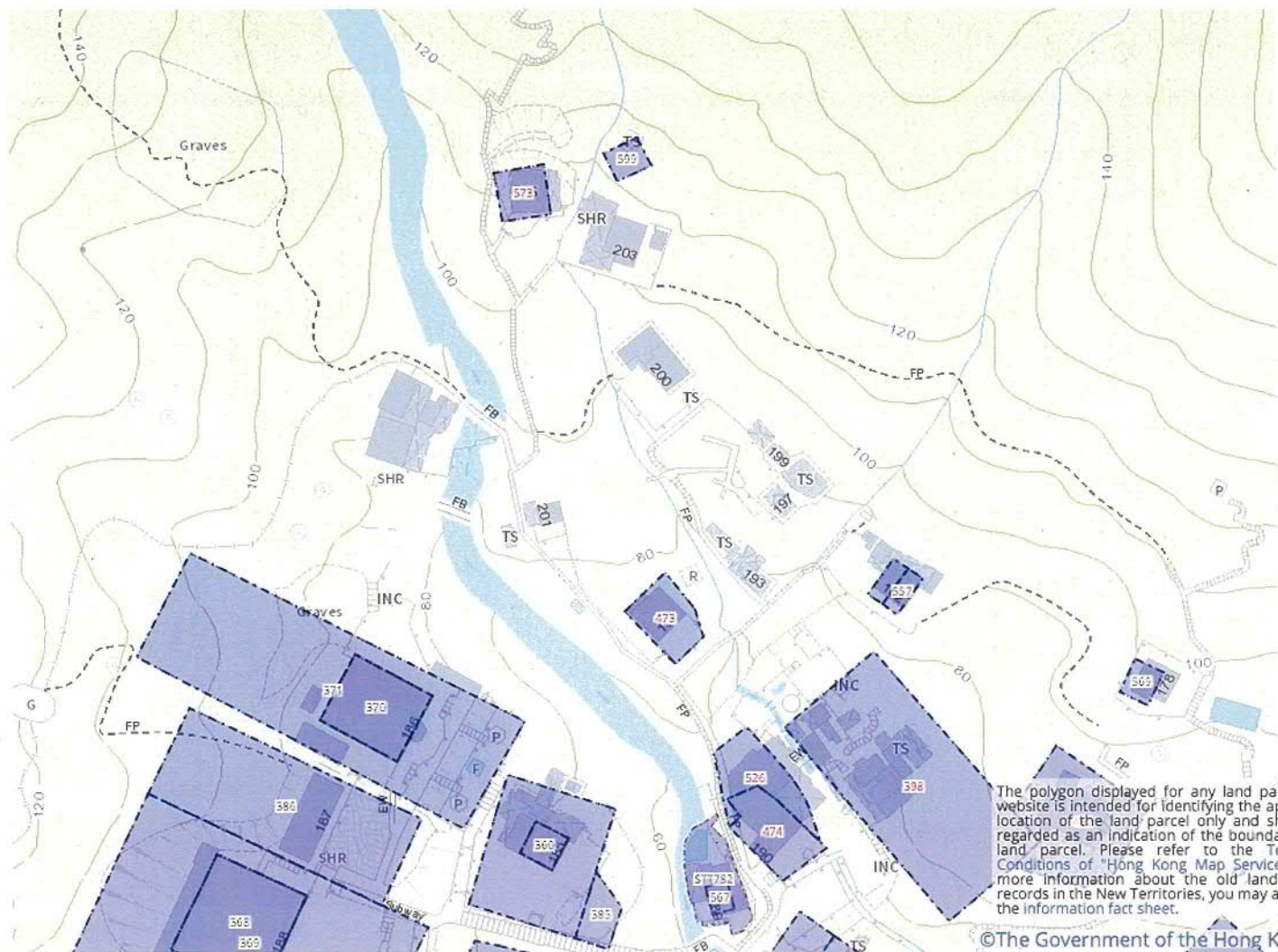
“The planning intention of this zone is to designate both existing recognized villages and areas of land considered suitable for village expansion. Land within this zone is primarily intended for development of Small Houses by indigenous villagers. It is also intended to concentrate village type development within this zone for a more orderly development pattern, efficient use of land and provision of infrastructures and services. Selected commercial and community uses serving the needs of the villagers and in support of the village development are always permitted on the ground floor of a New Territories Exempted House. Other commercial, community and recreational uses may be permitted on application to the Town Planning Board.”

17. It is plain that the development of the Lots as a columbarium of the size and scale described is wholly inconsistent with the planning intention of the V zone in the OZP. The planning intention set out in the OZP says specifically that the zoning is to designate both existing recognized villages and areas of land considered suitable for village expansion. Land within the zone is primarily intended for development of small houses by indigenous villagers. In other words, one is concerned with a predominantly village type neighbourhood. The planning intention goes on to say that “selected commercial and community uses serving the needs of the villagers and in support of the village development are always permitted on the ground floor of a New Territories Exempted House”, whereas “other commercial, community and recreational uses may be permitted on application to the Town Planning Board”. The Columbarium is obviously not intended to serve the needs of the neighbouring villagers only. Obviously, it is intended to attract business from others living elsewhere. Moreover, in full operation, it is bound to attract hundreds if not thousands of visitors at different times of the year and is likely to be used in conjunction with a range of religious or cultural practices such as incense burning and chanting. All this would have very little to do with supporting the village development of the neighbourhood. In this regard, it should be remembered that commercial and community uses serving the needs of the villagers and in support of the village development are only always permitted on the ground floor of a New Territories Exempted House. Here, one is talking about an entire building used for the purposes of housing over 1,000 individual columbaria.

- “(a) the traffic impact assessment submitted by the applicant has failed to demonstrate that the proposed columbarium would have no adverse pedestrian and vehicular traffic impact on the surrounding areas; and
- (b) the approval of the application would set an undesirable precedent for similar applications for the development of columbarium use. The cumulative impact of approving such similar applications would lead to proliferation of columbarium use and a general degradation of the traffic conditions of the area.”







The polygon displayed for any land parcel on this website is intended for identifying the approximate location of the land parcel only and should not be regarded as an indication of the boundary of the land parcel. Please refer to the Technical Information Conditions of "Hong Kong Map Service" for more information about the old land records in the New Territories, you may also refer to the information fact sheet.

# ***Daily Information Bulletin***

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## **Kai Tak building height restrictions lifted**

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The height restrictions of buildings in the territory which were previously prescribed to ensure the safety of aircraft using Kai Tak Airport are lifted today (Friday) upon the removal of the airport operation to the new aerodrome at Chek Lap Kok.

The Hong Kong Airport (Control of Obstructions) (Repeal) Order 1998 (L.N. 116 of 1998) taking effect today will repeal the Hong Kong Airport (Control of Obstructions) Order which prescribed restrictions on building height in respect of Kai Tak Airport.

"With the cessation of operation of Kai Tak Airport, the building height restrictions related to the former airport are no longer necessary," said a Planning, Environment and Lands Bureau spokesman.

"However, building height restrictions prescribed for the new airport at Chek Lap Kok under the Hong Kong Airport (Control of Obstructions) (No. 2) Order 1997(L.N. 218 of 1997) shall remain in force to ensure the safety of aircraft using the new airport."

"Plans showing the restrictions are deposited at the Land Registry for public inspection and can be bought at the Survey and Mapping Offices of the Lands Department," the spokesman added.

End/Friday, July 10, 1998

NNNN





























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25/11/2022 23:59

From: 各界關注骨灰龕法案大聯盟 <illegalccg@gmail.com>  
To: Town Planning Board <tpbpd@pland.gov.hk>  
File Ref:

1 attachment



反對祥霞精舍改劃申請事宜\_2022.11.25.pdf





各界關注骨灰龕法案大聯盟

ALLIANCE FOR THE CONCERN OVER COLUMBARIUM POLICY

致：骨灰所辦

有關：強烈反對 Y/TP/37 的規劃申請

各界關注骨灰龕法案大聯盟強烈反對上述的規劃申請，理由如下：

1. 有關項目的申請，曾作出多次的申請撤回及城規會拒絕申請的決定，反映有關項目在規劃指引的原則下有相當的爭議性。
2. 今次規劃的申請中，申請人聲稱獲邱氏成員理解及支持，但聯盟請委員注意，但邱氏成員共有 3 房，一房在林村，一房在大埔，另一房在錦山；據大聯盟的了解林村及大埔兩房皆強烈反對有關項目（因祥覆精舍就位於邱氏祠堂旁），另一方面，邱氏祖堂地的司理的選舉身份仍被受爭取，因此，申請人聲稱邱氏支持有關項目的申請，只是以偏蓋全；
3. 申請人在申請文件中表示在 2022 年 5 月 26 日的城規會議審議 Y/TP/27 的會議紀錄，表示有部份委員支持有關申請，但申請人並沒有提及更多委員認為：  
//27. 一名委員不支持這宗改劃申請，理由是申請地點的靈灰安置所用途與周圍環境不協調。副主席表示贊同，並稱批准這宗申請會為緊鄰申請地點南面的申請（編號 Y/TP/29）及其他同類申請立下不良先例。該名不支持這宗申請的委員進一步表示，由於附近的同類改劃申請（編號 Y/TP/29）涉及更大的範圍，拒絕理由 (a) 項指申請地點屬「零碎」改劃的用字可能不太合適，應適當修改。另一名委員贊同批准這宗改劃申請會為其他同類申請立下不良先例，亦認為並無劃增益可合理支持目前的建議。  
29. 一名委員認同靈灰安置所用途的需求與日俱增，但認為這宗申請的靈灰安置所過於接近住宅民居，故不支持這宗改劃申請。//

經委員商議後決定否決有關申請，理由：

33. 經進一步商議後，小組委員會決定不同意這宗申請，理由如下：「(a) 申請地點位於劃為「鄉村式發展」地帶的範圍內，該地帶的規劃意向主要是供原居村民發展小型屋宇。擬議的靈灰安置所用途與該區現有的鄉村環境不協調，尤緊鄰擬議用途西面和南面的住宅民居為甚。此外，亦沒有有力的規劃理據支持把申請地點由「鄉村式發展」地帶改劃為「政府、機構或社區 (3)」地帶，以便就靈灰安置所用途提出申請。申請地點目前劃作「鄉村式發展」地帶，實屬恰當；以及 (b) 批准這宗申請會為「鄉村式發展」地帶內的其他同類改劃申請立下不良先例。倘批准該等同類申請，累積影響所及，會導致零散的靈灰安置所發展，繼而引致該區的鄉村環境轉差和整體環境質素下降。」

而今次申請人的規劃申請的規模及理據沒有太大的改變下，聯盟認為城規會應尊重 2022. 05. 26 的審議決定，反對有關申請。

申請人聲稱申請的龕位只有 1,700 個，但事實上申請人向骨灰所辦申請的可存存骨灰數目達 3,860 份骨灰，

聯盟請委員注意，有關項目屬違規發展，申請人聲稱自 2007 年開始申請，但一直被當村的村民反對，在 2022 年 5 月 26 日的會議上已有委員提問錦山村的人口問題，根據回覆，現時錦山村有超過 5 千人口，而且附近仍有多個骨灰龕場的規範化申請，包括長覆淨覆，小靈山，半春園等等，一旦仍獲，將帶出不良的先例。

基於以上的原因，聯盟強烈反對有關改劃的申請

各界關注骨灰龕法案大聯盟謹啟  
2022.11.25.



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**Objection to App No. Y/TP/37**

14/01/2023 06:21

From:

To:

File Ref:

"tpbpd@pland.gov.hk" &lt;tpbpd@pland.gov.hk&gt;

To whom it may concern.

Dear Sir/ Madam

I am writing to request you to decline the application ( No. Y/TP/37) of rezoning the village type development to institution or community etc, other than its original purpose.

Furthermore, apart from charging each application to a payment of not less than \$200000, I also suggest the same application of rezoning should not be repeated more than once in a year.

Limited government resources have to be allocated to more important and essential aspects such as planning a more spacious, quiet and pollution free HK, not to processing applications of which the ultimate use is against the government's green policy.

Please tell the applicant concerned to treat living people better rather than the deceased ones.

Well beings of living people are the top priority among all regarding the precious limited land use in HK.

To conclude, I object to any rezoning applications of the village type development all over hk, let alone beautiful Taipo.

Thank you for your attention.

Regards

A love Taipo resident

☐ Urgent ☐ Return Receipt Requested ☐ Sign ☐ Encrypt ☐ Mark Subject Restricted ☐ Expand personal&publi



**Re: 反對 Y/TP/37，擾民, 民居中不適合**  
29/01/2023 09:48

From:

To:

File Ref:

[REDACTED]  
tpbpd@pland.gov.hk

在 2023年1月17日週二 07:52，Ming Leung <[REDACTED]> 寫道：

在 2022年11月8日週二 07:54，Ming Leung <[REDACTED]> 寫道：

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review****參考編號****Reference Number:**

230202-234154-26311

**提交限期****Deadline for submission:**

03/02/2023

**提交日期及時間****Date and time of submission:**

02/02/2023 23:41:54

**有關的規劃申請編號****The application no. to which the comment relates:**

Y/TP/37

**「提意見人」姓名/名稱****Name of person making this comment:**

先生 Mr. Lin Hon Kwong

**意見詳情****Details of the Comment :**

本人返對該處申請改劃及擴建做[靈灰安置所]。現時錦山路該地段是一條雙程的行車通道即双向每邊也只有一條行車道線.交通路面不能應付車輛雙程行車線泊車.本人已留意到多年來該處未經批準的靈灰安置所(長霞淨院)已造成春秋二祭前後該處已有車輛雙程行車線非法泊車,此情況已影響造成有對行車,單車及行人造成危險,為顧及安全交通及行人問題,[靈灰安置所]不應建立於該處,城規會於2020年9月否決長霞淨院申請時亦明確指出,批准該申請會立下不良先例,使鄉郊環境惡化。長霞淨院屬違規骨灰龕場,目前亦未獲私營灰安置所發牌委員會批出經營牌照,我認同城規會一旦批准是次申請,將為「先經營,後申請」的不良經營手法開立極壞先例,並變相鼓勵及縱容其他以此不良手法經營的違規骨灰龕場,破壞鄉郊居住環境,城規會剛於2020年9月已否決同一申請人就長霞淨院提出同樣的改劃申請,當時城規會已明確指出骨灰安置所與現有鄉村不能共存,並沒有有力的理據將上址改作骨灰安置所用途,我認為申請人相隔一段時間後提出一模一樣的方案,根本未能回應上述否決理據,更毫不尊重城規會的決定及程序。在近次重陽節(2022年10月4日)錦山路路邊非法拍車也造成過線行車危險情況。



至各尊敬的

城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
由現有的「鄉村式發展」地帶改劃為「政府，機構或社區 (3)」地帶的改劃申請  
(編號：Y/TP/37)

本人得悉申請人已向城規會就上述申請地點擬議由「鄉村式發展」地帶改劃為「政府，機構或社區 (3)」地帶的規劃申請。經了解了主要發展內容後，本人支持上述申請，因為批准該改劃申請配合適當的土地用途規劃管控「鄉村式發展」未包括的用途，是恰當的做法，其他地域也批過類同的該劃申請，這有助減少用途地帶功能的混亂。同時，該規劃申請與先前改劃申請方案相比，擬議改劃方案清楚劃分自家的出入口及活動範圍欄，是負責任的做法。

專此

簽署：

A handwritten signature in blue ink, appearing to be "陳志強" (Chan Chi Keung), written over a horizontal line.

日期：2023 年 1 月 27 日



至各尊敬的

城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
由現有的「鄉村式發展」地帶改劃為「政府，機構或社區（3）」地帶的改劃申請  
（編號：Y/TP/37）

本人得悉申請人已向城規會就上述申請地點擬議由「鄉村式發展」地帶改劃為「政府，機構或社區（3）」地帶的規劃申請。經了解了主要發展內容後，本人支持上述申請，因為該申請地點位置處於四通八達的公共交通工具服務網絡，包括地鐵、巴士及小巴，是非常方便市民拜祭。同時，該改劃方案會嚴格遵循日後在私營骨置所發牌委員會註冊的管理方案，包括在清明節，重陽節及農曆新年期間不營業和「預約拜訪」機制。就此，透過這些人流管制方案，該改劃申請會有序地營運，不會對錦山村居民帶來不便。

專此

簽署：

日期：2023 年 1 月 27 日



就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

230805-123120-99221

提交限期

Deadline for submission:

25/08/2023

提交日期及時間

Date and time of submission:

05/08/2023 12:31:20

有關的規劃申請編號

The application no. to which the comment relates:

Y/TP/37

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. chan

意見詳情

Details of the Comment :

骨灰安置所與現有鄉村 #不能共存

未售的龕位涉數以億/千萬/百萬元計的高收入

批准申請會立下 #不良先例，造成的連鎖效應會令鄉郊環境惡化

位置太接近民居，若成為正規的私營骨灰龕場，與附近已建成的宗教式建築物續擴張經營，

骨灰龕位「有價有市」，如改變鄉村式發展，

恐會造成的連鎖效應及加劇對居民及地帶的不能逆轉的影響。

申請地點本身與附近村屋是相隔非常近的隔離，影響外觀/樓價/人流/車流/環境等

這不是提供一個獨立出入口，用鐵欄圍封申請範圍，就能改善

因為存在本身是附近村屋是相隔非常近的隔離

(vii) 成為業界及回應社區關注的好典範；

(viii) 能夠提供與周邊住宅發展的最大隔離空間；及

(ix) 就社區對這類設施提供一個有意義的選擇。

倘若「城規會」同意是次改劃申請，所有上列優點都能完全體現出落實政府施政合作夥伴

的精神<<< 以上只會顯示官商勾結，漠視民意／人民的典範

政府容許不停向城規會提出改劃申請，居民要提出幾多次提出反對呢？

還是強要我們接受？

申請者未有任何成功申請，但現已於附近地方進行改建，包括對本區及附近的土地利用、交通、環境、排水、排污、園景、視覺景觀及樓價造成非常大的不良影響，嚴重違反鄉村式發展

請政府嚴謹執法

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review****參考編號****Reference Number:**

230807-211435-96043

**提交限期****Deadline for submission:**

25/08/2023

**提交日期及時間****Date and time of submission:**

07/08/2023 21:14:35

**有關的規劃申請編號****The application no. to which the comment relates:**

Y/TP/37

**「提意見人」姓名/名稱****Name of person making this comment:**

女士 Ms. Chu Man Hei

**意見詳情****Details of the Comment :**

太近民居，影响居民正常生活，特別交通上配套。如果精舍成功改變用途，會做了先例，上面個間長霞淨院就會用此案例嚟做申請的依據。

就規劃申請/覆核提出意見 Making Comment on Planning Application / Review

參考編號

Reference Number:

230814-160750-17157

提交限期

Deadline for submission:

25/08/2023

提交日期及時間

Date and time of submission:

14/08/2023 16:07:50

有關的規劃申請編號

The application no. to which the comment relates:

Y/TP/37

「提意見人」姓名/名稱

Name of person making this comment:

先生 Mr. 楊潤錢

意見詳情

Details of the Comment :

繼2012、2018及2019年後，該申請再度嘗試向城規會提議作骨灰龕場。是次申請內容同以往的申請一樣都是舊調重彈，不斷的重覆改劃申請，浪費公共資源，目的只係拖延政府的執法行動，將以往非法佔用官地規範化，這對守法循規的經營者並不公允。

祥霞精舍方總共有1700個骨灰龕位，如果先人家屬集體於清明、重陽等節日來掃墓，人數無法負荷。再加上一不少人會自駕到上址，容易引發與村民的爭執。

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**Objection to rezoning application Y/TP/37**

14/08/2023 06:05

From:

To:

File Ref:

"tpbpd@pland.gov.hk" &lt;tpbpd@pland.gov.hk&gt;

To whom it may concern.

Dear Sir/ Madam

I am writing to request you to decline the application ( No. Y/TP/37) of rezoning the village type development to institution or community etc, other than its original purpose.

Furthermore, apart from charging each application to a payment of not less than \$10000, I also suggest the same application of rezoning should not be repeated more than once in a year.

Limited government resources have to be allocated to more important and essential aspects such as planning a more spacious, quiet and pollution free HK, not to processing applications of which the ultimate use is against the government's green policy.

Please tell the applicant concerned to treat living people better rather than the deceased ones.

Well beings of living people are the top priority among all in the precious land use in HK.

To conclude, I object to any rezoning applications of the village type development all over hk, let alone beautiful Taipo.

Thank you for your attention.

Regards

A love Taipo resident

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk



**To : Secretary, Town Planning Board**

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

Y/TP/37

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

We wish to reject the proposed planning idea as we feel this will disrupt the neighbourhood.

「提意見人」姓名/名稱 Name of person/company making this comment ANGELA CHUNG

簽署 Signature AL 日期 Date 12/8/23



致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk



**To : Secretary, Town Planning Board**

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By Fax : 2877 0245 or 2522 8426

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有關的規劃申請編號 The application no. to which the comment relates

Y/TP/37

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

上一次幾年前祥榮精舍曾經向政府城規會申請將 Y/TP37 用作放置先人的骨灰後經金村民反對，城規會不批准，因此已結業有幾年，為什麼又再申請作同樣用途，城規會又發信給該村村民，這不是城規會在勞民傷財？真是多此一舉，那個地方除了放骨灰，沒有其他用途嗎？

如果申請人同樣將 Y/TP37 用作放置先人的骨灰，我們當然反對，反對，反對，為什麼呢？因為錦山村一向是住家區是一條寧靜的村落，村民安居樂業，大家認為這地方万万不能放置骨灰，最主要的是交通配套不夠，現在長榮精舍仍在經營我不知道城規會是否已批准經營但是每年清明和重陽節，那条錦山路給那些拜祭的人的車輛將道路塞得水洩不通，這裡的村民出入也很麻煩，還有這裡住的是中國人，住的地方附近最忌有墳場放骨灰盒的靈異場所，像現在長榮精舍一樣，如有先人骨灰盒上位，那些道士的在打齋，發出那些鑼鼓聲，笛聲，在說又多一鬼，我們中國人認為人

「提意見人」姓名/名稱 Name of person/company making this comment \_\_\_\_\_

簽署 Signature

S. Lock

日期 Date

16/08/2023

死了以後就同一只鬼，中國人一向怕鬼（靈魂）人們不是常說“見鬼都怕”嗎？這樣對民的精神、心理、生活造成很嚴重的影響，尤其是对小朋友的弱小心靈，還有更重要的是放置骨灰盒對附近的物業的價值、租售都造成不利影響，請問城規會的官員們，您們會不會買或租這些物業，您們會否提出反對呢？我們再一次提出反對，城規會批准上述地地用作放骨灰盒，希望城規會像上次一樣不批准上述地放骨灰盒，現在香港不是在鬧住屋荒嗎？何不將這些地方規劃做鄉村屋，租或售給住在先廬或割房的人呢？或將這些屋裝成可作圖書館等字樓或貨倉等用途，這樣對村民精神、心理、生活皆不會造成影響，村民可安居樂業，這何樂而不為？最後請城規會諸位官員仍能夠多了解民意！不要官商勾結！！謝謝！！

致城市規劃委員會秘書：

專人送遞或郵遞：香港北角渣華道 333 號北角政府合署 15 樓

傳真：2877 0245 或 2522 8426

電郵：tpbpd@pland.gov.hk



**To : Secretary, Town Planning Board**

By hand or post : 15/F, North Point Government Offices, 333 Java Road, North Point, Hong Kong

By Fax : 2877 0245 or 2522 8426

By e-mail : tpbpd@pland.gov.hk

有關的規劃申請編號 The application no. to which the comment relates

Y/TP/37

意見詳情 (如有需要，請另頁說明)

Details of the Comment (use separate sheet if necessary)

反對這項申請。其申請地地一直是一排民居，左右都是一排的村屋。如果作為放骨灰的地地，一定要遷移民居，不能帶來居民的不適，不論是交通、噪音和空氣質素下降的不良影響。

「提意見人」姓名/名稱 Name of person/company making this comment CHUNG MZI LING

簽署 Signature

日期 Date

Aug, 13, 2023



致

香港北角渣華道 333 號

北角政府合署 15 樓

城市規劃委員會

72

有關祥霞精舍擬議骨灰安置所的改劃申請  
(申請編號:Y/TP/37)

得悉於 2023 年 8 月 14 日，申請人提交交通影響評估報告(修訂版)及已收集的公眾意見作該改劃申請的進一步資料，本人支持該改劃申請，原因如下：

1. 擬議骨灰安置所規模雖然不大，但能夠為本區居民提供必需的社區服務；在春秋二祭期間不開放的安排是完全可以接受的，這方案是足進社會好先例。我/我們支持。☒
2. 申請人提議提供的 1,700 個骨灰位，大多數為夫妻和家庭成員而設，不但可以減少不必要的額外交通流量，有助減低春秋二祭對道路容量壓力，同時，也符合社會部分市民需求，提供多一項選擇。非常支持。☒
3. 倘若獲得城規會批准這次申請會成為在春秋二祭期間不開放骨灰安置所的好例子，令其他龕場及社會大眾逐步接受多元的掃墓模式。對春秋二祭期間的道路交通問題，可以得到改善。我/我們支持此改劃申請。☒
4. 這個申請不但能為原居民提供除土葬外多一個處理家人身後事的選擇，加上位於交通便利的地理位置，居民可以隨時探訪拜祭。我們認同申請人在其提交經修訂的交通影響評估報告，回應了運輸署的進一步意見，證實交通安排方案是可行及不會造成任何額外的交通影響。我/我們期望城規會能儘早批准該改劃申請，讓居民能儘快地安放先人的骨灰。☒
5. 我們支持該改劃申請，因為骨灰安置所不但位於太和火車站附近，亦有其他便捷的公共交通設施，交通非常方便。訪客能使用不同的公共交通工具到達骨灰安置所拜祭先人，比起其他骨灰安置所方便得多。而春秋二祭不對外開放的安排是可以接受的。所以，我/我們支持該改劃申請。☒
6. 擬議改劃相關的「鄉村式發展」地帶為「政府、機構或社區」地帶是合理的做法，也不會影響「鄉村式發展」地帶的需求。該改劃申請計劃是屬於社區需要的設施，為當地提供服務，實行地區化的多元社區設施，我/我們支持。☒
7. 擬議骨灰安置所設有場內守則要求訪客在骨灰安置所內採用微煙拜祭，所有訪客均嚴禁在堂內燃點香燭、大香及燃燒冥鏹，是可以接受的做法。我/我們支持該改劃申請。☒

簽名：

曾嘉茵

日期：

22/8/2023

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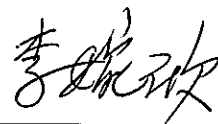
至各尊敬的

城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
由現有的「鄉村式發展」地帶改劃為「政府，機構或社區 (3)」地帶的改劃申請  
(編號：Y/TP/37)

本人是錦山村居民。這個申請不但能為原居民除土葬外提供多一個處理家人身後事的選擇，地點又適當，居民可以隨時探訪拜祭。專此

簽署：



日期：2023 年 8 月 23 日

至各尊敬的

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城市規劃委員會：

有關新界大埔錦山路第 74 至 75 號及毗連政府土地  
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(編號：Y/TP/37)

政府及城規會應該以多元化的社區配套來應對今天社會的需求，既然申請人得到當區民意的大力支持，應當盡快批准，讓該社區設施可以及早回復其功能應對社會需求。專此

簽署：Colvin Yuen

日期：2023 年 8 月 25 日

(3)



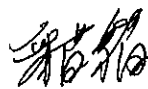
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(編號：Y/TP/37)

政府及城規會不應該一成不變地固執地認為骨灰安置所是一種於「鄉村式發展」地帶的不當用途，如果連選擇都沒有，絕對不是市民之福。我支持此申請。專此

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日期：2023 年 8 月 25 日

至各尊敬的

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倘若能夠執行在春秋二祭期間不開放骨灰安置所的安排，是絕對有助減少當區帶來的人流壓力。我們支持此申請，也支持擬議在春秋二祭不開放的建議方案。專此

簽署： 

日期：2023 年 8 月 24 日

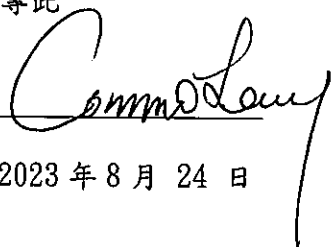
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(編號：Y/TP/37)

申請人提議提供骨灰位，大多數對象是夫妻和家庭成員，是可以減少不必要的額外交通行程，也有助減低春秋二祭對道路容量壓力，得到當區居民大眾也支持，它是值得鼓勵的龕場設施，配合當今社會需要，為市民提供多一項選擇。支持！專此

簽署：



日期：2023 年 8 月 24 日

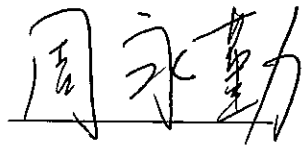
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本人也支持在春秋二祭期間不開放的安排，絕對是先導的做法，也帶給公眾多一項選擇，同時又得到當區居民的支持和接受。我希望城規會儘早批准此申請，讓居民能合法地安放先人的骨灰。專此

簽署：



日期：2023 年 8 月 23 日

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擬議改劃相關的「鄉村式發展」地帶為「政府、機構或社區」地帶是恰當的，也不會影響「鄉村式發展」地帶土地的需求。同時，又得到本區居民的支持，屬可以接受的社區設施，服務當區。所以我支持。專此

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日期：2023 年 8 月 23 日




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我支持這個申請。祥霞精舍現時是不能出售龕位，直至得到規劃許可骨灰辦才處理骨灰安置所牌照手續，令很多有需要的居民都無法在祥霞精舍購買龕位。希望城規會急市民所急，儘早批准這規劃申請，使祥霞精舍繼續服務社區。專此

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日期：2023 年 8 月 22 日

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(編號：Y/TP/37)

我們是大埔區居民。雖然擬議骨灰安置所規模不大，但能夠為本區居民提供必需的社區服務，地點亦便捷；同時，提議在春秋二祭期間不開放，又得到錦山村的支持和接受，是完全可以管控和接受的申請方案，又有發牌機制監督。因此我支持這個規劃申請。專此

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日期：2023 年 8 月 22 日

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申請中的的骨灰安置所在春秋二祭期間禁止開放，平日都有實行人流管制措施，訪客需事先預約才可以進入骨灰龕殿，理順了擠迫的情況，又得到近鄰的支持，以及錦山村的支持，體現出區內的接受性和包容性，是難得的申請個案，非常值得支持。專此

簽署: Helen Lee

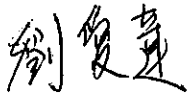
日期: 2023 年 8 月 22 日

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支持這個申請。現時政府已在興建或規劃的骨灰安置所都在偏遠地區，例如小蠔灣、沙嶺等。該申請地點位置處於四通八達的公共交通工具服務網絡，是非常方便市民拜祭，同時，也有助減輕其他地區對該等社區設施的壓力。春秋二祭不開放也有助提供市民多一項選擇，是多元社會的考慮。專此

簽署： 

日期：2023 年 8 月 22 日

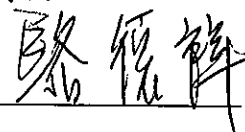
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申請地點大部分申請土地是私人地段，申請人也承諾把有部分靈灰位向錦山村原住民提供 50 以當時公營龕位出售價之平均值的雙灰位予錦山村原住民，是對錦山村原居民的貢獻社區，同時，也有助減少傳統土葬方式，是多贏的，值得支持。專此

簽署：



日期：2023 年 8 月 21 日



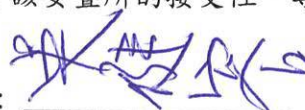
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支持。今時今日的骨灰安置所，有發牌監管，是屬於寧靜，清潔，及社會必須的設施。申請人提交錦山村村民（包括申請地點的鄰里）支持，反映了該安置所的接受性。專此

簽署：



日期：2023 年 8 月 21 日

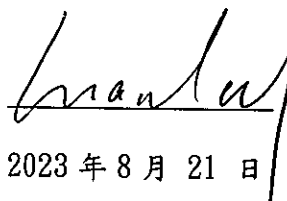
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(編號：Y/TP/37)

祥霞精舍位於錦山路旁，地理位置很好，況且有很多公共交通工具均可到達，俱備完善人行道系統，方便錦山村村民的同時，也方便其他使用者。

專此

簽署:   
日期: 2023 年 8 月 21 日

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(編號：Y/TP/37)

支持批准這個申請。現今香港的骨灰龕位供不應求，而骨灰龕一直都是社區的必要設施之一。祥霞精舍可幫助減輕當今社會對私營骨灰安置所的需求壓力，分擔地區性對該等設施的壓力，加上又得到錦山村的居民支持，特別是其左鄰右里的支持，也能為市民提供多一項選擇。專此

簽署： pat Ho

日期：2023 年 8 月 21 日

致：城規會秘書處

有關：強烈反對 Y/TP/37 的規劃申請

各界關注骨灰龕法案大聯盟強烈反對上述的規劃申請，理由如下：

1. 該申請曾以 Y/TP/18 及 Y/TP/27 編號提出同一申請，分別於 2013.11.08 及 2020.05.26 兩度被城規會否決，而有否決的基本原因是有關規劃不符合規劃原意及交通影響，亦太近民居。然而，申請人今次提交的申請表上並沒有提及過去被否決的紀錄。現時錦山村除申請人的祥霞精舍違規龕場外，**還有長霞淨苑，該申請亦被城規會以有關申請不符合規劃原意理由之一被否決**。大聯盟再次重申，城規會需要留意上訴法庭否決元朗新田老圍村的明月山的司法覆核一案，上訴庭法官清楚指出，骨灰安置所並不符合鄉村式發展的規劃用途的性質，明月山即使提出上訴，最後也被上訴法院判其敗訴，因此，大聯盟認為城規會必須緊守此基線，不要立下不良先例，屆時會引發更多更大的爭議。
2. 在錦山一帶已有 6 個截算前的私營骨灰安置所，靈顯法院已獲城規會批准改變土地用途，祥霞精舍及長霞淨苑座落在錦山村內，兩處所都多次被城規會拒絕有關申請，由於太近民居，當年有草擬有關法案時，前食衛局副局長梁卓偉及前局長高永文醫生皆有提及私營骨灰安置所不宜太接近民居，以致位於紅磡區的私營龕場大部份皆被拒絕發牌。祥霞精舍錦山村內，與民居毗鄰，實不宜將用途改作龕場。
3. 在錦山村已有兩個龕場(包括祥霞精舍)屬截算前骨灰安置所，兩該區總共有 6 個截算前骨灰安置所需要申請改變土地用途作龕場之用，其中靈顯法院已獲城規會批准，因此，該區違規龕場林立，有部份都遠離民居，申請人聲稱在當區設立私營龕場有助錦山村原居民的需要，不過，原居民已有認可葬區的安排，若需要骨灰龕位，區內及公營亦有龕位可以供應。



龕場名稱	地址	骨灰份數 (骨灰所辦資料)	骨灰龕位數目 (城規會資料)	紀念牌位數目 (城規會資料)
長霞淨院	丈量約份第 6 約地段第 1087 號及第 1130 號及毗鄰政府土地	22,556	11,726	3,049
半春園	丈量約份第 6 約地段第 1119A 分段(部份)及第 1260A 分段(部份)	3,922	--	--
小靈山(靈顯法院)地藏殿壁龕	(靈顯法院)內地藏殿及壁龕(丈量約份 6 約地段編號 1171)	813	--	--
祥霞精舍	丈量約份第 6 約地段第 738 號 C 分段及 738 號 C 分段第 1 小分段	3,860	1,700	--
常寂園	丈量約份第 5 約地段 136 號餘段(部分)及 138 號餘段(部分)及毗鄰政府土地	1,526	--	--
般若精舍	丈量約份第 5 約地段第 1006 號餘段(部分)	5,893	--	--
		38,570		

4. 今次規劃的申請中，申請人聲稱獲邱氏成員理解及支持，但聯盟請委員注意，但邱氏成員共有 3 房，一房在林村，一房在大埔，另一房在錦山；據大聯盟的了解林村及大埔兩房皆強烈反對有關項目(因祥霞精舍就位於邱氏祠堂旁)，另一方面，邱氏祖堂地的司理的選舉身份仍被受爭取，因此，申請人聲稱邱氏支持有關項目的申請，只是以偏蓋全。而且至今邱氏祖堂因在委任那房人做司理出現爭議，至今該祖堂仍未有司理，因此，錦山一房不能代表整條村的意願。

5. 申請人在申請文件中表示在 2022 年 5 月 26 日的城規會議審議 Y/TP/27 的會議紀錄，表示有意份委員支持有關申請，但申請人並沒有提及更多委員認為：

//27. 一名委員不支持這宗改劃申請，理由是申請地點的靈灰安置用途與周圍環境不協調。副主席表示贊同，並稱批准這宗申請會為緊鄰申請地點南面的申請(編號 Y/TP/29)及其他同類申請立下不良先例。該名不支持這宗申請的委員進一步表示，由於附近的同類改劃申請(編號 Y/TP/29)涉及更大的範圍，拒絕理由 (a)項指申請地點屬「零碎」改劃的用字可能不太合適，應適當修改。另一名委員贊同批准這宗改劃申請會為其他同類申請立下不良先例，亦認為並無劃增益可合理支持目前的建議。

29. 一名委員認同靈灰安置用途的需求與日俱增，但認為這宗申請的靈灰安置所過於接近住宅民居，故不支持這宗改劃申請。//

經委員商議後決定否決有關申請，理由：

33. 經進一步商議後，小組委員會決定不同意這宗申請，理由如下：「(a)申請地點位於劃為「鄉村式發展」地帶的範圍內，該地帶的規劃意向主要是供原居村民發展小型屋宇。擬議的靈灰安置用途與該區現有的鄉村環境不協調，尤緊鄰擬議用途西面和南面的住宅民居為甚。此外，亦沒有有力的規劃理據支持把申地點由「鄉村式發展」地帶改劃為「政府、機構或社區(3)」地帶，以便就靈灰安置用途提出申請。申請地點目前劃作「鄉村式發展」地帶，實屬恰當；以及(b)批准這宗申請會為「鄉村式發展」地帶內的其他同類改劃申請立下不良



先例。倘批准該等同類申請，累積影響所及，會導致零散的靈灰安置所發展，繼而引致該區的鄉村環境轉差和整體環境質素下降。」

而今次申請人的規劃申請的規模及理據沒有太大的改變下，聯盟認為城規會應尊重 2022.05.26 的審議決定，反對有關申請。

6. 聯盟請委員注意，有關項目屬違規發展，申請人聲稱自 2007 年開始申請，但一直被當村的村民反對，在 2022 年 5 月 26 日的會議上已有委員提問錦山村的人口問題，根據回覆，現時錦山村有超過 5 千人口，而且附近仍有多個骨灰龕場的規範化申請，包括長覆淨覆，般若精舍，常寂園，半春園等等，一旦仍獲，將帶出不良的先例。

7. 根據元朗明月山一案的判決書(CACV 231/2011)，第 19 及 20 點

19. *In the proper construction of the words used in an OZP, one is not restricted only to the plan or the notes. One should also have regard to the explanatory statement, although it does not form part of the plan : Henderson Real Estate Agency Ltd v Lo Chai Wan [1997] HKLRD*

*258, 267A-E. Paragraph 9.4.1 of the Explanatory Statement repeats the planning intention for the V zone. Paragraph 9.4.2 adds that the boundaries of a V zone are drawn up "having regard to the existing village 'environs', outstanding Small House demands for the next ten years, topography and site constraints".*

*20. This, in my view, reinforces my earlier point that the operation of the Columbarium on the Lots is wholly incompatible with the planning intention of the V zoning. In particular, it is quite inconsistent with the preservation of and respect for the existing village environs – given the kind and scale of activities that the Columbarium would generate. As counsel for the Director submitted, the planning intention here is for the preservation of the village settings and the controlled development, appropriate to the rural environment, of the designated village related areas. Small-scale structures such as shrines and ancestral halls are very much part of the rural village scene. But not so commercial, large-scale columbaria which are markedly out of the character with such environment.*

第 19 點：需要「考慮到現有村莊的“環境”，未來十年對小型房屋的需求，地形和場地限制」

第 20 點：雖然小型神龕或祠堂可以視為鄉村的一部份，但以商業營運的大型骨灰龕場並不符合鄉村的規劃原則。

因此在鄉村式發展地帶以商業營運大型骨灰龕場並不符合鄉村的規劃原則，一旦獲批，將會成為不良先例。

基於以上原因，大聯盟強烈反對 Y/TP/37 的改劃申請。

各界關注骨灰龕法案大聯盟謹啟  
2023.08.24

**就規劃申請/覆核提出意見 Making Comment on Planning Application / Review**

參考編號

**Reference Number:**

230825-231436-31755

提交限期

**Deadline for submission:**

25/08/2023

提交日期及時間

**Date and time of submission:**

25/08/2023 23:14:36

有關的規劃申請編號

**The application no. to which the comment relates:**

Y/TP/37

「提意見人」姓名/名稱

**Name of person making this comment:**

先生 Mr. C Chiu

意見詳情

**Details of the Comment :**

反對理由是影響地區原有的寧靜，增加交通負荷，人流變得複雜，治安環境衛生方面惡化，引發山火的可能